



CONSTRUCTION PLANS FOR

PINO YARDS INTERIM

DRAINAGE IMPROVEMENTS

DRAWING INDEX

SHEET TITLE

COVER SHEET

PLAN VIEW

CROSS-SECTIONS AND DETAILS

SPILLWAY PLAN AND SECTIONS

SHEET NO.

1

2

3

4

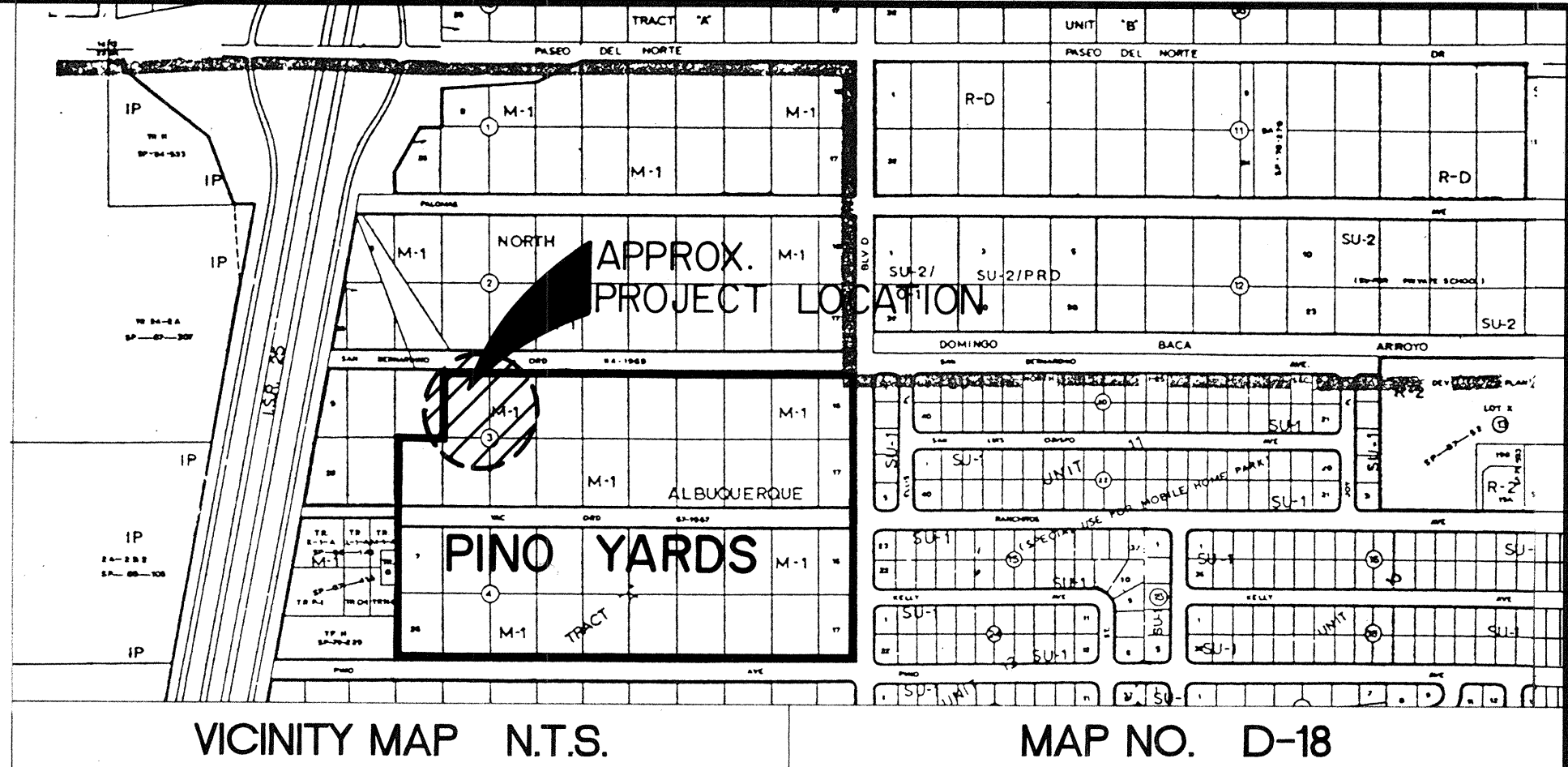
DRAINAGE DESIGN NOTE:

DRAINAGE DESIGN IS FOR EXISTING CONDITION FLOW

AND IS NOT INTENDED FOR FUTURE SITE DEVELOPMENT,

DESIGN INTENT IS FOR DRAINAGE COLLECTION

AND OIL SEPARATION



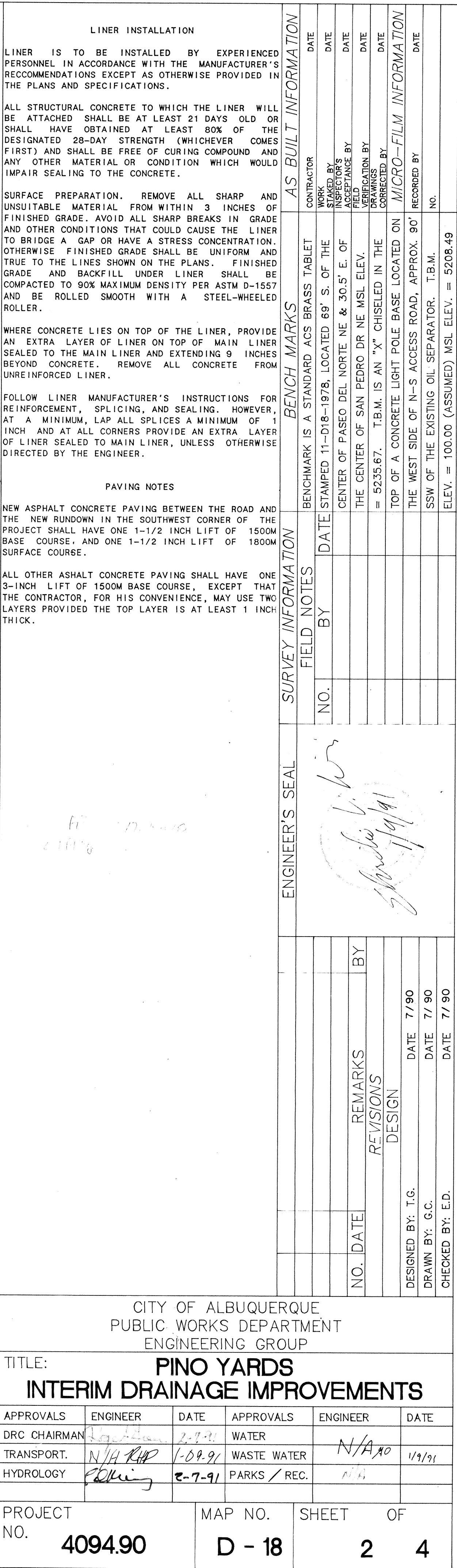
VICINITY MAP N.T.S.

MAP NO. D-18

GENERAL NOTES

- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREIN, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (1986) INCLUDING CURRENT UPDATES (MAY ALSO BE REFERRED TO AS THE STANDARD SPECIFICATIONS).
- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT LINE LOCATING SERVICE, 260-1990, FOR LOCATION OF EXISTING UTILITIES.
- ALL UTILITIES SHOWN AND ANY OTHER UTILITIES THAT ARE NOT SHOWN ON THE PLANS BUT ARE WITHIN THE CONSTRUCTION AREA SHALL BE LOCATED, IDENTIFIED AND PROTECTED BY THE CONTRACTOR AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS SHOWN ON DRAWINGS PRIOR TO STARTING WORK AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR INCONSISTENCIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH MINIMUM AMOUNT OF DELAY.
- ALL WORK SHALL BE DONE BY QUALIFIED PERSONNEL IN AN APPROVED STANDARD OF PRACTICE.
- THE COST OF ADDITIONAL DESIGN WORK DUE TO ERRORS AND OMISSIONS IN CONSTRUCTION SHALL BE BORNE BY THE CONTRACTOR.
- SURVEY MONUMENTS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE ENGINEER AND SHALL NOTIFY THE ENGINEER AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE ENGINEER. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATION OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE SPECIFICATION.
- CONCRETE. CONCRETE FOR THIS PROJECT SHALL BE AS GIVEN IN THE STANDARD SPECIFICATIONS AND THE SUPPLEMENTAL TECHNICAL SPECIFICATIONS.
- COMPACTION. COMPACTION UNDER AND BEHIND PAVING AND CONCRETE STRUCTURES SHALL BE 95% OF MAXIMUM DRY DENSITY PER ASTM D-1557 AND SHALL EXTEND 2 FEET BEYOND THE STRUCTURES OR THE EDGE OF PAVING EXCEPT THAT COMPACTION ADJACENT TO THE OIL SEPARATOR SIDEWALLS SHALL BE AT 90%. COMPACTION UNDER THE POND LINER SHALL BE 90%.
- LAYOUT. THE PROJECT IS DESIGNED TO BE LAID OUT BY MEASUREMENTS FROM EXISTING FENCE LINES RATHER THAN BY BEARINGS AND DISTANCES.
- ELEVATIONS. ELEVATIONS SHOWN ARE BASED ON THE ASSUMED TBM ELEVATION OF 100.00.
- STORED MATERIAL. THE CITY OF ALBUQUERQUE SHALL REMOVE STORED MATERIAL FROM THE CONSTRUCTION AREA PRIOR TO THE START OF CONSTRUCTION.
- DRAINAGE. THE CONTRACTOR SHALL ENSURE THAT ALL PERMANENT WORK IS CARRIED ON IN AREAS FREE FROM WATER. SEE THE SUPPLEMENTAL TECHNICAL SPECIFICATIONS.
- EROSION. THE CONTRACTOR SHALL INSURE THAT HIS ACTIVITIES DO NOT PRODUCE EROSION ONTO OR ON PUBLIC OR PRIVATE PROPERTY, INCLUDING STREETS, ROADS, AND ARROYOS.
- PAVEMENT REMOVAL. THE PAVING TO BE REMOVED UNDER THIS PROJECT HAS GENERALLY BEEN CONTAMINATED WITH ENGINE AND HYDRAULIC OIL. THE CONTRACTOR SHALL DISPOSE OF THE ASPHALT IN A EID PERMITTED LANDFILL.
- MAINTENANCE. THE OWNER IS RESPONSIBLE FOR MAINTAINING THE OIL/WATER SEPARATOR IN GOOD CONDITION AND REMOVING OIL AND GRIT. AT A MINIMUM THE SEPARATOR SHOULD BE CLEANED IN MAY AND NOVEMBER EACH YEAR AND AFTER EVERY MAJOR STORM. THE SEPARATOR IS DESIGNED TO BE CLEANED FROM OUTSIDE THE PONDING AREA BY USE OF A TRUCK-MOUNTED VACUUM CLEANER (VACTOR).
- CONSTRUCTION SIGNS. NO CONSTRUCTION SIGNS ARE REQUIRED FOR THIS PROJECT UNDER SPECIAL PROVISIONS SECTION 3.
- PAVEMENT REMOVAL DOCUMENTATION. THE CONTRACTOR WILL SUPPLY THE CITY WITH WRITTEN PROOF DOCUMENTING THE QUANTITY OF ASPHALT REMOVED AND DELIVERED TO THE EID PERMITTED LANDFILL, THE TIME OF DELIVERY, AND THE LOCATION FROM WHICH IT WAS REMOVED. THE CONTRACTOR WILL NOTIFY THE PROJECT MANAGER BEFORE THE ASPHALT IS HAULED OFFSITE.

REV.	SHEETS	CITY ENGR.	DATE	DATE	DATE
APPROVAL OF REVISIONS					
PREPARED BY: <b>RESOURCE TECHNOLOGY INCORPORATED</b>		APPROVED FOR CONSTRUCTION <i>[Signature]</i> 2-7-91			
PROJECT NO. <b>4094.90</b>		SHEET <b>1</b> OF <b>4</b>			

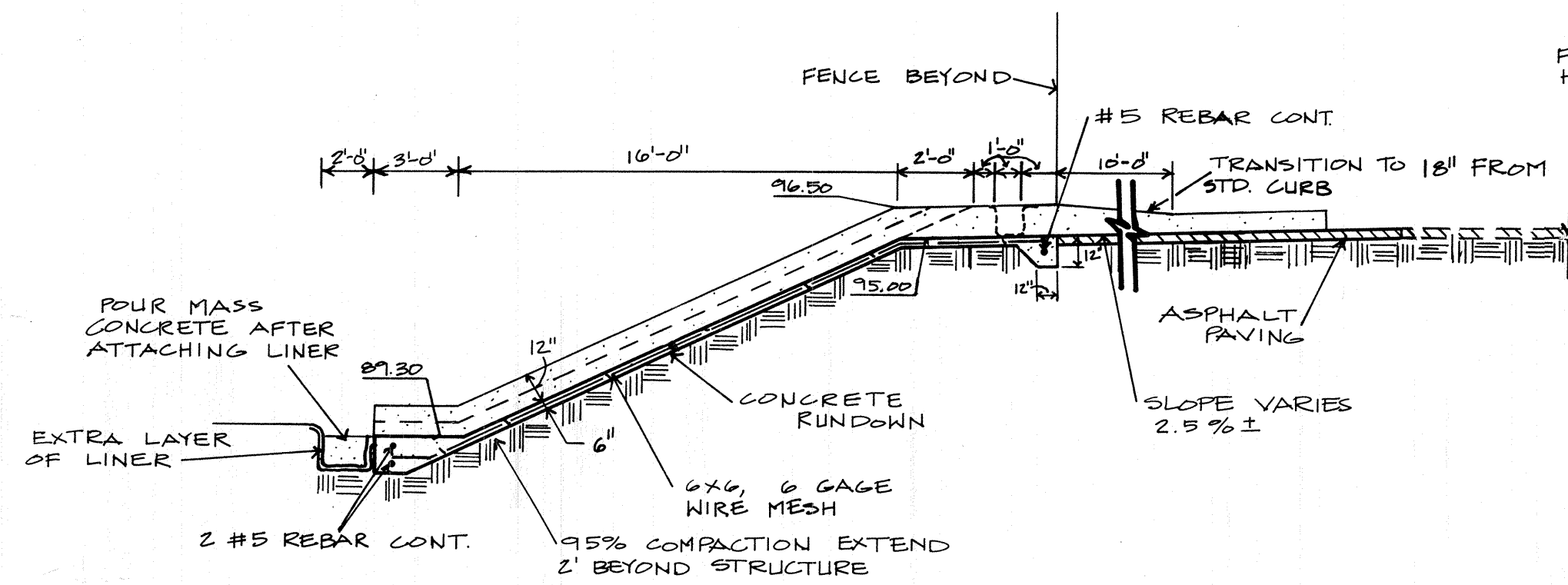




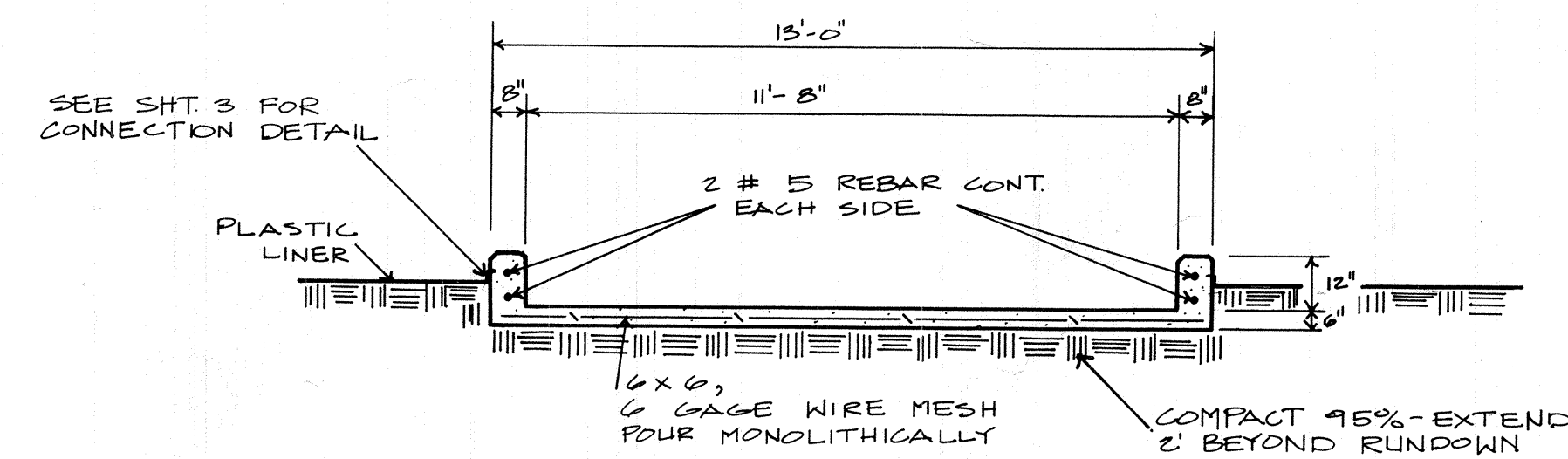
LINER INSTALLATION											
<p>LINER IS TO BE INSTALLED BY EXPERIENCED PERSONNEL IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS EXCEPT AS OTHERWISE PROVIDED IN THE PLANS AND SPECIFICATIONS.</p> <p>ALL STRUCTURAL CONCRETE TO WHICH THE LINER WILL BE ATTACHED SHALL BE AT LEAST 21 DAYS OLD OR SHALL HAVE OBTAINED AT LEAST 80% OF THE DESIGNATED 28-DAY STRENGTH (WHICHEVER COMES FIRST) AND SHALL BE FREE OF CURING COMPOUND AND ANY OTHER MATERIAL OR CONDITION WHICH WOULD IMPAIR SEALING TO THE CONCRETE.</p> <p>SURFACE PREPARATION. REMOVE ALL SHARP AND UNSUITABLE MATERIAL FROM WITHIN 3 INCHES OF FINISHED GRADE. AVOID ALL SHARP BREAKS IN GRADE AND OTHER CONDITIONS THAT COULD CAUSE THE LINER TO BRIDGE A GAP OR HAVE A STRESS CONCENTRATION. OTHERWISE FINISHED GRADE SHALL BE UNIFORM AND TRUE TO THE LINES SHOWN ON THE PLANS. FINISHED GRADE AND BACKFILL UNDER LINER SHALL BE COMPACTED TO 90% MAXIMUM DENSITY PER ASTM D-1557 AND BE ROLLED SMOOTH WITH A STEEL-WHEELED ROLLER.</p> <p>WHERE CONCRETE LIES ON TOP OF THE LINER, PROVIDE AN EXTRA LAYER OF LINER ON TOP OF MAIN LINER SEALED TO THE MAIN LINER AND EXTENDING 9 INCHES BEYOND CONCRETE. REMOVE ALL CONCRETE FROM UNREINFORCED LINER.</p> <p>FOLLOW LINER MANUFACTURER'S INSTRUCTIONS FOR REINFORCEMENT, SPlicing, AND SEALING. HOWEVER, AT A MINIMUM, LAP ALL SPLICES A MINIMUM OF 1 INCH AND AT ALL CORNERS PROVIDE AN EXTRA LAYER OF LINER SEALED TO MAIN LINER, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.</p> <p>PAVING NOTES</p> <p>NEW ASPHALT CONCRETE PAVING BETWEEN THE ROAD AND THE NEW RUNDOWN IN THE SOUTHWEST CORNER OF THE PROJECT SHALL HAVE ONE 1-1/2 INCH LIFT OF 1500M BASE COURSE AND ONE 1-1/2 INCH LIFT OF 1800M SURFACE COURSE.</p> <p>ALL OTHER ASHALT CONCRETE PAVING SHALL HAVE ONE 3-INCH LIFT OF 1500M BASE COURSE, EXCEPT THAT THE CONTRACTOR, FOR HIS CONVENIENCE, MAY USE TWO LAYERS PROVIDED THE TOP LAYER IS AT LEAST 1 INCH THICK.</p> <p>ALL EXPOSED CONCRETE EDGES TO BE CHAMFERED PER STANDARD SPECIFICATIONS.</p>						AS BUILT INFORMATION					
						CONTRACTOR	DATE	INSPECTOR'S	DATE	FIELD OFFICE BY	DATE
						VERIFICATION BY	DATE	CHECKED BY	DATE	MICRO-FILM INFORMATION	DATE
						RECORDED BY	DATE	NO.			
						BENCH MARKS					
SURVEY INFORMATION											
FIELD NOTES											
NO.	BY	DATE									
ENGINEER'S SEAL											
<div><div></div><div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></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# RUNDOWN DETAILS



SECTION C-C  
N.T.S.



SECTION D-D  
SCALE 1"=3'

ACCESS SIGN TO BE MADE OF 3/4" MARINE PLYWOOD OR 1/8" ALUMINUM

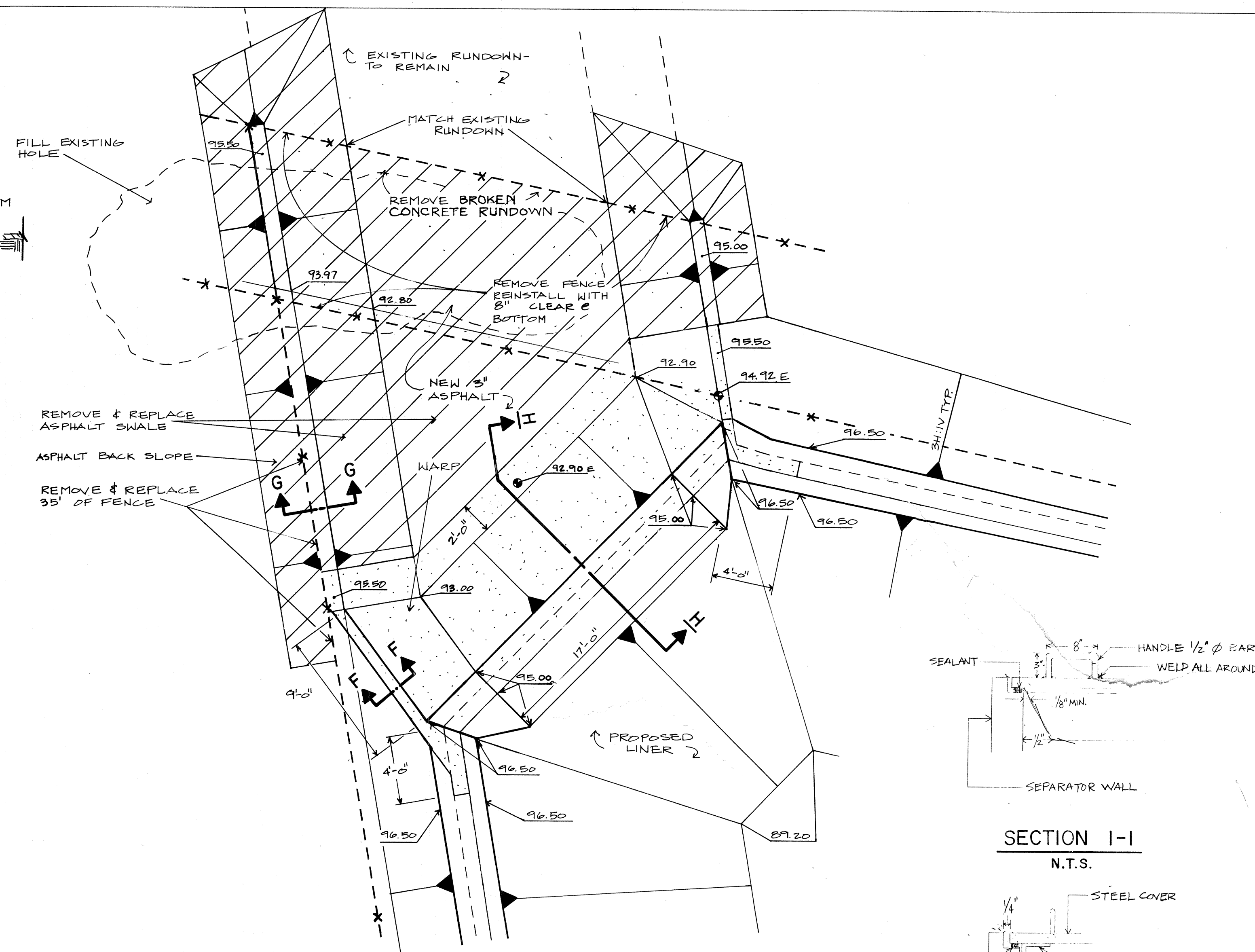
**CAUTION**  
DO NOT DRIVE  
ON POND LINER

ACCESS SIGN  
N.T.S.

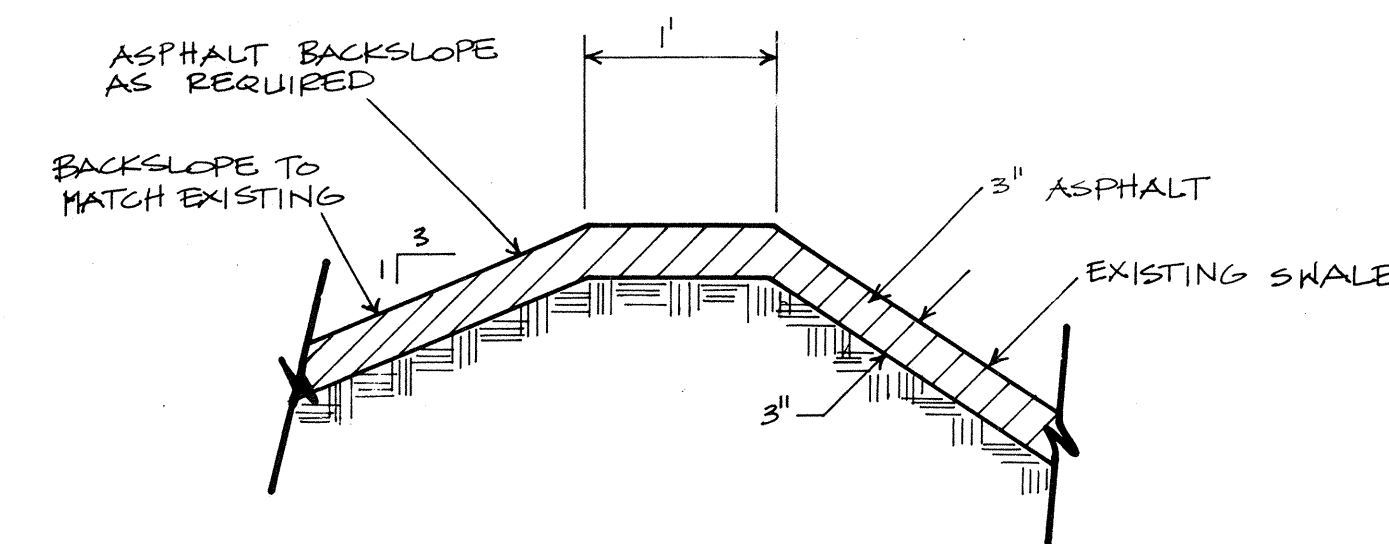
MAINTENANCE SIGN TO BE MADE OF 3/4" MARINE PLYWOOD OR 1/8" ALUMINUM

**NOTICE**  
CLEAN OIL SEPARATOR  
AND GRIT CHAMBER EACH  
MAY AND NOVEMBER, AND  
AFTER EACH MAJOR STORM

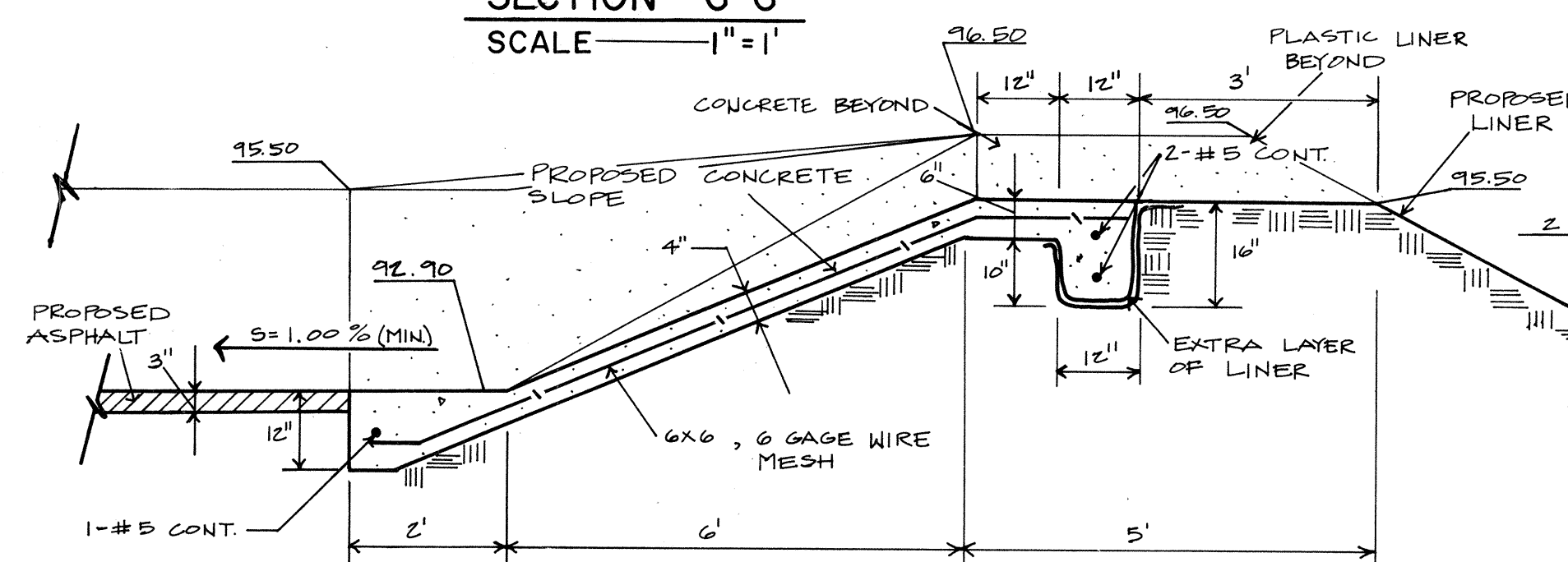
MAINTENANCE SIGN  
N.T.S.



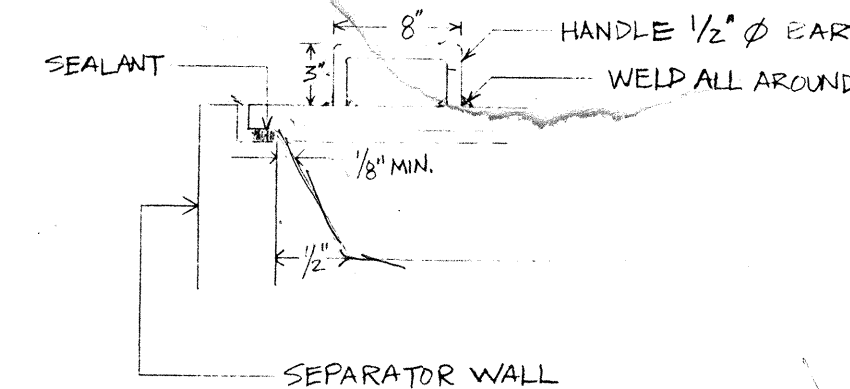
SPILLWAY PLAN VIEW  
SCALE 1"=5'



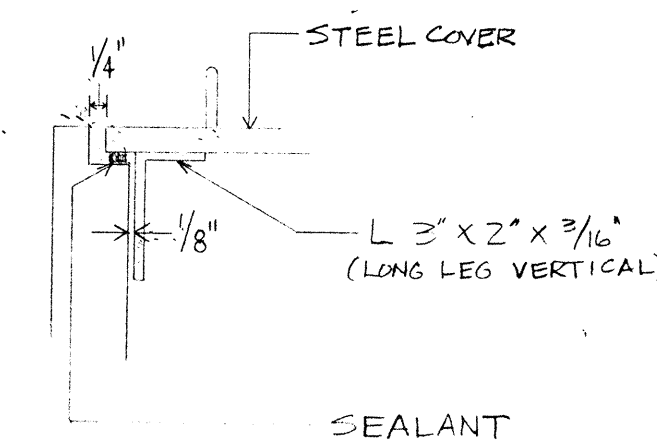
SECTION G-G  
SCALE 1"=1'



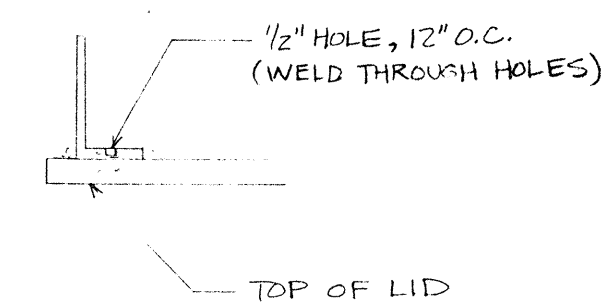
SECTION H-H  
SCALE 1"=2'



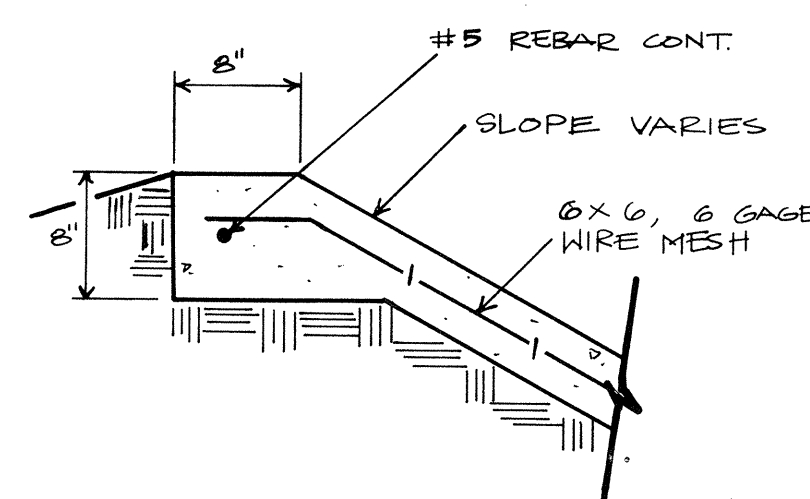
SECTION I-I  
N.T.S.



SECTION J-J  
N.T.S.



WELD DETAIL  
N.T.S.



SECTION F-F  
SCALE 1"=1'

CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING GROUP					
TITLE: <b>PINO YARDS INTERIM DRAINAGE IMPROVEMENTS</b>					
APPROVALS	ENGINEER	DATE	APPROVALS	ENGINEER	DATE
DRG CHAIRMAN	<i>[Signature]</i>	2-7-91	WATER	<i>[Signature]</i>	1/9/91
TRANSPORT.	<i>[Signature]</i>	1-6-91	WASTE WATER	<i>[Signature]</i>	1/9/91
HYDROLOGY	<i>[Signature]</i>	2-7-91	PARKS/REC.	<i>[Signature]</i>	1/9/91
PROJECT NO. <b>4094.90</b>		MAP NO. <b>D-18</b>		SHEET <b>4</b> OF <b>4</b>	

AS BUILT INFORMATION		BENCH MARKS		SURVEY INFORMATION		FIELD NOTES		ENGINEER'S SEAL		REVISIONS		DESIGN	
CONTRACTOR	DATE	WORK	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE

LINER INSTALLATION  
 LINER IS TO BE INSTALLED BY EXPERIENCED PERSONNEL IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS EXCEPT AS OTHERWISE PROVIDED IN THE PLANS AND SPECIFICATIONS.  
 ALL STRUCTURAL CONCRETE TO WHICH THE LINER WILL BE ATTACHED SHALL BE AT LEAST 21 DAYS OLD OR SHALL HAVE OBTAINED AT LEAST 80% OF THE DESIGNATED 28-DAY STRENGTH (WHICHEVER COMES FIRST) AND SHALL BE FREE OF CURING COMPOUND AND ANY OTHER MATERIAL OR CONDITION WHICH WOULD IMPAIR SEALING TO THE CONCRETE.  
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 WHERE CONCRETE LIES ON TOP OF THE LINER, PROVIDE AN EXTRA LAYER OF LINER ON TOP OF MAIN LINER SEALED TO THE MAIN LINER AND EXTENDING 9 INCHES BEYOND CONCRETE. REMOVE ALL CONCRETE FROM UNREINFORCED LINER.  
 FOLLOW LINER MANUFACTURER'S INSTRUCTIONS FOR REINFORCEMENT, SPLICING, AND SEALING. HOWEVER, AT A MINIMUM, LAP ALL SPLICES A MINIMUM OF 1 INCH AND AT ALL CORNERS PROVIDE AN EXTRA LAYER OF LINER SEALED TO MAIN LINER, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.  
 PAVING NOTES  
 NEW ASPHALT CONCRETE PAVING BETWEEN THE ROAD AND THE NEW RUNDOWN IN THE SOUTHWEST CORNER OF THE PROJECT SHALL HAVE ONE 1-1/2 INCH LIFT OF 1500M BASE COURSE AND ONE 1-1/2 INCH LIFT OF 1800M SURFACE COURSE.  
 ALL OTHER ASPHALT CONCRETE PAVING SHALL HAVE ONE 3-INCH LIFT OF 1500M BASE COURSE, EXCEPT THAT THE CONTRACTOR, FOR HIS CONVENIENCE, MAY USE TWO LAYERS PROVIDED THE TOP LAYER IS AT LEAST 1 INCH THICK.