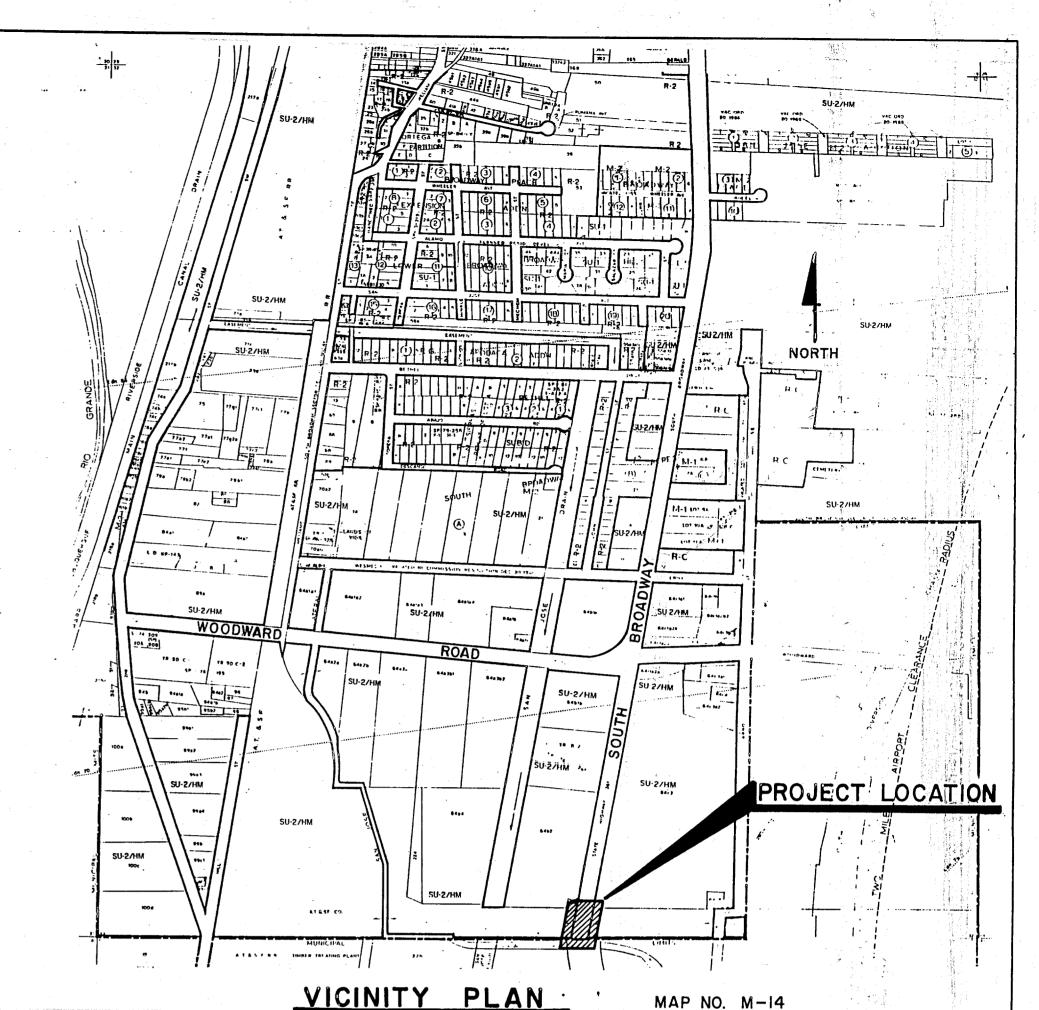


CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING GROUP

BROADWAY RAIL CROSSING SOUTH OF WOODWARD ROAD

CHIEF CONSTRUCTION ENGINEER DATE 4-30-99

COVER SHEET									
SITE AND GRADING	PLAN								
DETAILS									
STRIPING PLAN									
TRAFFIC CONTROL									
TRAFFIC CONTROL									



2. TWO WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) AND

PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE SHALL NOTIFY THE ENGINEER OR THE SURVEYOR SO THAT THE

CONSTRUCTION LIMITS SHALL BE REMOVED, STORED AND RE-SET

5. THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7), PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITION OF THE STANDARD

6. FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION CO-ORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER

8. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI

8A. VERTICAL TRANSITION LEVELING COURSE SHALL BE TEMPORARILY INSTALLED FROM EXISTING ASPHALT APPROACHES TO P.C.C. TRANSITION SLAB.

11. CONTRACTOR SHALL NOT BE HELD RESPONSIBLE FOR DELAYS CAUSED CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT TRANSPORTATION DEVELOPMENT TITLE: BROADWAY RAIL CROSSING

WEATHER DOES NOT PERMIT.

10. ALL PHASING SHALL CONTINUE UNINTERRTUPTED(A THROUGH F)UNLESS

ITS ASSIGNS, SHALL REMOVE THE RAIL SPUR, TO INCLUDE BALLAST. THERE SHALL BE TOTAL

CONTRACTOR SHALL CONSTRUCT AND COMPACT SIX (6) INCHES OF ASPHALT TREATED BASE (ATB)

BURLINGTON NORTHERN RAILWAY COMPANY, OR ITS ASSIGNS, SHALL REPLACE BALLAST AND

INSTALLATION OF THE OMNI CONCRETE SYSTEM. THERE SHALL BE TOTAL CLOSURE OF THE ROADWAY

REMOVE EXISTING ROADWAY AND THE CONTRACTOR SHALL CONSTRUCT REINFORCED CONCRETE APPROACH SLABS, TO INCLUDE FOUR (4)

SHALL INFORM ORLANDO GARCIA, NMSHTD, 841-2700

PHASE. WORKING HOURS SHALL BE 7 AMTO 7 PM

RAILROAD CROSSING LEGEND, BOTH SIDES, UPON COMPLETION OF PHASE E. ONE LANE OF TRAFFIC EACH DIRECTION SHALL BE MAINTAINED THIS PHS

THIS PHASE: NMSHOT
CONTRACTOR SHALL INSTALL HOT PLASTIC

WORKING HOURS SHALL BE 7 AM TO 7 PM THE PHASE.

BNRC SHALL INFORM ART KELLY, 768-2768.

RELAY RAIL TO EXISTING ELEVATIONS, TO INCLUDE

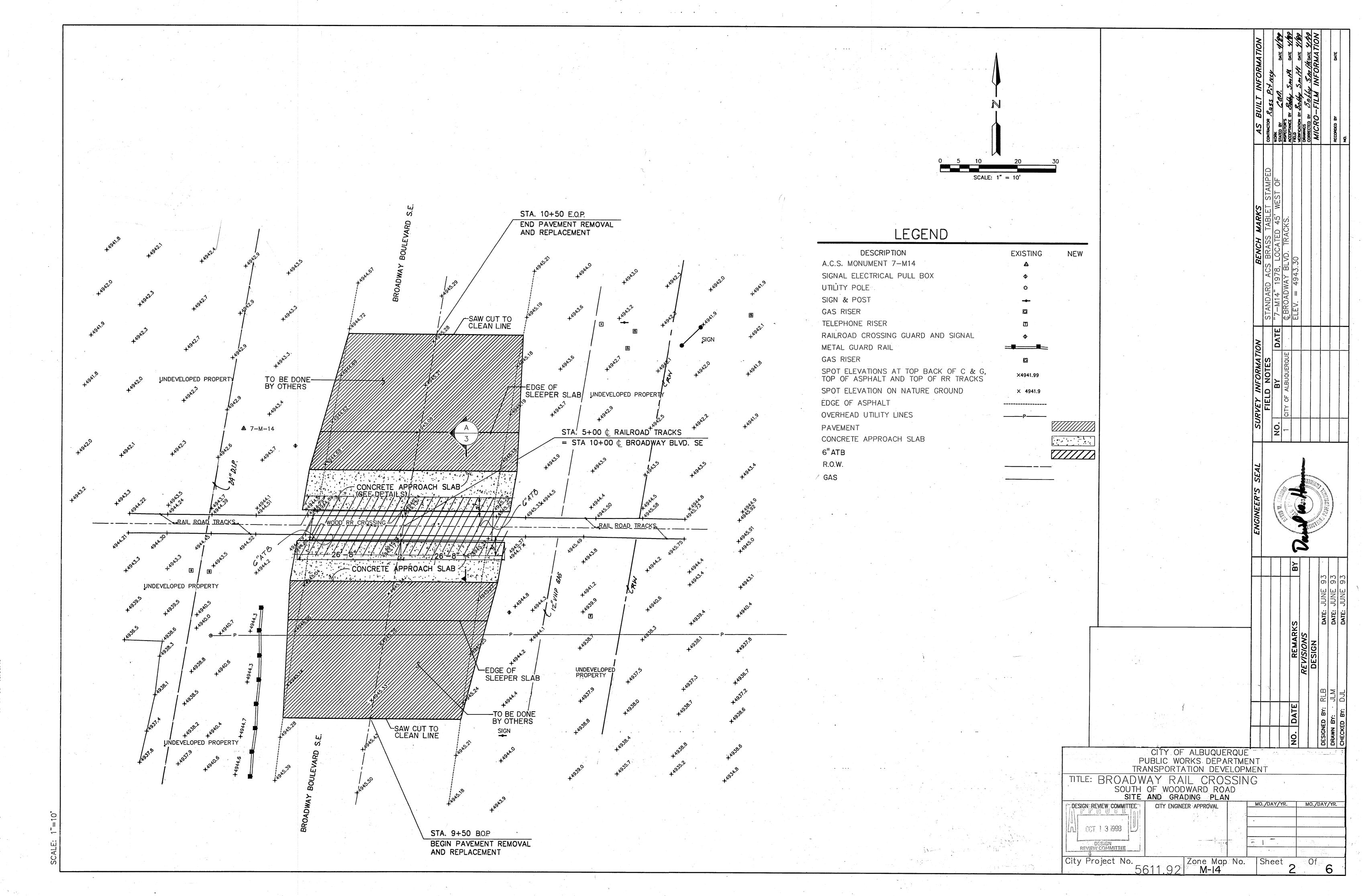
ROADWAY FOR THIS PHASE.

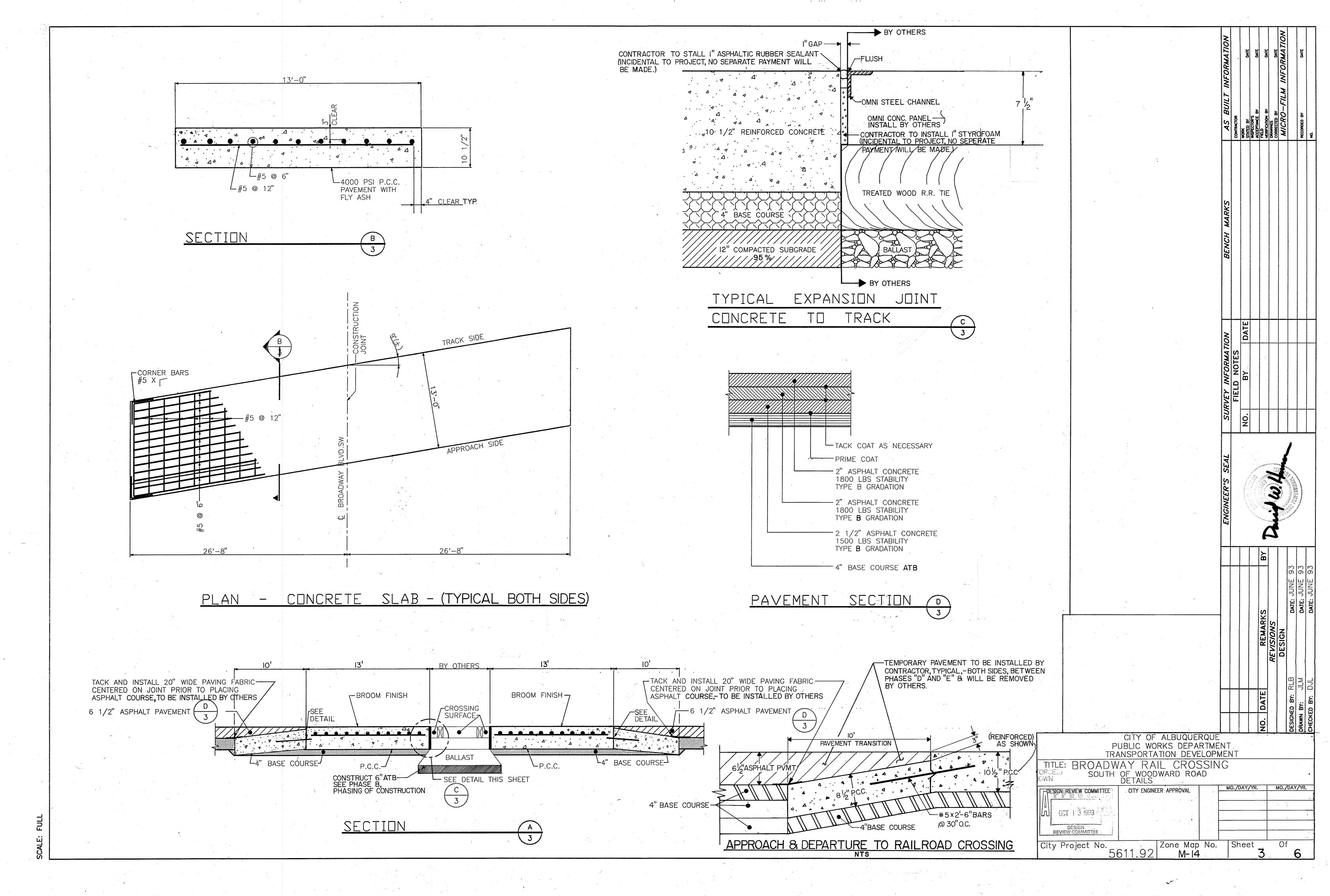
IN THE BALLAST FOOTING AREA AS SHOWN ON THE PLANS. THERE SHALL BE TOTAL CLOSURE OF

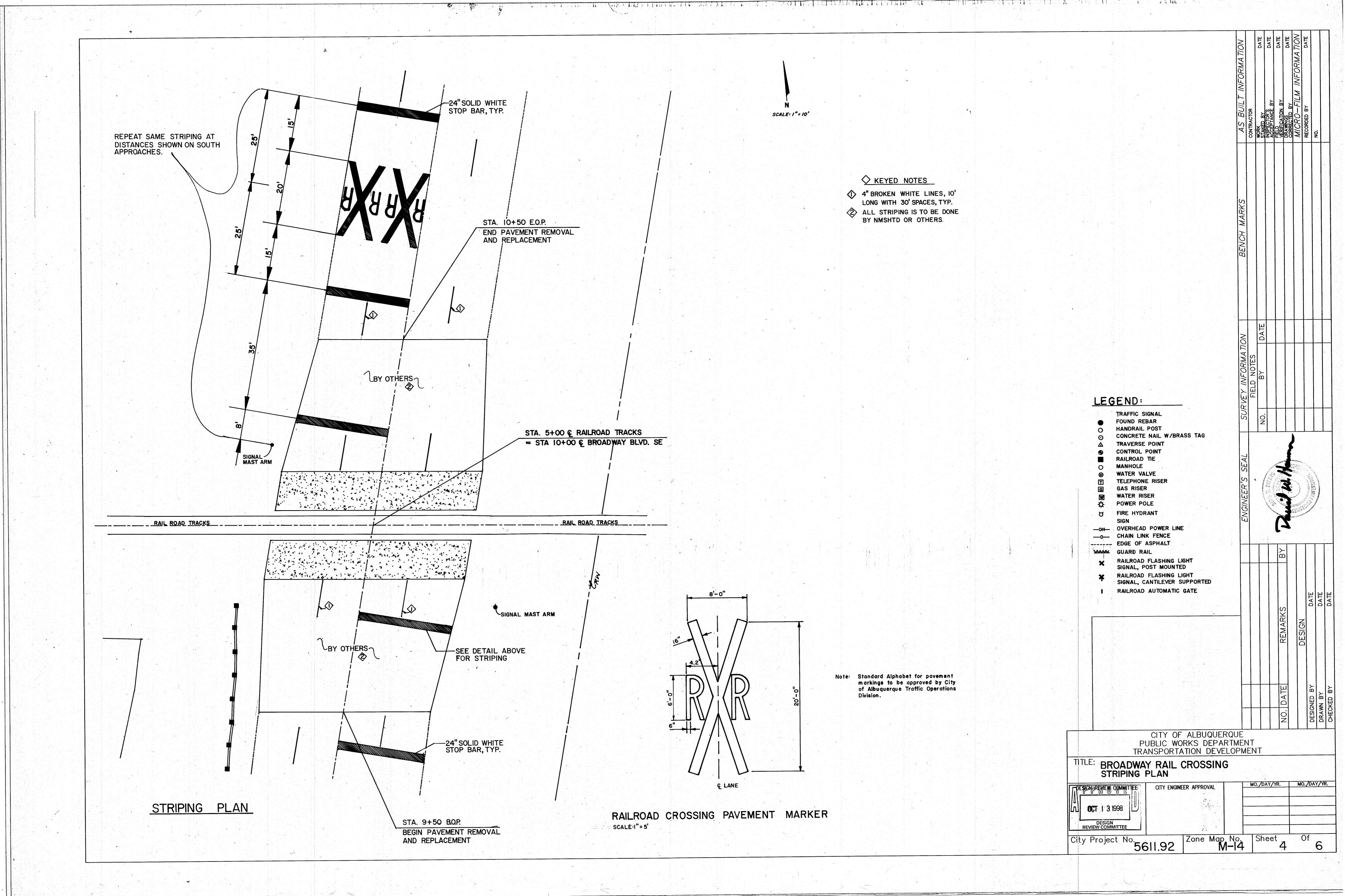
CLOSURE OF THE ROADWAY FOR THIS PHASE.
TRAFFIC BARRICADING TO BE BY CONTRACTOR

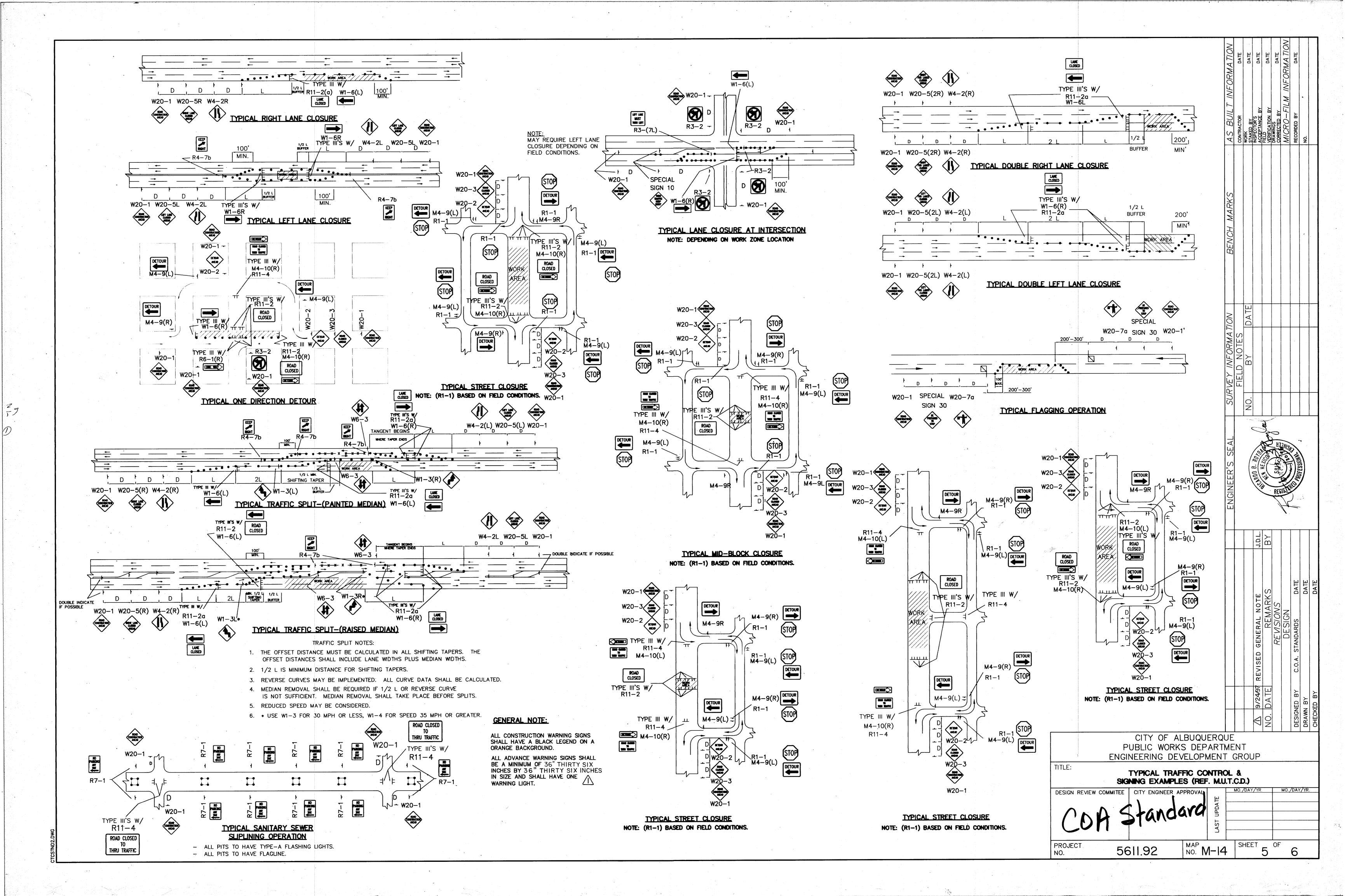
SOUTH OF WOODWARD ROAD MO./DAY/YR. MO./DAY/YR. Zone Map No. Sheet

* Traffic Control to be provided by the Contractor.





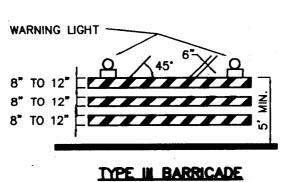




CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

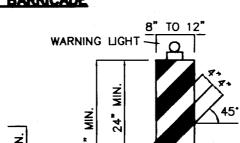
- CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.
- 2. CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.
- 3. THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.
- 4. CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION
- 5. CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.
- 6. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- 7. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET-UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR EQUIVALENT.
- 8. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
- 9. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.
- 10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.
- 11. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
- 12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.
- 13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELLED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.
- 14. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.
- 15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.
- 16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.
- 17. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.
- 18. ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- -48 HOURS PRIOR TO OCCUPYING OR CLOSING OF A RIGHT-OF-WAY. CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.
- 20. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.

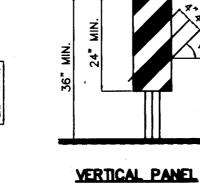
- 21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.
- 22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING: STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. 2. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION. SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.
- 23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.
- 24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION. WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.
- 25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.
- 26. ADVANCE WARNING SIGNS SHALL BE 36"x36" MIN. WITH SUPER ENGINEERING GRADE SHEETING OR BETTER. MOUNTING HEIGTH AT TOP OF SIGN SHALL BE THE SAME AS FOR A 48" SIGN AS INDICATED IN THE M.U.T.C.D. 3
- 27. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORKSITE, ALL GRAFFITI SHALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND TEMPORARY. 4



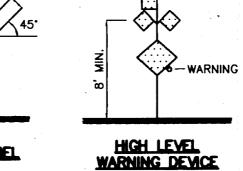
BASE VARIES

CONES

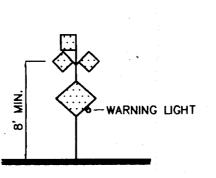




8" TO 12"



TRAFFIC CONTROL ELEMENTS



LEGEND

WORK AREA

BARRICADE - TYPE I, TYPE II, OR BARREL BARRICADE - TYPE III

VERTICAL PANEL

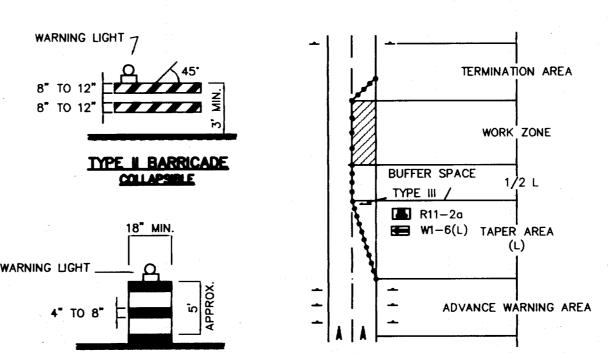
WARNING SIGN

DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET

SPACING BETWEEN BARRICADES- A DISTANCE MEASURED IN FEET EQUAL TO THE SPEED LIMIT OF THE STREET

TAPER LENGTH - SEE CHART BELOW

THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.



TAPER LENGTH (L) MINIMUM MAXIMUM DEVICE NUMBER | SPACING IN FEET DEVICES LANE FOR TAPER LANE 80 20

TAPER REQUIREMENTS

LIMIT ALONG | AFTER TAPER TAPER 115 125 25 30 205 225 245 35 270 320 40 295 540 450 495 13 45 550 600 50 550 605 660 55 55 13

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

SPEED MILES PER HOUR	MINIMUM DIST BETWEEN SIGNS	TANCE IN FEET FROM LAST SIGN TO TAPER			
0-20	10 X SPEED LIMIT	10 X SPEED LIMIT			
25-30	10 X SPEED LIMIT	10 X SPEED LIMIT			
30-35	10 X SPEED LIMIT	10 X SPEED LIMIT			
40-45	10 X SPEED LIMIT	10 X SPEED LIMIT			
50-60	10 X SPEED LIMIT	10 X SPEED LIMIT			

TAPER CRITERIA

TAPER LENGTH TYPE OF TAPER **UPSTREAM TAPER:** MERGING TAPER L MINIMUM SHIFTING TAPER 1/2 L MINIMUM: 1/2 L MINIMUM SHOULDER TAPER 100 FEET MAXIMUM TWO-WAY TRAFFIC TAPER 100 FEET PER LANE DOWNSTREAM TAPERS

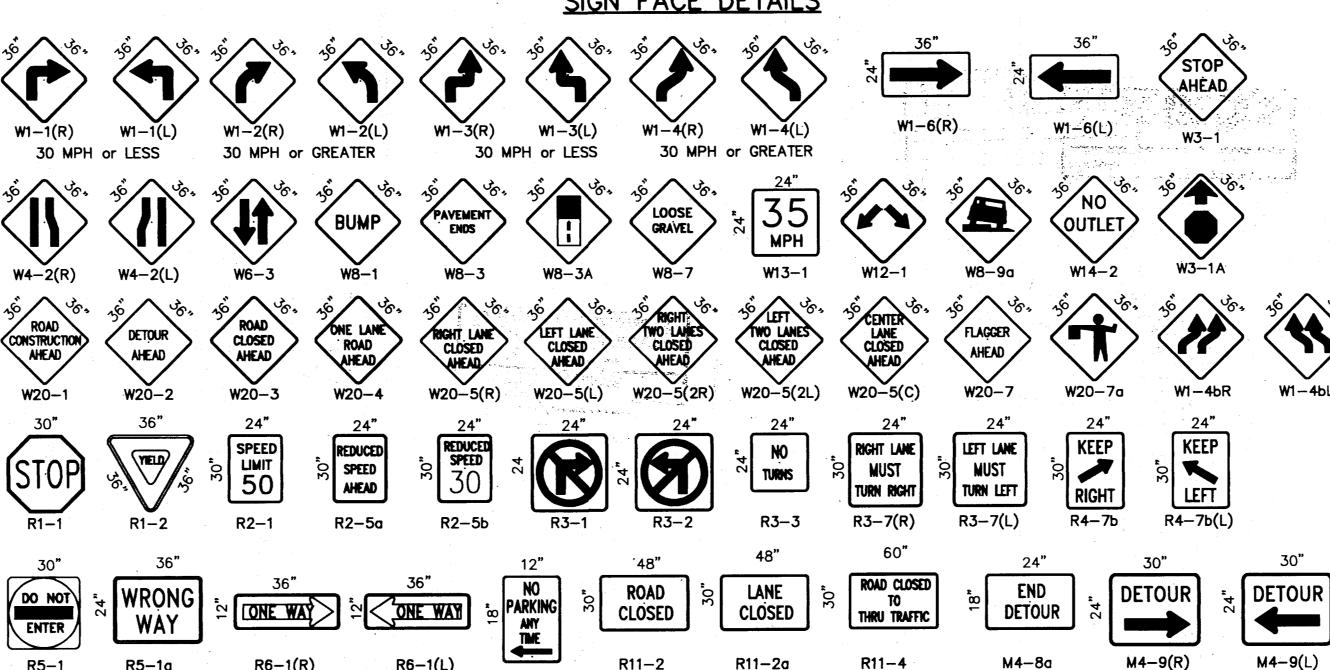
TAPER LENGTH COMPUTATION SPEED LIMIT $L = \frac{WS^2}{60}$ 40 MPH OR LESS 45 MPH OR GREATER $L = W \times S$ L = TAPER LENGTHW = WDTH OF OFFSET IN FEET S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

	9/25/97 ADD NOTE NO. 27	9/24/97 REVISED NOTE NO. 26	9/24/97 REVISED NOTE NO.19	9/24,97 REVISED NOTE NO. II	REMARKS	REVISIONS	DESIGN	DATE	C.O.A. STANDARDS DATE	DATE
	SAST ADD	197 REV	197 REV	4.97 REV	JE			ВҮ		37
	₽ 9/25	₹ 9/5r	73/6	7/6 V	NO. DATE			DESIGNED BY	DRAWN BY	CHECKED BY
FROI	JF	<u> </u>			<u> </u>	L	I	L	l	

CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP

TITLE: SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS DESIGN REVIEW COMMITEE | CITY ENGINEER APPROVAL SHEET 6 OF MAP NO. **M-14** PROJECT 5611.92

SIGN FACE DETAILS



THRU TRAFFIC KEEP PREPARED TO **SPECIAL**

SIGN 30

R6-1(R)

R5-1a

SIGN

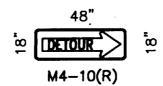
ACCESS **SPECIAL**

SIGN

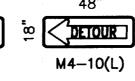
ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.

R11-2

R11-2a



R11-4



CONSTRUCTION

END

G20-2