

TIME: 06-14-93 12:45:03

DWG: L:\CAD\278212\L-COVER1

11/11

SCALE: 1"=1'



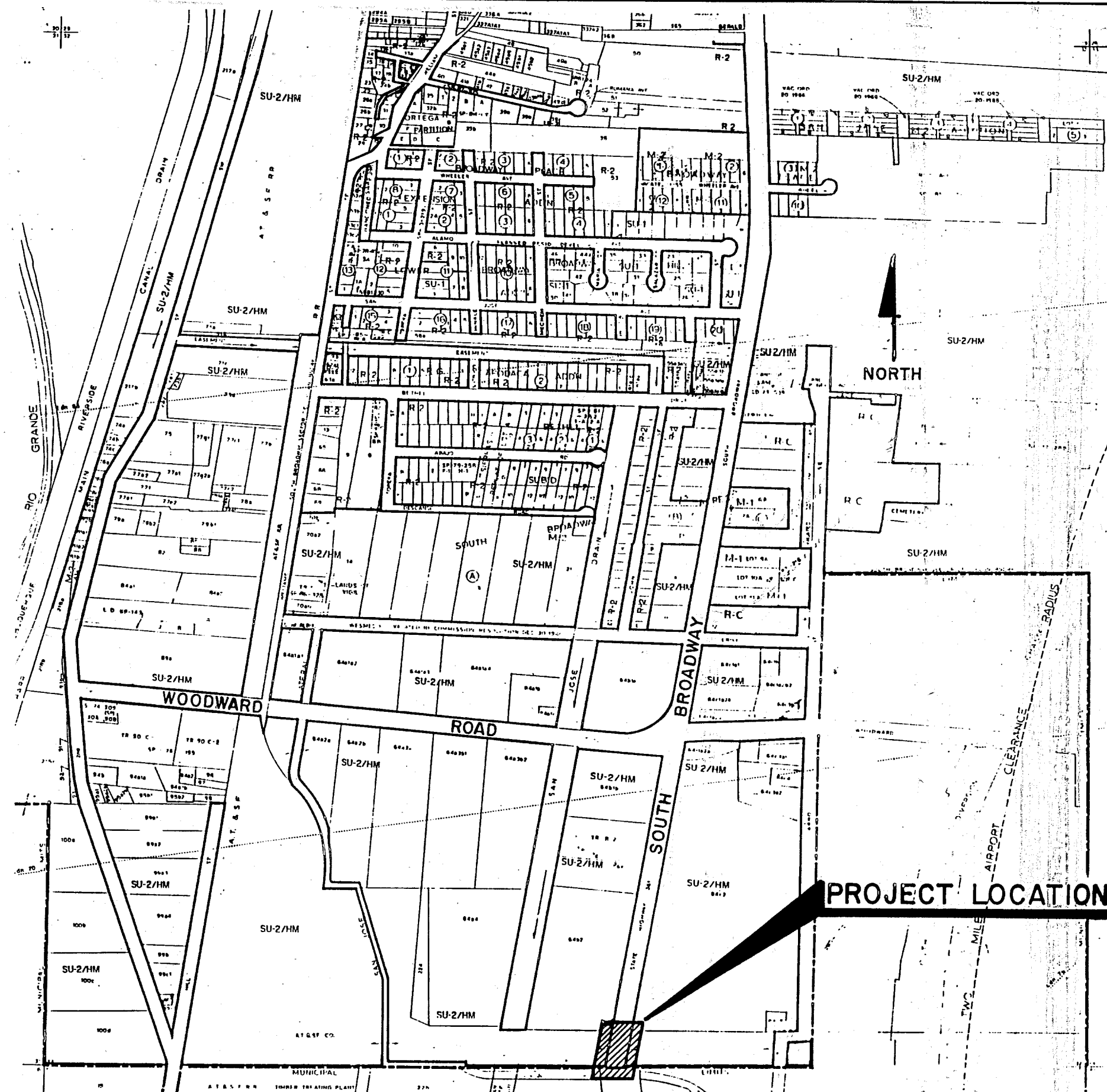
CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING GROUP

BROADWAY RAIL CROSSING
SOUTH OF WOODWARD ROAD

INDEX

COVER SHEET	1
SITE AND GRADING PLAN	2
DETAILS	3
STRIPING PLAN	4
TRAFFIC CONTROL	5
TRAFFIC CONTROL	6

APPROVAL OF AS BUILT DRAWINGS
CHIEF CONSTRUCTION ENGINEER
John Chang
DATE 4-30-99



VICINITY PLAN MAP NO. M-14

GENERAL NOTES
NOTICE TO CONTRACTORS

1. CITY OF ALBUQUERQUE SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION AS AMENDED THROUGH UPDATE 6, WILL BE REFERRED TO HEREIN AS THE "STANDARD SPECIFICATION".
2. TWO WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) AND DETERMINE LOCATION OF EXISTING UTILITIES.
3. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR THE SURVEYOR SO THAT THE CONFLICT CAN BE RESOLVED WITHOUT DELAY.
4. ALL EXISTING SIGNS, MARKERS, DELINEATORS, ETC., WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED, STORED AND RE-SET BY THE CONTRACTOR.
5. THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE ENGINEER AND SHALL NOTIFY THE ENGINEER AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITION OF THE STANDARD SPECIFICATIONS.
6. FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION CO-ORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONSTRUCTION CO-ORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (768-2551) PRIOR TO OCCUPYING AN INTERSECTION.
7. ANY WORK OCCURRING WITHIN AN ARTERIAL ROADWAY REQUIRES 24 HOURS CONSTRUCTION.
8. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.
- 8A. VERTICAL TRANSITION LEVELING COURSE SHALL BE TEMPORARILY INSTALLED FROM EXISTING ASPHALT APPROACHES TO RCC TRANSITION SLAB.

9. PHASING OF CONSTRUCTION

PHASE	ALLOWABLE TIME	WORK
A	24 HOURS	BURLINGTON NORTHERN RAILWAY COMPANY OR ITS ASSIGNS, SHALL REMOVE THE RAIL SPUR, TO INCLUDE BALLAST. THERE SHALL BE TOTAL CLOSURE OF THE ROADWAY FOR THIS PHASE. TRAFFIC BARRICADING TO BE BY CONTRACTOR.
B	24 HOURS	CONTRACTOR SHALL CONSTRUCT AND COMPACT SIX (6) INCHES OF ASPHALT TREATED BASE (ATB) IN THE BALLAST FOOTING AREA AS SHOWN ON THE PLANS. THERE SHALL BE TOTAL CLOSURE OF ROADWAY FOR THIS PHASE.
C	24 HOURS	BURLINGTON NORTHERN RAILWAY COMPANY, OR ITS ASSIGNS, SHALL REPLACE BALLAST AND RELAY RAIL TO EXISTING ELEVATIONS, TO INCLUDE INSTALLATION OF THE OMNI CONCRETE SYSTEM. THERE SHALL BE TOTAL CLOSURE OF THE ROADWAY FOR THIS PHASE. UPON BEGINNING THIS PHASE, BNRC SHALL INFORM ART KELLY, 768-2768.
D	16 DAYS 240 HOURS	REMOVE EXISTING ROADWAY AND THE CONTRACTOR SHALL CONSTRUCT REINFORCED CONCRETE APPROACH SLABS, TO INCLUDE FOUR (4) INCH BASE COURSE AND COMPACTED BASE COURSE, ON ONE-HALF OF ROADWAY WIDTH AT A TIME. ONE LANE OF TRAFFIC EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE. UPON BEGINNING THIS PHASE, THE CONTRACTOR SHALL INFORM ORLANDO GARCIA, NMSHTD, 841-2700. WORKING HOURS SHALL BE 7 AM TO 7 PM THIS PHASE.
E	120 HOURS 5 DAYS	THE NEW MEXICO STATE HIGHWAY AND TRANSPORTATION DEPARTMENT SHALL CONSTRUCT ASPHALT ROADWAY APPROACHES, ONE HALF OF THE ROADWAY WIDTH AT A TIME, AS SHOWN ON THE PLANS. ONE LANE OF TRAFFIC EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE. WORKING HOURS SHALL BE 7 AM TO 7 PM THIS PHASE. NMSHTD
F	8 HOURS	CONTRACTOR SHALL INSTALL HOT PLASTIC RAILROAD CROSSING LEGEND, BOTH SIDES, UPON COMPLETION OF PHASE E. ONE LANE OF TRAFFIC EACH DIRECTION SHALL BE MAINTAINED THIS PHASE.
10.		ALL PHASING SHALL CONTINUE UNINTERRUPTED (A THROUGH F) UNLESS WEATHER DOES NOT PERMIT.
11.		CONTRACTOR SHALL NOT BE HELD RESPONSIBLE FOR DELAYS CAUSED BY OTHERS.

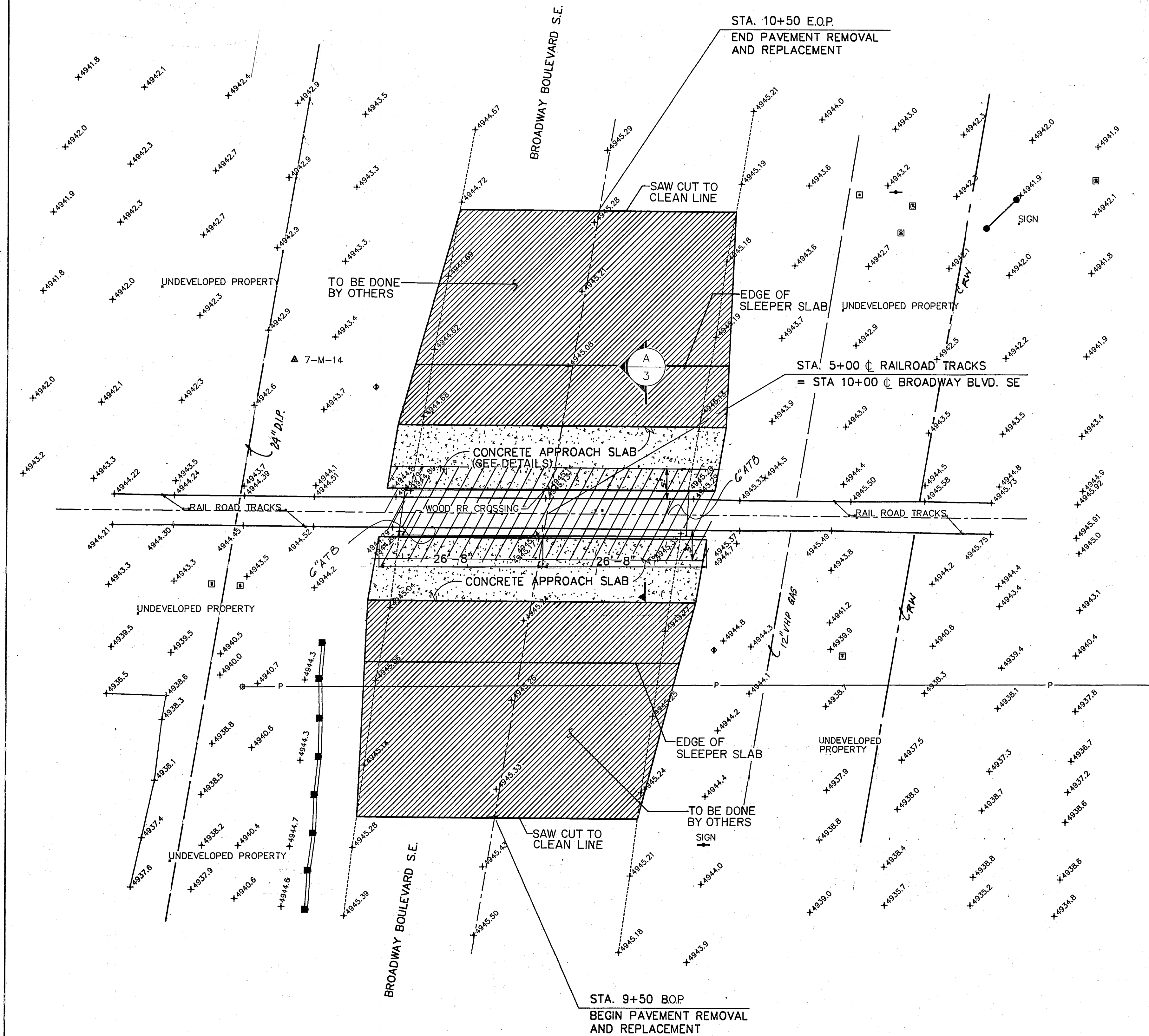
CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT TRANSPORTATION DEVELOPMENT			
TITLE: BROADWAY RAIL CROSSING SOUTH OF WOODWARD ROAD DETAILS			
DESIGN-REVIEW COMMITTEE APPROVED OCT 13 1998	CITY ENGINEER APPROVAL	MO./DAY/YR.	MO./DAY/YR.
City Project No. 5611.92	Zone Map No. M-14	Sheet 1	Of 6

* Traffic Control to be provided by the Contractor.

TIME: 06-14-93 13:50:46

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SCALE: 1"=10'



LEGEND

DESCRIPTION	EXISTING	NEW
A.C.S. MONUMENT 7-M14	▲	
SIGNAL ELECTRICAL PULL BOX	◆	
UTILITY POLE	○	
SIGN & POST	+	
GAS RISER	□	
TELEPHONE RISER	■	
RAILROAD CROSSING GUARD AND SIGNAL	◆	
METAL GUARD RAIL	—	—
GAS RISER	□	
SPOT ELEVATIONS AT TOP BACK OF C & G, TOP OF ASPHALT AND TOP OF RR TRACKS	X 4941.99	
SPOT ELEVATION ON NATURE GROUND	X 4941.9	
EDGE OF ASPHALT	—	
OVERHEAD UTILITY LINES	—	
PAVEMENT		▨
CONCRETE APPROACH SLAB		▨
6" ATB		▨
R.O.W.		▨
GAS		▨

AS BUILT INFORMATION			
CONTRACTOR	Russ P. Inc.	DATE	4/94
DESIGNED BY	RLB	DATE	5/93
DRAWN BY	JLM	DATE	5/93
CHECKED BY	DJL	DATE	5/93
MICRO-FILM INFORMATION			
RECORDED BY		DATE	
NO.			

BENCH MARKS	
STANDARD ACS BRASS TABLET STAMPED	
7-M14 1978, LOCATED 45' WEST OF	
RAILROAD CROSSING	
ELEV. = 4943.30	

SURVEY INFORMATION	
FIELD NOTES	DATE
NO. 1	CITY OF ALBUQUERQUE


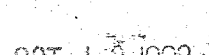


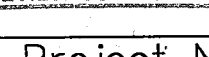

ENGINEER'S SEAL	
BY	DATE

REVISIONS	
NO.	DATE

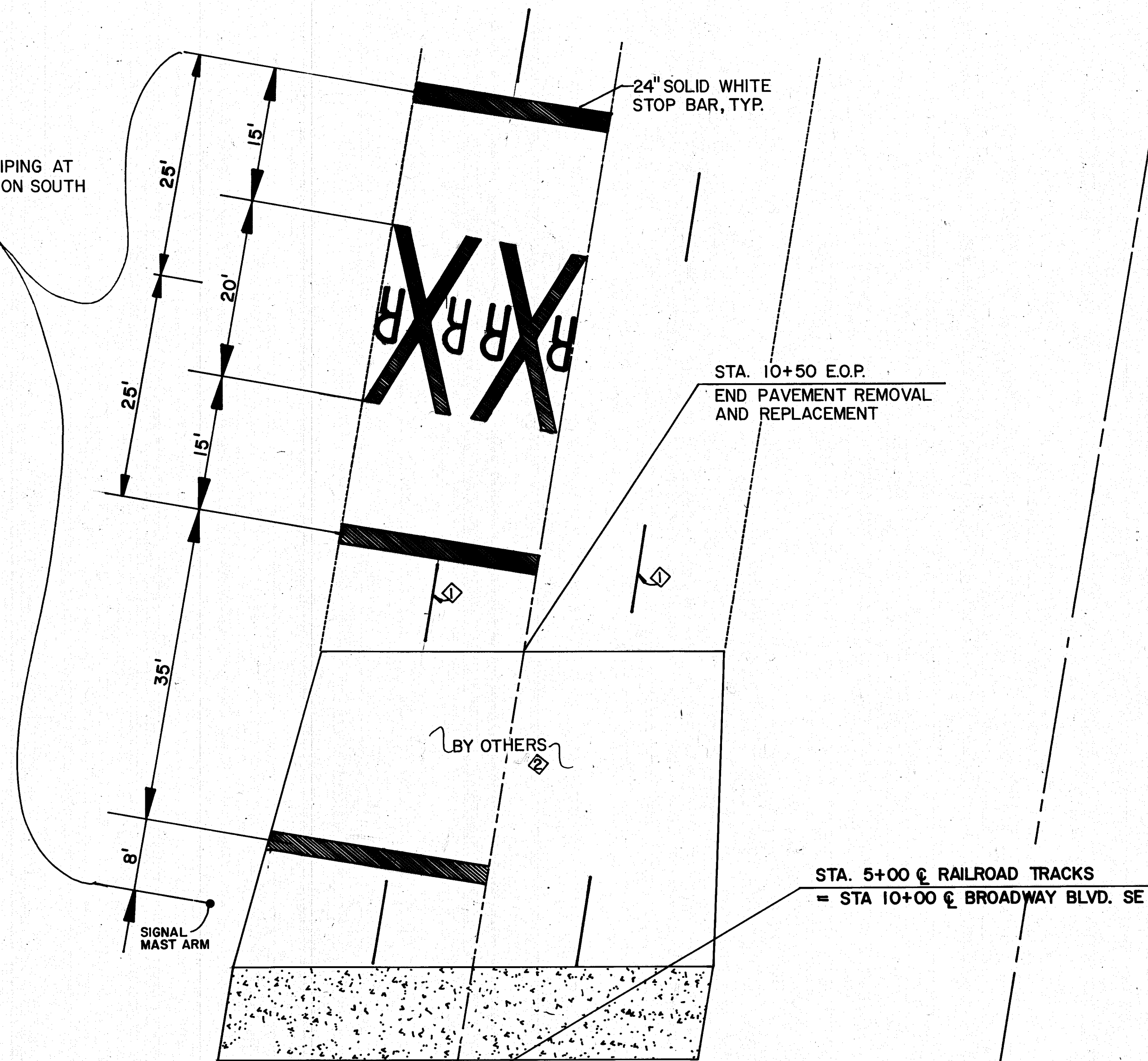
DESIGNED BY: RLB	DATE: JUNE 93
DRAWN BY: JLM	DATE: JUNE 93
CHECKED BY: DJL	DATE: JUNE 93

CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT TRANSPORTATION DEVELOPMENT			
TITLE: BROADWAY RAIL CROSSING SOUTH OF WOODWARD ROAD SITE AND GRADING PLAN			
DESIGN REVIEW COMMITTEE OCT 13 1993 DESIGN REVIEW COMMITTEE	CITY ENGINEER APPROVAL	MO./DAY/YR.	MO./DAY/YR.
City Project No. 5611.92	Zone Map No. M-14	Sheet 2	Of 6

DWG: L:\CAD\278212\L:C3

		CITY ENGINEER APPROVAL		MO./DAY/YR.		MO./DAY/YR.	
							
							
							
							
							
City Project No. 5611 92		Zone Map No. M-14		Sheet 3		Of 6	

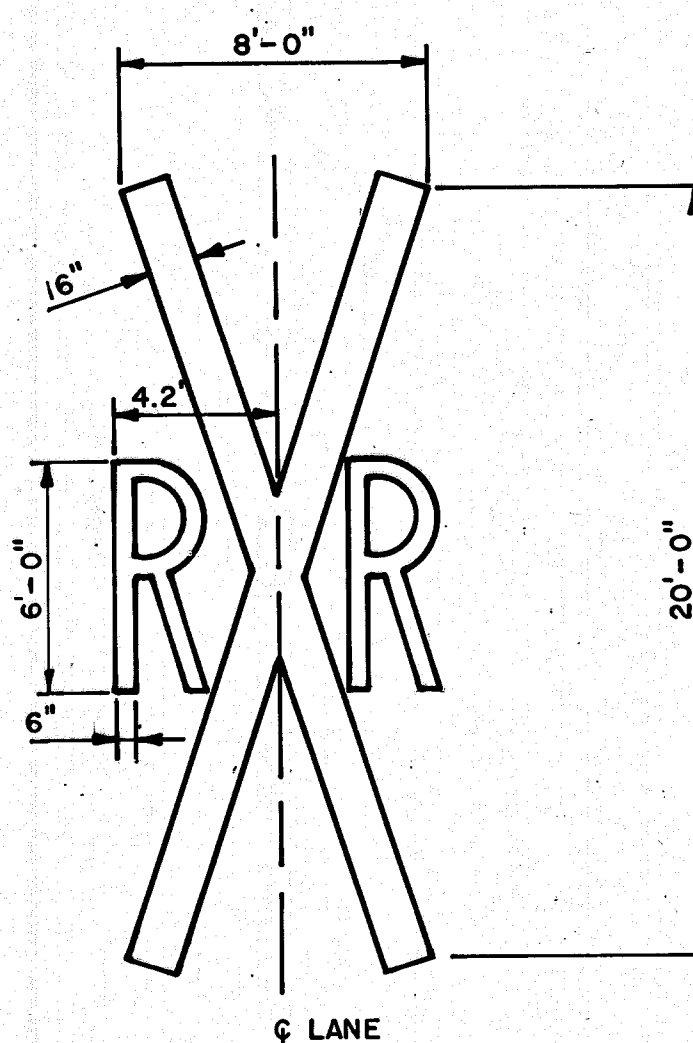
REPEAT SAME STRIPING AT DISTANCES SHOWN ON SOUTH APPROACHES.



- KEYED NOTES**
- ◇ 4" BROKEN WHITE LINES, 10' LONG WITH 30' SPACES, TYP.
 - ◇ ALL STRIPING IS TO BE DONE BY NMSHTD OR OTHERS.

LEGEND:

- TRAFFIC SIGNAL
- FOUND REBAR
- △ HANDRAIL POST
- CONCRETE NAIL W/BRASS TAG
- △ TRAVERSE POINT
- CONTROL POINT
- RAILROAD TIE
- MANHOLE
- WATER VALVE
- TELEPHONE RISER
- GAS RISER
- WATER RISER
- ★ POWER POLE
- FIRE HYDRANT
- SIGN
- OH— OVERHEAD POWER LINE
- CHAIN LINK FENCE
- EDGE OF ASPHALT
- XXXXX GUARD RAIL
- ✕ RAILROAD FLASHING LIGHT SIGNAL, POST MOUNTED
- ✕ RAILROAD FLASHING LIGHT SIGNAL, CANTILEVER SUPPORTED
- I RAILROAD AUTOMATIC GATE



Note: Standard Alphabet for pavement markings to be approved by City of Albuquerque Traffic Operations Division.

STRIPING PLAN

RAILROAD CROSSING PAVEMENT MARKER
SCALE: 1" = 5'

AS BUILT INFORMATION				BENCH MARKS				SURVEY INFORMATION				ENGINEER'S SEAL			
CONTRACTOR	NO.	DATE	BY	NO.	DATE	BY	NO.	NO.	DATE	BY	NO.	NO.	DATE	BY	NO.
WORK STARTED BY															
STOPPED BY															
ACCEPTANCE BY															
FIELD LOCATION BY															
DRAWINGS															
CORRECTED BY															
MICRO-FILM INFORMATION															
RECORDED BY															
NO.															

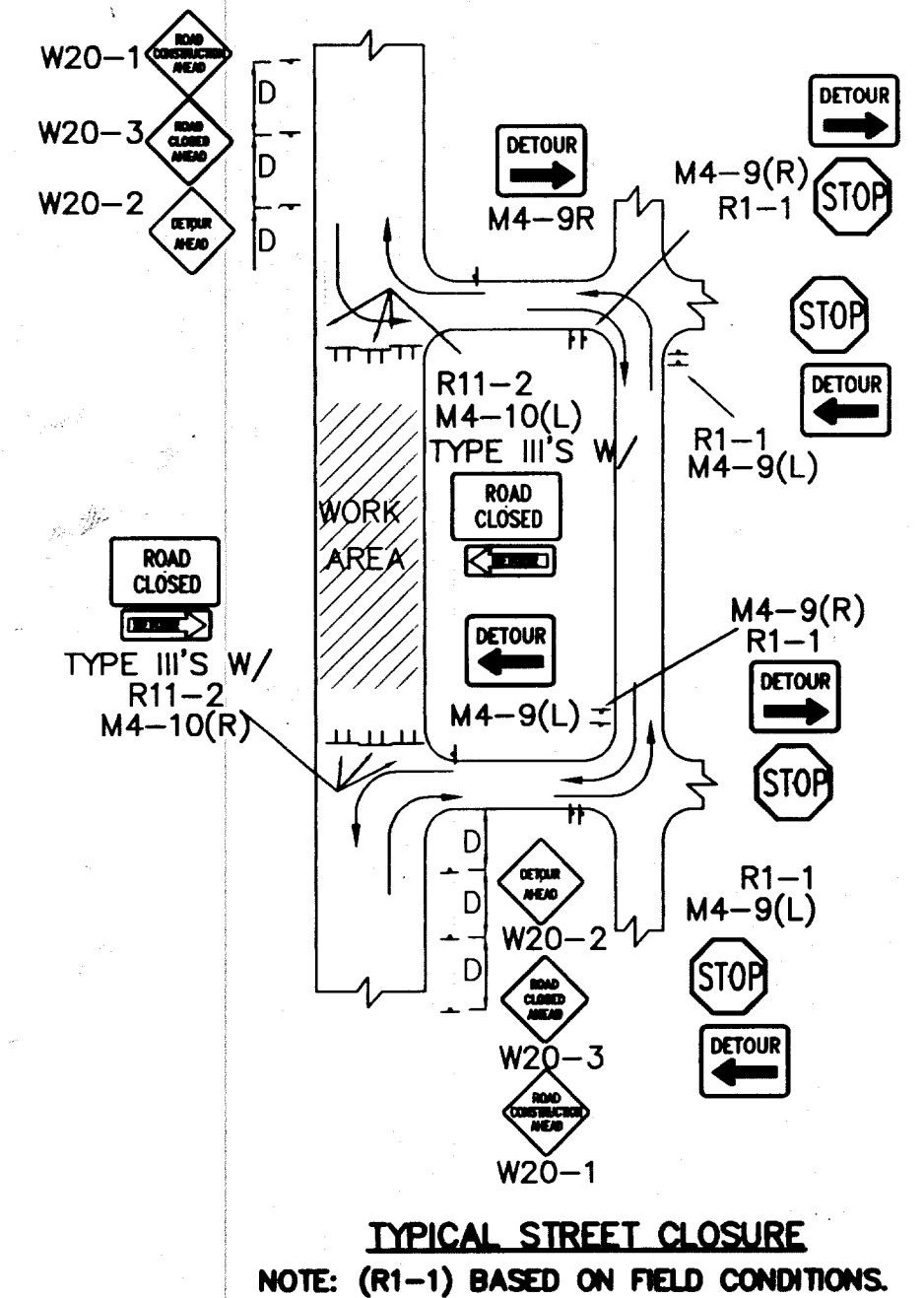
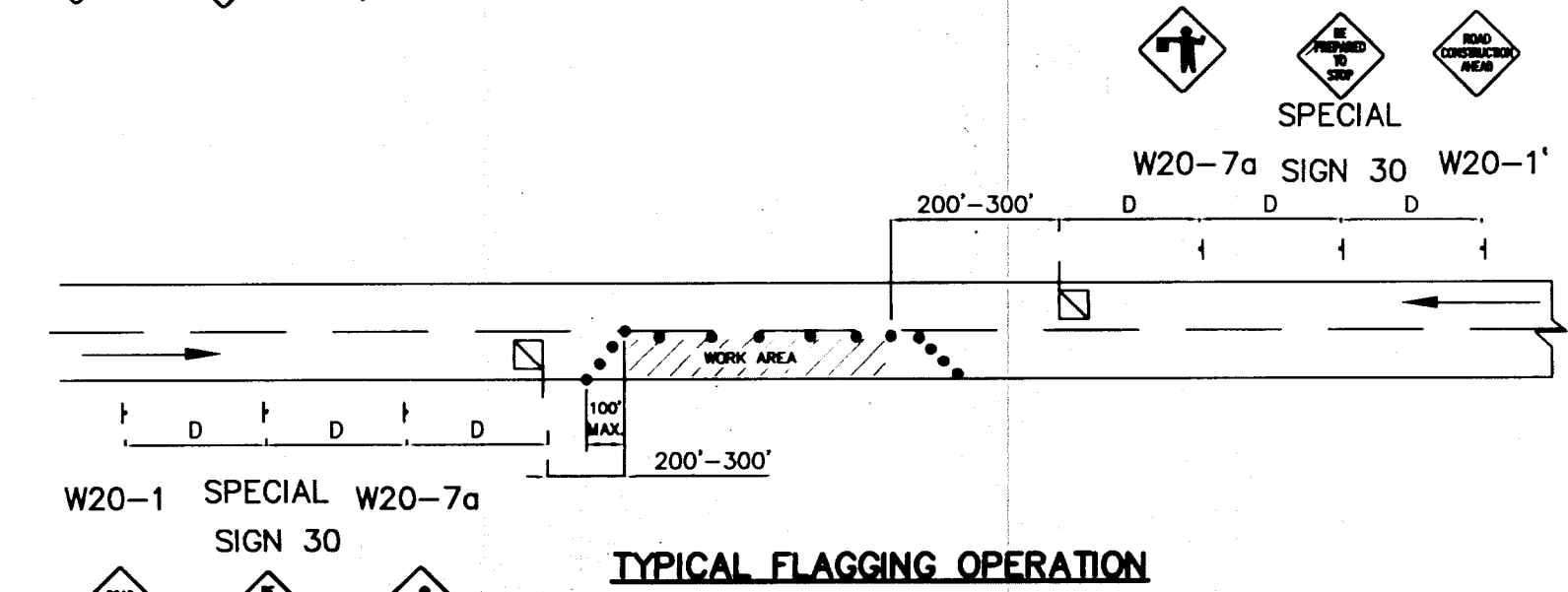
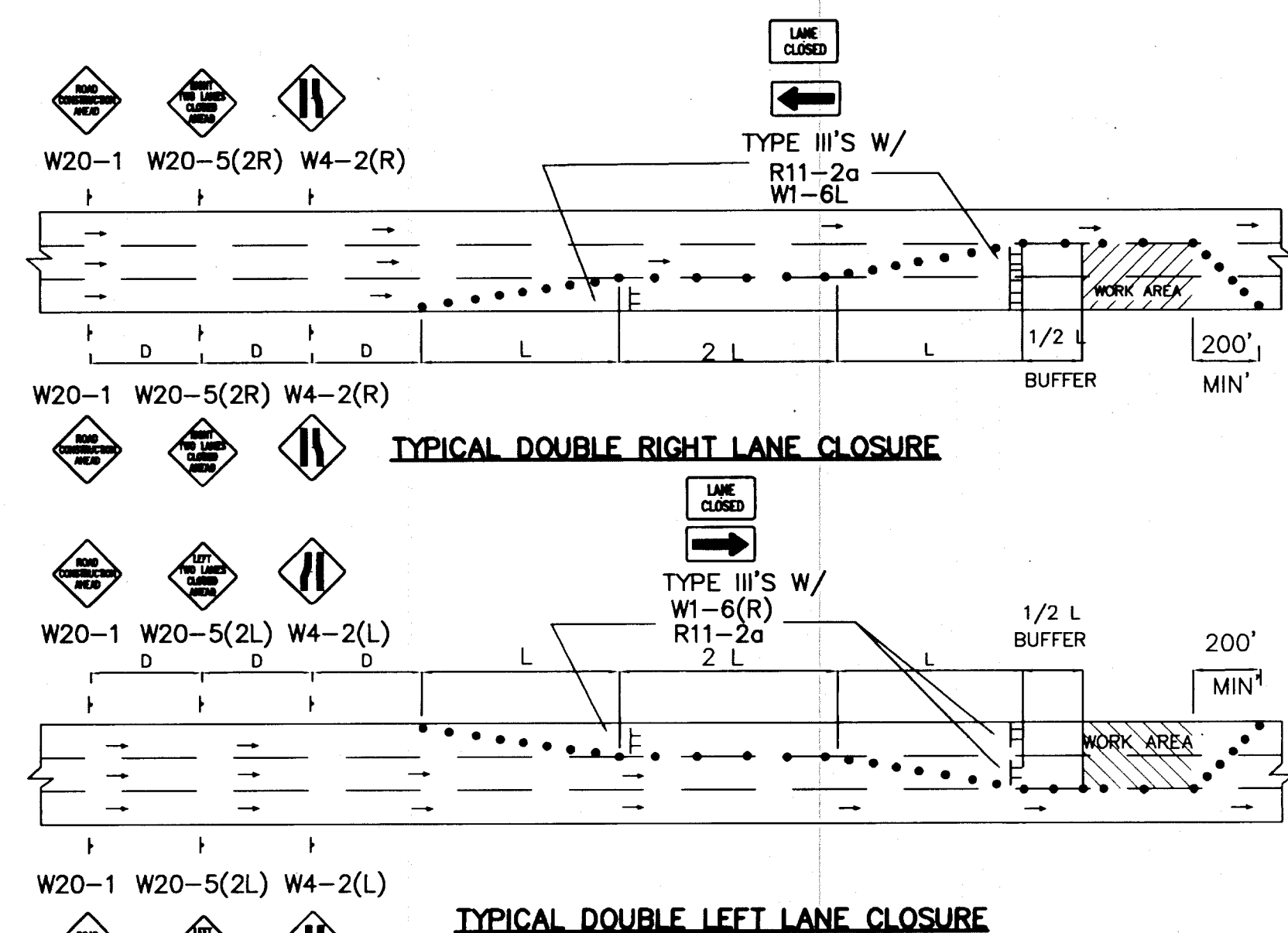
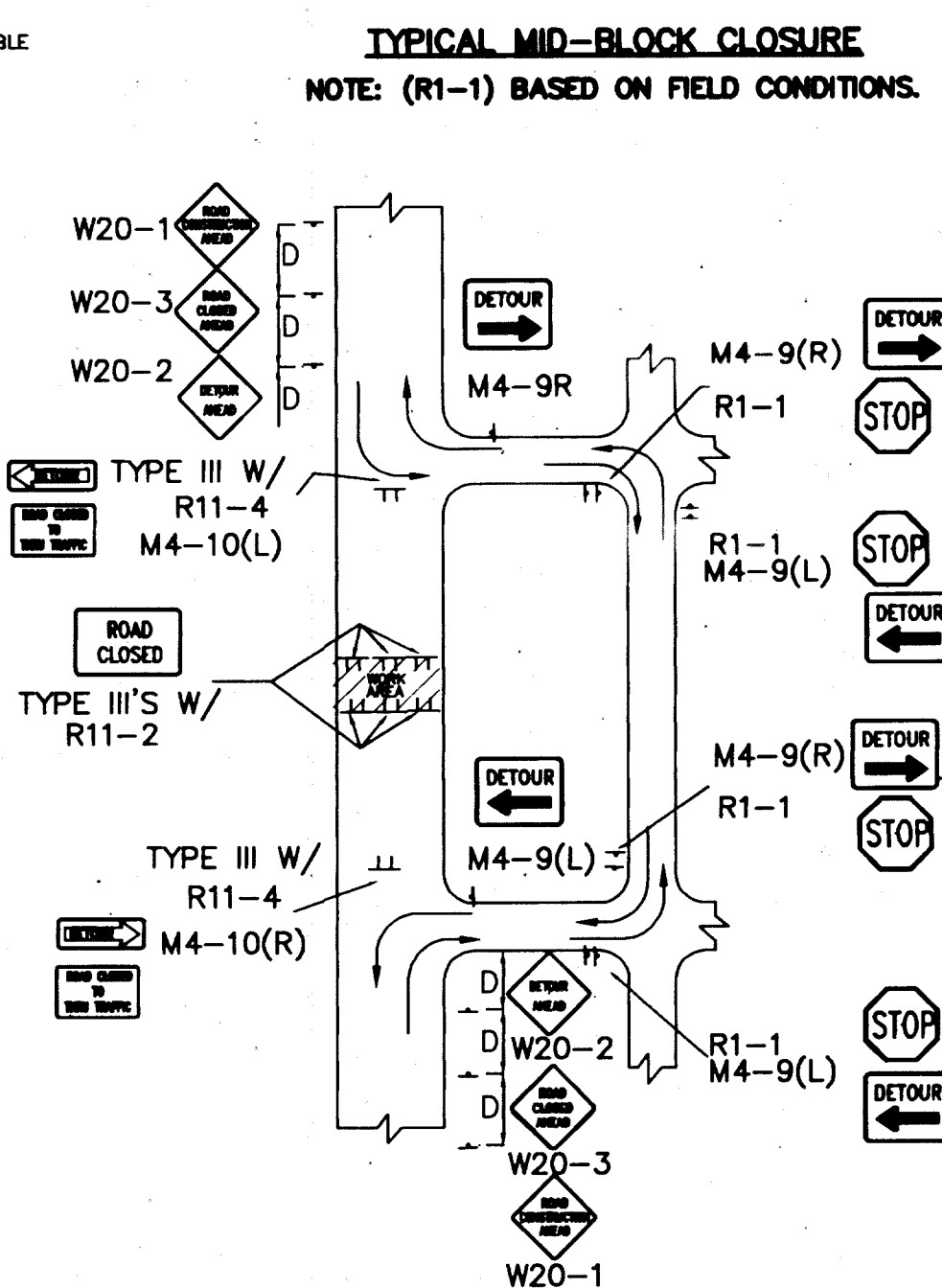
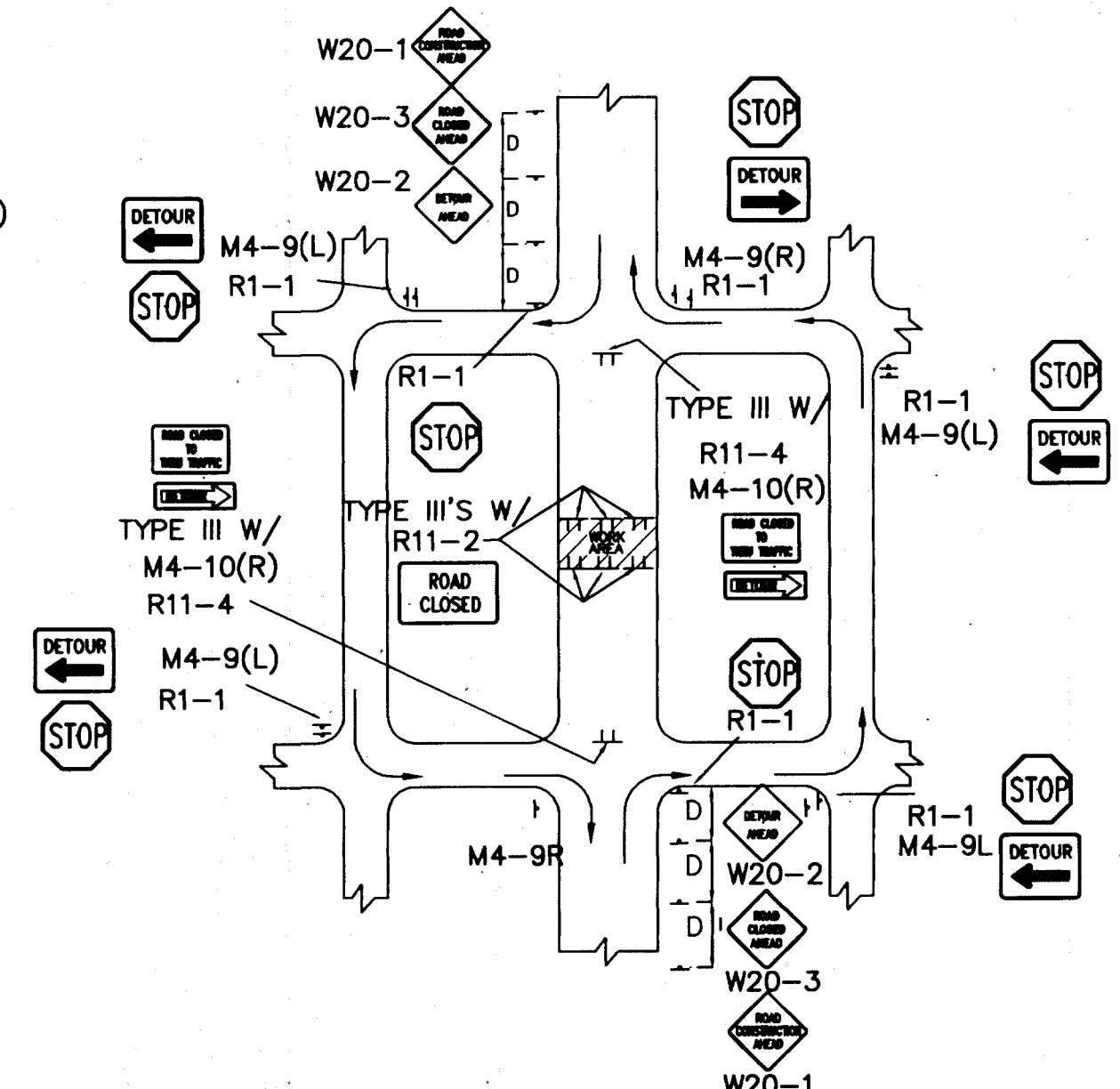
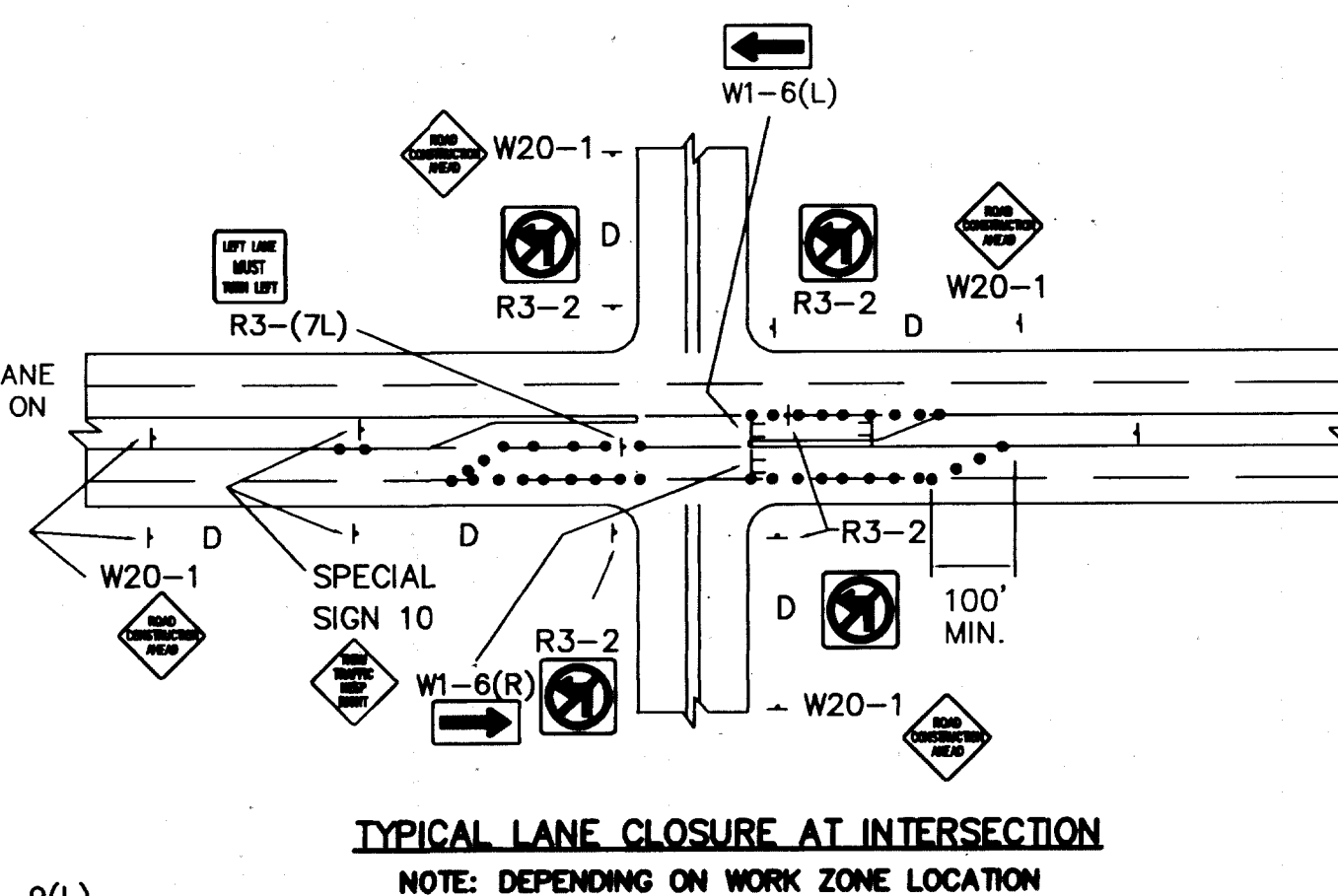
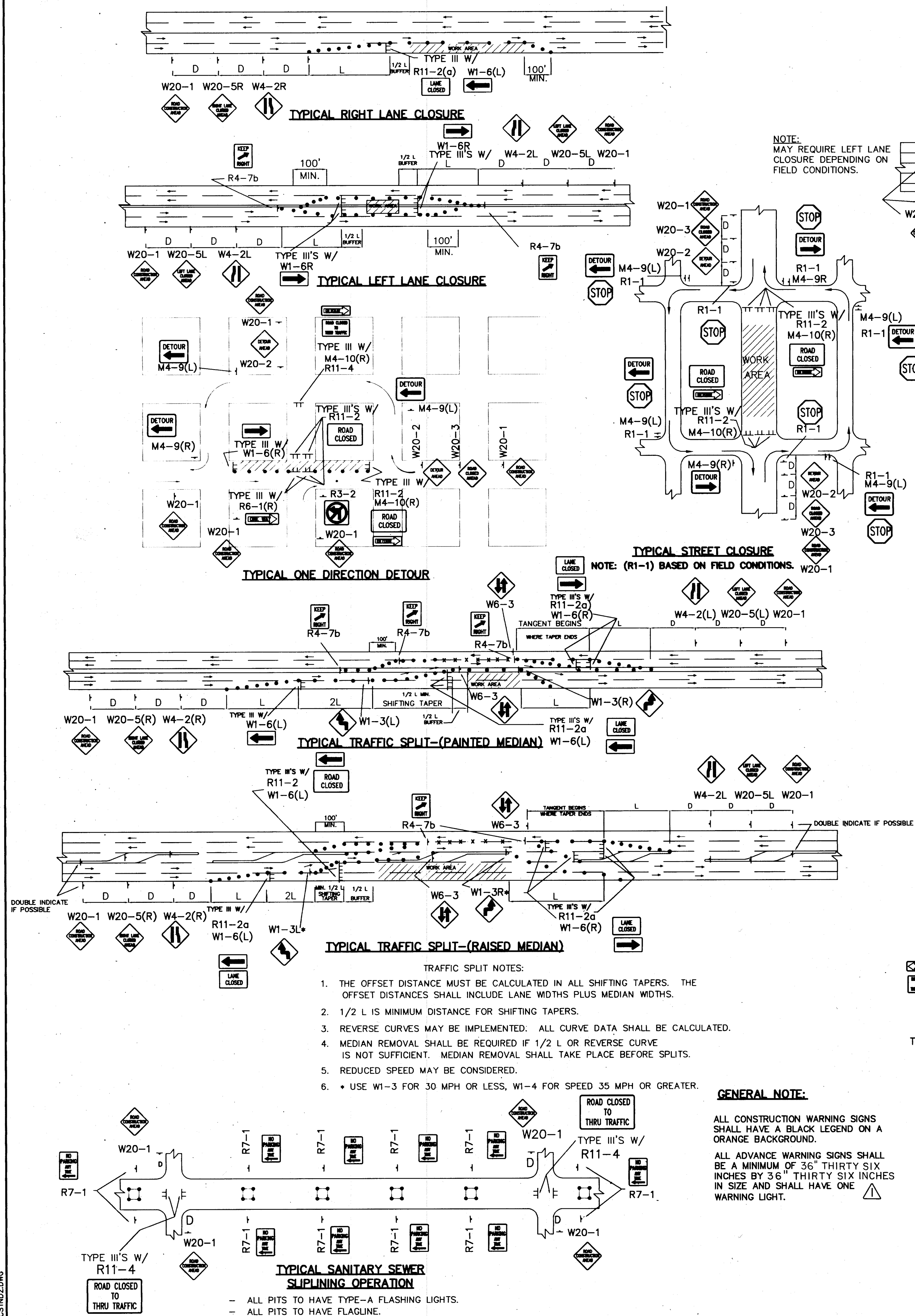


CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
TRANSPORTATION DEVELOPMENT

TITLE: **BROADWAY RAIL CROSSING STRIPING PLAN**

DESIGN REVIEW COMMITTEE	CITY ENGINEER APPROVAL	MO./DAY/YR.	MO./DAY/YR.
OCT 13 1998			
DESIGN REVIEW COMMITTEE			

City Project No. **5611.92** Zone Map No. **M-14** Sheet **4** Of **6**



CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP					
TITLE: <div style="text-align: center;"> TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES (REF. M.U.T.C.D.) </div>					
DESIGN REVIEW COMMITTEE		CITY ENGINEER APPROVAL		LAST UPDATE	MO./DAY/YR.
<div style="font-size: 2em; font-family: cursive;">COA Standard</div>					
PROJECT NO.		5611.92		MAP NO.	M-14
				SHEET	5 OF 6

