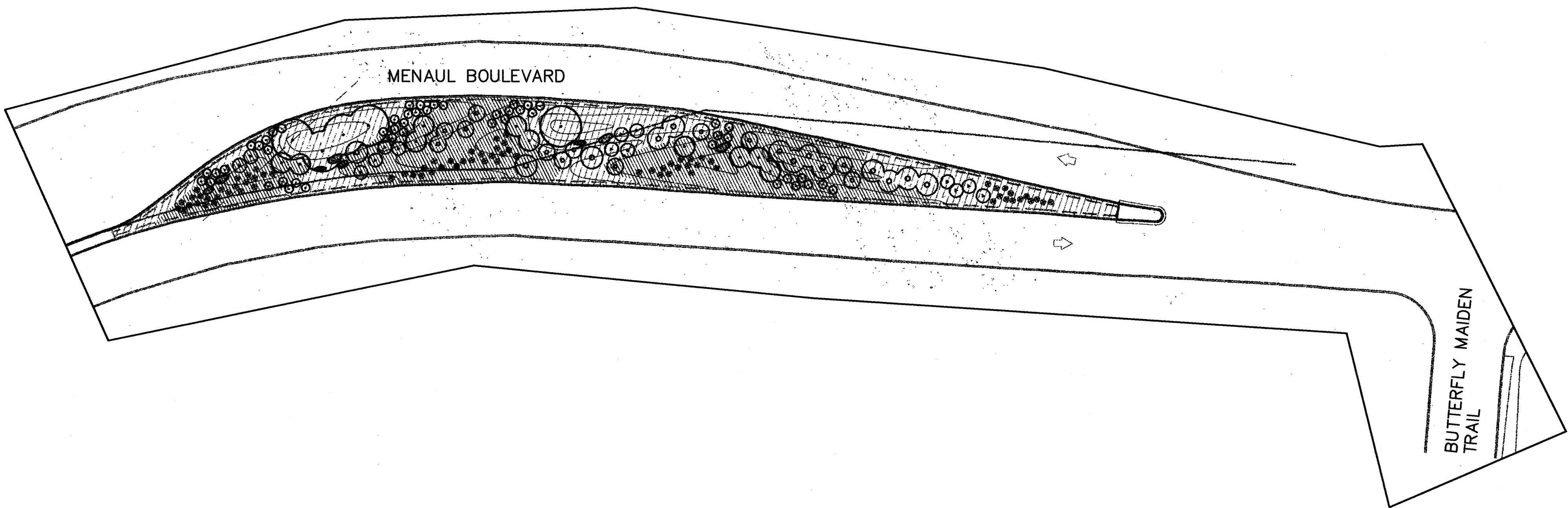


# MENAU MEDIAN REDESIGN AND CONSTRUCTION

MENAU BOULEVARD AT BUTTERFLY MAIDEN TRAIL  
CITY OF ALBUQUERQUE

## LANDSCAPE IMPROVEMENTS INDEX

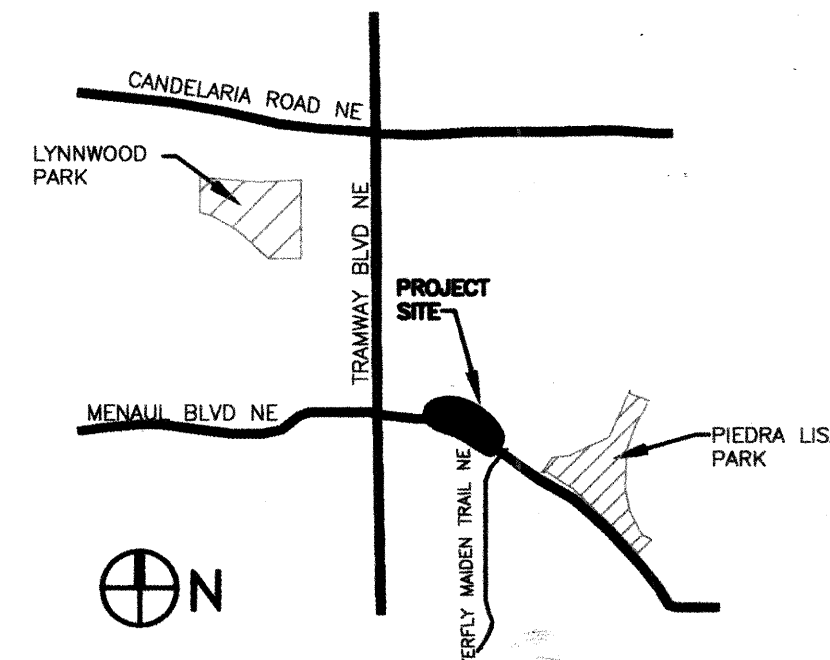
| SHEET | DESCRIPTION  |
|-------|--|
| 1     | TITLE SHEET  |
| 2     | PLANTING PLANS AND DETAILS                             |
| 3     | IRRIGATION PLANS AND DETAILS                           |
| 4-5   | TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES (REF MUTCD) |



FOR INFORMATION ONLY

### CONSTRUCTION NOTES:

- WHEN ABUTTING NEW CURB AND GUTTER TO EXISTING PAVEMENT, A 1' WIDE SECTION OF EXISTING PAVEMENT ADJACENT TO THE CURB AND GUTTER SHALL BE SAWCUT, REMOVED, AND REPLACED AS PER THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL ABIDE BY ALL LOCAL, STATE, AND FEDERAL LAWS, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS.
- CONTRACTOR IS TO PROVIDE AS-BUILT DRAWINGS TO THE CITY PROJECT MANAGER PRIOR TO FINAL APPROVAL.
- ALL ELECTRICAL, TELEPHONE, CABLE TV, GAS AND OTHER UTILITY LINES, CABLES, AND APPURTENANCES ENCOUNTERED DURING CONSTRUCTION THAT REQUIRE RELOCATION, SHALL BE COORDINATED WITH THAT UTILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL NECESSARY UTILITY ADJUSTMENTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR DELAYS OR INCONVENIENCES CAUSED BY UTILITY COMPANY WORK CREWS. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE HIS ACTIVITIES TO ALLOW UTILITY CREWS TO PERFORM THEIR REQUIRED WORK.
- A DISPOSAL SITE FOR ALL EXCESS EXCAVATION MATERIAL, AND UNSUITABLE MATERIAL SHALL BE OBTAINED BY THE CONTRACTOR IN COMPLIANCE WITH APPLICABLE ENVIRONMENTAL REGULATIONS AND APPROVED BY THE CONSTRUCTION OBSERVER. ALL COSTS INCURRED IN OBTAINING A DISPOSAL SITE AND HAUL THERETO SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.
- THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EXISTING UTILITY LINES WITHIN THE CONSTRUCTION AREA. ANY DAMAGE TO EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE AND APPROVED BY THE CONSTRUCTION OBSERVER.
- CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT PROPERTIES RESULTING FROM THE CONSTRUCTION PROCESS IS THE RESPONSIBILITY OF THE CONTRACTOR. ANY COSTS INCURRED FOR REPAIRS SHALL BE THE COST OF THE CONTRACTOR.
- OVERNIGHT PARKING OF CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS OR DESIGNATED TRAFFIC LANES. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL WITHIN THE PUBLIC RIGHT-OF-WAY.
- THE CONTRACTOR SHALL OBTAIN ALL THE NECESSARY PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION (I.E. BARRICADING, TOPSOIL DISTURBANCE AND EXCAVATION PERMITS, ETC.)
- THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE AT HIS EXPENSE ANY AND ALL PROPERTY CORNERS DESTROYED DURING
- ALL PERMANENT PAVEMENT MARKING AND TRAFFIC SIGNING SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR PER PLAN.
- THE CONTRACTOR SHALL PREPARE A CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN AND OBTAIN APPROVAL OF SUCH PLAN FROM THE CITY OF ALBUQUERQUE, TRAFFIC ENGINEERING DEPARTMENT, PRIOR TO BEGINNING ANY CONSTRUCTION WORK ON OR ADJACENT TO CONSTRUCTION. ALL PROPERTY CORNERS MUST BE RESET BY A REGISTERED LAND SURVEYOR.
- ALL BARRICADES AND CONSTRUCTION SIGNING SHALL CONFORM TO APPLICABLE SECTIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), U.S. DEPARTMENT OF TRANSPORTATION, LATEST EDITION.
- THE CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION BARRICADES EXISTING STREETS, AND SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
- ALL SAWCUT PAVEMENT SHALL HAVE A UNIFORM EDGE AND BE SPRAYED WITH TACK.



VICINITY MAP  
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#### NOTICE TO CONTRACTORS

- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION AS AMENDED THROUGH UPDATE No.6
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR SURVEYOR IMMEDIATELY SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONST. COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (768-2551) PRIOR TO OCCUPYING AN INTERSECTION. REFER TO SECTION 19 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
- ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED WITH PLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR TO LOCATION AS EXISTING OR AS INDICATED BY THIS PLAN SET.
- CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK. IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS. ANY PERMANENT SURVEY MONUMENT LOCATED WITHIN 50' OF THE PROJECT LIMITS NOT SHOWN ON THE PLANS THAT IS DESTROYED DURING CONSTRUCTION WILL BE REPLACED AT DESIGNER'S EXPENSE.
- CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING IN THE VICINITY OF EXISTING CITY FACILITIES. ANY DAMAGED ITEMS SHALL BE REPAIRED OR REPLACED IN KIND, AS DETERMINED BY THE OWNER'S REPRESENTATIVE. REPLACEMENTS SHALL BE AT THE CONTRACTOR'S EXPENSE, AS PER C.O.A. SPECS.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM ALL EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.

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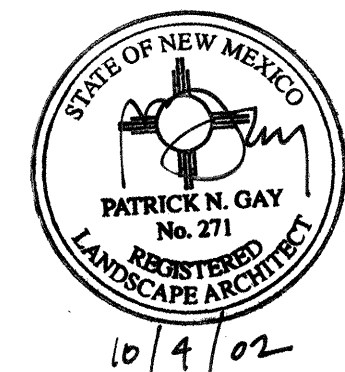
#### THE FOLLOWING NOTES ALSO APPLY WHEN CHECKED

- ☐ ALL UTILITIES AND UTILITY SERVICE LINES SHALL BE INSTALLED PRIOR TO PAVING.
- ☒ BACKFILL COMPACTION SHALL BE ACCORDING TO SPECIFIED STREET USE.
- ☐ TACK COAT REQUIREMENTS SHALL BE DETERMINED BY THE PROJECT ENGINEER.
- ☐ SIDEWALKS AND WHEELCHAIR RAMPS WITHIN THE CURB RETURNS SHALL BE CONSTRUCTED WHEREVER A NEW CURB RETURN IS CONSTRUCTED.
- ☐ IF CURB IS DEEPENED FOR A DRIVEPAD, THE DRIVEPAD SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB AND GUTTER.
- ☐ ALL STORM DRAINAGE FACILITIES SHALL BE COMPLETED PRIOR TO FINAL ACCEPTANCE.
- ☐ THE REQUESTOR OR DEVELOPER SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT OF CURB AND GUTTER SIDEWALK DAMAGED AFTER APPROVAL BY THE CITY ENGINEER OF WORK COMPLETED BY THE CONTRACTOR.



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PLANNING  
LANDSCAPE ARCHITECTURE  
MARKET ANALYSIS






| REV.                        | SHEETS | CITY ENGINEER     | DATE               | USER DEPARTMENT | DATE  | USER DEPARTMENT | DATE |
|-----------------------------|--------|-------------------|--------------------|-----------------|---|-----------------|------|
| ENGINEERS STAMP & SIGNATURE |        | APPROVALS         | ENGINEER           | DATE            | APPROVED FOR CONSTRUCTION                         |                 |      |
|                             |        | DRC Chairman      | <i>[Signature]</i> | 10-10-02        | <i>[Signature]</i> 10/17/02<br>City Engineer Date |                 |      |
|                             |        | Transportation    | <i>[Signature]</i> | 10/11/02        |   |                 |      |
|                             |        | Water/Wastewater  | <i>[Signature]</i> | 10/16/02        |   |                 |      |
|                             |        | Hydrology         | <i>[Signature]</i> | 10-11-02        |   |                 |      |
|                             |        | C.I.P.            | <i>[Signature]</i> | 10/16/02        |   |                 |      |
|                             |        | Const. Coordinat. | <i>[Signature]</i> | 10-16-02        |   |                 |      |
| CITY PROJECT NO.            |        |                   |                    | 6037.02         | SHEET 1 OF 5                                      |                 |      |





## MATERIALS LEGEND







| Symbol  |                                    | Qty     |
|---|------------------------------------|---------|
|  | Existing Boulders (45 CU.FT. each) | 9       |
|  | Boulders (45 CU.FT. each)          | 3       |
|  | 2"-4" Angular Grey Cobble          | 2840 SF |

FIELD VERIFY EXISTING 10" CI WATER LINE. MAINTAIN 10' DISTANCE FROM WATERLINE WITH TREE LOCATIONS. CONTACT LANDSCAPE ARCHITECT TO RELOCATE TREES IN FIELD IF REQUIRED.

[illegible]





|   |  |             |
|---|--|-------------|
|    | 3/4" WATER METER   |             |
|  | ALTEC 4 STATION CONTROLLER—LEIT MODEL 4004 WITH MCCOL 33" MOUNTING COLUMN WITH MKIT 4000 MOUNTING. INSTALL ACCORDING TO MANUFACTURER'S INSTRUCTIONS. |             |
|  | RAINBIRD PEB SERIES ELECTRIC 1" MASTER VALVE WITH FEBCO 3/4" 825Y PVB BACKFLOW ASSEMBLY. INSTALL IN STRONGBOX SBBC—15ALI INSULATED ENCLOSURE.        | DETAIL 2703 |
|  | SPEARS TRUE UNION SCH. 80 BALL VALVE   |             |
|  | RAINBIRD 100PEB 1" CONTROL VALVE   |             |
|  | RAINBIRD 1402 .5 GPM FLOOD BUBBLER   | DETAIL 2711 |

— · — 3/4" SCH 40 MAINLINE

———— 3/4" SCH 40 LATERAL LINE

--- IRRIGATION SLEEVES--IRRIGATION SLEEVES TO BE SIZED TWO  
--- SIZES LARGER THAN THE PIPE TO BE SLEEVED, UNLESS  
OTHERWISE NOTED ON PLANS.

## IRRIGATION NOTES

CONTRACTOR SHALL REPLACE, IN KIND, ANY EXISTING PLANT MATERIALS DAMAGED DURING THE COURSE OF WORK, AS DETERMINED BY THE OWNER'S REPRESENTATIVE. NO ADDITIONAL PAYMENT SHALL BE MADE FOR REPAIR OF DAMAGES.

CONTRACTOR SHALL REPLACE, IN KIND, ALL EXISTING STREET PAVEMENT, CURB AND GUTTER AND SIDEWALK, DISTURBED DUE TO INSTALLATION OF WATER METER, SERVICE AND SLEEVES.

REFERENCE C.O.A STD DWGS # 2301, WATERLINE CONNECTION,  
#2430, SIDEWALK, #2415, CURB AND GUTTER AND #2465,  
ARTERIAL/COLLECTOR STREET SECTION.

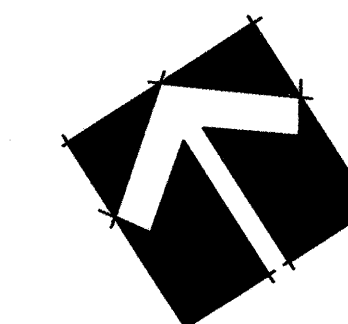
IRRIGATION SYSTEM AS SHOWN WAS DESIGNED WITH AN ESTIMATED 70 PSI STATIC AT THE POINT OF CONNECTION. CONTRACTOR SHALL FIELD VERIFY STATIC PRESSURE PRIOR TO THE START OF WORK. IF FIELD CONDITIONS ARE OTHER THAN STATED, CONTRACTOR SHALL CONTACT LANDSCAPE ARCHITECT BEFORE PROCEEDING WITH WORK.

DRI-WATER SHALL BE PLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS ON ALL PLANT MATERIALS NOT IRRIGATED BY BUBBLERS. SEE DETAIL THIS PAGE. IF PLANTED AFTER SEPTEMBER 15TH, WATER PLANT THOROUGHLY AT INSTALLATION AND INSTALL DRIWATER BETWEEN MARCH 1 AND 15TH OF FOLLOWING SPRING.

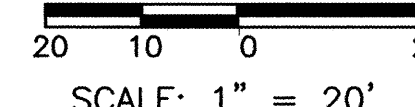
WATER METER AND SERVICE TO BE PAID BY THE CITY OF ALBUQUERQUE PER SYLVIA FETTES (FAMILY AND COMMUNITY SERVICES). MAINTENANCE AND WATER SUBSEQUENT TO CONTRACTOR'S WARRANTY TO BE PROVIDED BY COA PARKS AND RECREATION- PARK MANAGEMENT DIVISION.



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CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

1. CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.

2. CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.

3. THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.

4. CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.

5. CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.

6. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.

7. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET-UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR EQUIVALENT.

8. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.

9. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.

10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.

11. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.

12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.

13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.

14. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.

15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.

16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.

17. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.

18. ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.

19. 48 HOURS PRIOR TO OCCUPYING OR CLOSING OF A RIGHT-OF-WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.

20. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.

21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.

22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING:  
1. STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.  
2. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION.  
3. SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.

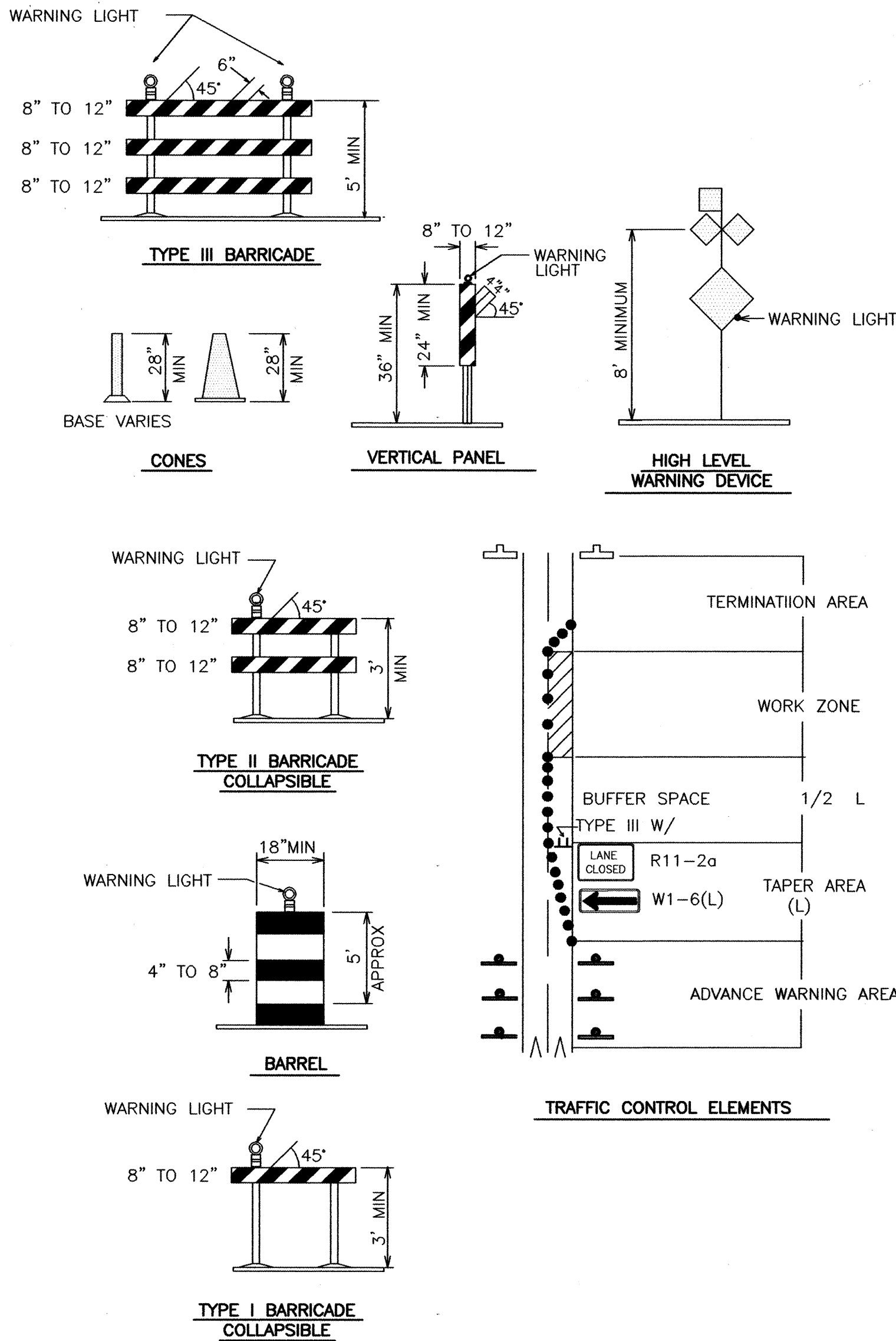
23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.

24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION. WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.

25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.

26. ADVANCE WARNING SIGNS SHALL BE 36"x36" MIN. WITH SUPER ENGINEERING GRADE SHEETING OR BETTER. MOUNTING HEIGHT AT TOP OF SIGN SHALL BE THE SAME AS FOR A 48" SIGN AS INDICATED IN THE M.U.T.C.D.

27. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORKSITE. ALL GRAFFITI SHALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND TEMPORARY.



LEGEND

- WORK AREA  
BARRICADE - TYPE I, TYPE II, OR BARREL  
BARRICADE - TYPE III  
VERTICAL PANEL  
WARNING SIGN  
DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET  
FLAGMAN POSITION  
SPACING BETWEEN BARRICADES - A DISTANCE MEASURED IN FEET EQUAL TO THE STREET LIMIT OF THE STREET  
TAPER LENGTH - SEE CHART BELOW

THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

TAPER REQUIREMENT

| SPEED LIMIT (MPH) | TAPER LENGTH(L) (FEET) |          |          | MINIMUM NUMBER OF DEVICES FOR TAPER | MAXIMUM DEVICE SPACING IN FEET |             |
|-------------------|------------------------|----------|----------|-------------------------------------|--------------------------------|-------------|
|                   | 10' LANE               | 11' LANE | 12' LANE |                                     | ALONG TAPER                    | AFTER TAPER |
| 20                | 70                     | 75       | 80       | 5                                   | 20                             | 20          |
| 25                | 105                    | 115      | 125      | 6                                   | 25                             | 25          |
| 30                | 150                    | 165      | 180      | 7                                   | 30                             | 30          |
| 35                | 205                    | 225      | 245      | 8                                   | 35                             | 35          |
| 40                | 270                    | 295      | 320      | 9                                   | 40                             | 40          |
| 45                | 450                    | 495      | 540      | 13                                  | 45                             | 45          |
| 50                | 500                    | 550      | 600      | 13                                  | 50                             | 50          |
| 55                | 550                    | 605      | 660      | 13                                  | 55                             | 55          |

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

| SPEED LIMIT (MPH) | MINIMUM DISTANCE IN FEET BETWEEN SIGNS | FROM LAST SIGN TO TAPER |
|-------------------|--|-------------------------|
| 0-20              | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 25-30             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 30-35             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 40-45             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 50-60             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |

TAPER CRITERIA

| TYPE OF TAPER         | TAPER LENGTH      |
|-----------------------|-------------------|
| UPSTREAM TAPER:       |                   |
| MERGING TAPER         | L MINIMUM         |
| SHIFTING TAPER        | 1/2 L MINIMUM     |
| SHOULDER TAPER        | 1/2 L MINIMUM     |
| TWO-WAY TRAFFIC TAPER | 100 FEET MAXIMUM  |
| DOWNSTREAM TAPERS     | 100 FEET PER LANE |

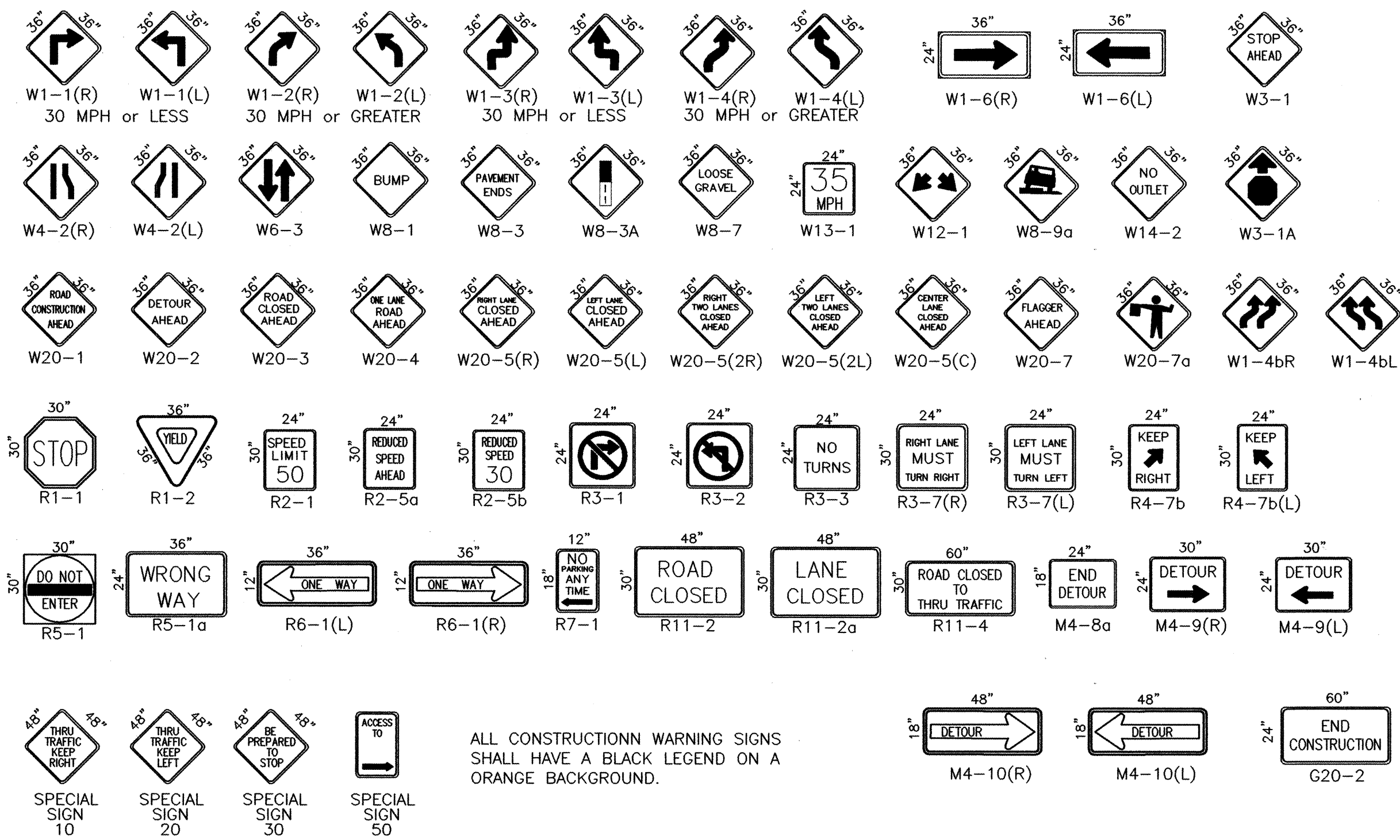
TAPER LENGTH COMPUTATION

| SPEED LIMIT       | L                     |
|-------------------|-----------------------|
| 40 MPH OR LESS    | $L = \frac{WS^2}{60}$ |
| 40 MPH OR GREATER | $L = W \times S$      |

L = TAPER LENGTH  
W = WIDTH OF OFFSET IN FEET  
S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

FOR INFORMATION ONLY

SIGN FACE DETAILS



ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.

LEGEND

- WORK AREA  
BARRICADE - TYPE I, TYPE II, OR BARREL  
BARRICADE - TYPE III  
VERTICAL PANEL  
WARNING SIGN  
DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET  
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THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

TAPER REQUIREMENT

| SPEED LIMIT (MPH) | TAPER LENGTH(L) (FEET) |          |          | MINIMUM NUMBER OF DEVICES FOR TAPER | MAXIMUM DEVICE SPACING IN FEET |             |
|-------------------|------------------------|----------|----------|-------------------------------------|--------------------------------|-------------|
|                   | 10' LANE               | 11' LANE | 12' LANE |                                     | ALONG TAPER                    | AFTER TAPER |
| 20                | 70                     | 75       | 80       | 5                                   | 20                             | 20          |
| 25                | 105                    | 115      | 125      | 6                                   | 25                             | 25          |
| 30                | 150                    | 165      | 180      | 7                                   | 30                             | 30          |
| 35                | 205                    | 225      | 245      | 8                                   | 35                             | 35          |
| 40                | 270                    | 295      | 320      | 9                                   | 40                             | 40          |
| 45                | 450                    | 495      | 540      | 13                                  | 45                             | 45          |
| 50                | 500                    | 550      | 600      | 13                                  | 50                             | 50          |
| 55                | 550                    | 605      | 660      | 13                                  | 55                             | 55          |

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

| SPEED LIMIT (MPH) | MINIMUM DISTANCE IN FEET BETWEEN SIGNS | FROM LAST SIGN TO TAPER |
|-------------------|--|-------------------------|
| 0-20              | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 25-30             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 30-35             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 40-45             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |
| 50-60             | 10 X SPEED LIMIT                       | 10 X SPEED LIMIT        |

TAPER CRITERIA

| TYPE OF TAPER         | TAPER LENGTH      |
|-----------------------|-------------------|
| UPSTREAM TAPER:       |                   |
| MERGING TAPER         | L MINIMUM         |
| SHIFTING TAPER        | 1/2 L MINIMUM     |
| SHOULDER TAPER        | 1/2 L MINIMUM     |
| TWO-WAY TRAFFIC TAPER | 100 FEET MAXIMUM  |
| DOWNSTREAM TAPERS     | 100 FEET PER LANE |

TAPER LENGTH COMPUTATION

| SPEED LIMIT       | L                     |
|-------------------|-----------------------|
| 40 MPH OR LESS    | $L = \frac{WS^2}{60}$ |
| 40 MPH OR GREATER | $L = W \times S$      |

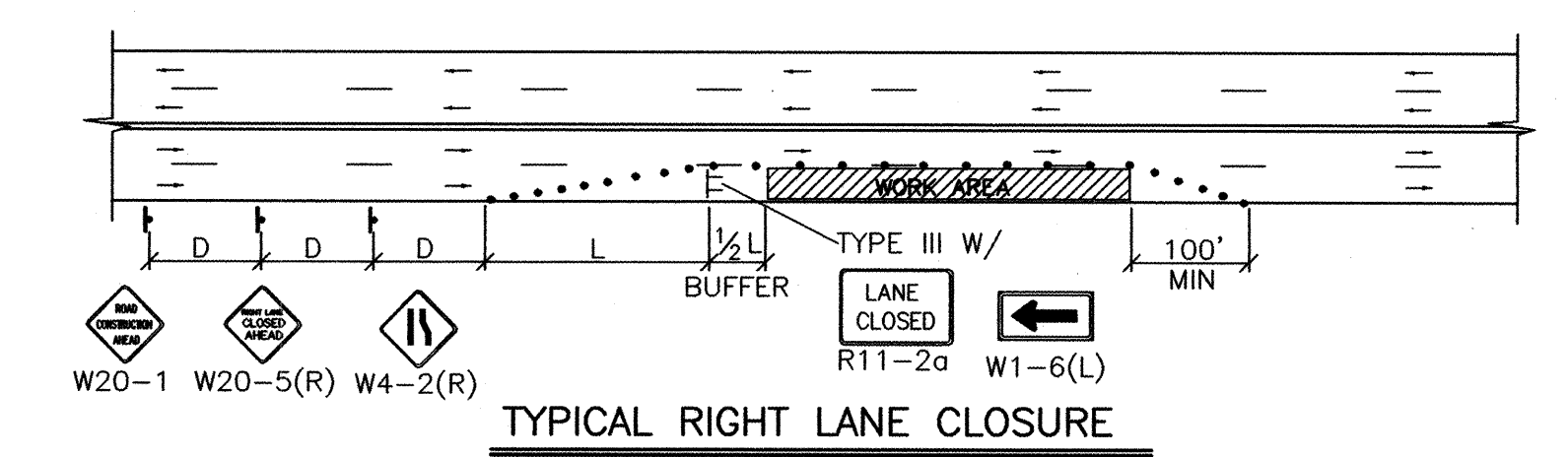
L = TAPER LENGTH  
W = WIDTH OF OFFSET IN FEET  
S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

FOR INFORMATION ONLY

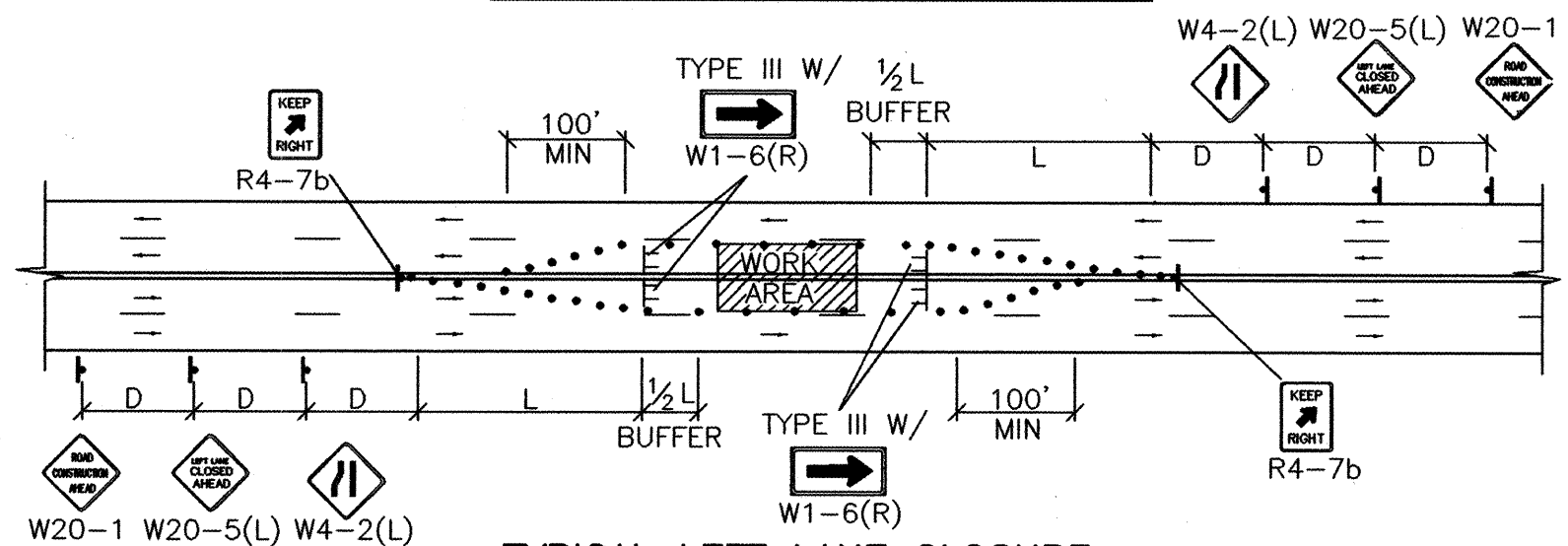
CITY OF ALBUQUERQUE  
PUBLIC WORKS DEPARTMENT  
FAMILY AND COMMUNITY SERVICES

TITLE: TRAFFIC CONTROL  
SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS  
DESIGN REVIEW COMMITTEE  
CITY ENGINEER APPROVAL  
MO./DAY/YR.  
CITY PROJECT NO. 6037.02  
ZONE MAP NO. H-23-Z  
SHEET 4 OF 5

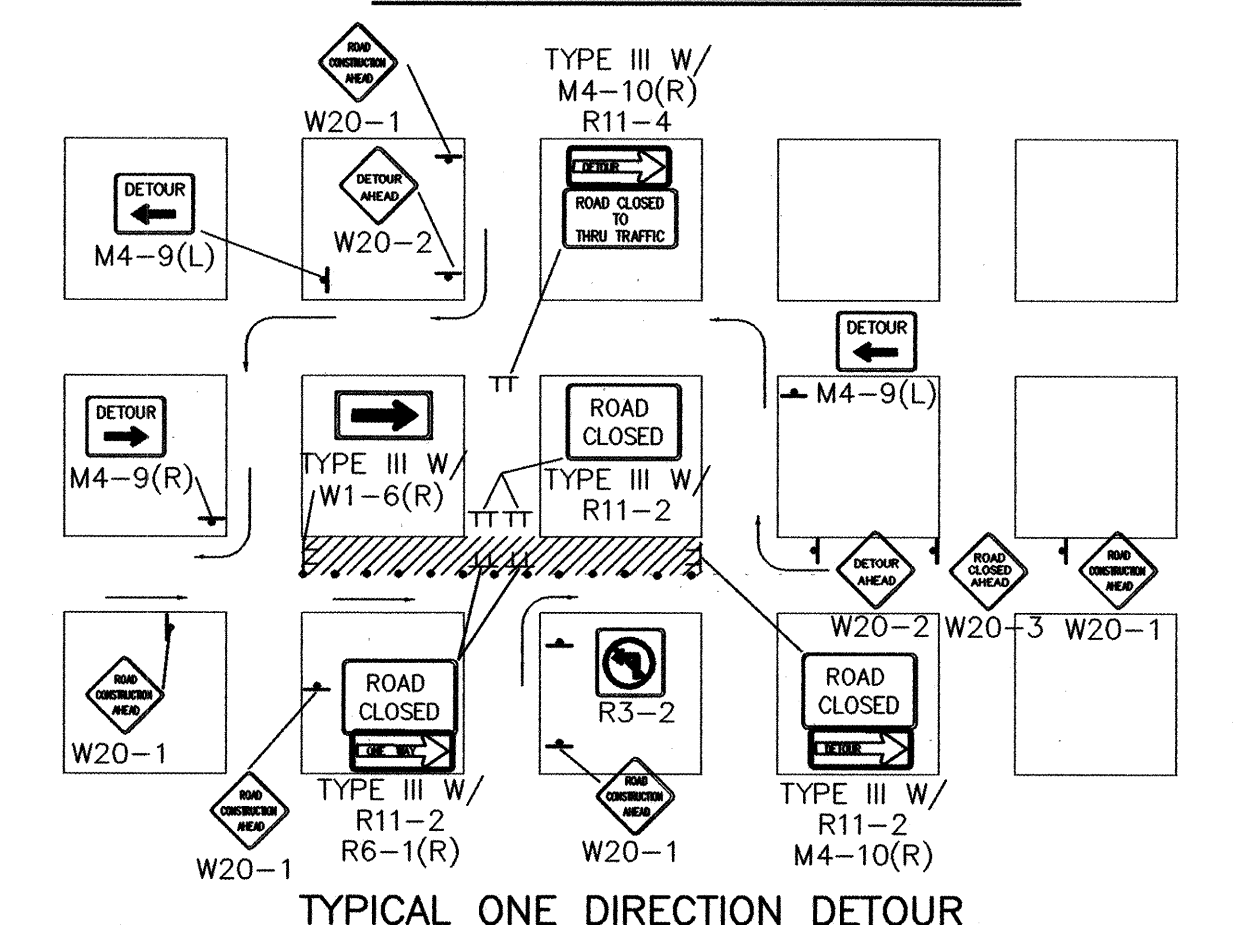




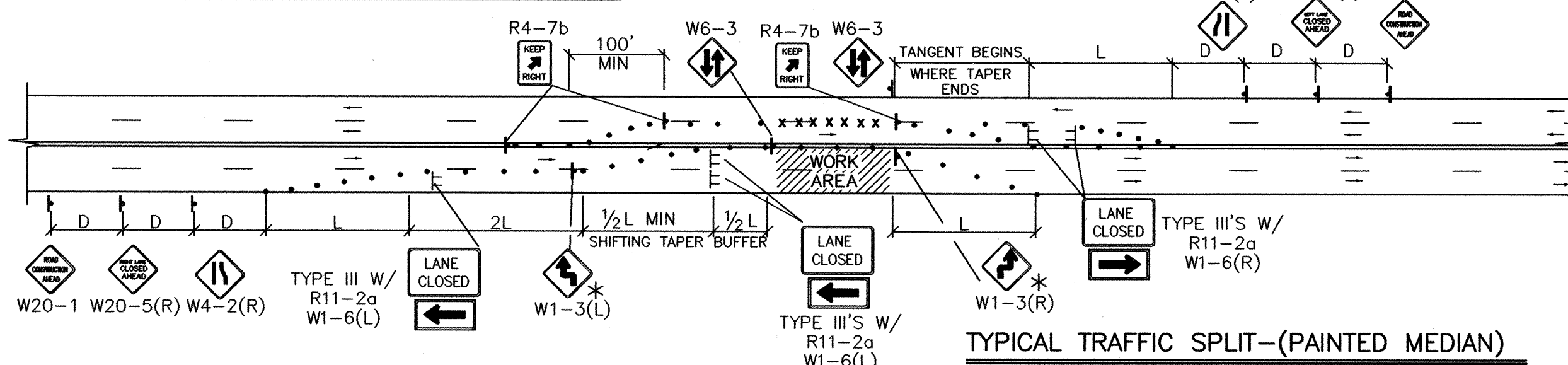
TYPICAL RIGHT LANE CLOSURE



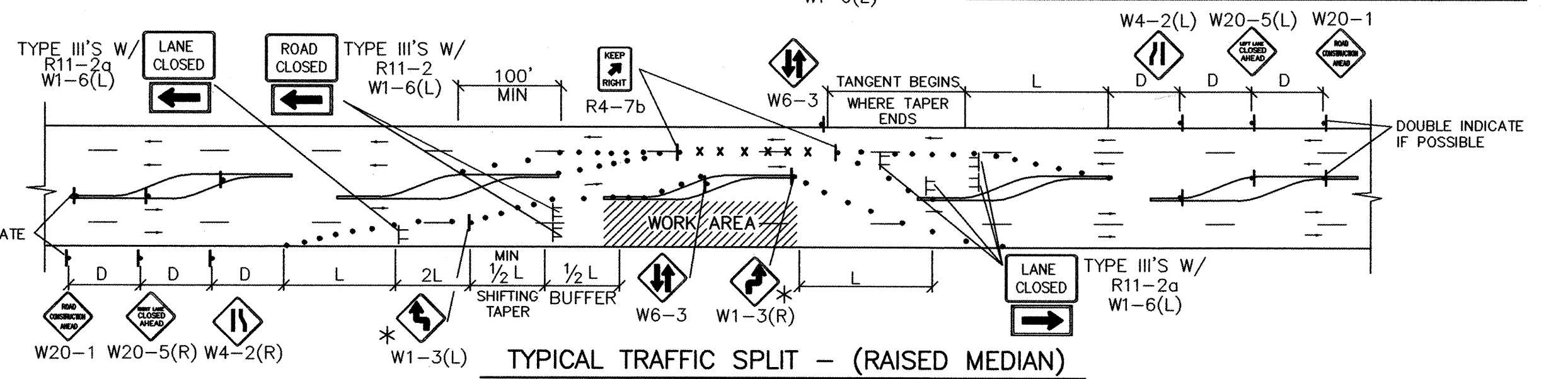
TYPICAL LEFT LANE CLOSURE



TYPICAL ONE DIRECTION DETOUR

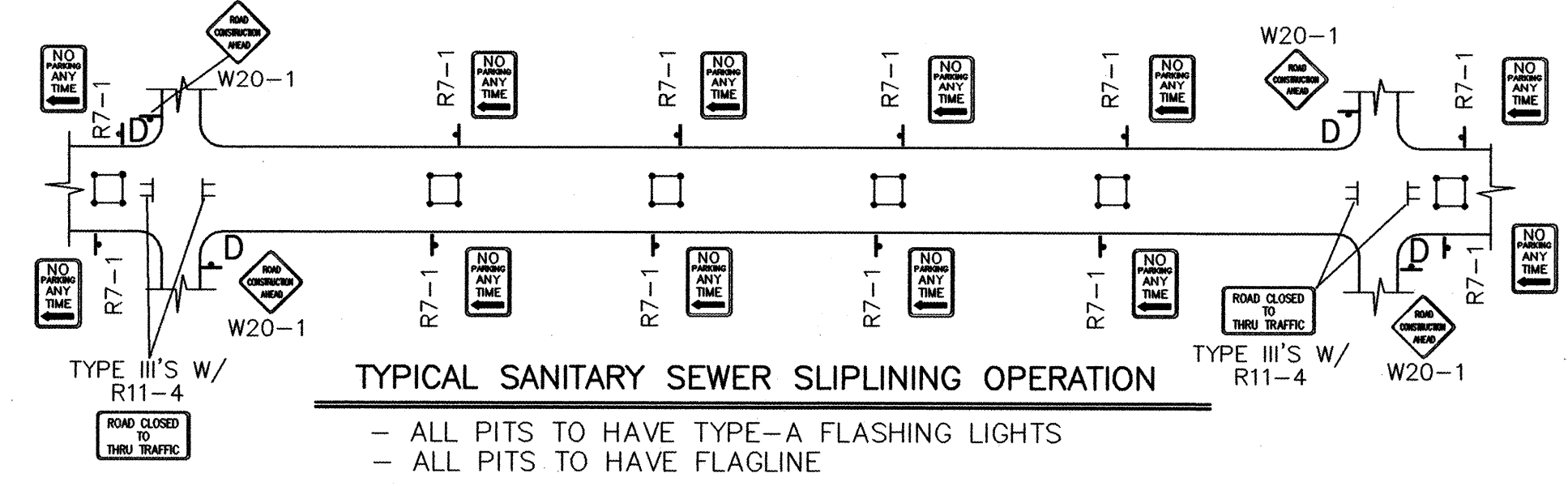


TYPICAL TRAFFIC SPLIT - (PAINTED MEDIAN)



TYPICAL TRAFFIC SPLIT - (RAISED MEDIAN)

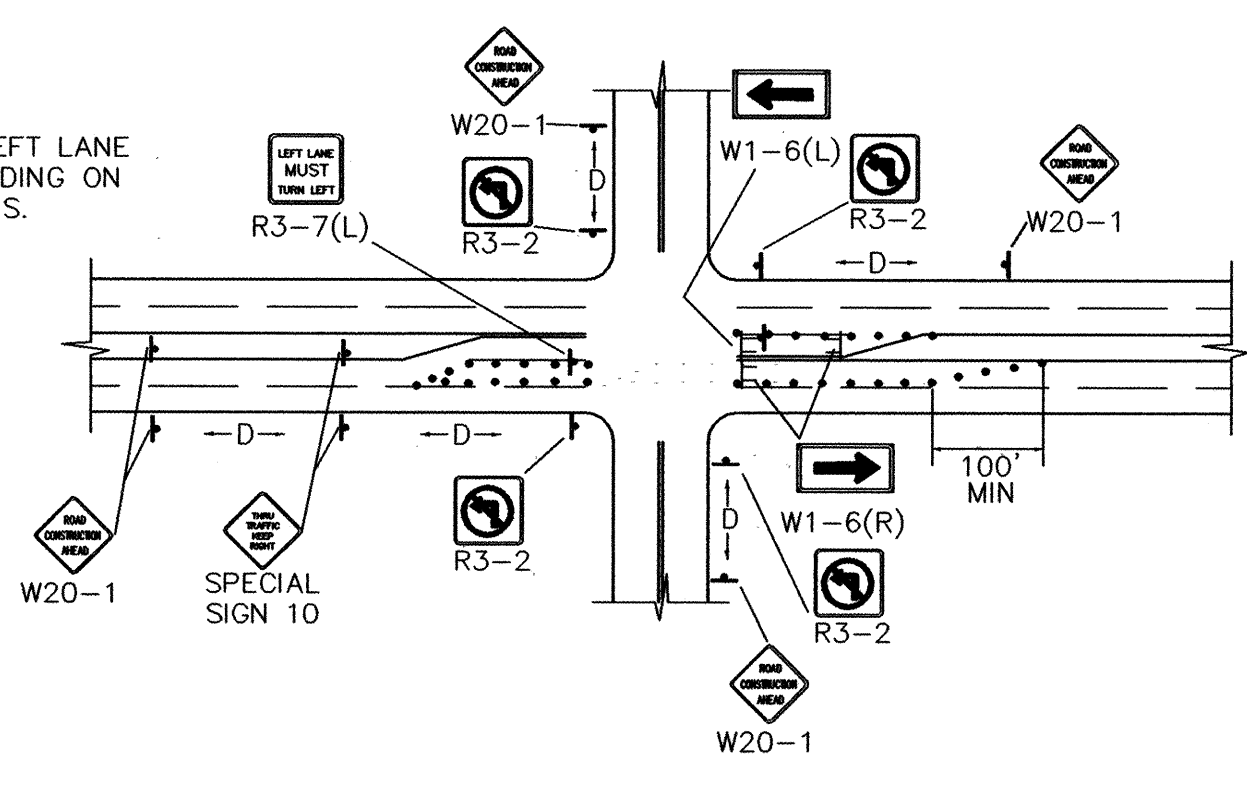
- TRAFFIC SPLIT NOTES:
1. THE OFFSET DISTANCE MUST BE CALCULATED IN ALL SHIFTING TAPERS. THE OFFSET DISTANCES SHALL INCLUDE LANE WIDTHS PLUS MEDIAN WIDTHS.
  2. 1/2 L IS THE MINIMUM DISTANCE FOR SHIFTING TAPERS.
  3. REVERSE CURVES MAY BE IMPLEMENTED. ALL CURVE DATA SHALL BE CALCULATED.
  4. MEDIAN REMOVAL SHALL BE REQUIRED IF 1/2 L OR REVERSE CURVE IS NOT SUFFICIENT. MEDIAN REMOVAL SHALL TAKE PLACE BEFORE SPLITS.
  5. REDUCED SPEED MAY BE CONSIDERED.
  6. \* USE W1-3 FOR 30 MPH OR LESS, W1-4 FOR SPEED 35 MPH OR GREATER.



TYPICAL SANITARY SEWER SLIPLINING OPERATION

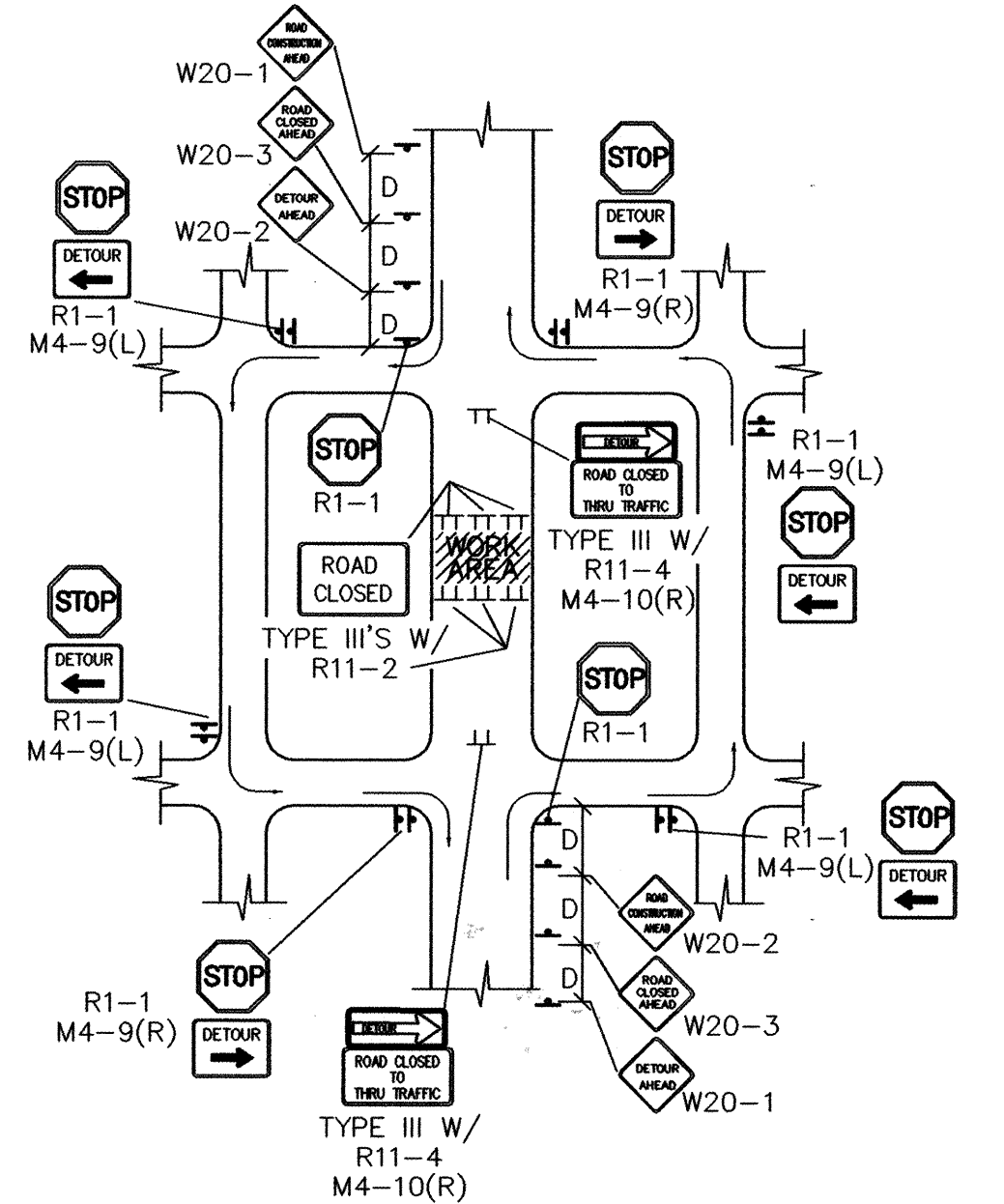
- ALL PITS TO HAVE TYPE-A FLASHING LIGHTS
- ALL PITS TO HAVE FLAGLINE

NOTE:  
MAY REQUIRE LEFT LANE  
CLOSURE DEPENDING ON  
FIELD CONDITIONS.



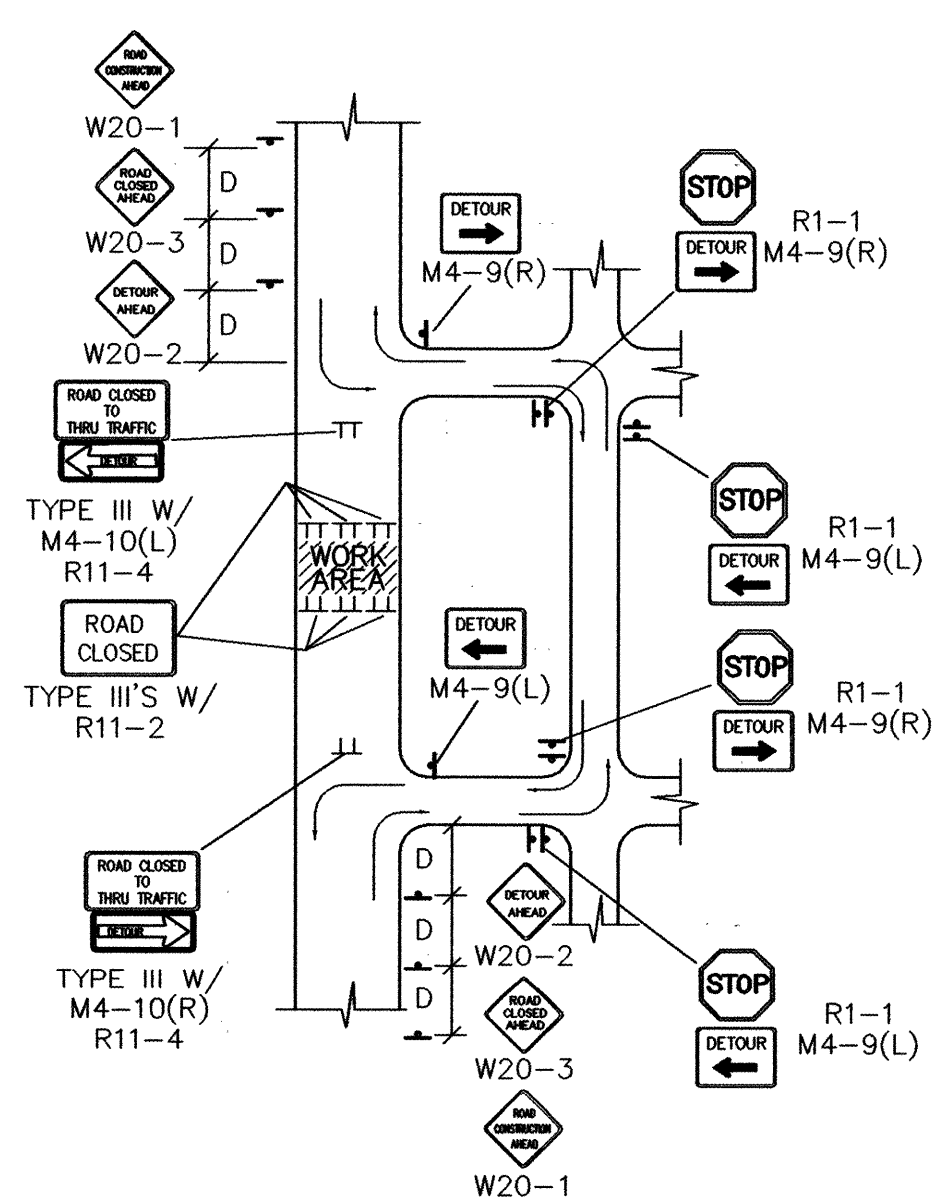
TYPICAL LANE CLOSURE AT INTERSECTION

NOTE: DEPENDING ON WORK ZONE LOCATION.



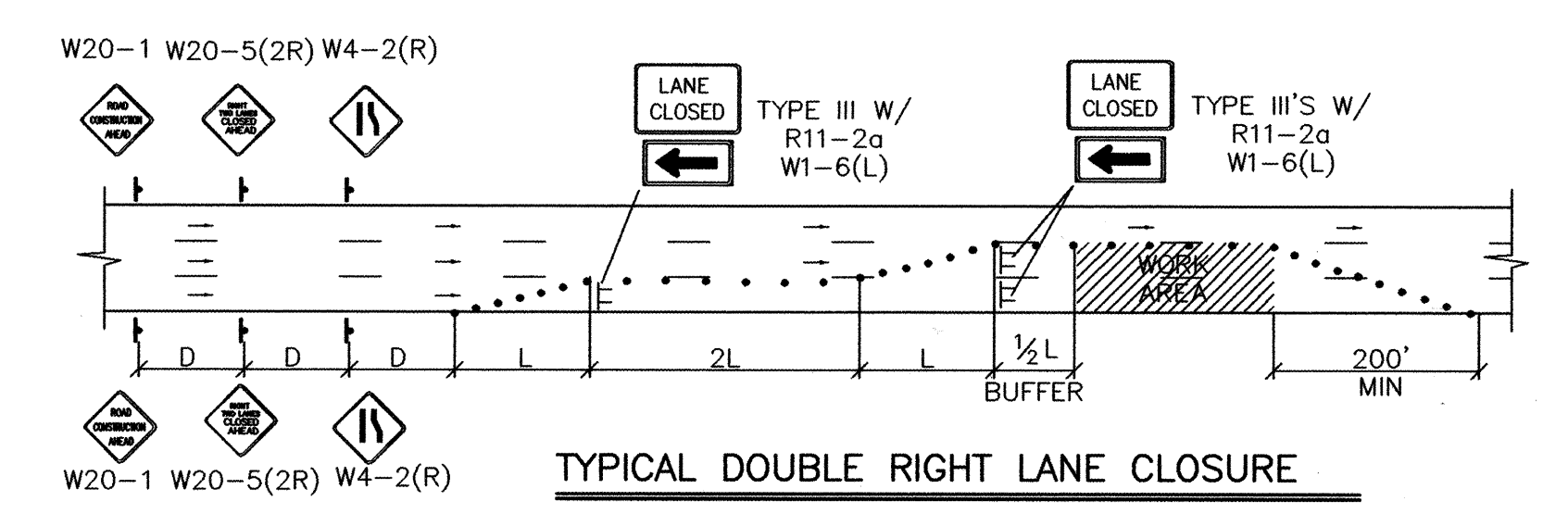
TYPICAL MID-BLOCK CLOSURE

NOTE: (R1-1) BASED ON FIELD CONDITIONS.

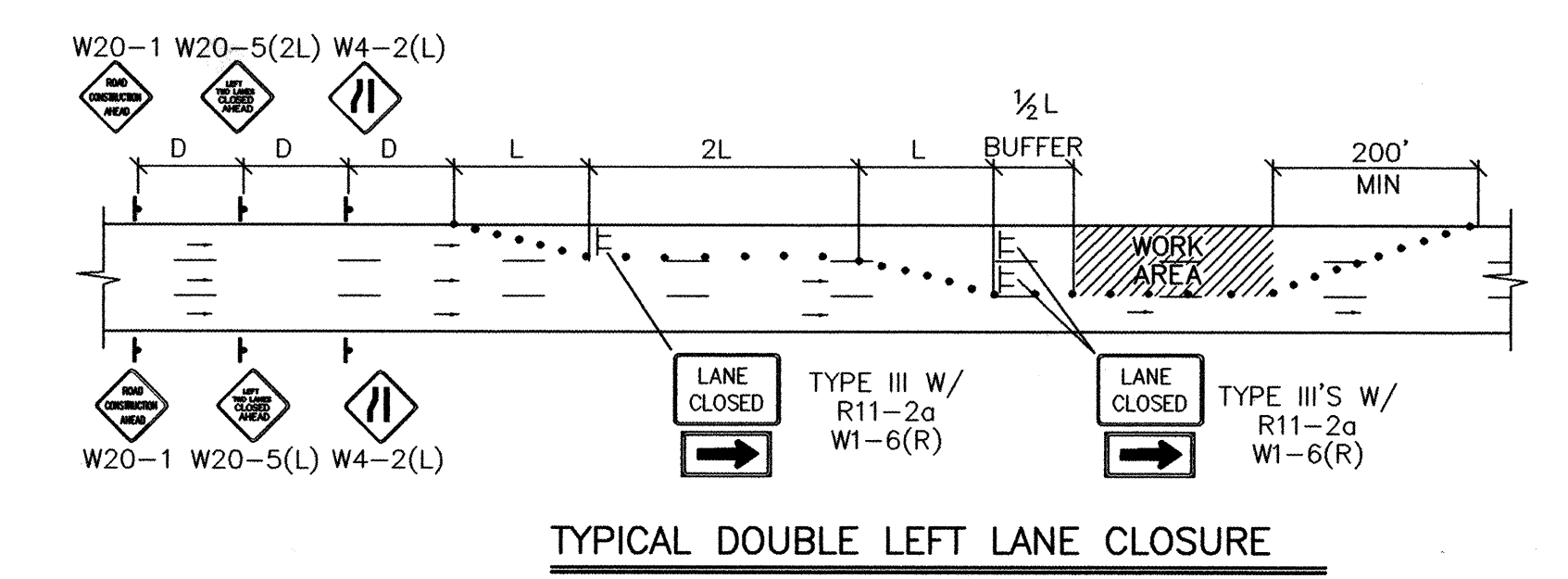


TYPICAL STREET CLOSURE

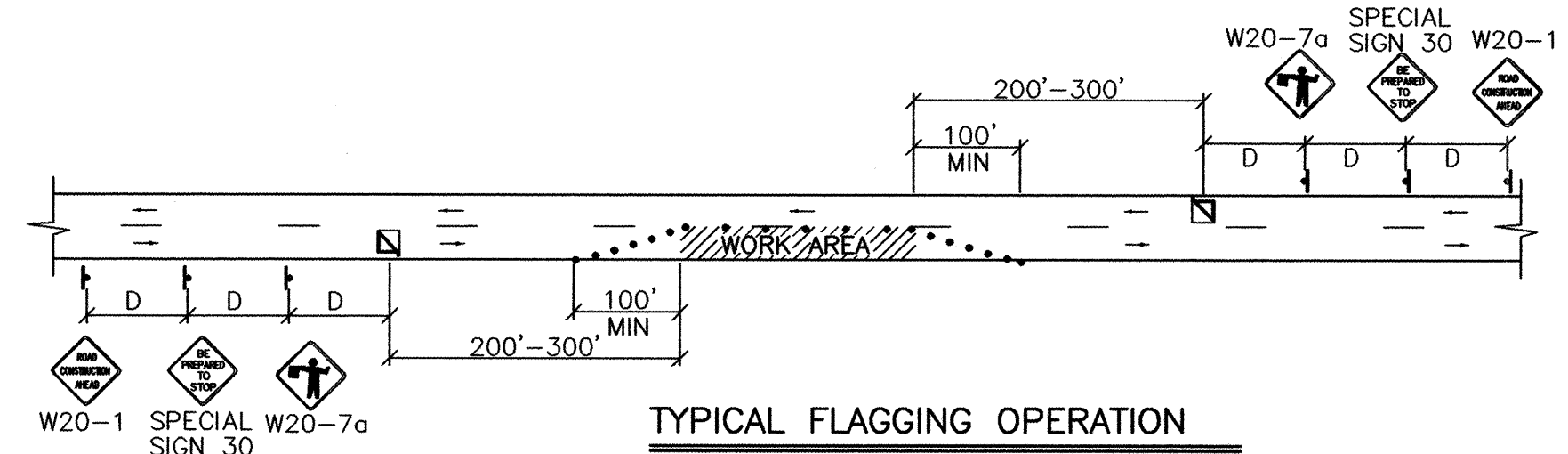
NOTE: (R1-1) BASED ON FIELD CONDITIONS.



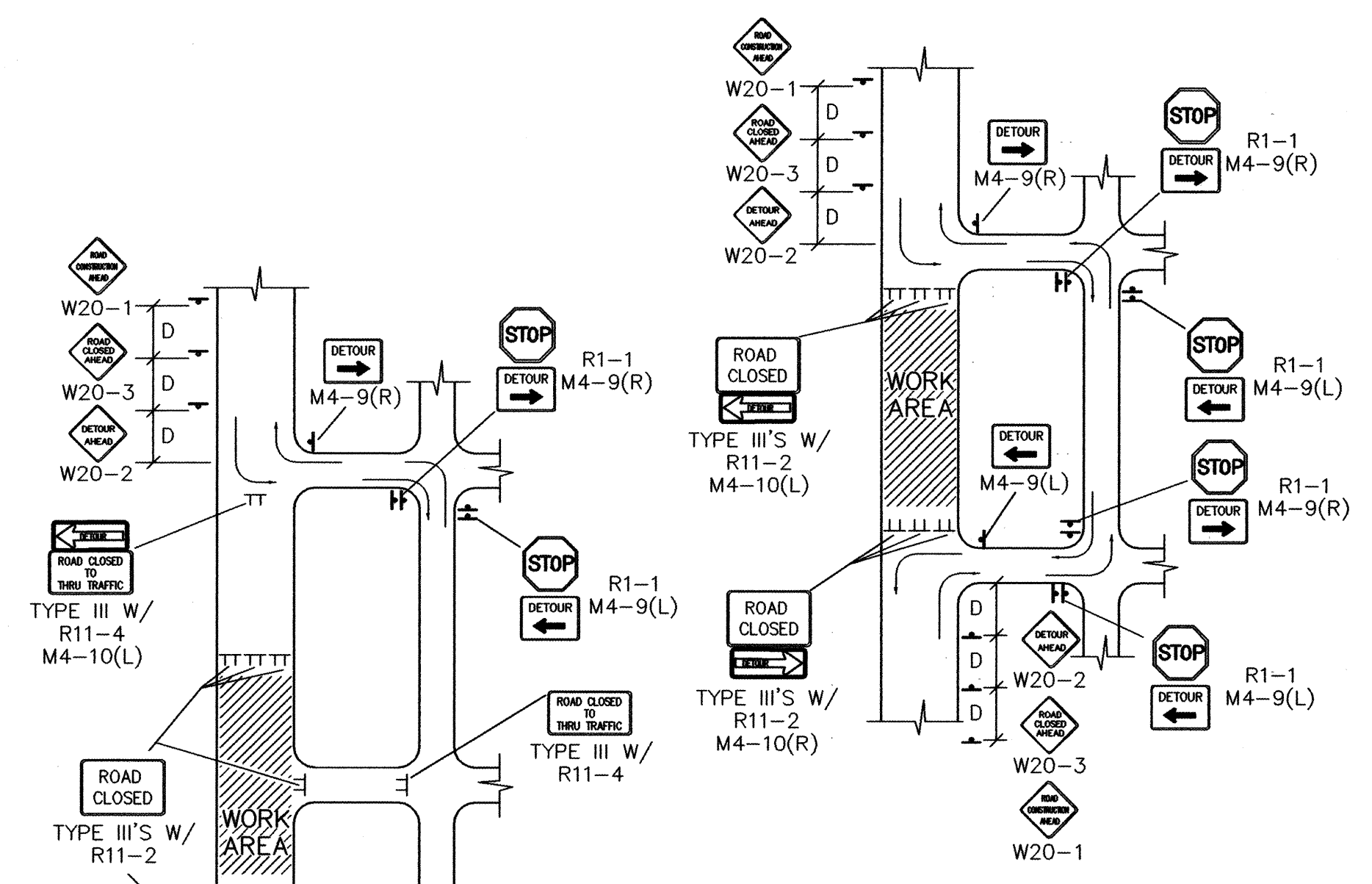
TYPICAL DOUBLE RIGHT LANE CLOSURE



TYPICAL DOUBLE LEFT LANE CLOSURE



TYPICAL FLAGGING OPERATION



TYPICAL STREET CLOSURE

NOTE: (R1-1) BASED ON FIELD CONDITIONS.

FOR INFORMATION ONLY

|   |                        |                     |              |
|---|------------------------|---------------------|--------------|
| <p><b>CITY OF ALBUQUERQUE</b><br/>PUBLIC WORKS DEPARTMENT<br/>ENGINEERING DEVELOPMENT GROUP</p> |                        |                     |              |
| <p>TITLE: TYPICAL TRAFFIC CONTROL &amp; SIGNING EXAMPLES (REF MUTCD)</p>                        |                        |                     |              |
| DESIGN REVIEW COMMITTEE   | CITY ENGINEER APPROVAL | MO./DAY/YR.         | MO./DAY/YR.  |
| <p>DESIGNED BY COA<br/>DRAWN BY COA<br/>CHECKED BY COA</p>                                      |                        | DATE 09/27/0        | DATE 09/27/0 |
| <p>CITY PROJECT NO. 6037.02</p>   |                        | ZONE MAP NO. H-23-Z | SHEET 5 OF 5 |