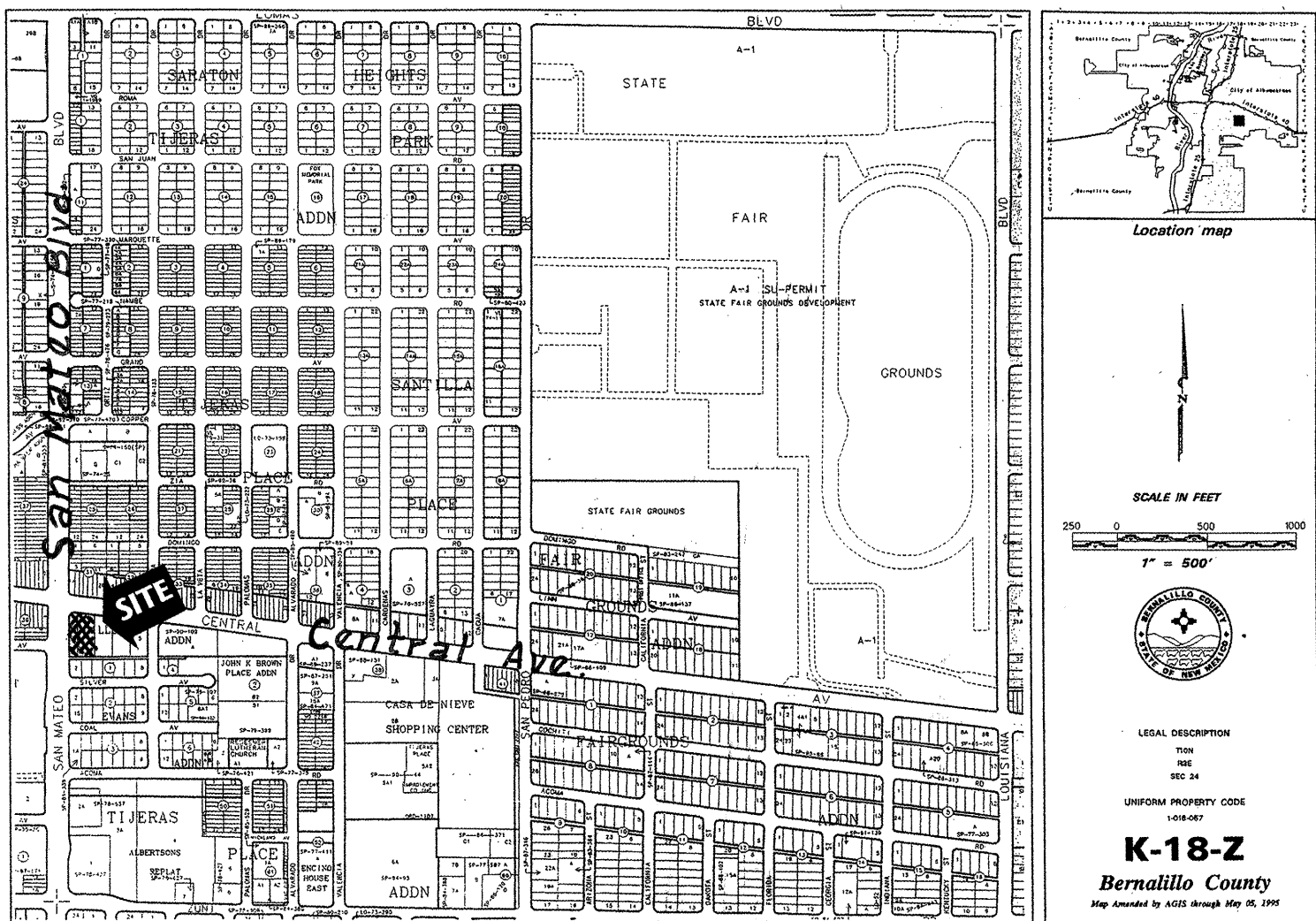


CONSTRUCTION DRAWINGS

FOR

OCTOPUS CARWASH ALLEY EXPANSION

FOR INFORMATION ONLY



SITE LOCATION

GENERAL NOTES

NOTICE TO CONTRACTORS

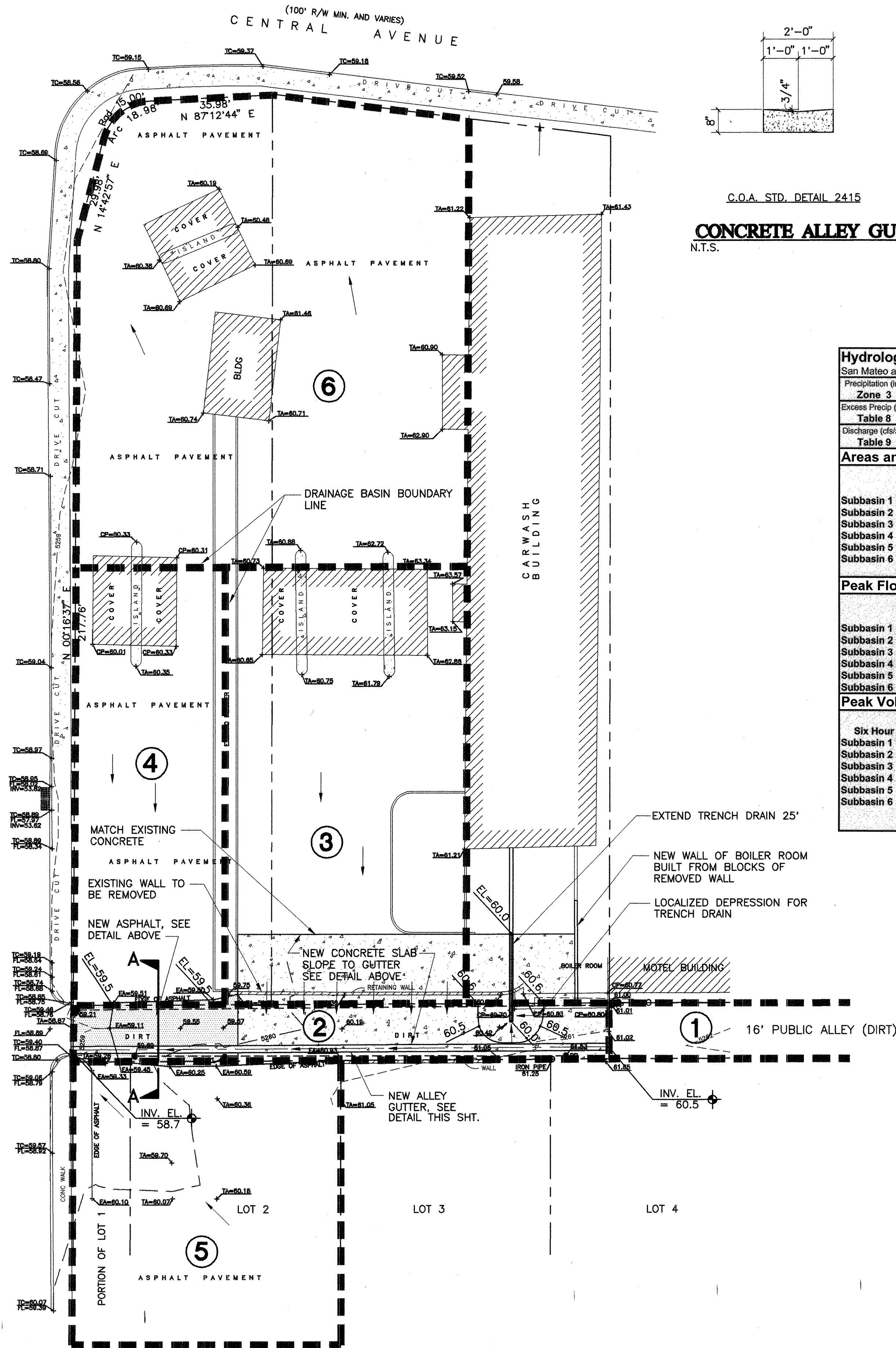
- CITY OF ALBUQUERQUE SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION AS AMENDED THROUGH UPDATE 6, WILL BE REFERRED TO HEREIN AS THE "STANDARD SPECIFICATIONS".
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) AND DETERMINE LOCATION OF EXISTING UTILITIES.
- PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR THE SURVEYOR SO THAT THE CONFLICT CAN BE RESOLVED WITHOUT DELAY.
- ALL EXISTING SIGNS, MARKERS, DELINEATORS, ETC., WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED, STORED AND RE-SET BY THE CONTRACTOR.
- FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONSTRUCTION COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (768-2551) PRIOR TO OCCUPYING AN INTERSECTION. SEE SECTION 19 OF THE SPECIFICATIONS.
- ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED WITH PLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR IN EXISTING OR NEW LOCATION AS INDICATED BY THIS SET.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE ENGINEER AND SHALL NOTIFY THE ENGINEER AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
- ANY WORK OCCURRING WITHIN AN ARTERIAL ROADWAY REQUIRES 24 HOURS CONSTRUCTION.

INDEX OF SHEETS

- C1 COVER SHEET
- C2 GRADING AND DRAINAGE PLAN
- C3 PLAN AND PROFILE
- C4 SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS
- C5 TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES (REF. M.U.T.C.D.)

REV.	SHEETS	CITY ENGINEER	DATE	USER DEPARTMENT	DATE	USER DEPARTMENT	DATE
ENGINEERS STAMP & SIGNATURE		APPROVALS		ENGINEER	DATE	*****	
		DRC Chairman	<i>William J. Rodriguez</i>	7-1-99	7-1-99	APPROVED FOR CONSTRUCTION	
		Transportation	<i>W. J. Rodriguez</i>	5-4-99	5-4-99	<i>Frank J. Gugin</i> 11-21-99 City Engineer Date	
		Water/Wastewater	<i>R. W. Rame</i>	5-4-99	5-4-99		
		Hydrology	<i>J. Rame</i>	5-4-99	5-4-99		
		CIP					
Constr. Mngmt.							
Constr. Coord.							
City Project No.							
				611681		Sheet	No.
						1	5

SAN MATEO BOULEVARD S.E.



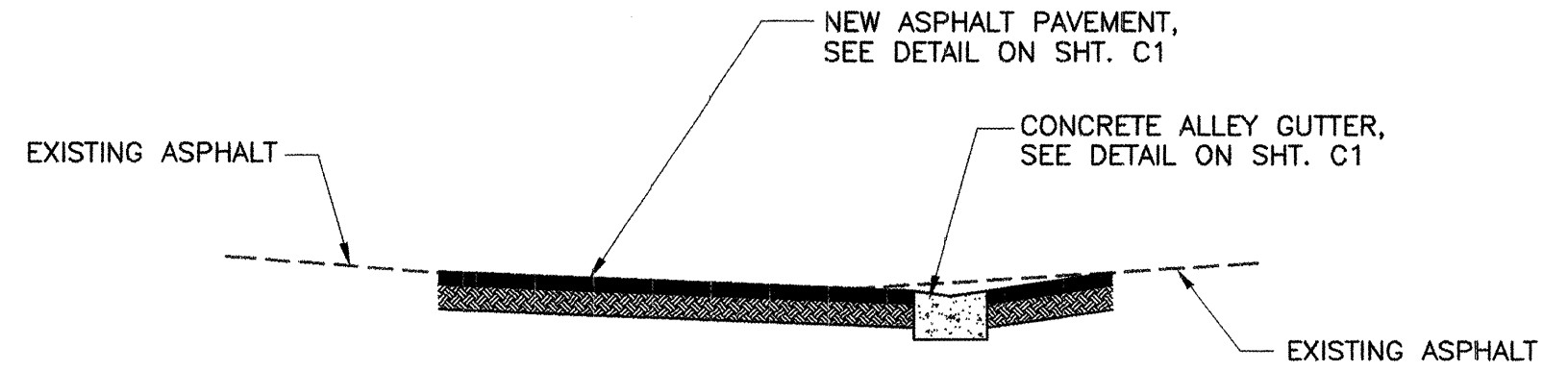
C.O.A. STD. DETAIL 2415

CONCRETE ALLEY GUTTER
N.T.S.

ASPHALT SECTION
N.T.S.

CONCRETE SECTION
N.T.S.

Hydrologic Calculations - COA DPM 22.2									
San Mateo and Central Location					Octopus Car Wash				
Precipitation (in.)					August 21, 1998				
Zone 3					Six Hour Storm				
Excess Precip (in.)					Table 8				
Discharge (cfs/ft)					Table 9				
Areas and Land Treatments:									
Land Treatments - Existing Conditions					Land Treatments - Fully Developed Conditions				
Subbasin 1					Subbasin 1				
Subbasin 2					Subbasin 2				
Subbasin 3					Subbasin 3				
Subbasin 4					Subbasin 4				
Subbasin 5					Subbasin 5				
Subbasin 6					Subbasin 6				
Peak Flow Rates Generated:									
Peak Flow Rate - Existing Conditions					Peak Flow Rate - Developed Conditions				
Subbasin 1					Subbasin 1				
Subbasin 2					Subbasin 2				
Subbasin 3					Subbasin 3				
Subbasin 4					Subbasin 4				
Subbasin 5					Subbasin 5				
Subbasin 6					Subbasin 6				
Peak Volumes									
Runoff Volume - Existing Conditions					Runoff Volume - Developed Conditions				
Subbasin 1					Subbasin 1				
Subbasin 2					Subbasin 2				
Subbasin 3					Subbasin 3				
Subbasin 4					Subbasin 4				
Subbasin 5					Subbasin 5				
Subbasin 6					Subbasin 6				



SECTION A-A
N.T.S.

Site Location - As shown by the Vicinity Map (Zone Atlas Map K-18), the 0.89 acre site on the southeast corner of Central Boulevard and San Mateo Boulevard SE. At present, the site is developed and drains roughly from east to west onto San Mateo Boulevard. The purpose of this project is to reclaim a 16 foot wide portion of the adjacent alleyway to the south for use as additional staging for entering the car wash building. The vast majority of the surrounding area is currently developed, thereby making this a modification to an existing site within an infill area. The proposed improvements consist of 1,680 square feet of additional concrete paving and 750 square feet of additional asphalt paving.

Legal Description - Lots 1 and 2 Gillett Addition, Albuquerque, New Mexico.

Benchmark - Basis of elevation is from City of Albuquerque control station "6-K18A" with elevation stamped 5,247.33 feet above sea level.

Flood Zone - As shown by Panel 354 of 825 of the National Flood Insurance Program Flood Insurance Rate Maps (FIRM) for the City of Albuquerque, New Mexico, dated September 20, 1996, a portion of this site does lie within a designated Flood Hazard Zone AO. This is due to the limited capacities of Central Boulevard and San Mateo Boulevard in these locations.

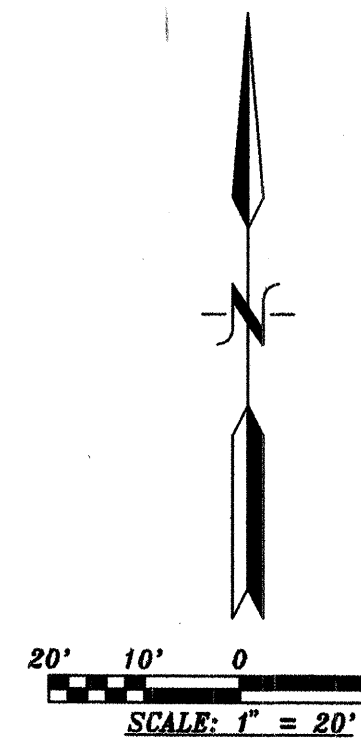
Existing Conditions - Currently, the project site drains from east to west over the paved parking area associated with the gas station and car wash. Runoff from the unpaved alleyway is limited to only the runoff that falls onto the alley itself between San Mateo Boulevard and Madeira Drive SE, a distance of approximately 460 ft. The runoff generated in the unimproved alleyway currently discharges to the west into San Mateo Boulevard utilizing the existing driveway. The property to the South also discharges to the driveway. A combination drop inlet is located approximately 60 feet north of the southwest driveway.

Proposed Grading - The Grading and Drainage Plan shows 1) existing and proposed grades indicated by spot elevations and contours at one foot intervals with continuity between existing and proposed grades; 2) the limit of existing and proposed improvements. The unimproved alley south of the project site is to be improved with concrete pavement. The ensuing area will be graded and paved to flow west in a proposed valley gutter to the existing driveway as shown on the plan. A negligible increase in runoff will discharge to the existing catch basin in San Mateo Boulevard west of the site.

Hydrologic Methods - The drainage basin map shows eight separate subbasins 1 through 6 to assess peak flow rates at various points around the project site culminating on San Mateo Boulevard SE. The calculations which appear hereon analyze both the existing and developed conditions for the 100-year, 6-hour rainfall event. The process outlined in the DPM, Section 22.2 was used to quantify the peak flow rates and volumes. As shown by these calculations, the fully developed commercial improvements will result in a minimal increase in runoff generated by the site.

The subject property increases the existing peak runoff by less than one cubic foot per second as shown on the calculations. A spreadsheet for Precipitation Zone 3 is included on this plan. This spreadsheet outlines the peak runoff and volume generated for each subbasin for existing and proposed fully developed conditions.

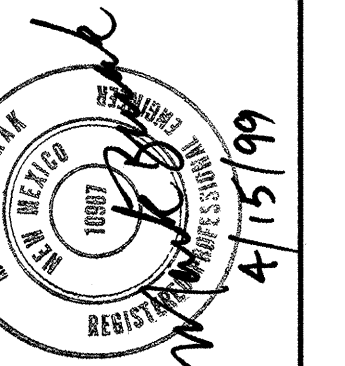
FOR INFORMATION ONLY

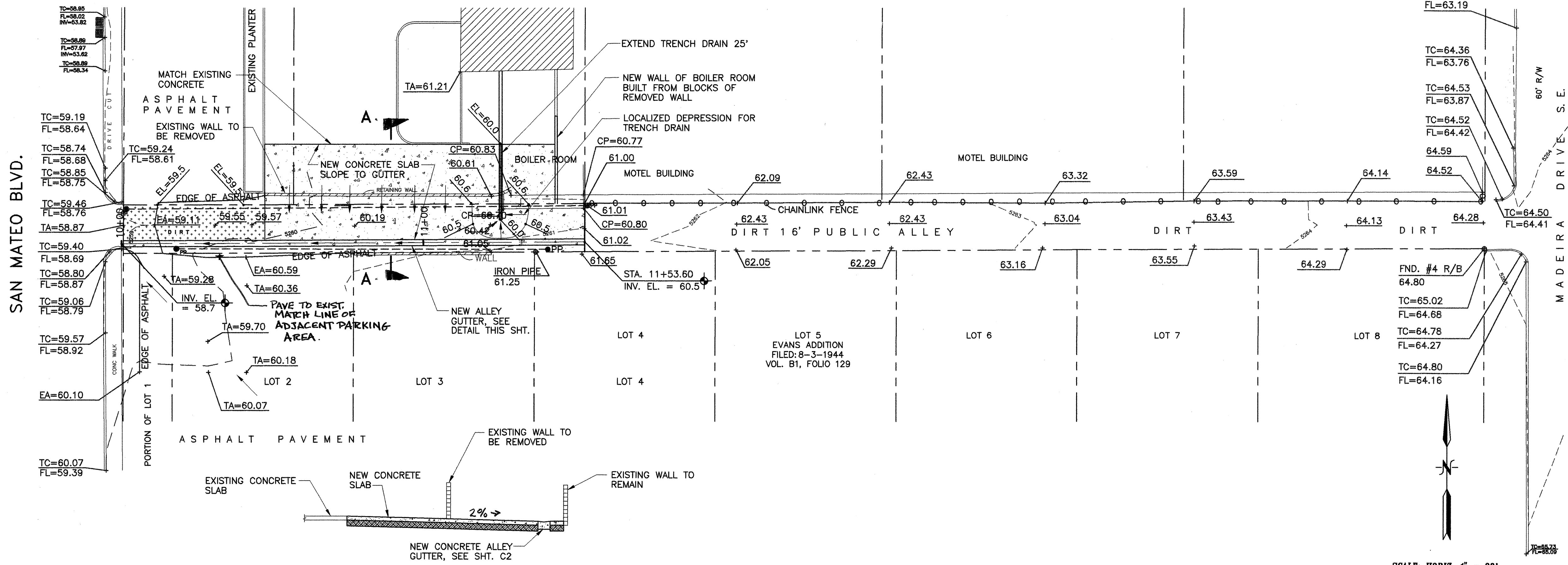


Approved
For Inclusion
Only

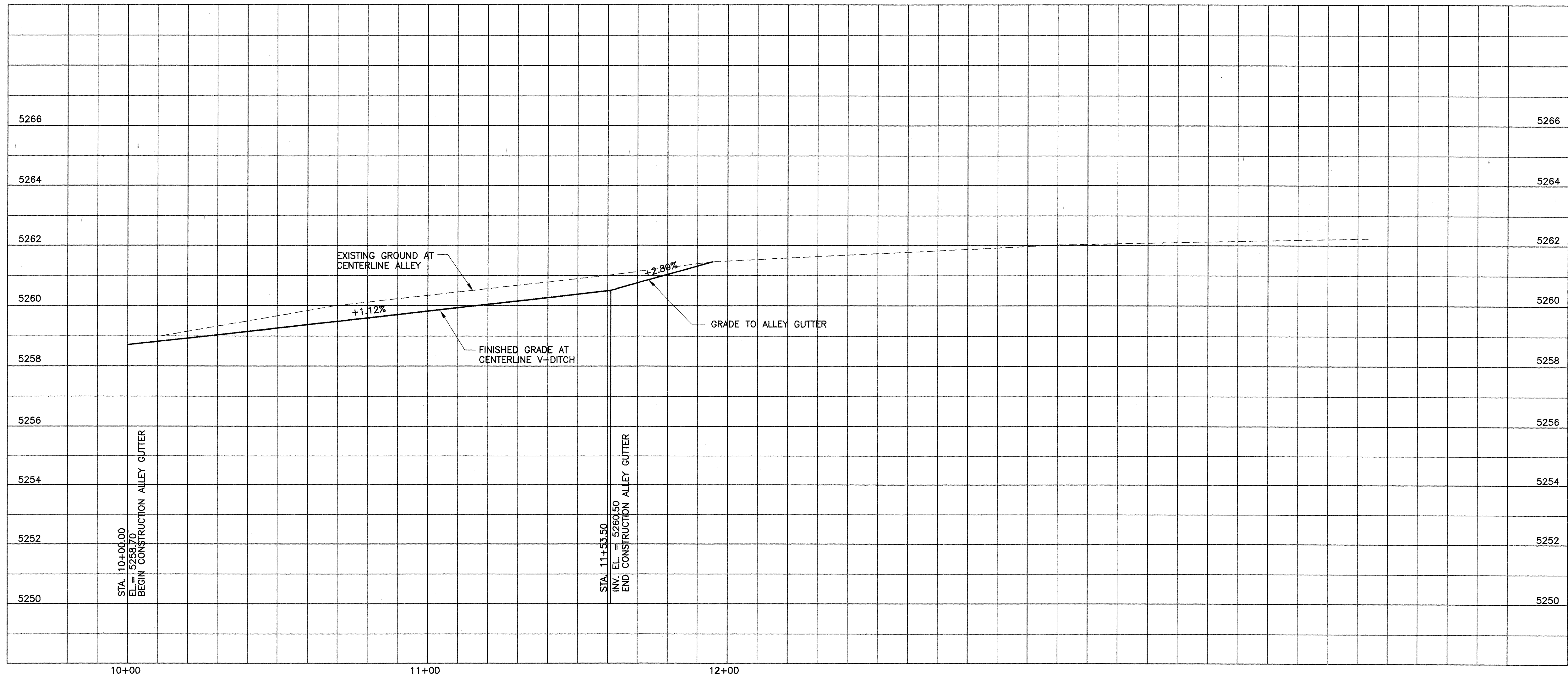
CITY OF ALBUQUERQUE MUNICIPAL DEVELOPMENT DEPARTMENT ENGINEERING DIVISION			
TITLE: OCTOPUS CARWASH ALLEY EXPANSION GRADING AND DRAINAGE PLAN			
Design Review Committee APPROVED JUL - 1 1999 DESIGN REVIEW COMMITTEE	City Engineer Approval APPROVED NOV 24 1999 CITY ENGINEER	Mo / Day / Yr	Mo / Day / Yr
City Project No. 641681	Zone Map No. K-18	Sheet 2	Of 5

AS BUILT INFORMATION		BENCH MARKS		SURVEY INFORMATION		ENGINEER'S SEAL		REVISIONS		DESIGN		DATE		DATE		DATE	
CONTRACTOR	DATE	CONTRACTOR	DATE	NO.	BY	NO.	BY	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE
WORK STARTED BY	DATE	WORK STOPPED BY	DATE	NO.	BY	NO.	BY	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE
ACCEPTANCE BY	DATE	FIELD CHECK BY	DATE	NO.	BY	NO.	BY	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE
DRAWINGS CORRECTED BY	DATE	RECORDED BY	DATE	NO.	BY	NO.	BY	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE
MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION	
NO.		NO.		NO.		NO.		NO.		NO.		NO.		NO.		NO.	





SECTION A-A
N.T.S.



AS BUILT INFORMATION		BENCH MARKS		SURVEY INFORMATION		ENGINEER'S SEAL	
CONTRACTOR	DATE	City of Albuquerque Central Station "6-KBA" Located in the NW Corner of Jackson and Central Avenues - Brass cap in sidewalk Elevation = 5247.33 MSL	FIELD NOTES	DATE		NO. DATE REMARKS REVISIONS DESIGN	DESIGNED BY DRAWN BY CHECKED BY
WORKED BY	DATE		BY				
INSPECTOR'S ACCEPTANCE BY	DATE		NO.				
VERIFICATION BY	DATE		DATE				
MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION		MICRO-FILM INFORMATION	
RECORDED BY	DATE	RECORDED BY	DATE	RECORDED BY	DATE	RECORDED BY	DATE

FOR INFORMATION ONLY

FOR INFORMATION ONLY

CITY OF ALBUQUERQUE
MUNICIPAL DEVELOPMENT DEPARTMENT
ENGINEERING DIVISION

TITLE: **OCTOPUS CARWASH ALLEY EXPANSION
PLAN AND PROFILE**

Design Review Committee: Approved: Approved:

City Project No. **611681** Zone Map No. **K-18** Sheet **3** Of **5**

CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

- CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.
- CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.
- THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.
- CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.
- CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.
- ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET-UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR EQUIVALENT.
- CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
- ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.
- ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.
- ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
- CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.
- EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELLED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.
- CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.
- CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.
- ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- 48 HOURS PRIOR TO OCCUPANCY OR CLOSING OF A RIGHT-OF-WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.
- ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.


21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.

22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING:
1. STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
2. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION.
3. SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.

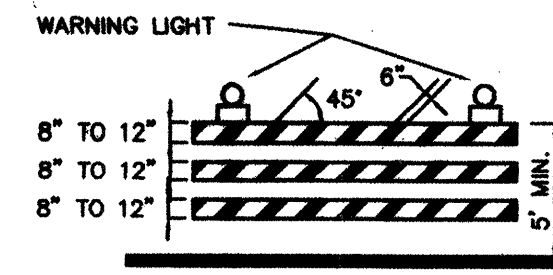
23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.

24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION. WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.

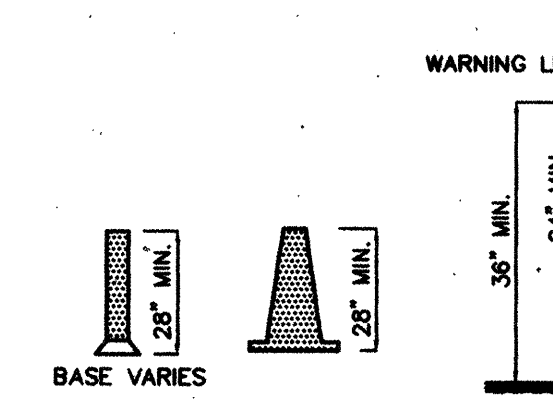
25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.

26. ADVANCE WARNING SIGNS SHALL BE 36"x36" MIN. WITH SUPER ENGINEERING GRADE SHEETING OR BETTER. MOUNTING HEIGHT AT TOP OF SIGN SHALL BE THE SAME AS FOR A 48" SIGN AS INDICATED IN THE M.U.T.C.D. 

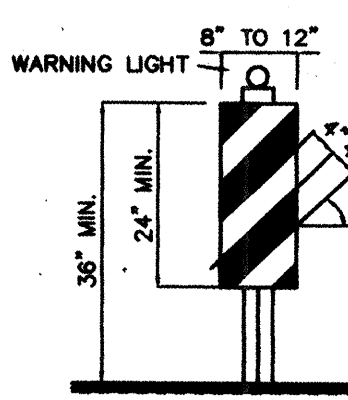
27. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORKSITE. ALL GRAFFITI SHALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND TEMPORARY. 



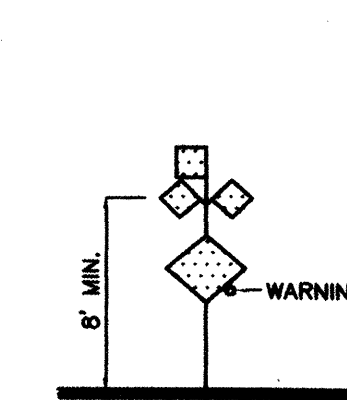
TYPE II BARRICADE



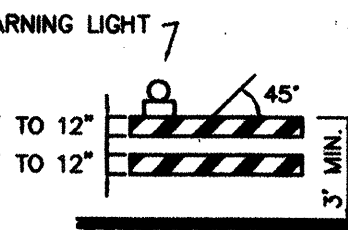
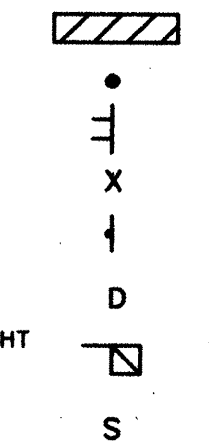
CONES



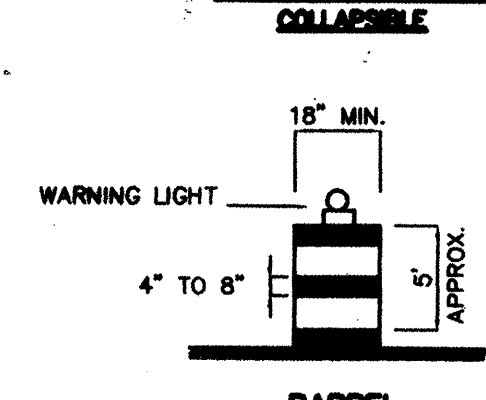
VERTICAL PANEL



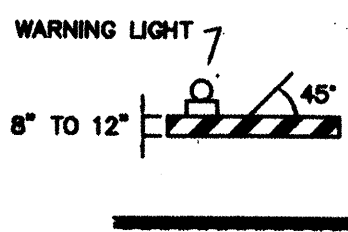
HIGH LEVEL WARNING DEVICE



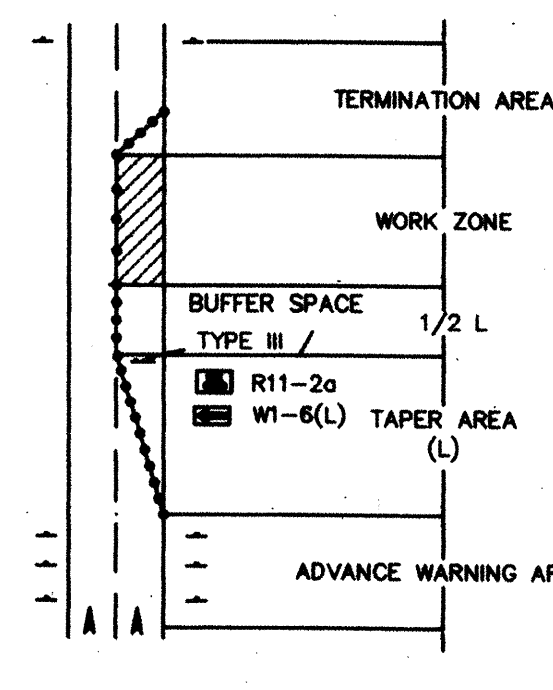
TYPE II BARRICADE



BARREL



TYPE I BARRICADE



TRAFFIC CONTROL ELEMENTS

LEGEND

- WORK AREA
BARRICADE - TYPE I, TYPE II, OR BARREL
BARRICADE - TYPE III
VERTICAL PANEL
WARNING SIGN
DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET
FLAGMAN POSITION
SPACING BETWEEN BARRICADES - A DISTANCE MEASURED IN FEET EQUAL TO THE SPEED LIMIT OF THE STREET
TAPER LENGTH - SEE CHART BELOW
THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

TAPER REQUIREMENTS

SPEED LIMIT (MPH)	TAPER LENGTH (L) (FEET)			MINIMUM NUMBER OF DEVICES FOR TAPER	MAXIMUM DEVICE SPACING IN FEET	
	10' LANE	11' LANE	12' LANE		ALONG TAPER	AFTER TAPER
20	70	75	80	5	20	20
25	105	115	125	6	25	25
30	150	165	180	7	30	30
35	205	225	245	8	35	35
40	270	295	320	9	40	40
45	450	495	540	13	45	45
50	500	550	600	13	50	50
55	550	605	660	13	55	55

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

SPEED LIMIT (MPH)	MINIMUM DISTANCE IN FEET	
	BETWEEN SIGNS	FROM LAST SIGN TO TAPER
0-20	10 X SPEED LIMIT	10 X SPEED LIMIT
25-30	10 X SPEED LIMIT	10 X SPEED LIMIT
30-35	10 X SPEED LIMIT	10 X SPEED LIMIT
40-45	10 X SPEED LIMIT	10 X SPEED LIMIT
50-60	10 X SPEED LIMIT	10 X SPEED LIMIT

TAPER CRITERIA

TYPE OF TAPER	TAPER LENGTH
UPSTREAM TAPER:	
MERGING TAPER	L MINIMUM
SHIFTING TAPER	1/2 L MINIMUM
SHOULDER TAPER	1/2 L MINIMUM
TWO-WAY TRAFFIC TAPER	100 FEET MAXIMUM
DOWNSTREAM TAPERS	100 FEET PER LANE

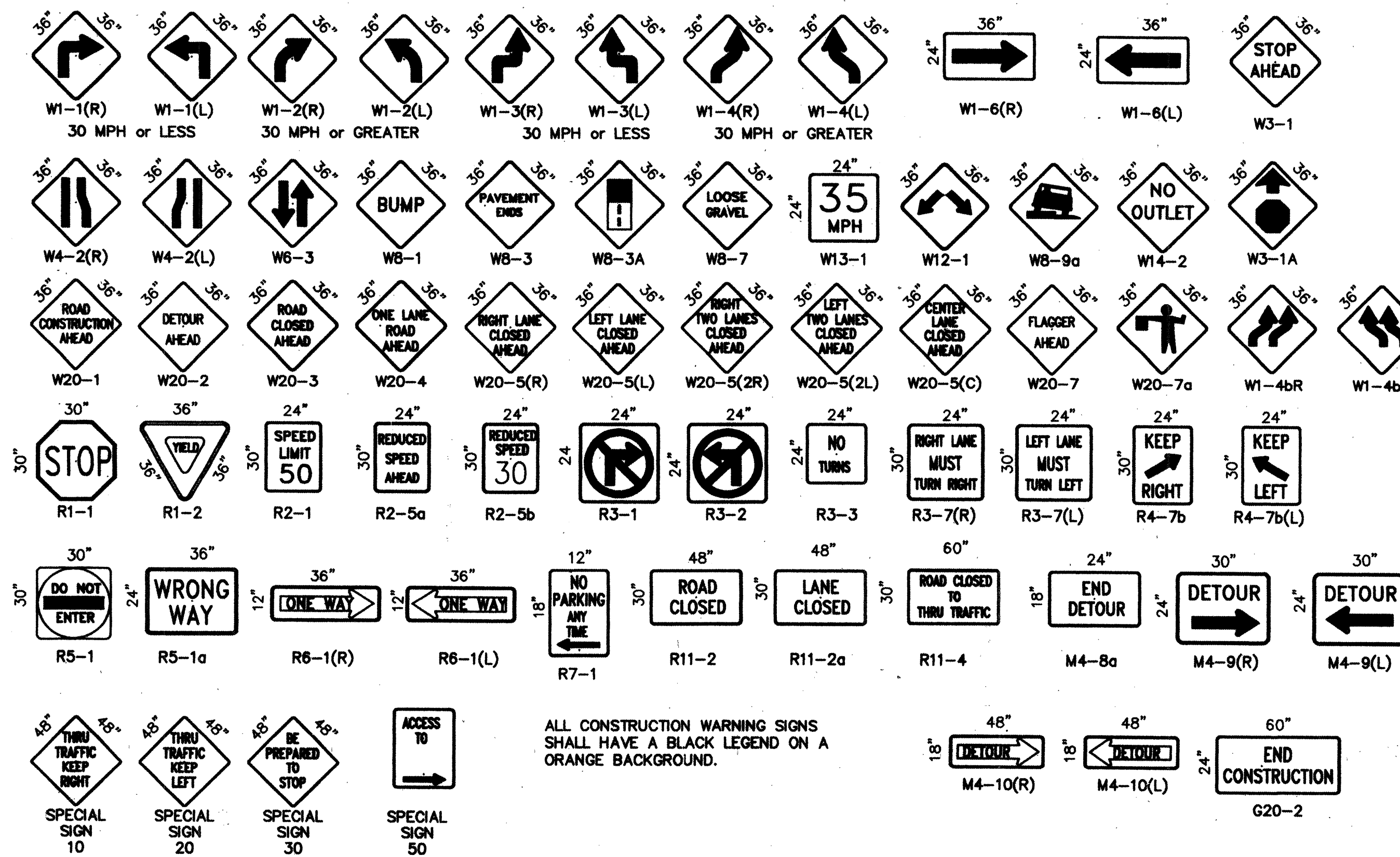
TAPER LENGTH COMPUTATION

SPEED LIMIT	L = WS ²
40 MPH OR LESS	60
45 MPH OR GREATER	L = W x S

L = TAPER LENGTH
W = WIDTH OF OFFSET IN FEET
S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

FOR INFORMATION ONLY

SIGN FACE DETAILS



ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.

CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

TITLE: **SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS**

DESIGN REVIEW COMMITTEE: **COA Standard**

QTY ENGINEER APPROVAL: **611681**

MAP NO. **4** OF **5**

PROJECT NO. **611681**

SHEET **4** OF **5**

