

VILLAGE OF TIJERAS

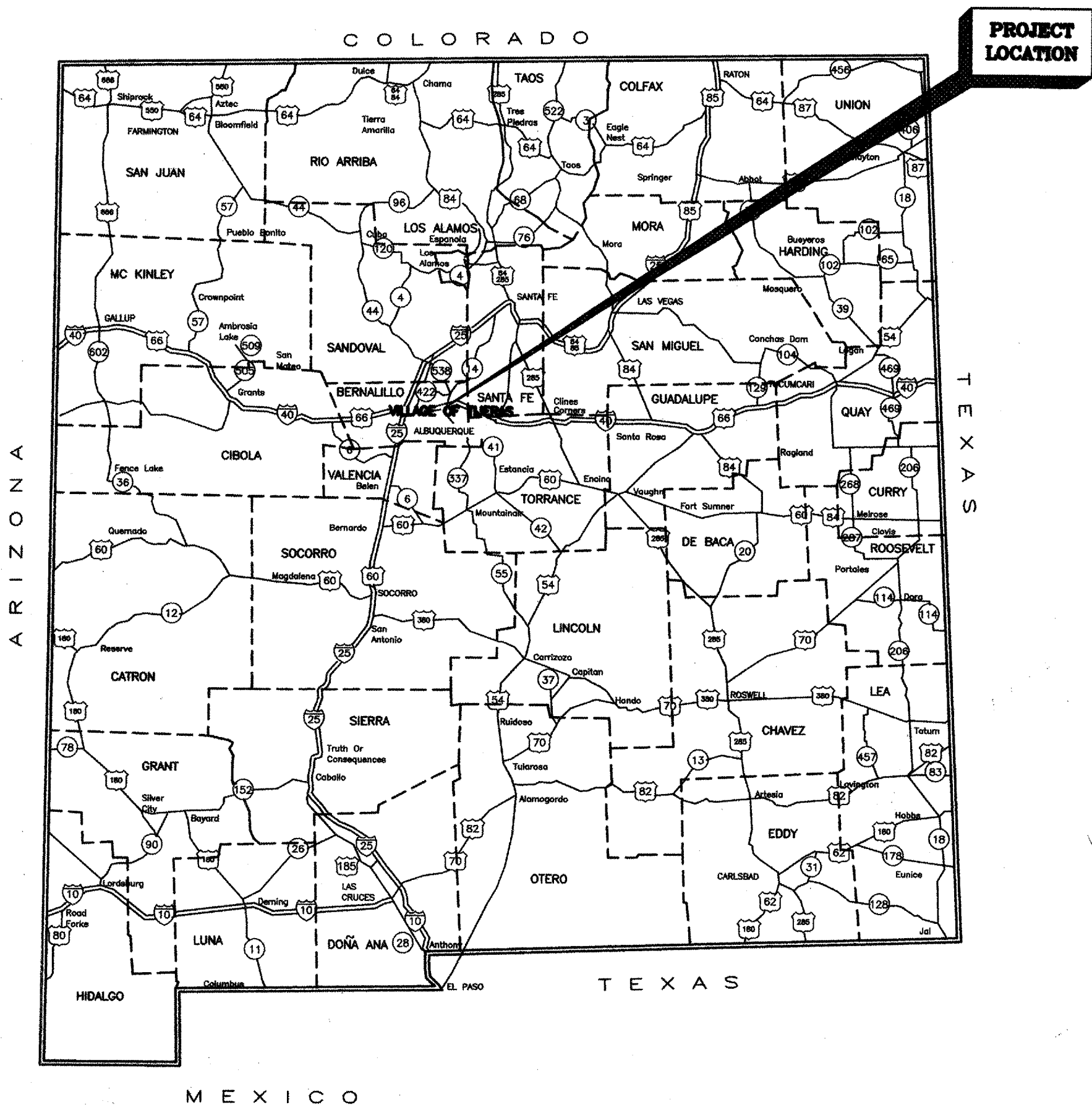


BERNALILLO COUNTY, NEW MEXICO

PLANS FOR CONSTRUCTION OF

TIJERAS SEWER EXTENSION NMED

JULY 1998



LOCATION MAP

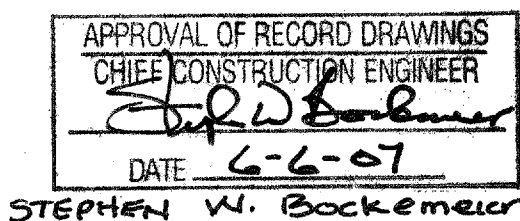
SHEET	TITLE
1	TITLE SHEET / LOCATION MAP
2	GENERAL NOTES / VICINITY MAP
3	SANITARY SEWER PLAN AND PROFILE STA. 10+00 TO STA. 23+50
4	SANITARY SEWER PLAN AND PROFILE STA. 23+50 TO STA. 36+51.79
5	TRAFFIC CONTROL DETAILS
6	MISCELLANEOUS DETAILS
7	MISCELLANEOUS DETAILS

THESE DRAWINGS DO NOT INCLUDE NECESSARY
COMPONENTS FOR CONSTRUCTION SAFETY WHICH
SHALL REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.

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PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT
THE WRITTEN CONSENT OF MOLZEN-CORBIN & ASSOCIATES.

CERTIFICATION:

I, RON R. MOSHER, REGISTERED
PROFESSIONAL ENGINEER NO. 10753
HEREBY CERTIFY THAT THE FOLLOWING
PLANS AND DESIGNS WERE MADE
UNDER MY SUPERVISION AND DIRECTION
AND THAT SAME IS TRUE AND CORRECT
TO THE BEST OF MY KNOWLEDGE AND BELIEF

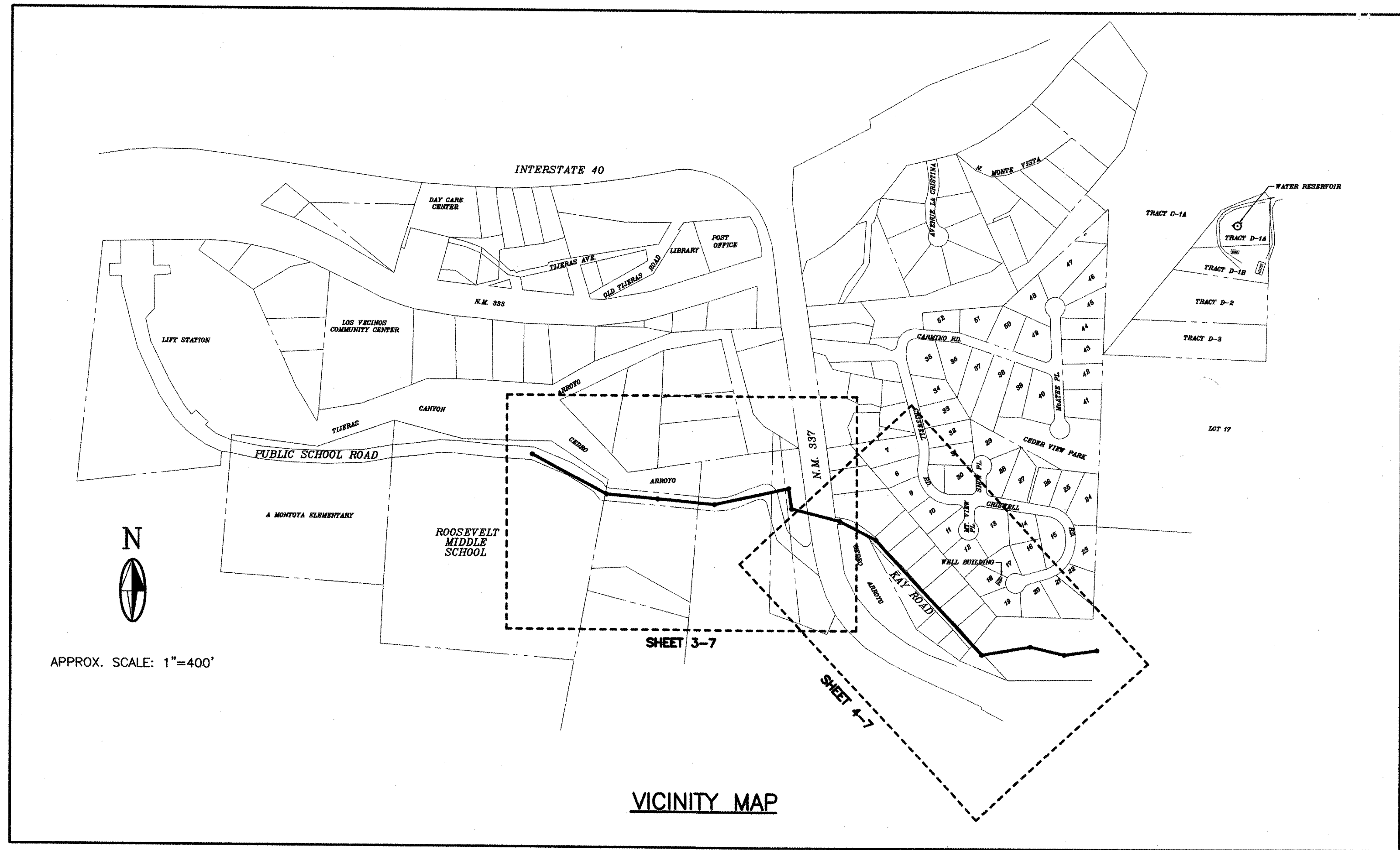


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N.M.P.E.

NO. 10753

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Sheet 26 of 7
COA. Project Number 6221.61



GENERAL NOTES

- THE CONTRACTOR SHALL CONFINE HIS OPERATIONS TO THE CONSTRUCTION LIMITS OF THIS PROJECT AND WILL BE RESPONSIBLE FOR ANY PRIVATE AGREEMENTS NECESSARY TO EXECUTE THIS CONTRACT. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGES CAUSED BY HIS EQUIPMENT TO PUBLIC OR PRIVATE PROPERTY.
- THE LOCATION OF ALL UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO EXCAVATION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES 48 HOURS BEFORE COMMENCING WORK IN THE AREAS NEAR UNDERGROUND UTILITY LINES. CONTRACTOR SHALL NOT INTERFERE WITH UTILITY LINE OPERATION AND SHALL COORDINATE ALL WORK AFFECTING EXISTING UTILITIES WITH THE APPROPRIATE AUTHORITY FOR EACH UTILITY, AND THE VILLAGE INSPECTOR SHALL BE PROMPTLY NOTIFIED OF ANY PROBLEMS OR CONFLICTS ENCOUNTERED. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR ANY DELAYS, INCONVENIENCE, OR DAMAGE SUSTAINED DUE TO ANY INTERFERENCE FROM SAID UTILITY APPURTENANCES.
- THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN ACCESS TO ALL MAIL BOXES DURING CONSTRUCTION, IN ACCORDANCE WITH LOCAL POST OFFICE REGULATIONS. THIS ALSO APPLIES TO ALL FINAL PLACEMENT OF MAIL BOXES. THE CONTRACTOR SHALL CONTACT THE LOCAL POST OFFICE AND THE MAIL BOX OWNER PRIOR TO RELOCATION. THERE WILL BE NO DIRECT COMPENSATION FOR THIS WORK. THE CONTRACTOR SHALL RELOCATE THE NEWSPAPER DELIVERY BOXES AS REQUIRED TO MAINTAIN DELIVERY, DURING AND AFTER CONSTRUCTION, IN COORDINATION WITH THE LANDOWNERS. THERE WILL BE NO DIRECT COMPENSATION FOR THIS WORK.
- ANY EXISTING SIGNS THAT MAY BE DISTURBED DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES AND CONSTRUCTION SIGNING AS OUTLINED IN THE "MUTCD" (LATEST EDITION) DURING CONSTRUCTION PERIOD (WORKING AND NON-WORKING HOURS). ANY NECESSARY DEVIATION FROM THE "MUTCD" SHALL HAVE PRIOR APPROVAL OF THE ENGINEER OR VILLAGE INSPECTOR. THE CONTRACTOR SHALL SUBMIT A COPY OF THE TRAFFIC CONTROL PLAN TO THE ENGINEER AND VILLAGE INSPECTOR AT THE PRECONSTRUCTION MEETING. THIS PLAN SHALL SATISFY THE VILLAGE REQUIREMENTS FOR PUBLIC SAFETY AND TRAVELING PUBLIC AS WELL AS THE REQUIREMENTS OF "MUTCD" AND SHALL BE REVISED AS NECESSARY TO MEET THE REQUIREMENTS DURING THE CONSTRUCTION PERIOD. THE SECTIONS OF ROADWAY WHICH ARE OPEN TO TRAFFIC SHALL BE KEPT IN GOOD RIDING CONDITION AND CLEAR OF HAZARDS TO TRAFFIC. THE SAFETY AND COMFORT OF THE TRAVELING PUBLIC AND ACCESS TO BUSINESSES AND OTHER TURNOUTS SHALL BE A PRIMARY CONSIDERATION.
- ACCESS TO ALL LOCAL BUSINESSES AND RESIDENCES SHALL BE KEPT OPEN AT ALL TIMES. ANY ACCESS CLOSURE MUST BE SCHEDULED WITH THE PROPERTY OWNERS AT LEAST 24 HOURS IN ADVANCE AND APPROVED BY THE VILLAGE INSPECTOR.
- WHEN ABUTTING NEW PAVEMENT, CUT EXISTING PAVEMENT TO A NEAT STRAIGHT LINE TO MATCH NEW SURFACING DEPTH. PAVEMENT CUT SHALL BE MADE AT A LOCATION SUITABLE FOR REMOVING BROKEN OR CRACKED PAVEMENT. A CONTINUOUS CUT IS REQUIRED AND STITCH CUTTING IS NOT ALLOWABLE. NO PAYMENT WILL BE MADE FOR CUTTING EXISTING PAVEMENT, AND IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
- DURING THE CONSTRUCTION OF THIS PROJECT, SOME OVERHEAD AND UNDERGROUND UTILITY ADJUSTMENTS MAY HAVE TO BE DONE CONCURRENTLY BY OTHER UTILITY COMPANIES. THE CONTRACTOR SHALL COORDINATE AND ADVISE THE UTILITY COMPANIES, ALLOWING ENOUGH TIME SO THAT THE REQUIRED UTILITY ADJUSTMENTS DO NOT IMPEDE THE CONTRACTOR'S WORK. ALL COORDINATION AND UTILITY RELOCATIONS WILL BE ACCOMPLISHED AT NO ADDITIONAL COST TO THE OWNER.
- THE USE OF HEAVY ROLLING EQUIPMENT AND/OR VIBRATORY ROLLERS (GREATER THAN 35 TONS) TO OBTAIN THE REQUIRED DENSITIES IN EMBANKMENT, EMBANKMENT FOUNDATION, MISCELLANEOUS FILLS & SUBGRADE WILL NOT BE PERMITTED IN URBAN OR OTHER AREAS WHERE THE USE OF SUCH EQUIPMENT WOULD DAMAGE UNDERGROUND UTILITY FACILITIES OR OTHER PERMANENT INSTALLATIONS.
- NO EQUIPMENT SHALL BE STORED WITHIN THE RIGHT-OF-WAY DURING NON-WORKING HOURS.
- THE CONTRACTOR SHALL NOT EXCEED LEGAL LOADS ON EXISTING PAVED ROADWAYS.
- IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY FOR ALL COORDINATION BETWEEN THE VARIOUS PRIVATE OR VILLAGE OWNED UTILITIES AFFECTED BY THE CONSTRUCTION OF THE PROJECT.
- CONTRACTOR SHALL REPLACE ANY DAMAGED FENCES, LANDSCAPING, OR ANY EXISTING SURFACE FEATURES TO THEIR ORIGINAL OR BETTER CONDITION AT NO ADDITIONAL EXPENSE TO THE OWNER.

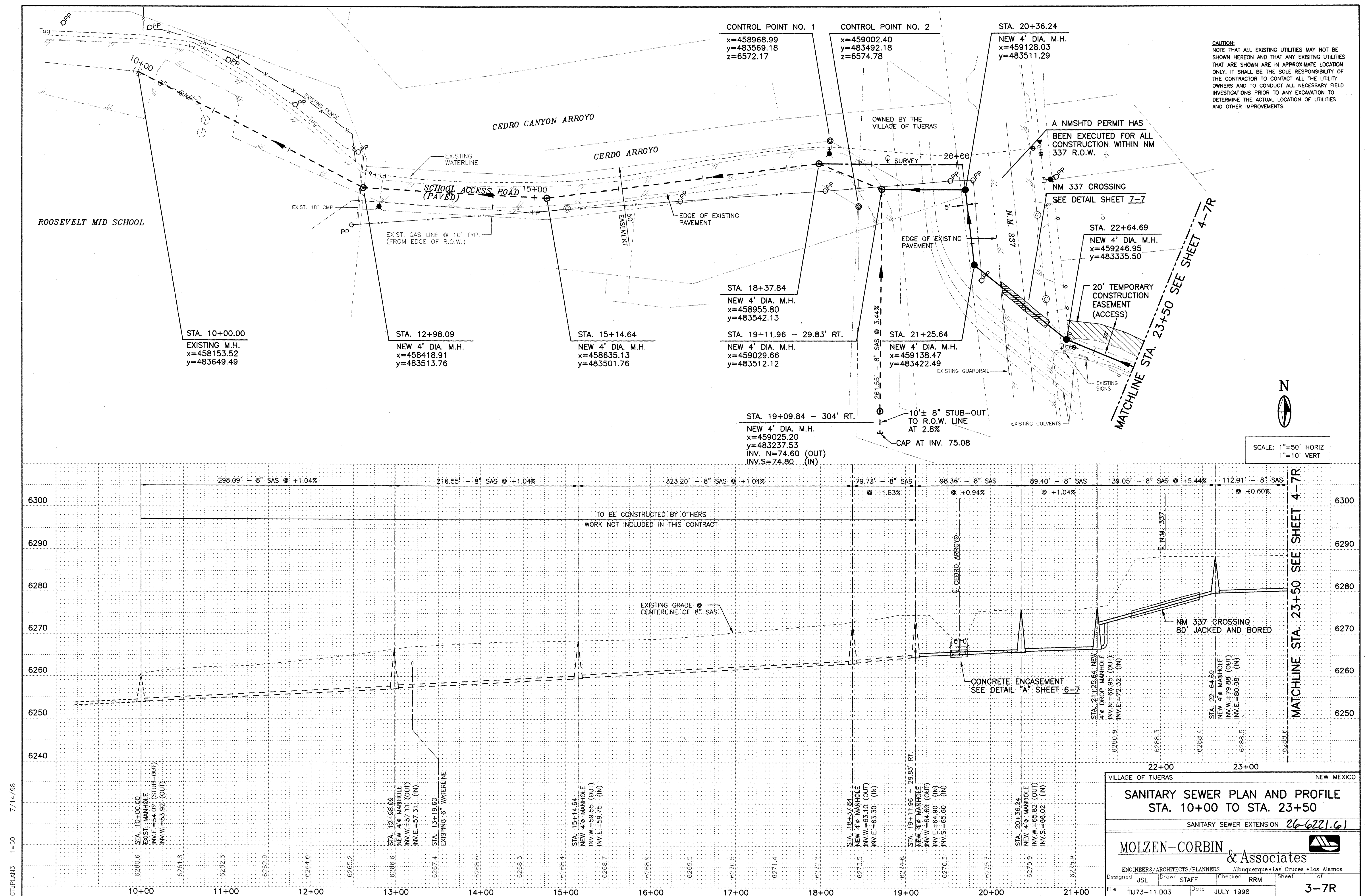
GENERAL NOTES (CONT.)

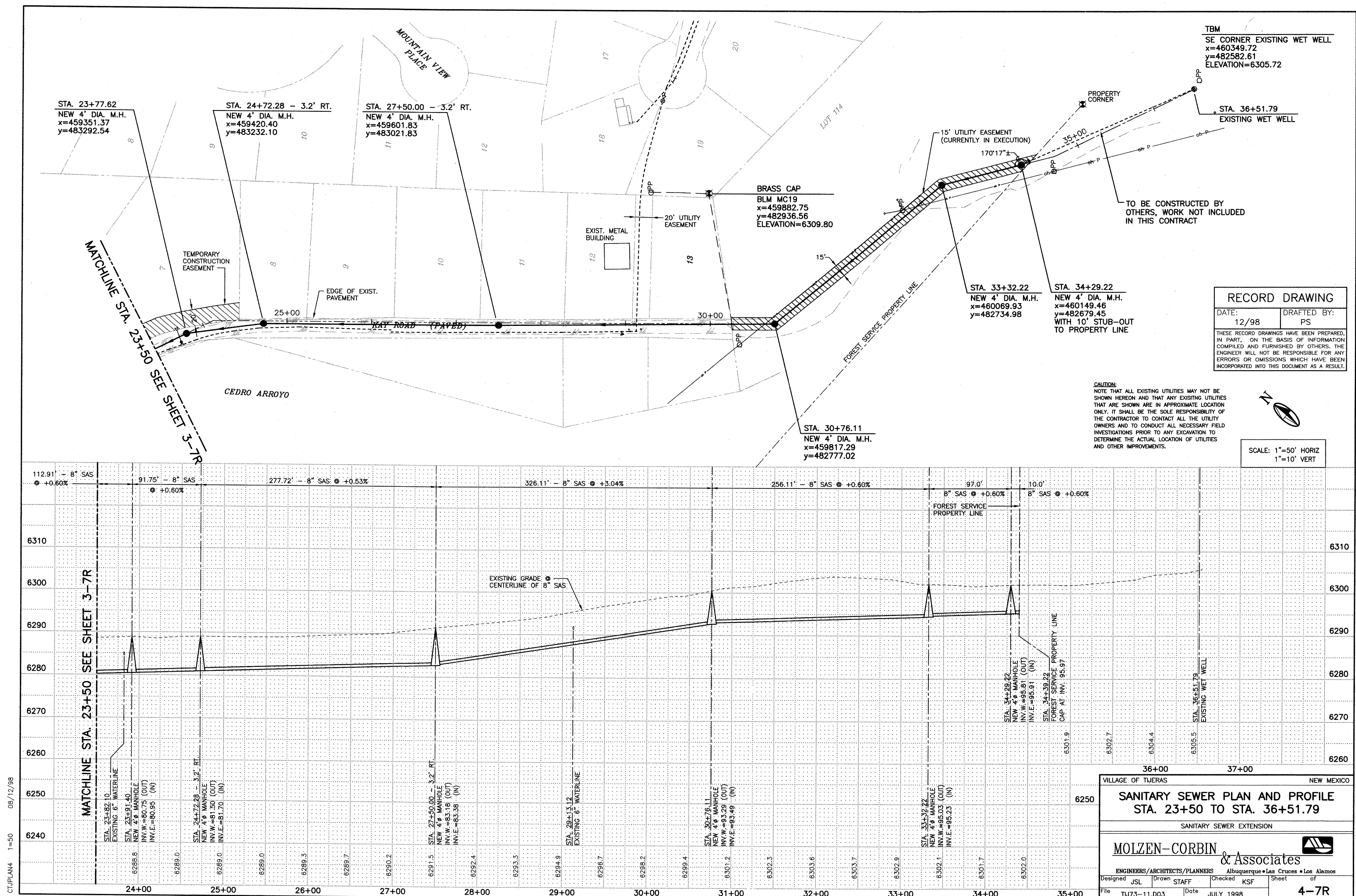
- IN THE EVENT THE CONTRACTOR ENCOUNTERS A PREVIOUSLY UNDISCOVERED ARCHAEOLOGICAL SITE WHICH IS NOT PART OF THE PLANS, THE CONTRACTOR SHALL TERMINATE ALL FURTHER OPERATION IN THAT IMMEDIATE AREA UNTIL THE ARCHAEOLOGICAL PRESERVATION AGENCY HAS HAD THE OPPORTUNITY TO SURVEY THE SITE. THE CONTRACTOR SHALL CONTACT THE HISTORIC PRESERVATION OFFICE AT (505)827-6320 AND THE CONSULTING ENGINEER AT (505)242-5700 IMMEDIATELY TO ADDRESS THE NEWLY DISCOVERED ARCHAEOLOGICAL SITE. THIS TERMINATION OF WORK SHALL NOT PRECLUDE CONTINUATION OF WORK IN OTHER AREAS NOR SHALL IT ENTITLE THE CONTRACTOR TO ADDITIONAL PAYMENT IN ANY FORM, OTHER THAN AN EXTENSION OF TIME.
- THE CONTRACTOR MAY ENCOUNTER SOME ROCK EXCAVATION DURING THE TRENCHING OPERATIONS FOR THE NEW SANITARY SEWER LINE. ANY ROCK THAT IS ENCOUNTERED DURING THESE TRENCHING OPERATIONS SHALL BE PAID UNDER THE "ROCK EXCAVATION" ITEM ON A CUBIC YARD (CY) BASIS. THE VILLAGE'S FIELD INSPECTOR SHALL COUNT TRUCKLOADS OF ROCK TO BE HAULED FROM THE SITE. ONLY THOSE LOADS ACCOUNTED FOR BY THE VILLAGE'S FIELD INSPECTOR SHALL BE INCLUDED IN THE QUANTITY FOR PAYMENT. ALL ROCK EXCAVATION SHALL BE HAULED AND DISPOSED OF AS PART OF THIS ITEM. REPLACEMENT OF SUITABLE BACKFILL MATERIAL TO ACCOUNT FOR THE ROCK EXCAVATION REMOVAL SHALL BE CONSIDERED A PART OF THE "ROCK EXCAVATION" ITEM AND NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.
- THE TEMPORARY CONSTRUCTION EASEMENT (TCE) NEAR THE INTERSECTION OF NM 337 AND KAY ROAD IS SO THAT THE CONTRACTOR MAY MAINTAIN ACCESS TO ALL RESIDENTS ALONG KAY ROAD DURING THE CONSTRUCTION OF THE PROJECT. THE RELOCATION OF THE EXISTING FENCE IN THIS AREA TO THE OUTSIDE EDGE OF THE TCE WILL BE REQUIRED. A TEMPORARY 6" THICK BASE COURSE SURFACE WILL ALSO BE REQUIRED WITHIN THIS TCE DURING CONSTRUCTION FOR TRAFFIC. ALL THE WORK ASSOCIATED WITH MAINTAINING ACCESS TO THE RESIDENTS ALONG KAY ROAD THROUGH THIS TCE AS DESCRIBED ABOVE SHALL BE CONSIDERED A PART OF THE TRAFFIC CONTROL PLAN AND NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS WORK.
- WHEN CONSTRUCTING THE SEWER LINE IN KAY ROAD, THE EXISTING ASPHALT SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AND SHALL BE PAID UNDER THE "REMOVAL AND DISPOSAL OF EXISTING ASPHALT" ITEM. THE REPLACEMENT SECTION FOR KAY ROAD SHALL CONSIST OF TWO - 11 FOOT DRIVING LANES WITH A 2% NORMAL CROWN. THE PAVING SECTION FOR KAY ROAD SHALL CONSIST OF A 3" ASPHALT CONCRETE SURFACE COURSE WITH A 6" AGGREGATE BASE COURSE LIFT ON 6" OF PREPARED SUBGRADE AS SHOWN ON SHEET 6-7.
- THE REMOVAL OF ANY STRUCTURE OR OBSTRUCTION, INCLUDING TREES, BUSHES, OR SHRUBBERY NECESSARY TO EXECUTE THIS CONTRACT WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.

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VILLAGE OF TJERAS		NEW MEXICO	
GENERAL NOTES VICINITY MAP 26-6221-4 COA PROJ #10			
SANITARY SEWER EXTENSION			
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Designed JSL	Drawn STAFF	Checked RRM	Sheet of
File TJ73-11.D03	Date JULY 1998	2-7	





TBM
SE CORNER EXISTING WET WELL
x=460349.72
y=482582.61
ELEVATION=6305.72

STA. 36+51.79
EXISTING WET WELL

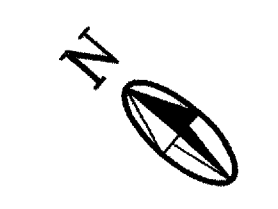
BRASS CAP
BLM MC19
x=459882.75
y=482936.56
ELEVATION=6309.80

STA. 33+32.22
NEW 4' DIA. M.H.
x=460069.93
y=482734.98

STA. 34+29.22
NEW 4' DIA. M.H.
x=460149.46
y=482679.45
WITH 10' STUB-OUT
TO PROPERTY LINE

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CAUTION:
NOTE THAT ALL EXISTING UTILITIES MAY NOT BE SHOWN HEREON AND THAT ANY EXISTING UTILITIES THAT ARE SHOWN ARE IN APPROXIMATE LOCATION ONLY. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ALL THE UTILITY OWNERS AND TO CONDUCT ALL NECESSARY FIELD INVESTIGATIONS PRIOR TO ANY EXCAVATION TO DETERMINE THE ACTUAL LOCATION OF UTILITIES AND OTHER IMPROVEMENTS.



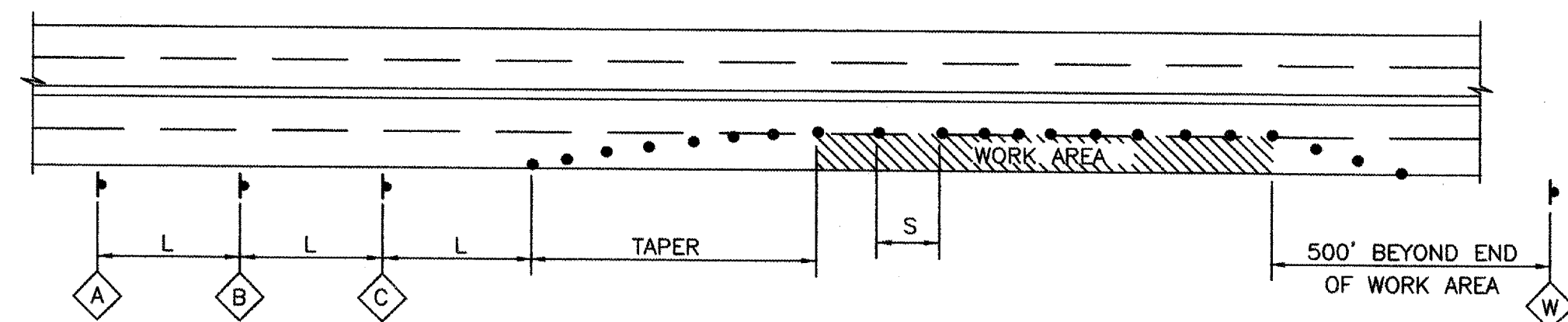
SCALE: 1"=50' HORIZ
1"=10' VERT

VILLAGE OF TUERAS				NEW MEXICO	
SANITARY SEWER PLAN AND PROFILE STA. 23+50 TO STA. 36+51.79					
SANITARY SEWER EXTENSION					
MOLZEN-CORBIN & Associates					
ENGINEERS/ARCHITECTS/PLANNERS Albuquerque • Las Cruces • Los Alamos					
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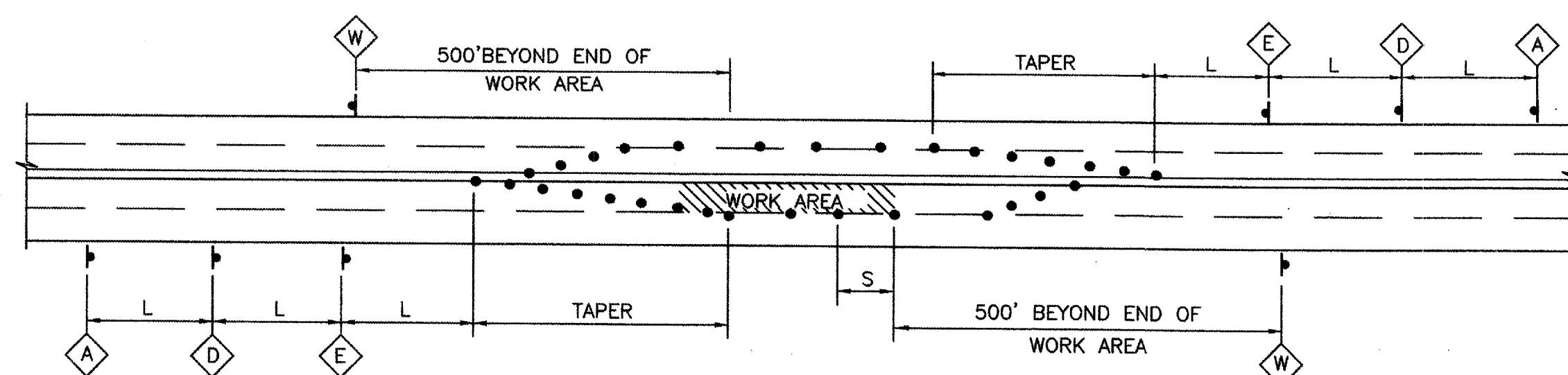
CUIPLAN4 1=50 08/12/98

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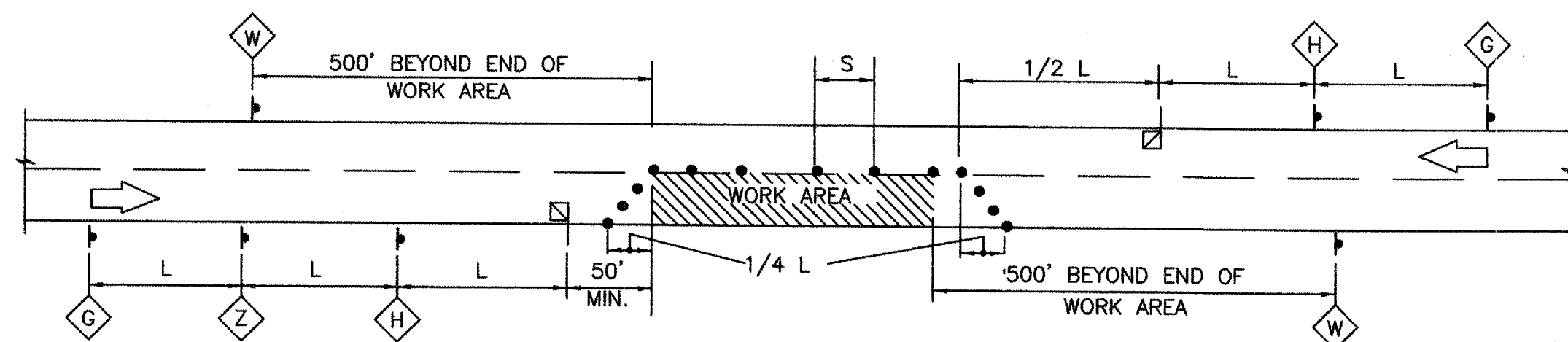
COA Proj 26-6221-61



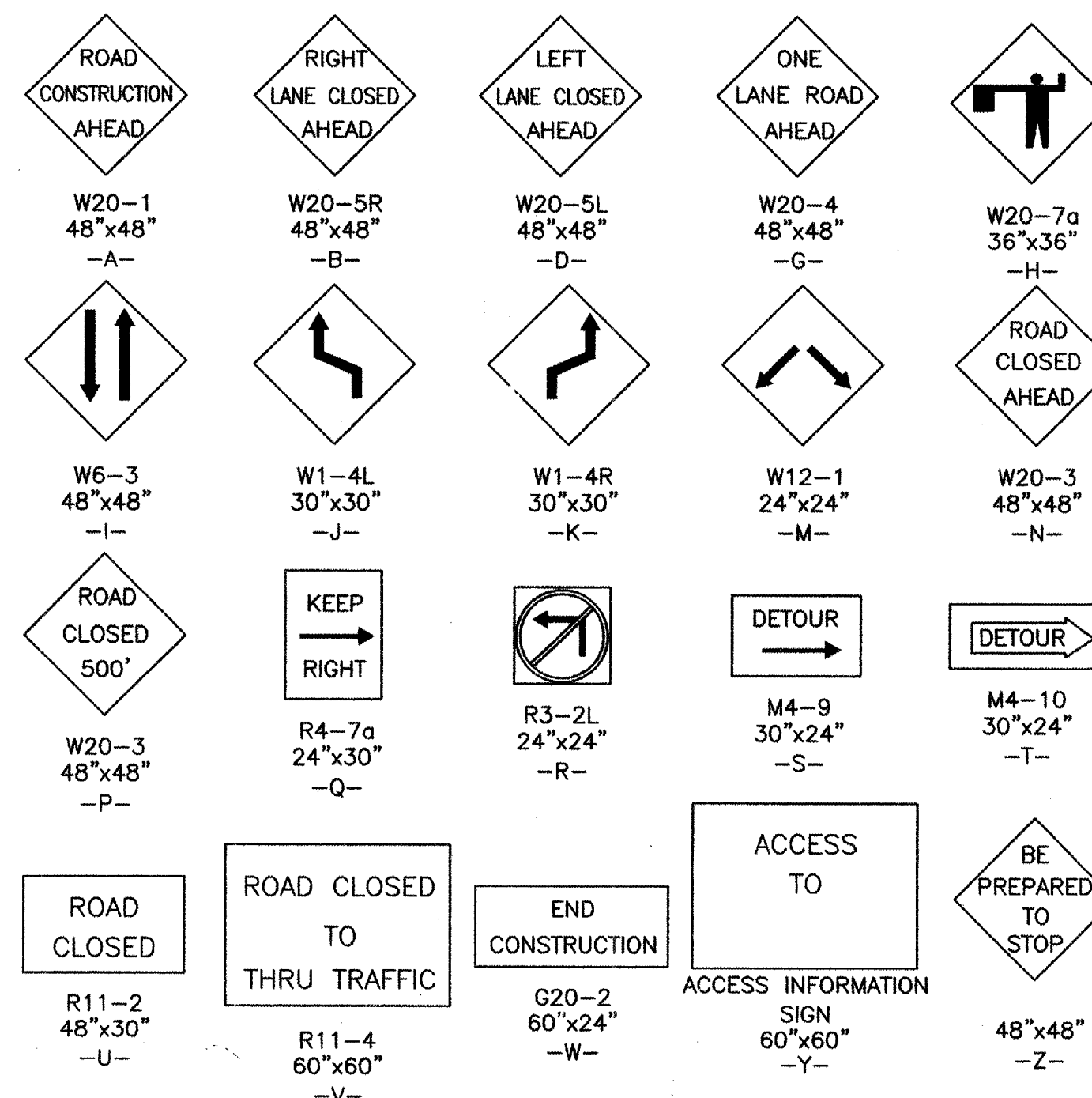
TYPICAL RIGHT LANE CLOSURE



TYPICAL LEFT LANE CLOSURE



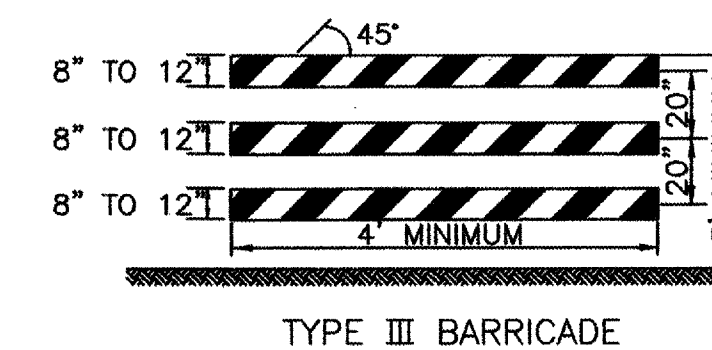
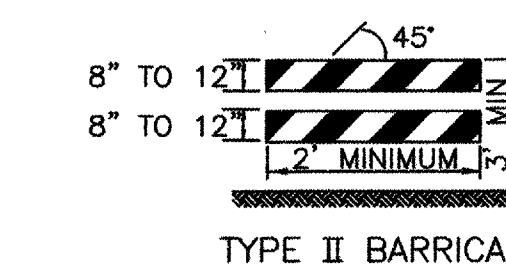
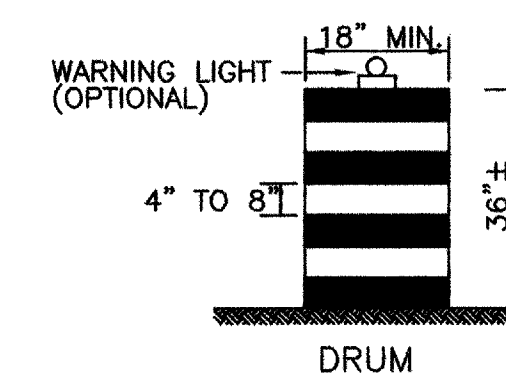
TYPICAL FLAGGING OPERATION



NOTE: ARROW DIRECTION TO BE ADJUSTED AS NEEDED.

TAPER REQUIREMENTS

SPEED LIMIT (MPH)	TAPER LENGTH (FEET)			MAXIMUM DISTANCE BETWEEN DEVICES (FEET)	MINIMUM NUMBER OF DEVICES REQUIRED
	10' LANE	11' LANE	12' LANE		
25	104	115	125	25	6
30	150	165	185	30	7
35	204	225	245	35	8
40	267	293	320	40	9
45	450	495	540	45	13
50	500	550	600	50	13
55	550	605	660	55	13



NOTE: FLASHING OR STEADY BURN WARNING LIGHTS SHOULD BE USED ON BARRICADES, PANELS, AND DRUMS AS NEEDED.

LEGEND

- WORK AREA
- TRAFFIC SIGN & QUANTITY (SEE THIS SHEET FOR DESIGNATION)
- BARRICADE - TYPE I, TYPE II, OR BARREL
- BARRICADE - TYPE III
- VERTICAL PANEL
- WARNING SIGN
- DISTANCE BETWEEN SIGNS-A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET
- FLAGMAN POSITION
- SPACING BETWEEN BARRICADES-A DISTANCE MEASURED IN FEET EQUAL TO THE SPEED LIMIT OF THE STREET

TAPER LENGTH- ON CHART AT LEFT

TANGENT LENGTH- IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET

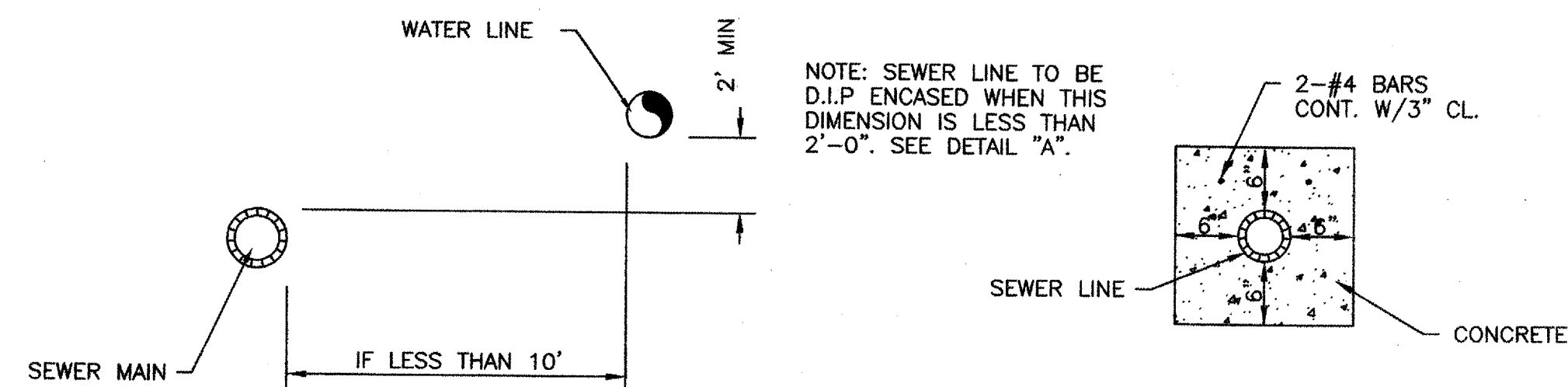
NOTES

- ALL ADVANCE WARNING SIGNS ARE TO BE DOUBLE INDICATED WHENEVER MEDIANS ARE PRESENT.
- TRANSFORMATION FROM EXISTING SIGNALS TO SPAN WIRE IS TO OCCUR AT OFF PEAK HOURS.
- THIS IS A STANDARD TRAFFIC CONTROL PLAN AND SOME TRAFFIC CONTROL SITUATIONS MAY BE SHOWN THAT ARE NOT REQUIRED ON THIS PARTICULAR PROJECT.
- IT IS ANTICIPATED MINOR ADJUSTMENTS WILL BE REQUIRED TO THE TRAFFIC SIGNING AND BARRICADING SHOWN ON THIS DRAWING TO MEET ACTUAL FIELD CONDITIONS. THE CONTRACTOR SHALL MAKE SUCH ADJUSTMENTS AT THE DIRECTION OF THE ENGINEER WITHOUT ADDITIONAL COST TO THE OWNER.

RECORD DRAWING

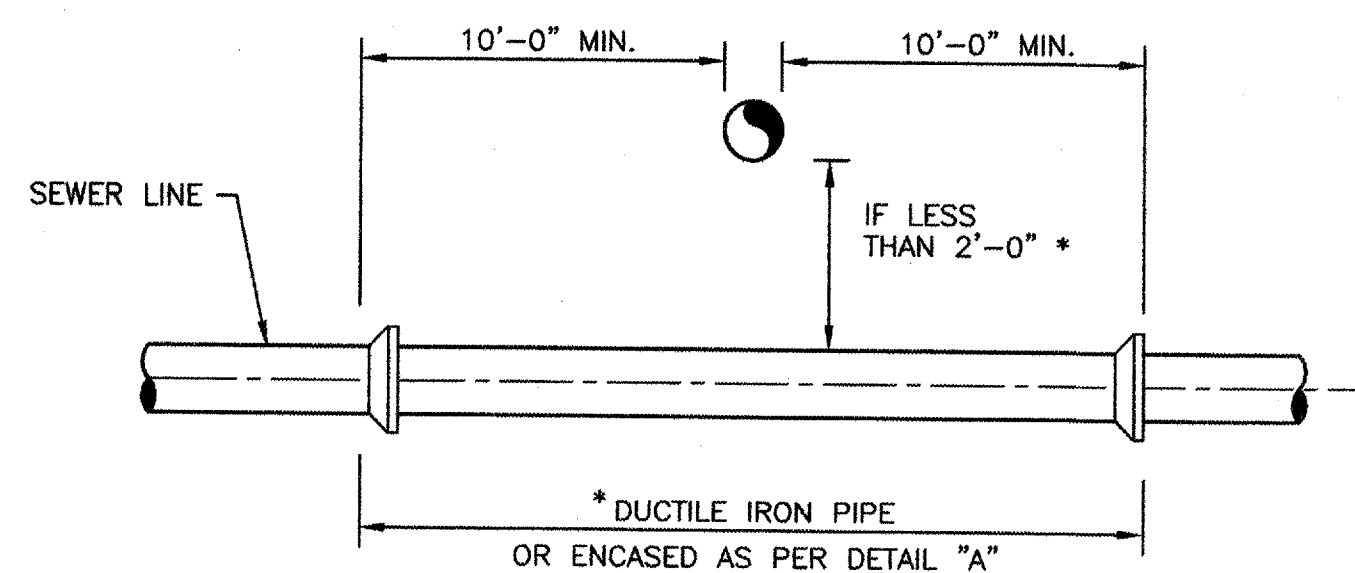
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VILLAGE OF TIJERAS		NEW MEXICO	
TRAFFIC CONTROL DETAILS			
SANITARY SEWER EXTENSION			
MOLZEN-CORBIN & Associates		ENGINEERS/ARCHITECTS/PLANNERS	
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File TJ73-11.D03	Date JULY 1998		

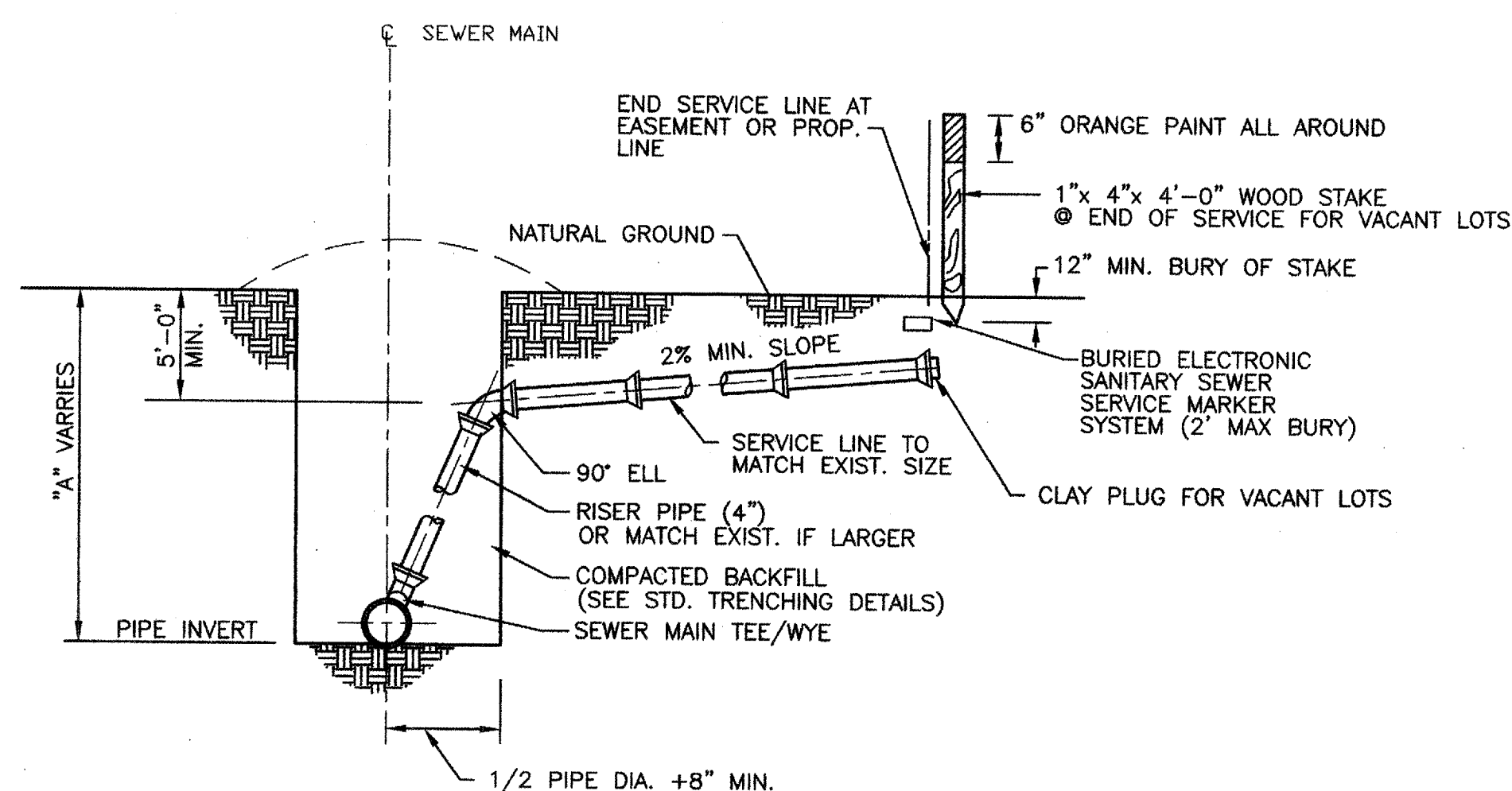


PARALLEL LINES
N.T.S.

CONCRETE ENCASEMENT DETAIL "A"
N.T.S.

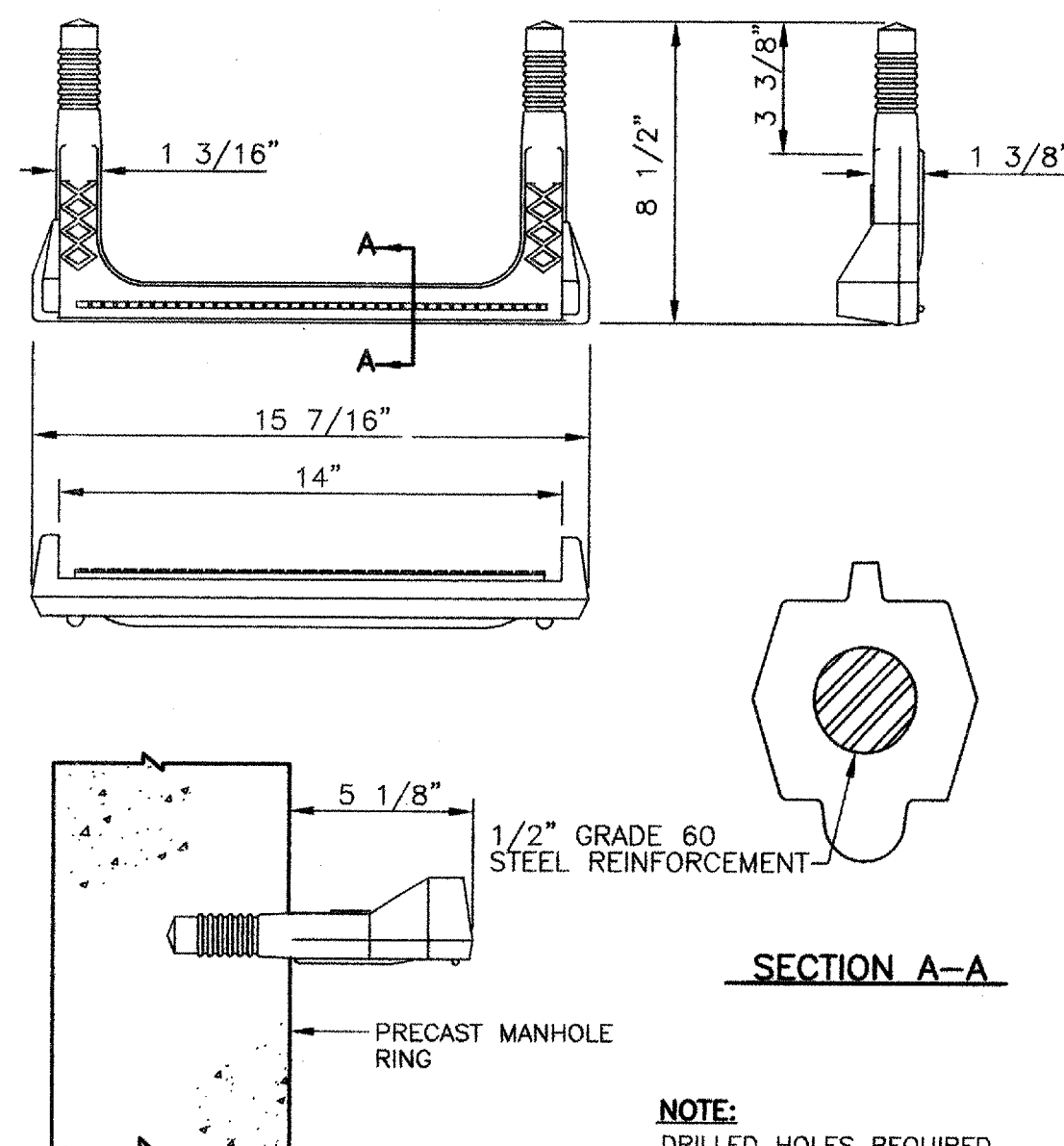


CROSSED LINES
SCALE: NONE

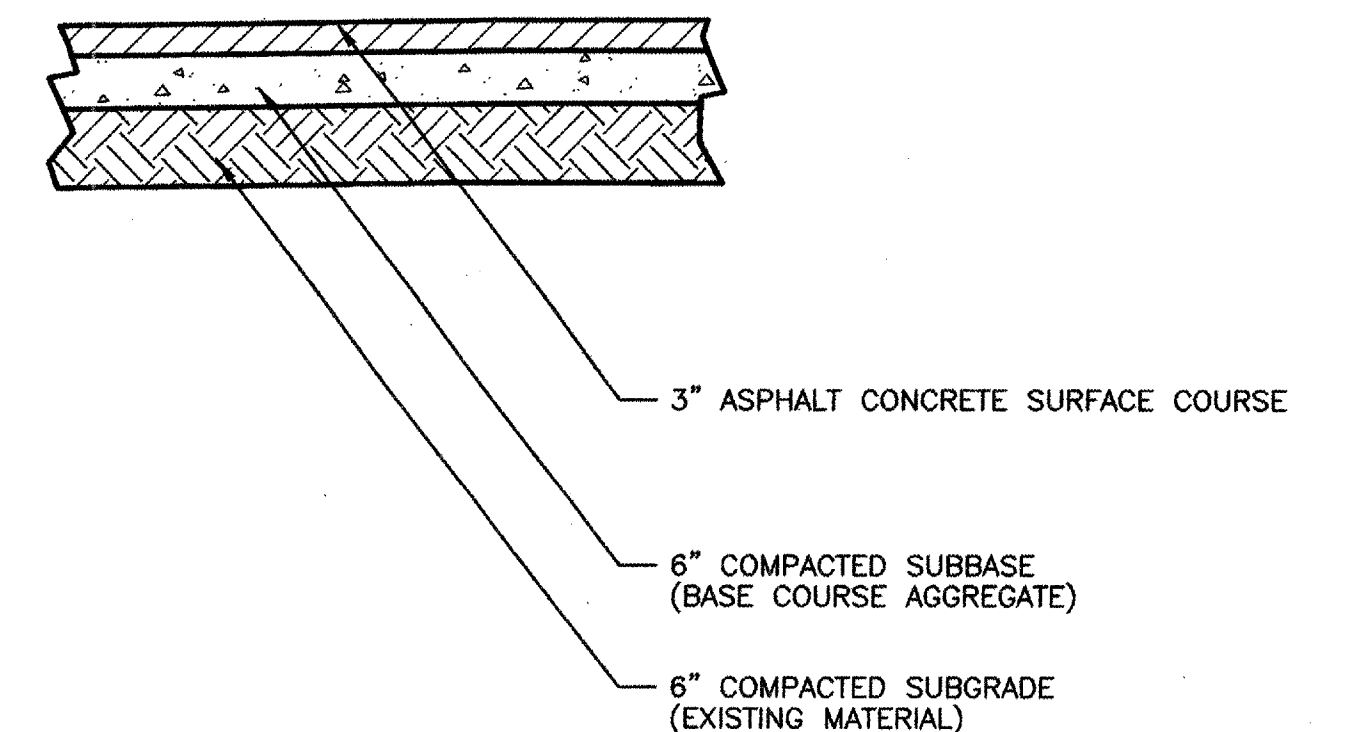


NOTES:
WHEN "A" IS LESS THAN 7'-0", DELETE RISER PIPE, USE 45° ELL IN PLACE OF 90° ELL, TURN SEWER MAIN TEE TOWARD SERVICE SIDE AT A 45° AND USE 1 CU. FT. CONC. BLOCKING AT TEE.

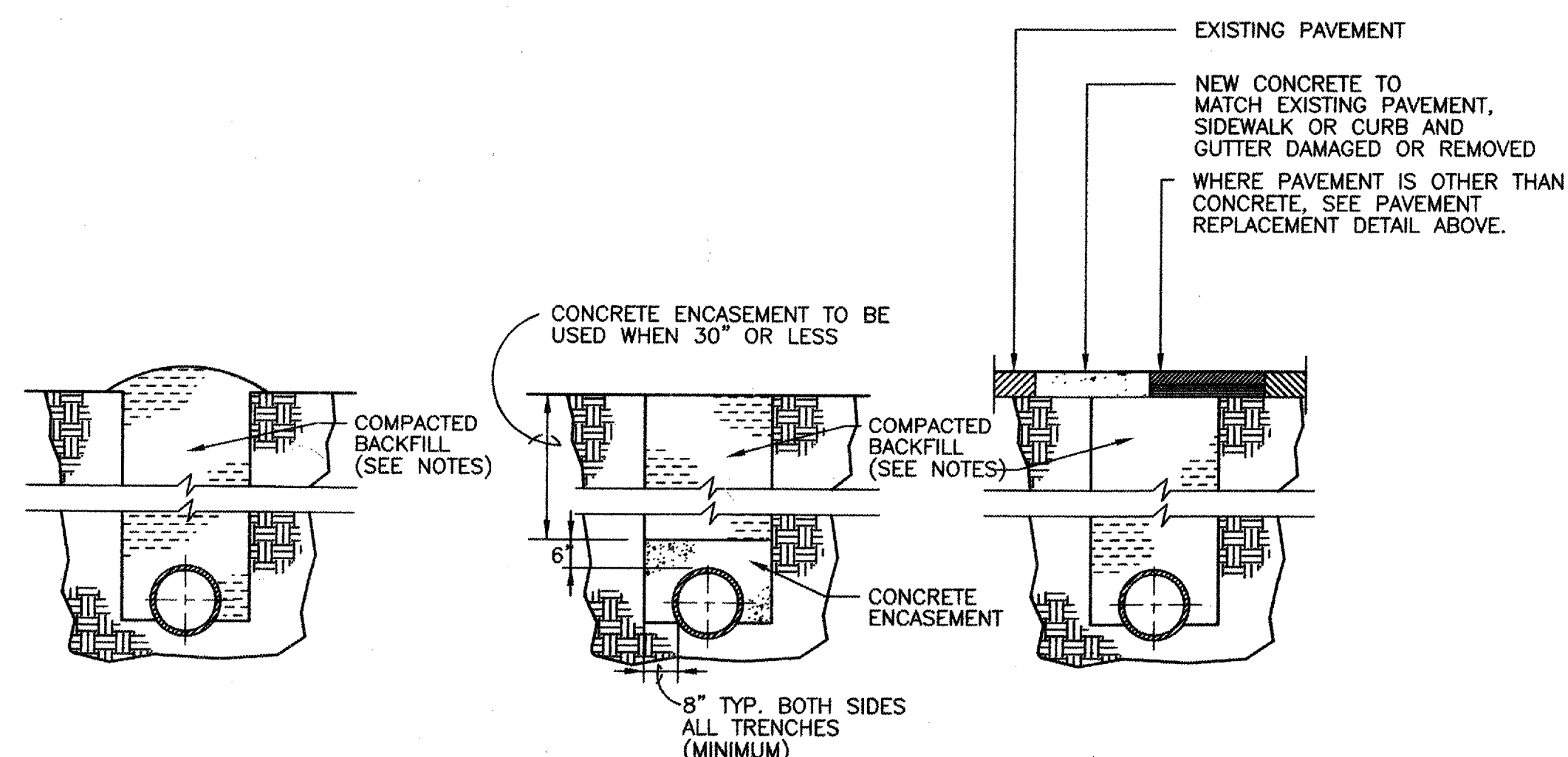
TYPICAL SEWER SERVICE CONNECTION
SCALE: NONE



COPOLYMER POLYPROPYLENE PLASTIC STEP
SCALE: NONE



PAVEMENT REPLACEMENT DETAIL
SCALE: NONE



ORDINARY TRENCH

NOTES:
BACKFILL IN AREAS OTHER THAN STREETS, ALLEYS AND DRIVEWAYS SHALL BE COMPACTED TO AT LEAST THE NATURAL DENSITY OF THE UNDISTURBED MATERIAL

UNPAVED ROAD TRENCH

NOTES:
BACKFILL IN UNPAVED STREETS, ALLEYS, AND DRIVEWAYS SHALL BE COMPACTED TO 90% COMPACTION AS DETERMINED BY AASHTO T-180

UNDER PAVEMENT

NOTES:
BACKFILL SHALL BE COMPACTED TO 90% WITHIN 12" OF THE SUBGRADE SURFACE AND THE TOP 12" SHALL BE COMPACTED TO 95% COMPACTION AS DETERMINED BY AASHTO T-180

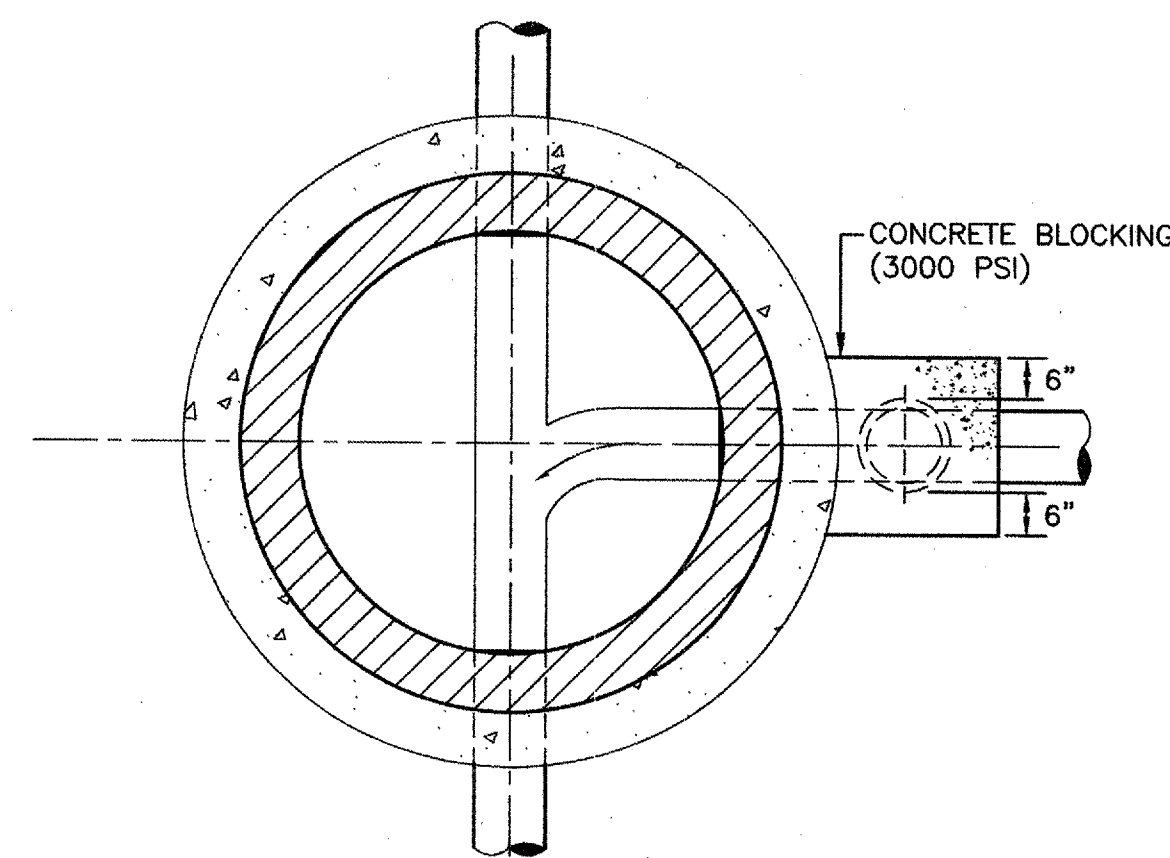
NOTE:
SEE PIPE BEDDING REQUIREMENTS IN SPECS.

STANDARD TRENCHING DETAILS
SCALE: NONE

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VILLAGE OF TIJERAS		NEW MEXICO	
MISCELLANEOUS DETAILS			
SANITARY SEWER EXTENSION			
ENGINEERS/ARCHITECTS/PLANNERS		Albuquerque • Las Cruces • Los Alamos	
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File TJ73-11.D03	Date JULY 1998	6-7	

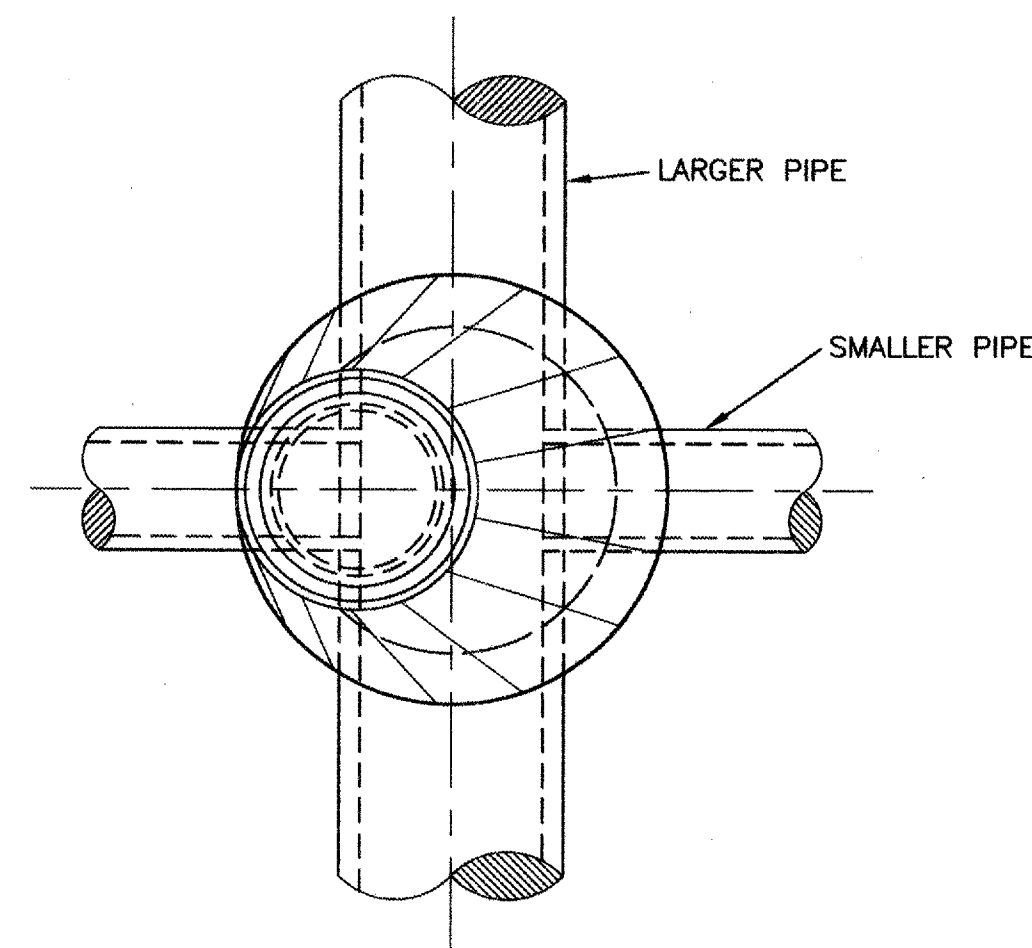
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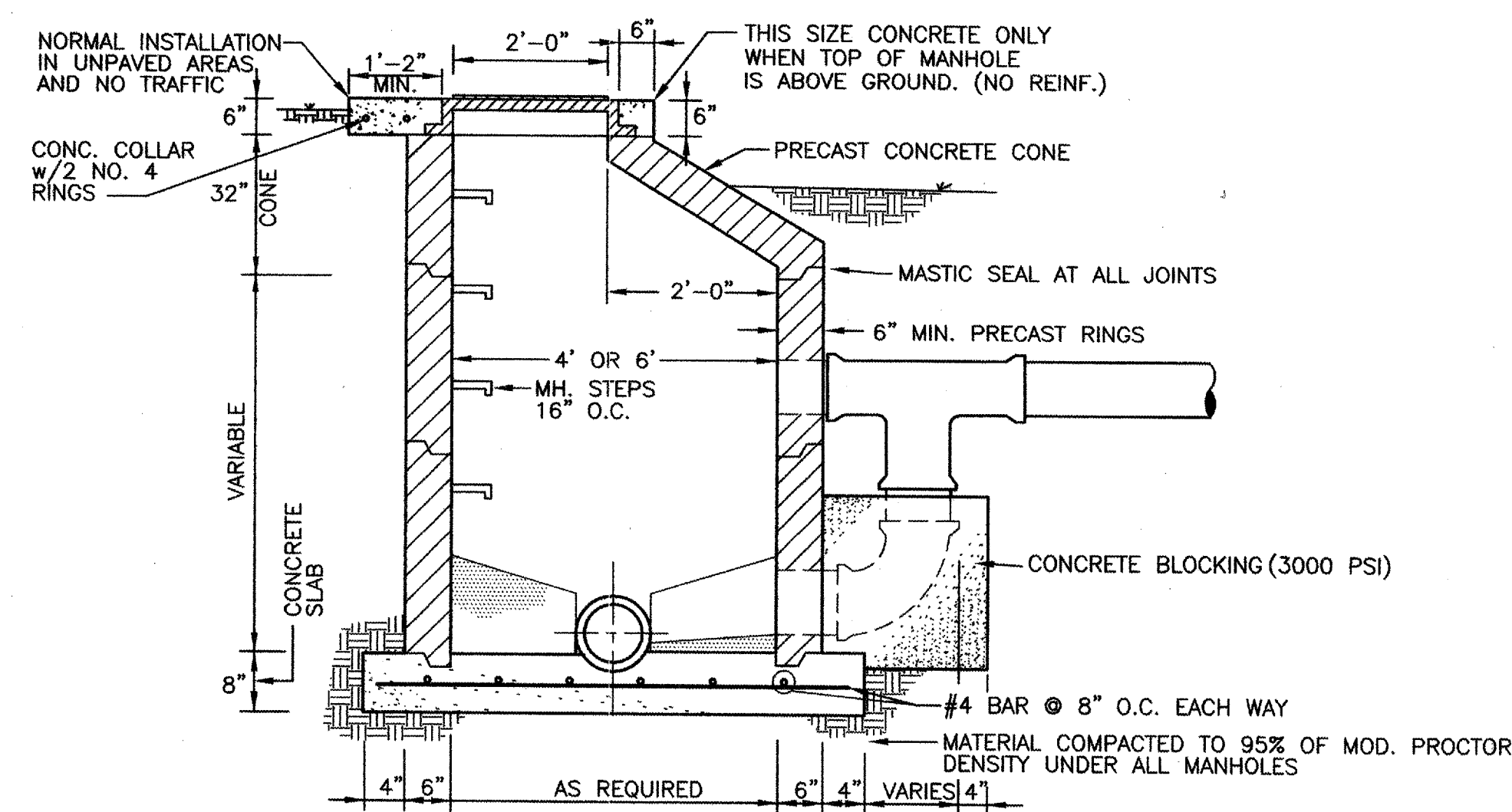
TYPICAL PLAN

NOTES:

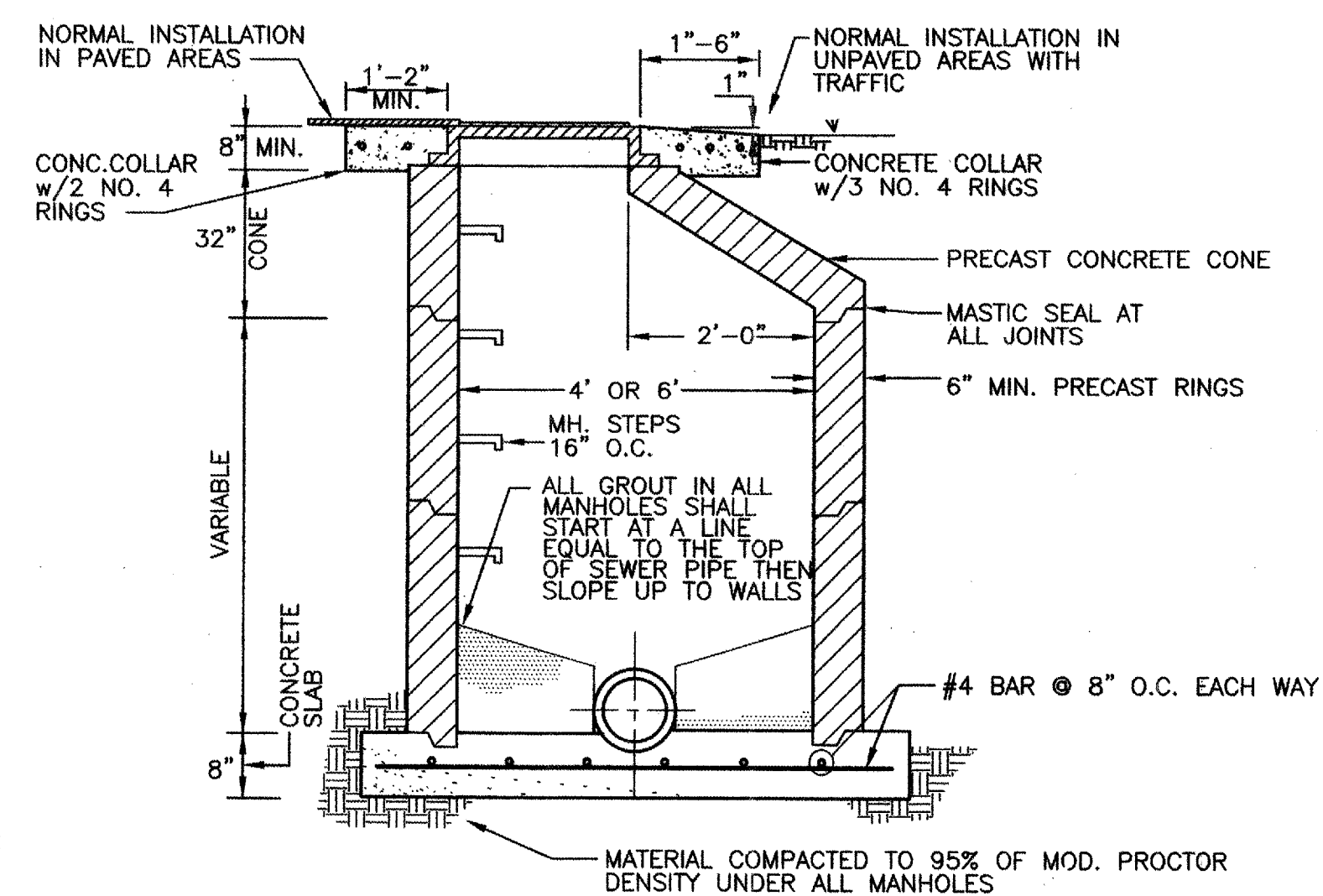
1. ECCENTRIC CONES SHALL BE USED FOR MANHOLE ON ALL PIPES 24" DIA. AND LARGER REGARDLESS OF DEPTH.
2. ECCENTRIC CONES SHALL BE USED ON ALL MANHOLES 10' DEPTH OR GREATER.
3. FOR 6' DIA. PRECAST CONCRETE MANHOLES:
(A) A 6' DIA. PRECAST CONC. ECCENTRIC CONE MUST BE USED OR
(B) A 4' DIA. PRECAST ECCENTRIC CONE ON A 6' DIA. FLAT SLAB WITH A 4' DIA. ECCENTRIC HOLE. THE CONE, SLAB, AND THE MANHOLE BARREL MUST BE KEYED TOGETHER.
4. MANHOLE RING AND COVER MAY BE ADJUSTED WITH PRECAST GRADE ADJUSTMENT RINGS 8" MAX. OR 4 COURSE MAX. BRICKS.



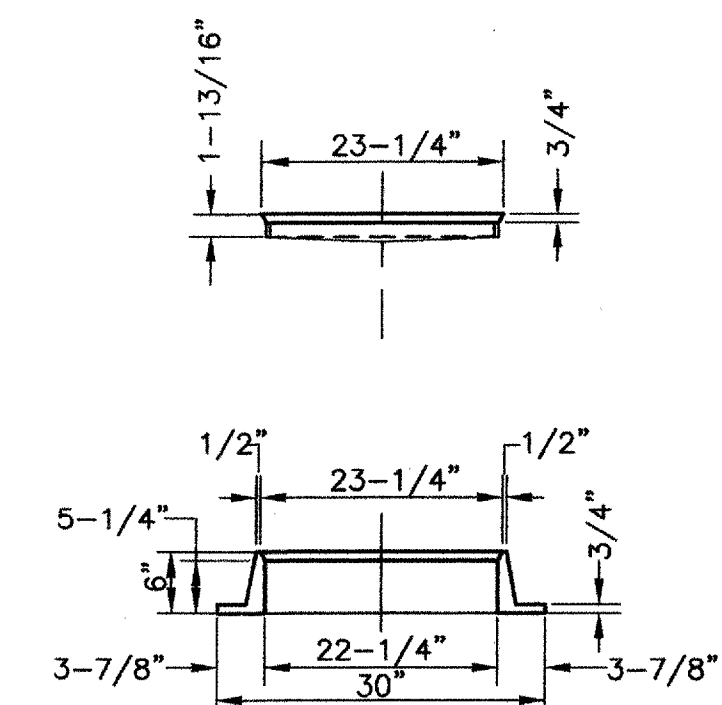
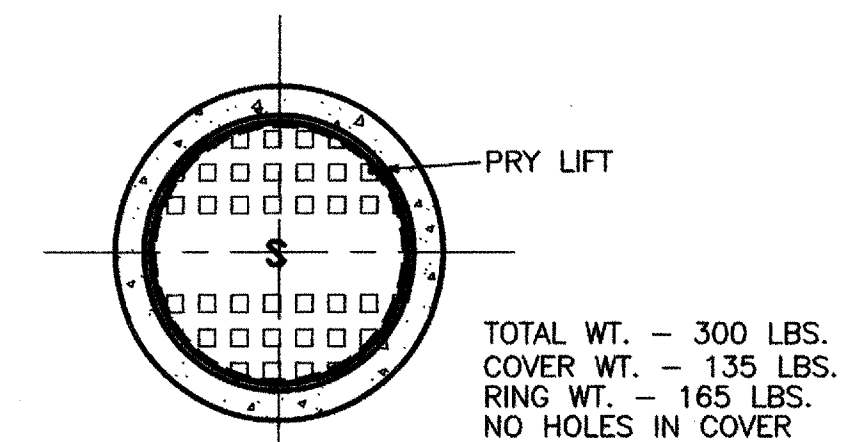
TYPICAL PLAN



STANDARD DROP MANHOLE DETAIL
SCALE: NONE



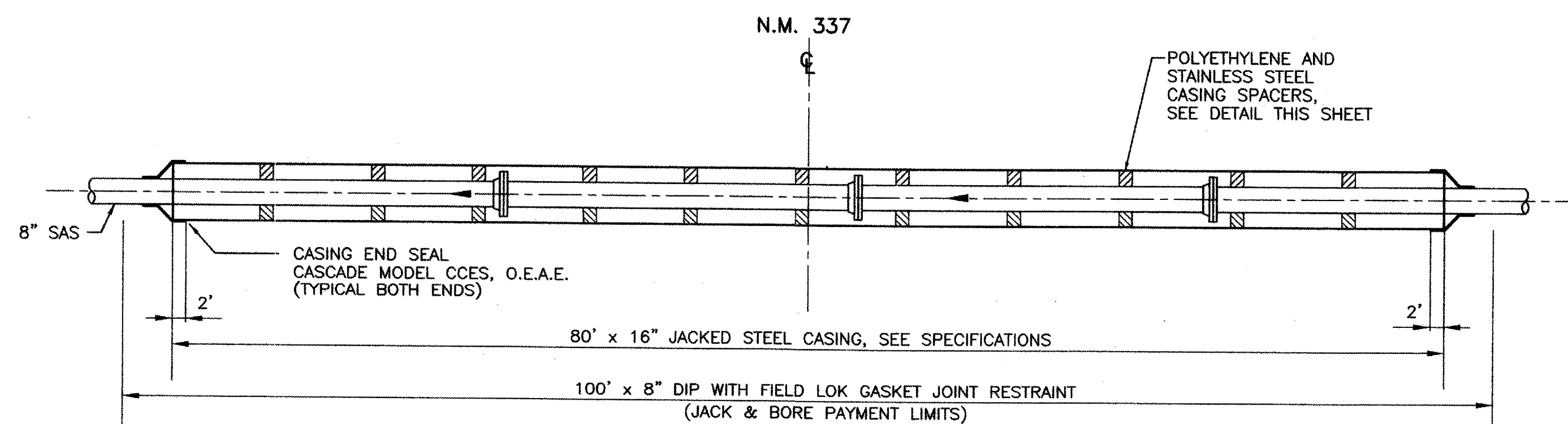
STANDARD ECCENTRIC CONE MANHOLE DETAIL
SCALE: NONE



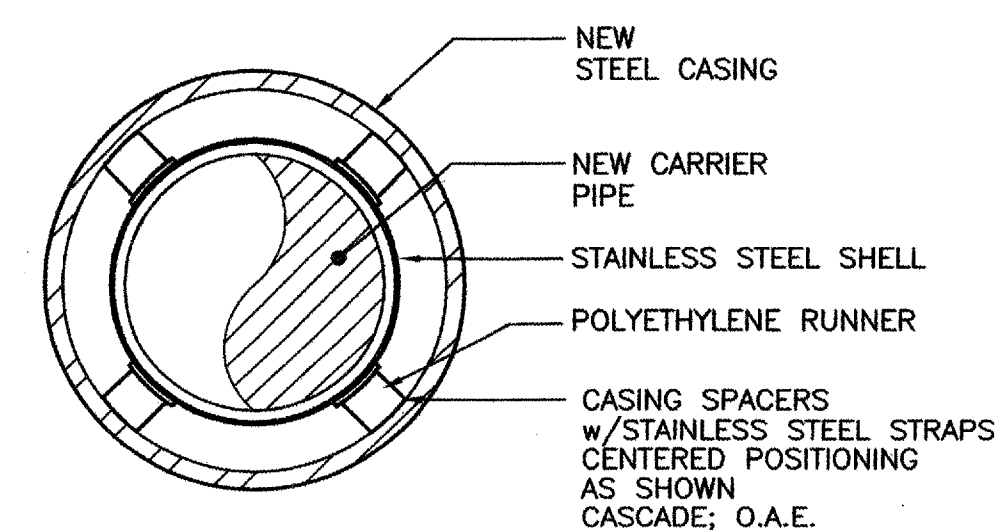
NOTE:
PROVIDE AS SHOWN OR APPROVED EQUAL

CAST IRON MANHOLE RING AND COVER
SCALE: NONE

* SEE SHEET 3-7 FOR ELEVATIONS



JACK AND BORE DETAIL
SCALE: NONE
(LOOKING EAST)



TYPICAL FOR ALL JACK & BORING DETAILS

CASING SPACER DETAIL
SECTION THRU SPACER
SCALE: NONE

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ENGINEERS/ARCHITECTS/PLANNERS Albuquerque • Las Cruces • Los Alamos	
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File TIJ73-11.D03	Date JULY 1998
7-7	

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CUJET2D 1" = 1' 7/10/98