

New Mexico

DEPARTMENT OF TRANSPORTATION

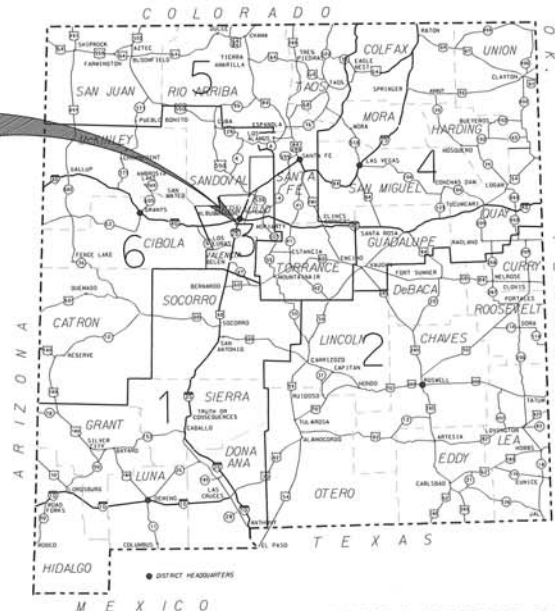
CONSTRUCTION PLANS

VOLUME II: SERIES 6 THROUGH 16**

I-25 AND RIO BRAVO
INTERCHANGE RECONSTRUCTION
UNIT I: CITY OF ALBUQUERQUE
UNIT II: BERNALILLO COUNTY
CN A300280



I-25 AND RIO BRAVO
INTERCHANGE
CONSTRUCTION
MP 219.8 TO
MP 221.4



** EXCLUDES 13-SERIES: ROADWAY CROSS SECTIONS

As-Built

MOT SHEET INDEX

SHEET NO.	DESCRIPTION	REV. DATE
6-1	QUANTITIES - SHEET INDEX	
6-1A	UNIT 1 QUANTITIES	
6-1B	UNIT 2 QUANTITIES	
6-1C	QUANTITIES	
6-2	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 0	
6-3	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 1	
6-4	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 1A, 1B & 1C	
6-5	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 2	
6-6	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 2A, 2B & 2C	
6-7	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 3	
6-8	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 3A, 3B, 3C, & 3D	
6-9	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 4	
6-10	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 4A	
6-11	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 5	
6-12	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 6	
6-13	TRAFFIC CONTROL NOTES	
6-14	TRAFFIC CONTROL NOTES	
6-15	TRAFFIC CONTROL DETAILS	
6-16	TYPICAL SECTIONS - RIO BRAVO PHASE 0	
6-17	TYPICAL SECTIONS - RIO BRAVO PHASE 1	
6-18	TYPICAL SECTIONS - 125 PHASE 1	
6-19	TYPICAL SECTIONS - RIO BRAVO PHASE 2	
6-20	TYPICAL SECTIONS - 125 PHASE 2	
6-21	TYPICAL SECTIONS - RIO BRAVO PHASE 3A, 3B, 3C & 3D	
6-22	TYPICAL SECTIONS - RIO BRAVO PHASE 3A, 3B, 3C & 3D 1-25 PHASE 3	
6-23	TYPICAL SECTIONS - RIO BRAVO PHASE 4	
6-24	TYPICAL SECTIONS - 125 PHASE 4	
6-25	TYPICAL SECTIONS - RIO BRAVO PHASE 4A	
6-26	TYPICAL SECTIONS - RIO BRAVO PHASE 5	
6-27	TYPICAL SECTIONS - RIO BRAVO PHASE 6	
6-28	RIO BRAVO PHASE 0 SHEET 1	
6-29	RIO BRAVO PHASE 0 SHEET 2	
6-30	RIO BRAVO PHASE 0 SHEET 3	
6-31	RIO BRAVO PHASE 0 SHEET 4	
6-32	RIO BRAVO PHASE 0 SHEET 5	
6-33	RIO BRAVO PHASE 0 SHEET 6	
6-34	RIO BRAVO PHASE 1 SHEET 1	
6-35	RIO BRAVO PHASE 1 SHEET 2	
6-36	RIO BRAVO PHASE 1 SHEET 3	
6-37	RIO BRAVO PHASE 1 SHEET 4	
6-38	BROADWAY PHASE 1A SHEET 1	
6-39	BROADWAY PHASE 1B SHEET 1	
6-40	BROADWAY PHASE 1C SHEET 1	
6-41	125 PHASE 1 SHEET 1	
6-42	125 PHASE 1 SHEET 2	
6-43	125 PHASE 1 SHEET 3	
6-44	125 PHASE 1 SHEET 4	
6-45	125 PHASE 1 SHEET 5	
6-46	RIO BRAVO PHASE 2 SHEET 1	
6-47	RIO BRAVO PHASE 2 SHEET 2	
6-48	RIO BRAVO PHASE 2 SHEET 3	
6-49	RIO BRAVO PHASE 2 SHEET 4	
6-50	BROADWAY PHASE 2A SHEET 1	
6-51	BROADWAY PHASE 2B SHEET 1	
6-52	BROADWAY PHASE 2C SHEET 1	
6-53	125 PHASE 2 SHEET 1	
6-54	125 PHASE 2 SHEET 2	
6-55	125 PHASE 2 SHEET 3	
6-56	125 PHASE 2 SHEET 4	
6-57	125 PHASE 2 SHEET 5	
6-58	RIO BRAVO PHASE 3 SHEET 1	
6-59	RIO BRAVO PHASE 3 SHEET 2	
6-60	RIO BRAVO PHASE 3 SHEET 3	
6-61	RIO BRAVO PHASE 3 SHEET 4	
6-62	BROADWAY PHASE 3A SHEET 1	
6-63	BROADWAY PHASE 3B SHEET 1	
6-64	BROADWAY PHASE 3C SHEET 1	
6-65	BROADWAY PHASE 3D SHEET 1	
6-66	125 PHASE 3 SHEET 1	
6-67	125 PHASE 3 SHEET 2	
6-68	125 PHASE 3 SHEET 3	
6-69	125 PHASE 3 SHEET 4	
6-70	125 PHASE 3 SHEET 5	

6-71	125 PHASE 4 SHEET 1
6-72	125 PHASE 4 SHEET 2
6-73	125 PHASE 4 SHEET 3
6-74	125 PHASE 4 SHEET 4
6-75	125 PHASE 4 SHEET 5
6-76	125 PHASE 4 SHEET 6
6-77	RIO BRAVO PHASE 4A SHEET 1
6-78	RIO BRAVO PHASE 4A SHEET 2
6-79	RIO BRAVO PHASE 4A SHEET 3
6-80	RIO BRAVO PHASE 5 SHEET 1
6-81	RIO BRAVO PHASE 5 SHEET 2
6-82	RIO BRAVO PHASE 5 SHEET 3
6-83	RIO BRAVO PHASE 6 SHEET 1
6-84	RIO BRAVO PHASE 6 SHEET 2
6-85	RIO BRAVO PHASE 6 SHEET 3
6-86	PHASE 1 - SB NORTH CROSSOVER SHEET 1
6-87	PHASE 1 - SB NORTH CROSSOVER SHEET 2
6-88	PHASE 1 - SB NORTH CROSSOVER SHEET 3
6-89	PHASE 1 - SB NORTH CROSSOVER SHEET 4
6-90	PHASE 1 - DETOUR NB ON RAMP
6-91	PHASE 2 - SB SOUTH CROSSOVER SHEET 1
6-92	PHASE 2 - SB SOUTH CROSSOVER SHEET 2
6-93	PHASE 2 - NB ON RAMP
6-94	TBMP SIGNAL GENERAL NOTES AND QUANTITIES
6-95	TBMP SIGNAL BROADWAY PLAN PHASE 1A
6-96	TBMP SIGNAL BROADWAY PLAN PHASE 1B
6-97	TBMP SIGNAL BROADWAY PLAN PHASE 1C
6-98	TBMP SIGNAL BROADWAY PLAN PHASE 2A
6-99	TBMP SIGNAL BROADWAY PLAN PHASE 2B
6-100	TBMP SIGNAL BROADWAY PLAN PHASE 2C
6-101	TBMP SIGNAL BROADWAY PLAN PHASE 3A
6-102	TBMP SIGNAL BROADWAY PLAN PHASE 3B
6-103	TBMP SIGNAL BROADWAY PLAN PHASE 3C
6-104	TBMP SIGNAL BROADWAY PLAN PHASE 3D
6-105	TBMP SIGNAL 1-25 PLAN PHASE 1
6-106	TBMP SIGNAL 1-25 PLAN PHASE 2
6-107	TBMP SIGNAL 1-25 PLAN PHASE 3
6-108	TBMP SIGNAL 1-25 PLAN PHASE 4
6-109	TBMP SIGNAL 1-25 PLAN PHASE 4A
6-110	TBMP SIGNAL 1-25 PLAN PHASE 5
6-111	TBMP SIGNAL 1-25 PLAN PHASE 6
6-112	TBMP SIGNAL 1-25 PLAN PHASE 6
6-113	TBMP SIGNAL UNIVERSITY PLAN (ALL PHASES)
6-114	TEMPORARY SIGNAL PROFILE DETAILS

UNIT 1 MOT SUMMARY OF QUANTITIES				
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY	
405000	DE TOUR PAVEMENT CONSTRUCTION	SQ.YD.	14,371	
000010	TEMPORARY CWS RETAINED BY THE CONTRACTOR	L.F.T.	19,640	
000019	RE SETTING OF CONCRETE WALL BARRIER	L.F.T.	14,990	
000710	TEMPORARY CWS RETAINED BY THE CONTRACTOR (20') LN.FT.	L.F.T.	7,720	
000719	RE SETTING OF CONCRETE WALL BARRIER (20') LN.FT.	L.F.T.	6,500	
018000	TRAFFIC CONTROL MANAGEMENT	LS	LS	
018011	PUBLIC AWARENESS	LS	LS	
702610	PORTABLE CHANGEABLE MESSAGE SN	EACH	5	
702710	MOBILE TRAFFIC SIGNAL SYSTEM	EACH	2	
702810	TRAFFIC CONTROL DEVICES FOR CONSTRUCTION	LS	LS	
702850	LAW ENFORCEMENT IN CONSTRUCTION ZONE	ALLOW	ALLOW	
703205	TUBULAR FLEXIBLE TRAFFIC MARKER	EACH	50	
704099	TEMPORARY STRIPING	L.F.T.	249,224	
704101	TEMPORARY WORD OR SYMBOL	EACH	90	
704875	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TD	EACH	154	
704876	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TG	EACH	69	
704877	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TH	EACH	46	
720100	SAND BARREL VEHICLE IMPACT ATTENUATOR UNIT	EACH	3	
720110	RE MOVE/RESET IMPACT ATTENUATOR UNIT	EACH	7	
721000	REMOVAL OF PAVEMENT STRIPE	L.F.T.	150,000	
721101	REMOVAL OF PAVEMENT MARKING	EACH	25	

UNIT 1 TRAFFIC CONTROL DEVICES FOR CONSTRUCTION (L.S.)				
NMDOT SPEC.	ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
702	SS 702	CONSTRUCTION SIGNING	SQ.FT.	760
	SS 702	STEEL POSTS AND BASE POSTS FOR CONSTRUCTION SIGNING	L.F.T.	940
	SS 702	BARRICADE, TYPE B-8	EACH	21
	SS 702	VERTICAL PANEL, TYPE BACK TO BACK	EACH	190
	SS 702	CHANNELIZATION DEVICES TYPE DRUM	EACH	70
	SS 702	SEQUENTIAL ARROW DISPLAY	EACH	3

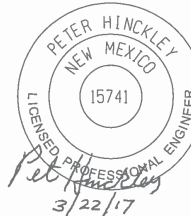
UNIT 2 MOT SUMMARY OF QUANTITIES				
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY	PROJ. TOTAL
405000	DE TOUR PAVEMENT CONSTRUCTION	SQ.YD.	15,400	26,579
000010	TEMPORARY CWS RETAINED BY THE CONTRACTOR	L.F.T.	3,810	23,450
000019	RE SETTING OF CONCRETE WALL BARRIER	L.F.T.	3,640	18,630
000710	TEMPORARY CWS RETAINED BY THE CONTRACTOR (20') LN.FT.	L.F.T.	1,140	8,880
000719	RE SETTING OF CONCRETE WALL BARRIER (20') LN.FT.	L.F.T.	940	5,440
018000	TRAFFIC CONTROL MANAGEMENT	LS	LS	LS
018011	PUBLIC AWARENESS	LS	LS	LS
702610	PORTABLE CHANGEABLE MESSAGE SN	EACH	6	11
702710	MOBILE TRAFFIC SIGNAL SYSTEM	EACH	2	4
702810	TRAFFIC CONTROL DEVICES FOR CONSTRUCTION	LS	LS	LS
702850	LAW ENFORCEMENT IN CONSTRUCTION ZONE	ALLOW	ALLOW	ALLOW
703205	TUBULAR FLEXIBLE TRAFFIC MARKER	EACH	393	443
704099	TEMPORARY STRIPING	L.F.T.	66,493	315,717
704101	TEMPORARY WORD OR SYMBOL	EACH	132	222
704875	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TD	EACH	248	402
704876	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TG	EACH	416	485
704877	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TH	EACH	521	567
720100	SAND BARREL VEHICLE IMPACT ATTENUATOR UNIT	EACH	2	5
720110	RE MOVE/RESET IMPACT ATTENUATOR UNIT	EACH	5	12
721000	REMOVAL OF PAVEMENT STRIPE	L.F.T.	60,000	210,000
721101	REMOVAL OF PAVEMENT MARKING	EACH	45	70

UNIT 2 TRAFFIC CONTROL DEVICES FOR CONSTRUCTION (L.S.)				
NMDOT SPEC.	ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
702	SS 702	CONSTRUCTION SIGNING	SQ.FT.	1,100
	SS 702	STEEL POSTS AND BASE POSTS FOR CONSTRUCTION SIGNING	L.F.T.	1,430
	SS 702	BARRICADE, TYPE B-8	EACH	11
	SS 702	VERTICAL PANEL, TYPE BACK TO BACK	EACH	615
	SS 702	CHANNELIZATION DEVICES TYPE DRUM	EACH	47
	SS 702	SEQUENTIAL ARROW DISPLAY	EACH	6

NOTES:

1. ESTIMATED QUANTITIES INDICATED FOR ITEM 702810 - TRAFFIC CONTROL DEVICES FOR CONSTRUCTION, ARE FOR CONTRACTORS INFORMATION ONLY. PAYMENT FOR ALL TRAFFIC CONTROL DEVICES REQUIRED DURING CONSTRUCTION SHALL BE INCLUDED IN ITEM 702810 AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE THEREFOR.

2. CONSTRUCTION PHASES RUN IN SERIES. THE CONTRACTOR SHALL NOT WORK ON SEPARATE PHASES CONCURRENTLY UNLESS APPROVED BY THE PROJECT MANAGER. THE CONTRACTOR MAY REQUEST TO ALTER THE SUGGESTED PHASING AND TRAFFIC CONTROL. APPROVAL OF THE CONTRACTOR'S REQUEST WILL BE AT THE SOLE DISCRETION OF THE PROJECT MANAGER. THE NMDOT WILL NOT CONSIDER INCREASES IN COSTS DUE TO PHASE CHANGES AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR QUANTITY OVERRUNS AND ADDITIONAL COST ASSOCIATED WITH THE REQUEST.



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3	REVISED BARRIER QUANTITIES AND ADDED NOTE	5/21/17	KL
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1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
1-25/RIO BRAVO INTERCHANGE			
SHEET INDEX			
MISCELLANEOUS QUANTITIES			

 DESIGNED BY: AECOM
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 TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6021 America Parkway, N.E. Suite 800
 Albuquerque, New Mexico 87110
 1-800-853-7500

 AECOM
 www.aecom.com

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1:

SHEET NO. 6 of 10

MOT SHEET INDEX

SHEET NO.	DESCRIPTION	REV. DATE
6-1	QUANTITIES - SHEET INDEX	
6-1A	UNIT 1 QUANTITIES	
6-1B	UNIT 2 QUANTITIES	
6-1C	QUANTITIES	
6-2	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 0	
6-3	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 1	
6-4	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 1A, 1B & 1C	
6-5	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 2	
6-6	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 2A, 2B & 2C	
6-7	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 3	
6-8	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 3A, 3B, 3C & 3D	
6-9	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 4	
6-10	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 4A	
6-11	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 5	
6-12	SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 6	
6-13	TRAFFIC CONTROL NOTES	
6-14	TRAFFIC CONTROL NOTES	
6-15	TRAFFIC CONTROL DETAILS	
6-16	TYPICAL SECTIONS - RIO BRAVO PHASE 0	
6-17	TYPICAL SECTIONS - RIO BRAVO PHASE 1	
6-18	TYPICAL SECTIONS - 125 PHASE 1	
6-19	TYPICAL SECTIONS - RIO BRAVO PHASE 2	
6-20	TYPICAL SECTIONS - 125 PHASE 2	
6-21	TYPICAL SECTIONS - RIO BRAVO PHASE 3A, 3B, 3C & 3D	
6-22	TYPICAL SECTIONS - RIO BRAVO PHASE 3A, 3B, 3C & 3D 125 PHASE 3	
6-23	TYPICAL SECTIONS - RIO BRAVO PHASE 4	
6-24	TYPICAL SECTIONS - 125 PHASE 4	
6-25	TYPICAL SECTIONS - RIO BRAVO PHASE 4A	
6-26	TYPICAL SECTIONS - RIO BRAVO PHASE 5	
6-27	TYPICAL SECTIONS - RIO BRAVO PHASE 6	
6-28	RIO BRAVO PHASE 0 SHEET 1	
6-29	RIO BRAVO PHASE 0 SHEET 2	
6-30	RIO BRAVO PHASE 0 SHEET 3	
6-31	RIO BRAVO PHASE 0 SHEET 4	
6-32	RIO BRAVO PHASE 0 SHEET 5	
6-33	RIO BRAVO PHASE 0 SHEET 6	
6-34	RIO BRAVO PHASE 1 SHEET 1	
6-35	RIO BRAVO PHASE 1 SHEET 2	
6-36	RIO BRAVO PHASE 1 SHEET 3	
6-37	RIO BRAVO PHASE 1 SHEET 4	
6-38	BROADWAY PHASE 1A SHEET 1	
6-39	BROADWAY PHASE 1B SHEET 1	
6-40	BROADWAY PHASE 1C SHEET 1	
6-41	05 PHASE 1 SHEET 1	
6-42	05 PHASE 1 SHEET 2	
6-43	05 PHASE 1 SHEET 3	
6-44	05 PHASE 1 SHEET 4	
6-45	05 PHASE 1 SHEET 5	
6-46	RIO BRAVO PHASE 2 SHEET 1	
6-47	RIO BRAVO PHASE 2 SHEET 2	
6-48	RIO BRAVO PHASE 2 SHEET 3	
6-49	RIO BRAVO PHASE 2 SHEET 4	
6-50	BROADWAY PHASE 2A SHEET 1	
6-51	BROADWAY PHASE 2B SHEET 1	
6-52	BROADWAY PHASE 2C SHEET 1	
6-53	05 PHASE 2 SHEET 1	
6-54	05 PHASE 2 SHEET 2	
6-55	05 PHASE 2 SHEET 3	
6-56	05 PHASE 2 SHEET 4	
6-57	05 PHASE 2 SHEET 5	
6-58	RIO BRAVO PHASE 3 SHEET 1	
6-59	RIO BRAVO PHASE 3 SHEET 2	
6-60	RIO BRAVO PHASE 3 SHEET 3	
6-61	RIO BRAVO PHASE 3 SHEET 4	
6-62	BROADWAY PHASE 3A SHEET 1	
6-63	BROADWAY PHASE 3B SHEET 1	
6-64	BROADWAY PHASE 3C SHEET 1	
6-65	BROADWAY PHASE 3D SHEET 1	
6-66	05 PHASE 3 SHEET 1	
6-67	05 PHASE 3 SHEET 2	
6-68	05 PHASE 3 SHEET 3	
6-69	05 PHASE 3 SHEET 4	
6-70	05 PHASE 3 SHEET 5	

6-71	05 PHASE 4 SHEET 1
6-72	05 PHASE 4 SHEET 2
6-73	05 PHASE 4 SHEET 3
6-74	05 PHASE 4 SHEET 4
6-75	05 PHASE 4 SHEET 5
6-76	05 PHASE 4 SHEET 6
6-77	RIO BRAVO PHASE 4A SHEET 1
6-78	RIO BRAVO PHASE 4A SHEET 2
6-79	RIO BRAVO PHASE 4A SHEET 3
6-80	RIO BRAVO PHASE 5 SHEET 1
6-81	RIO BRAVO PHASE 5 SHEET 2
6-82	RIO BRAVO PHASE 5 SHEET 3
6-83	RIO BRAVO PHASE 6 SHEET 1
6-84	RIO BRAVO PHASE 6 SHEET 2
6-85	RIO BRAVO PHASE 6 SHEET 3
6-86	PHASE 1 - SB NORTH CROSSOVER SHEET 1
6-87	PHASE 1 - SB NORTH CROSSOVER SHEET 2
6-88	PHASE 1 - SB NORTH CROSSOVER SHEET 3
6-89	PHASE 1 - SB NORTH CROSSOVER SHEET 4
6-90	PHASE 1 - DETOUR NB ON RAMP
6-91	PHASE 2 - SB SOUTH CROSSOVER SHEET 1
6-92	PHASE 2 - SB SOUTH CROSSOVER SHEET 2
6-93	PHASE 2 - NB ON RAMP
6-94	TEMP SIGNAL GENERAL NOTES AND QUANTITIES
6-95	TEMP SIGNAL BROADWAY PLAN PHASE 1A
6-96	TEMP SIGNAL BROADWAY PLAN PHASE 1B
6-97	TEMP SIGNAL BROADWAY PLAN PHASE 1C
6-98	TEMP SIGNAL BROADWAY PLAN PHASE 2A
6-99	TEMP SIGNAL BROADWAY PLAN PHASE 2B
6-100	TEMP SIGNAL BROADWAY PLAN PHASE 2C
6-101	TEMP SIGNAL BROADWAY PLAN PHASE 3A
6-102	TEMP SIGNAL BROADWAY PLAN PHASE 3B
6-103	TEMP SIGNAL BROADWAY PLAN PHASE 3C
6-104	TEMP SIGNAL BROADWAY PLAN PHASE 3D
6-105	TEMP SIGNAL 125 PLAN PHASE 1
6-106	TEMP SIGNAL 125 PLAN PHASE 2
6-107	TEMP SIGNAL 125 PLAN PHASE 3
6-108	TEMP SIGNAL 125 PLAN PHASE 4
6-109	TEMP SIGNAL 125 PLAN PHASE 4A
6-110	TEMP SIGNAL 125 PLAN PHASE 5
6-111	TEMP SIGNAL 125 PLAN PHASE 5A
6-112	TEMP SIGNAL 125 PLAN PHASE 6
6-113	TEMP SIGNAL UNIVERSITY PLAN (ALL PHASES)
6-114	TEMPORARY SIGNAL PROFILE DETAILS

UNIT 1 MOT SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
405000	DETOUR PAVEMENT CONSTRUCTION	SQ.YD	11,371
606010	TEMPORARY CWB RETAINED BY THE CONTRACTOR	LFT	19,612
606015	RESETTING OF CONCRETE WALL BARRIER	LFT	13,076
606019	TEMPORARY CWB RETAINED BY THE CONTRACTOR (20) LN.FT.	LFT	7,717
606019	RESETTING OF CONCRETE WALL BARRIER (20) LN.FT.	LFT	8,475
702010	PORTABLE CHANGEABLE MESSAGE SIGN	EACH	5
702710	MOBILE TRAFFIC SIGNAL SYSTEM	EACH	2
702810	TRAFFIC CONTROL DEVICES FOR CONSTRUCTION	LS	LS
703205	TUBULAR FLEXIBLE TRAFFIC MARKER	EACH	50
704059	TEMPORARY STRIPING	LFT	700,667
704101	TEMPORARY WORD OR SYMBOL	EACH	90
704075	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TD	EACH	154
704076	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TG	EACH	69
704077	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TH	EACH	46
720100	SAND BARREL VEHICLE IMPACT ATTENUATOR UNIT	EACH	3
720110	REMOVE/RESET IMPACT ATTENUATOR UNIT	EACH	7
721000	REMOVAL OF PAVEMENT STRIPE	LFT	150,000
721101	REMOVAL OF PAVEMENT MARKING	EACH	25

UNIT 1 TRAFFIC CONTROL DEVICES FOR CONSTRUCTION (L.S.)

HMDDT SPEC.	ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
	SS 702	CONSTRUCTION SIGNING	SQ.FT	760
	SS 702	STEEL POSTS AND BASE POSTS FOR CONSTRUCTION SIGNING	LFT	940
	SS 702	BARRICADE, TYPE III-S	EACH	21
	SS 702	VERTICAL PANEL, TYPE BACK TO BACK	EACH	190
	SS 702	CHANNELIZATION DEVICES TYPE DRUM	EACH	70
	SS 702	SEQUENTIAL ARROW DISPLAY	EACH	3

UNIT 2 MOT SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY	PROJ. TOTAL QUANTITY
405000	DETOUR PAVEMENT CONSTRUCTION	SQ.YD	15,508	26,879
606010	TEMPORARY CWB RETAINED BY THE CONTRACTOR	LFT	3,067	23,419
606015	RESETTING OF CONCRETE WALL BARRIER	LFT	2,502	15,578
606019	TEMPORARY CWB RETAINED BY THE CONTRACTOR (20) LN.FT.	LFT	1,122	8,639
606019	RESETTING OF CONCRETE WALL BARRIER (20) LN.FT.	LFT	923	9,398
702010	PORTABLE CHANGEABLE MESSAGE SIGN	EACH	6	11
702710	MOBILE TRAFFIC SIGNAL SYSTEM	EACH	2	4
702810	TRAFFIC CONTROL DEVICES FOR CONSTRUCTION	LS	LS	LS
703205	TUBULAR FLEXIBLE TRAFFIC MARKER	EACH	393	443
704059	TEMPORARY STRIPING	LFT	346,102	1,046,769
704101	TEMPORARY WORD OR SYMBOL	EACH	132	222
704075	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TD	EACH	248	402
704076	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TG	EACH	416	485
704077	TEMPORARY REFLECTIVE RAISED PVMT MARKER TYPE TH	EACH	521	567
720100	SAND BARREL VEHICLE IMPACT ATTENUATOR UNIT	EACH	2	5
720110	REMOVE/RESET IMPACT ATTENUATOR UNIT	EACH	5	12
721000	REMOVAL OF PAVEMENT STRIPE	LFT	60,000	210,000
721101	REMOVAL OF PAVEMENT MARKING	EACH	45	70

UNIT 2 TRAFFIC CONTROL DEVICES FOR CONSTRUCTION (L.S.)

HMDDT SPEC.	ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY	PROJ. TOTAL QUANTITY
	SS 702	CONSTRUCTION SIGNING	SQ.FT	1,100	1,850
	SS 702	STEEL POSTS AND BASE POSTS FOR CONSTRUCTION SIGNING	LFT	1,430	2,370
	SS 702	BARRICADE, TYPE III-S	EACH	11	32
	SS 702	VERTICAL PANEL, TYPE BACK TO BACK	EACH	615	895
	SS 702	CHANNELIZATION DEVICES TYPE DRUM	EACH	47	117
	SS 702	SEQUENTIAL ARROW DISPLAY	EACH	6	9

NOTE: ESTIMATED QUANTITIES INDICATED FOR ITEM 702810 - TRAFFIC CONTROL DEVICES FOR CONSTRUCTION, ARE FOR CONTRACTORS INFORMATION ONLY. PAYMENT FOR ALL TRAFFIC CONTROL DEVICES REQUIRED DURING CONSTRUCTION SHALL BE INCLUDED IN ITEM 702810 AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE THEREFOR.



8			
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4			
3			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SHEET INDEX			
MISCELLANEOUS QUANTITIES			

As-Built

UNIT 1 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTORS INFORMATION ONLY - ITEM NUMBER SS 702

SIGN CODE	MESSAGE	SIZE (IN.)		SIGN AREA EA (SQ. FT.)	NO. OF SIGNS	TOTAL SIGN AREA (SQ. FT.)	POST LENGTHS (UN. FT.)			POST TOTAL (LIN. FT.)	BARRICADE- TYPE III (EA)	VERTICAL PANEL- BK TO BK (EA)	CHANNELIZATION DEVICES TYPE DRUM (EA)
		L	W				LEFT	CENTER	RIGHT				
G20-2-36	END ROAD WORK	18	36	4.5	5	22.5		12		60			
R2-1-36-35	SPEED LIMIT 35	36	48	12	1	12	12		12	24			
R2-1-48-45	SPEED LIMIT 45	48	60	20	1	20	12		12	24			
R2-1-48-55	SPEED LIMIT 55	48	60	20	3	60	12		12	72			
R2-1-48-65	SPEED LIMIT 65	48	60	20	2	40	12		12	48			
R3-5	RIGHT TURN ONLY	24	30	5	1	5		12		12			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30	30	6.25	1	6.25		12		12			
R4-7A	KEEP RIGHT	24	30	5	4	20	ATTACH TO VERTICAL PANEL			0			
R4-7B	KEEP LEFT	24	30	5	1	5	ATTACH TO VERTICAL PANEL			0			
R11-2-48	ROAD CLOSED	30	48	10	7	70	ATTACH TO BARRICADE			0			
W1-4bR-36	DOUBLE REVERSE CURVE RIGHT	36	36	9	3	27		12		36			
W1-4bL-36	DOUBLE REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W3-6-48	ARROW	24	48	8	4	32	ATTACH TO BARRICADE			0			
W3-5-36-35	SPEED LIMIT 35 AHEAD	36	36	9	1	9		12		12			
W3-5-48-55	SPEED LIMIT 55 AHEAD	48	48	16	2	32	12		12	48			
W4-1-48	MERGE	48	48	16	2	32	12		12	48			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48	48	16	1	16	12		12	24			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48	48	16	1	16	12		12	24			
W4-3-36	ADDED LANE	36	36	9	1	9		12		12			
W4-3-48	ADDED LANE	48	48	16	2	32	12		12	48			
W13-1P-35	SPEED LIMIT 35	24	24	4	2	8		12		24			
W13-1P-55	SPEED LIMIT 55	30	30	6.25	1	6.25		12		12			
W20-1-36	ROAD WORK AHEAD	36	36	9	4	36		12		48			
W20-1-48	ROAD WORK AHEAD	48	48	16	1	16	12		12	24			
W20-1-48-1	ROAD WORK 1 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48	48	16	2	32	12		12	48			
W20-5L-36	LEFT LANE CLOSED AHEAD	36	36	9	1	9		12		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48	48	16	2	32	12		12	48			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48	48	16	1	16	12		12	24			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48	48	16	1	16	12		12	24			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48	48	16	1	16	12		12	24			
SPECIALTY	NO SHOULDER	48	48	16	1	16	12		12	24			
SPECIALTY	RIGHT LANE EXIT ONLY	48	48	16	2	32	12		12	48			
SPECIALTY	CUSTOM- TWO LEFT TURN ARROWS	36	36	9	1	9		12		12			
PROJECT TOTAL						751				936	21	182	69
PROJECT USE						760				940	21	190	70

NOTE:

1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.

2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 1 - VEHICULAR IMPACT ATTENUATOR

ITEM NUMBER 720110

PHASE	LOCATION	EACH	REMARKS
PHASE 0			
PHASE 1	I-25 SB	2	
	RIO BRAVO WB	1	
PHASE 2	I-25 NB	1	
	I-25 SB	1	
PHASE 3	I-25 SB	2	
PHASE 4	I-25 NB	2	
	I-25 SB	1	
PHASE 5			
PHASE 6			

UNIT 1 - RESETTling OF CONCRETE WALL 10' BARRIER

ITEM NUMBER 606619

PHASE	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LIN. FT.
PHASE 0					
PHASE 1					
PHASE 2	I-25 NB	1708+70 TO 1753+10	7' RT TO 10' LT	4444	4450
	I-25 NB	1675+34 TO 1678+12	36' LT TO 36' LT	278	280
	I-25 SB	1120+00 TO 1346+45	22' LT TO 15' LT	2645	2650
	I-25 NB	1675+34 TO 1694+11	6' LT TO 8' LT	1875	1880
PHASE 3	I-25 NB	1675+34 TO 1678+02	2' LT TO 2' LT	268	270
	I-25 NB	1675+34 TO 1687+59	12' LT TO 17' RT	1226	1230
	I-25 NB	1682+55 TO 1687+60	6' LT TO 5' LT	504	510
	I-25 SB	1120+00 TO 1352+32	22' LT TO 5' RT	3233	3240
PHASE 4	I-25 SB	1345+72 TO 1350+50	17 RT TO 21' RT	478	480
PHASE 5					
PHASE 6					
TOTAL					14990

UNIT 1 - RESETTling OF CONCRETE WALL 20' BARRIER

ITEM NUMBER 606719

PHASE	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LIN. FT.
PHASE 0					
PHASE 1					
PHASE 2					
PHASE 3	I-25 NB	1675+34 TO 1694+11	45' LT TO 40' LT	1875	1880
PHASE 4	I-25 NB	1680+29 TO 1716+12	4' RT TO 6' LT	3582	3600
	I-25 NB	1722+93 TO 1753+10	9' LT TO 3' LT	3018	3020
PHASE 5					
PHASE 6					
TOTAL					8500



6				
5				
4				
3	REVISED RESET BARRIER SCHEDULE	3/21/17	KL	
2				
1	ADDITION OF ITEM NUMBERS	3/7/17	KL	
NO.	DESCRIPTION	DATE	BY	

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE

UNIT 1
MOT QUANTITIES

DESIGNED BY: AECOM
PHONE: 505.895.7500
EMAIL:

Design File: p:\1717479-PW\INT.oeconline.local\AECOM_0501_NA\Documents\60393326-125-Rio Bravo\A300280\Plans\6_Sheets\A300280\T501A.dgn
3/22/2017 9:53:50 AM k.je.lehto
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TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
501 America Parkway NE, Suite 500
Albuquerque, New Mexico 87110
T 505.895.7500

AECOM
www.aecom.com

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1

SHEET NO. 6 - 01A

UNIT 1 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTOR INFORMATION ONLY- ITEM NUMBER SS 702

SIGN CODE	MESSAGE	SIZE (IN.)	SIGN AREA	NO. OF	TOTAL SIGN	POST LENGTHS (LIN. FT.)	POST TOTAL	BARRICADE	VERTICAL PANEL	CHANNELIZATION
		L W	EA (SQ. FT.)	SIGNS	AREA (SQ. FT.)	LEFT CENTER RIGHT	(LIN. FT.)	TYPE III (EA)	BK TO BK (EA)	DEVICES TYPE
G20-2-36	END ROAD WORK	18 36	4.5	5	22.5		12			DRUM (EA)
R2-1-36-35	SPEED LIMIT 35	36 48	12	1	12	12	12			
R2-1-48-45	SPEED LIMIT 45	48 60	20	1	20	12	12			
R2-1-48-55	SPEED LIMIT 55	48 60	20	3	60	12	12			
R2-1-48-65	SPEED LIMIT 65	48 60	20	2	40	12	12			
R3-5	RIGHT TURN ONLY	24 30	5	1	5		12			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30 30	6.25	1	6.25		12			
R4-7A	KEEP RIGHT	24 30	5	4	20					
R4-7B	KEEP LEFT	24 30	5	1	5					
R11-2-48	ROAD CLOSED	30 48	10	7	70					
W1-4bR-36	DOUBLE REVERSE CURVE RIGHT	36 36	9	3	27		12			
W1-4bL-36	DOUBLE REVERSE CURVE LEFT	36 36	9	1	9		12			
W1-6-48	ARROW	24 48	8	4	32					
W3-5-36-35	SPEED LIMIT 35 AHEAD	36 36	9	1	9		12			
W3-5-48-55	SPEED LIMIT 55 AHEAD	48 48	16	2	32	12	12			
W4-1-48	MERGE	48 48	16	2	32	12	12			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48 48	16	1	16	12	12			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48 48	16	1	16	12	12			
W4-3-36	ADDED LANE	36 36	9	1	9		12			
W4-3-48	ADDED LANE	48 48	16	2	32	12	12			
W13-1P-35	SPEED LIMIT 35	24 24	4	2	8		12			
W13-1P-55	SPEED LIMIT 55	30 30	6.25	1	6.25		12			
W20-1-36	ROAD WORK AHEAD	36 36	9	4	36		12			
W20-1-48	ROAD WORK AHEAD	48 48	16	1	16	12	12			
W20-1-48-1	ROAD WORK 1 MILE	48 48	16	2	32	12	12			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48 48	16	2	32	12	12			
W20-5L-36	LEFT LANE CLOSED AHEAD	36 36	9	1	9		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48 48	16	2	32	12	12			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48 48	16	1	16	12	12			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48 48	16	1	16	12	12			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48 48	16	1	16	12	12			
SPECIALTY	NO SHOULDER	48 48	16	1	16	12	12			
SPECIALTY	RIGHT LANE EXIT ONLY	48 48	16	2	32	12	12			
SPECIALTY	CUSTOM- TWO LEFT TURN ARROWS	36 36	9	1	9		12			
PROJECT TOTAL					751		936	21	182	69
PROJECTUSE					760		940	21	190	70

NOTE:

1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.

2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 1 - VEHICULAR IMPACT ATTENUATOR

ITEM NUMBER 720110

PHASE	LOCATION	TEACH	REMARKS
PHASE 0			
PHASE 1	I-25 SB	2	
	RIO BRAVO WB	1	
PHASE 2	I-25 NB	1	
	I-25 SB	1	
PHASE 3	I-25 SB	2	
PHASE 4	I-25 NB	2	
	I-25 SB	1	
PHASE 5			
PHASE 6			

UNIT 1 - RESETTNG OF CONCRETE WALL 10' BARRIER

ITEM NUMBER 606619

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1708+70 TO 1753+10	4444	RT	4450	
	I-25 NB	1675+34 TO 1678+12	278	RT	280	
	I-25 SB	1320+00 TO 1346+45	2645	LT	2650	
PHASE 3	I-25 NB	1666+12 TO 1675+34	923	LT	930	
PHASE 4	I-25 SB	1345+72 TO 1350+50	478	LT	480	
PHASE 5						
PHASE 6						

UNIT 1 - RESETTNG OF CONCRETE WALL 20' BARRIER

ITEM NUMBER 606719

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1664+12 TO 1675+34	1122	RT	1140	
PHASE 3						
PHASE 4						
PHASE 5						
PHASE 6						



6				
5				
4				
3				
2				
1	ADDITION OF ITEM NUMBERS	3/7/2017	KL	
NO.	DESCRIPTION	DATE	BY	
REVISIONS (OR CHANGE NOTICES)				
NEW MEXICO DEPARTMENT OF TRANSPORTATION				
I-25/RIO BRAVO INTERCHANGE				
UNIT 1				
MOT QUANTITIES				

EMAIL

PHONE (505) 856-7500

DESIGNED BY: AECOM

UNIT 1 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTORS INFORMATION ONLY

SIGN CODE	MESSAGE	SIZE (IN.)		SIGN AREA EA (SQ. FT.)	NO. OF SIGNS	TOTAL SIGN AREA (SQ. FT.)	POST LENGTHS (LIN. FT.)			POST TOTAL (LIN. FT.)	BARRICADE- TYPE III (EA)	VERTICAL PANEL- BK TO BK (EA)	CHANNELIZATION DEVICES TYPE DRUM (EA)
		L	W				LEFT	CENTER	RIGHT				
G20-3-36	END ROAD WORK	18	36	4.5	5	22.5		12		60			
R2-1-36-35	SPEED LIMIT 35	36	48	12	1	12	12		12	24			
R2-1-48-45	SPEED LIMIT 45	48	60	20	1	20	12		12	24			
R2-1-48-55	SPEED LIMIT 55	48	60	20	3	60	12		12	72			
R2-1-48-65	SPEED LIMIT 65	48	60	20	2	40	12		12	48			
R3-5	RIGHT TURN ONLY	24	30	5	1	5		12		12			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30	30	6.25	1	6.25		12		12			
R4-7A	KEEP RIGHT	24	30	5	4	20	ATTACH TO VERTICAL PANEL			0			
R4-7B	KEEP LEFT	24	30	5	1	5	ATTACH TO VERTICAL PANEL			0			
R11-2-48	ROAD CLOSED	30	48	10	7	70	ATTACH TO BARRICADE			0			
W1-4B-36	DOUBLE REVERSE CURVE RIGHT	36	36	9	1	9		12		36			
W1-4B-36	DOUBLE REVERSE CURVE LEFT	36	36	9	1	9		12		36			
W1-6-48	ARROW	24	48	8	4	32	ATTACH TO BARRICADE			0			
W3-5-36-35	SPEED LIMIT 35 AHEAD	36	36	9	1	9		12		12			
W3-5-48-55	SPEED LIMIT 55 AHEAD	48	48	16	2	32	12		12	48			
W4-1-48	MERGE	48	48	16	2	32	12		12	48			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48	48	16	1	16	12		12	24			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48	48	16	1	16	12		12	24			
W4-3-36	ADDED LANE	36	36	9	1	9		12		12			
W4-3-48	ADDED LANE	48	48	16	2	32	12		12	48			
W13-1P-35	SPEED LIMIT 35	24	24	4	2	8		12		24			
W13-1P-55	SPEED LIMIT 55	30	30	6.25	1	6.25		12		12			
W20-1-36	ROAD WORK AHEAD	36	36	9	4	36		12		48			
W20-1-48	ROAD WORK AHEAD	48	48	16	1	16	12		12	24			
W20-1-48-1	ROAD WORK 1 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48	48	16	2	32	12		12	48			
W20-5L-36	LEFT LANE CLOSED AHEAD	36	36	9	1	9		12		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48	48	16	2	32	12		12	48			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48	48	16	1	16	12		12	24			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48	48	16	1	16	12		12	24			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48	48	16	1	16	12		12	24			
SPECIALTY	NO SHOULDER	48	48	16	1	16	12		12	24			
SPECIALTY	RIGHT LANE EXIT ONLY	48	48	16	2	32	12		12	48			
SPECIALTY	CUSTOM- TWO LEFT TURN ARROWS	36	36	9	1	9		12		12			
PROJECT TOTAL						751				936	21	182	69
PROJECT USE						760				940	21	190	70

NOTE:

- 1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.
2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 1 - VEHICULAR IMPACT ATTENUATOR

PHASE	LOCATION	EACH	REMARKS
PHASE 0			
PHASE 1	I-25 SB	2	
	RIO BRAVO WB	1	
PHASE 2	I-25 NB	1	
	I-25 SB	1	
PHASE 3	I-25 SB	2	
PHASE 4	I-25 NB	2	
	I-25 SB	1	
PHASE 5			
PHASE 6			

UNIT 1 - RESETTling OF CONCRETE WALL 10' BARRIER

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1708+70 TO 1753+10	4444	RT	4450	
	I-25 NB	1675+34 TO 1678+12	278	RT	280	
	I-25 SB	1320+00 TO 1346+45	2645	LT	2650	
PHASE 3	I-25 NB	1666+12 TO 1675+34	923	LT	930	
PHASE 4	I-25 SB	1345+72 TO 1350+50	478	LT	480	
PHASE 5						
PHASE 6						

UNIT 1 - RESETTling OF CONCRETE WALL 20' BARRIER

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1664+12 TO 1675+34	1122	RT	1140	
PHASE 3						
PHASE 4						
PHASE 5						
PHASE 6						



6			
5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
UNIT 1			
MOT QUANTITIES			

DESIGNED BY: AECOM

PHONE: (505) 855-7200

EMAIL:

UNIT 2 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTORS INFORMATION ONLY (ITEM NUMBER SS 702)

SIGN CODE	MESSAGE	SIZE (IN.)		SIGN AREA EA (SQ. FT.)	NO. OF SIGNS	TOTAL SIGN AREA (SQ. FT.)	POST LENGTHS (LIN. FT.)			POST TOTAL (LIN. FT.)	BARRICADE- TYPE III (EA)	VERTICAL PANEL- BK TO BK (EA)	CHANNELUZATION DEVICES TYPE DRUM (EA)
		L	W				LEFT	CENTER	RIGHT				
G20-2-36	END ROAD WORK	18	36	4.5	5	22.5		12		60			
R2-1-36-35	SPEED LIMIT 35	36	48	12	3	36	12		12	72			
R2-1-36-45	SPEED LIMIT 45	36	48	12	3	36	12		12	72			
R2-1-36-55	SPEED LIMIT 55	36	48	12	2	24	12		12	48			
R2-1-48-45	SPEED LIMIT 45	48	60	20	1	20	12		12	24			
R2-1-48-55	SPEED LIMIT 55	48	60	20	2	40	12		12	48			
R2-1-48-65	SPEED LIMIT 65	48	60	20	2	40	12		12	48			
R2-1-48-75	SPEED LIMIT 75	48	60	20	2	40	12		12	48			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30	30	6.25	5	31.25		12		60			
R4-7A	KEEP RIGHT	24	30	5	10	50	ATTACH TO VERTICAL PANEL			0			
R9-9	SIDEWALK CLOSED	18	36	4.5	1	4.5		12		12			
R11-2-48	ROAD CLOSED	30	48	10	6	60	ATTACH TO BARRICADE			0			
R11-4	ROAD CLOSED TO THRU TRAFFIC	30	60	12.5	1	12.5	12		12	24			
SP-10R-30	BUSINESS ACCESS	24	30	5	1	5		12		12			
W1-4R-36	REVERSE CURVE RIGHT	36	36	9	1	9		12		12			
W1-4L-36	REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-36	DOUBLE REVERSE CURVE RIGHT	36	36	9	3	27		12		36			
W1-4BL-36	DOUBLE REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-48	DOUBLE REVERSE CURVE RIGHT	48	48	16	1	16	12		12	24			
W1-6-48	ARROW	24	48	8	6	48	ATTACH TO BARRICADE			0			
W3-5-36-35	SPEED LIMIT 35 AHEAD	36	36	9	2	18		12		24			
W3-5-36-45	SPEED LIMIT 45 AHEAD	36	36	9	2	18		12		24			
W3-5-48-65	SPEED LIMIT 65 AHEAD	48	48	16	2	32	12		12	48			
W4-2R-36	RIGHT LANE ENDS/MERGE LEFT	36	36	9	2	18		12		24			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48	48	16	2	32	12		12	48			
W4-2L-36	LEFT LANE ENDS/MERGE RIGHT	36	36	9	2	18		12		24			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48	48	16	2	32	12		12	48			
W4-3-36	ADDED LANE	36	36	9	2	18		12		24			
W4-3-48	ADDED LANE	48	48	16	1	16	12		12	24			
W9-2-36	LANE ENDS MERGE RIGHT	36	36	9	1	9		12		12			
W12-1	LANE SPLIT	36	36	9	1	9	ATTACH TO VERTICAL PANEL			0			
W20-1-36	ROAD WORK AHEAD	36	36	9	3	27		12		36			
W20-1-48-1/2	ROAD WORK 1/2 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1	ROAD WORK 1 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48	48	16	2	32	12		12	48			
W20-5L-36	LEFT LANE CLOSED AHEAD	36	36	9	2	18		12		24			
W20-5R-36	RIGHT LANE CLOSED AHEAD	36	36	9	1	9		12		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48	48	16	1	16	12		12	24			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5R-48-1	RIGHT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
SPECIALTY	BIKE LANE CLOSED AHEAD	36	36	9	1	9		12		12			
SPECIALTY	MULTI USE PATH CLOSED	48	48	16	1	16	12		12	24			
SPECIALTY	CUSTOM- LEFT, THRU, THRU/RIGHT	36	48	12	1	12	12		12	24			
SPECIALTY	RIGHT LANE RIGHT TURN ONLY	36	36	9	1	9		12		12			
PROJECT TOTAL						1099.75				1428	11	615	47
PROJECT USE						1100				1430	11	620	50

NOTE:

- 1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.
2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 2 - RESETTING OF CONCRETE WALL 20' BARRIER

ITEM NUMBER 606619

PHASE	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LIN. FT.
PHASE 0					
PHASE 1					
PHASE 2					
PHASE 3	I-25 NB	1666+12 TO 1675+34	84' LT TO 45' LT	923	940
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL					940

UNIT 2 - RESETTING OF CONCRETE WALL 10' BARRIER

ITEM NUMBER 606619

PHASE	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LIN. FT.
PHASE 0					
PHASE 1					
PHASE 2	RIO BRAVO	2016+83 TO 2022+36	21' RT TO 2' LT	556	560
	I-25 NB	1663+24 TO 1669+84	60' LT TO 44' LT	660	660
	I-25 NB	1666+68 TO 1675+34	39' LT TO 36' LT	866	870
	I-25 NB	1664+12 TO 1675+34	2' LT TO 6' LT	1122	1130
PHASE 3	I-25 NB	1673+24 TO 1675+34	5' LT TO 2' LT	210	210
	I-25 NB	1673+25 TO 1675+34	26' LT TO 12' LT	210	210
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL					3640

UNIT 2 - VEHICULAR IMPACT ATTENUATOR

ITEM NUMBER 720110

PHASE	LOCATION	EACH	REMARKS
PHASE 0			
PHASE 1	RIO BRAVO WB	1	
	I-25 SB	1	
	I-25 NB	1	
PHASE 2	RIO BRAVO EB	1	
	I-25 NB	1	
PHASE 3	I-25 NB	1	
PHASE 4			
PHASE 5			
PHASE 6			



5				
4				
3	REVISED RESET BARRIER SCHEDULE	3/21/17	KL	
2				
1	ADDITION OF ITEM NUMBERS	3/7/17	KL	
NO.	DESCRIPTION	DATE	BY	

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE

UNIT 2
MOT QUANTITIES

UNIT 2 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTORS INFORMATION ONLY ITEM NUMBER SS 702

SIGN CODE	MESSAGE	SIZE (IN.)		SIGN AREA EA (SQ. FT.)	NO. OF SIGNS	TOTAL SIGN AREA (SQ. FT.)	POST LENGTHS (LIN. FT.)			POST TOTAL (LIN. FT.)	BARRICADE- TYPE III (EA)	VERTICAL PANEL- BK TO BK (EA)	CHANNELIZATION DEVICES TYPE DRUM (EA)
		L	W				LEFT	CENTER	RIGHT				
G20-2-36	END ROAD WORK	18	36	4.5	5	22.5			12	60			
R2-1-36-35	SPEED LIMIT 35	36	48	12	3	36	12		12	72			
R2-1-36-45	SPEED LIMIT 45	36	48	12	3	36	12		12	72			
R2-1-36-55	SPEED LIMIT 55	36	48	12	2	24	12		12	48			
R2-1-48-45	SPEED LIMIT 45	48	60	20	1	20	12		12	24			
R2-1-48-55	SPEED LIMIT 55	48	60	20	2	40	12		12	48			
R2-1-48-65	SPEED LIMIT 65	48	60	20	2	40	12		12	48			
R2-1-48-75	SPEED LIMIT 75	48	60	20	2	40	12		12	48			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30	30	6.25	5	31.25		12		60			
R4-7A	KEEP RIGHT	24	30	5	10	50	ATTACH TO VERTICAL PANEL			0			
R9-9	SIDEWALK CLOSED	18	36	4.5	1	4.5			12	12			
R11-2-48	ROAD CLOSED	30	48	10	6	60	ATTACH TO BARRICADE			0			
R11-4	ROAD CLOSED TO THRU TRAFFIC	30	60	12.5	1	12.5	12		12	24			
SP-10R-30	BUSINESS ACCESS	24	30	5	1	5		12		12			
W1-4R-36	REVERSE CURVE RIGHT	36	36	9	1	9		12		12			
W1-4L-36	REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-36	DOUBLE REVERSE CURVE RIGHT	36	36	9	3	27		12		36			
W1-4BL-36	DOUBLE REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-48	DOUBLE REVERSE CURVE RIGHT	48	48	16	1	16	12		12	24			
W1-6-48	ARROW	24	48	8	6	48	ATTACH TO BARRICADE			0			
W3-5-36-35	SPEED LIMIT 35 AHEAD	36	36	9	2	18		12		24			
W3-5-36-45	SPEED LIMIT 45 AHEAD	36	36	9	2	18		12		24			
W3-5-48-65	SPEED LIMIT 65 AHEAD	48	48	16	2	32	12		12	48			
W4-2R-36	RIGHT LANE ENDS/MERGE LEFT	36	36	9	2	18		12		24			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48	48	16	2	32	12		12	48			
W4-2L-36	LEFT LANE ENDS/MERGE RIGHT	36	36	9	2	18		12		24			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48	48	16	2	32	12		12	48			
W4-3-36	ADDED LANE	36	36	9	2	18		12		24			
W4-3-48	ADDED LANE	48	48	16	1	16	12		12	24			
W9-2-36	LANE ENDS MERGE RIGHT	36	36	9	1	9		12		12			
W12-1	LANE SPLIT	36	36	9	1	9	ATTACH TO VERTICAL PANEL			0			
W20-1-36	ROAD WORK AHEAD	36	36	9	3	27		12		36			
W20-1-48-1/2	ROAD WORK 1/2 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1	ROAD WORK 1 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48	48	16	2	32	12		12	48			
W20-5L-36	LEFT LANE CLOSED AHEAD	36	36	9	2	18		12		24			
W20-5R-36	RIGHT LANE CLOSED AHEAD	36	36	9	1	9		12		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48	48	16	1	16	12		12	24			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5R-48-1	RIGHT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
SPECIALTY	BIKE LANE CLOSED AHEAD	36	36	9	1	9		12		12			
SPECIALTY	MULTI USE PATH CLOSED	48	48	16	1	16	12		12	24			
SPECIALTY	CUSTOM- LEFT, THRU, THRU/RIGHT	36	48	12	1	12		12		24			
SPECIALTY	RIGHT LANE RIGHT TURN ONLY	36	36	9	1	9		12		12			
PROJECT TOTAL						1099.75				1428	11	615	47
PROJECT USE						1100				1430	11	620	50

NOTE:

- 1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.
2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 2 - RESETTNG OF CONCRETE WALL 20' BARRIER

ITEM NUMBER 606719

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1675+34 TO 1694+11	1875	RT	1880	
PHASE 3						
PHASE 4						
PHASE 5						
PHASE 6						

UNIT 2 - RESETTNG OF CONCRETE WALL 10' BARRIER

ITEM NUMBER 606619

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	UN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	RIO BRAVO	2016+83 TO 2022+86	556	RT	560	
	I-25 NB	1663+24 TO 1669+84	660	LT	660	
	I-25 NB	1666+68 TO 1675+34	866	LT	870	
PHASE 3	I-25 NB	1675+34 TO 1752+56	7717	LT	7720	
PHASE 4						
PHASE 5						
PHASE 6						

UNIT 2 - VEHICULAR IMPACT ATTENUATOR

ITEM NUMBER 720110

PHASE	LOCATION	EACH	REMARKS
PHASE 0			
PHASE 1	RIO BRAVO WB	1	
	I-25 SB	1	
	I-25 NB	1	
PHASE 2	RIO BRAVO EB	1	
	I-25 NB	1	
PHASE 3	I-25 NB	1	
PHASE 4			
PHASE 5			
PHASE 6			



6			
5			
4			
3			
2			
1	ADDITION OF ITEM NUMBERS	5/7/2017	KL
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
UNIT 2 MOT QUANTITIES			

DESIGNED BY: AECOM

PHONE: (505) 855-7500

EMAIL:

UNIT 2 SIGN, POST AND DEVICE SCHEDULE - FOR CONTRACTORS INFORMATION ONLY

SIGN CODE	MESSAGE	SIZE (IN.)		SIGN AREA EA (SQ. FT.)	NO. OF SIGNS	TOTAL SIGN AREA (SQ. FT.)	POST LENGTHS (LIN. FT.)			POST TOTAL (LIN. FT.)	BARRICADE- TYPE III (EA)	VERTICAL PANEL- BK TO BK (EA)	CHANNELIZATION DEVICES TYPE DRUM (EA)
		L	W				LEFT	CENTER	RIGHT				
G20-2-36	END ROAD WORK	18	36	4.5	5	22.5		12		60			
R2-1-36-35	SPEED LIMIT 35	36	48	12	3	36	12		12	72			
R2-1-36-45	SPEED LIMIT 45	36	48	12	3	36	12		12	72			
R2-1-36-55	SPEED LIMIT 55	36	48	12	2	24	12		12	48			
R2-1-48-45	SPEED LIMIT 45	48	60	20	1	20	12		12	24			
R2-1-48-55	SPEED LIMIT 55	48	60	20	2	40	12		12	48			
R2-1-48-65	SPEED LIMIT 65	48	60	20	2	40	12		12	48			
R2-1-48-75	SPEED LIMIT 75	48	60	20	2	40	12		12	48			
R3-7-30	RIGHT LANE MUST TURN RIGHT	30	30	6.25	5	31.25		12		60			
R4-7A	KEEP RIGHT	24	30	5	10	50	ATTACH TO VERTICAL PANEL			0			
R9-9	SIDEWALK CLOSED	18	36	4.5	1	4.5		12		12			
R11-2-48	ROAD CLOSED	30	48	10	6	60	ATTACH TO BARRICADE			0			
R11-4	ROAD CLOSED TO THRU TRAFFIC	30	60	12.5	1	12.5	12		12	24			
SP-10R-30	BUSINESS ACCESS	24	30	5	1	5		12		12			
W1-4R-36	REVERSE CURVE RIGHT	36	36	9	1	9		12		12			
W1-4L-36	REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-36	DOUBLE REVERSE CURVE RIGHT	36	36	9	3	27		12		36			
W1-4BL-36	DOUBLE REVERSE CURVE LEFT	36	36	9	1	9		12		12			
W1-4BR-48	DOUBLE REVERSE CURVE RIGHT	48	48	16	1	16	12		12	24			
W1-6-48	ARROW	24	48	8	6	48	ATTACH TO BARRICADE			0			
W3-5-36-35	SPEED LIMIT 35 AHEAD	36	36	9	2	18		12		24			
W3-5-36-45	SPEED LIMIT 45 AHEAD	36	36	9	2	18		12		24			
W3-5-48-65	SPEED LIMIT 65 AHEAD	48	48	16	2	32	12		12	48			
W4-2R-36	RIGHT LANE ENDS/MERGE LEFT	36	36	9	2	18		12		24			
W4-2R-48	RIGHT LANE ENDS/MERGE LEFT	48	48	16	2	32	12		12	48			
W4-2L-36	LEFT LANE ENDS/MERGE RIGHT	36	36	9	2	18		12		24			
W4-2L-48	LEFT LANE ENDS/MERGE RIGHT	48	48	16	2	32	12		12	48			
W4-3-36	ADDED LANE	36	36	9	2	18		12		24			
W4-3-48	ADDED LANE	48	48	16	1	16	12		12	24			
W9-2-36	LANE ENDS MERGE RIGHT	36	36	9	1	9		12		12			
W12-1	LANE SPLIT	36	36	9	1	9	ATTACH TO VERTICAL PANEL			0			
W20-1-36	ROAD WORK AHEAD	36	36	9	3	27		12		36			
W20-1-48-1/2	ROAD WORK 1/2 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1	ROAD WORK 1 MILE	48	48	16	2	32	12		12	48			
W20-1-48-1.5	ROAD WORK 1.5 MILE	48	48	16	2	32	12		12	48			
W20-5L-36	LEFT LANE CLOSED AHEAD	36	36	9	2	18		12		24			
W20-5R-36	RIGHT LANE CLOSED AHEAD	36	36	9	1	9		12		12			
W20-5R-48	RIGHT LANE CLOSED AHEAD	48	48	16	1	16	12		12	24			
W20-5R-48-1/2	RIGHT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5R-48-1	RIGHT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1/2	LEFT LANE CLOSED 1/2 MILE	48	48	16	2	32	12		12	48			
W20-5L-48-1	LEFT LANE CLOSED 1 MILE	48	48	16	2	32	12		12	48			
SPECIALTY	BIKE LANE CLOSED AHEAD	36	36	9	1	9		12		12			
SPECIALTY	MULTI USE PATH CLOSED	48	48	16	1	16	12		12	24			
SPECIALTY	CUSTOM- LEFT, THRU, THRU/RIGHT	36	48	12	1	12	12		12	24			
SPECIALTY	RIGHT LANE RIGHT TURN ONLY	36	36	9	1	9		12		12			
PROJECT TOTAL						1099.75				1428	11	615	47
PROJECT USE						1100				1430	11	620	50

NOTE:

- 1) QUANTITIES SHOWN ARE BASED ON THE PHASE THAT WOULD REQUIRE THE MOST OF THAT DEVICE.
2) QUANTITIES ARE TO BE USED FOR MULTIPLE PHASES.

UNIT 2 - RESETTING OF CONCRETE WALL 20' BARRIER

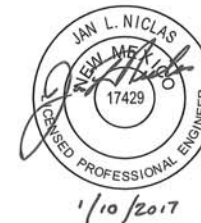
PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	LIN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	I-25 NB	1675+34 TO 1694+11	1875	RT	1880	
PHASE 3						
PHASE 4						
PHASE 5						
PHASE 6						

UNIT 2 - RESETTING OF CONCRETE WALL 10' BARRIER

PHASE	ALIGNMENT	STATION TO STATION	LENGTH	LOCATION	LIN. FT.	REMARKS
PHASE 0						
PHASE 1						
PHASE 2	RIO BRAVO	2016+83 TO 2022+36	556	RT	560	
	I-25 NB	1663+24 TO 1669+84	660	LT	660	
	I-25 NB	1666+68 TO 1675+34	866	LT	870	
PHASE 3	I-25 NB	1675+34 TO 1752+56	7717	LT	7720	
PHASE 4						
PHASE 5						
PHASE 6						

UNIT 2 - VEHICULAR IMPACT ATTENUATOR

PHASE	LOCATION	EACH	REMARKS
PHASE 0			
PHASE 1	RIO BRAVO WB	1	
	I-25 SB	1	
	I-25 NB	1	
PHASE 2	RIO BRAVO EB	1	
	I-25 NB	1	
PHASE 3	I-25 NB	1	
PHASE 4			
PHASE 5			
PHASE 6			



6			
5			
4			
3			
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
UNIT 2 MOT QUANTITIES			

UNIT 1 - CONCRETE WALL 10' BARRIER RETAINED

ITEM NUMBER 606610

	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LN. FT.
PHASE 0					
PHASE 1	I-25 NB	1675+34 TO 1694+89	2' LT TO 34' RT	1960	1960
	I-25 SB	1287+89 TO 1298+93	15' RT TO 14' RT	1106	1110
	RIO BRAVO	2050+00 TO 2053+00	27' RT TO 9' RT	296	300
	RIO BRAVO	2053+73 TO 2060+45	18' RT TO 10' LT	670	670
	I-25 NB	1700+58 TO 1706+57	225' RT TO 105' RT	631	640
	I-25 NB	1701+13 TO 1746+03	25' RT TO 27' LT	4490	4490
	I-25 SB	1304+15 TO 1354+47	16' RT TO 31' RT	5032	5040
	I-25 SB	1300+57 TO 1354+31	221' LT TO 18' LT	5427	5430
PHASE 2					
PHASE 3					
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL				19640	

UNIT 1 - CONCRETE WALL 20' BARRIER RETAINED

ITEM NUMBER 606710

	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LN. FT.
PHASE 0					
PHASE 1					
PHASE 2	I-25 NB	1675+34 TO 1694+11	6' LT TO 8' LT	1875	1880
PHASE 3	I-25 NB	1694+11 TO 1752+56	40' LT TO 121' LT	5837	5840
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL				7720	

UNIT 2 - CONCRETE WALL 10' BARRIER RETAINED

ITEM NUMBER 606610

	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LN. FT.
PHASE 0					
PHASE 1	I-25 SB	1258+30 TO 1275+00	24' RT TO 16' RT	1669	1670
	I-25 NB	1663+24 TO 1669+84	60' LT TO 44' LT	660	660
	I-25 NB	1664+62 TO 1675+34	6' LT TO 2' LT	1072	1080
	RIO BRAVO	2017+17 TO 2021+15	17' LT TO 2' RT	399	400
PHASE 2					
PHASE 3					
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL				3810	

UNIT 2 - CONCRETE WALL 20' BARRIER RETAINED

ITEM NUMBER 606710

	ALIGNMENT	STATION TO STATION	OFFSET TO OFFSET	LENGTH	LN. FT.
PHASE 0					
PHASE 1					
PHASE 2	I-25 NB	1664+12 TO 1675+34	2' LT TO 6' LT	1122	1140
PHASE 3					
PHASE 4					
PHASE 5					
PHASE 6					
TOTAL				1140	

UNIT 1 RETROREFLECTORIZED PAINTED MARKINGS 4"

ITEM NUMBER 704099

STATION TO STATION	SOLID	DASHED	LOCATION	LN. FT.
PHASE 1				
2047+40 TO 2060+23	5160	1115	RIO BRAVO	10878
1275+40 TO 1359+10	37185	17892	I-25	83316
PHASE 2				
2047+40 TO 2060+23	5344	1523	RIO BRAVO	11450
1675+34 TO 1759+10	48375	16332	I-25	104916
PHASE 3				
2047+40 TO 2060+23	5315	1166	RIO BRAVO	11213
1275+40 TO 1358+79	52381	12412	I-25	110968
PHASE 4				
2047+40 TO 2060+23	5134	1660	RIO BRAVO	11098
1675+34 TO 1760+48	46831	25560	I-25	106442
PHASE 5				
2047+40 TO 2061+73	4625	2169	RIO BRAVO	10335
1675+34 TO 1752+60	54745	41428	I-25	130204
PHASE 6				
2047+40 TO 2060+50	7647	2545	RIO BRAVO	16567
1678+12 TO 1747+62	39788	27411	I-25	93282
Unit 1 TOTAL				700667

NOTE:
1) QUANTITIES INCLUDE 2 COATS.
2) QUANTITIES DO NOT INCLUDE STOP BARS.
3) STATIONS ARE APPROXIMATES.

UNIT 2 RETROREFLECTORIZED PAINTED MARKINGS 4"

ITEM NUMBER 704099

STATION TO STATION	SOLID	DASHED	LOCATION	LN. FT.
PHASE 1				
2014+54 TO 2047+40	15399	6391	RIO BRAVO	33994
1658+25 TO 1675+34	7070	1844	I-25	15062
PHASE 1A				
2989+53 TO 3008+37	6451		BROADWAY	12902
PHASE 1B				
2990+63 TO 3007+91	6215		BROADWAY	12430
PHASE 1C				
2990+63 TO 3007+88	4337		BROADWAY	8674
PHASE 2				
2014+54 TO 2047+40	16015	3760	RIO BRAVO	33910
1658+25 TO 1675+34	6602	2762	I-25	14585
PHASE 2A				
2989+52 TO 3009+08	5330		BROADWAY	10660
PHASE 2B				
2991+17 TO 3008+35	4969		BROADWAY	9938
PHASE 2C				
2991+17 TO 3008+35	5717		BROADWAY	11434
PHASE 3				
2014+54 TO 2047+40	14336	4707	RIO BRAVO	31026
1655+61 TO 1675+34	9802	1504	I-25	20356
PHASE 3A				
2991+17 TO 3008+35	5578		BROADWAY	11156
PHASE 3B				
2991+17 TO 3008+35	4678		BROADWAY	9356
PHASE 3C				
2990+64 TO 3007+92	4786		BROADWAY	9572
PHASE 3D				
2990+64 TO 3007+92	5386		BROADWAY	10772
PHASE 4				
2026+56 TO 2047+40	9952	6553	RIO BRAVO	23181
1666+62 TO 1675+34	3536	2616	I-25	8380
PHASE 4A				
1666+62 TO 1675+34	3080	3283	I-25	7802
PHASE 5				
2031+54 TO 2047+40	7459	5212	RIO BRAVO	17524
1659+95 TO 1675+34	3488	4038	I-25	8995
PHASE 6				
2014+54 TO 2047+40	10590	6429	RIO BRAVO	24395
Unit 2 TOTAL				346102

5				
4				
3				
2	ADDITION OF RETAINED BARRIER SCHEDULE	3/21/17	KL	
1	ADDITION OF ITEM NUMBERS	3/7/17	KL	
NO.	DESCRIPTION	DATE	BY	

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE


MOT QUANTITIES




DESIGNED BY: AECOM

PHONE: (505) 855-7500

EMAIL:

UNIT 1 RETROREFLECTORIZED PAINTED MARKINGS 4"				
ITEM NUMBER 704099 				
LENGTH (FT.)				
STATION TO STATION	SOLID	DASHED	LOCATION	LIN. FT.
PHASE 1				
2047+40 to 2060+23	5160	1115	RIO BRAVO	10878
1275+40 to 1359+10	37485	17892	I-25	83316
PHASE 2				
2047+40 to 2060+23	5344	1523	RIO BRAVO	11450
1675+34 to 1759+10	48375	16332	I-25	104916
PHASE 3				
2047+40 to 2060+23	5315	1166	RIO BRAVO	11213
1275+40 to 1358+79	52381	12412	I-25	110998
PHASE 4				
2047+40 to 2060+23	5134	1660	RIO BRAVO	11098
1675+34 to 1760+48	46831	25560	I-25	106442
PHASE 5				
2047+40 to 2061+73	4625	2169	RIO BRAVO	10335
1675+34 to 1752+60	54745	41428	I-25	130204
PHASE 6				
2047+40 to 2060+50	7647	2545	RIO BRAVO	16567
1678+12 to 1747+62	39788	27411	I-25	93282
Unit 1 TOTAL				700667

NOTE:
1) QUANTITIES INCLUDE 2 COATS.
2) QUANTITIES DO NOT INCLUDE STOP BARS.
3) STATIONS ARE APPROXIMATES.

UNIT 2 RETROREFLECTORIZED PAINTED MARKINGS 4"				
ITEM NUMBER 704099 				
LENGTH (FT.)				
STATION TO STATION	SOLID	DASHED	LOCATION	LIN. FT.
PHASE 1				
2014+54 to 2047+40	15399	6391	RIO BRAVO	33994
1658+25 to 1675+34	7070	1844	I-25	15062
PHASE 1A				
2989+53 to 3008+37	6451		BROADWAY	12902
PHASE 1B				
2990+63 to 3007+91	6215		BROADWAY	12430
PHASE 1C				
2990+63 to 3007+88	4337		BROADWAY	8674
PHASE 2				
2014+54 to 2047+40	16015	3760	RIO BRAVO	33910
1658+25 to 1675+34	6602	2762	I-25	14865
PHASE 2A				
2989+52 to 3009+08	5330		BROADWAY	10660
PHASE 2B				
2991+17 to 3008+35	4969		BROADWAY	9938
PHASE 2C				
2991+17 to 3008+35	5717		BROADWAY	11434
PHASE 3				
2014+54 to 2047+40	14336	4707	RIO BRAVO	31026
1655+61 to 1675+34	9802	1504	I-25	20356
PHASE 3A				
2989+17 to 3008+35	5578		BROADWAY	11156
PHASE 3B				
2991+17 to 3008+35	4678		BROADWAY	9356
PHASE 3C				
2990+64 to 3007+92	4786		BROADWAY	9572
PHASE 3D				
2990+64 to 3007+92	5386		BROADWAY	10772
PHASE 4				
2026+56 to 2047+40	9952	6553	RIO BRAVO	23181
1666+62 to 1675+34	3536	2616	I-25	8380
PHASE 4A				
1666+62 to 1675+34	3080	3283	I-25	7802
PHASE 5				
2031+54 to 2047+40	7459	5212	RIO BRAVO	17524
1659+95 to 1675+34	3488	4038	I-25	8995
PHASE 6				
2014+54 to 2047+40	10590	6429	RIO BRAVO	24395
Unit 2 TOTAL				346102



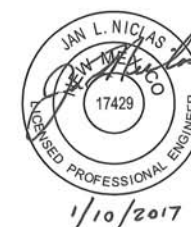
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NO.	ADDITION OF ITEM NUMBERS	5/7/2017	KL	
	DESCRIPTION	DATE	BY	
REVISIONS (OR CHANGE NOTICES)				
NEW MEXICO DEPARTMENT OF TRANSPORTATION I-25/RIO BRAVO INTERCHANGE				
MOT QUANTITIES				

DESIGNED BY: AECOM
PHONE: (855) 855-7500
EMAIL:

UNIT 1 RETROREFLECTORIZED PAINTED MARKINGS 4"				
STATION TO STATION	LENGTH (FT.)		LOCATION	LIN. FT.
	SOLID	DASHED		
PHASE 1				
2047+40 to 2060+23	5140	1115	RIO BRAVO	10878
1275+40 to 1359+10	37185	17892	I-25	83316
PHASE 2				
2047+40 to 2060+23	5344	1523	RIO BRAVO	11450
1675+34 to 1759+10	48375	16332	I-25	104916
PHASE 3				
2047+40 to 2060+23	5315	1166	RIO BRAVO	11215
1275+40 to 1358+79	52381	12412	I-25	110968
PHASE 4				
2047+40 to 2060+23	5134	1660	RIO BRAVO	11098
1675+34 to 1760+48	46831	25560	I-25	106442
PHASE 5				
2047+40 to 2061+73	4625	2169	RIO BRAVO	10335
1675+34 to 1752+60	54745	41428	I-25	130204
PHASE 6				
2047+40 to 2060+50	7647	2545	RIO BRAVO	16567
1678+12 to 1747+62	39788	27411	I-25	93282
Unit 1 TOTAL				700667

NOTE:
 1) QUANTITIES INCLUDE 2 COATS.
 2) QUANTITIES DO NOT INCLUDE STOP BARS.
 3) STATIONS ARE APPROXIMATES.

UNIT 2 RETROREFLECTORIZED PAINTED MARKINGS 4"				
STATION TO STATION	LENGTH (FT.)		LOCATION	LIN. FT.
	SOLID	DASHED		
PHASE 1				
2014+54 to 2047+40	15199	6391	RIO BRAVO	33994
1658+25 to 1675+34	7070	1844	I-25	15062
PHASE 1A				
2989+53 to 3008+37	6451		BROADWAY	12902
PHASE 1B				
2990+63 to 3007+91	6215		BROADWAY	12430
PHASE 1C				
2990+63 to 3007+88	4337		BROADWAY	8674
PHASE 2				
2014+54 to 2047+40	16615	3760	RIO BRAVO	33910
1658+25 to 1675+34	6602	2762	I-25	14585
PHASE 2A				
2989+52 to 3009+08	5330		BROADWAY	10660
PHASE 2B				
2991+17 to 3008+35	4969		BROADWAY	9938
PHASE 2C				
2991+17 to 3008+35	5717		BROADWAY	11434
PHASE 3				
2014+54 to 2047+40	16536	4707	RIO BRAVO	31026
1655+61 to 1675+34	9802	1504	I-25	20356
PHASE 3A				
2991+17 to 3008+35	5578		BROADWAY	11156
PHASE 3B				
2991+17 to 3008+35	4678		BROADWAY	9356
PHASE 3C				
2990+64 to 3007+92	4786		BROADWAY	9572
PHASE 3D				
2990+64 to 3007+92	5386		BROADWAY	10772
PHASE 4				
2026+56 to 2047+40	9952	6553	RIO BRAVO	23181
1666+62 to 1675+34	3536	2616	I-25	8380
PHASE 4A				
1666+62 to 1675+34	3080	3283	I-25	7802
PHASE 5				
2031+54 to 2047+40	7459	5212	RIO BRAVO	12524
1659+95 to 1675+34	3488	4038	I-25	8995
PHASE 6				
2014+54 to 2047+40	10590	6429	RIO BRAVO	24395
Unit 2 TOTAL				346102



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT QUANTITIES			

EMAIL:

PHONE: (505) 865-7250

DESIGNED BY: AECOM

Design File: p:\617478-PWINT.aecom\line.local\AECOM_0501_na\Documents\60393326-125-Rio Bravo\A300280\Plans\6_Sheets\A30028011501C.dgn

TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
8801 American Parkway NE, Suite 300

AECOM DESIGNED BY: AECOM

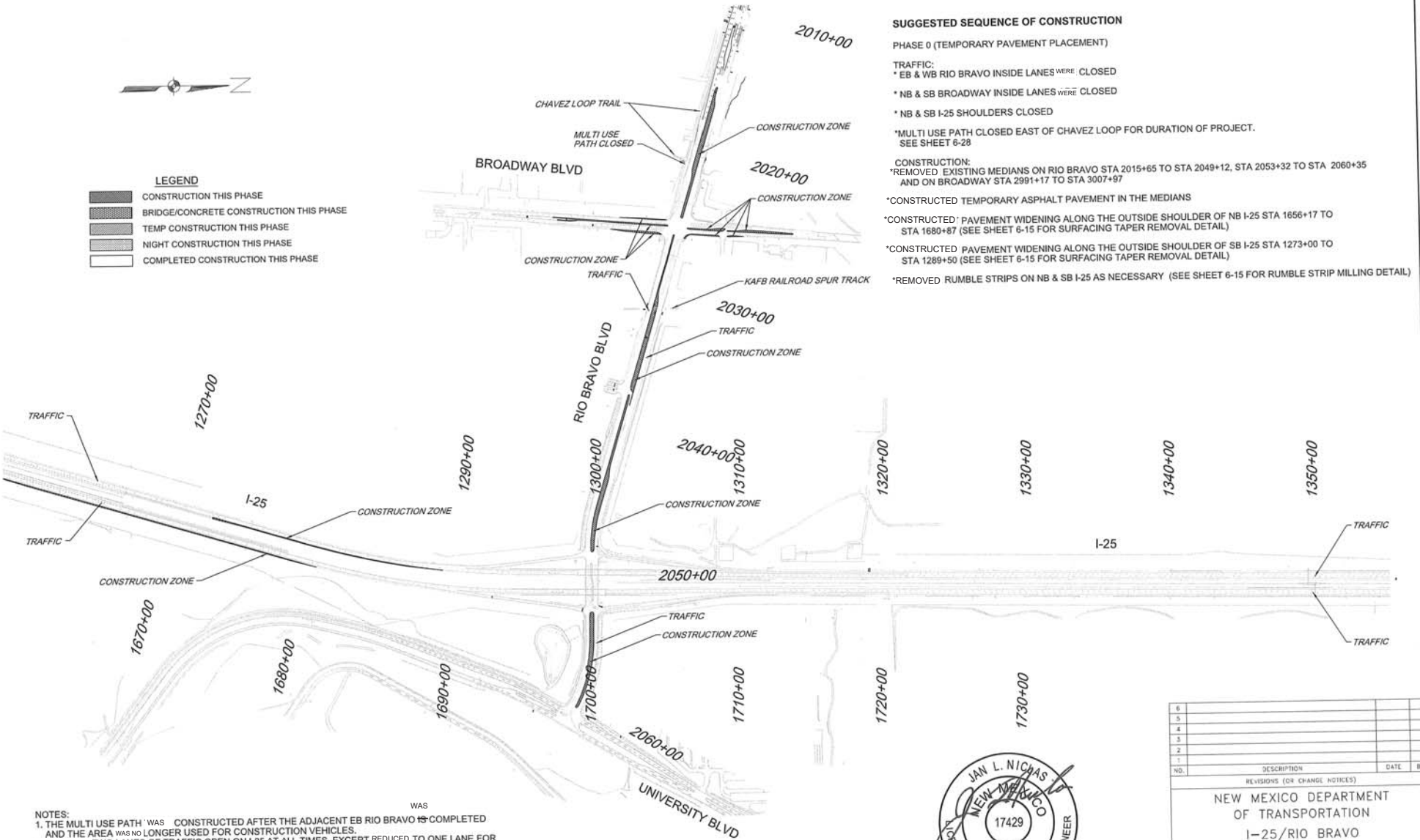
NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1:

SHEET NO. 6 - 01C



LEGEND

	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE



SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 0 (TEMPORARY PAVEMENT PLACEMENT)

TRAFFIC:

* EB & WB RIO BRAVO INSIDE LANES WERE CLOSED

* NB & SB BROADWAY INSIDE LANES WERE CLOSED

* NB & SB I-25 SHOULDERS CLOSED

* MULTI USE PATH CLOSED EAST OF CHAVEZ LOOP FOR DURATION OF PROJECT. SEE SHEET 6-28

CONSTRUCTION:

* REMOVED EXISTING MEDIANS ON RIO BRAVO STA 2015+65 TO STA 2049+12, STA 2053+32 TO STA 2060+35 AND ON BROADWAY STA 2991+17 TO STA 3007+97

* CONSTRUCTED TEMPORARY ASPHALT PAVEMENT IN THE MEDIANS

* CONSTRUCTED PAVEMENT WIDENING ALONG THE OUTSIDE SHOULDER OF NB I-25 STA 1656+17 TO STA 1680+87 (SEE SHEET 6-15 FOR SURFACING TAPER REMOVAL DETAIL)

* CONSTRUCTED PAVEMENT WIDENING ALONG THE OUTSIDE SHOULDER OF SB I-25 STA 1273+00 TO STA 1289+50 (SEE SHEET 6-15 FOR SURFACING TAPER REMOVAL DETAIL)

* REMOVED RUMBLE STRIPS ON NB & SB I-25 AS NECESSARY (SEE SHEET 6-15 FOR RUMBLE STRIP MILLING DETAIL)

NOTES:

1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPIU INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN AT THE SOUTH DIVERSION CHANNEL AND UNDER I-25. RIO BRAVO WAS CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
3. PEDESTRIAN AND BICYCLE ACCESS WAS NOT BE ALLOWED IN THE CONSTRUCTION ZONE. "NO PEDESTRIAN" SIGNS (R5-10A) WERE BE PLACED AT THE DIRECTION OF THE ENGINEER.

PHASE 0



1/10/2017

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 1 OF 11)			
PHASE 0			

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 1

- TRAFFIC: WAS
- * NB I-25 TRAFFIC WAS SHIFTED TO THE RIGHT LANE AND SHOULDER
 - * NB ENTRANCE & EXIT RAMP TRAFFIC REMAINS ON EXISTING RAMPS
 - * SB I-25 REMAINED ON EXISTING LANES NORTH OF RIO BRAVO AND IS SHIFTED TO THE INSIDE LANE & SHOULDER SOUTH OF RIO BRAVO AND THEN BACK TO THE OUTSIDE LANE AND SHOULDER SOUTH OF STA 1276+25
 - * SB ENTRANCE & EXIT RAMP TRAFFIC REMAINED ON EXISTING RAMPS
 - * EB RIO BRAVO TRAFFIC WAS SHIFTED TO THE SOUTH LANES WEST OF BROADWAY AND SHIFTED BACK WEST OF THE SOUTH DIVERSION CHANNEL
 - * WB RIO BRAVO TRAFFIC WAS SHIFTED TO THE TEMPORARY PAVEMENT IN THE MEDIAN AREA
- CONSTRUCTION:
- * CONSTRUCTED NB I-25 INSIDE SHOULDER AND TWO LANES FROM THE SOUTH TIE-IN TO STA 1725+95 INCLUDING THE NB EXIT RAMP STRUCTURE UNDER NB I-25 AND THE INSIDE SHOULDER AND THREE LANES OF THE NB I-25 STRUCTURE OVER RIO BRAVO
 - * CONSTRUCTED THE WB TO NB ENTRANCE RAMP FROM RIO BRAVO TO STA 413+50
 - * CONSTRUCTED THE SB I-25 CROSSOVER NORTH OF RIO BRAVO
 - * CONSTRUCTED THE SB I-25 CROSSOVER SOUTH OF RIO BRAVO
 - * CONSTRUCTED WB RIO BRAVO OUTSIDE THREE LANES FROM THE WEST TIE-IN TO THE SB EXIT RAMP AND FROM THE NB ENTRANCE RAMP TO UNIVERSITY INCLUDING THE NORTH HALF OF THE STRUCTURE OVER THE SOUTH DIVERSION CHANNEL
 - * CONSTRUCTED THE DRIVEWAY ON NORTH SIDE OF RIO BRAVO STA 2021+90 IN HALVES TO MAINTAIN ACCESS TO VALERO CONVENIENCE STORE
 - * CONSTRUCTED RR POND
 - * EARTHWORK OPERATIONS OF UNCLASSIFIED EXCAVATION IN SOUTHEAST QUADRANT
- STRUCTURES:
- * CONSTRUCTED EAST HALF NB EXIT BRIDGE WITH TEMPORARY SHORING
 - * CONSTRUCTED NB I-25 BRIDGE OVER RIO BRAVO
 - * CONSTRUCTED MIDDLE PORTION OF RETAINING WALL D3-2
 - * CONSTRUCTED RETAINING WALL D4
 - * CONSTRUCTED NORTH HALF OF SOUTH DIVERSION CHANNEL BRIDGE

PHASE 1

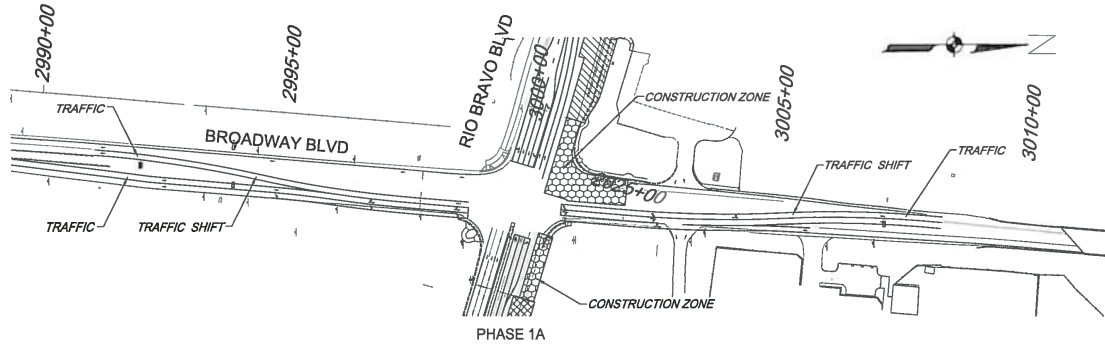


1/10/2017

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 2 OF 11)			
PHASE 1			

NOTES:

1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS LONGER USED FOR CONSTRUCTION VEHICLES.
2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPIR INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
3. PEDESTRIAN AND BICYCLE ACCESS WAS NOT BE ALLOWED IN THE CONSTRUCTION ZONE. "NO PEDESTRIAN" SIGNS (R5-10A) WERE BE PLACED AT THE DIRECTION OF THE ENGINEER.



SUGGESTED SEQUENCE OF CONSTRUCTION RIO BRAVO/BROADWAY INTERSECTION

FOR RIO BRAVO TRAFFIC SEE PHASE 1

PHASE 1A

- TRAFFIC:
* NB BROADWAY TRAFFIC ON EXISTING OUTSIDE LANE AND TEMPORARY PAVEMENT WITH SHARED THRU/LEFT & RIGHT
- △ SB BROADWAY TRAFFIC SHIFTED TO EXISTING INSIDE NB LANES WITH SHARED THRU/LEFT & RIGHT
- CONSTRUCTION:
* CONSTRUCT NW CORNER OUTSIDE LANES AND SHOULDERS
- * CONSTRUCT NE RIO BRAVO OUTSIDE LANE AND RIGHT TURN LANE STA 2021+75 TO STA 2026+40

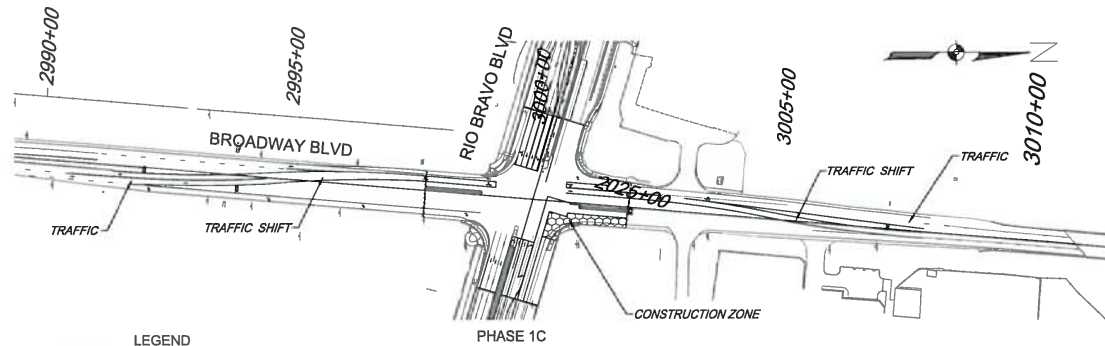
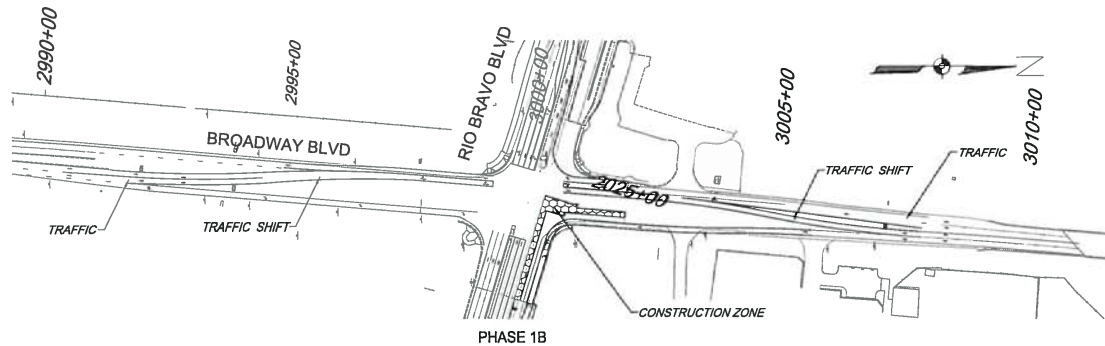
PHASE 1B

- TRAFFIC:
* NB BROADWAY THRU TRAFFIC SHIFTED TO EXISTING INSIDE SB LANES WITH THRU/LEFT
- * NB BROADWAY RIGHT ON EXISTING RIGHT LANE
- △ SB BROADWAY TRAFFIC SHIFTED TO RIGHT WITH SB SHARED THRU/LEFT & RIGHT TURN LANE
- * RIO BRAVO WB TO NB SHIFTED TO NEW RIGHT TURN LANE
- CONSTRUCTION:
* CONSTRUCT NE CORNER BROADWAY INSIDE THRU LANE
- * CONSTRUCT NE CORNER RIO BRAVO INSIDE THRU LANE

PHASE 1C

- TRAFFIC:
* BROADWAY TRAFFIC REMAINS ON PHASE 1B CONFIGURATION
- * RIO BRAVO WB RIGHT TURN SHIFTS TO LANE ADJACENT TO WB THRU TRAFFIC
- CONSTRUCTION:
* CONSTRUCT REMAINDER OF NE CORNER OUTSIDE LANES AND SHOULDERS

- NOTES:
1. MAINTAIN LOCAL BUSINESS ACCESS AT ALL TIMES.
2. PEDESTRIAN AND BICYCLE ACCESS WILL NOT BE ALLOWED IN THE ALLOWED IN THE CONSTRUCTION ZONE.
* "NO PEDESTRIAN SIGNS" (R5-10A) OR "SIDEWALK CLOSED" (R9-9) SHALL BE PLACED AT THE DIRECTION OF THE ENGINEER.



LEGEND	
	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE



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1	SPELLING CORRECTION	3/7/2017	DJB
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO			
INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 3 OF 11)			
PHASE 1A, B & C			

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SUGGESTED SEQUENCE OF CONSTRUCTION RIO BRAVO/BROADWAY INTERSECTION

FOR RIO BRAVO TRAFFIC SEE PHASE 1

PHASE 1A

- TRAFFIC:**
- * NB BROADWAY TRAFFIC ON EXISTING OUTSIDE LANE AND TEMPORARY PAVEMENT WITH SHARED THRU/LEFT & RIGHT
 - * SB RIO BROADWAY TRAFFIC SHIFTED TO EXISTING INSIDE NB LANES WITH SHARED THRU/LEFT & RIGHT
- CONSTRUCTION:**
- * CONSTRUCT NW CORNER OUTSIDE LANES AND SHOULDERS
 - * CONSTRUCT NE RIO BRAVO OUTSIDE LANE AND RIGHT TURN LANE STA 2021+75 TO STA 2026+40

PHASE 1B

- TRAFFIC:**
- * NB BROADWAY THRU TRAFFIC SHIFTED TO EXISTING INSIDE SB LANES WITH THRU/LEFT
 - * NB BROADWAY RIGHT ON EXISTING RIGHT LANE
 - * SB RIO BROADWAY TRAFFIC SHIFTED TO RIGHT WITH SB SHARED THRU/LEFT & RIGHT TURN LANE
 - * RIO BRAVO WB TO NB SHIFTED TO NEW RIGHT TURN LANE

- CONSTRUCTION:**
- * CONSTRUCT NE CORNER BROADWAY INSIDE THRU LANE
 - * CONSTRUCT NE CORNER RIO BRAVO INSIDE THRU LANE

PHASE 1C

- TRAFFIC:**
- * BROADWAY TRAFFIC REMAINS ON PHASE 1B CONFIGURATION
 - * RIO BRAVO WB RIGHT TURN SHIFTS TO LANE ADJACENT TO WB THRU TRAFFIC
- CONSTRUCTION:**
- * CONSTRUCT REMAINDER OF NE CORNER OUTSIDE LANES AND SHOULDERS

NOTES:

1. MAINTAIN LOCAL BUSINESS ACCESS AT ALL TIMES.
 2. PEDESTRIAN AND BICYCLE ACCESS WILL NOT BE ALLOWED IN THE ALLOWED IN THE CONSTRUCTION ZONE.
- *NO PEDESTRIAN SIGNS* (R5-10A) OR *SIDEWALK CLOSED* (R9-9) SHALL BE PLACED AT THE DIRECTION OF THE ENGINEER.

LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE



NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
SEQUENCE OF
CONSTRUCTION OVERVIEW
(SHEET 3 OF 11)
PHASE 1A, B & C

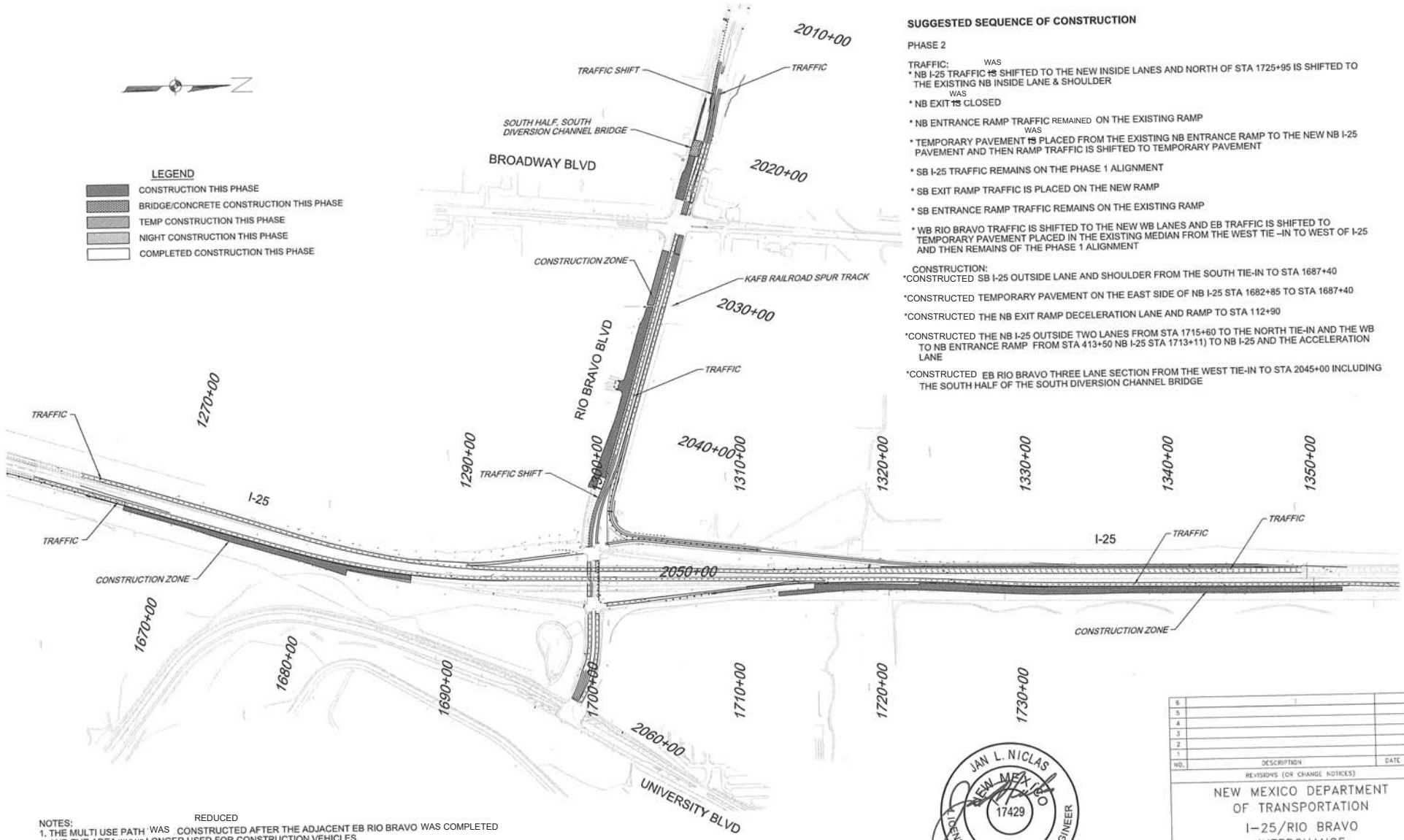
LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 2

- TRAFFIC: WAS
- * NB I-25 TRAFFIC IS SHIFTED TO THE NEW INSIDE LANES AND NORTH OF STA 1725+95 IS SHIFTED TO THE EXISTING NB INSIDE LANE & SHOULDER
 - WAS
 - * NB EXIT IS CLOSED
 - * NB ENTRANCE RAMP TRAFFIC REMAINED ON THE EXISTING RAMP
 - WAS
 - * TEMPORARY PAVEMENT IS PLACED FROM THE EXISTING NB ENTRANCE RAMP TO THE NEW NB I-25 PAVEMENT AND THEN RAMP TRAFFIC IS SHIFTED TO TEMPORARY PAVEMENT
 - * SB I-25 TRAFFIC REMAINS ON THE PHASE 1 ALIGNMENT
 - * SB EXIT RAMP TRAFFIC IS PLACED ON THE NEW RAMP
 - * SB ENTRANCE RAMP TRAFFIC REMAINS ON THE EXISTING RAMP
 - * WB RIO BRAVO TRAFFIC IS SHIFTED TO THE NEW WB LANES AND EB TRAFFIC IS SHIFTED TO TEMPORARY PAVEMENT PLACED IN THE EXISTING MEDIAN FROM THE WEST TIE-IN TO WEST OF I-25 AND THEN REMAINS OF THE PHASE 1 ALIGNMENT
- CONSTRUCTION:
- * CONSTRUCTED SB I-25 OUTSIDE LANE AND SHOULDER FROM THE SOUTH TIE-IN TO STA 1687+40
 - * CONSTRUCTED TEMPORARY PAVEMENT ON THE EAST SIDE OF NB I-25 STA 1682+85 TO STA 1687+40
 - * CONSTRUCTED THE NB EXIT RAMP DECELERATION LANE AND RAMP TO STA 112+90
 - * CONSTRUCTED THE NB I-25 OUTSIDE TWO LANES FROM STA 1715+60 TO THE NORTH TIE-IN AND THE WB TO NB ENTRANCE RAMP FROM STA 413+50 NB I-25 STA 1713+11 TO NB I-25 AND THE ACCELERATION LANE
 - * CONSTRUCTED EB RIO BRAVO THREE LANE SECTION FROM THE WEST TIE-IN TO STA 2045+00 INCLUDING THE SOUTH HALF OF THE SOUTH DIVERSION CHANNEL BRIDGE



- NOTES:**
1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
 2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPUI INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
 3. PEDESTRIAN AND BICYCLE ACCESS WAS NOT BE ALLOWED IN THE CONSTRUCTION ZONE. "NO PEDESTRIAN" SIGNS (R5-10A) WERE PLACED AT THE DIRECTION OF THE ENGINEER.



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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

SEQUENCE OF CONSTRUCTION OVERVIEW (SHEET 4 OF 11)

PHASE 2

PHASE 2



- ### LEGEND

FOR RIO BRAVO TRAFFIC SEE PHASE 2

PHASE 2A

TRAFFIC:

TRAFFIC:
 * BROADWAY TRAFFIC REMAINED ON PHASE 1B CONFIGURATION WITH NB RIGHT TURN LANE ADJACENT TO SHARED THRU/LEFT
 * NB BROADWAY RIGHT TURN LANE USED OUTSIDE LANE OF NEW PHASE 1 LANES

CONSTRUCTION:

*CONSTRUCTED SE CORNER WITH ALL EB RIO BRAVO LANES AND ALL NB BROADWAY LANES

PHASE 2B

TRAFFIC:

- * NB BROADWAY TRAFFIC SHIFTED TO NEW NB OUTSIDE LANE AND RIGHT TURN LANE WITH SHARED THRU/LEFT LANE AND RIGHT TURN LANE

SHARED THRU/LEFT LANE AND RIGHT TURN LANE

 * SB BROADWAY TRAFFIC SHIFTED TO NEW NB BROADWAY INSIDE THRU LANE

* SB BROADWAY RIGHT TURN LANE TRAFFIC KEPT ON NEW SB RIGHT TURN LANE

CONSTRUCTION:

*CONSTRUCTED' ALL BROADWAY SB LANES AT SW CORNER

*CONSTRUCTED RIO BRAVO EB RT TURN LANE AND TWO OUTSIDE THRU LANES

PHASE 2C

TRAFFIC:

* BROADWAY TRAFFIC REMAINED ON PHASE 2B CONFIGURATION

* EB RIO BRAVO RIGHT TURN SHIFTED TO NEW RIGHT TURN LANE AND USES NEW BROADWAY SB LANE

CONSTRUCTION:

CONSTRUCTED RIO BRAVO SW CORNER INSIDE THRU LANE

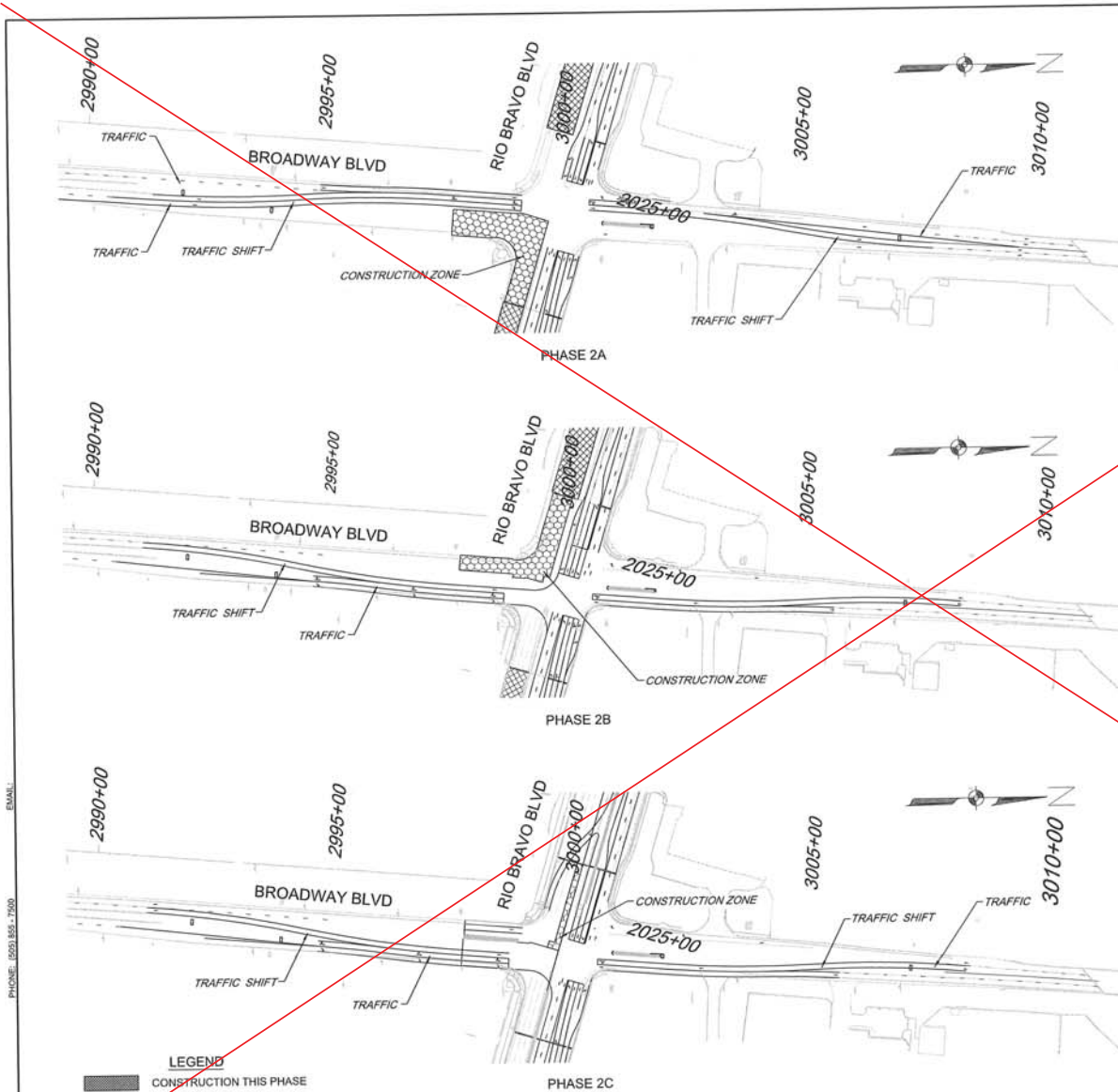
NOTES:

- 2 PEDESTRIAN AND BICYCLE ACCESS WAS NOT ~~BE~~ ALLOWED IN THE CONSTRUCTION ZONE.
"NO PEDESTRIAN SIGNS" (R5-10A) OR "SIDEWALK CLOSED" (R9-9) WAS ~~BE~~ PLACED AT
THE DIRECTION OF THE ENGINEER.



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1	SPELLING CORRECTION	3/7/2017	
NO.	DESCRIPTION	DATE	
REVISIONS (OR CHANGE NOTICES)			

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
SEQUENCE OF
CONSTRUCTION OVERVIEW
(SHEET 5 OF 11)
PHASE 2A, B & C



SUGGESTED SEQUENCE OF CONSTRUCTION RIO BRAVO/BROADWAY INTERSECTION

FOR RIO BRAVO TRAFFIC SEE PHASE 2

PHASE 2A

- TRAFFIC:
- * BROADWAY TRAFFIC REMAINS ON PHASE 1B CONFIGURATION WITH NB RIGHT TURN LANE ADJACENT TO SHARED THRU/LEFT
 - * NB BROADWAY RIGHT TURN LANE USES OUTSIDE LANE OF NEW PHASE 1 LANES

- CONSTRUCTION:
- * CONSTRUCT SE CORNER WITH ALL EB RIO BRAVO LANES AND ALL NB BROADWAY LANES

PHASE 2B

- TRAFFIC:
- * NB BROADWAY TRAFFIC SHIFTED TO NEW NB OUTSIDE LANE AND RIGHT TURN LANE WITH SHARED THRU/LEFT LANE AND RIGHT TURN LANE
 - * SB RIO BROADWAY TRAFFIC SHIFTED TO NEW NB BROADWAY INSIDE THRU LANE
 - * SB BROADWAY RIGHT TURN LANE TRAFFIC KEPT ON NEW SB RIGHT TURN LANE

- CONSTRUCTION:
- * CONSTRUCT ALL BROADWAY SB LANES AT SW CORNER
 - * CONSTRUCT RIO BRAVO EB RT TURN LANE AND TWO OUTSIDE THRU LANES

PHASE 2C

- TRAFFIC:
- * BROADWAY TRAFFIC REMAINS ON PHASE 2B CONFIGURATION
 - * EB RIO BRAVO RIGHT TURN SHIFTED TO NEW RIGHT TURN LANE AND USES NEW BROADWAY SB LANE

- CONSTRUCTION:
- * CONSTRUCT RIO BRAVO SW CORNER INSIDE THRU LANE

- NOTES:
1. MAINTAIN LOCAL BUSINESS ACCESS AT ALL TIMES.
 2. PEDESTRIAN AND BICYCLE ACCESS WILL NOT BE ALLOWED IN THE CONSTRUCTION ZONE. "NO PEDESTRIAN SIGNS" (R5-10A) OR "SIDEWALK CLOSED" (R9-9) SHALL BE PLACED AT THE DIRECTION OF THE ENGINEER.



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 5 OF 11)			
PHASE 2A, B & C			

LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 3

TRAFFIC: WAS

* NB I-25 TRAFFIC IS SHIFTED TO THE NEW OUTSIDE LANE AND SHOULDER

* NB EXIT RAMP REMAINS CLOSED

* NB ENTRANCE RAMP TRAFFIC REMAINED ON EXISTING

* SB I-25 TRAFFIC IS SHIFTED TO THE NEW INSIDE LANE AND SHOULDER OF THE NEW NB LANES USING TEMPORARY CROSS OVERS NORTH AND SOUTH OF THE PROJECT LIMITS

* SB EXIT RAMP TRAFFIC REMAINED ON THE NEW RAMP

* SB ENTRANCE RAMP TRAFFIC WAS CLOSED

* WB RIO BRAVO TRAFFIC REMAINED ON THE PHASE 2 ALIGNMENT

* EB RIO BRAVO TRAFFIC WAS PLACED ON THE NEW EB LANES FROM THE WEST TIE-IN TO THE EXISTING LANES WEST OF I-25 TO UNIVERSITY

CONSTRUCTION:

* CONSTRUCTED THE REMAINDER OF THE SB I-25 FROM THE NORTH TIE-IN TO THE SOUTH TIE-IN EXCEPT THE INSIDE SHOULDER FROM STA 1346+20 TO THE NORTH TIE-IN, INCLUDING THE NB EXIT RAMP STRUCTURE UNDER I-25 AND THE SB I-25 BRIDGE OVER RIO BRAVO

* CONSTRUCTED THE SB ENTRANCE RAMP ACCELERATION LANE TO THE ML GORE

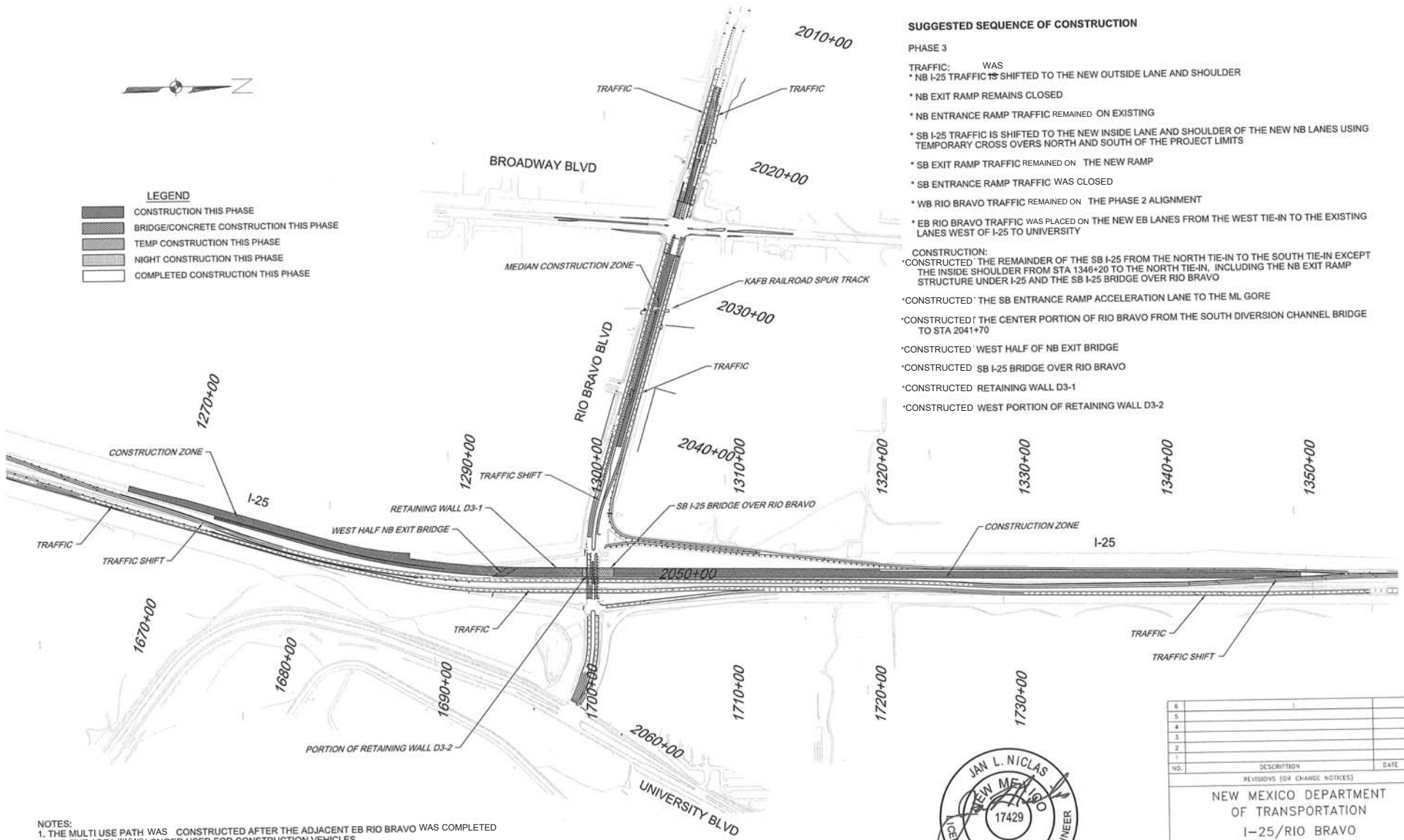
* CONSTRUCTED THE CENTER PORTION OF RIO BRAVO FROM THE SOUTH DIVERSION CHANNEL BRIDGE TO STA 2041+70

* CONSTRUCTED WEST HALF OF NB EXIT BRIDGE

* CONSTRUCTED SB I-25 BRIDGE OVER RIO BRAVO

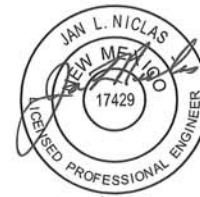
* CONSTRUCTED RETAINING WALL D3-1

* CONSTRUCTED WEST PORTION OF RETAINING WALL D3-2



- NOTES:**
1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
 2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCE TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPIU INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS BE CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
 3. PEDESTRIAN AND BICYCLE ACCESS WERE NOT ALLOWED IN THE CONSTRUCTION ZONE. "NO PEDESTRIAN" SIGNS (R5-10A) WERE PLACED AT THE DIRECTION OF THE ENGINEER.

PHASE 3



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 6 OF 11)			
PHASE 3			

**SUGGESTED SEQUENCE OF CONSTRUCTION
RIO BRAVO/BROADWAY INTERSECTION**

PHASE 3A

TRAFFIC:

- * BROADWAY TRAFFIC REMAINED ON PHASE 2C CONFIGURATION
- * EB RIO BRAVO TRAFFICUSED NEW RIGHT TURN LANE AND TWO OUTSIDE LANES
- * EB RIO BRAVO LEFT TURN LANE SHIFTED TO WB LANES ADJACENT TO WB THRU LANES
- * WB RIO BRAVO ON NEW WB LANES

CONSTRUCTION:

- * CONSTRUCTED CENTER LEFT TURN LANES OF RIO BRAVO ON THE WEST SIDE OF THE INTERSECTION

PHASE 3B

TRAFFIC:

- * BROADWAY TRAFFIC REMAINED ON PHASE 3A CONFIGURATION
- * WB RIO BRAVO TRAFFIC REMAINED ON PHASE 3A CONFIGURATION
- * EB RIO BRAVO TRAFFIC REMAINED ON PHASE 3A CONFIGURATION WITH LEFT TURN LANE ADJACENT TO EB THRU LANES

CONSTRUCTION:

- * CONSTRUCTED REMAINDER OF WB RIO BRAVO LANES ON THE WEST SIDE OF THE INTERSECTION

PHASE 3C

TRAFFIC:

- * SB BROADWAY TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO OUTSIDE TWO LANES OF NEW SB BROADWAY WITH A SHARED THRU/LEFT AND RIGHT TURN LANE
- * NB BROADWAY THRU/LEFT TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO THE NEW CENTER SB BROADWAY LANE
- * NB BROADWAY RIGHT TURN LANE USED NEW RIGHT TURN LANE
- * WB RIO BRAVO TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO NEW WB OUTSIDE LANES WITH A LEFT, TWO THRU AND A RIGHT
- * EB RIO BRAVO TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO NEW EB OUTSIDE LANES WITH A LEFT, TWO THRU AND A RIGHT

CONSTRUCTION:

- * CONSTRUCTED TWO CENTER LEFT TURN LANES OF RIO BRAVO IN THE EAST SIDE OF THE INTERSECTION

PHASE 3D

TRAFFIC:

- * BROADWAY TRAFFIC REMAINED ON PHASE 3C CONFIGURATION
- * WB RIO BRAVO TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO NEW WB OUTSIDE LANES WITH TWO THRU AND A RIGHT
- * WB RIO BRAVO LEFT TURN TRAFFIC ~~WAS~~ ^{WAS} SHIFTED TO THE NEW INSIDE LEFT TURN
- * EB RIO BRAVO TRAFFIC REMAINED ON PHASE 3C CONFIGURATION

CONSTRUCTION:

- * CONSTRUCTED THE REMAINDER OF THE WB LEFT TURN AND THRU LANES OF RIO BRAVO IN THE EAST SIDE OF THE INTERSECTION

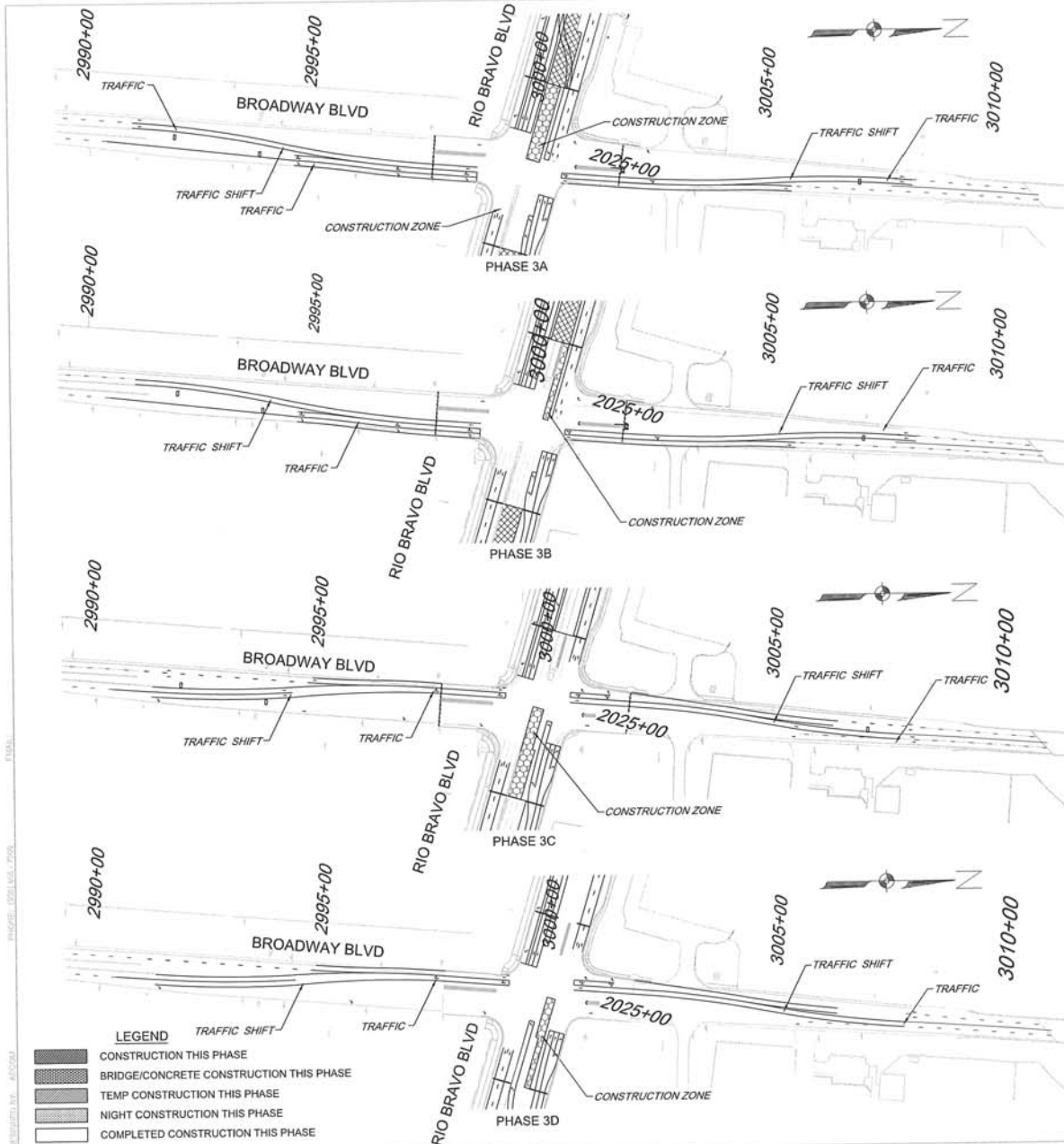
NOTES:

- 1 MAINTAINED LOCAL BUSINESS ACCESS AT ALL TIMES.
- 2 PEDESTRIAN AND BICYCLE ACCESS WAS NOT ~~BE~~ ALLOWED IN THE CONSTRUCTION ZONE.
- "NO PEDESTRIAN SIGNS" (R5-10A) OR "SIDEWALK CLOSED" (R9-9) WERE ~~BE~~ PLACED AT THE DIRECTION OF THE ENGINEER.



1/10/2017

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
SEQUENCE OF
CONSTRUCTION OVERVIEW
(SHEET 7 OF 11)
PHASE 3A,B,C & D



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 4

TRAFFIC:

* NB I-25 TRAFFIC WAS SHIFTED TO THE NEW INSIDE LANE AND SHOULDER FROM THE SOUTH TIE-IN TO STA 1715+60, THEN CROSSED OVER TO THE NEW OUTSIDE LANE AND SHOULDER AT STA 1725+95

* NB EXIT RAMP TRAFFIC REMAINED CLOSED

* NB ENTRANCE RAMP TRAFFIC WAS SHIFTED TO TEMPORARY PAVEMENT FROM RIO BRAVO TO THE NEW WB TO NB RAMP

* SB I-25 TRAFFIC WAS SHIFTED TO THE NEW INSIDE LANE AND SHOULDER AT THE NORTH TIE-IN, THEN SHIFTED ONTO THE NEW SB LANES FOR COMPLETION OF THE INSIDE SHOULDER CONSTRUCTION

* SB EXIT RAMP TRAFFIC IS ON THE NEW ALIGNMENT

* SB ENTRANCE RAMP REMAINED CLOSED

* EB RIO BRAVO TRAFFIC REMAINED ON THE PHASE 3 ALIGNMENT

* WB RIO BRAVO TRAFFIC IS ON THE NEW ALIGNMENT FROM THE WEST TIE-IN TO STA 2039+00 AND WAS SHIFTED TO THE EXISTING ALIGNMENT TO UNIVERSITY

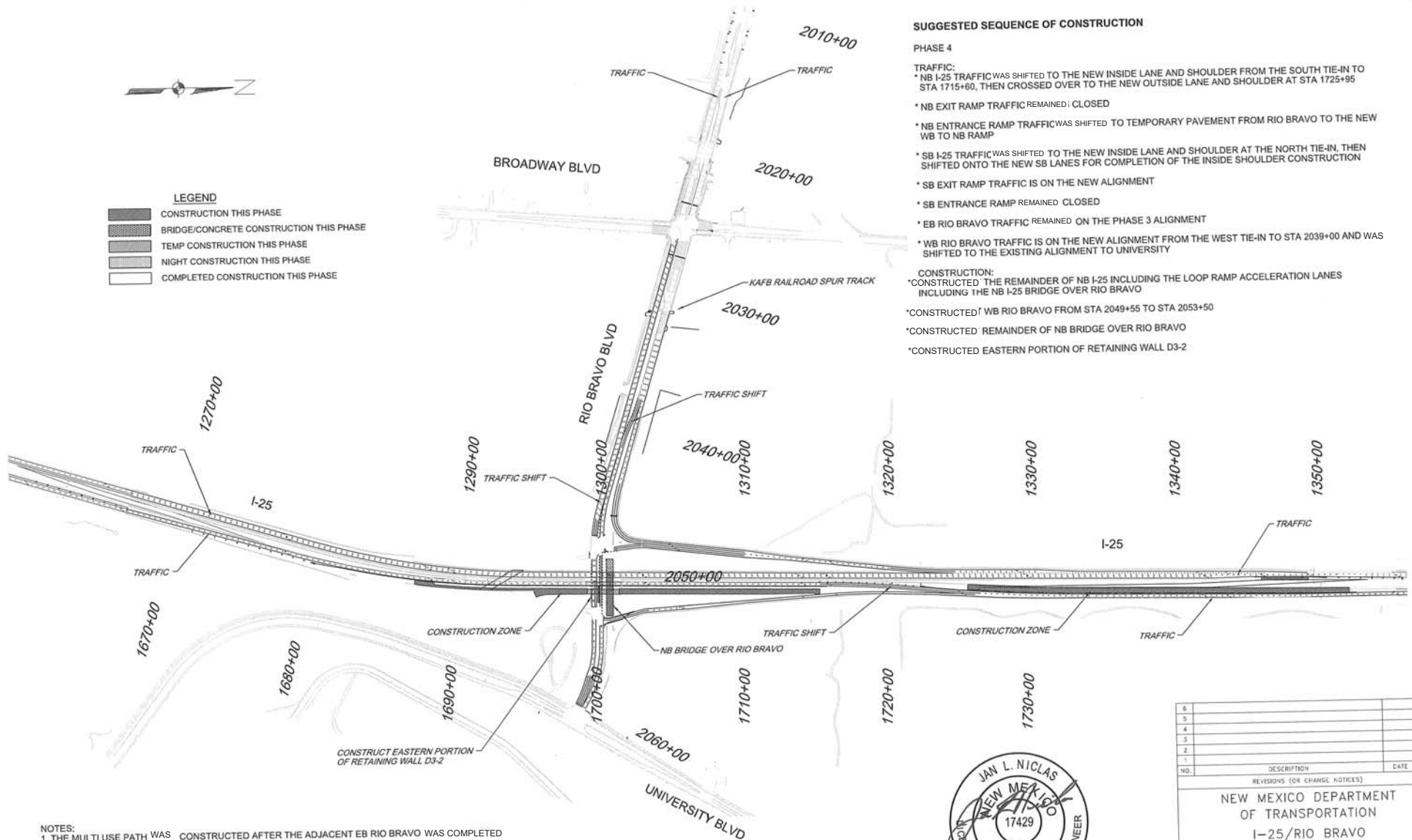
CONSTRUCTION:

* CONSTRUCTED THE REMAINDER OF NB I-25 INCLUDING THE LOOP RAMP ACCELERATION LANES INCLUDING THE NB I-25 BRIDGE OVER RIO BRAVO

* CONSTRUCTED WB RIO BRAVO FROM STA 2049+55 TO STA 2053+50

* CONSTRUCTED REMAINDER OF NB BRIDGE OVER RIO BRAVO

* CONSTRUCTED EASTERN PORTION OF RETAINING WALL D3-2



NOTES:

1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPIR INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS BE CLOSED AT NIGHT FOR BRIDGE DEMO. GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
3. PEDESTRIAN AND BICYCLE ACCESS WAS NOT ~~BE~~ ALLOWED IN THE CONSTRUCTION ZONE.

NO PEDESTRIAN SIGNS (R5-10A) WERE ~~BE~~ PLACED AT THE DIRECTION OF THE ENGINEER.

PHASE 4



1/10/2017

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 8 OF 11)			
PHASE 4			



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

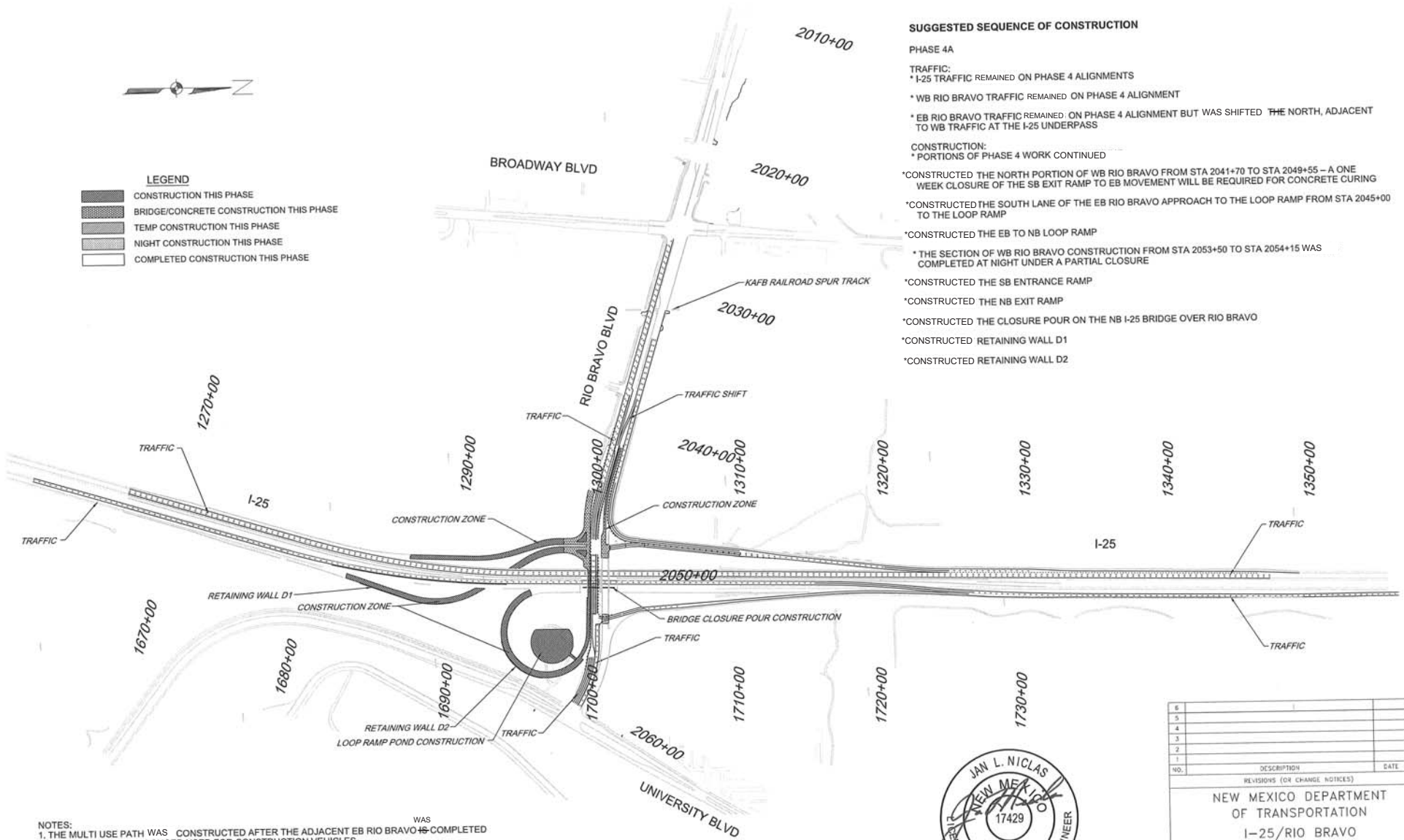
PHASE 4A

TRAFFIC:

- * I-25 TRAFFIC REMAINED ON PHASE 4 ALIGNMENTS
- * WB RIO BRAVO TRAFFIC REMAINED ON PHASE 4 ALIGNMENT
- * EB RIO BRAVO TRAFFIC REMAINED ON PHASE 4 ALIGNMENT BUT WAS SHIFTED THE NORTH, ADJACENT TO WB TRAFFIC AT THE I-25 UNDERPASS

CONSTRUCTION:

- * PORTIONS OF PHASE 4 WORK CONTINUED
- * CONSTRUCTED THE NORTH PORTION OF WB RIO BRAVO FROM STA 2041+70 TO STA 2049+55 - A ONE WEEK CLOSURE OF THE SB EXIT RAMP TO EB MOVEMENT WILL BE REQUIRED FOR CONCRETE CURING
- * CONSTRUCTED THE SOUTH LANE OF THE EB RIO BRAVO APPROACH TO THE LOOP RAMP FROM STA 2045+00 TO THE LOOP RAMP
- * CONSTRUCTED THE EB TO NB LOOP RAMP
- * THE SECTION OF WB RIO BRAVO CONSTRUCTION FROM STA 2053+50 TO STA 2054+15 WAS COMPLETED AT NIGHT UNDER A PARTIAL CLOSURE
- * CONSTRUCTED THE SB ENTRANCE RAMP
- * CONSTRUCTED THE NB EXIT RAMP
- * CONSTRUCTED THE CLOSURE POUR ON THE NB I-25 BRIDGE OVER RIO BRAVO
- * CONSTRUCTED RETAINING WALL D1
- * CONSTRUCTED RETAINING WALL D2



- NOTES:
1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
 2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPIRIT INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS BE CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
 3. PEDESTRIAN AND BICYCLE ACCESS WERE NOT BE ALLOWED IN THE CONSTRUCTION ZONE.
- "NO PEDESTRIAN" SIGNS (R5-10A) WERE BE PLACED AT THE DIRECTION OF THE ENGINEER.

PHASE 4A



1/10/2017

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 9 OF 11)			
PHASE 4A			

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 5

TRAFFIC:

* I-25 AND RAMP TRAFFIC OPENED ON ALL NEW ALIGNMENTS

* THE EB RIO BRAVO LOOP RAMP APPROACH WAS REDUCED TO ONE LANE

* WB RIO BRAVO TRAFFIC WAS SHIFTED TO THE TWO OUTSIDE LANES AT THE INTERCHANGE

CONSTRUCTION:

* CONSTRUCTED EB RIO BRAVO LANES AND MEDIAN FROM STA 2041+70 TO STA 2048+00 AND STA 2050+20 TO UNIVERSITY

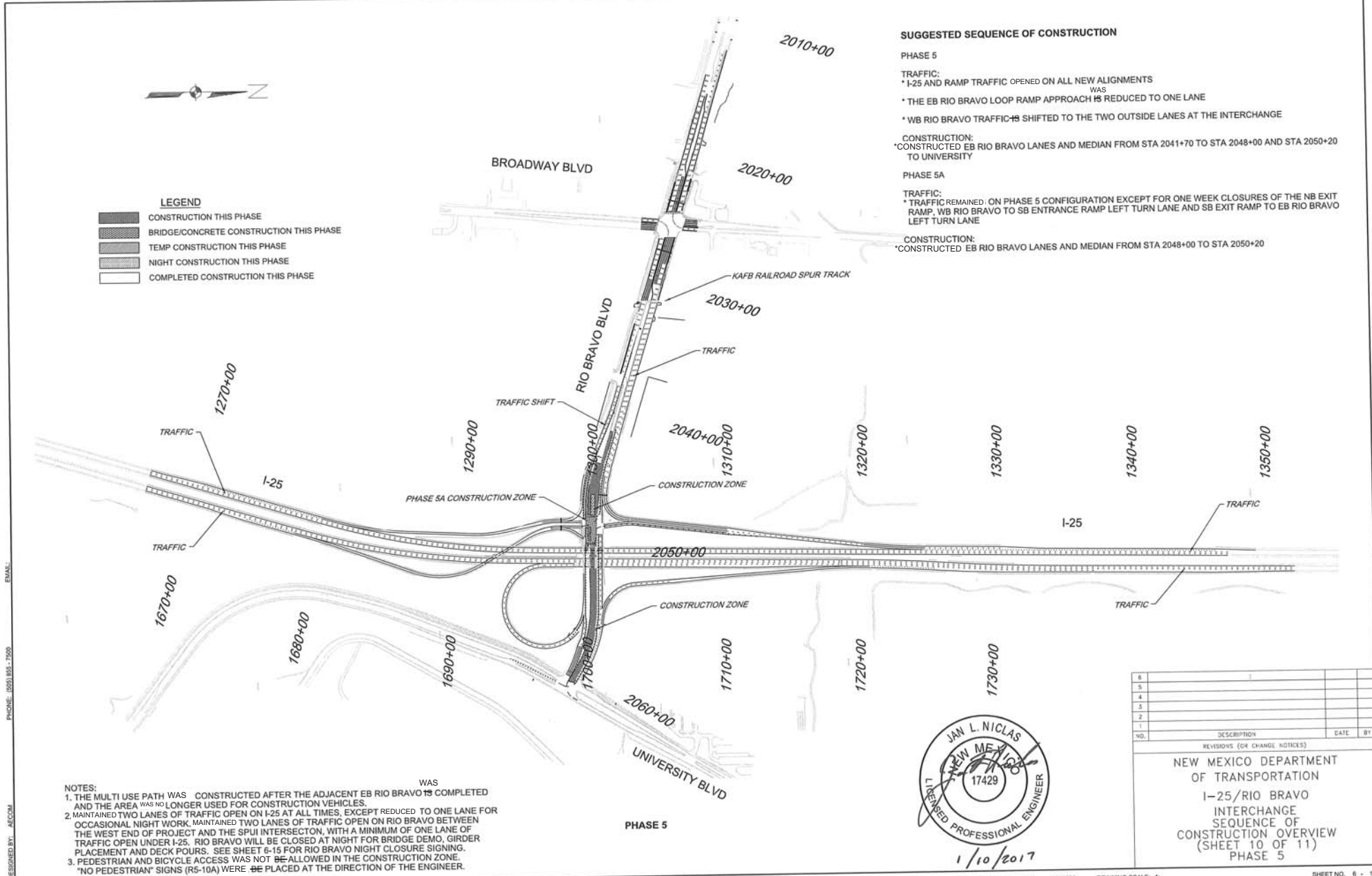
PHASE 5A

TRAFFIC:

* TRAFFIC REMAINED ON PHASE 5 CONFIGURATION EXCEPT FOR ONE WEEK CLOSURES OF THE NB EXIT RAMP, WB RIO BRAVO TO SB ENTRANCE RAMP LEFT TURN LANE AND SB EXIT RAMP TO EB RIO BRAVO LEFT TURN LANE

CONSTRUCTION:

* CONSTRUCTED EB RIO BRAVO LANES AND MEDIAN FROM STA 2048+00 TO STA 2050+20



EMAIL

PHONE: (505) 865-7300

DESIGNED BY: AECOM

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE

SUGGESTED SEQUENCE OF CONSTRUCTION

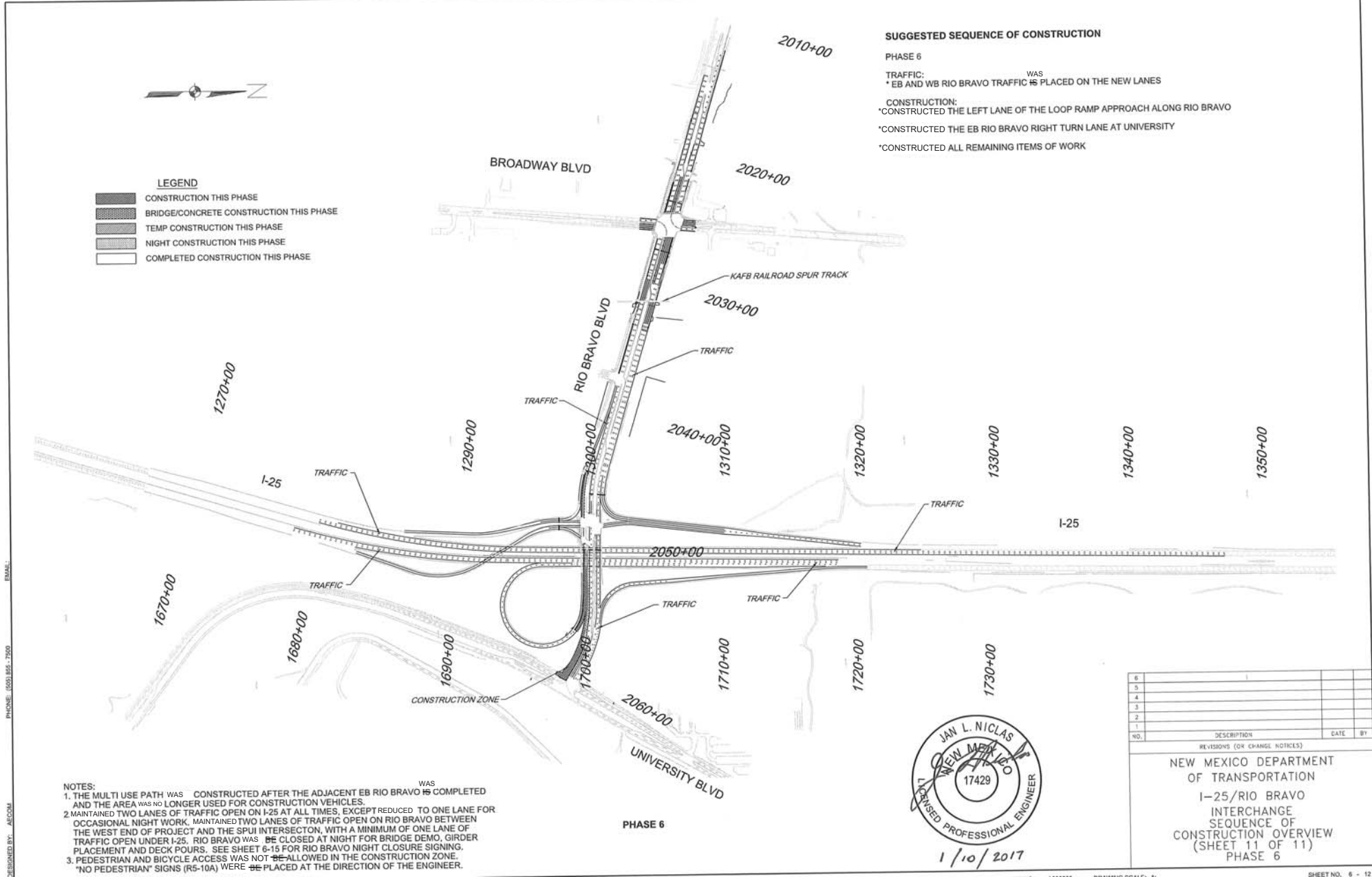
PHASE 6

TRAFFIC: WAS
* EB AND WB RIO BRAVO TRAFFIC IS PLACED ON THE NEW LANES

CONSTRUCTION:
* CONSTRUCTED THE LEFT LANE OF THE LOOP RAMP APPROACH ALONG RIO BRAVO

* CONSTRUCTED THE EB RIO BRAVO RIGHT TURN LANE AT UNIVERSITY

* CONSTRUCTED ALL REMAINING ITEMS OF WORK



NOTES:

1. THE MULTI USE PATH WAS CONSTRUCTED AFTER THE ADJACENT EB RIO BRAVO WAS COMPLETED AND THE AREA WAS NO LONGER USED FOR CONSTRUCTION VEHICLES.
 2. MAINTAINED TWO LANES OF TRAFFIC OPEN ON I-25 AT ALL TIMES, EXCEPT REDUCED TO ONE LANE FOR OCCASIONAL NIGHT WORK. MAINTAINED TWO LANES OF TRAFFIC OPEN ON RIO BRAVO BETWEEN THE WEST END OF PROJECT AND THE SPUI INTERSECTION, WITH A MINIMUM OF ONE LANE OF TRAFFIC OPEN UNDER I-25. RIO BRAVO WAS BE CLOSED AT NIGHT FOR BRIDGE DEMO, GIRDER PLACEMENT AND DECK POURS. SEE SHEET 6-15 FOR RIO BRAVO NIGHT CLOSURE SIGNING.
 3. PEDESTRIAN AND BICYCLE ACCESS WAS NOT BE ALLOWED IN THE CONSTRUCTION ZONE.
- *NO PEDESTRIAN" SIGNS (R5-10A) WERE BE PLACED AT THE DIRECTION OF THE ENGINEER.

PHASE 6



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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
SEQUENCE OF CONSTRUCTION OVERVIEW			
(SHEET 11 OF 11)			
PHASE 6			

TRAFFIC CONTROL GENERAL NOTES:

1. **Traffic Control:** All Temporary Traffic Control (TTC) shall be placed in accordance with the NMDOT Standard Specifications for Highway and Bridge Construction (latest edition) and the Manual on Uniform Traffic Control Devices (latest edition) and current revisions with the following constraints:

- No substitutions will be allowed for channelization devices type drum unless otherwise noted in the plans.
- Temporary portable sign stands are an unnecessary hazard when not in use. Unused temporary sign stands shall be removed from the roadway. If temporary sign stands are staged for future use (approved by the project manager) they shall be folded up and stored away from the paved shoulder.
- Use of Type I or II barricades on roadways with speed limit greater than 40 mph is strictly prohibited.
- The work zone shall comply with, but not limited to, NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control.

2. **BOP and EOP Signing:** BOP and EOP signing in accordance with Standard Drawing 702-03-1/1 shall be placed at the project limits prior to construction operations commencing and shall remain in place throughout the duration of the project or as directed by the project manager. Advance warning signs shall be placed at all side streets.

3. **FLAGGING:** Flagging shall be provided for safety per the plan or as directed by the project manager and shall conform to the MUTCD latest edition. The flaggers, applicable signs and other related items shall be considered incidental to the completion of the project and no separate measurement or payment will be made.

- All flaggers shall be certified and shall have their certification available for review at all times when on duty.
- Flagging operations shall adhere to NCHRP 476 Guidelines for Design and Operation of Nighttime Traffic Control. Flaggers shall wear high-visibility safety apparel that meets Performance Class 2 or 3.

4. **INGRESS AND EGRESS:** The contractor shall provide ingress and egress to local residences and businesses for the duration of the project. If access closure is required, the contractor shall request the closure through the project manager. Upon approval, the contractor shall coordinate such closure with the property owners and the project manager at least 48 hours in advance. All work associated with this shall be considered incidental to the completion of the project and no separate payment or measurement will be made.

5. **PORTABLE CHANGEABLE MESSAGE SIGNS:** The contractor shall supply Portable Changeable Message signs, which will be retained by the contractor. The message boards shall be utilized to convey messages, expected delays, and detours to motorists as required. Messages should be determined by the contractor and approved by the project manager. Two Weeks prior to start of construction name locations, and number of message signs to be placed.

6. **TRAFFIC CONTROL PLANS:** This traffic control plan (TCP) represents a suggested method for traffic control during construction. Adjustments to the details of this TCP and requirements within the plan may be necessary due to construction activities, or as directed by the project manager. If the contractor elects to make any changes to the TCP or sequence of construction, the contractor shall submit four (4) 11" X 17" copies of the proposed TCP to the project manager at least two (2) weeks prior to implementation. The TCP shall conform to the current editions of the MUTCD, NMDOT Standard Specifications and AASHTO Roadside Design Guide. The TCP shall be in computer drafted format and shall be designed, stamped, and revised as necessary by a current New Mexico Licensed professional engineer and submitted to the Project Manager for approval. All costs associated with developing the TCP and any additional devices associated with the TCP shall be incidental to Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.

7. **PUBLIC INFORMATION:** The Contractor / TCP firm shall contact the District Public Information Officer, through the district office, to confirm the actual start dates of the construction and the contractor's schedule a minimum of 48 hours before any work listed in the TCP is performed.

8. **REMOVAL OF CONSTRUCTION SIGNING:** All temporary traffic control signs, sign posts and post bases installed with the construction project shall be REMOVED by the contractor at the completion of the project. Removal shall consist of complete extraction of the bases from the ground. This work shall be incidental to the completion of the project and no separate measurement or payment will be made.

9. **CONFLICTING SIGNS:** All Conflicting Signs within or in advance of the work zone shall be covered completely with an opaque non-reflecting transmitting material so as not to damage the sign. The contractor is to use an approved method of covering existing signs so as not to damage/distort the sign sheeting or markings. The Contractor shall not place tape directly on the face of the sign. Failure to adhere to this requirement will result in the Contractor replacing the sign at no cost to the NMDOT.

10. **TEMPORARY STRIPING:** The use of black paint to cover existing lane lines or symbols is strictly prohibited. All temporary striping shall be placed before opening any work zone or portion of a work zone in accordance with the MUTCD and the approved traffic control plan. This work shall be included in Item No. 704100, "Removable Marking Tape" and no separate measurement or payment will be made, unless otherwise noted in the contract.

11. **CONSTRUCTION SIGNING:** All construction signing shall meet retroreflectivity requirements listed in section 702.2.1 "Construction Signing" of the NMDOT Standard Specifications.

- All construction signing on the interstate and on high speed (greater or equal to 45 MPH) multi-lane divided facilities shall be double indicated (left and right shoulders).
- All signs that are part of work zone that are in place for more than 3 days shall be placed on breakaway posts. If there are physical restrictions at the site that prohibit the sign from being placed on posts, the contractor shall notify the District Traffic Engineer and obtain a waiver.
- All warning and regulatory signs shall meet the following size requirements:
Interstate: Warning sign 48"x48" Regulatory 48"x60"
Non-Interstate: Warning sign 36"x36" Regulatory 36"x42"
- The following reflectivity material shall be used on all construction signing placed on NMDOT roadways.

SIGN	SIGN CODE	COLOR	LETTER SHEETING	BACKGROUND SHEETING
APPROACH SIGN	W20-XX	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
CHEVRONS	W1-8	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
CURVES	W1-2	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
REVERSE CURVE	W1-4	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
VERGE	W4-1	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
NO PASSING ZONE	W14-3	BLUFLUORESCENT ORANGE	---	TYPE VII, IX, XI
FLAGGER PADDLE	---	BLUFLUORESCENT ORANGE on Side 1 with RED on Side 2	---	TYPE VII, IX, XI Type IV White
ALL DRUMS	---	WHITE FLUORESCENT ORANGE	---	TYPE VIII, IX, XI Type IV White
All Other Const. Signs	---	BLUFLUORESCENT ORANGE	---	TYPE VIII, IX, XI

12. **REMOVAL OF TEMPORARY STRIPING:** All relevant temporary striping shall be removed upon completion of each phase of construction. The only approved method of stripe removal is water blasting. The contractor is to ensure that there is no conflicting striping through the work zone or through detours. This work shall be included in Item No. 618000, "Traffic Control Management," and no separate measurement or payment will be made, unless otherwise noted in the contract.

13. **SATISFACTORY WORKING CONDITION:** All traffic control devices used on this project shall be in satisfactory working condition and shall function equivalent to new equipment in accordance with the MUTCD (latest edition). Traffic Control work zone shall comply with requirements of FHWA 23CFR 630 Subpart K for traffic control devices. At the beginning of the project 100% of devices shall be in acceptable condition (new or like new). After 2 weeks at no time shall less than 75% of devices be in acceptable condition. All traffic devices shall be kept clean throughout the duration of the project. Any sign that is legged by graffiti shall be cleaned (as long as it does not affect the reflective sheeting) within 24 hours or removed and replaced.

14. **TRAFFIC CONTROL FIELD ADJUSTMENTS:** Location of device spacing shall be field verified to account for existing roadway features which may obstruct placement and/or view of devices. Any changes to the traffic control plan should be approved by the District Traffic Engineer or his/her designee. All field adjustments of signs should be approved by the District Traffic Engineer. This work shall be considered incidental to Item No. 618000 "Traffic Control Management" and no additional measurement or payment will be made, unless noted in the contract.

15. **PEDESTRIANS:** Routes/paths shall not be closed without providing a detour. ADA requirements shall be adhered to; ADA compliant devices shall be used for channelization.

16. **BICYCLES:** Shall be accommodated or rerouted per MUTCD recommendations.



NO	DATE	REV BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
TEMPORARY TRAFFIC CONTROL GENERAL NOTES			
APPROVED	ASIN [Signature] 1/10/17		
DESIGNED BY	DRAWN BY	CHECKED BY	
702-01-15 NMDOT GENERAL NOTES I-25/RIO BRAVO			

17. **TEMPORARY CONCRETE WALL BARRIER (CWB):** When flaring the leading end of a Temporary Wall Barrier (CWB) within a construction work zone, the flare rate shall be done in accordance with the rates shown in the table below: (NMDOT Standard Drawing 606-20-5/5)

Roadway Speed Limit	Minimum Taper/ Flare Rate	Desirable Taper/ Flare Rate
Less than 45 MPH	8:1	18:1
Between 45 MPH and 55 MPH	10:1	24:1
Greater than 55 MPH	15:1	30:1

- a. When temporary wall barrier is placed in a construction work zone, a 5' clear area is required between the CWB and the work zone to accommodate barrier deflection. When a 5' clear area is not attainable, CWB shall be anchored to the pavement surface.
 - b. Temporary CWB shall be provided with reflective barrier delineators as indicated in NMDOT standard drawing 606-21-1/1.
18. **CRASH ATTENUATORS:** The crash cushion attenuators shall be designed as per the District Traffic Engineer's recommendations. The District may elect to either utilize the pre-construction posted speed, or the 65% speed in the layout of the crash cushion attenuators within the work zone.
19. **DROP OFF POLICY:** In the areas of pavement operations or other activities within the traveled way and adjacent to the existing traveled lane, the contractor shall assure that no pavement drop-offs are left exposed during non-working hours. The contractor shall initiate corrective means as per the "New Mexico Department of Transportation Pavement Drop-off Guideline" to achieve a minimum 6:1 slope between traveled lanes and a minimum 3:1 slope adjacent to the existing traveled lane with two 11foot driving lanes as shown in the detail below. (AD241)



20. **Lane Closures:** The Contractor/TCF firm shall not place a lane closure taper along a horizontal curve. The taper shall be placed in advance of the horizontal curve so that it is visible to all oncoming traffic. On crest vertical curves, the Contractor/TCF firm shall place lane closures in advance of, or at the beginning of the curve to enhance visibility of the lane closure to oncoming traffic.
21. **Sequential Arrow Display:** Placement of the sequential arrow shall be at or near the beginning of the lane closure taper. In areas of insufficient pavement width, the sequential arrow may be placed within the taper, but not to exceed 1/2 the taper length. In all cases, the sequential arrow shall be placed behind the channelization devices. The shoulder shall be closed in advance of the merging taper to direct vehicular traffic to remain within the traveled way. (MUTCD 6F 61)
22. **ADDITIONAL SIGNS:** "BUMP", "LOOSE GRAVEL", "LANE DROP-OFF SIGN" sign placement: The contractor shall place W8-1 sign ("BUMP" - B/FO), W8-7 sign ("LOOSE GRAVEL" - B/FO) and/or W8-17 signs ("SHOULDER DROP-OFF" - B/FO) in advance of bridge approaches or other locations during cold milling and overlay operations as needed or as directed by the project manager.
23. **CLEAR ZONE:** All stationary objects within clear zone shall be properly shielded and outlined with drums mounted with Type "A" warning lights. Use of vertically mounted retro-reflective material in lieu of a Type A warning light is strictly prohibited.
 - a. Equipment, materials, or vehicles stored within Right-of-way (ROW) shall be outside of clear zone (based on existing posted speed).
 - b. Equipment, material or vehicles stored within clear zone shall be properly shielded.
 - c. Materials, work activities, equipment, and vehicles shall not be stored within the established buffer space of the project work zone.
 - d. All construction equipment, vehicles and materials shall remain behind traffic control devices.
24. **TRAFFIC CONTROL MANAGEMENT:** The contractor or the traffic Control Subcontractor shall provide a Traffic Control Supervisor on site during working hours for response within 1 hour to traffic control issues/concerns.

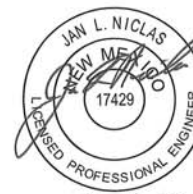
25. **INCIDENT MANAGEMENT:** Contractor is required to comply with requirements of FHWA CFR 630 Subpart J for Work Zone Safety and Mobility which shall include an Incident Management Plan to be utilized for the entire duration of the project. The Incident Management Plan shall contain a method to address traffic flow through the work zone during incidents. The Incident Management Plan must be reviewed and approved by the District Traffic Engineer. The plan shall contain the following as a minimum:

- Contacts for the contractor, local enforcement, safety agencies, municipal agencies, public information officer and NMDOT
- Steps to be followed during incidents
- Method of recording and reporting incidents

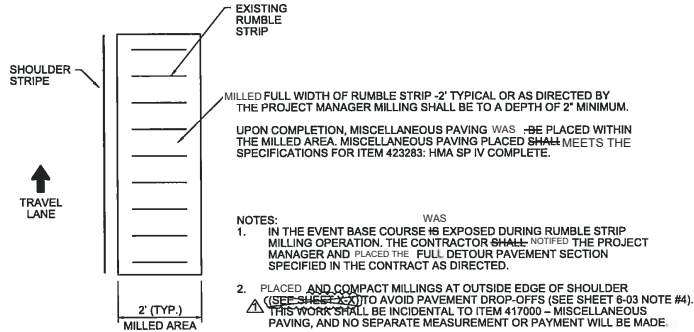
26. LIST OF INCIDENTALS - No Additional Payment Associated
LIST OF INCIDENTALS for Temporary Traffic Control/ MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

	LIST OF INCIDENTALS for Temporary Traffic Control
A.	MAINTENANCE OF TEMPORARY PAVEMENT MARKINGS FOR PROJECT DURATION

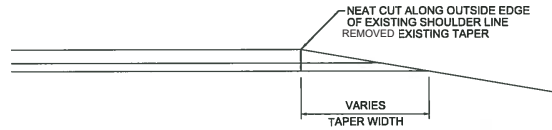
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REVISIONS (OR CHANGE NOTICES)			
<p>NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING</p>			
<p>TEMPORARY TRAFFIC CONTROL GENERAL NOTES</p>			
APPROVED:			
DESIGNED BY:	DRAWN BY:	CHECKED BY:	
<p>702-01-2/5 MOT GENERAL NOTES</p>			



1/10/2017



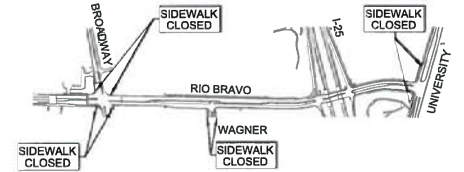
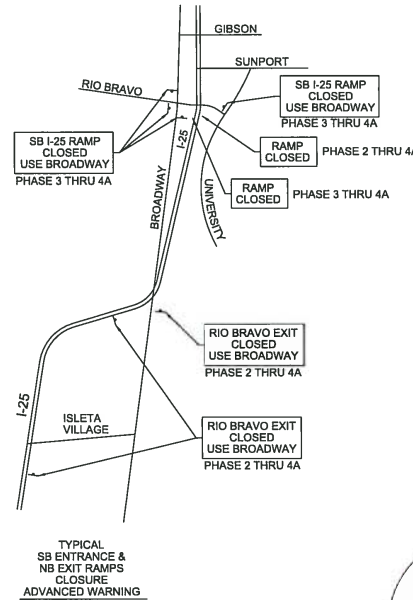
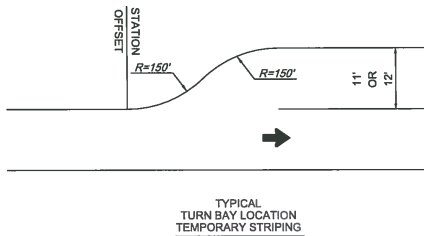
NOTE: MILLING OF RUMBLE STRIPS WERE BE INCIDENTAL TO ITEM 417000: MISCELLANEOUS PAVING (S.Y.). PLACED WITHIN THE MILLED AREA, AND NO SEPARATE MEASUREMENT OR PAYMENT WAS BE MADE.



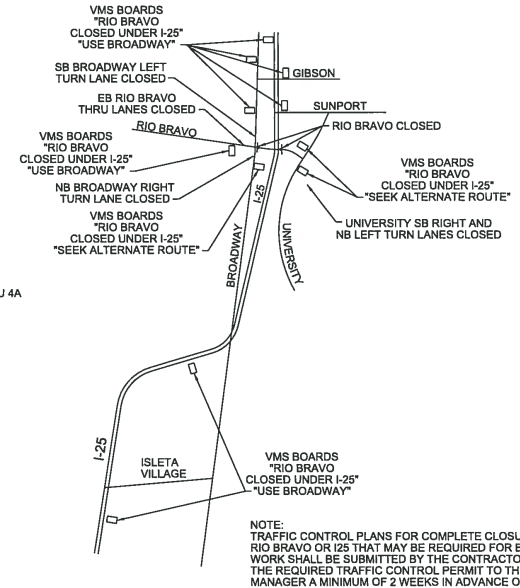
PRIOR TO WIDENING WITH DETOUR PAVEMENT WHERE INDICATED IN THE PLANS, THE EXISTING SURFACING TAPER WAS BE REMOVED PER THE DETAIL ABOVE.

SURFACING TAPER REMOVAL DETAIL

NOTE: REMOVAL OF EXISTING SURFACING TAPER WAS INCIDENTAL TO ITEM 405000 DETOUR PAVEMENT (S.Y.), AND NO SEPARATE MEASUREMENT OR PAYMENT WAS BE MADE.



- NOTES:
- PEDESTRIANS AND BICYCLE ACCESS WAS NOT BE ALLOWED IN THE CONSTRUCTION ZONE. BICYCLES WERE BE ALLOWED IN TRAFFIC LANES.
 - "NO PEDESTRIANS" (RS-10A) OR "SIDEWALK CLOSED" (RS-9) WAS BE PLACED AT THE DISCRETION OF THE ENGINEER.



NOTE: TRAFFIC CONTROL PLANS FOR COMPLETE CLOSURES OF RIO BRAVO OR I-25 THAT MAY BE REQUIRED FOR BRIDGE WORK SHALL BE SUBMITTED BY THE CONTRACTOR WITH THE REQUIRED TRAFFIC CONTROL PERMIT TO THE PROJECT MANAGER A MINIMUM OF 2 WEEKS IN ADVANCE OF WORK.

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1	REVISED NOTE 2	3/7/2017	GJB
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
TRAFFIC CONTROL DETAILS AND TYPICAL ROAD CLOSURE ADVANCED WARNINGS			



3/7/2017

DESIGNED BY: AECOM
 PHONE: (602) 855-7000
 EMAIL: JCL@aecom.com
 AECOM 408 NO. 6203328
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110

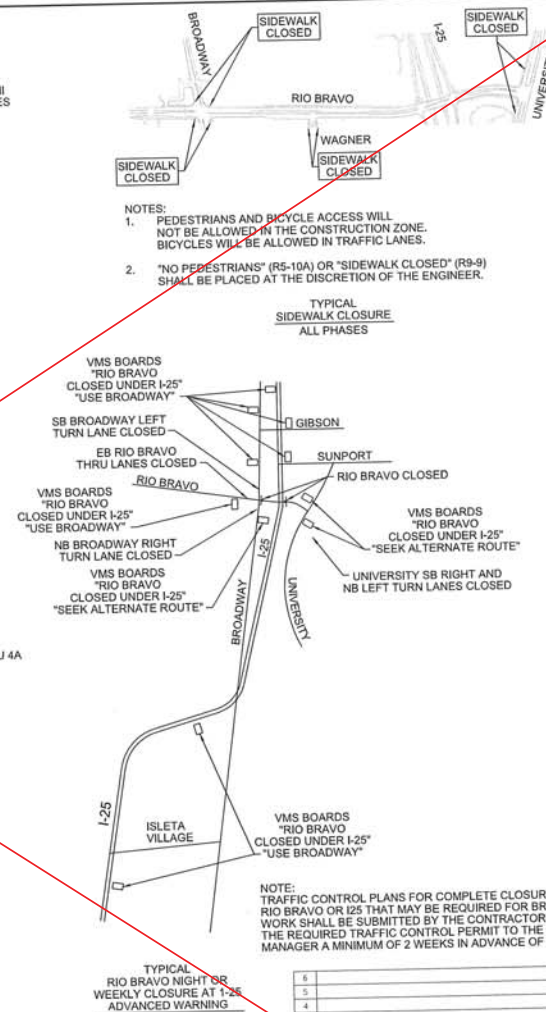
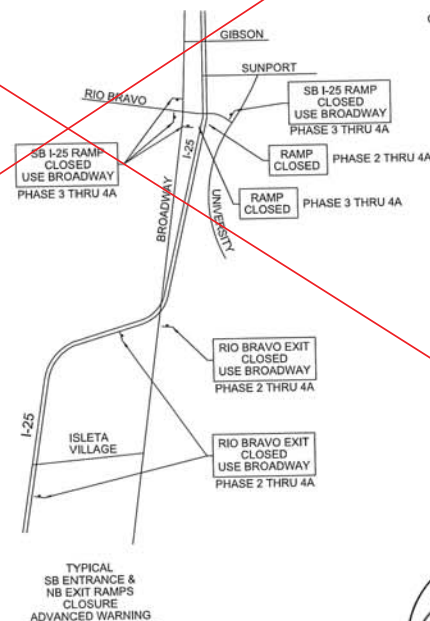
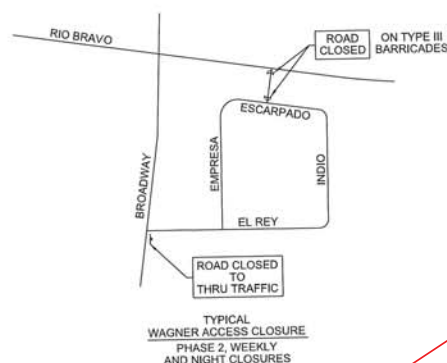
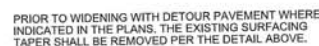
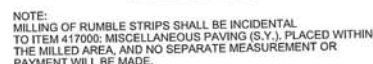
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 3/7/2017 11:58:50 AM kyle.lehlo
 NM001.pdf.plt

TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 T 505.863.7000

AECOM
 DESIGNED BY: AECOM
 www.aecom.com

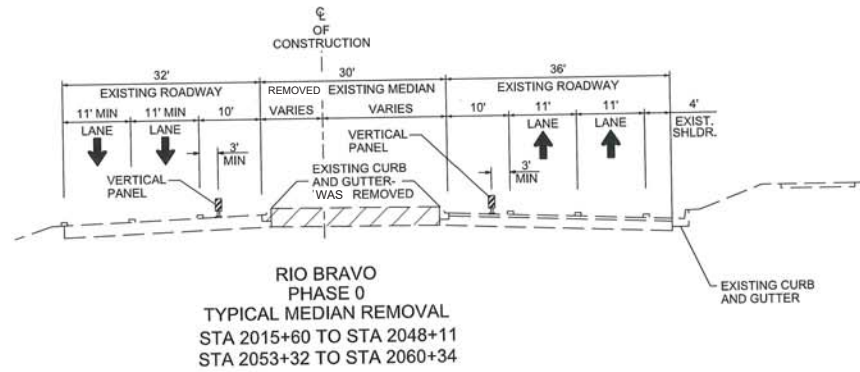
NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 15



NOTE:
TRAFFIC CONTROL PLANS FOR COMPLETE CLOSURES OF
RIO BRAVO OR I25 THAT MAY BE REQUIRED FOR BRIDGE
WORK SHALL BE SUBMITTED BY THE CONTRACTOR WITH
THE REQUIRED TRAFFIC CONTROL PERMIT TO THE PROJECT
MANAGER A MINIMUM OF 2 WEEKS IN ADVANCE OF WORK.

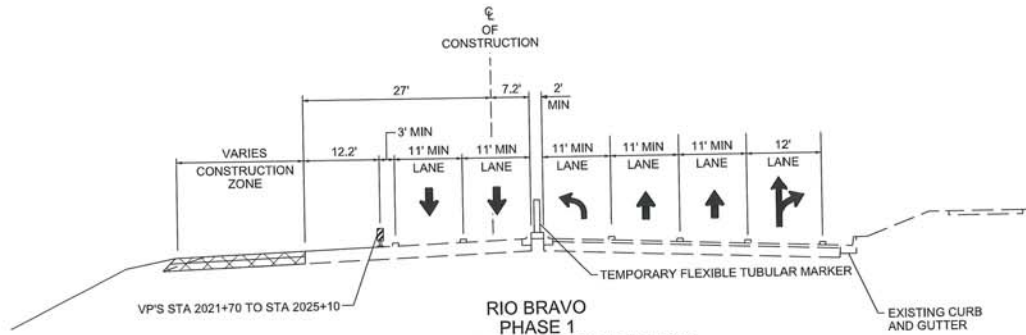
AECOM
AECOM TECHNICAL SERVICES, INC.
6501 Americas Parkway NE, Suite 900
Albuquerque, New Mexico 87110
PHONE: (505) 855-7500
EMAIL: XJXXXXXX@aecom.com
AECOM JOB NO. 62303326



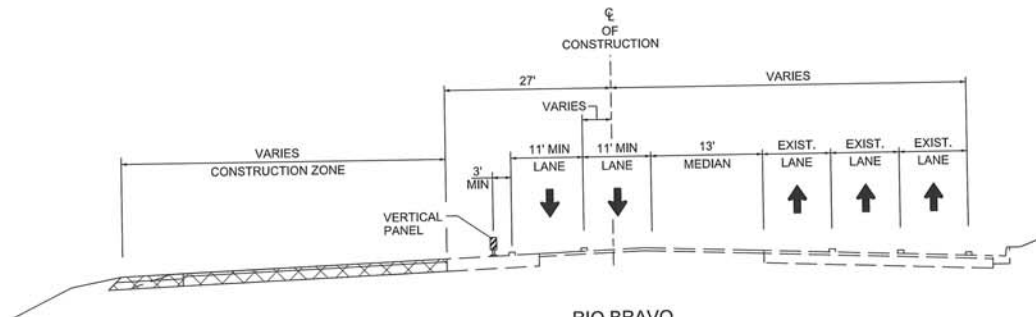
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 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD
 - STRIPE (4')

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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
NOT TYPICAL SECTIONS ROADWAY-PHASE 0 & 1 RIO BRAVO			

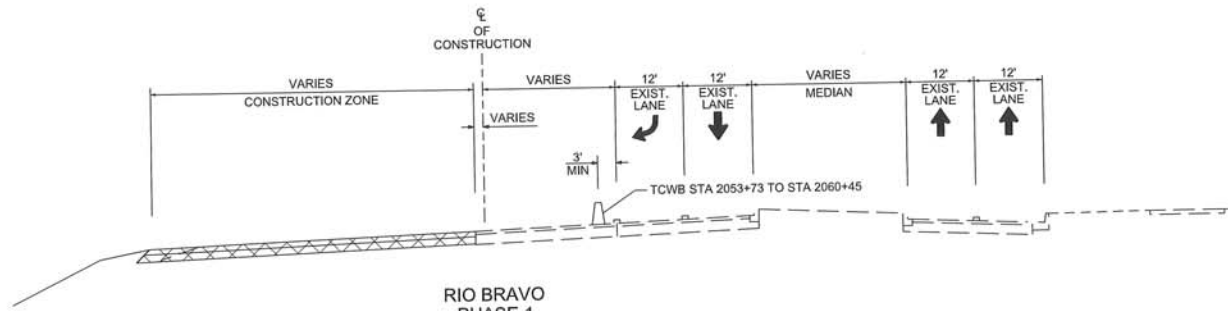




RIO BRAVO
PHASE 1
TYPICAL OUTSIDE CONSTRUCTION
STA 2024+00 TO STA 2025+16



RIO BRAVO
PHASE 1
TYPICAL OUTSIDE CONSTRUCTION
STA 2031+00 TO STA 2044+75



RIO BRAVO
PHASE 1
TYPICAL OUTSIDE CONSTRUCTION
STA 2054+14 TO STA 2060+74

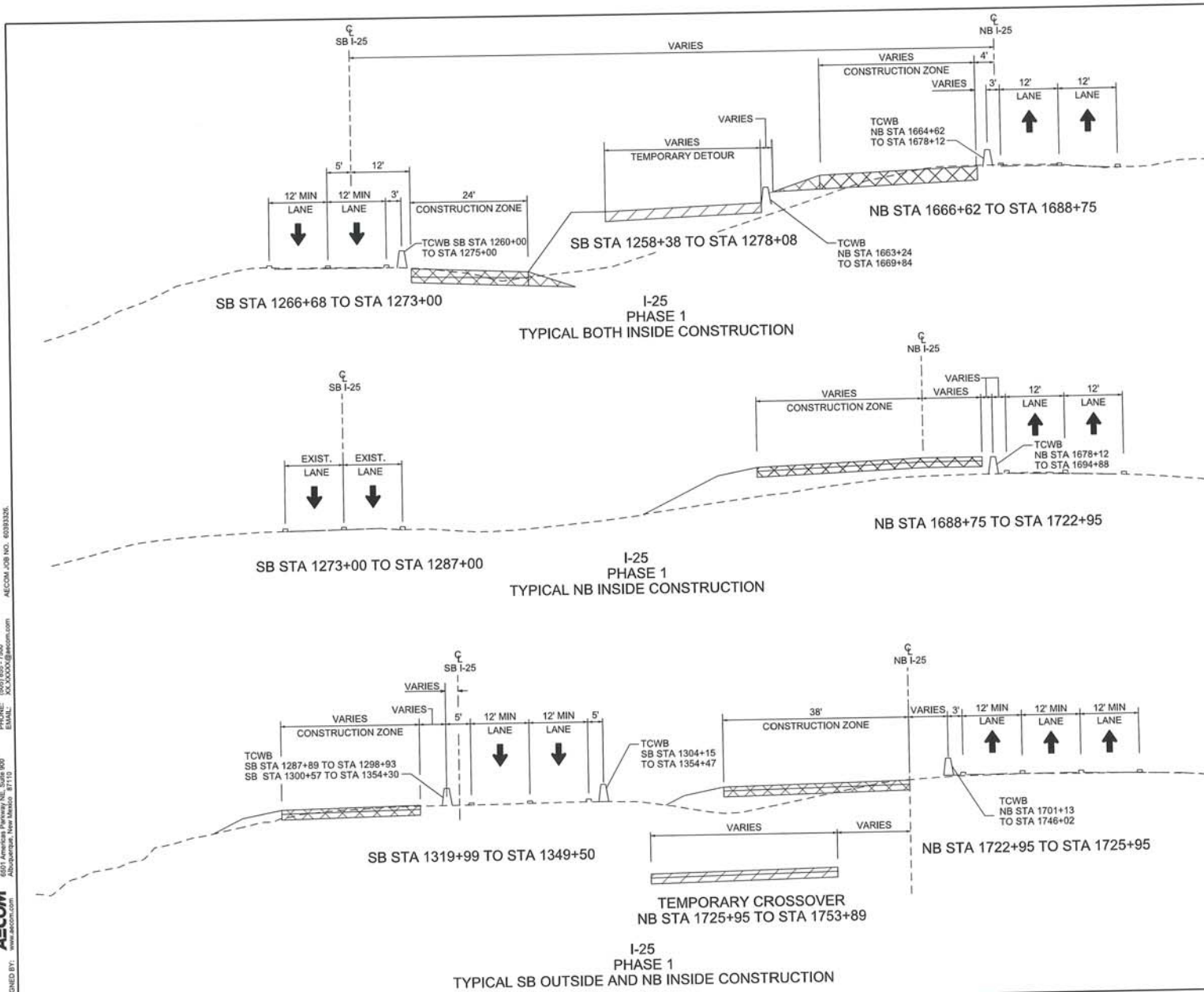


- LEGEND**
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 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S
 - ARROW BOARD
 - STRIPE (4')

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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

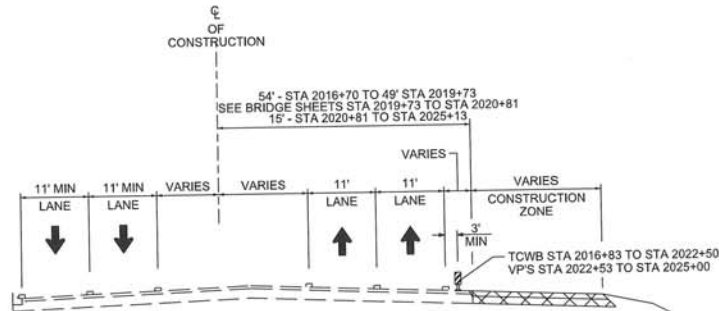
I-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 1
RIO BRAVO



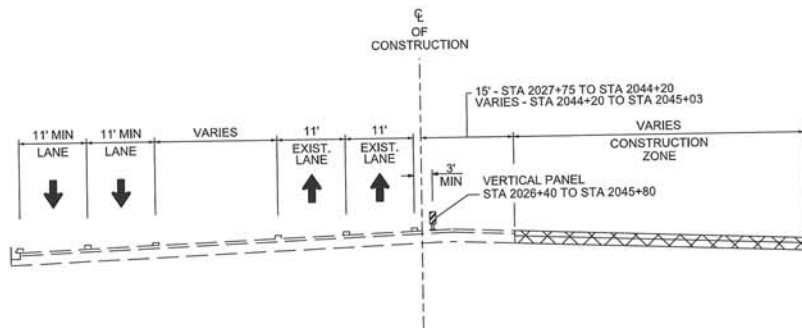
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 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
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 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD
 - STRIPE (4")

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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
NOT TYPICAL SECTIONS			
PHASE 1			
RIO BRAVO			

AECOM TECHNICAL SERVICES, INC.
 1500 JEFFERSON AVENUE, SUITE 100
 ALBUQUERQUE, NEW MEXICO 87110
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 FAX: (505) 855-7100
 EMAIL: AECOM@AECOM.COM
 DESIGNED BY: AECOM



RIO BRAVO
PHASE 2
TYPICAL OUTSIDE CONSTRUCTION
STA 2016+70 TO STA 2025+13



RIO BRAVO
PHASE 2
TYPICAL OUTSIDE CONSTRUCTION
STA 2027+75 TO STA 2045+03

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
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 - ARROW BOARD
 - STRIPE (4')

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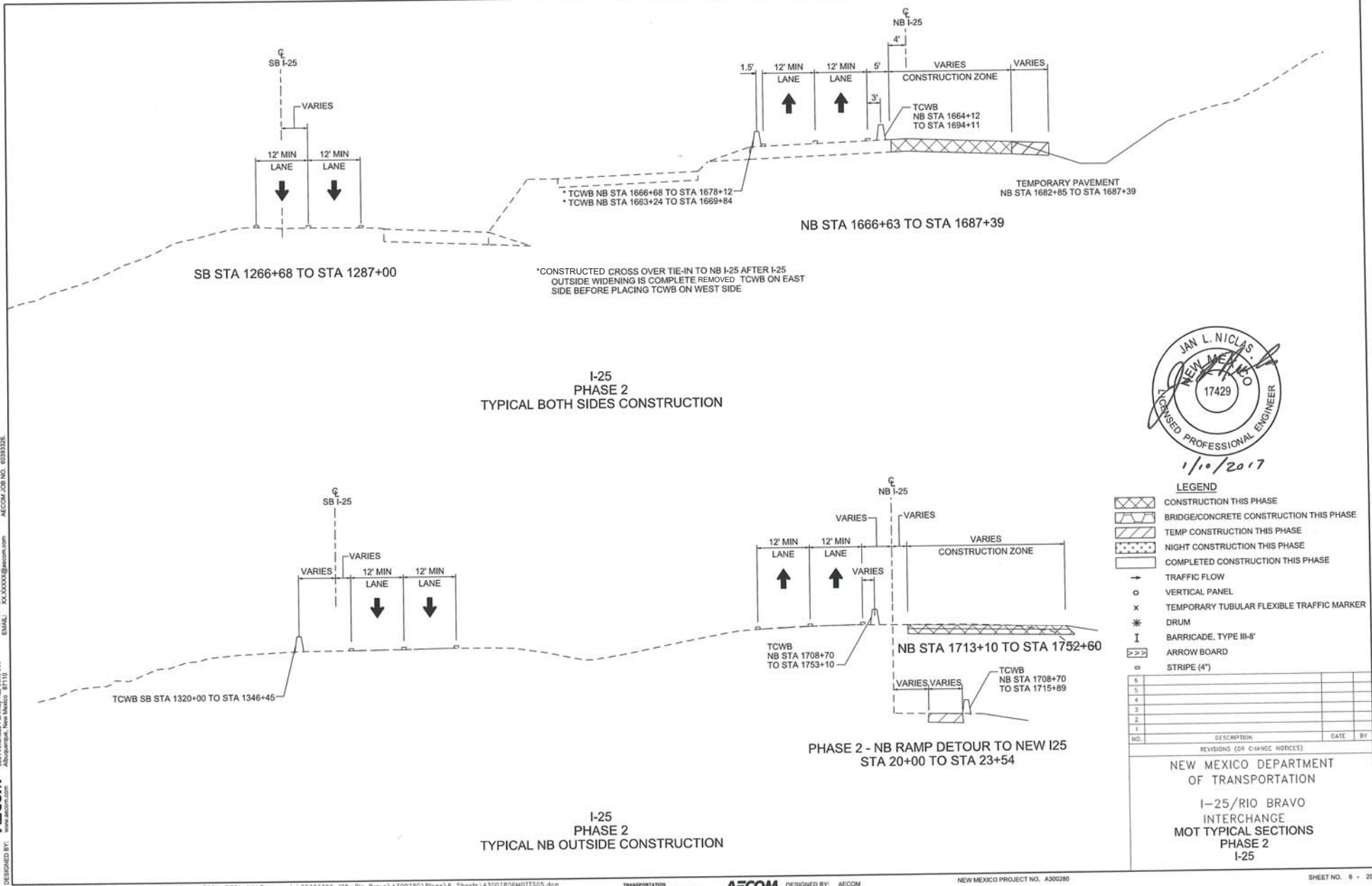
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION

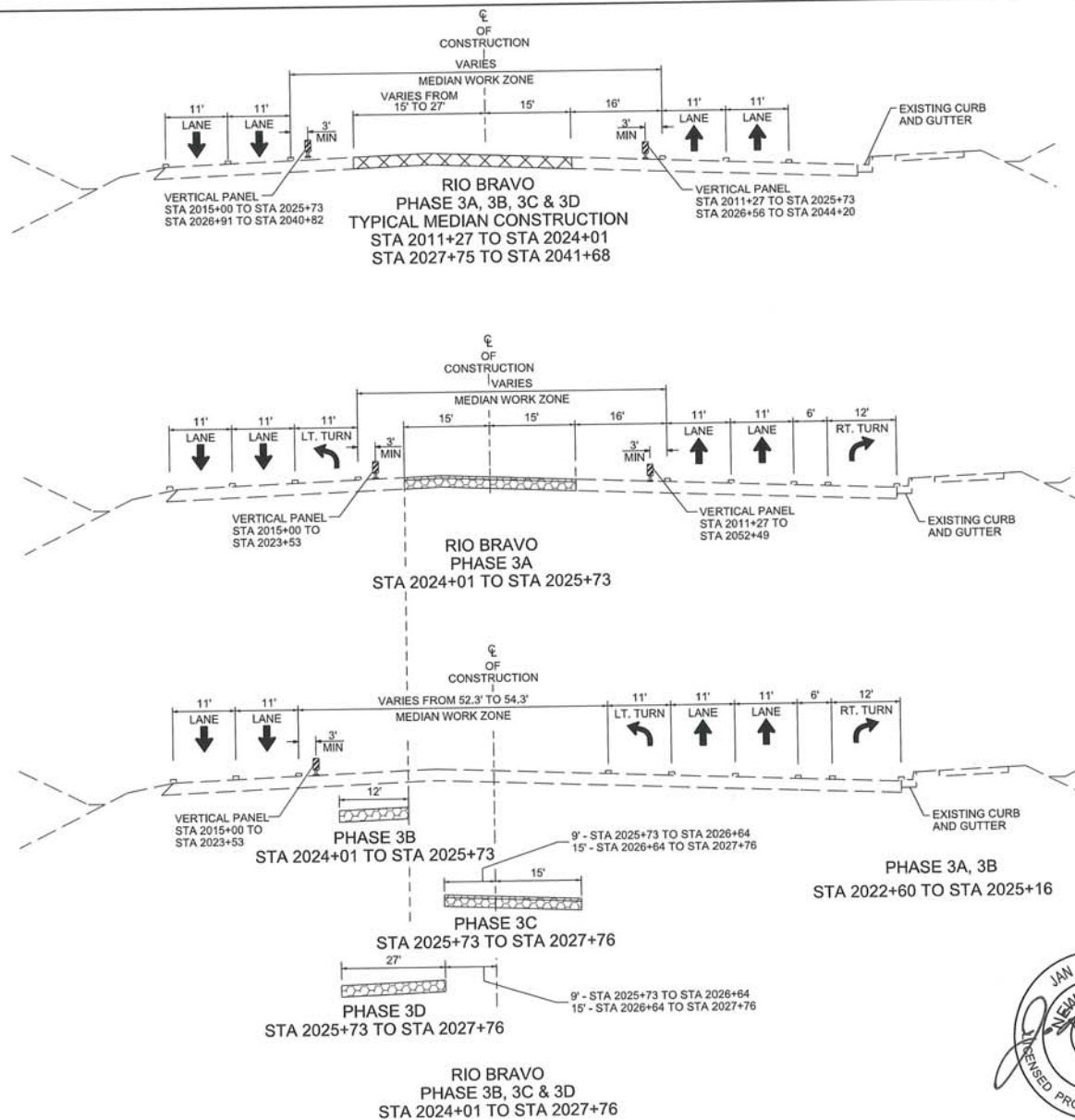
I-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 2
RIO BRAVO



1/10/2017



JAN L. NICLAS
NEW MEXICO
17429
LICENSED PROFESSIONAL ENGINEER
1/10/2017

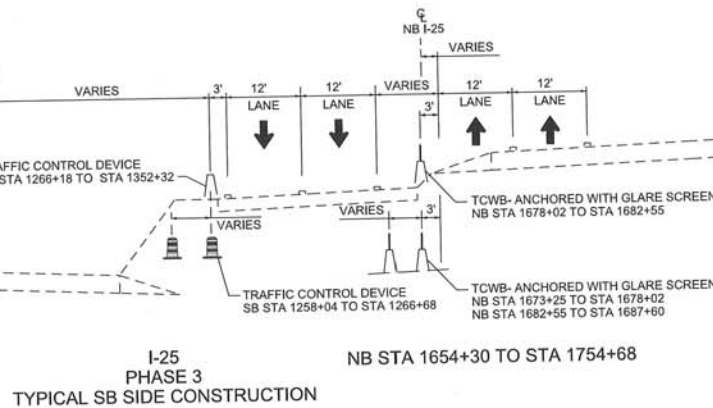
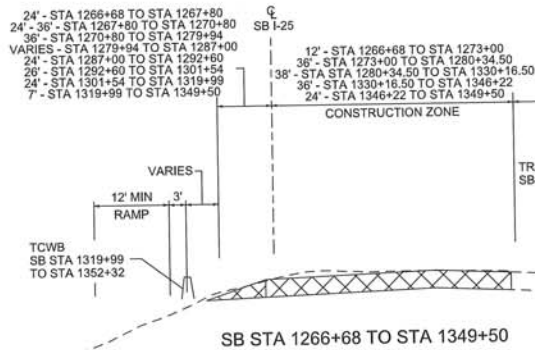
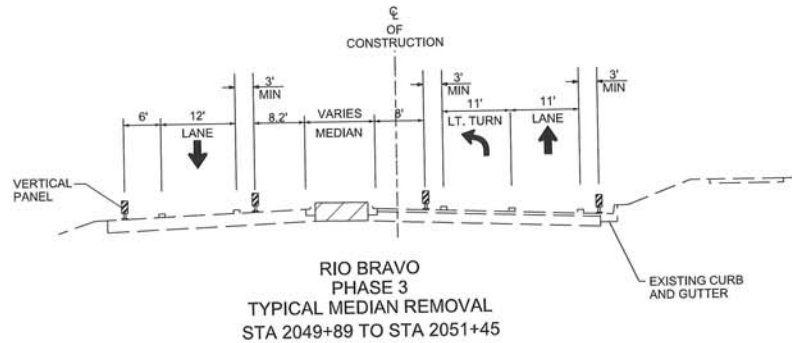
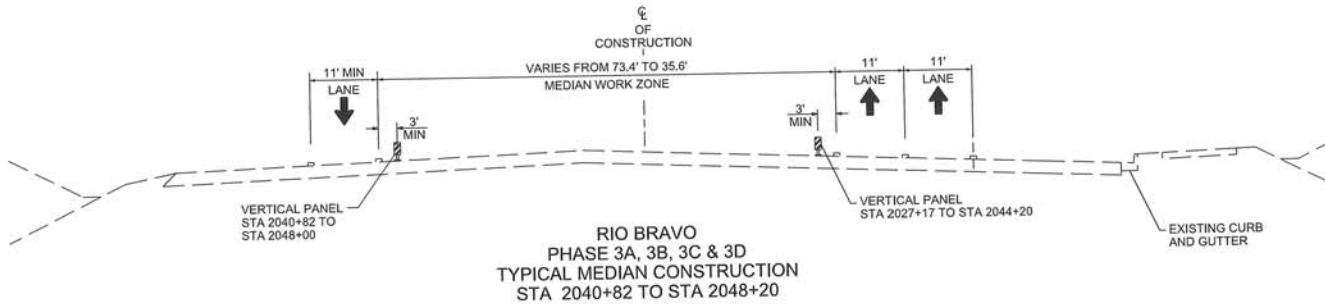


LEGEND	
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[Pattern]	TEMP CONSTRUCTION THIS PHASE
[Pattern]	NIGHT CONSTRUCTION THIS PHASE
[Pattern]	COMPLETED CONSTRUCTION THIS PHASE
[Symbol]	TRAFFIC FLOW
[Symbol]	VERTICAL PANEL
[Symbol]	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
[Symbol]	DRUM
[Symbol]	BARRICADE, TYPE III-8'
[Symbol]	ARROW BOARD
[Symbol]	STRIPE (4")

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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
NOT TYPICAL SECTIONS			
PHASE 3A, 3B, 3C & 3D			
RIO BRAVO			



1/10/2017



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-B
- ARROW BOARD
- STRIPE (4")

NO.	DESCRIPTION	DATE	BY
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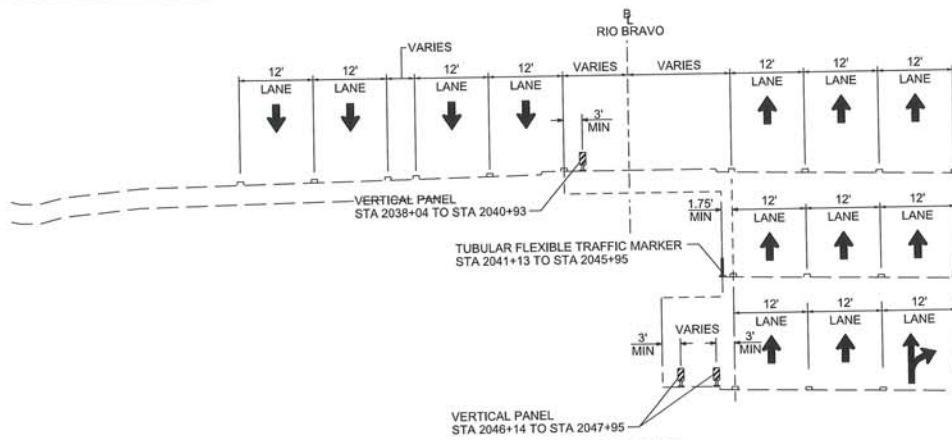
NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

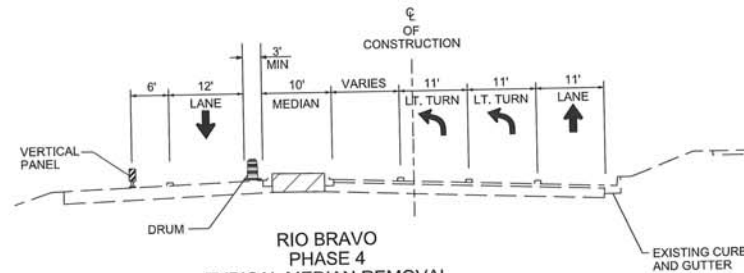
MOT TYPICAL SECTIONS

PHASE 3A, 3B, 3C & 3D RIO BRAVO

PHASE 3 I-25

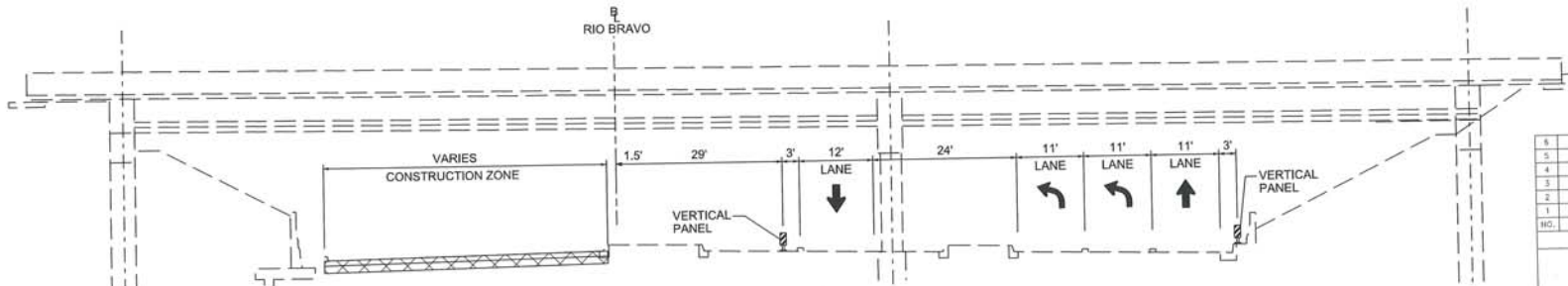


**RIO BRAVO
PHASE 4
TYPICAL CONSTRUCTION
STA 2038+04 TO STA 2047+98**



**RIO BRAVO
PHASE 4
TYPICAL MEDIAN REMOVAL
STA 2051+45 TO STA 2052+57**

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD
 - STRIPE (4')

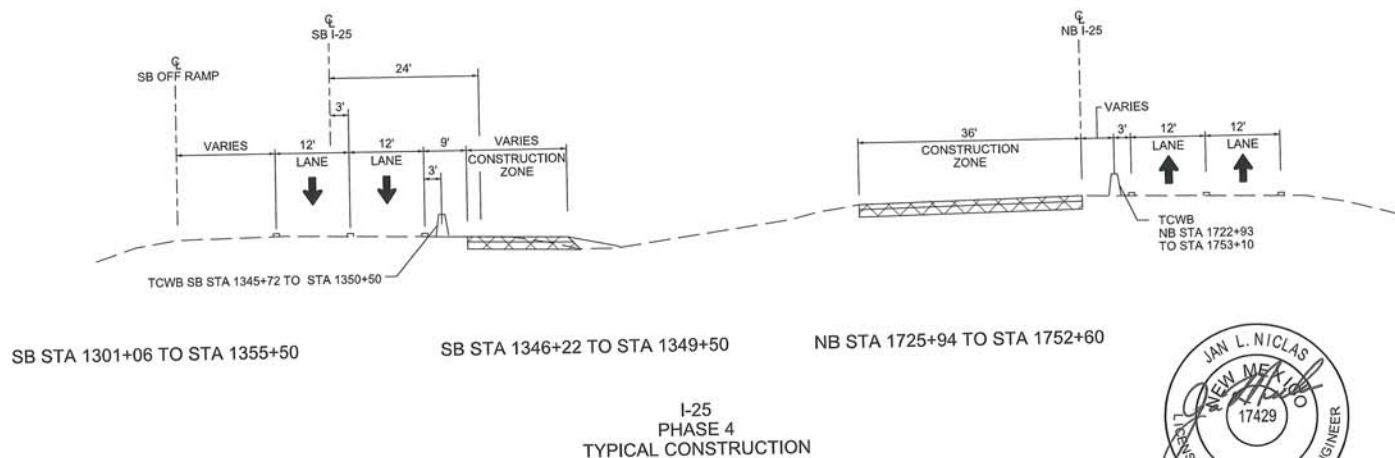
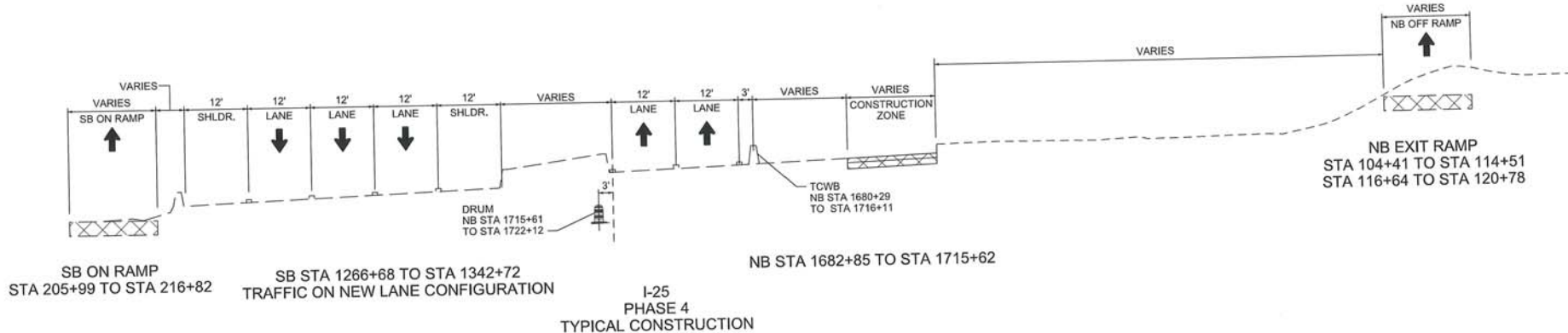


**RIO BRAVO
PHASE 4
TYPICAL CONSTRUCTION
STA 2049+55 TO STA 2053+50**

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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**NEW MEXICO DEPARTMENT
OF TRANSPORTATION**

**I-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 4
RIO BRAVO**



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD
 - STRIPE (4')

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)

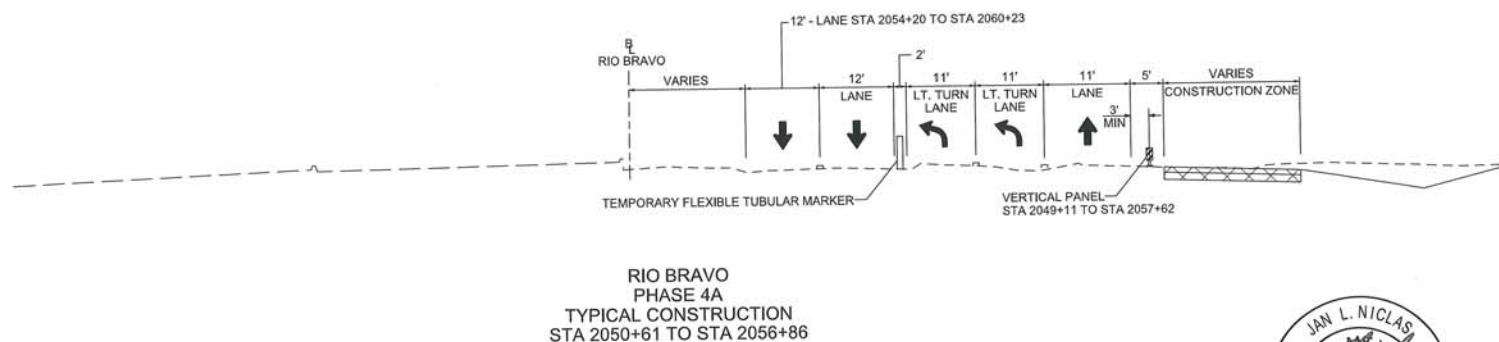
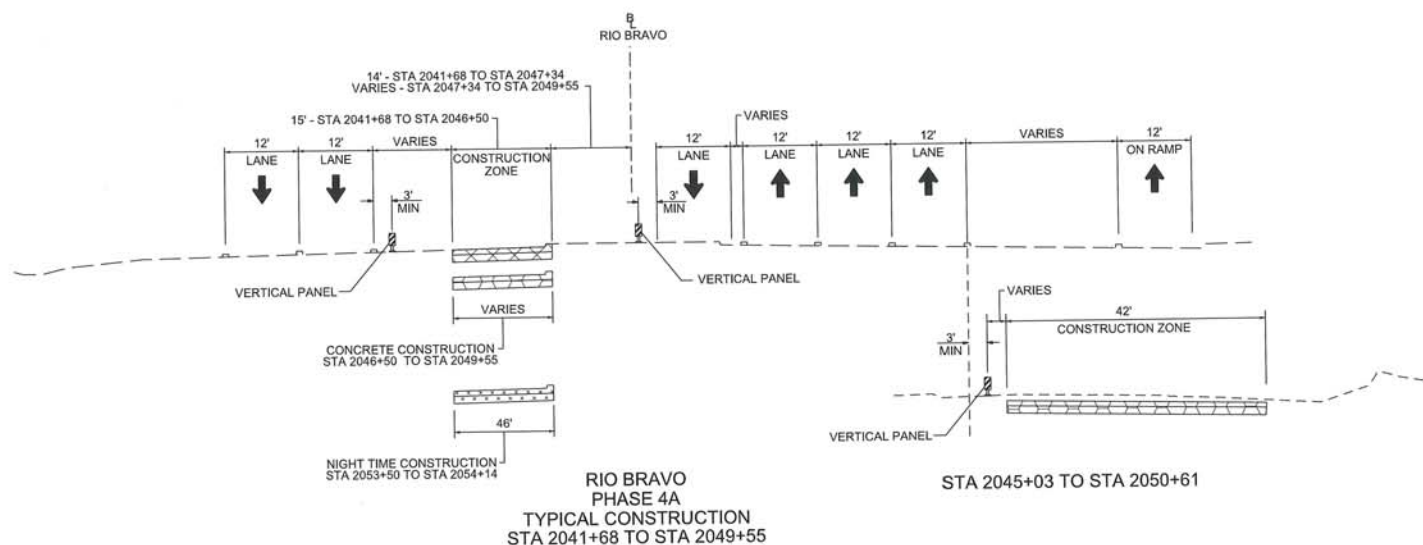
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 4
I-25



1/10/2017

AECOM TECHNICAL SERVICES, INC.
 6001 American Parkway, Suite 100
 Austin, TX 78731
 PHONE: (512) 451-7100
 FAX: (512) 451-7101
 EMAIL: AECOM@AECOM.COM
 AECOM JOB NO. 0303326

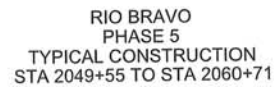
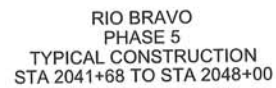


- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD
 - STRIPE (4')

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGE
NOT TYPICAL SECTIONS
PHASE 4A
RIO BRAVO

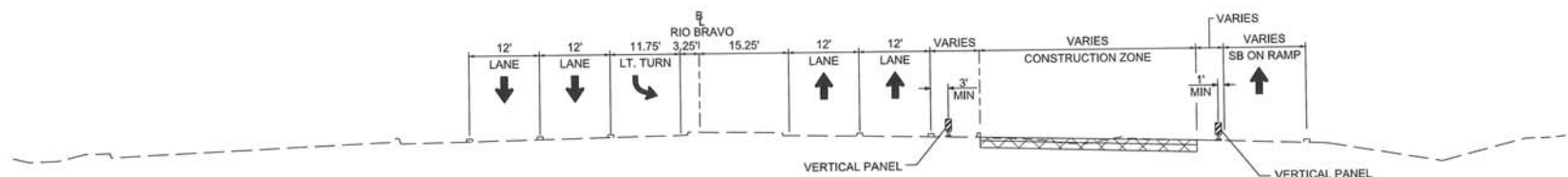


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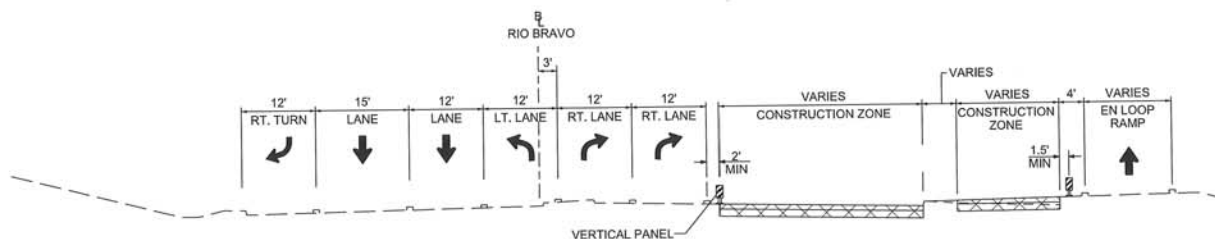
1-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 5
RIO BRAVO



RIO BRAVO
PHASE 6
TYPICAL CONSTRUCTION
STA 2050+19 TO STA 2054+75



RIO BRAVO
PHASE 6
TYPICAL CONSTRUCTION
STA 2054+75 TO STA 2060+83



EN LOOP RAMP
STA 304+23 TO STA 306+27

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
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 - DRUM
 - BARRICADE, TYPE III-8"
 - ARROW BOARD
 - STRIPE (4")

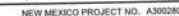
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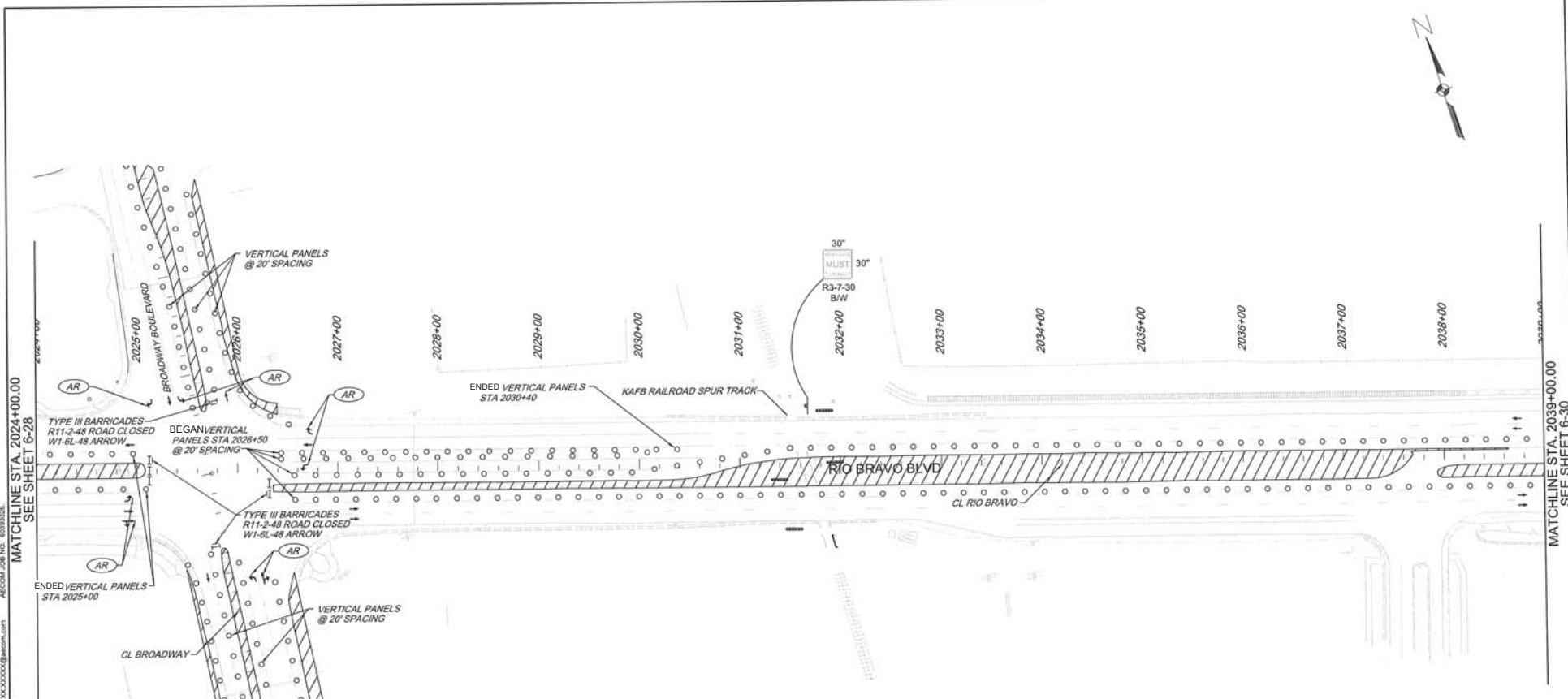
REVISIONS (OF CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE
MOT TYPICAL SECTIONS
PHASE 6
RIO BRAVO







MATCHLINE STA. 2024+00.00
SEE SHEET 6-28

MATCHLINE STA. 2039+00.00
SEE SHEET 6-30

LEGEND	
	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-S
	ARROW BOARD

LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)



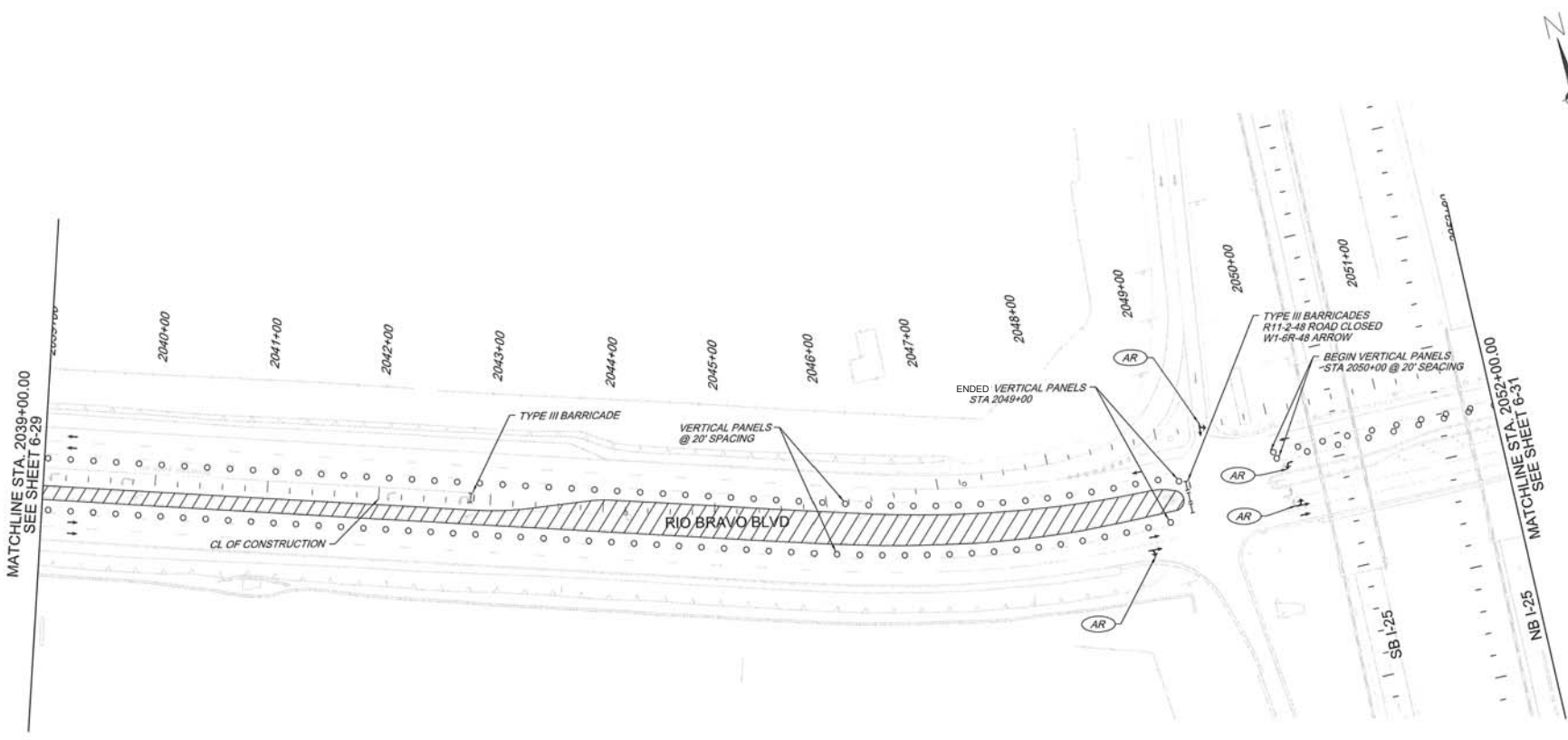
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1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 0



1/10/2017

ACCOM TECHNICAL SERVICES, INC.
 PHONE: (505) 855-7500
 EMAIL: TOLSON@accomm.com
 AECOM JOB NO. 60393326
 DESIGNED BY: AECOM



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)

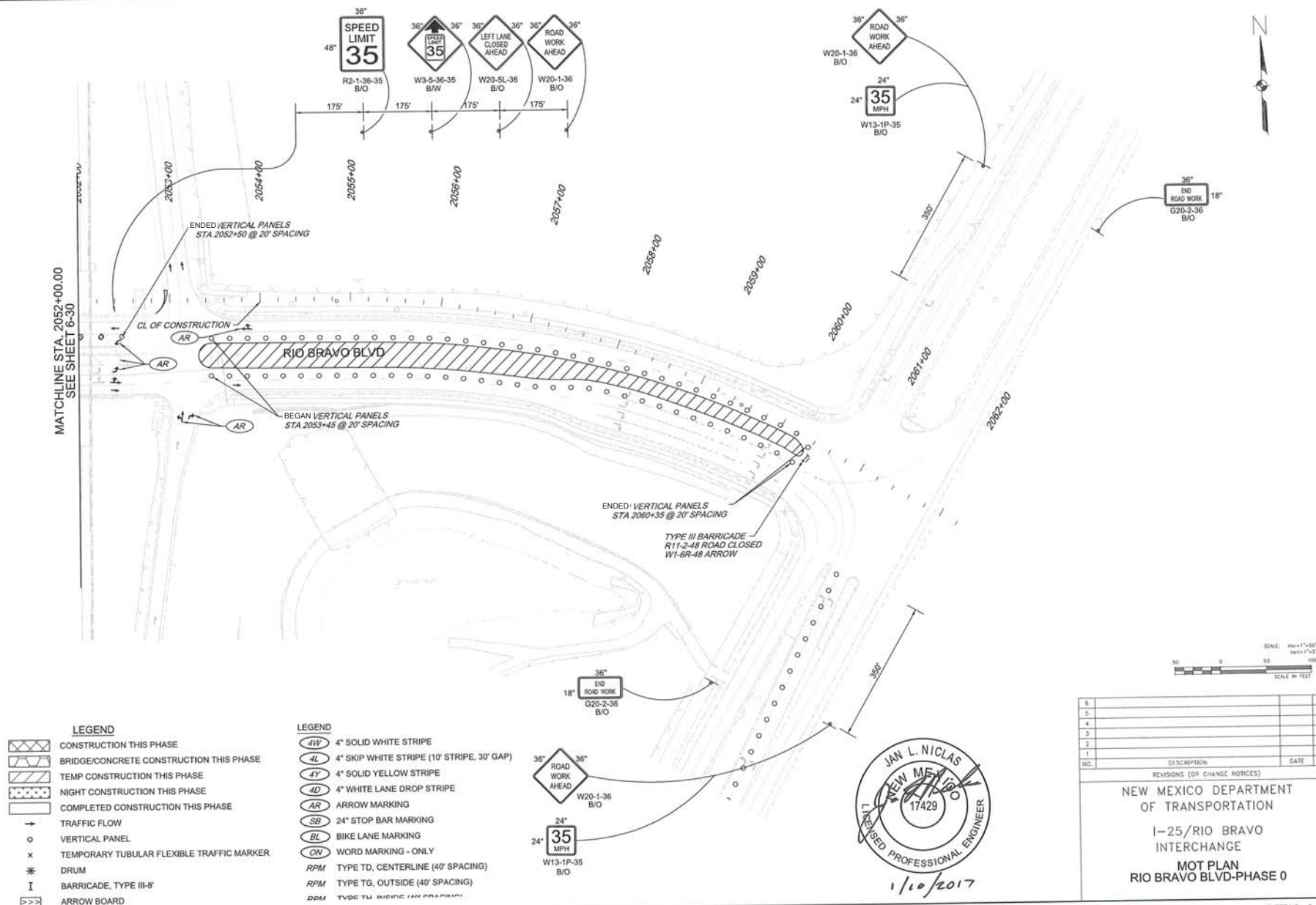


1/10/2017



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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN			
RIO BRAVO BLVD-PHASE 0			

MATCHLINE STA. 2052+00.00
SEE SHEET 6-30



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

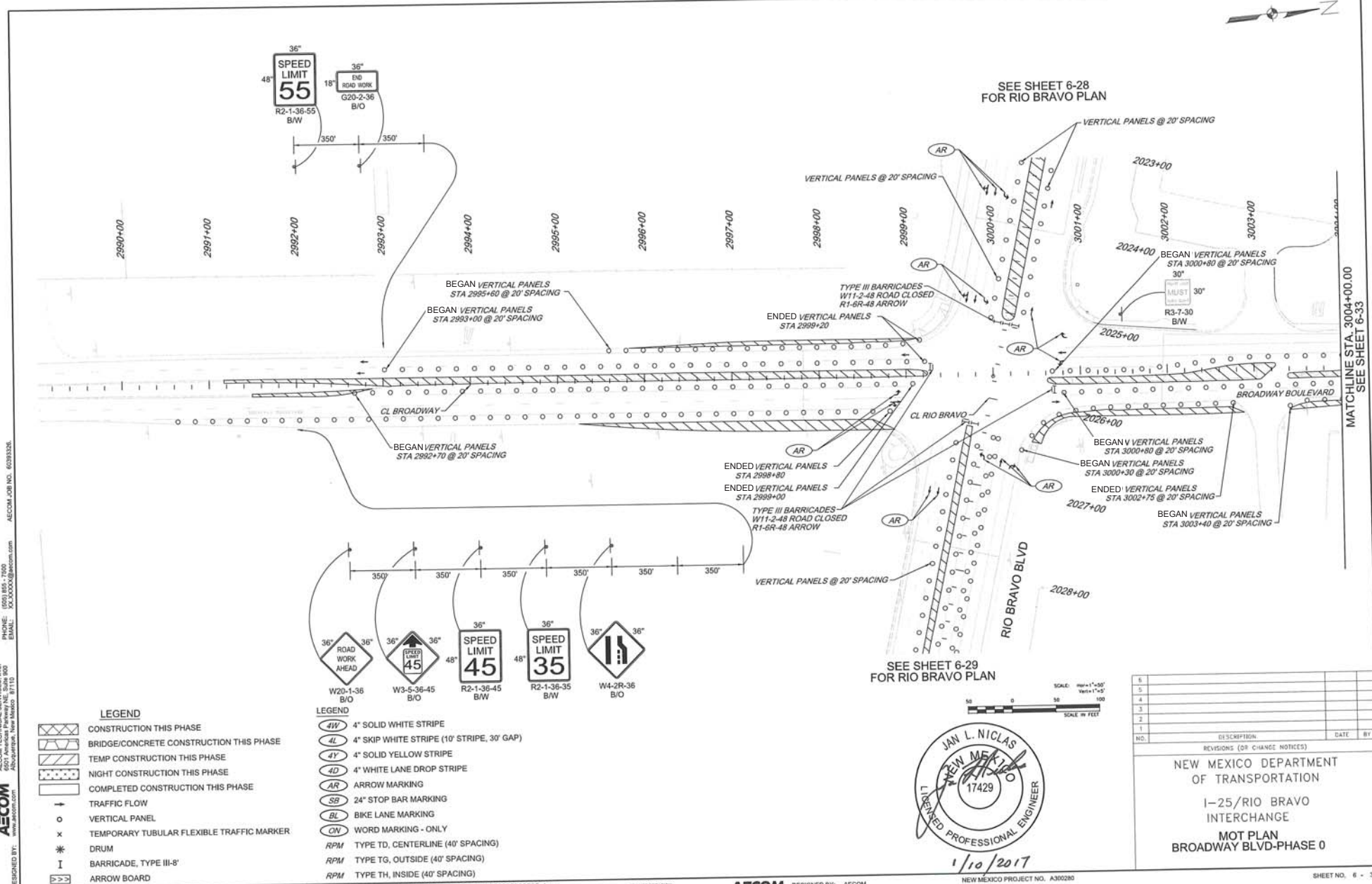
- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TU, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 0





DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 PHONE: (505) 855-7100
 EMAIL: XXXXXXX@aecom.com
 Albuquerque, New Mexico 87110



1/10/2017

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

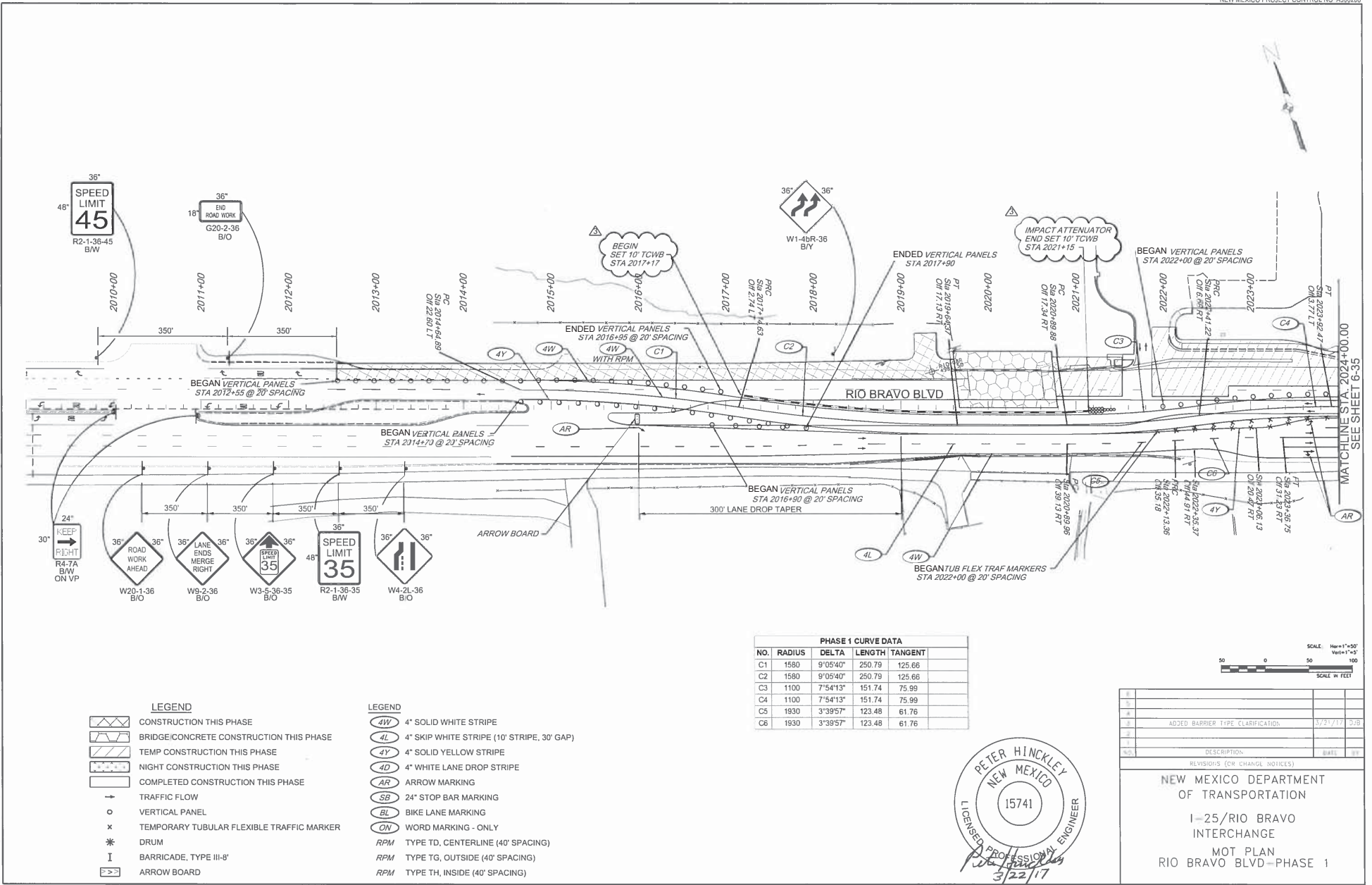
I-25/RIO BRAVO INTERCHANGE

MOT PLAN BROADWAY BLVD-PHASE 0



DESIGNED BY: AECOM
 6501 America Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 857-7500
 FAX: (505) 857-7500
 EMAIL: aecom@aecom.com

DESIGNED BY: AECOM
 6501 America Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 857-7500
 FAX: (505) 857-7500
 EMAIL: aecom@aecom.com



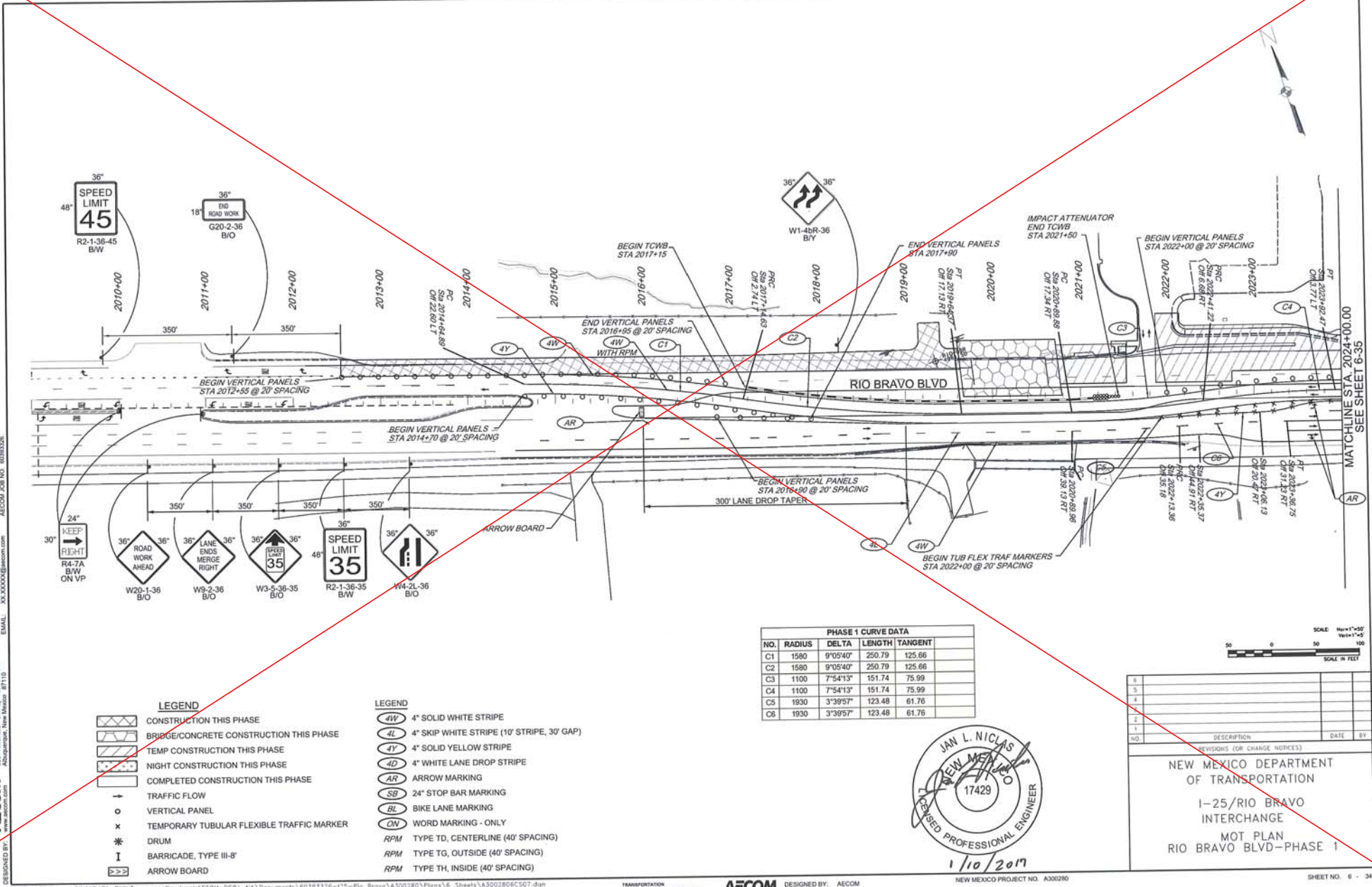
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TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 America Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 (505) 857-7500

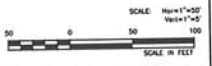
AECOM DESIGNED BY: AECOM
 www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 OF 34



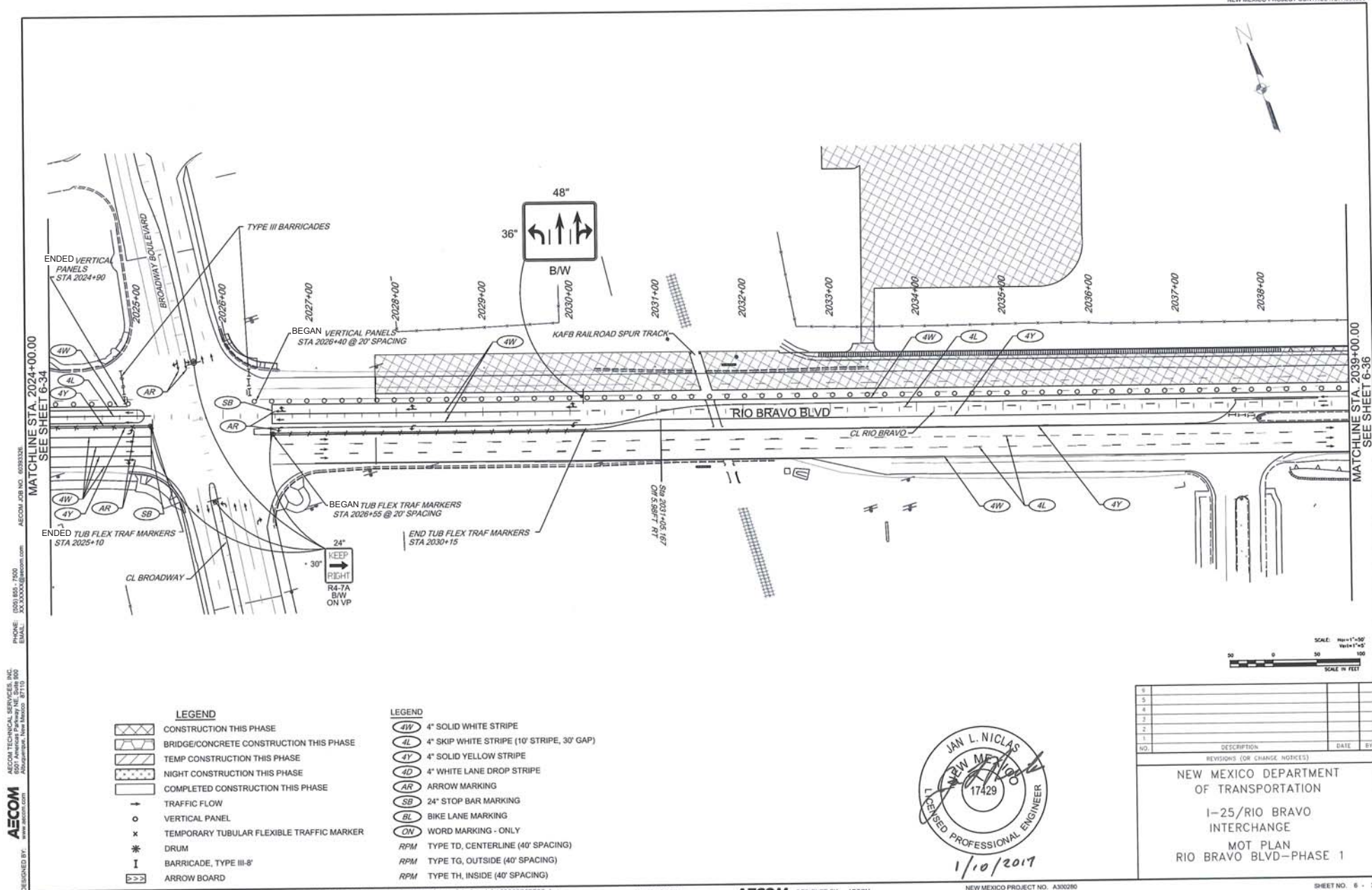
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NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	1580	9°05'40"	250.79	125.66
C2	1580	9°05'40"	250.79	125.66
C3	1100	7°54'13"	151.74	75.99
C4	1100	7°54'13"	151.74	75.99
C5	1930	3°39'57"	123.48	61.76
C6	1930	3°39'57"	123.48	61.76

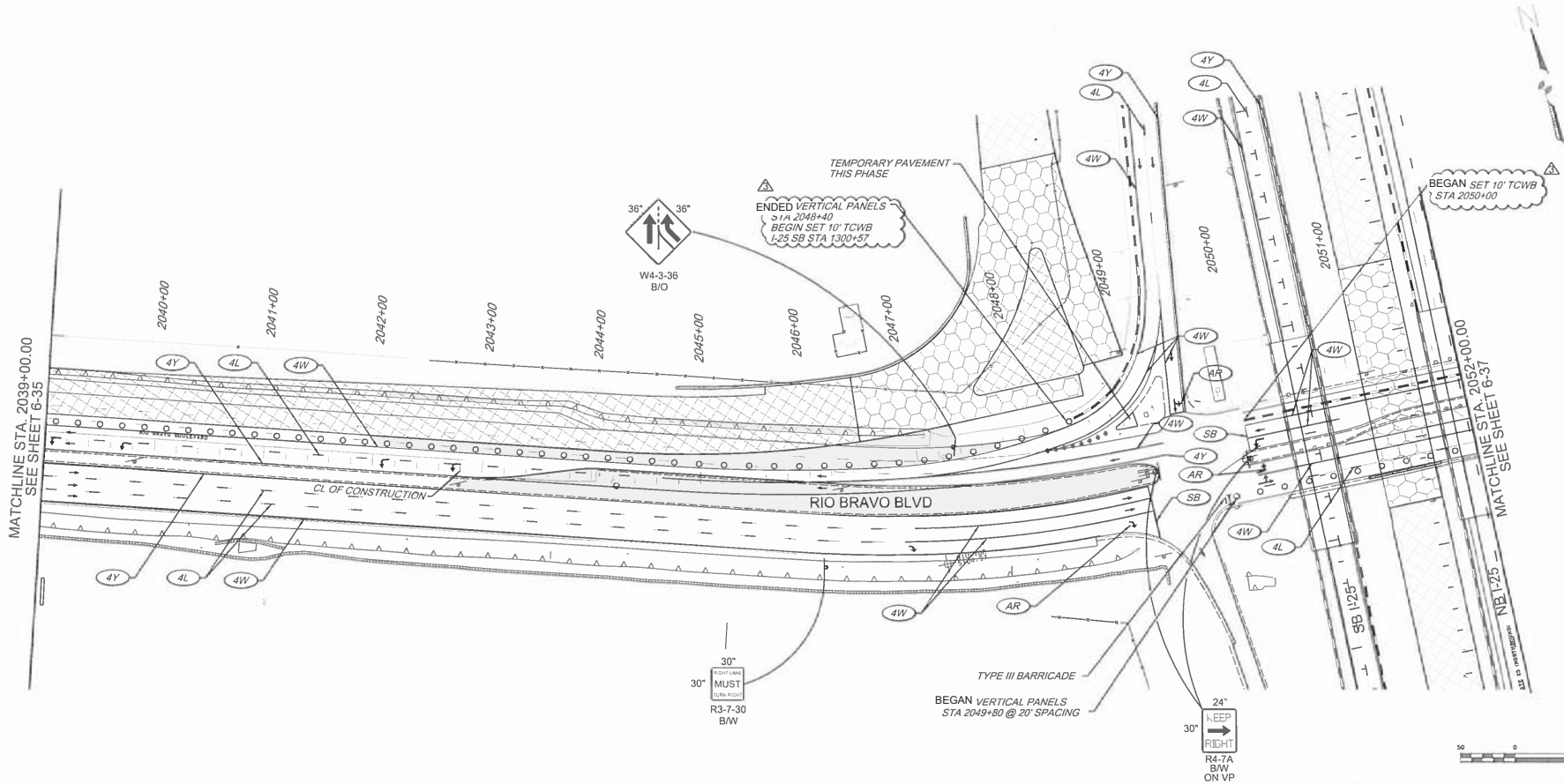


NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 1

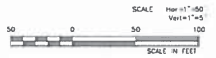
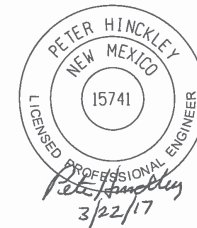
DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
1501 BBS - 7150
ALBUQUERQUE, NEW MEXICO 87110
PHONE: (505) 855-7150
FAX: (505) 855-7150
EMAIL: AECOM@AECOM.COM





- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
1	ADDED BARRIER TYPE CLARIFICATION	01/18	
2			
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REVISIONS (OR CHANGE NOTICES)

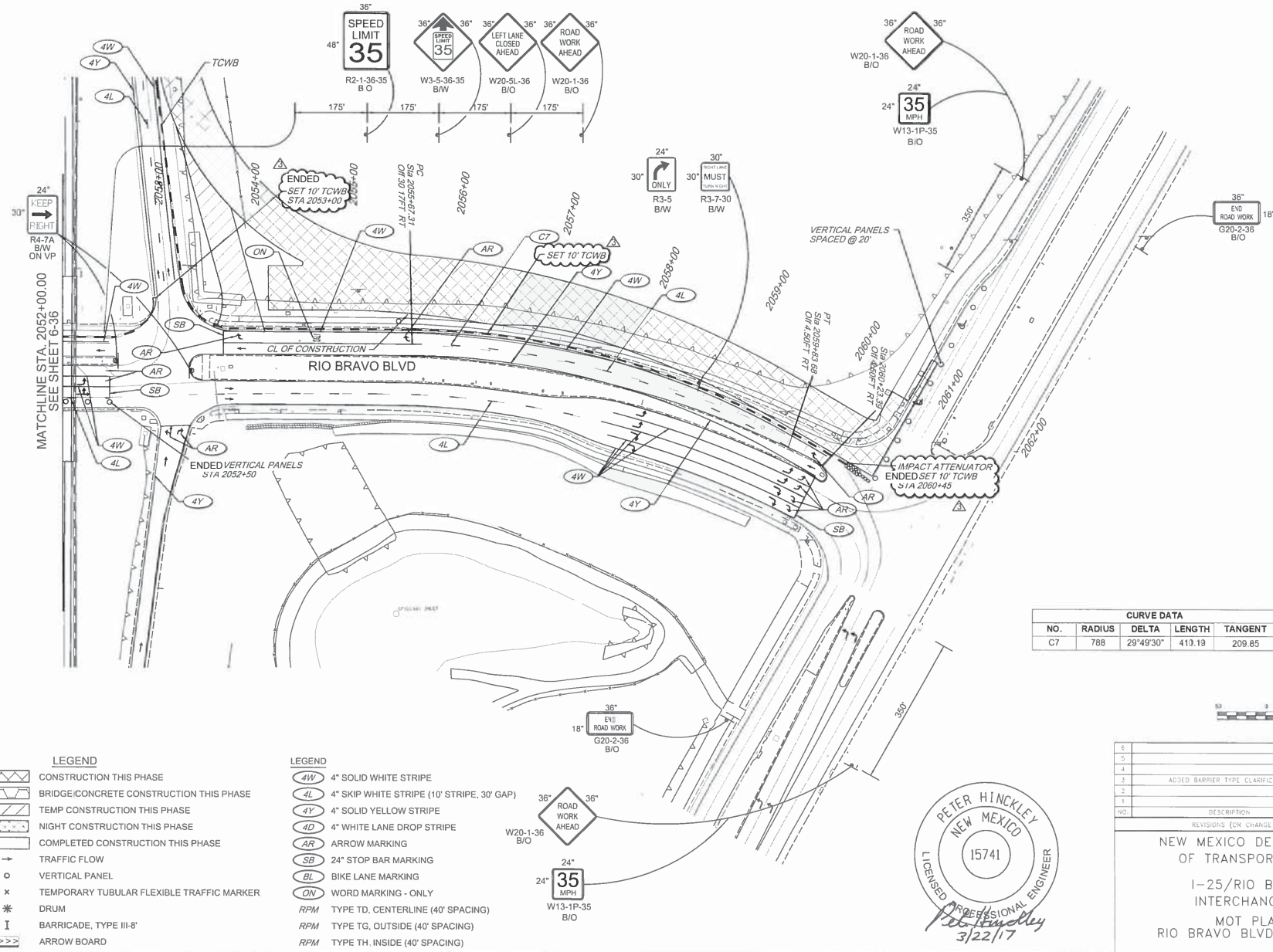
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

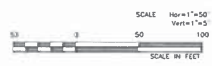
MOT PLAN
RIO BRAVO BLVD-PHASE 1



NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 1

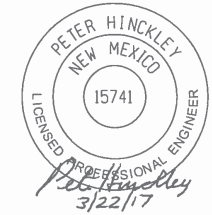


CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C7	788	29°49'30"	410.19	209.85



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8"
 - ARROW BOARD

- LEGEND**
- 4W 4" SOLID WHITE STRIPE
 - 4L 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4Y 4" SOLID YELLOW STRIPE
 - 4D 4" WHITE LANE DROP STRIPE
 - AR ARROW MARKING
 - SB 24" STOP BAR MARKING
 - BL BIKE LANE MARKING
 - ON WORD MARKING - ONLY
 - RPM TYPE TD, CENTERLINE (40' SPACING)
 - RPM TYPE TG, OUTSIDE (40' SPACING)
 - RPM TYPE TH, INSIDE (40' SPACING)



REVISIONS (OR CHANGE NOTICES)		
NO.	DESCRIPTION	DATE
6		
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3	ADDED BARRIER TYPE CLARIFICATION	DUB
2		
1		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25/RIO BRAVO INTERCHANGE		
MOT PLAN		
RIO BRAVO BLVD-PHASE 1		

DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 900
Albuquerque, New Mexico 87110
PHONE: (505) 855-7000
FAX: (505) 855-7000
EMAIL: AECOM_A300280_ND 8033328

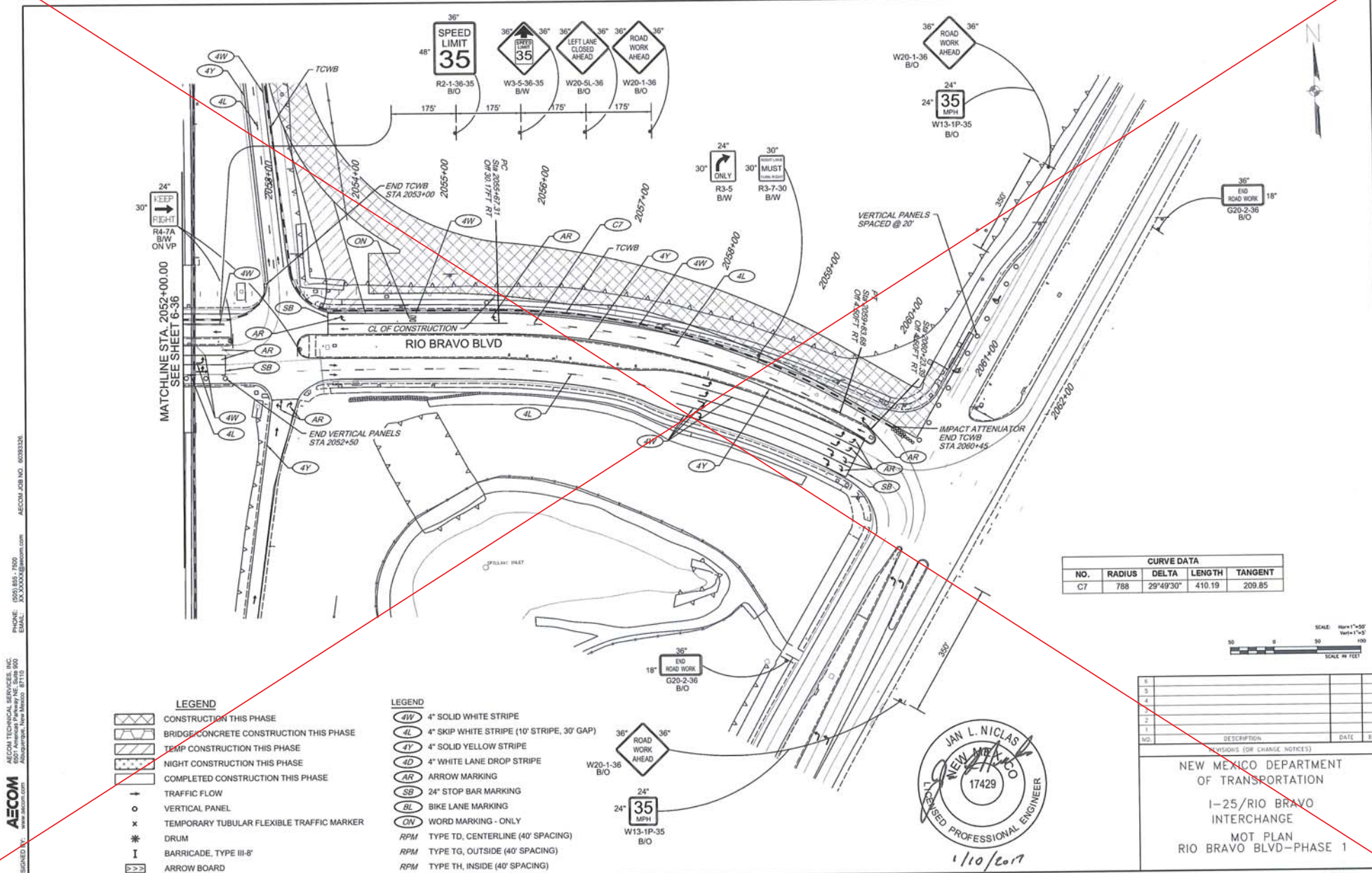
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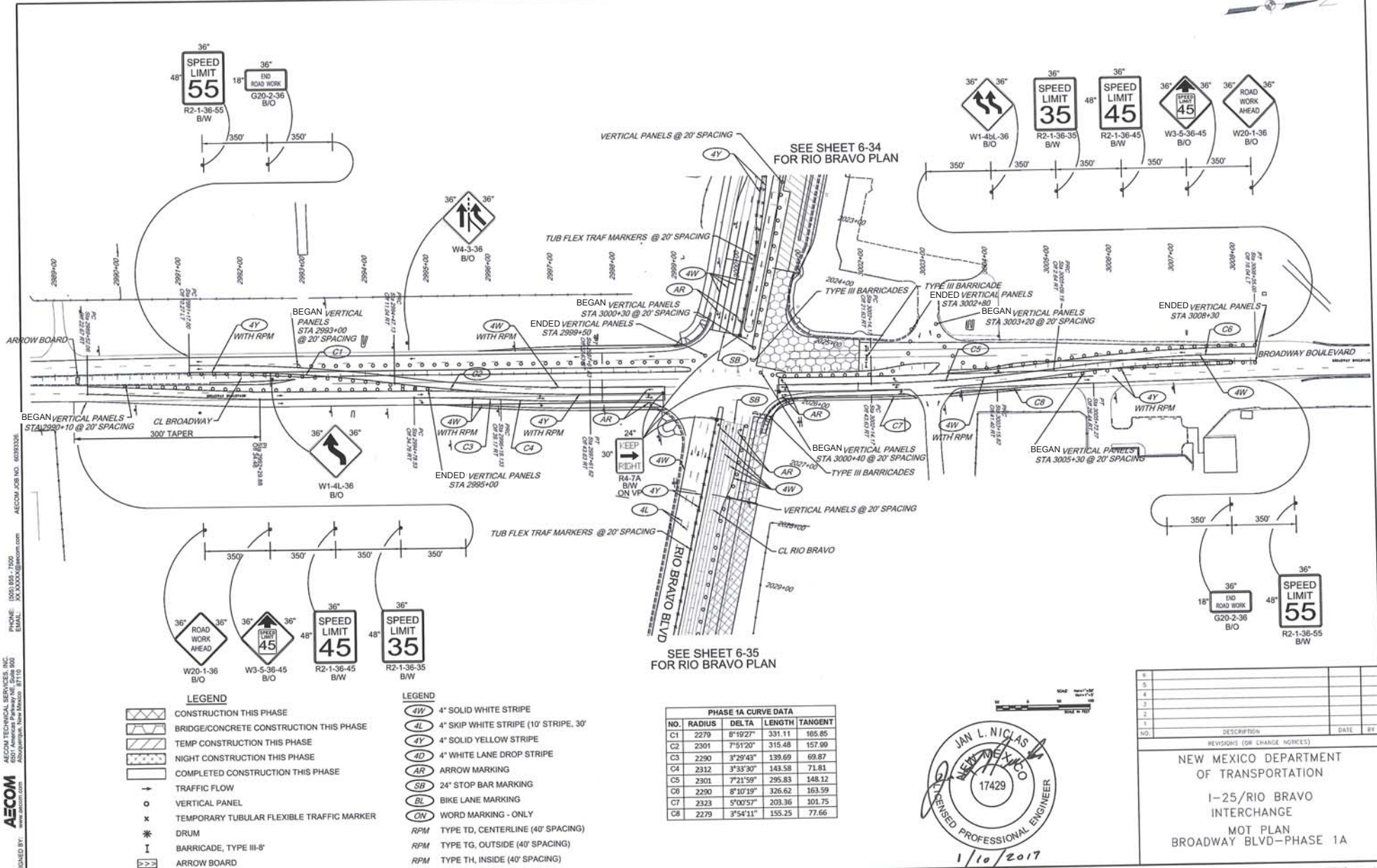
TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 900
Albuquerque, New Mexico 87110
T 505 855 7000

DESIGNED BY: AECOM
AECOM
www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 OF 27





- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S
 - ARROW BOARD
- LEGEND**
- 4W 4" SOLID WHITE STRIPE
 - 4L 4" SKIP WHITE STRIPE (10' STRIPE, 30'
 - 4Y 4" SOLID YELLOW STRIPE
 - 4D 4" WHITE LANE DROP STRIPE
 - AR ARROW MARKING
 - SB 24" STOP BAR MARKING
 - BL BIKE LANE MARKING
 - CN WORD MARKING - ONLY
 - RPM TYPE TD, CENTERLINE (40' SPACING)
 - RPM TYPE TG, OUTSIDE (40' SPACING)
 - RPM TYPE TH, INSIDE (40' SPACING)

PHASE 1A CURVE DATA

NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	2278	8°19'27"	331.11	105.85
C2	2201	7°51'20"	315.48	157.99
C3	2290	3°29'43"	139.69	69.87
C4	2312	3°33'30"	143.58	71.81
C5	2301	7°21'59"	295.83	148.12
C6	2290	8°10'19"	326.62	163.59
C7	2323	5°00'57"	203.36	101.75
C8	2279	3°54'11"	155.25	77.66



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		

NEW MEXICO DEPARTMENT OF TRANSPORTATION

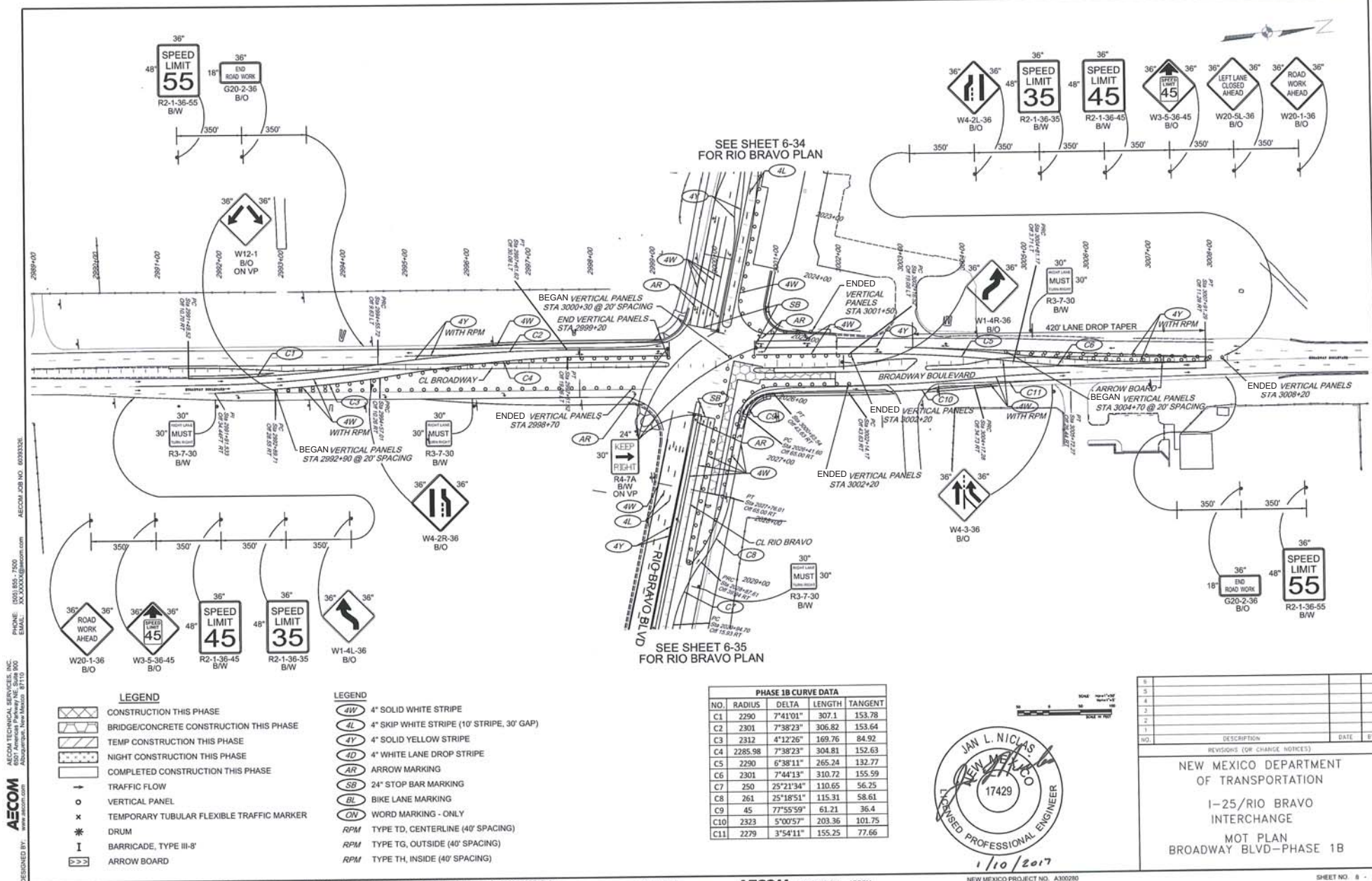
I-25/RIO BRAVO INTERCHANGE

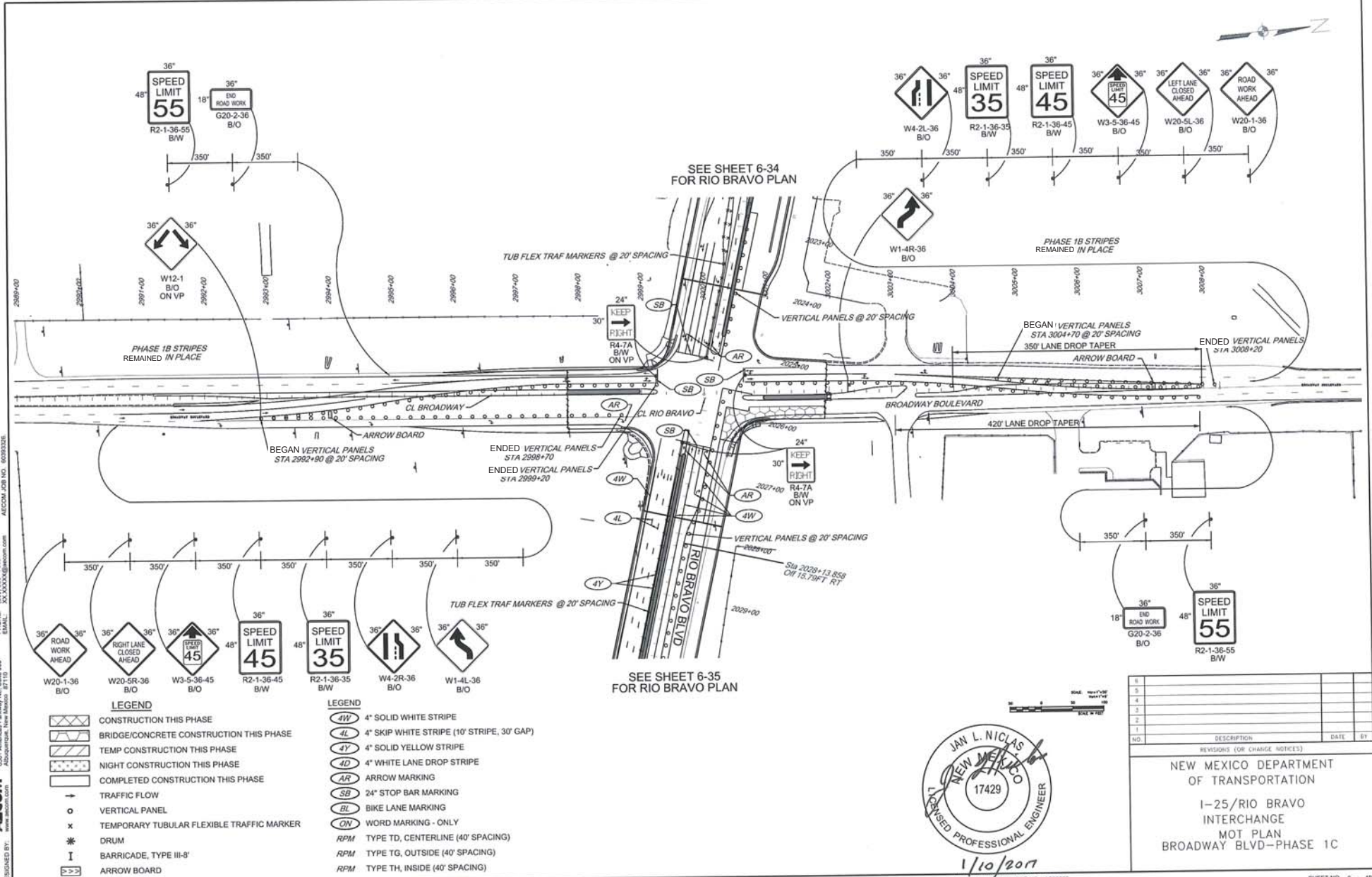
MOT PLAN

BROADWAY BLVD-PHASE 1A

DESIGNED BY: AECOM
 PHONE: (505) 854-7500
 FAX: (505) 854-7501
 EMAIL: AECOM@AECOM.COM
 AECOM TECHNICAL SERVICES, INC.
 Albuquerque, New Mexico 87110

1/10/2017





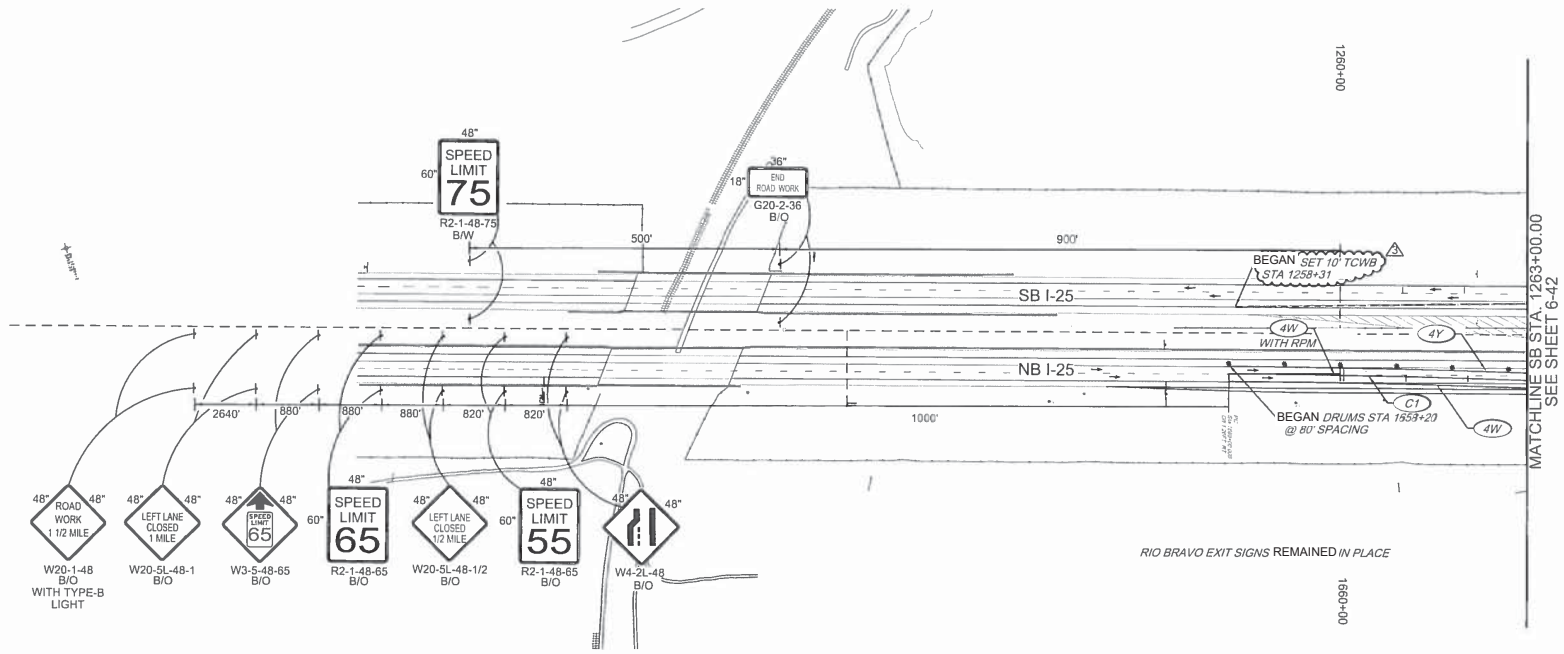
DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 1501 8th St., Suite 700
 Albuquerque, New Mexico 87102
 PHONE: 505.855.7500
 FAX: 505.855.7501
 EMAIL: aecom@aecom.com



1/10/2017
 NEW MEXICO PROJECT NO. A300280

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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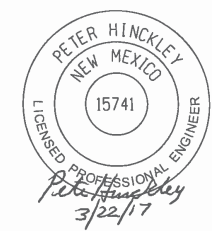
NEW MEXICO DEPARTMENT OF TRANSPORTATION
 I-25/RIO BRAVO INTERCHANGE
 MOT PLAN
 BROADWAY BLVD-PHASE 1C



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-B
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)

PHASE 1 CURVE DATA			
NO.	RADIUS	DELTA	LENGTH TANGENT
C1	17400	1°46'46"	540.44 270.24



NEW MEXICO DEPARTMENT OF TRANSPORTATION	
I-25/RIO BRAVO INTERCHANGE	
MOT PLAN I-25 PHASE 1	

DESIGNED BY AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 Americana Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 855-7000
 EMAIL: AECOM_JOB_NO. 6030320



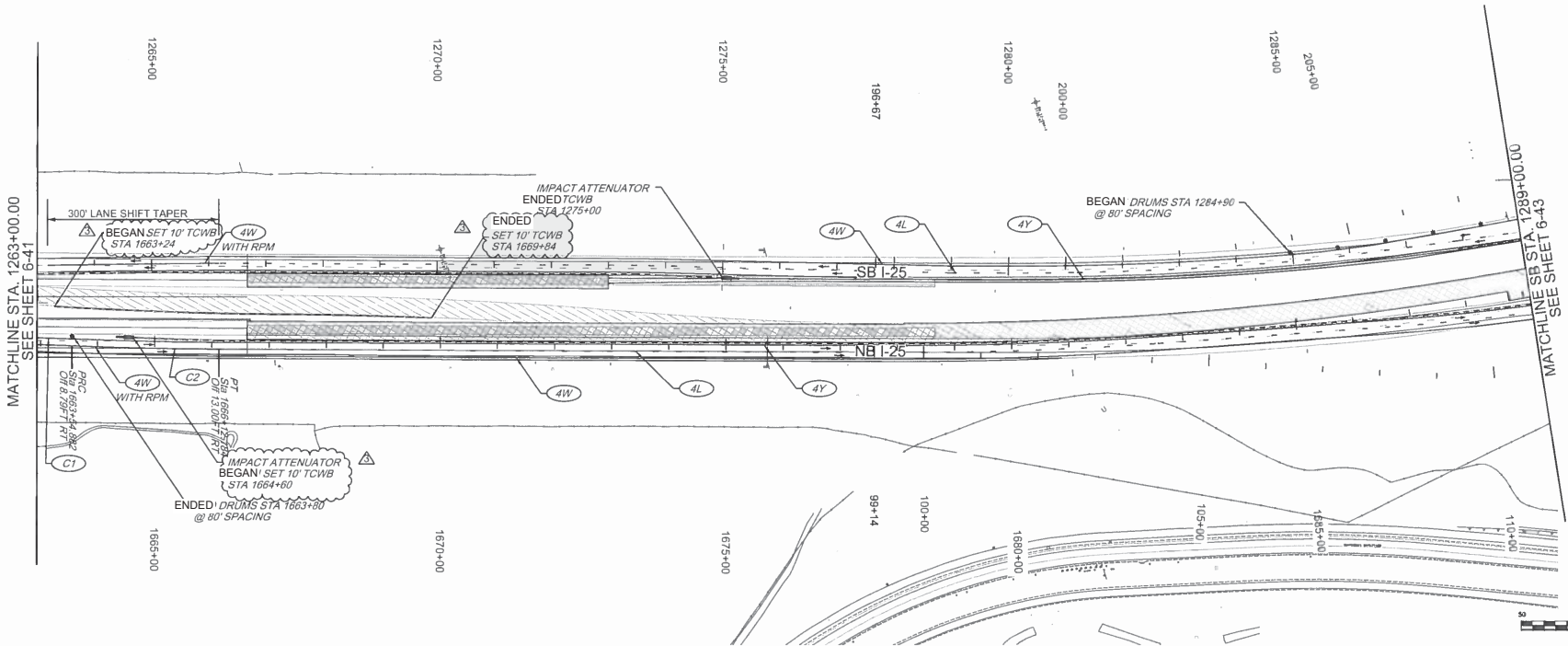
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
I-25 PHASE 1



1/10/2017

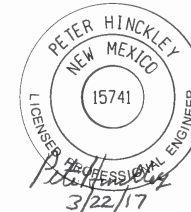
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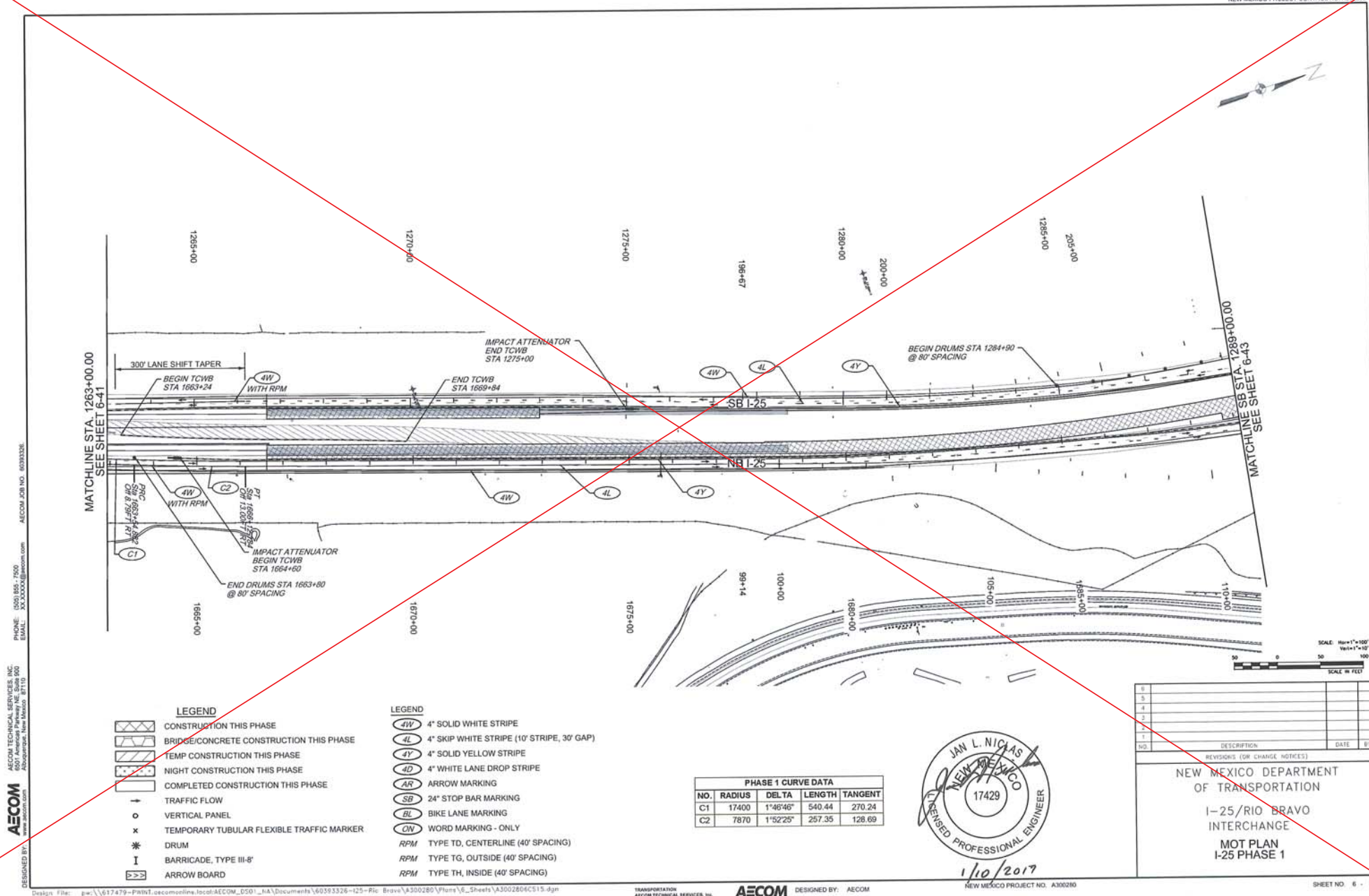
- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

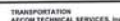
- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)

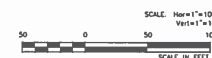
PHASE 1 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	17400	1°46'46"	540.44	270.24
C2	7870	1°52'25"	257.35	128.69



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3	ADDED BARRIER TYPE CLARIFICATION	8	7-15
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 1			



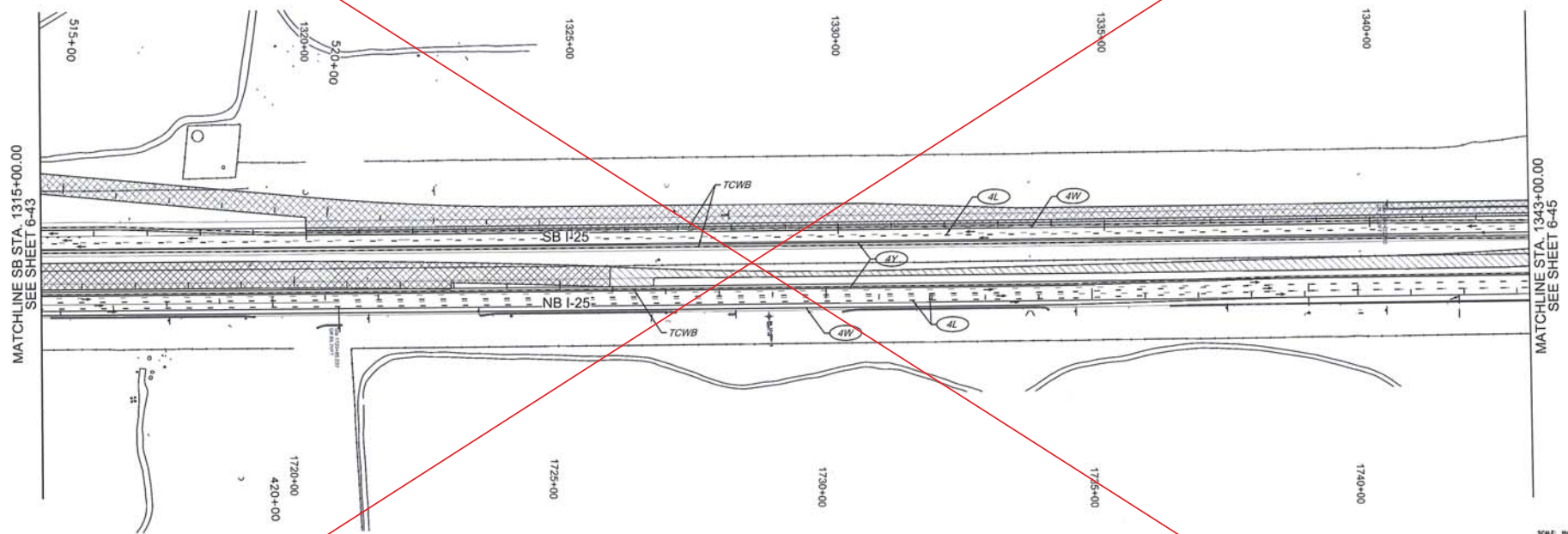


NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
I-25 PHASE 1



LEGEND	
<i>4W</i>	4" SOLID WHITE STRIPE
<i>4L</i>	4" SKIP WHITE STRIPE (10" STRIPE, 3½" GAP)
<i>4Y</i>	4" SOLID YELLOW STRIPE
<i>4D</i>	4" WHITE LANE DROP STRIPE
<i>AR</i>	ARROW MARKING
<i>SB</i>	24" STOP BAR MARKING
<i>BL</i>	BIKE LANE MARKING
<i>CN</i>	WORD MARKING - ONLY
<i>RPM</i>	TYPE TD, CENTERLINE (40" SPACING)
<i>RPM</i>	TYPE TG, OUTSIDE (40" SPACING)
<i>RPM</i>	TYPE TH, INSIDE (40" SPACING)



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-8'
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
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- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)

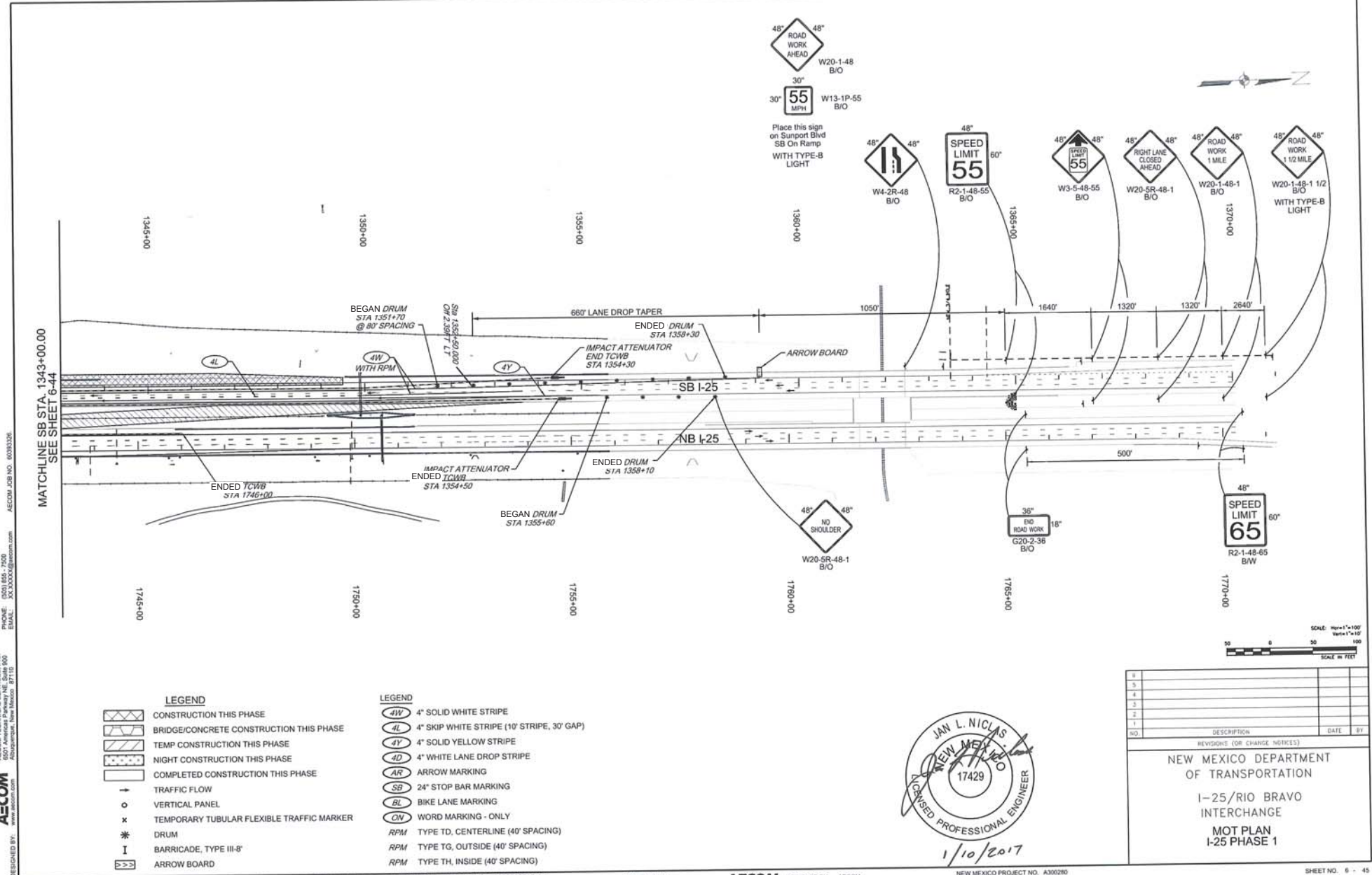
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

MOT PLAN
I-25 PHASE 1



1/10/2017



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

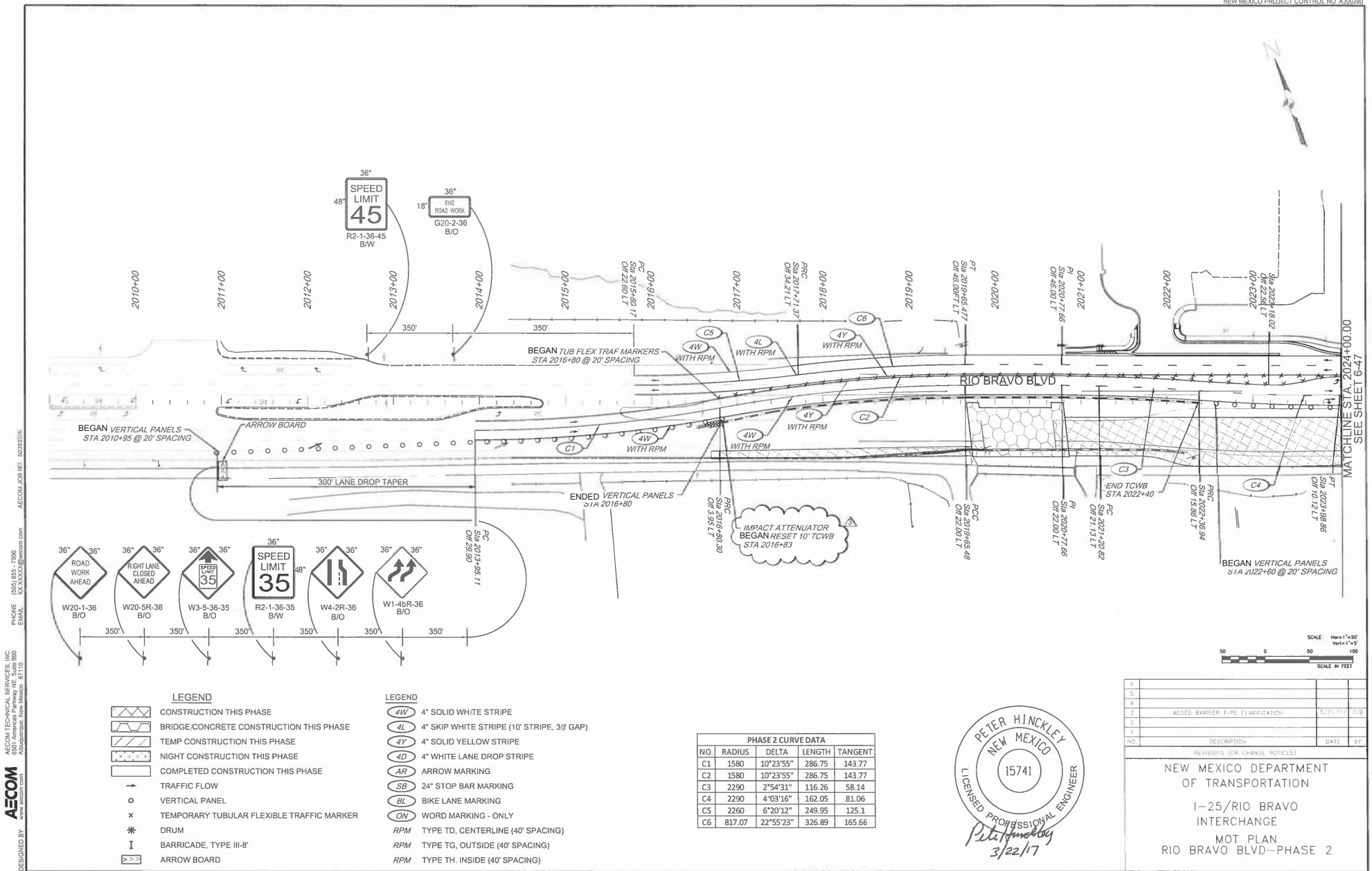
- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN I-25 PHASE 1

DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
PHONE: (505) 851-7500
FAX: (505) 851-7500
EMAIL: AECOM@AECOM.COM



DESIGNED BY: AECOM
www.aecom.com

ACCOM 208 W. 603938

PHONE: (505) 851-7000
FAX: (505) 851-7000
EMAIL: XXXXXX@aecom.com

ACCOM TECHNICAL SERVICES, INC.
6501 America Parkway NE, Suite 900
Albuquerque, New Mexico 87113

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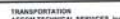
TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6501 America Parkway NE, Suite 900
Albuquerque, New Mexico 87113
1-505-851-7000

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 45







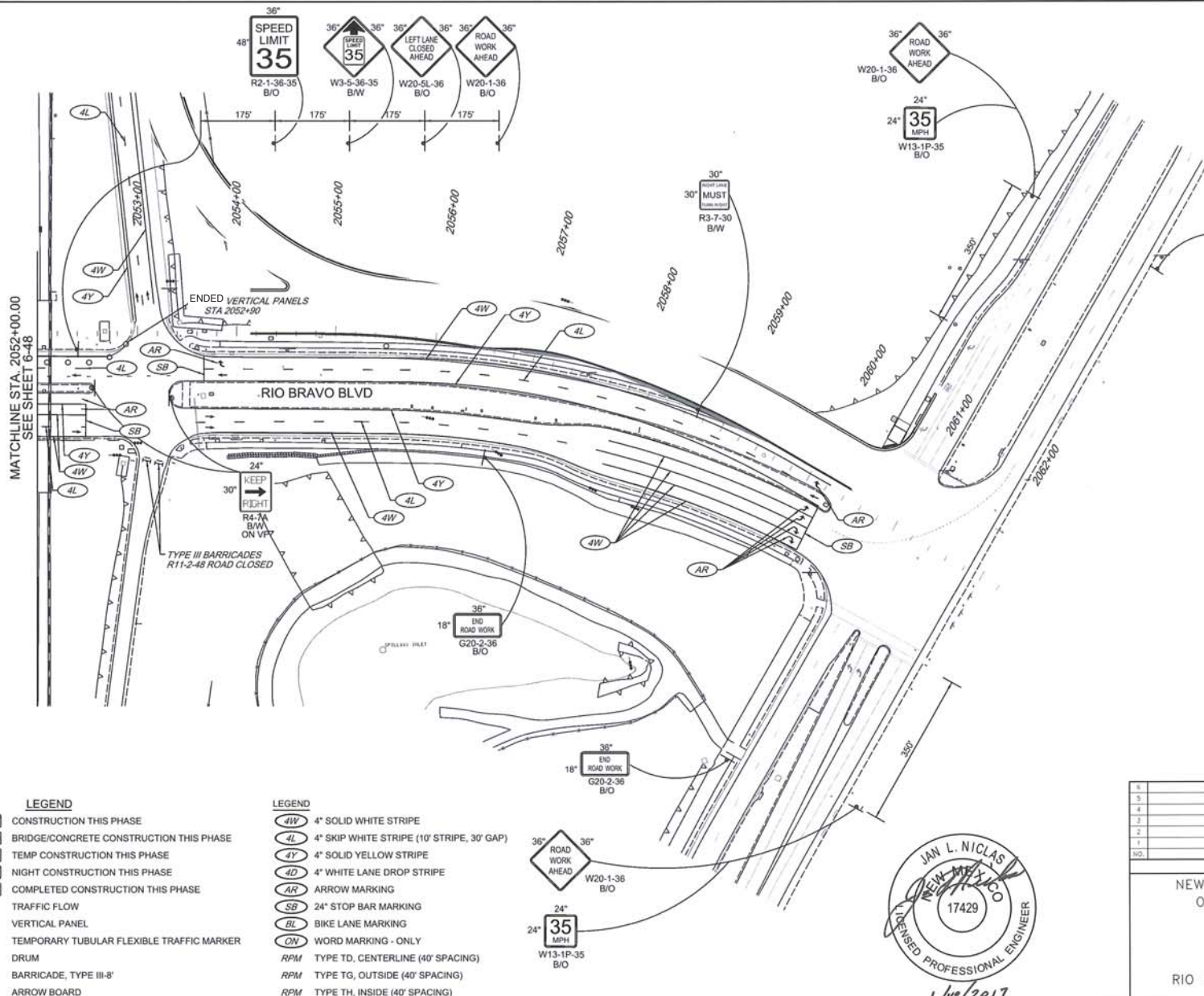
LEGEND	
<i>4W</i>	4" SOLID WHITE STRIPE
<i>4L</i>	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
<i>4Y</i>	4" SOLID YELLOW STRIPE
<i>4D</i>	4" WHITE LANE DROP STRIPE
<i>AR</i>	ARROW MARKING
<i>SB</i>	24" STOP BAR MARKING
<i>BL</i>	BIKE LANE MARKING
<i>CN</i>	WORD MARKING - ONLY
<i>RPM</i>	TYPE TD, CENTERLINE (40' SPACING)
<i>RPM</i>	TYPE TG, OUTSIDE (40' SPACING)
<i>RPM</i>	TYPE TH, INSIDE (40' SPACING)

PHASE 2 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGEN
C9	2290	8°05'11"	323.2	161.87
C10	954.47	21°12'37"	353.33	178.71
C11	2260	6°20'12"	249.95	125.1
C12	817.07	22°55'23"	326.89	165.66
C13	717.11	11°15'13"	140.85	70.65
C14	150	87°47'03"	229.82	144.31



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NO.	DESCRIPTION	DATE	

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 2



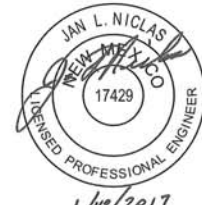
- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S'
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 2



1/10/2017

DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
6901 Avenida Parkway NE, Suite 500
Albuquerque, New Mexico 87113
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EMAIL: AECOM@AECOM.COM

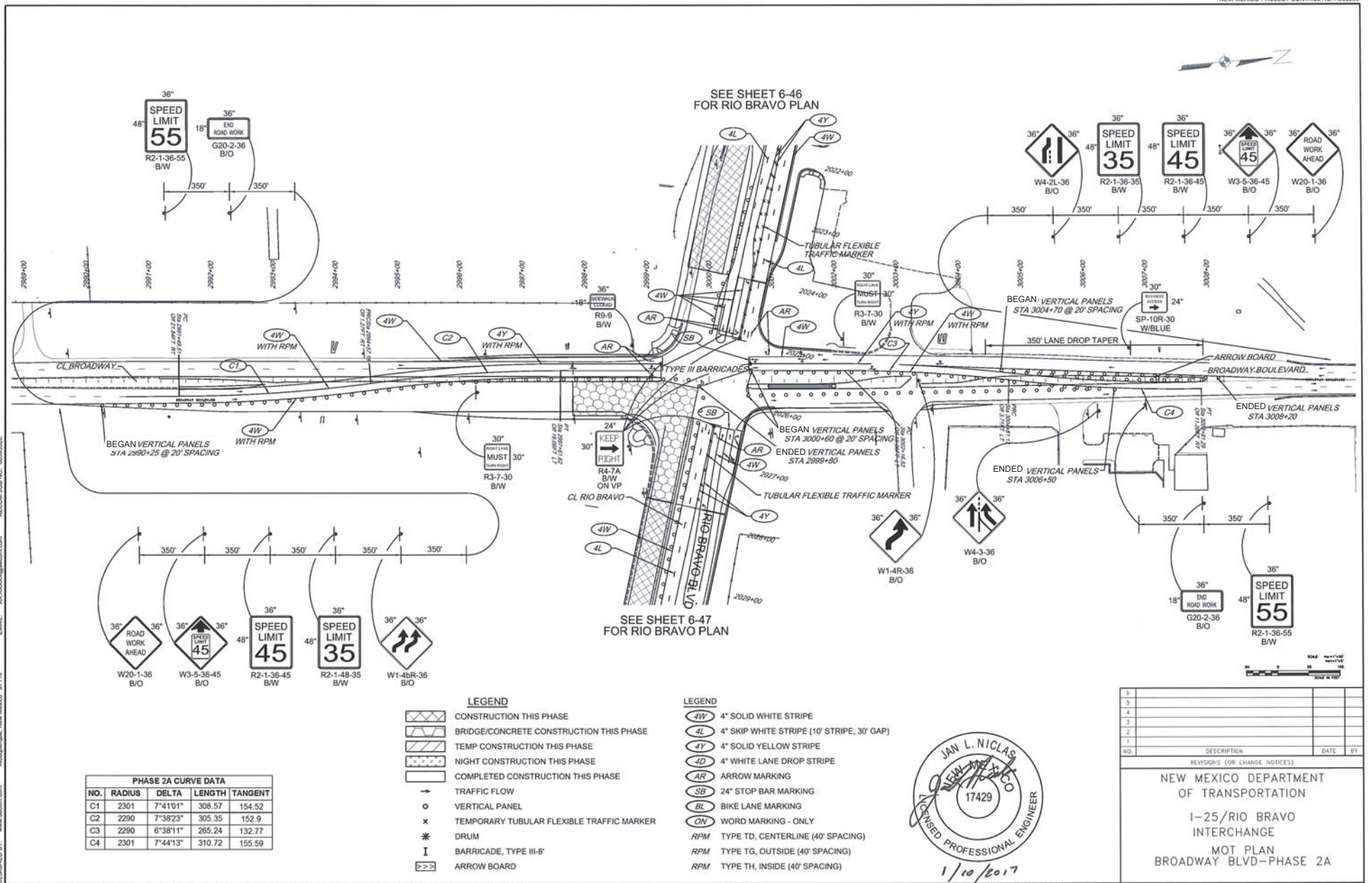
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TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6901 Avenida Parkway NE, Suite 500
Albuquerque, New Mexico 87113
PHONE: (505) 850-7500
FAX: (505) 850-7500
EMAIL: AECOM@AECOM.COM

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 49



DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 Avenida Parkway NE, Suite 500
 Albuquerque, New Mexico 87110
 PHONE: (505) 885-7500
 EMAIL: XXXXXXX@aecom.com
 AECOM JOB NO. 6039326

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TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 Avenida Parkway NE, Suite 500
 Albuquerque, New Mexico 87110
 T 505.885.7500

DESIGNED BY: AECOM
 AECOM



1/10/2017

NEW MEXICO PROJECT NO. A300280

NO.	DESCRIPTION	DATE	BY
1	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)

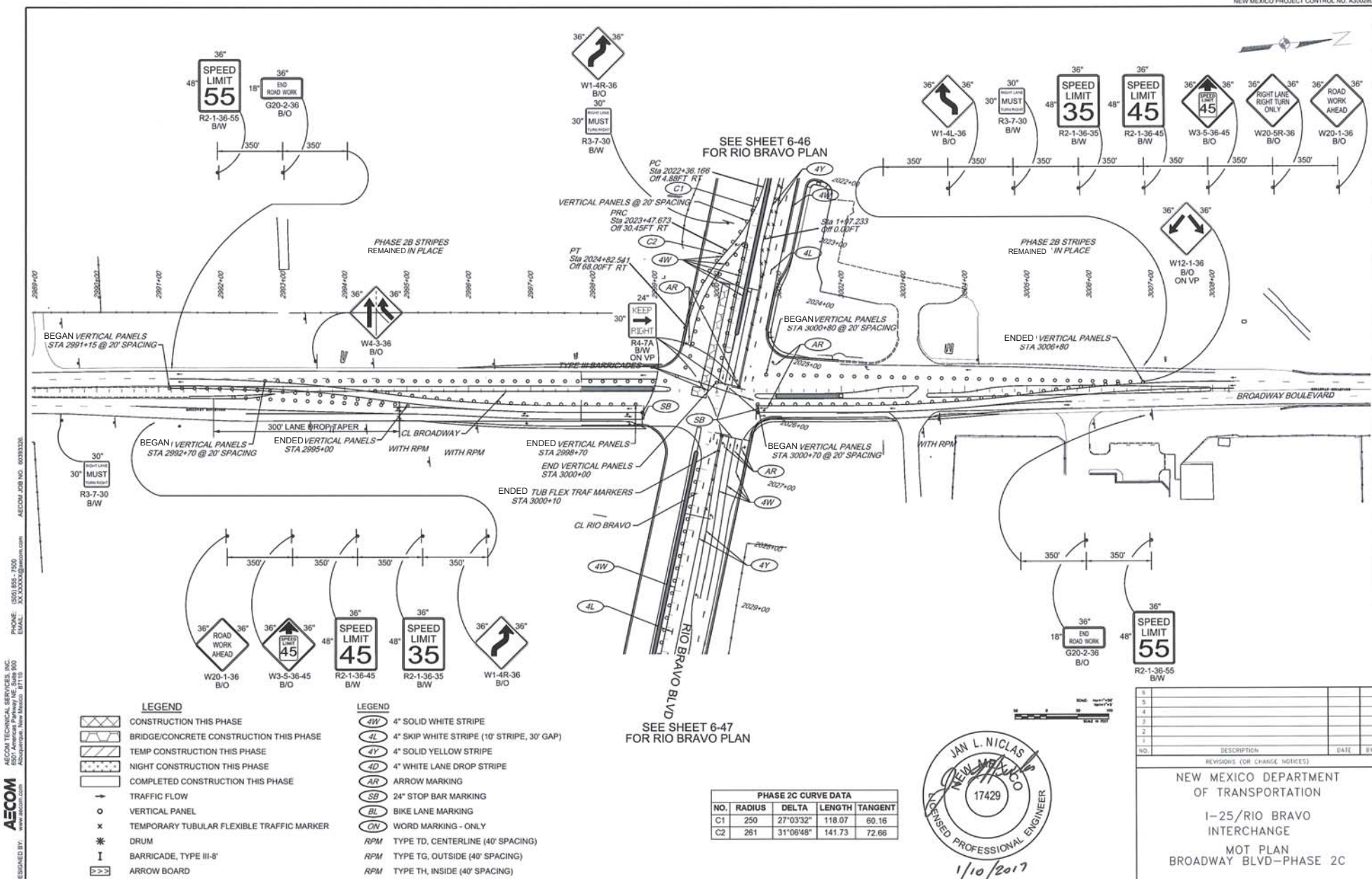
NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

MOT PLAN BROADWAY BLVD-PHASE 2A

SHEET NO. 6 - 50





DESIGNED BY: AECOM
www.aecom.com

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Albuquerque, New Mexico 87113

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FAX: (505) 855-7500
EMAIL: AECOM@aecom.com

AECOM JOB NO. 60333336

DESIGNED BY: AECOM

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TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 500
Albuquerque, New Mexico 87113
1-800-855-2866

AECOM
www.aecom.com

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280

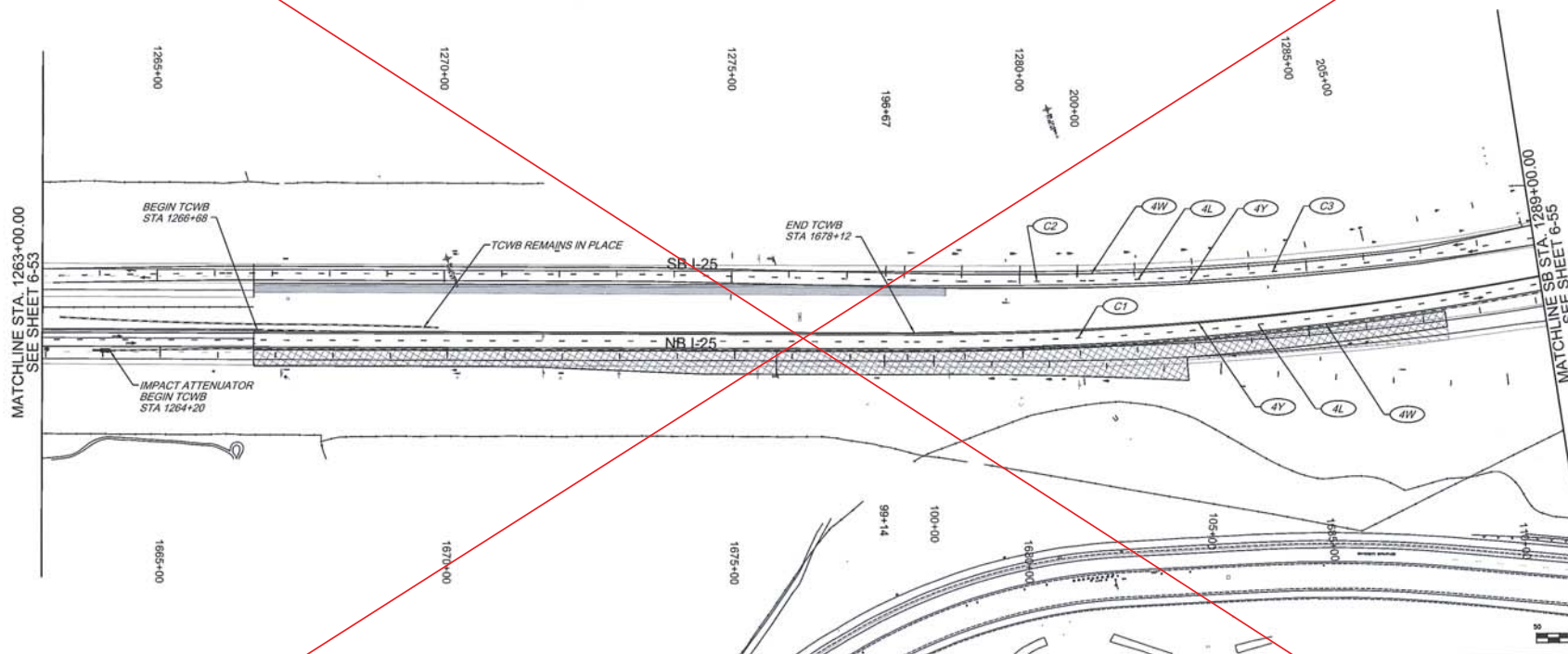
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MOT PLAN
I-25 PHASE 2

1/10/2017



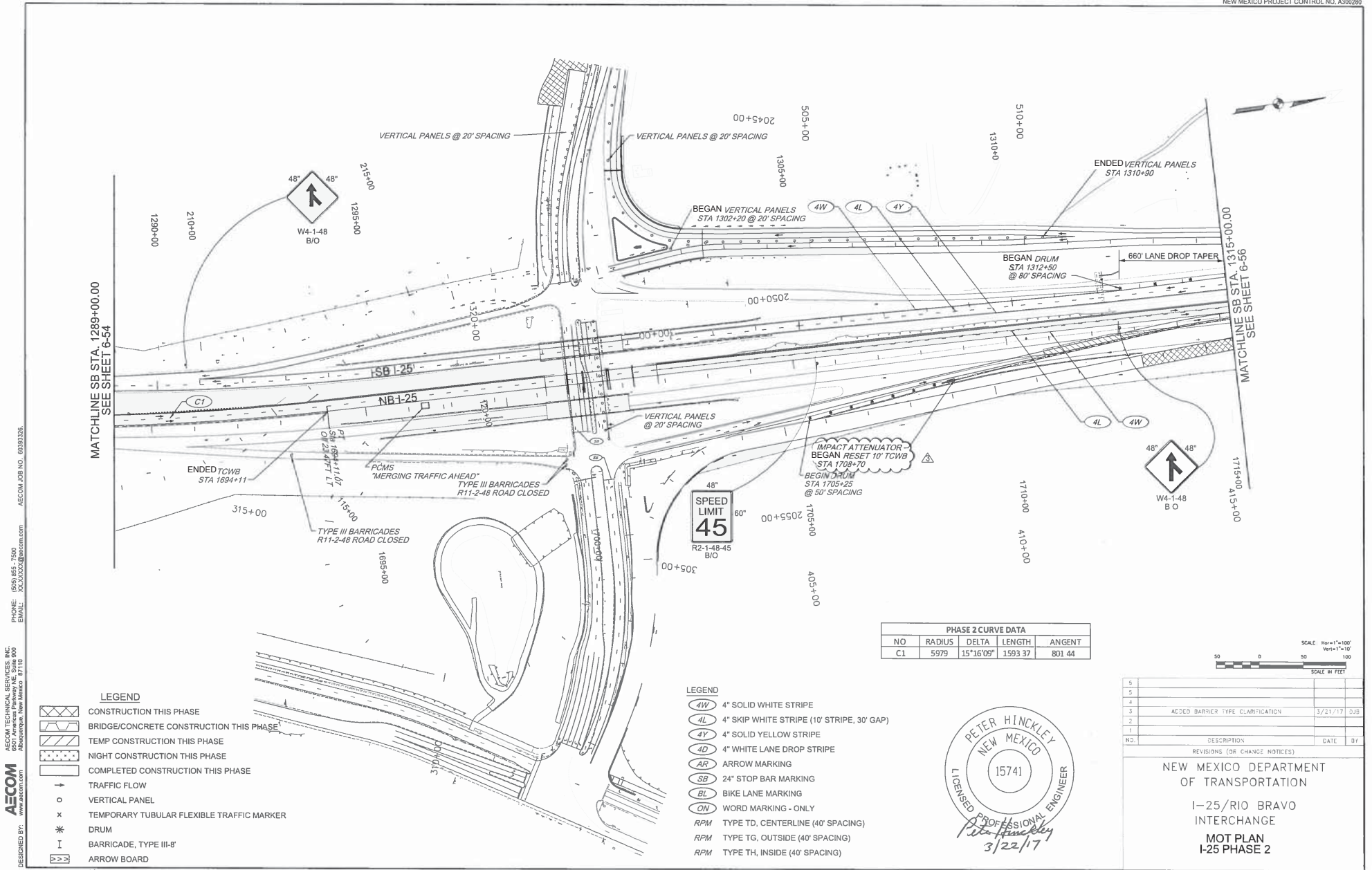


LEGEND	
	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-8'
	ARROW BOARD

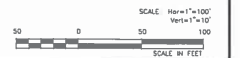
LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	RPM TYPE TD, CENTERLINE (40' SPACING)
	RPM TYPE TG, OUTSIDE (40' SPACING)
	RPM TYPE TH, INSIDE (40' SPACING)



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NO.	DESCRIPTION	DATE
REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION I-25/RIO BRAVO INTERCHANGE MOT PLAN I-25 PHASE 2		



PHASE 2 CURVE DATA				
NO	RADIUS	DELTA	LENGTH	ANGENT
C.1	5979	15°16'09"	1593.37	801.44

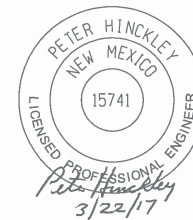


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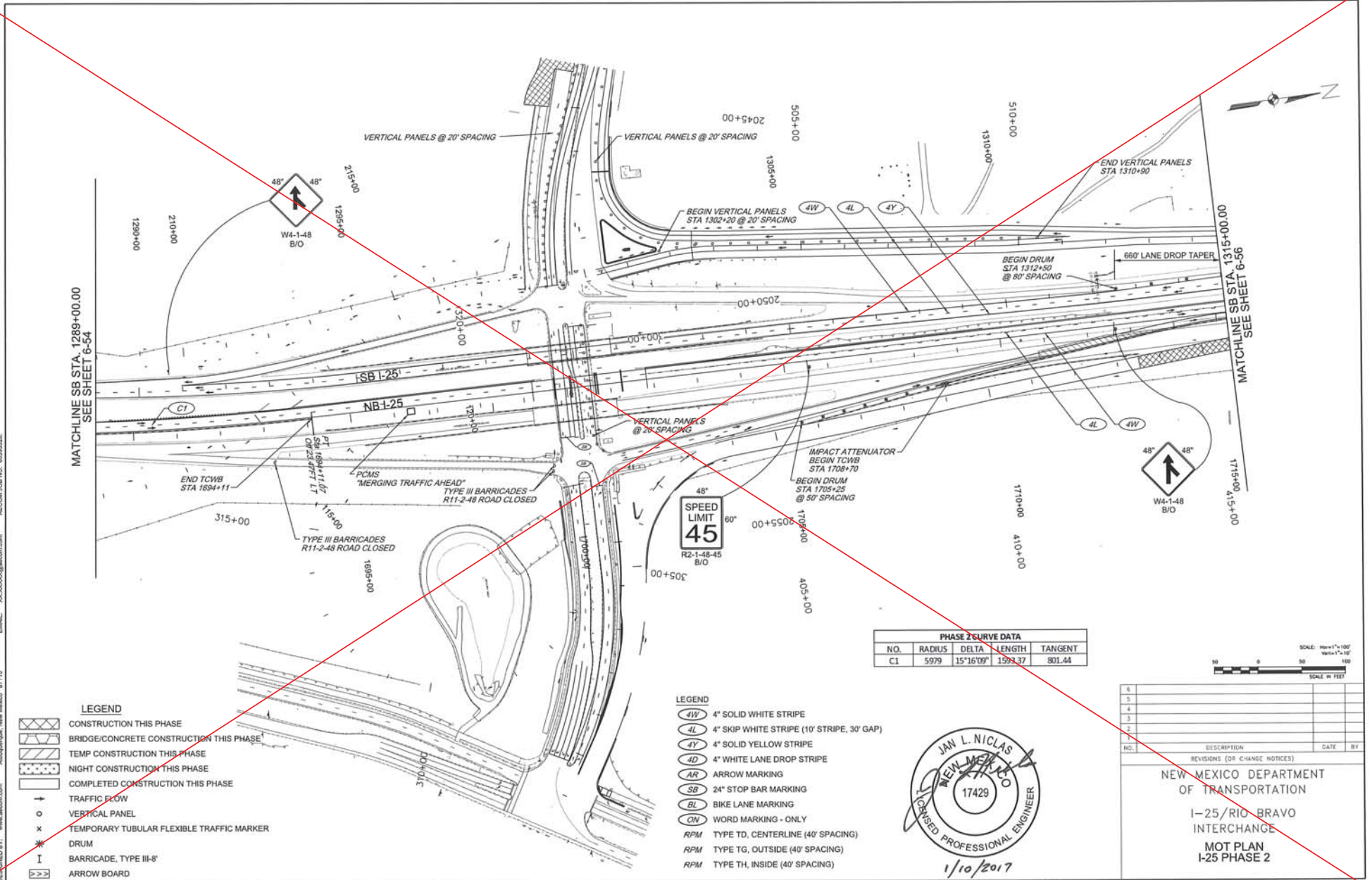
- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-8
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
- 24" STOP BAR MARKING
- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
6			
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3	ADDED BARRIER TYPE CLARIFICATION	3/21/17	DJS
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 2			



MATCHLINE SB STA. 1289+00.00
SEE SHEET 6-54

MATCHLINE SB STA. 1315+00.00
SEE SHEET 6-56

PHASE 2 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	5979	15°16'09"	1593.37	801.44

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



REVISIONS (OR CHANGE NOTICES)		
NO.	DESCRIPTION	DATE BY
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN I-25 PHASE 2

AECOM TECHNICAL SERVICES, INC.
 6501 America Parkway NE, Suite 900
 Albuquerque, New Mexico 87113
 PHONE: (505) 855-7500
 FAX: (505) 855-7500
 EMAIL: AECOM_J28_NO. 6033326
 AECOM_J28_NO. 6033326

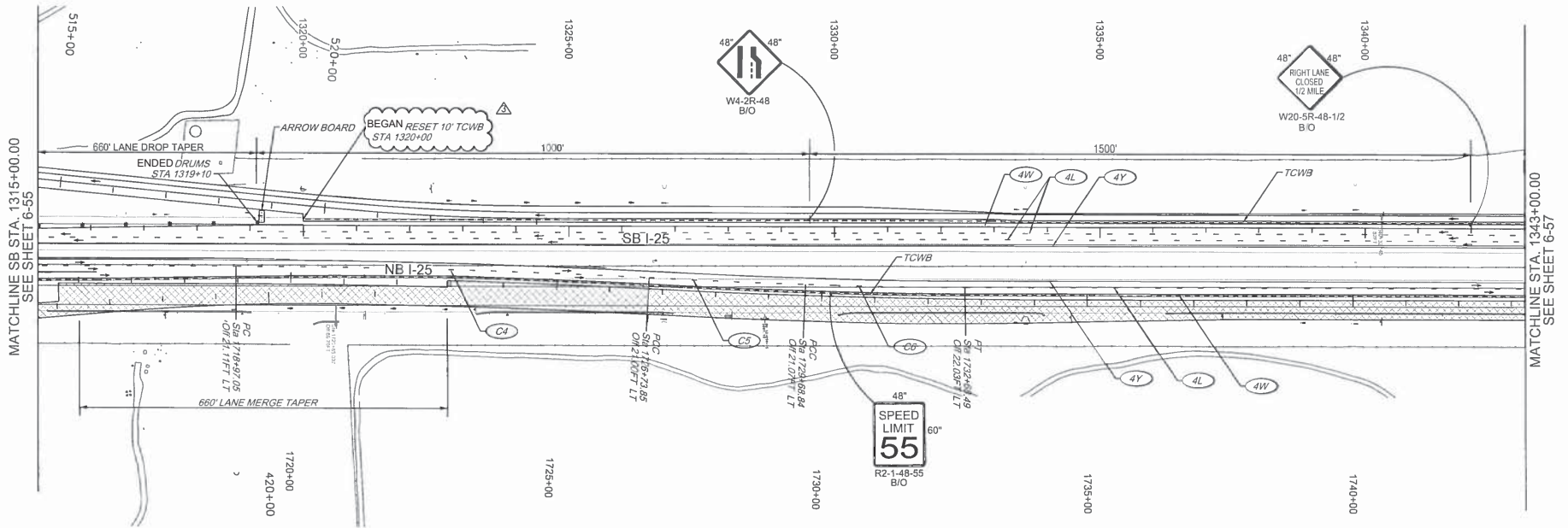
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TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6501 America Parkway NE, Suite 900
Albuquerque, New Mexico 87113
T 505.855.7500

AECOM DESIGNED BY: AECOM
www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 55



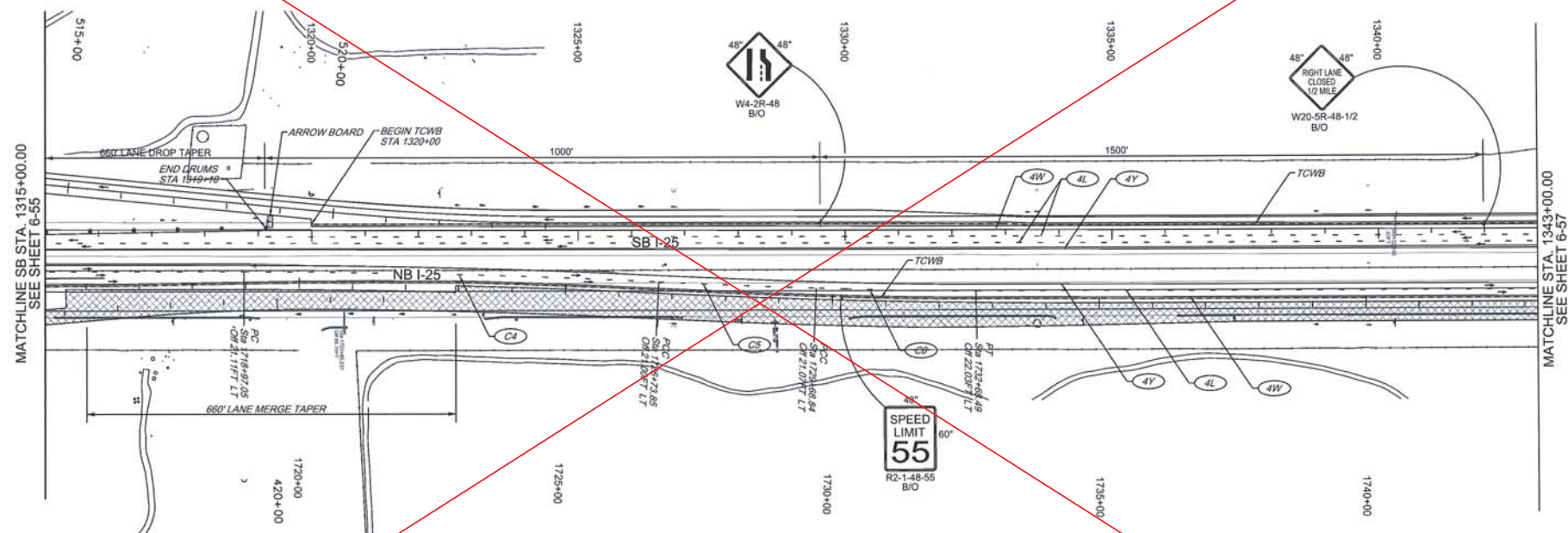
- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)

CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C4	15721	2°50'08"	777.84	389
C5	15679	1°04'36"	294.6	147.31
C6	7912	1°45'53"	243.69	121.86



DESCRIPTION	
VISIONS (CW CHAN)	
NEW MEXICO DEPARTMENT OF TRANSPORTATION	
I-25/RIO BRAVO INTERCHANGE	
MOT PLAN I-25 PHASE 2	



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S'
 - ARROW BOARD

- LEGEND**
- 4W 4" SOLID WHITE STRIPE
 - 4L 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4Y 4" SOLID YELLOW STRIPE
 - 4D 4" WHITE LANE DROP STRIPE
 - AR ARROW MARKING
 - SB 24" STOP BAR MARKING
 - BL BIKE LANE MARKING
 - OW WORD MARKING - ONLY
 - RPM TYPE TD, CENTERLINE (40' SPACING)
 - RPM TYPE TG, OUTSIDE (40' SPACING)
 - RPM TYPE TH, INSIDE (40' SPACING)

CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C4	15721	2°50'06"	777.84	389
C5	15679	1°04'36"	294.6	147.31
C6	7912	1°45'53"	243.69	121.86

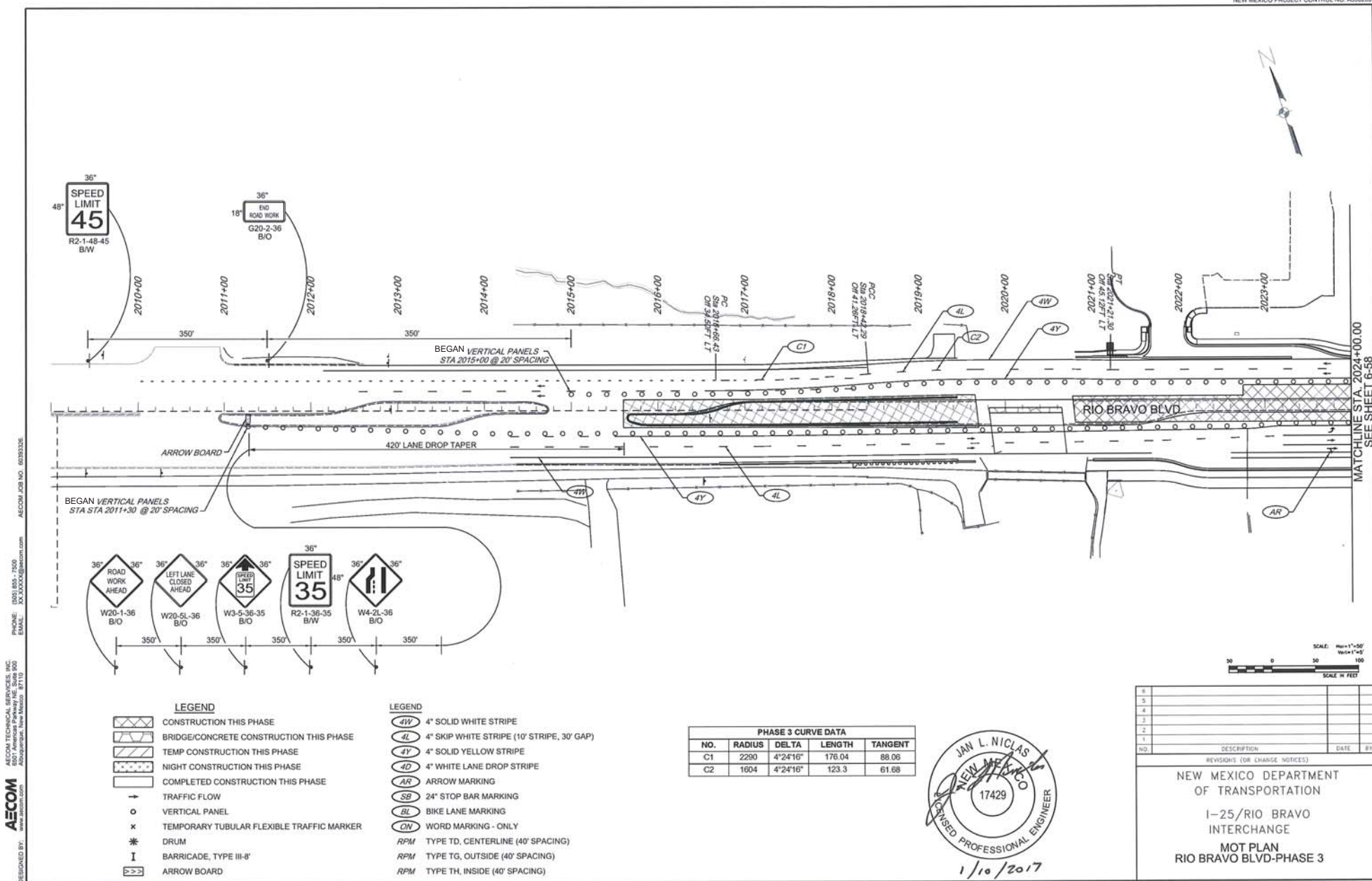


NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN I-25 PHASE 2







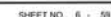
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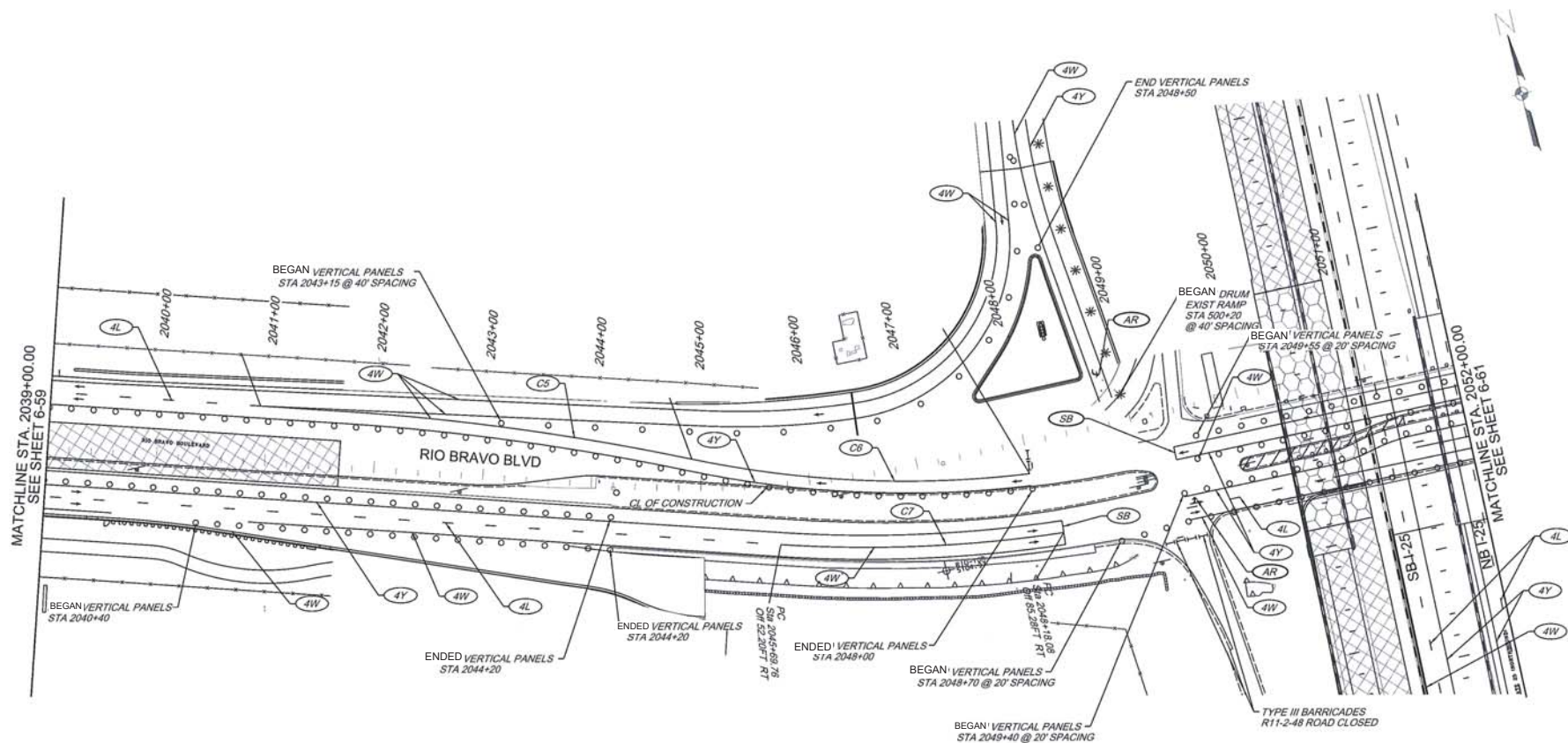
TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87113
 (505) 855-7500

AECOM
 DESIGNED BY: AECOM

JAN L. NICLAS
 17429
 LICENSED PROFESSIONAL ENGINEER
 1/10/2017

NEW MEXICO PROJECT CONTROL NO. A300280





LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-8'
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
- 24" STOP BAR MARKING
- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)

PHASE 3 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C5	2290	8°05'11"	323.2	161.87
C6	954.47	18°39'30"	310.82	156.8
C7	1211	12°16'46"	259.54	130.27



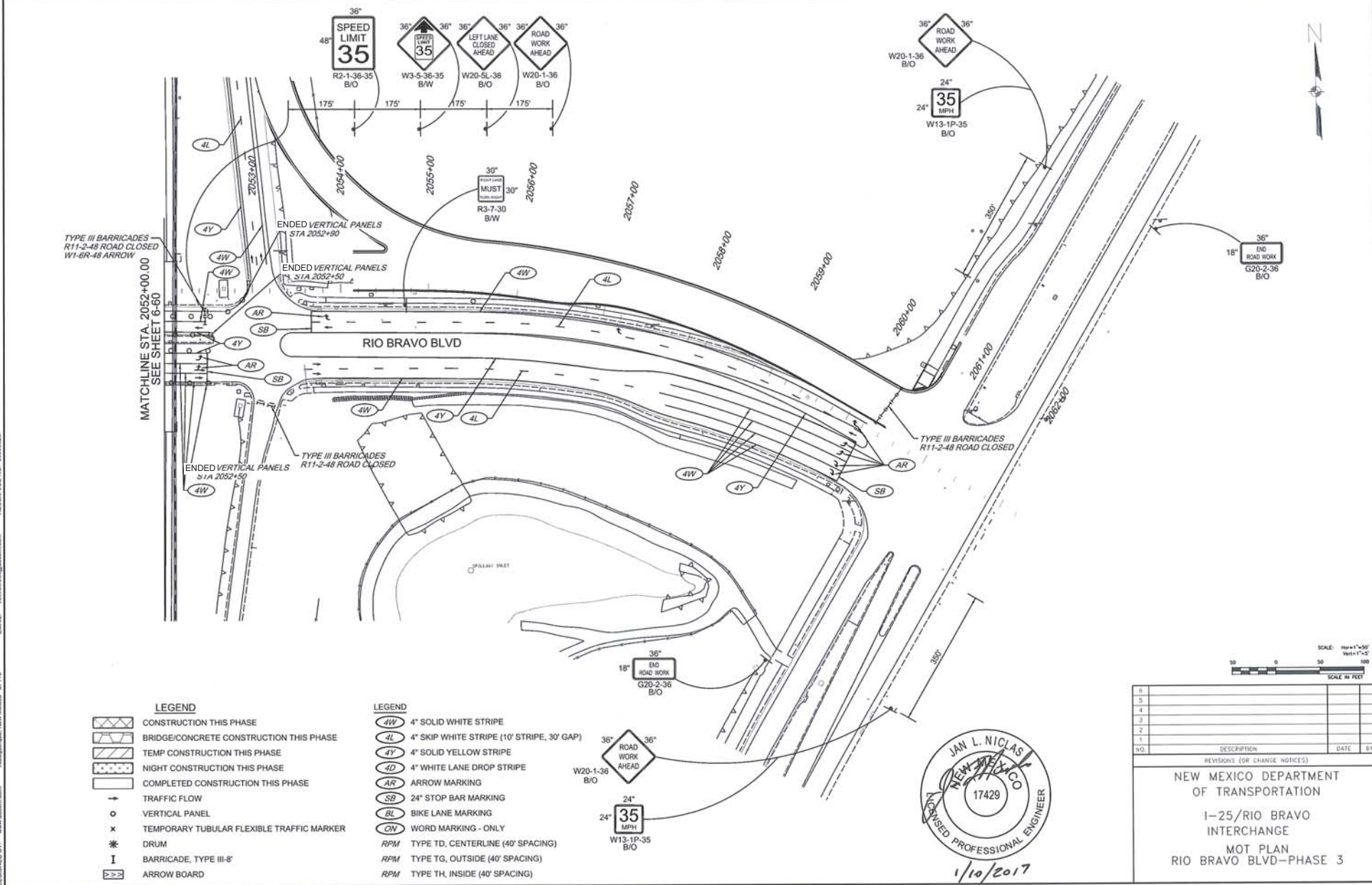
1/10/2017

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Vert=1"=5'
SCALE IN FEET

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN			
RIO BRAVO BLVD-PHASE 3			

PHONE: (505) 855-7500
FAX: (505) 855-7501
EMAIL: AECOM_208_NO_00000000

DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
1001 American Parkway NE, Suite 800
Albuquerque, New Mexico 87110



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-8'
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 3

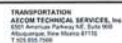
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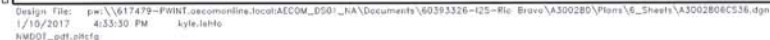
TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
300 Mountain Parkway NE, Suite 800
Albuquerque, New Mexico 87110
1-888-888-7268

AECOM DESIGNED BY: AECOM
www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 61







PHONE: (505) 855-7500
EMAIL: XX.XXXXX@att.net

AECOM TECHNICAL SERVICES, INC.
6501 Americas Parkway NE, Suite 900
Albuquerque, New Mexico 87110

AECOM
www.aecom.com

DESIGNED BY:

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TRANSPORTATION
AZCOM TECHNICAL SERVICES, Inc.
 6551 Americas Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 T 505.855.7500

AECOM DESIGNED BY: AECOM
www.aecom.com

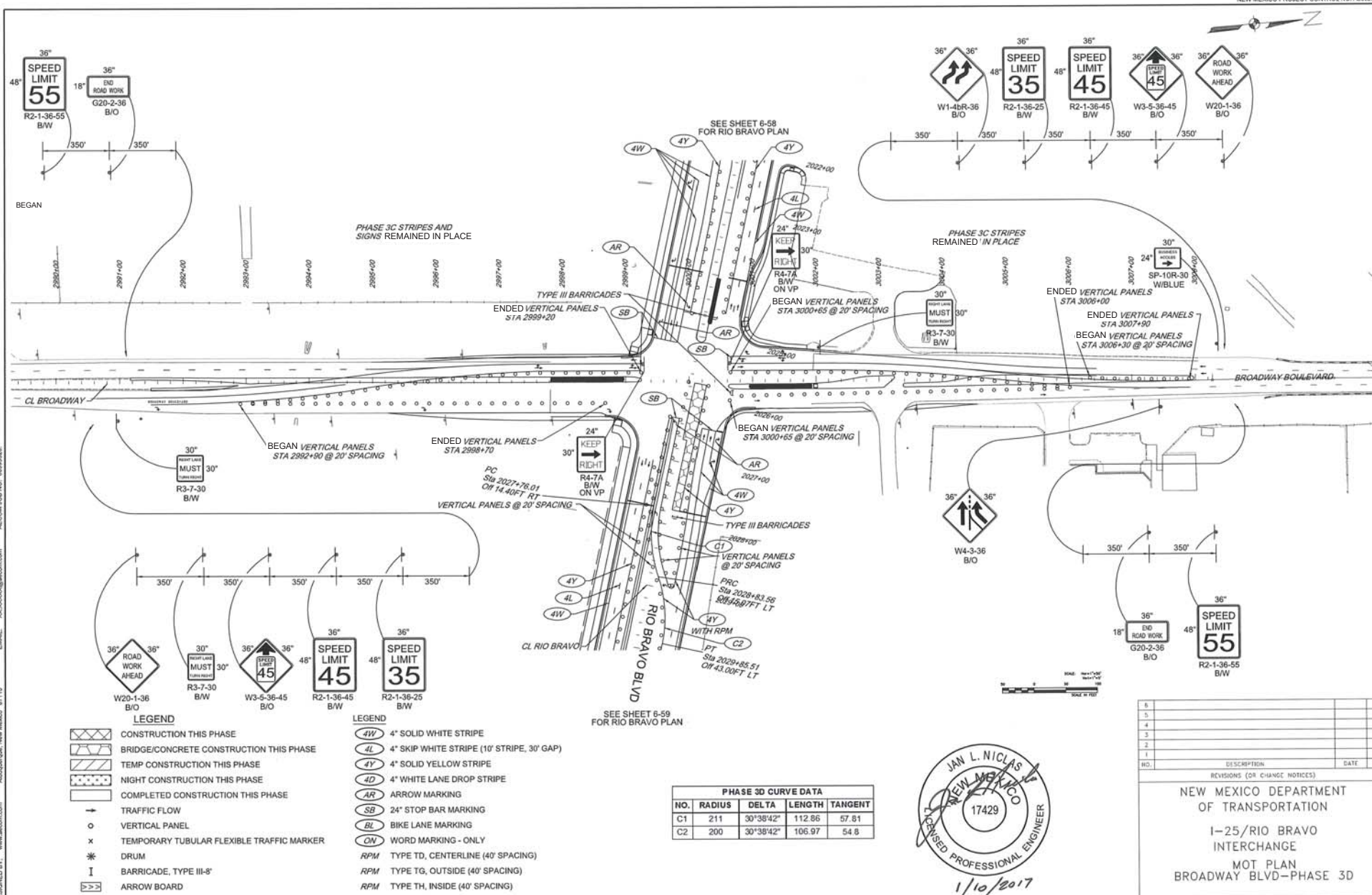
NEW MEXICO PROJECT NO. A300286

PHASE 3C CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGEN
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C2	2301	7°38'23"	306.82	153.64
C3	2301	6°38'11"	266.52	133.41
C4	2290	7°44'13"	309.23	154.85
C5	2312	5°00'16"	201.94	101.04



1/10/2017

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NO.	DESCRIPTION	DATE
REVISIONS (OR CHANGE NOTICE)		
<p>NEW MEXICO DEPARTMENT OF TRANSPORTATION</p> <p>I-25/RIO BRAVO INTERCHANGE</p> <p>MOT PLAN BROADWAY BLVD-PHASE 3C</p>		



PHONE: (505) 855-7500
EMAIL: kyle.jehta@aecom.com

DESIGNED BY: AECOM
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LEGEND	
	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-8'
	ARROW BOARD

LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)

PHASE 3 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	7870	2°17'44"	315.33	157.69



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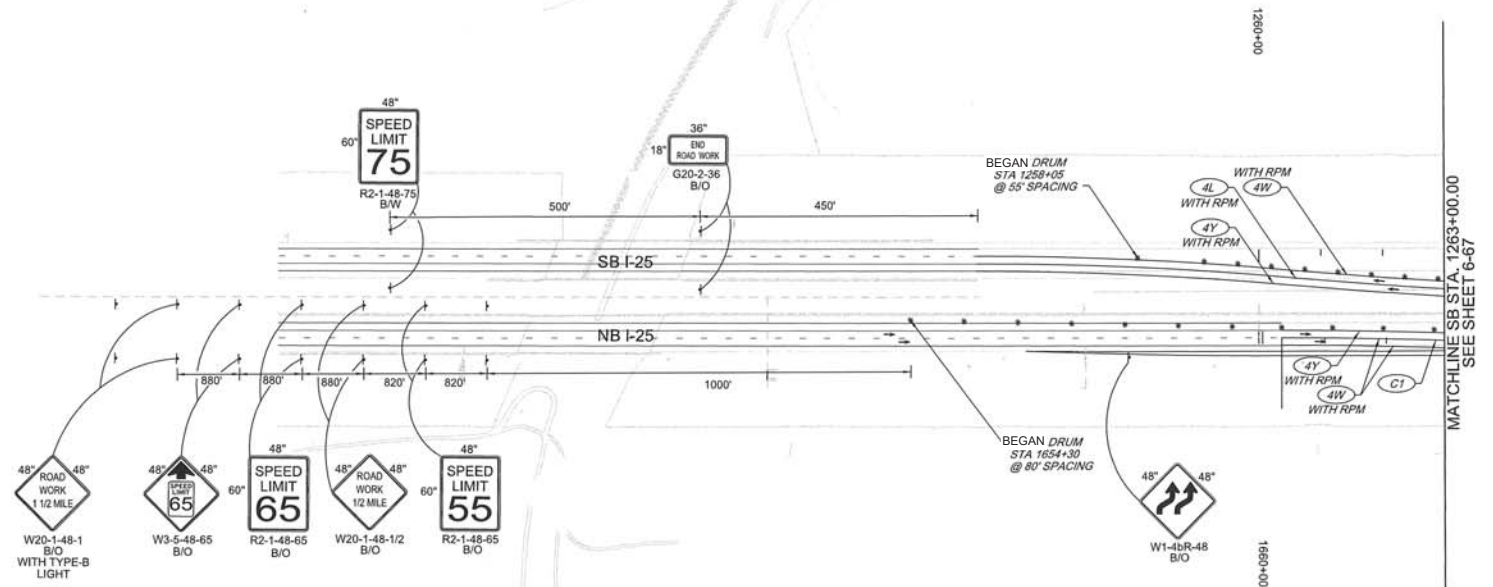
TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 900
Albuquerque, New Mexico 87110
1-800-541-7800

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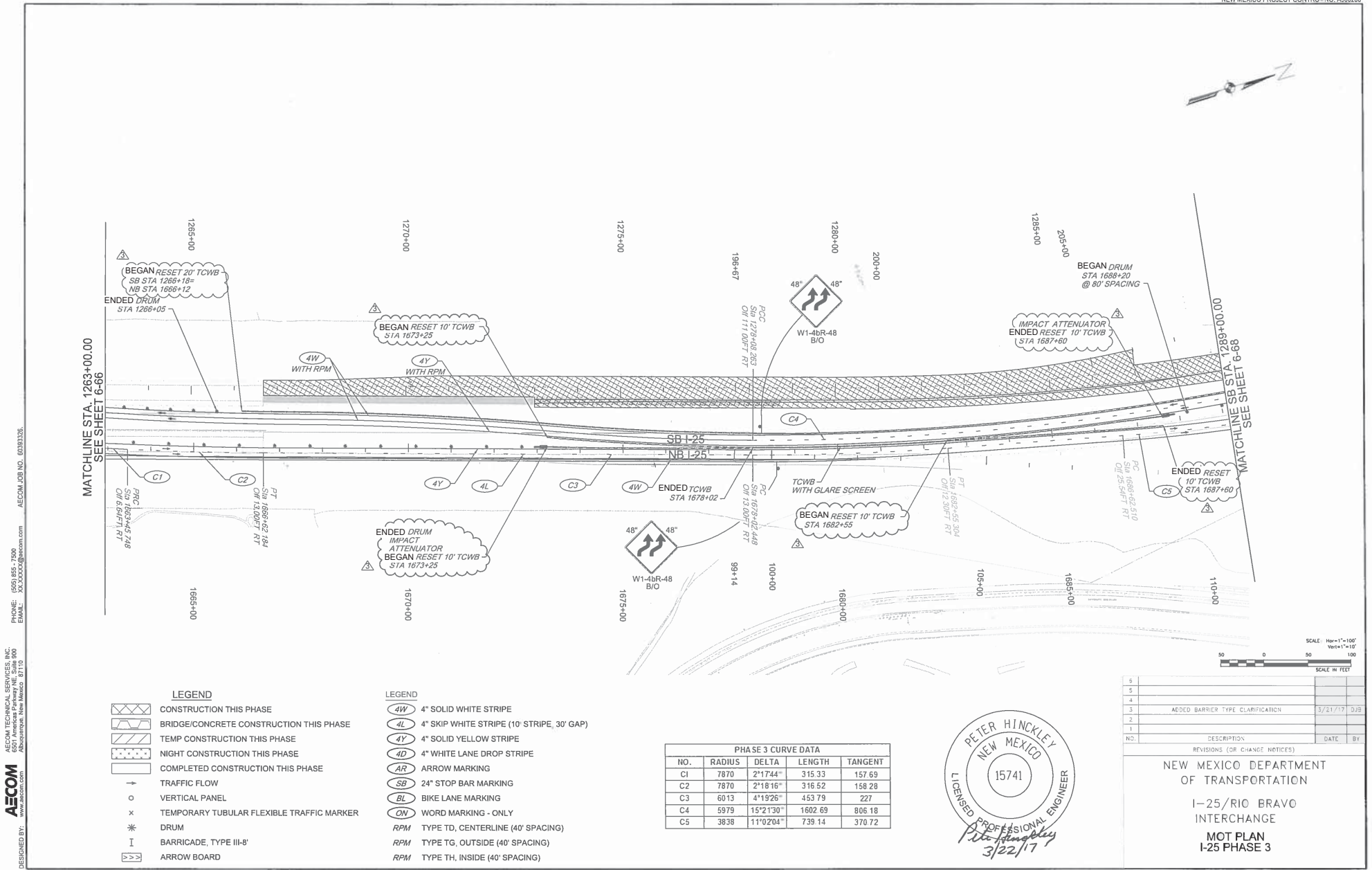
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 66



REVISIONS (OR CHANGE NOTICES)			
NO.	DESCRIPTION	DATE	BY
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NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 3			



PHONE: (505) 855-7500
EMAIL: AECOM_JOB_NO. 60393326

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6501 Americas Parkway, NE, Suite 900
Albuquerque, New Mexico 87110

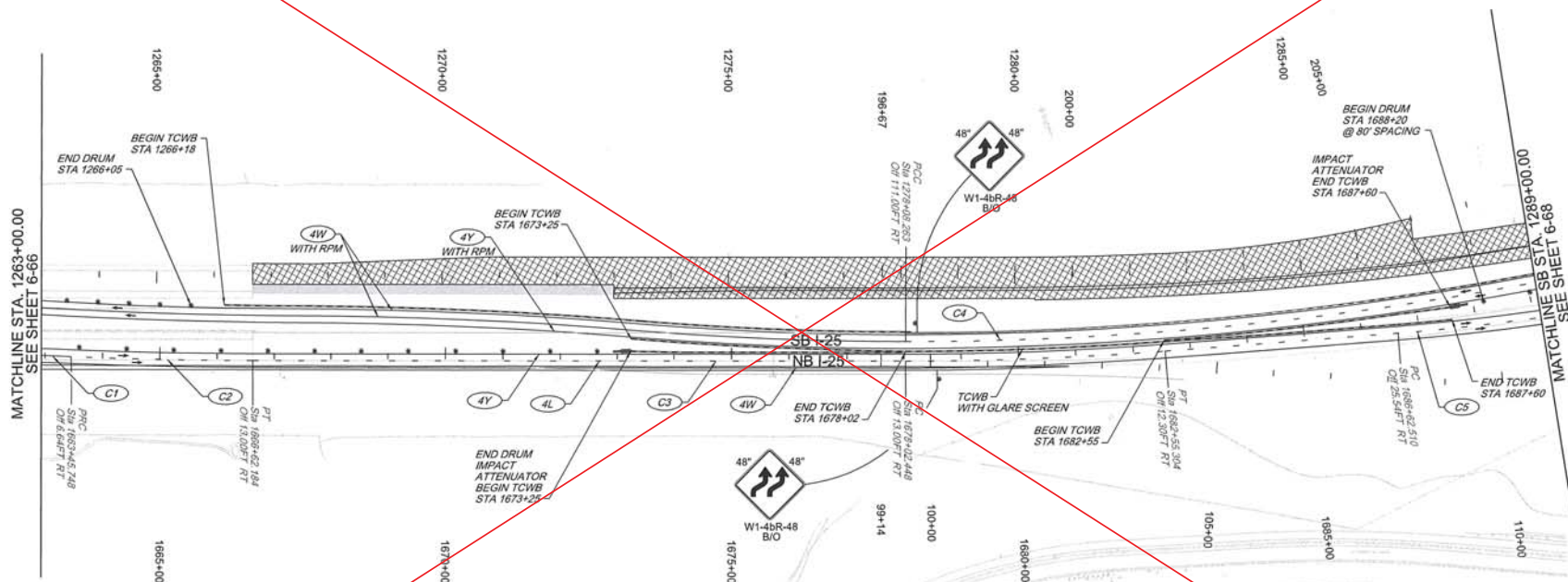
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 8 • 67



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-B
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
- 24" STOP BAR MARKING
- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)

PHASE 3 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	7870	2°17'44"	315.33	157.69
C2	7870	2°18'16"	316.52	158.28
C3	6013	4°19'26"	453.79	227
C4	5979	15°21'30"	1602.69	806.18
C5	3838	11°02'04"	739.14	370.72



1/10/2017

SCALE: Horizontal = 1"=100'
Vertical = 1"=40'

SCALE IN FEET

REVISIONS (OR CHANGE NOTICES)		DATE	BY
NO.	DESCRIPTION		
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

MOT PLAN
I-25 PHASE 3

AECOM JOB NO. 60393326

PHONE: (505) 855-7600
FAX: (505) 855-7601
EMAIL: aecom@aecom.comDESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 800
Albuquerque, New Mexico 87110

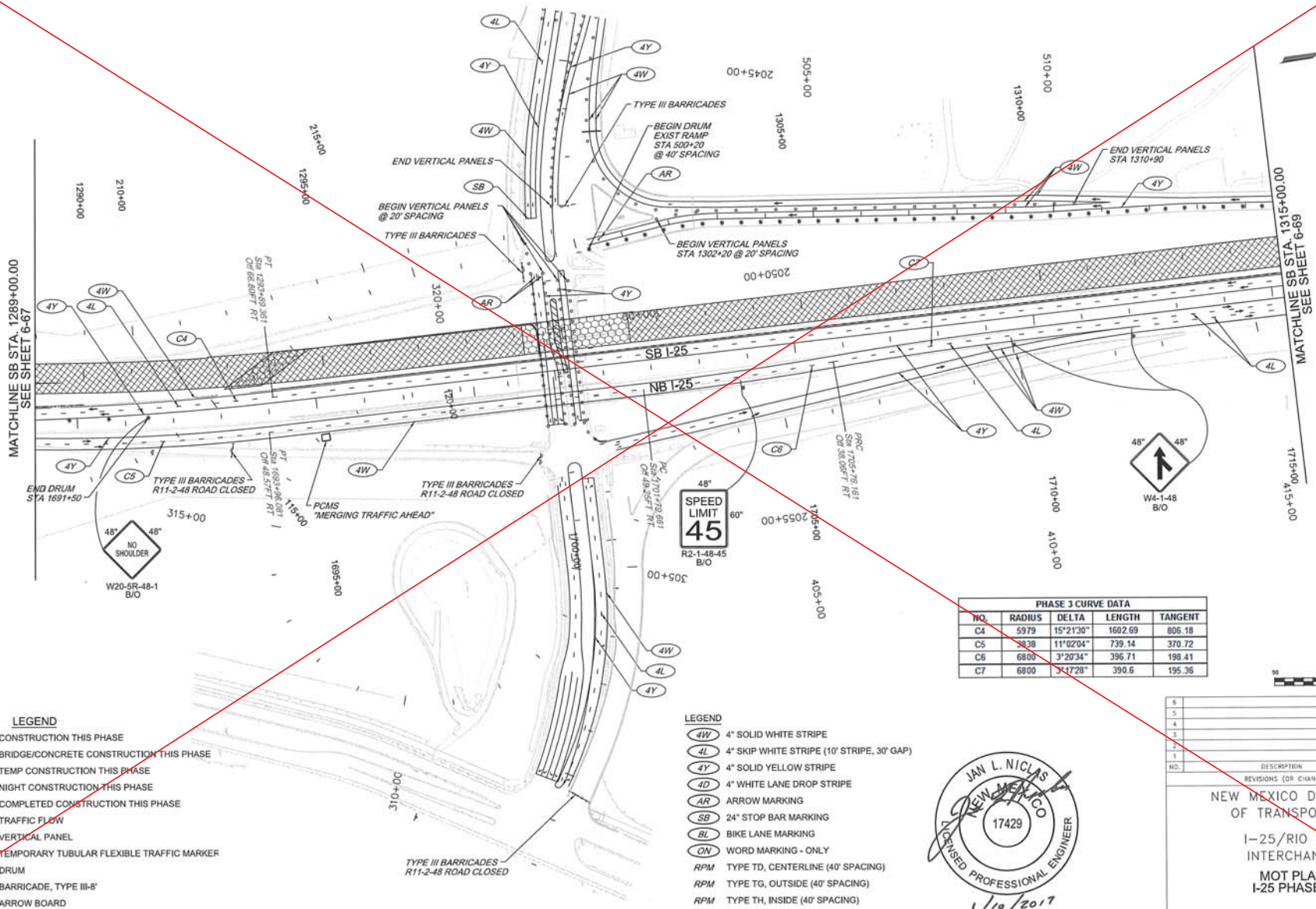
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 87



PHASE 3 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C4	5979	15°21'30"	1602.69	806.18
C5	5838	11°02'04"	739.14	370.72
C6	6800	3°20'34"	396.71	198.41
C7	6800	3°17'28"	390.6	195.36



REVISIONS (OR CHANGE NOTICES)		DATE	BY
NO.	DESCRIPTION		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

MOT PLAN I-25 PHASE 3



- LEGEND**
- 4W 4" SOLID WHITE STRIPE
 - 4L 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4Y 4" SOLID YELLOW STRIPE
 - 4D 4" WHITE LANE DROP STRIPE
 - AR ARROW MARKING
 - SB 24" STOP BAR MARKING
 - BL BIKE LANE MARKING
 - ON WORD MARKING - ONLY
 - RPM TYPE TD, CENTERLINE (40' SPACING)
 - RPM TYPE TG, OUTSIDE (40' SPACING)
 - RPM TYPE TH, INSIDE (40' SPACING)

- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-B
 - ARROW BOARD

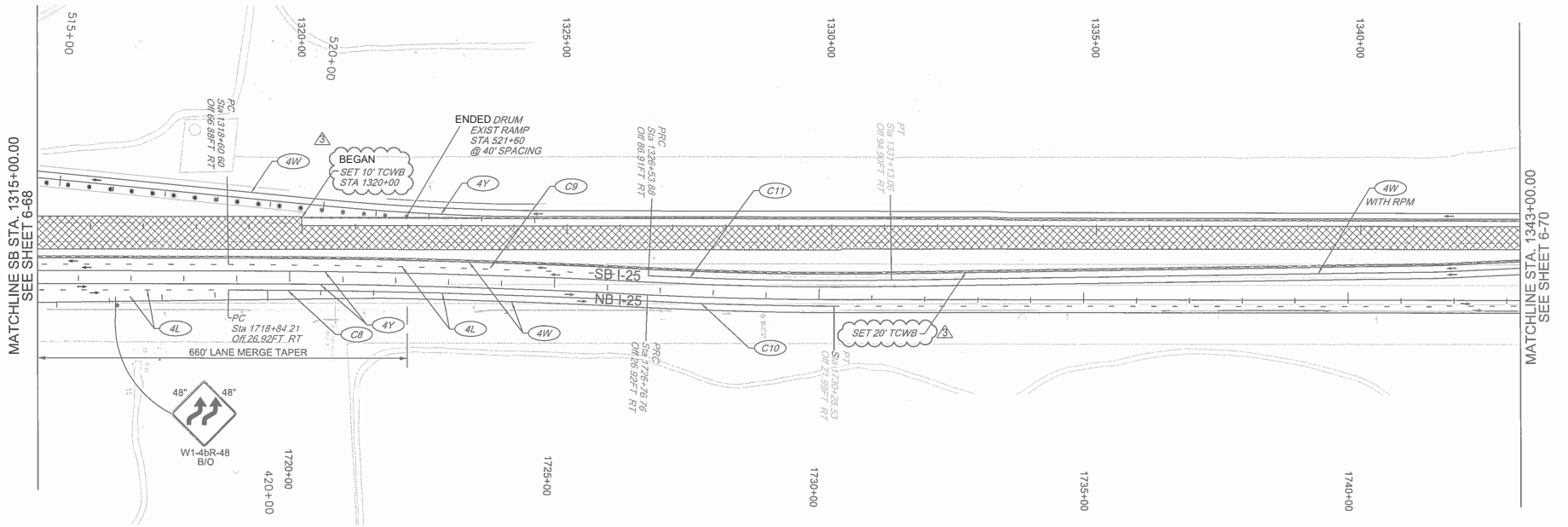
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 Albuquerque, New Mexico 87102
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 68



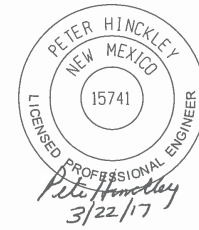
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SEE SHEET 6-68

MATCHLINE STA. 1343+00.00
SEE SHEET 6-70

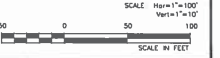
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	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-B
	ARROW BOARD

LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)

PHASE 3 CURVE DATA			
C8	15673.08	2°53'32"	395.68
C9	15721.12	2°53'32"	306.89
C10	6988.9	2°53'20"	176.23
C11	6940.86	3°47'30"	229.75



5			
4			
3	ADDED BARRIER TYPE CLARIFICATION	3/21/17	DJS
2			
1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 3			



PHONE: (505) 855-7000
FAX: (505) 855-7000
EMAIL: J.C.XXXXX@aecom.com

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AECOM TECHNICAL SERVICES, INC.
6501 Avenida Parkway NE, Suite 900
Albuquerque, New Mexico 87110

TRANSPORTATION
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 69

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PHASE 3 CURVE DATA				
C8	15673.08	2°53'32"	791.19	395.68
C9	15721.12	2°53'32"	793.61	396.89
C10	6988.9	2°53'20"	352.38	176.23
C11	6940.86	3°47'30"	459.34	229.75



8			
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
<p>NEW MEXICO DEPARTMENT OF TRANSPORTATION</p> <p>I-25/RIO BRAVO INTERCHANGE</p> <p>MOT PLAN I-25 PHASE 3</p>			

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Albuquerque, New Mexico 87110
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REQUESTED BY: (505) 858 - 7500
PHONE: XX-XXXX@aecom.com
EMAIL: AECOM JOB NO. 60293326

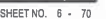
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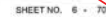
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 T 505.855.7500

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SHEET NO. 6 - 69





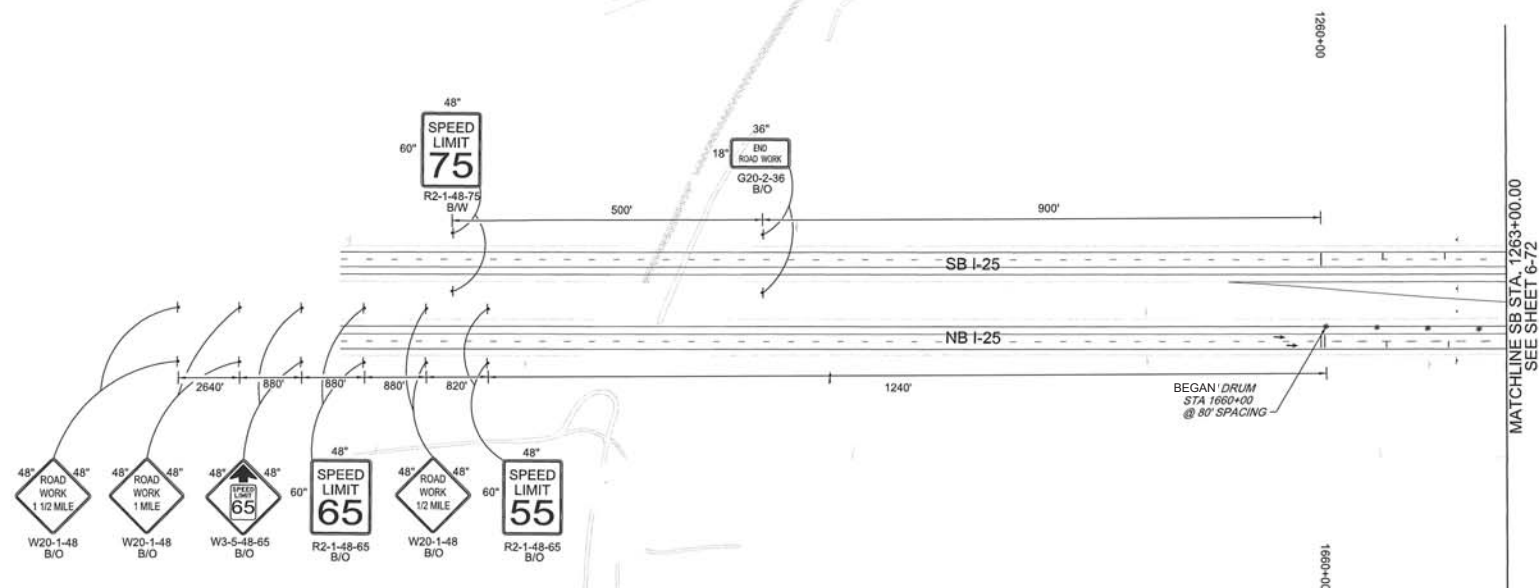
ACCOM JOB NO. 6033326
PHONE: (505) 855-7500
EMAIL: xx.xxxxx@accon.com

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6501 American Parkway NE, Suite 900
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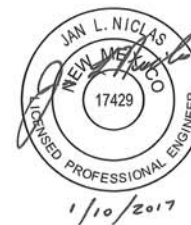
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LEGEND	
	CONSTRUCTION THIS PHASE
	BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-5'
	ARROW BOARD

LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)



SCALE: Horiz = 1"=100'
Vert = 1"=10'
SCALE IN FEET



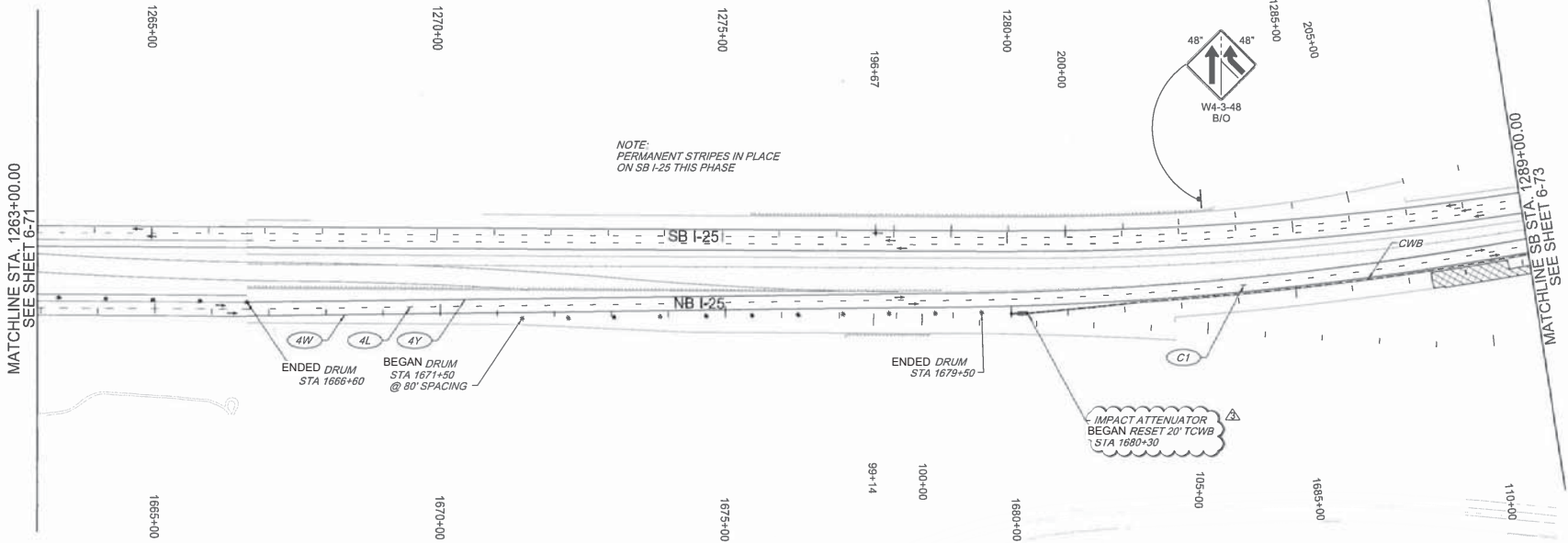
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NO.	DESCRIPTION	DATE BY
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NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25/RIO BRAVO INTERCHANGE		
MOT PLAN I-25 PHASE 4		

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SHEET NO. 6 - 71

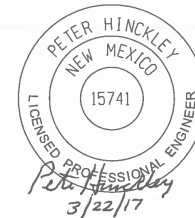


NOTE:
PERMANENT STRIPES IN PLACE
ON SB I-25 THIS PHASE

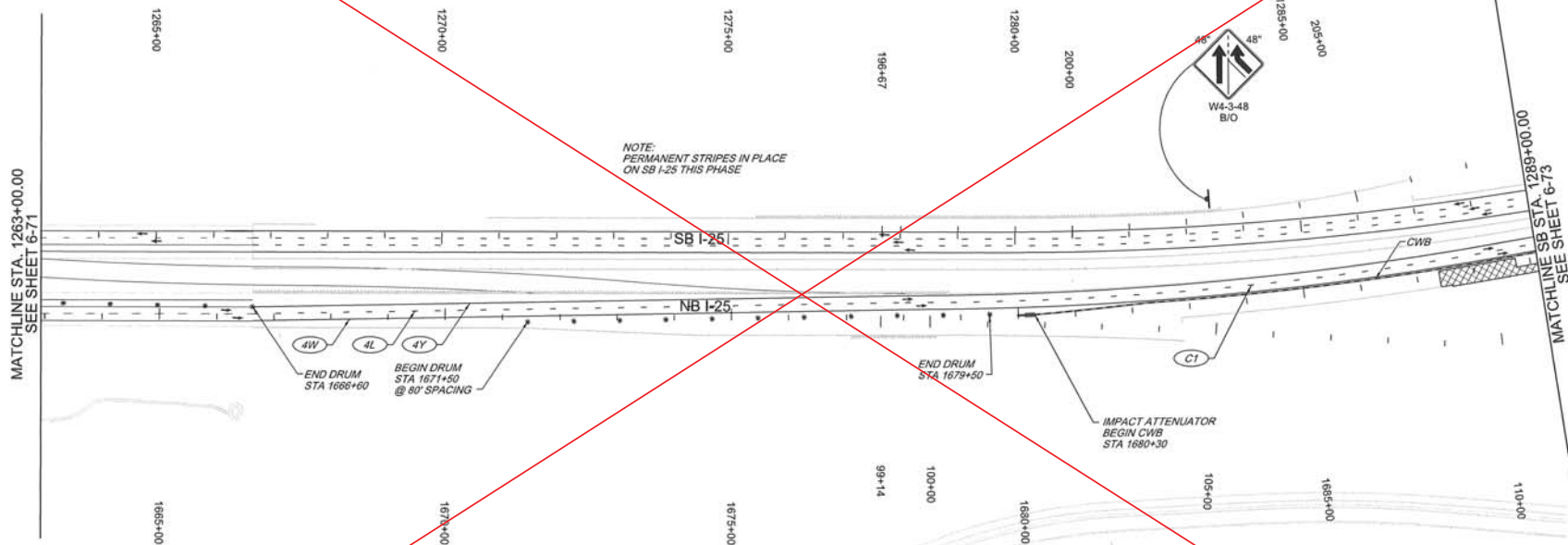
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	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-8'
	ARROW BOARD

LEGEND	
	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)

SCALE: Horiz=1"=100'
Vert=1"=10'
SCALE IN FEET



REVISIONS (OR CHANGE NOTICES)			
NO.	DESCRIPTION	DATE	BY
5			
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3	ADDED BARRIER TYPE CLARIFICATION	3/21/17	DJB
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NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 4			



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-8'
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
- 24" STOP BAR MARKING
- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)



1/10/2017



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

MOT PLAN I-25 PHASE 4

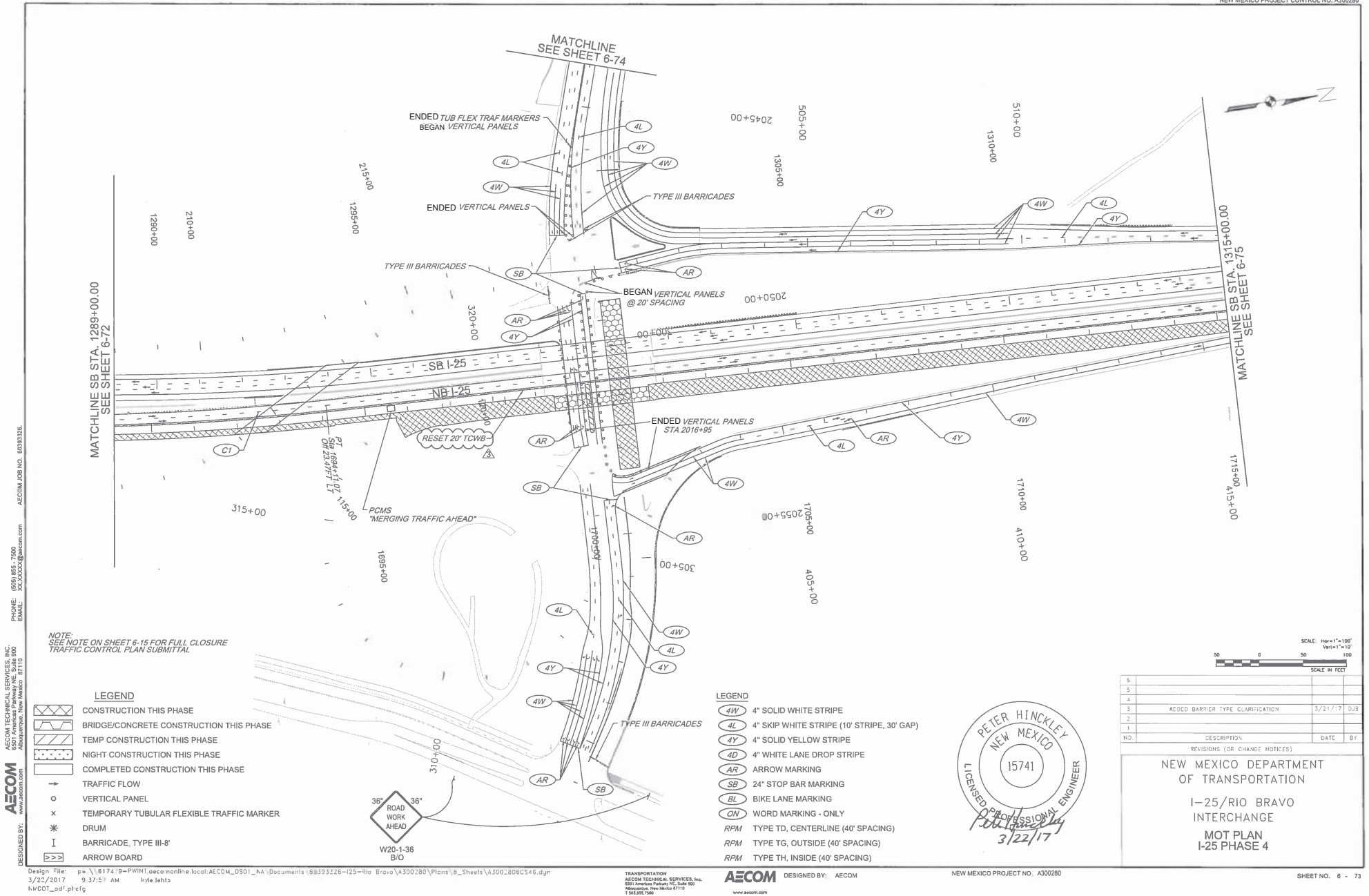
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PHONE: (505) 855-7600
EMAIL: xxxxxxxx@aecom.comAECOM TECHNICAL SERVICES, INC.
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 72





6		
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NO.	DESCRIPTION	DATE
REVISIONS (OR CHANGE NOTICES)		
<p align="center"> NEW MEXICO DEPARTMENT OF TRANSPORTATION I-25/RIO BRAVO INTERCHANGE MOT PLAN I-25 PHASE 4 </p>		

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PHONE: (505) 855-7500
EMAIL: XJXXXXX@aecom.com

AECOM JOB NO. 60393326.

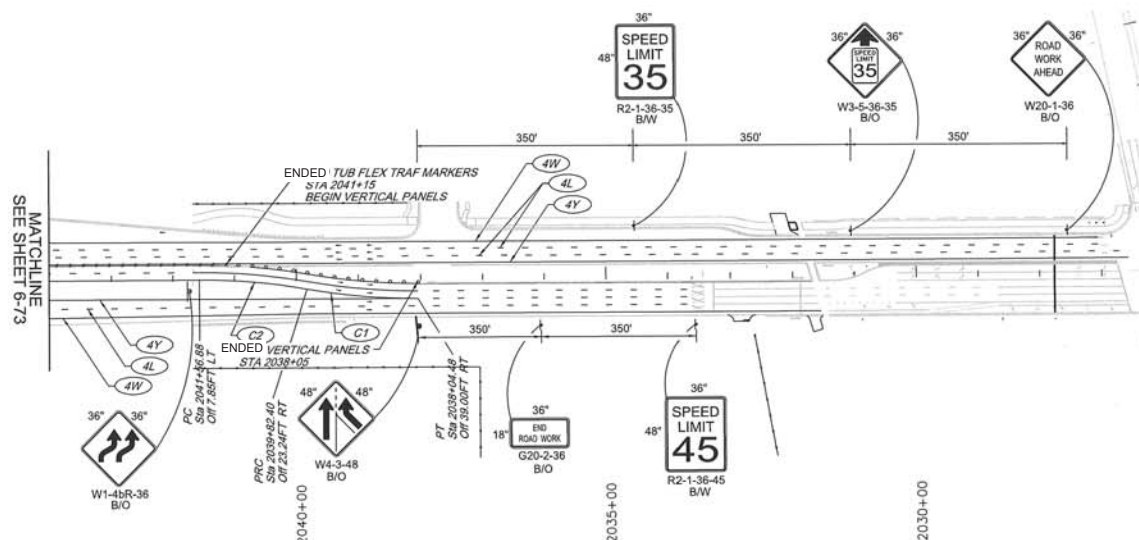
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 7.



LEGEND

- CONSTRUCTION THIS PHASE
- BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
- TEMP CONSTRUCTION THIS PHASE
- NIGHT CONSTRUCTION THIS PHASE
- COMPLETED CONSTRUCTION THIS PHASE
- TRAFFIC FLOW
- VERTICAL PANEL
- TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
- DRUM
- BARRICADE, TYPE III-5
- ARROW BOARD

LEGEND

- 4" SOLID WHITE STRIPE
- 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
- 4" SOLID YELLOW STRIPE
- 4" WHITE LANE DROP STRIPE
- ARROW MARKING
- 24" STOP BAR MARKING
- BIKE LANE MARKING
- WORD MARKING - ONLY
- TYPE TD, CENTERLINE (40' SPACING)
- TYPE TG, OUTSIDE (40' SPACING)
- TYPE TH, INSIDE (40' SPACING)

PHASE 4 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C1	1012	10°07'33"	178.85	89.66
C2	966	10°10'16"	175.39	87.92



NO.	DESCRIPTION	DATE	BY
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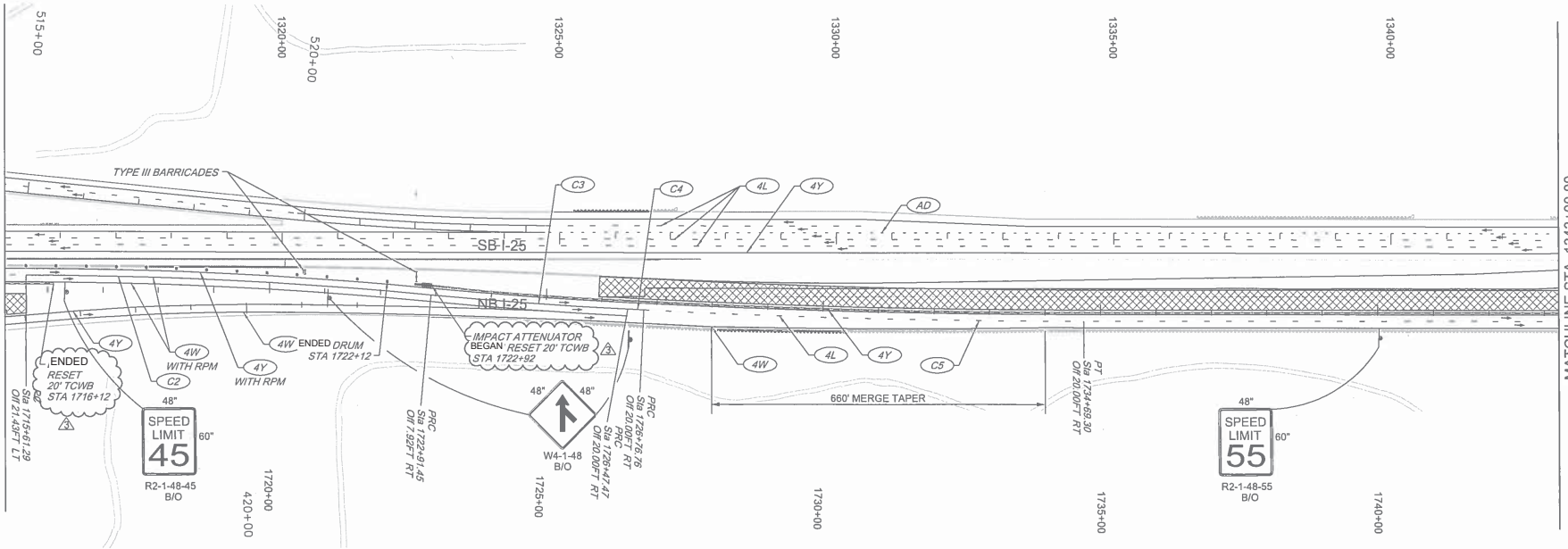
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGEMOT PLAN
I-25 PHASE 4



MATCHLINE SB STA. 1315+00.00
SEE SHEET 6-73

MATCHLINE STA. 1343+00.00
SEE SHEET 6-76



LEGEND

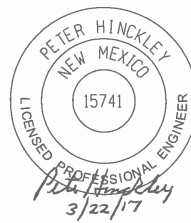
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	TEMP CONSTRUCTION THIS PHASE
	NIGHT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION THIS PHASE
	TRAFFIC FLOW
	VERTICAL PANEL
	TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
	DRUM
	BARRICADE, TYPE III-8'
	ARROW BOARD

LEGEND

	4" SOLID WHITE STRIPE
	4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
	4" SOLID YELLOW STRIPE
	4" WHITE LANE DROP STRIPE
	ARROW MARKING
	24" STOP BAR MARKING
	BIKE LANE MARKING
	WORD MARKING - ONLY
	TYPE TD, CENTERLINE (40' SPACING)
	TYPE TG, OUTSIDE (40' SPACING)
	TYPE TH, INSIDE (40' SPACING)

PHASE 4 CURVE DATA

NO.	RADIUS	DELTA	LENGTH	TANGENT
C2	7870	5°19'19"	730.99	365.76
C3	7870	2°35'29"	355.93	178
C4	15680	0°06'25"	29.25	14.62
C5	15720	2°53'32"	793.56	396.86



5			
4			
3	ADDED BARRIER TYPE CLARIFICATION	3/21/17	DJS
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1			
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 4			

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AECOM TECHNICAL SERVICES, INC.
6501 America Parkway NE, Suite 600
Albuquerque, New Mexico 87110

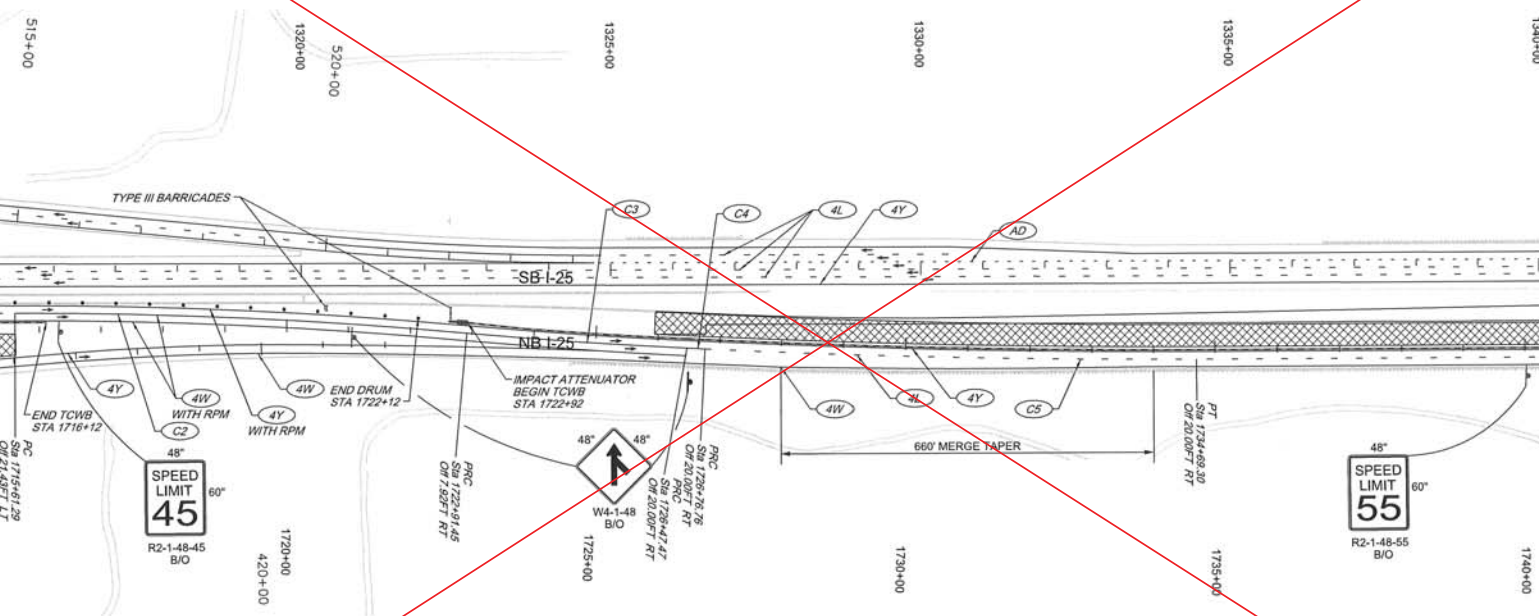
PHONE: (505) 855-7500
FAX: (505) 855-7500
EMAIL: XXXXXXX@aecom.com

AECOM JOB NO. 60393126

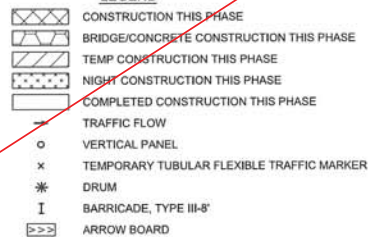


MATCHLINE SB STA. 1315+00.00
SEE SHEET 6-73

MATCHLINE STA. 1343+00.00
SEE SHEET 6-76



LEGEND



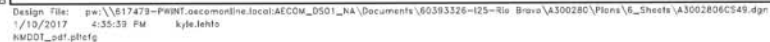
LEGEND

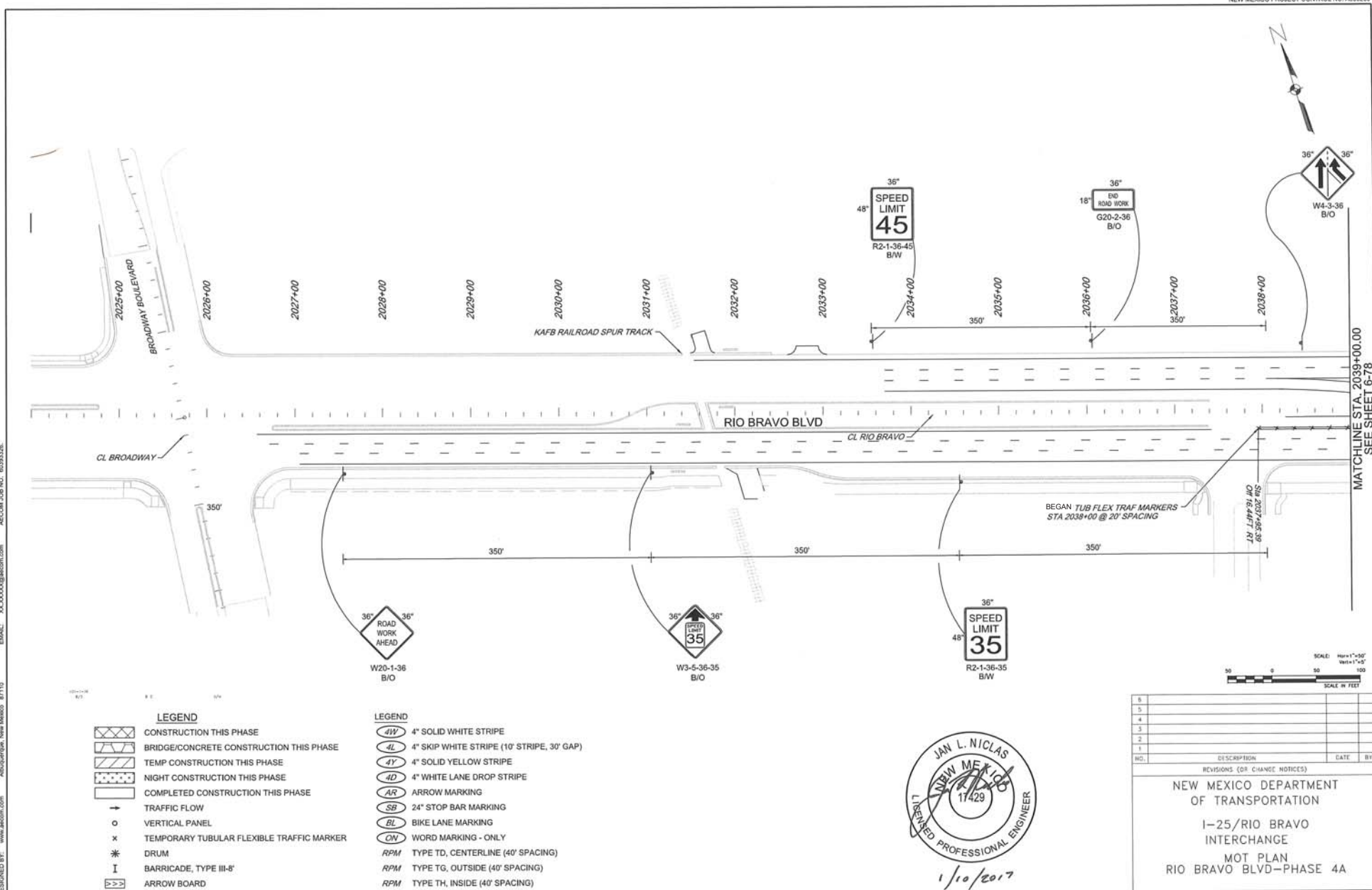


PHASE 4 CURVE DATA				
NO.	RADIUS	DELTA	LENGTH	TANGENT
C2	7870	5°19'19"	730.99	365.76
C3	7870	2°35'29"	355.93	178
C4	15680	0°06'25"	29.25	14.62
C5	15720	2°53'32"	793.56	396.86



8			
5			
3			
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN I-25 PHASE 4			





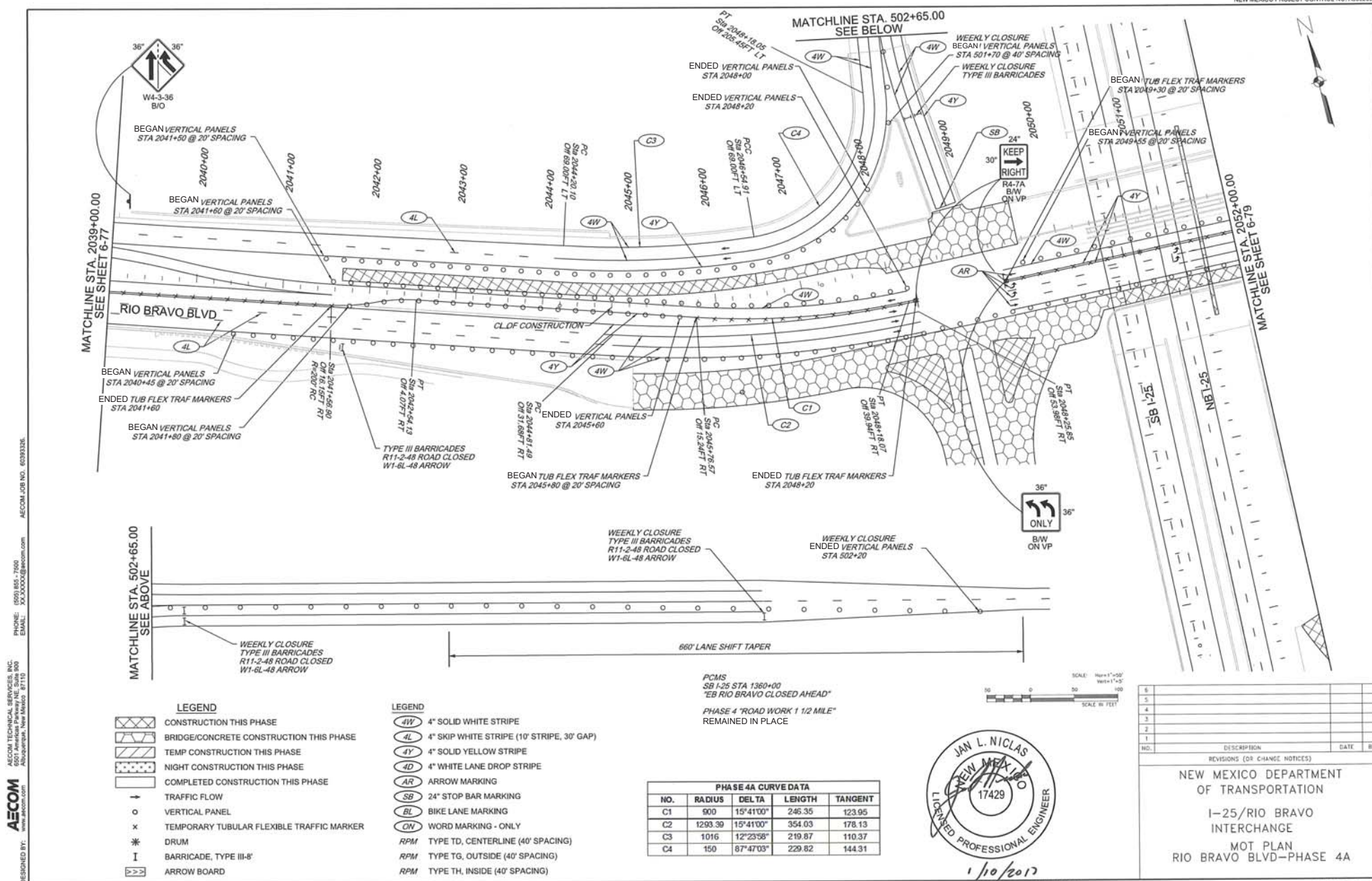
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 1/10/2017 4:35:45 PM kyle.jehto
 HMDOT_sdt.pptg

TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 Sandstone Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 1-888-888-7888

AECOM
 DESIGNED BY: AECOM
 www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 77



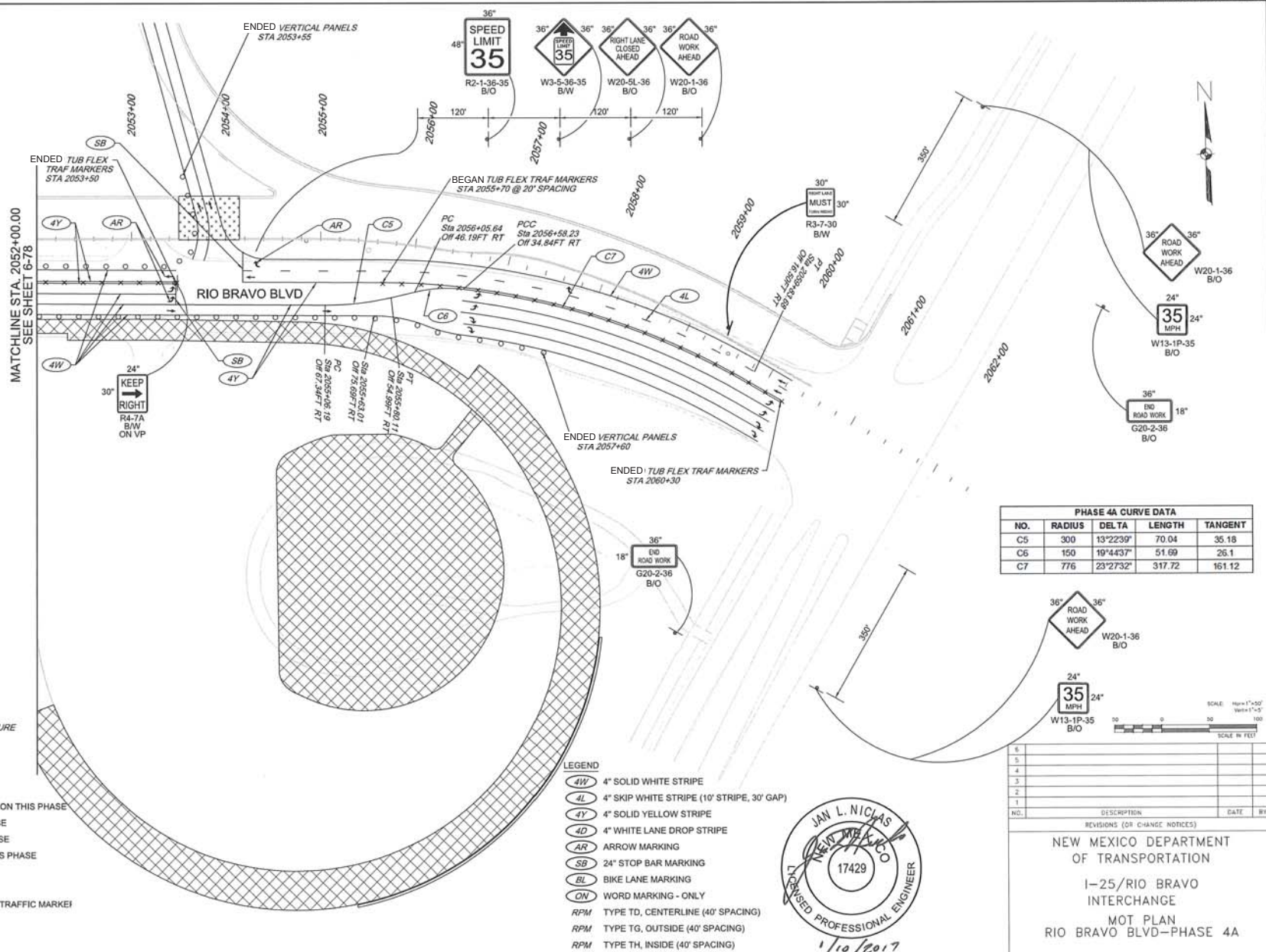
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 1/10/2017 4:35:51 PM kyle.lehto
 NMDOT_001.phetg

TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 600 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 (505) 243-1000

AECOM
 www.aecom.com



1/10/2017



ACCOM JOB NO. 6038326
PHONE: (505) 855-7600
EMAIL: XXXXXXX@acocom.com

ACCOM TECHNICAL SERVICES, INC.
6507 Americana Parkway NE, Suite 800
Albuquerque, New Mexico 87110
DESIGNED BY: www.acocom.com

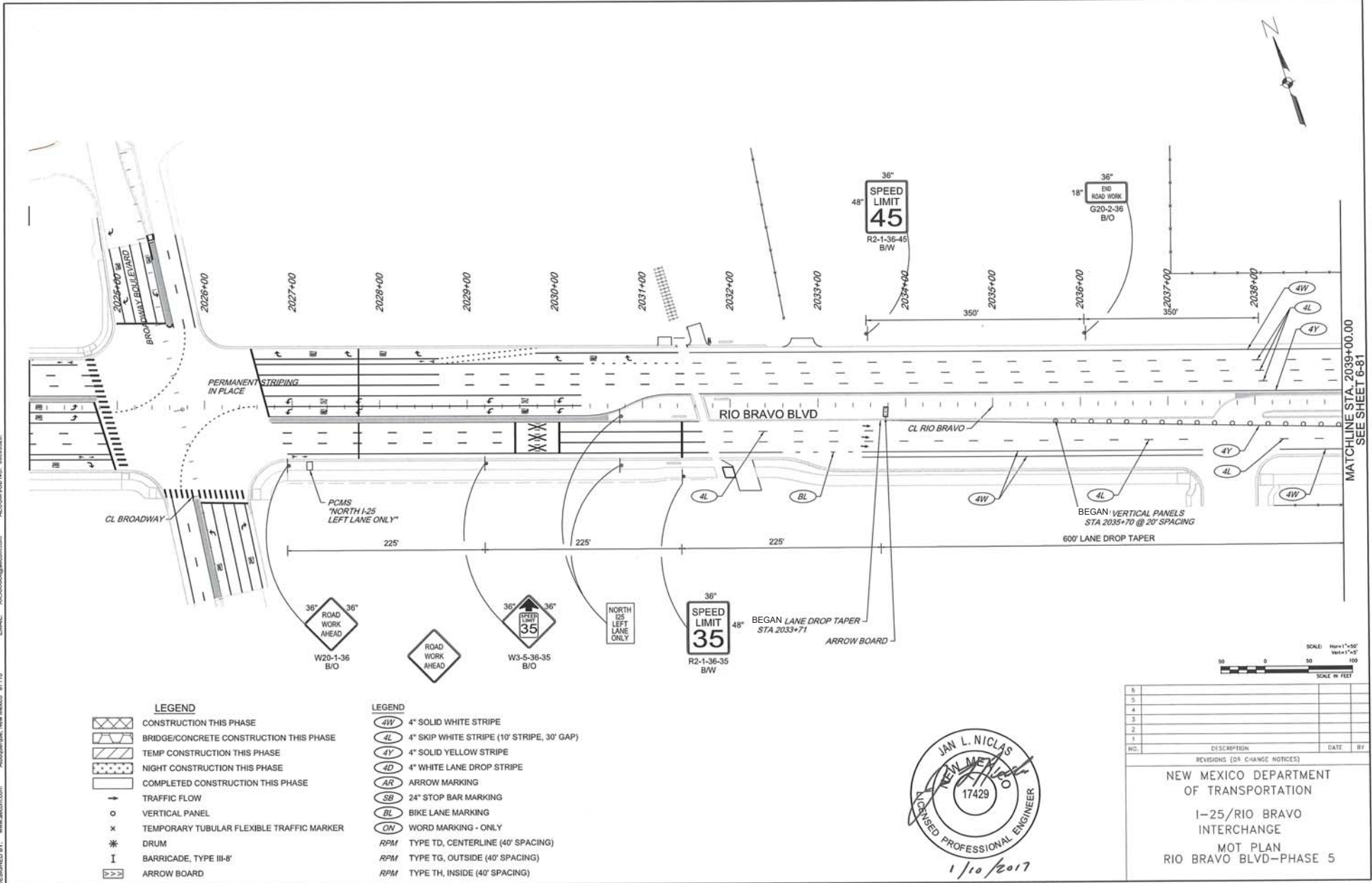
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1/10/2017 4:35:58 PM kyle.lehto
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TRANSPORTATION
ACCOM TECHNICAL SERVICES, INC.
6507 Americana Parkway NE, Suite 800
Albuquerque, New Mexico 87110
1-888-888-8888

DESIGNED BY: ACCOM
NEW MEXICO PROJECT NO. A300280



NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN
RIO BRAVO BLVD-PHASE 4A



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-B
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



6			
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NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN			
RIO BRAVO BLVD-PHASE 5			

DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 800
 Albuquerque, New Mexico 87110
 PHONE: (505) 885-7600
 EMAIL: xxxxxxxx@aecom.com
 AECOM JOB NO. 60383326

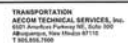
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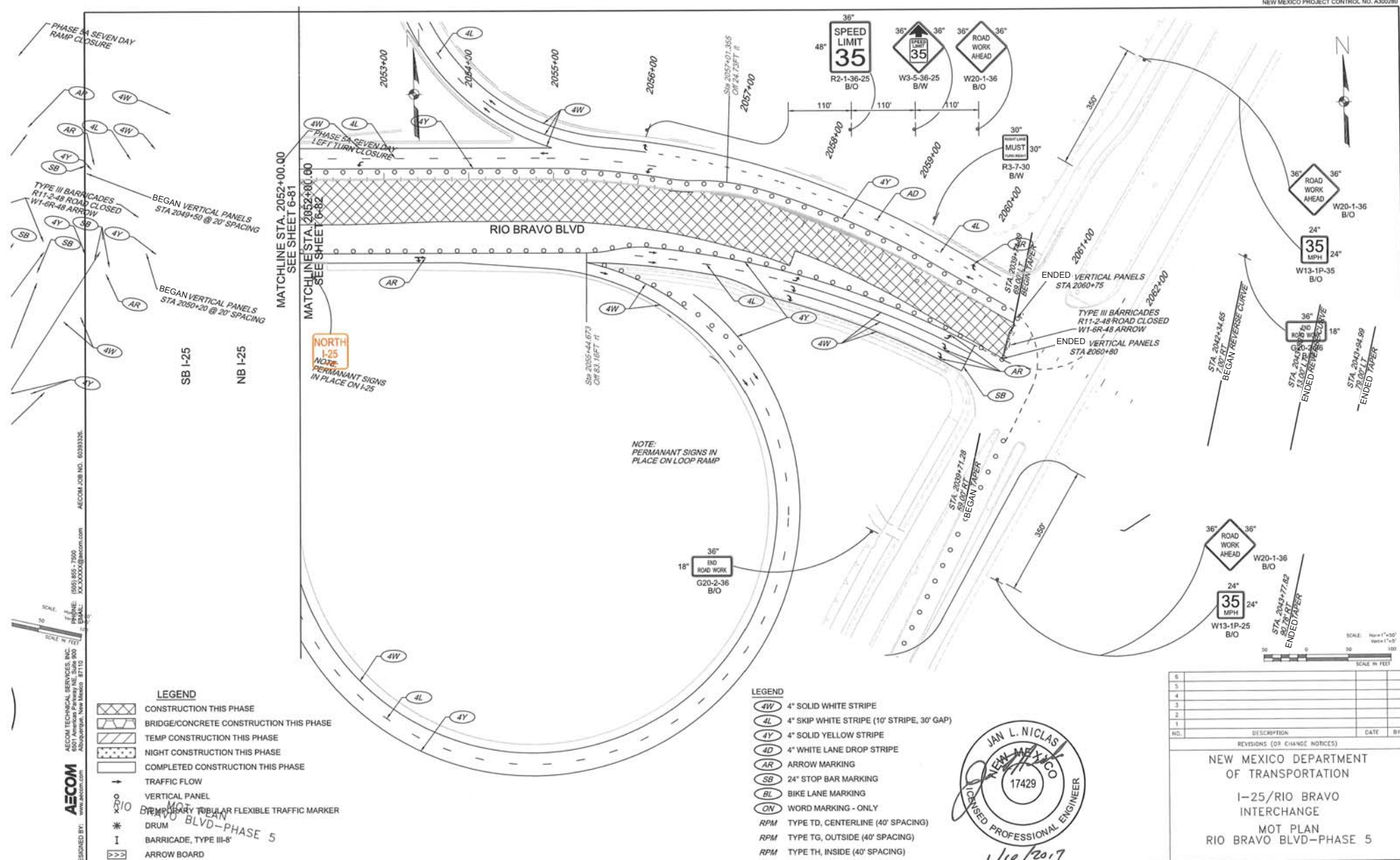
TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 800
 Albuquerque, New Mexico 87110
 1-800-885-7600

AECOM DESIGNED BY: AECOM
 www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 80

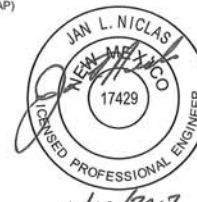




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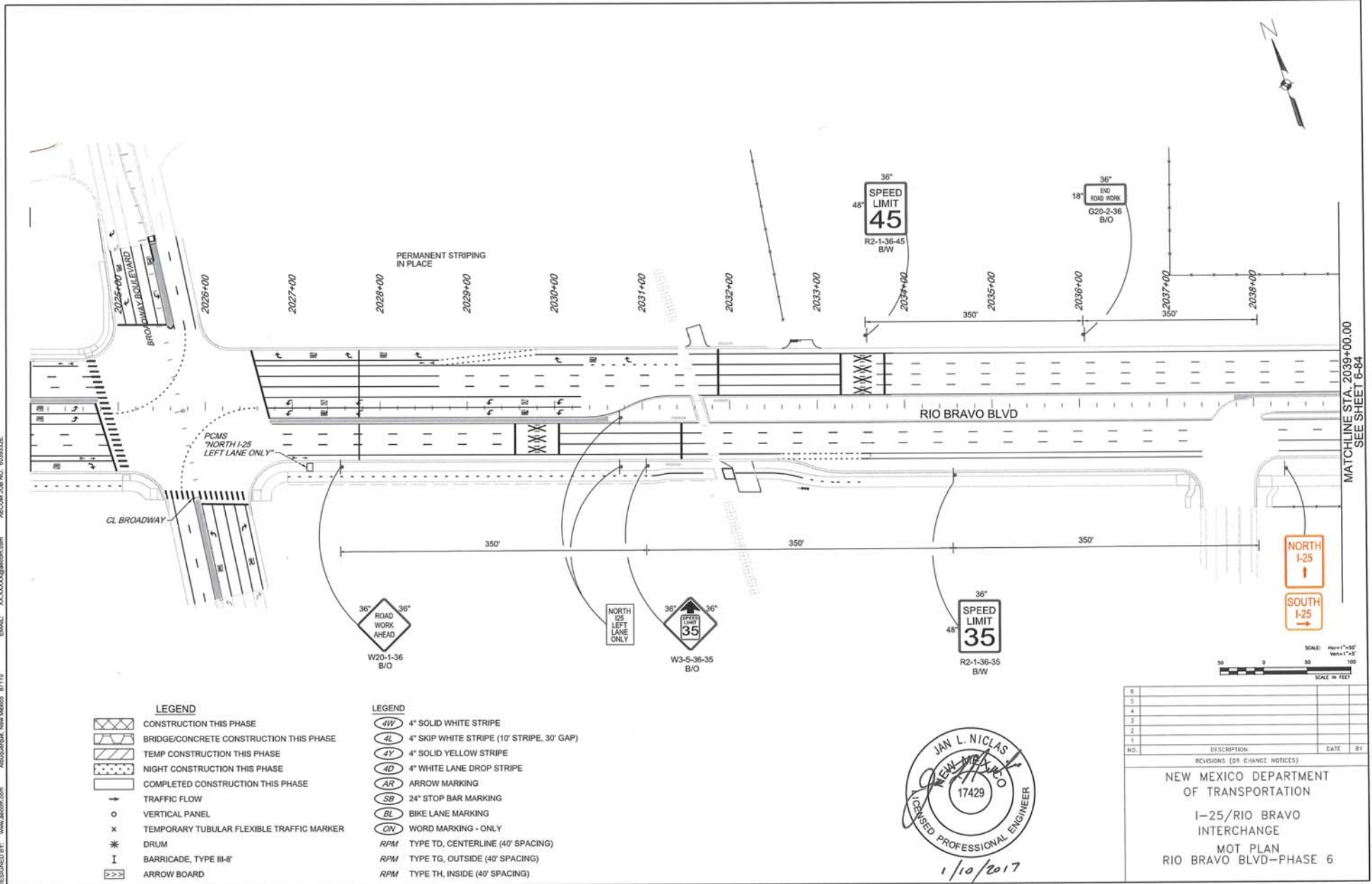
TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 600 American Parkway NE, Suite 100
 Albuquerque, New Mexico 87110
 T 505.243.7000

AECOM DESIGNED BY: AECOM
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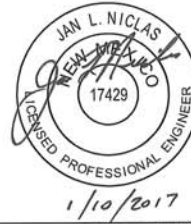
1/10/2017

NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN			
RIO BRAVO BLVD-PHASE 5			



- LEGEND**
- CONSTRUCTION THIS PHASE
 - BRIDGE/CONCRETE CONSTRUCTION THIS PHASE
 - TEMP CONSTRUCTION THIS PHASE
 - NIGHT CONSTRUCTION THIS PHASE
 - COMPLETED CONSTRUCTION THIS PHASE
 - TRAFFIC FLOW
 - VERTICAL PANEL
 - TEMPORARY TUBULAR FLEXIBLE TRAFFIC MARKER
 - DRUM
 - BARRICADE, TYPE III-S
 - ARROW BOARD

- LEGEND**
- 4" SOLID WHITE STRIPE
 - 4" SKIP WHITE STRIPE (10' STRIPE, 30' GAP)
 - 4" SOLID YELLOW STRIPE
 - 4" WHITE LANE DROP STRIPE
 - ARROW MARKING
 - 24" STOP BAR MARKING
 - BIKE LANE MARKING
 - WORD MARKING - ONLY
 - TYPE TD, CENTERLINE (40' SPACING)
 - TYPE TG, OUTSIDE (40' SPACING)
 - TYPE TH, INSIDE (40' SPACING)



NO.	DESCRIPTION	DATE	BY
6			
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1			

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

MOT PLAN

RIO BRAVO BLVD-PHASE 6

DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 855-7600
 EMAIL: xxxxxxxx@aecom.com
 AECOM JOB NO. 6033326

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 1/10/2017 4:36:24 PM kyle.jehto
 NWDOT_pdr.pltcf

TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 T 505.855.7600

DESIGNED BY: AECOM
 AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 83

PHONE: (505) 855-7600
 EMAIL: JOE.XXXXX@aecom.com

DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 4501 Americas Parkway NE, Suite 900
 Albuquerque, New Mexico 87110

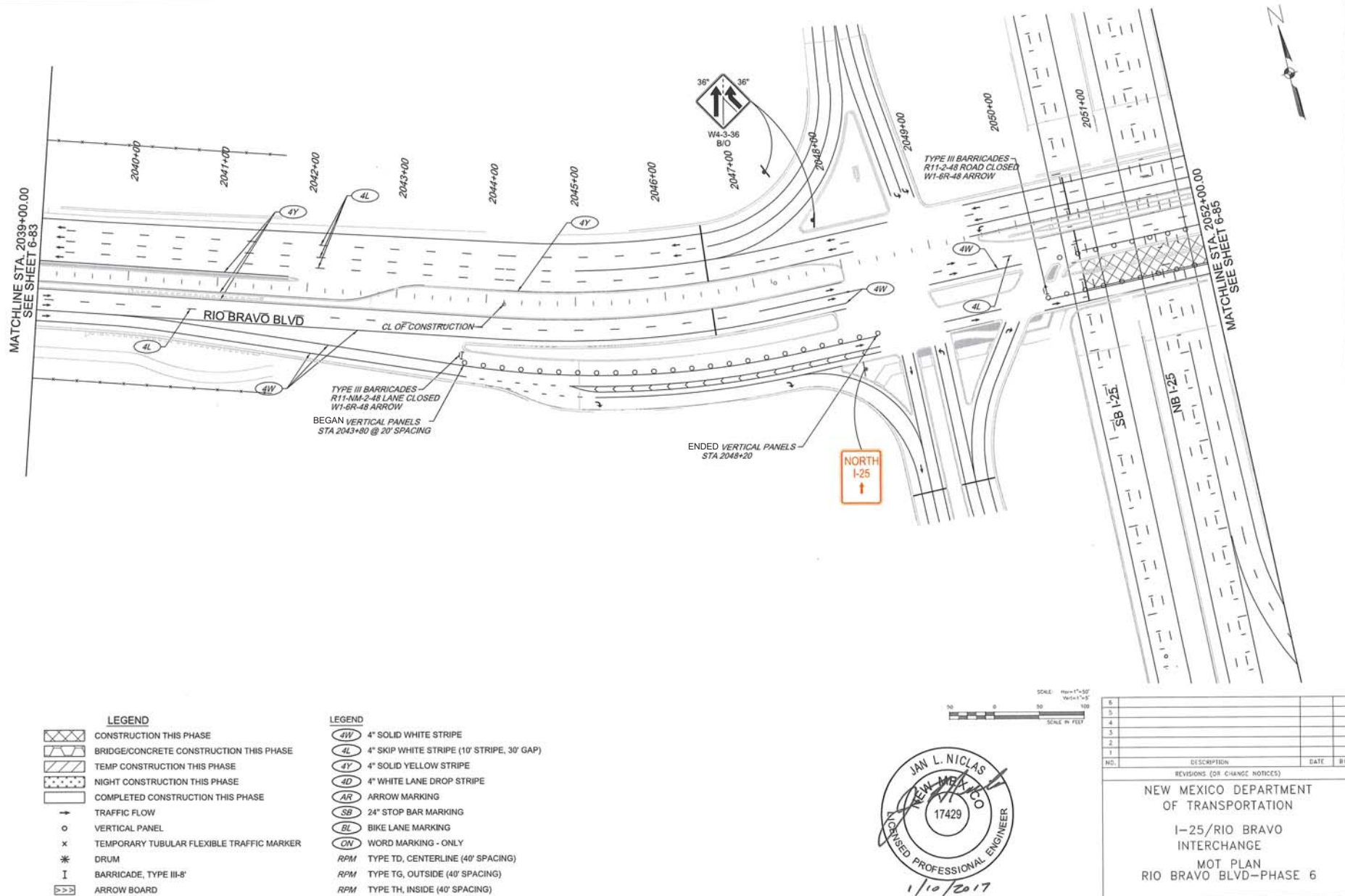
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 1/10/2017 4:56:30 PM kyle.jehta
 hmdot_ad1.phetg

TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 4501 Americas Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 1/10/2017

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 84



DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 Americas Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 865-7500
 EMAIL: JX.XXXXX@aecom.com
 AECOM JOB NO. 60393326

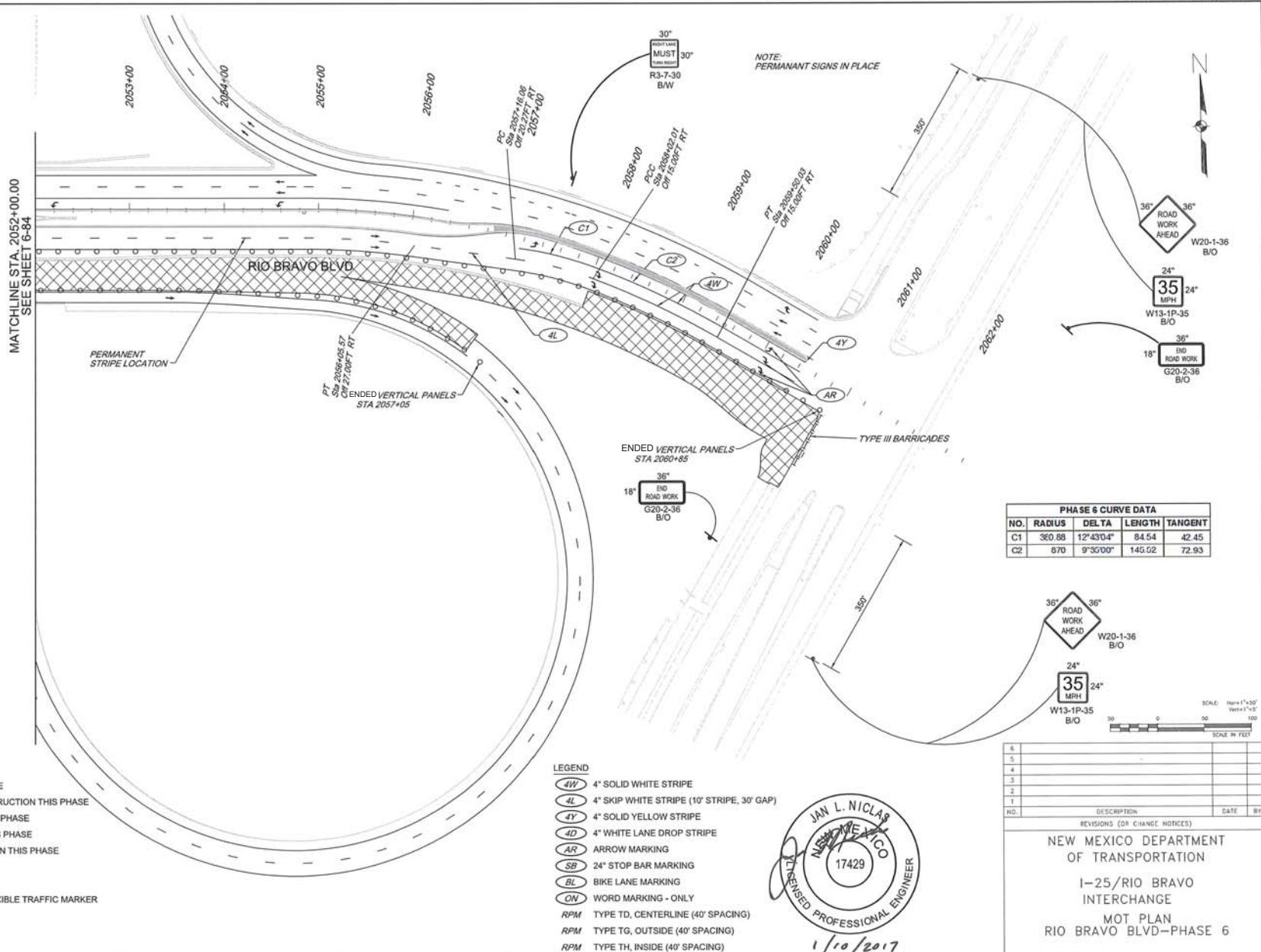
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 NWDOT_001.pltcf

TRANSPORTATION
 AECOM TECHNICAL SERVICES, Inc.
 6501 Americas Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 T 505.865.7500

DESIGNED BY: AECOM
 AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 85





CURVE DATA SB NORTH CROSSOVER

1
P.I. STA = 13+96.89
Δ = 2°53'30"
Dc = 0°21'52"
T = 396.89'
L = 793.61'
R = 15721.12'

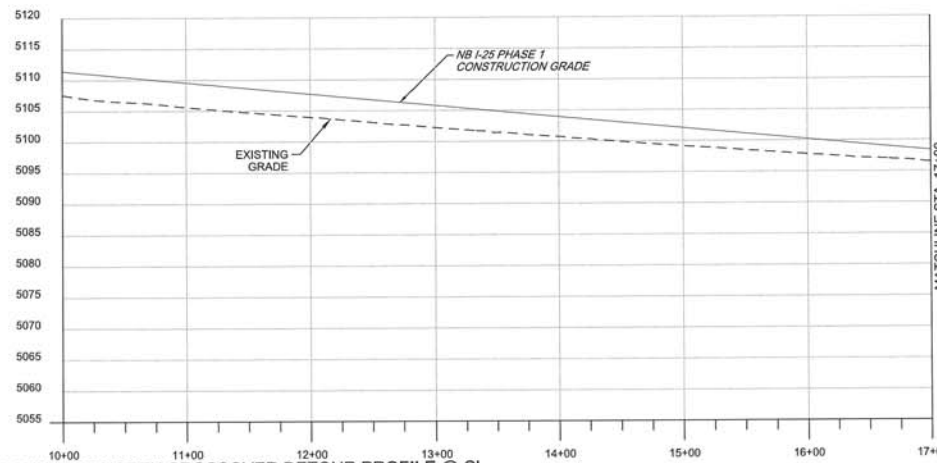
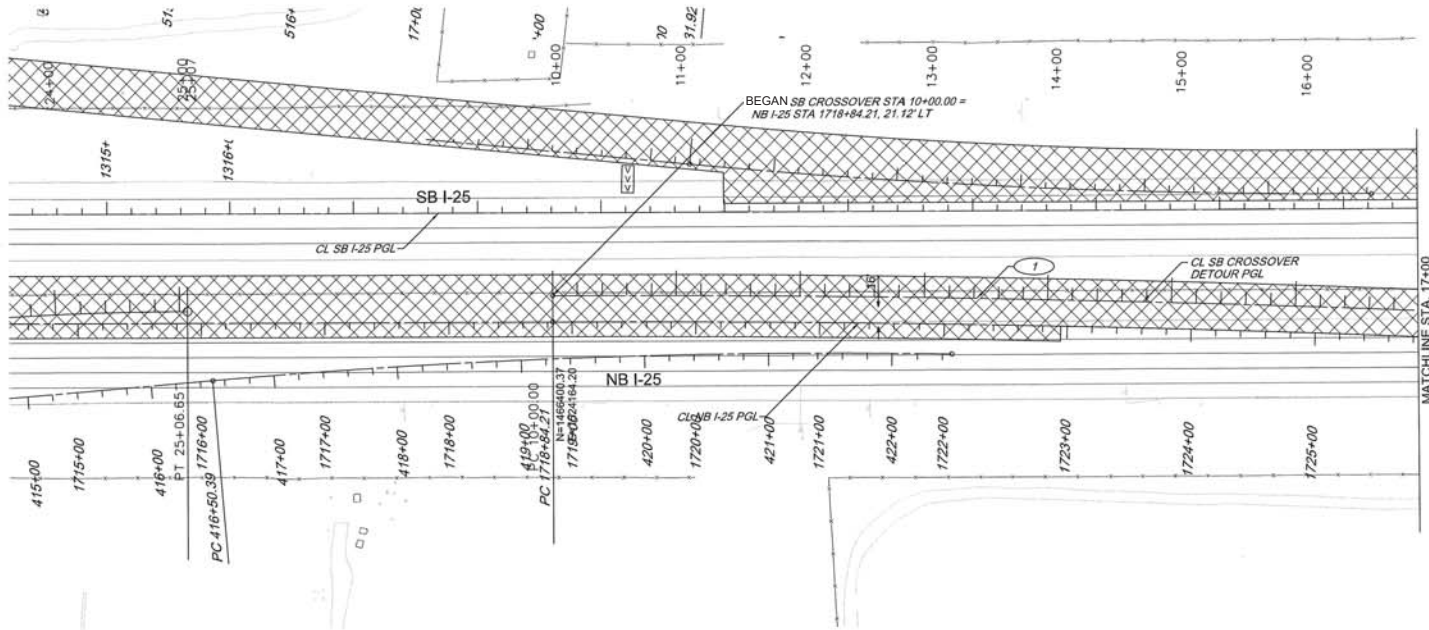


NO.	DESCRIPTION	DATE	BY
1	DESIGN		
2			
3			
4			
5			
6			

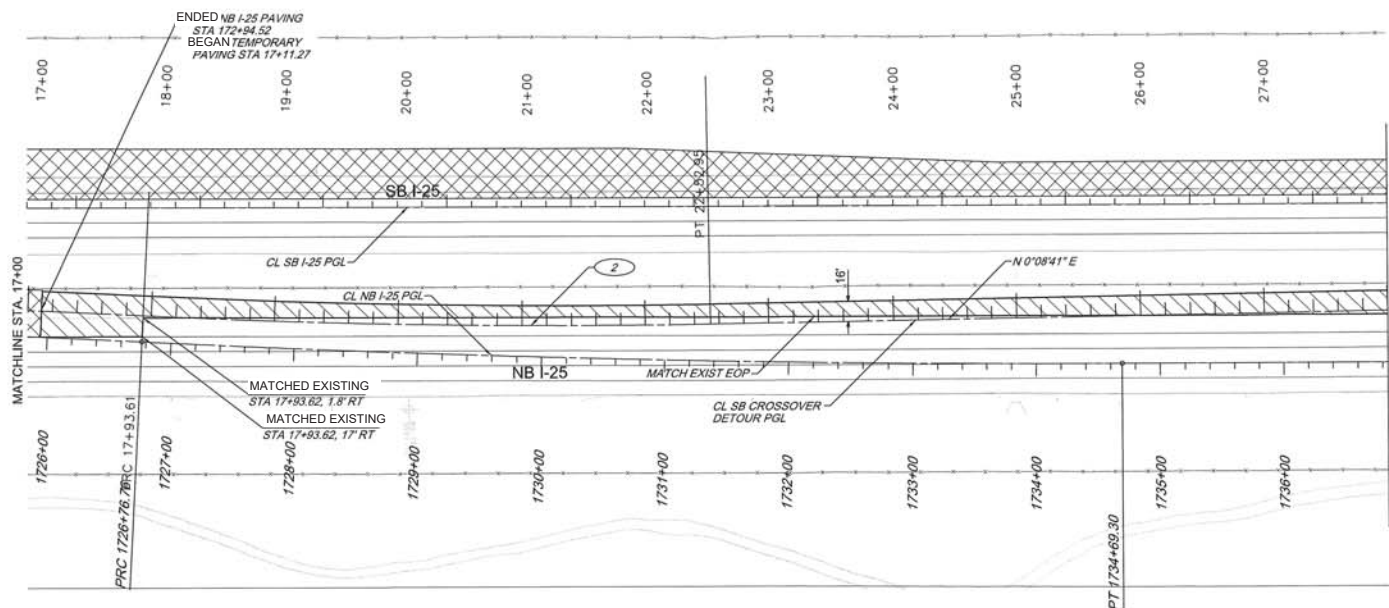
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE

MOT PLAN & PROFILE
PHASE 1 - SB NORTH CROSSOVER



SOUTHBOUND I-25 NORTH CROSSOVER DETOUR PROFILE @ CL



CURVE DATA SB NORTH CROSSOVER

2
P.I. STA = 20+23.37
Δ = 3°47'30"
Dc = 0°49'32"
L = 228.75'
T = 459.94'
R = 6940.66'



1/10/2017

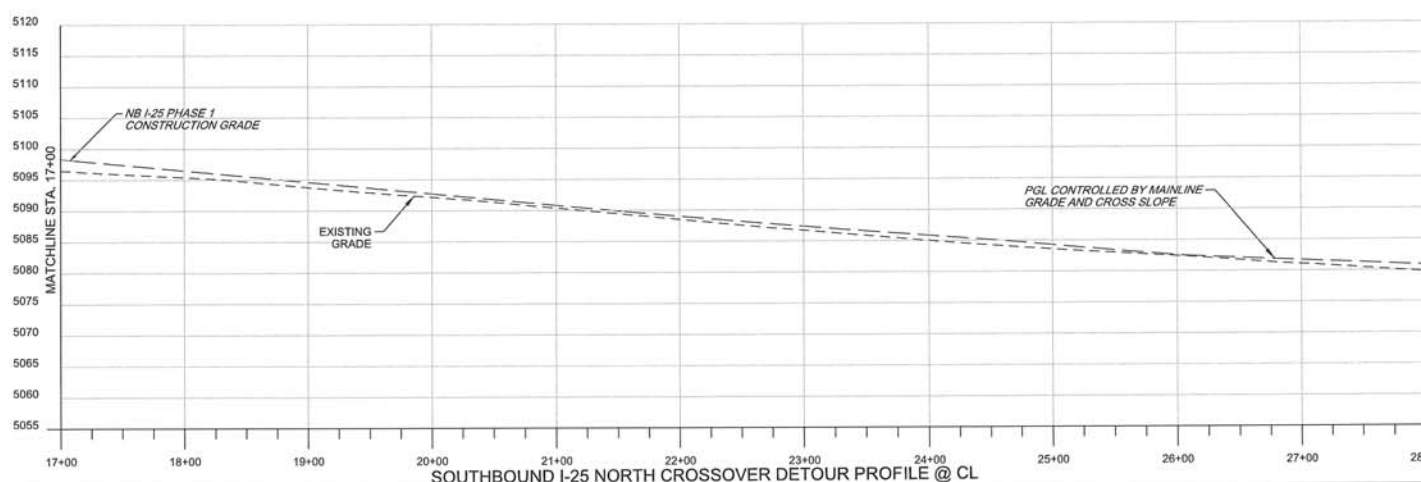
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Vert 1"=5'
SCALE IN FEET

NO.	DESCRIPTION	DATE	BY
1			
2			
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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE

MOT PLAN & PROFILE
PHASE 1 - SB NORTH CROSSOVER



SOUTHBOUND I-25 NORTH CROSSOVER DETOUR PROFILE @ CL

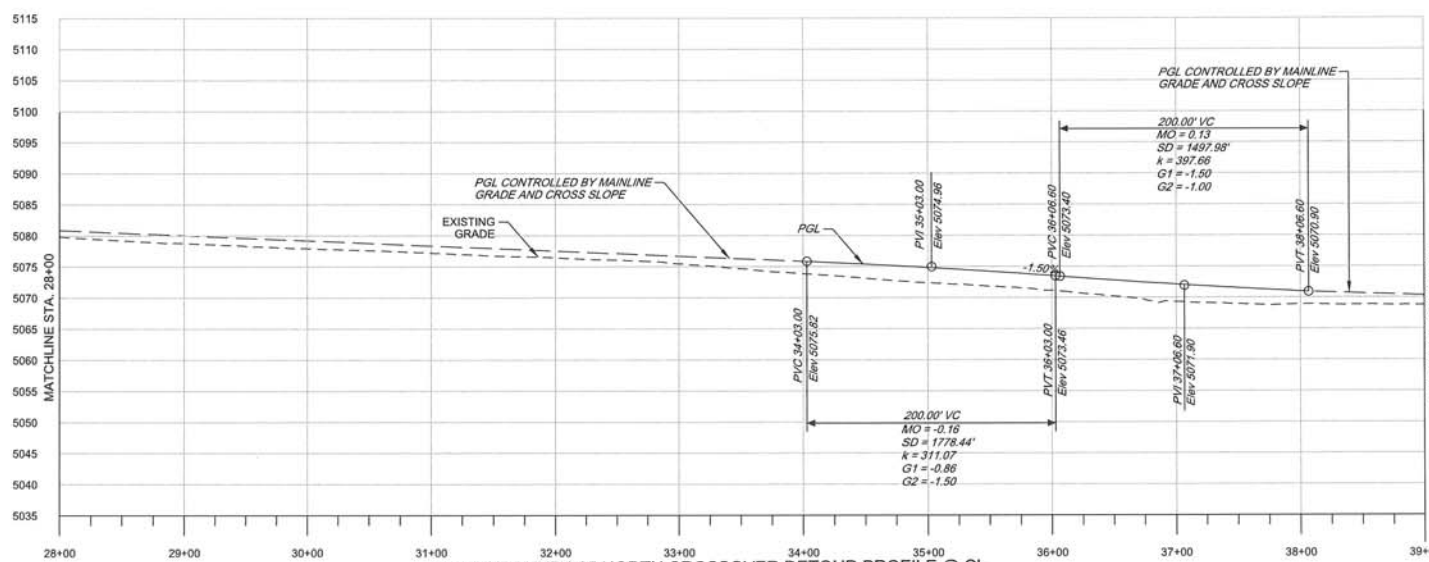
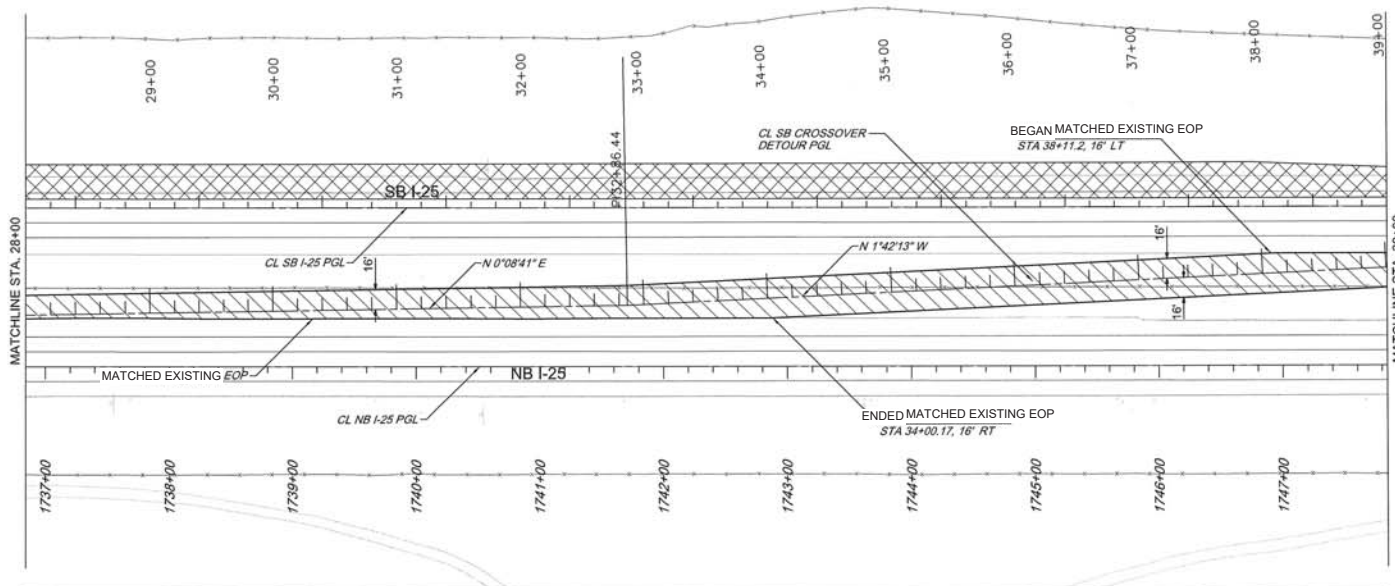
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1/10/2017 4:36:51 PM kyle.jehta
HWD01.pdf.phcfq

TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6001 Shattuck Parkway, Suite 100
Boulder, Colorado 80501
1.888.888.8888

AECOM
DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1:

SHEET NO. 6 - 87



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
4			
5			
6			

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN & PROFILE
PHASE 1 - SB NORTH CROSSOVER

DESIGNED BY: AECOM
PHONE: (505) 855-7500
EMAIL:

Design File: p:\1617478-PR01\AECOM\online\local\AECOM_0501_NA\Documents\60393326-I25-Rio Bravo\A300280\Plans\6_Sheets\A3002803MOTPP03.dgn
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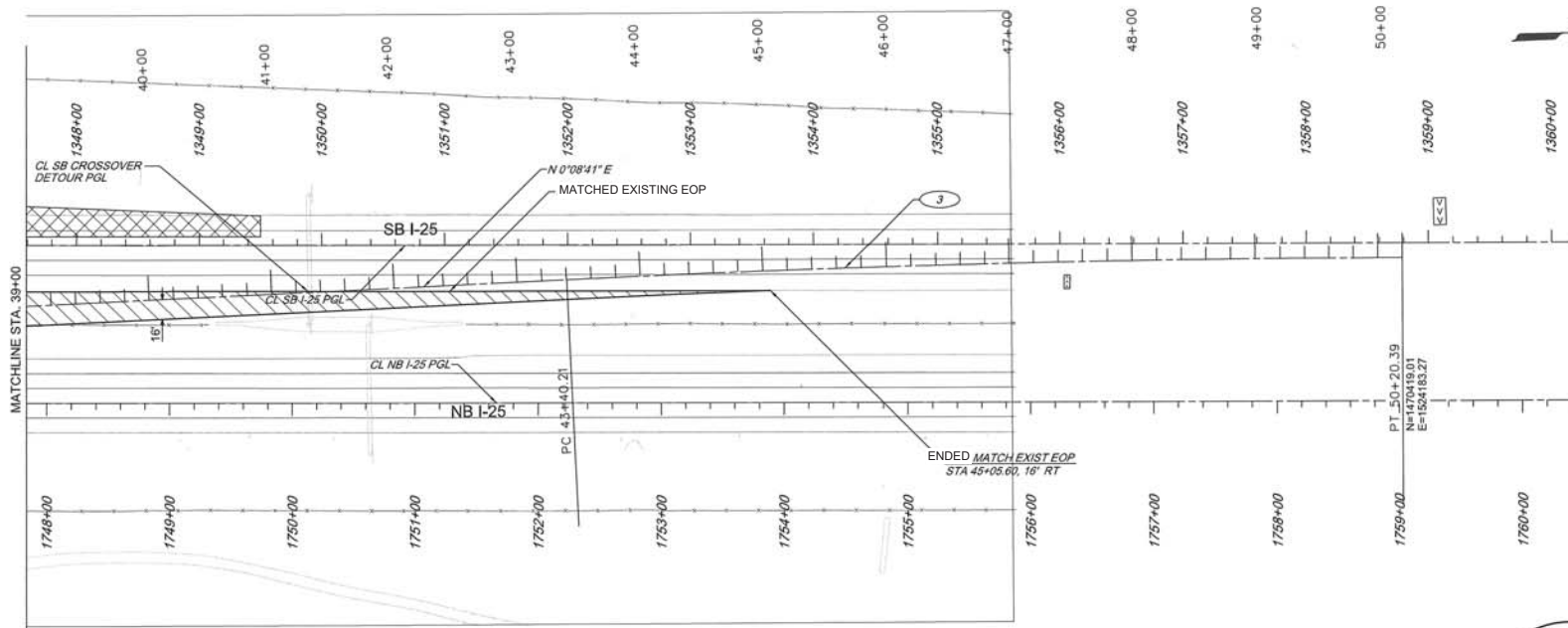
TRANSPORTATION
AECOM TECHNICAL SERVICES, Inc.
2000 Avenida Parkway SE, Suite 300
Albuquerque, New Mexico 87110
TEL: 505.243.0000

AECOM
www.aecom.com

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1:

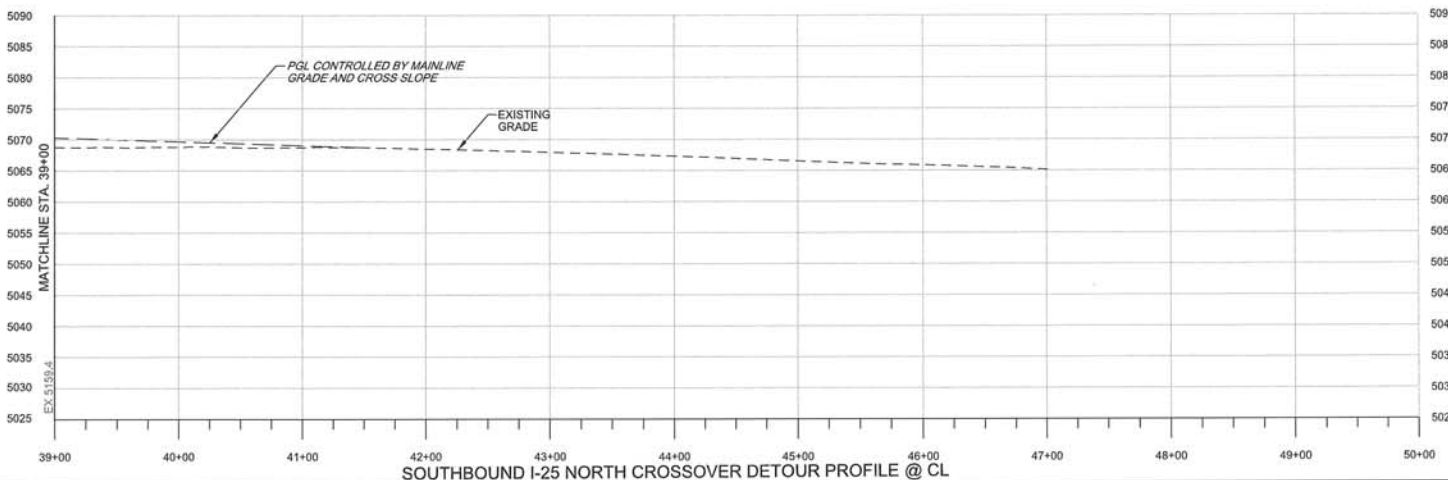
SHEET NO. 6 - 88



CURVE DATA

SB NORTH CROSSOVER

3
P.I. STA=46+80.36
Δ 2°45'50"
DS 0°24'23"
T 340.16'
L 680.18'
R 14100.00'



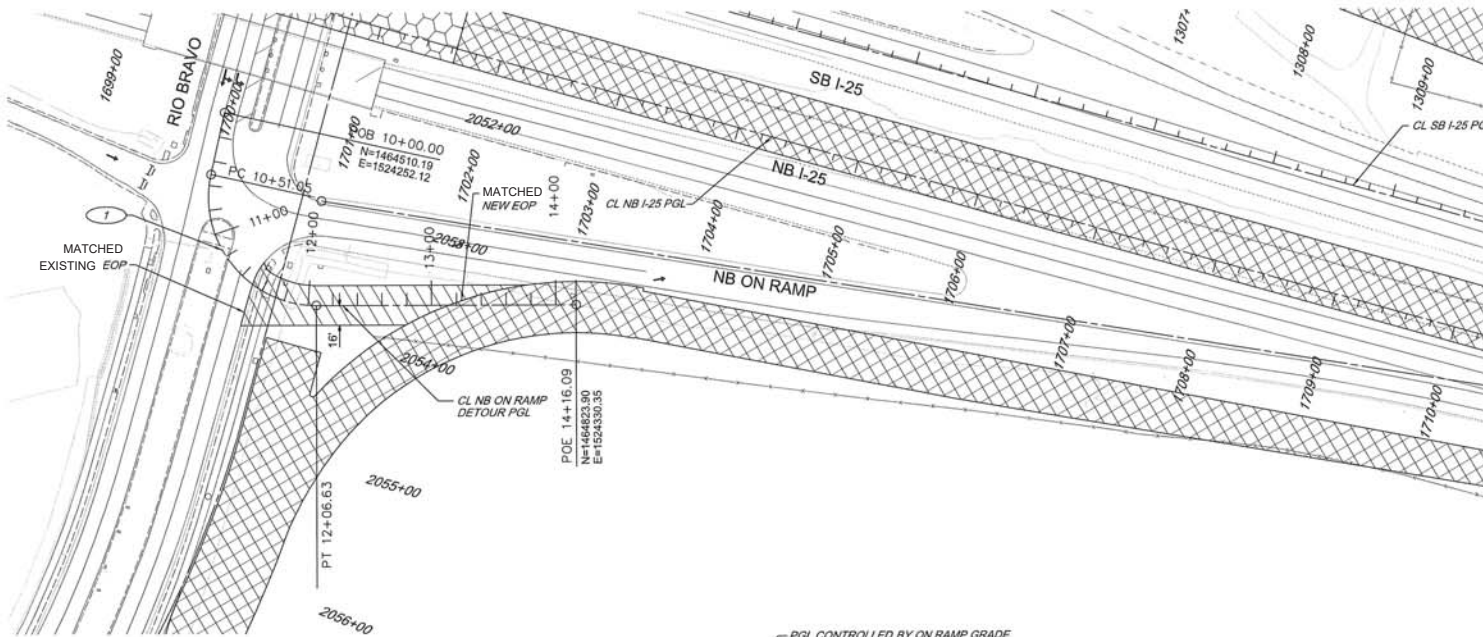
NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
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6			

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

MOT PLAN & PROFILE

PHASE 1 - SB NORTH CROSSOVER

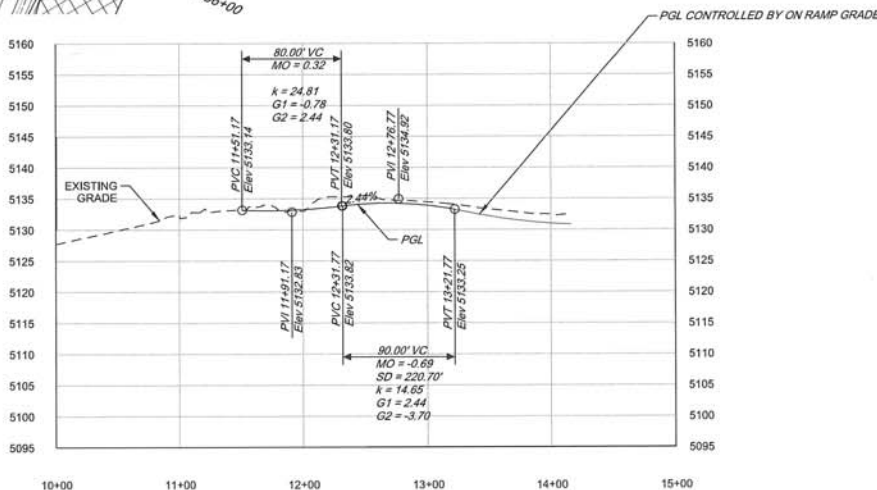


CURVE DATA DETOUR NB ON RAMP

1	$P1 STA = 11+59.37$ $\Delta = 102^\circ 27' 24''$ $DC = 65' 51.26''$ $T = 108.32'$ $L = 155.57'$ $R = 87.00'$
---	--



1/10/2017



PHASE 1 - DETOUR EB-NB PROFILE @ CL

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
4			
5			
6			

NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
MOT PLAN & PROFILE
PHASE 1 - DETOUR NB ON RAMP

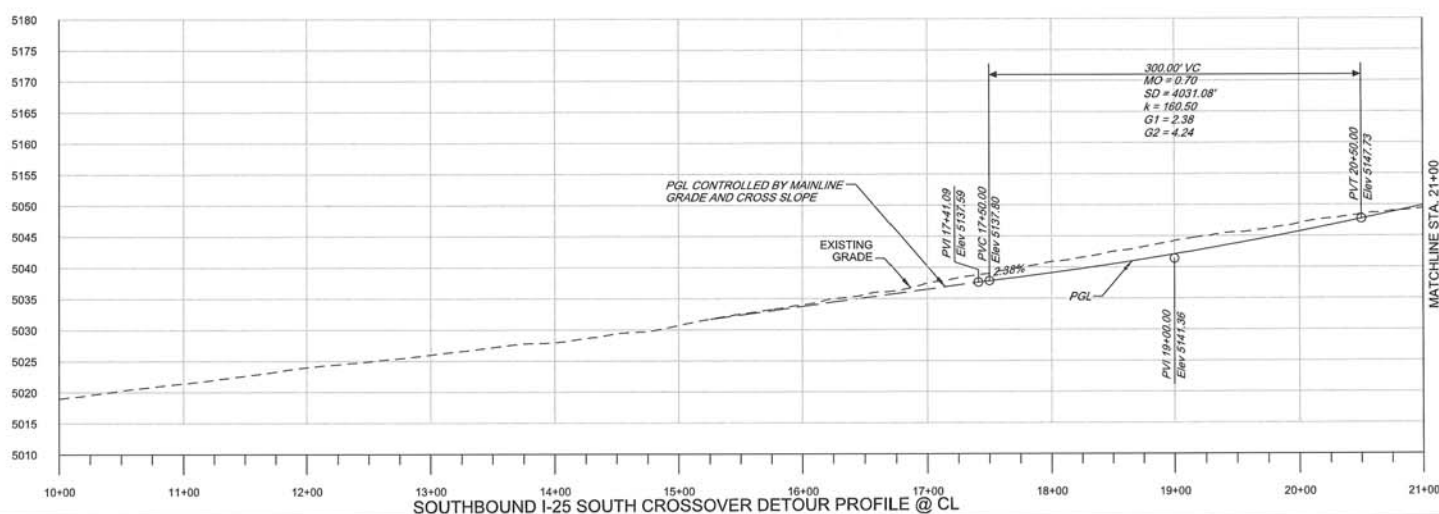
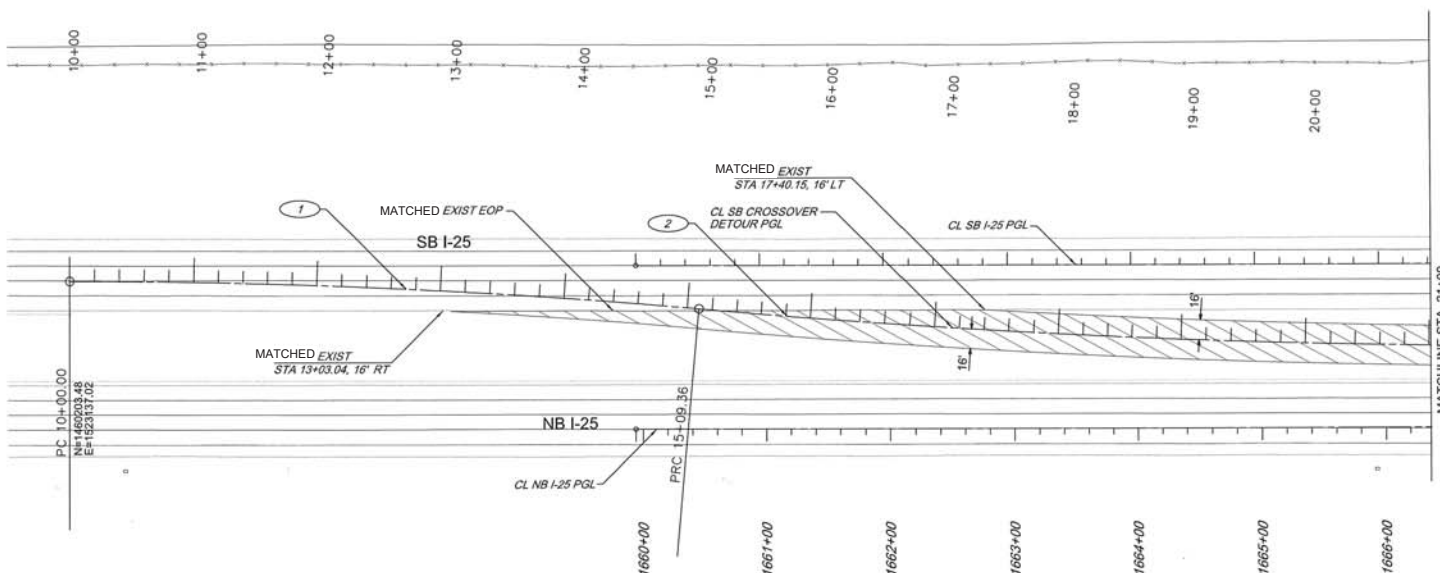


CURVE DATA

SB SOUTH CROSSOVER

1
P.I. STA = 12+54.85
Δ 5°07'12"
Dc 1°00'19"
T 254.85'
L 509.36'
R 5700.00'

2
P.I. STA = 18+50.72
Δ 4°58'02"
Dc 0°43'41"
T 341.36'
L 682.29'
R 7870.00'

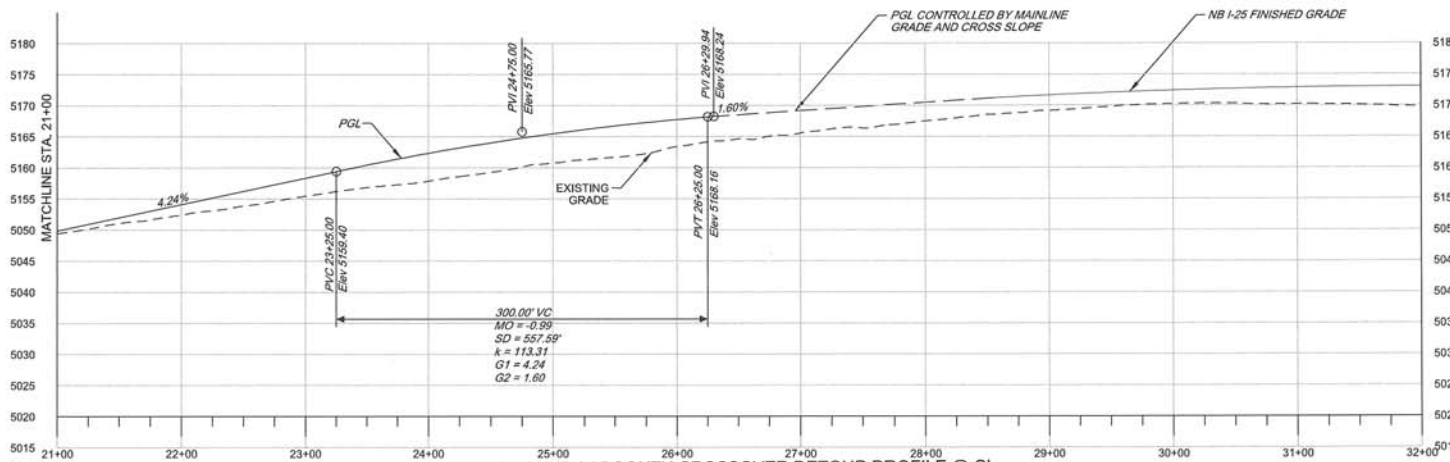
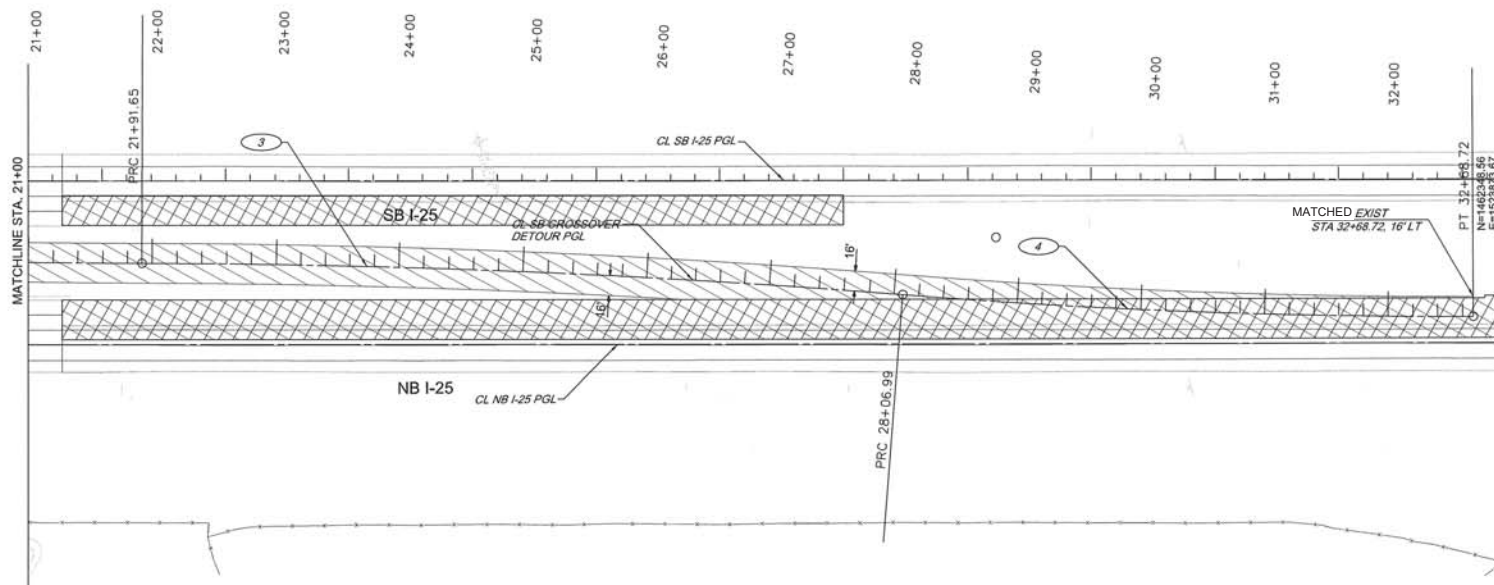


1/10/2017

SCALE: Horiz 1"=50'
Vert 1"=5'
SCALE IN FEET

NO.	REVISIONS (OR CHANGE NOTICES)	DATE	BY
1	DISCUSSION		
2			
3			
4			
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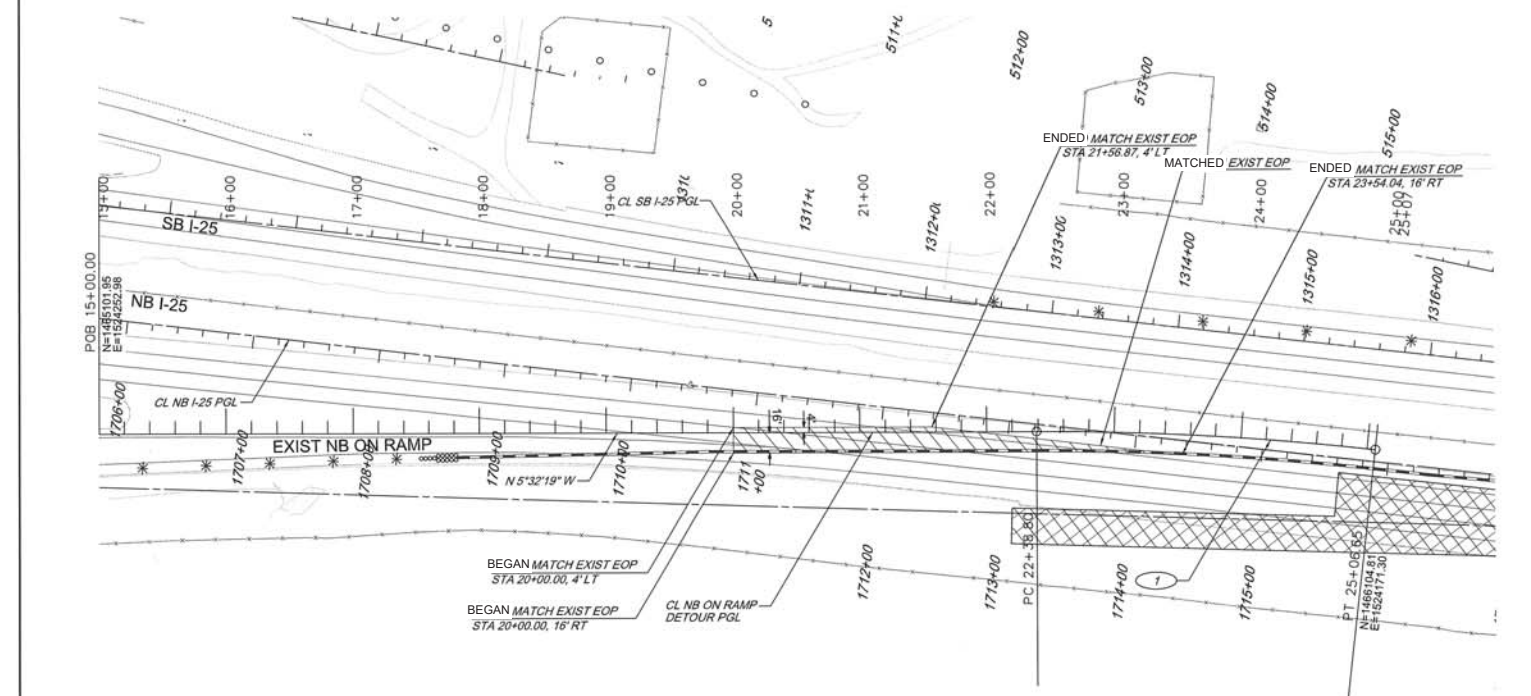
NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
MOT PLAN & PROFILE
PHASE 1 - SB SOUTH CROSSOVER



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
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NEW MEXICO DEPARTMENT
 OF TRANSPORTATION
 I-25/RIO BRAVO
 INTERCHANGE
 MOT PLAN & PROFILE
 PHASE 1 - SB SOUTH CROSSOVER

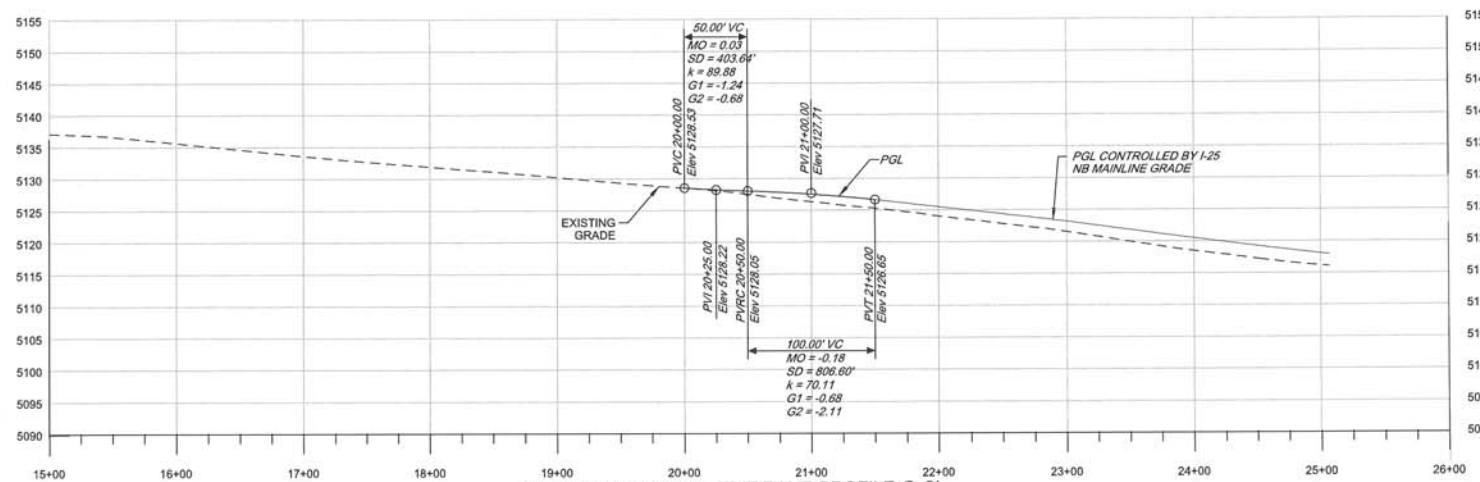
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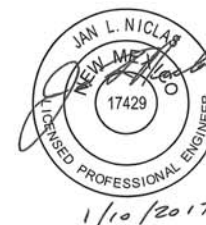
CURVE DATA

DETOUR NB EXIT RAMP

1
P.I. STA = 23+72.88
$\Delta = 6^{\circ}38'16''$
De = 2'28.41'
T = 134.07'
L = 267.85'
R = 2312.00'



PHASE 2 - DETOUR NB EXIT RAMP PROFILE @ CL



NO.	DESCRIPTION	DATE	BY
6			
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
MOT PLAN & PROFILE			
PHASE 2 - NB ON RAMP			

DESIGNED BY: AECOM
PHONE: (202) 855-7250
TX: 441

Design File: p:\1\617479-PWNT.aecomonline.local\AECOM_D501_NA\Documents\60393328-I25-Rio Bravo\A300280\Plans\6_Sheets\A300280\W01PP08.dgn
1/10/2017 4:57:26 PM kyle.lehto

TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
200 American Parkway SE, Suite 100

AECOM DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280 DRAWING SCALE: 1:

SHEET NO. 6 - 93

TRAFFIC SIGNAL LEGEND

NEW	EXISTING	ITEM
		PULL BOX
		SERVICE POLE
		METER PEDESTAL
		CONTROLLER CABINET
		CONDUIT RUN (SIGNALS)
		LOOP DETECTOR
		TRAFFIC SIGNAL PEDESTAL POLE
		CONDUIT RUN NUMBER (SIGNAL)
		TYPE II STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACKPLATE, AND OPTICAL DETECTOR
		TYPE III STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACKPLATE, OPTICAL DETECTOR, LUMINAIRE, AND VIDEO CAMERA
		PEDESTRIAN PUSH BUTTON (MOUNTED TO SIDE OF POLE WHERE INDICATED)
		PEDESTRIAN SIGNALS (MOUNTED TO SIDE OF POLE WHERE INDICATED)
		SPLICE CABINET
		TRAFFIC MANHOLE
		VIDEO CAMERA
		OVERHEAD WIRE
		OVERHEAD WIRE WITH TENSION CABLE

GENERAL NOTES:

- ALL MATERIAL AND INSTALLATION METHODS FOR THE SIGNAL WAS ~~BE~~ IN CONFORMANCE WITH THE APPLICABLE PARTS OF THE NMDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, 2014 EDITION.
- FOOTAGE OF POWER CABLE FOR LUMINAIRE, VIDEO POWER CABLE, OPTICAL DETECTOR CABLE, CONDUIT CLAMPS, NIPPLES, ELBOWS, AND TEES FOR TEMPORARY SIGNAL SPANS WERE CONSIDERED INCIDENTAL TO ITEM 702700 TEMPORARY SIGNAL SPAN.
- THE CONTRACTOR COORDINATED POWER SERVICE REQUIREMENTS WITH THE PUBLIC SERVICE COMPANY OF NEW MEXICO (PNM).
- IN THE EVENT OF ACCIDENTAL DAMAGE TO THE SIGNAL SYSTEM OR POWER OUTAGE, THE CONTRACTOR SHALL PROVIDE FLAGGERS OR TEMPORARY STOP SIGNS FOR THE DURATION OF PERIOD OF NON-OPERATION TO CONTROL TRAFFIC. ANY EQUIPMENT SHALL BE REPLACED AT NO EXPENSE TO THE PROJECT. SIGNALS SHALL BE RESTORED TO WORKING ORDER WITHIN 4 HOURS OF REPORTED NON-FUNCTION.
- IF THE CONTRACTOR REQUIRES A COPY OF THE WIRING/CONDUIT CHARTS THEY CAN BE OBTAINED FROM THE PROJECT ENGINEER UPON REQUEST.
- SINGLE CONDUCTOR 6 USED FOR NEUTRAL CONNECTION TO SIGNAL CABINET.

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EMAIL: AS300280@aecom.com

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TRANSPORTATION
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1 inch = 1 foot

AECOM
DESIGNED BY: AECOM
www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 94



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1	ADDENDUM 5	5/27/17	CPR
NO.	DESCRIPTION	DATE	BY
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
TEMPORARY SIGNAL GENERAL NOTES			
I-25/RIO BRAVO			

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TRAFFIC SIGNAL LEGEND

NEW	EXISTING	ITEM
		PULL BOX
		SERVICE POLE
		METER PEDESTAL
		CONTROLLER CABINET
		CONDUIT RUN (SIGNALS)
		LOOP DETECTOR
		TRAFFIC SIGNAL PEDESTAL POLE
		CONDUIT RUN NUMBER (SIGNAL)
		TYPE II STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACKPLATE, AND OPTICAL DETECTOR
		TYPE III STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACKPLATE, OPTICAL DETECTOR, LUMINAIRE, AND VIDEO CAMERA
		PEDESTRIAN PUSH BUTTON (MOUNTED TO SIDE OF POLE WHERE INDICATED)
		PEDESTRIAN SIGNALS (MOUNTED TO SIDE OF POLE WHERE INDICATED)
		SPLICE CABINET
		TRAFFIC MANHOLE
		VIDEO CAMERA
		OVERHEAD WIRE
		OVERHEAD WIRE WITH TENSION CABLE

GENERAL NOTES:

- ALL MATERIAL AND INSTALLATION METHODS FOR THE SIGNAL SHALL BE IN CONFORMANCE WITH THE APPLICABLE PARTS OF THE NMOT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, 2014 EDITION (GRAY BOOK).
- SIGNAL TIMING SHALL BE PROGRAMMED INTO SIGNAL CONTROL EQUIPMENT BY THE CONTRACTOR WITH EQUIPMENT MANUFACTURER'S FIELD ASSISTANCE AS APPROVED IN THE FIELD BY NMOT SIGNAL LABORATORY PERSONNEL.
- SEE STANDARD DRAWING 713S-02-1/1 FOR ADDITIONAL DETAILS FOR THE MACHINE VISION VEHICLE DETECTION SYSTEM.
- SEE STANDARD DRAWING 713S-01 1/1 FOR ADDITIONAL DETAILS FOR THE OPTICAL DETECTOR EMERGENCY VEHICLE PRE-EMPTION SYSTEM.
- FOOTAGE OF POWER CABLE FOR LUMINAIRE, VIDEO POWER CABLE, OPTICAL DETECTOR CABLE, CONDUIT CLAMPS, NIPPLES, ELBOWS, AND TEES FOR TEMPORARY SIGNAL SPANS WILL BE CONSIDERED INCIDENTAL TO ITEM 702700 TEMPORARY SIGNAL SPAN.
- THE CONTRACTOR SHALL COORDINATE POWER SERVICE REQUIREMENTS WITH THE PUBLIC SERVICE COMPANY OF NEW MEXICO (PNM).
- IN THE EVENT OF ACCIDENTAL DAMAGE TO THE SIGNAL SYSTEM OR POWER OUTAGE, THE CONTRACTOR SHALL PROVIDE FLAGGERS OR TEMPORARY STOP SIGNS FOR THE DURATION OF PERIOD OF NON-OPERATION TO CONTROL TRAFFIC. ANY EQUIPMENT SHALL BE REPLACED AT NO EXPENSE TO THE PROJECT. SIGNALS SHALL BE RESTORED TO WORKING ORDER WITHIN 4 HOURS OF REPORTED NON-FUNCTION.
- IF THE CONTRACTOR REQUIRES A COPY OF THE WIRING/CONDUIT CHARTS THEY CAN BE OBTAINED FROM THE PROJECT ENGINEER UPON REQUEST.
- ITEMS LISTED UNDER 702700 ARE ONLY A GENERAL DESCRIPTION OF THE REQUIRED WORK AND MATERIALS, AND MAY OR MAY NOT BE COMPLETE. THIS LIST DOES NOT INCLUDE ANY INCIDENTAL WORK OR MATERIALS REQUIRED BY THE SPECIAL PROVISIONS SERIALS (STANDARD DETAILS), SUPPLEMENTAL SPECIFICATIONS, OR THE STANDARD SPECIFICATIONS. ALL ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE COMPLETION OF THE PROJECT UNLESS OTHERWISE STATED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVELOPING AND IMPLEMENTING SIGNAL TIMINGS FOR TEMPORARY SIGNAL SPANS DURING CONSTRUCTION. ALL TEMPORARY SIGNAL TIMINGS SHALL BE APPROVED BY NMOT SIGNAL LABORATORY PERSONNEL.
- SINGLE CONDUCTOR 6 USED FOR NEUTRAL CONNECTION TO SIGNAL CABINET.

TEMPORARY TRAFFIC SIGNAL QUANTITIES I-25/RIO BRAVO

LINE ITEM	DESCRIPTION	UNIT	UNIT 1	UNIT 2	PROJECT TOTAL
001	SERVICE RISER (SIGNAL)	EACH	11	5	16
002	WOOD POLE FULLY TREATED	EACH	11	5	16
003	EYE BOLT 5/8"	EACH	22	10	32
004	2" SERVICE ENTRANCE WEATHERHEAD	EACH	11	5	16
005	COPPERWELD 3/4" X 10' GROUND ROD	EACH	11	5	16
006	GROUND ROD CLAMP	EACH	11	5	16
007	NO. 6 BARE COPPER GROUND WIRE	L.F.	2,101	990	3,091
008	3/8" HIGH STRENGTH GUY WIRE, POLE CLAMPS, TURNBUCKLE, AND THIMBLES	EACH	9	5	14
009	U-BOLT CLAMPS, AND DOWN GUYS	EACH	18	9	27
010	MULTI-CONDUCTOR CABLE 5	L.F.	9,285	3,600	12,885
011	MULTI-CONDUCTOR CABLE 7	L.F.	2,345	900	3,245
012	SINGLE CONDUCTOR 6	L.F.	2,101	990	3,091
013	4 SECTION TRAFFIC SIGNAL ASSEMBLY LED	EACH		2	2
014	4 SECTION BACKPLATE	EACH		2	2
015	TEMPORARY CABINET COMPLETE	EACH	1	1	2
016	POWER SERVICE INSTALLATION	L.S.	L.S.	L.S.	L.S.
017	SIGNAL/LIGHTING SYSTEM START-UP COSTS	ALLOW	ALLOW	ALLOW	ALLOW

702700 TEMPORARY SIGNAL SPAN

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 AECOM JOB NO. 60303326



NO.	DESCRIPTION	DATE	BY
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REVISIONS AND CHANGE NOTICES			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25/RIO BRAVO INTERCHANGE			
TEMPORARY SIGNAL GENERAL NOTES I-25/RIO BRAVO			

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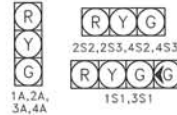
NEW MEXICO PROJECT NO. A300280

SHEET NO. 8 - 84

As-Built

NEW MEXICO PROJECT CONTROL NO. A300280

TYPICAL SIGNAL FACE LENS ARRANGEMENTS



ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

	SIGNAL POLE OR CABINET ID
	CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

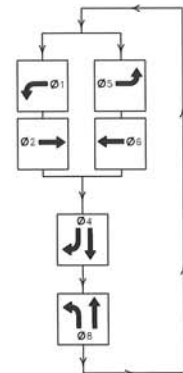
INITIALIZATION

STEADY ALL RED, THEN PHASES 2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

SIGNAL PHASING



NO.	DESCRIPTION	DATE	BY
1			
2			
3			
4			
5			
6			

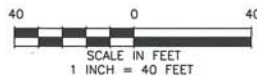
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 1A BROADWAY BLVD & RIO BRAVO BLVD

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20'	2024+13	91.23' LT
SWP2	TIMBER POLE-35'	2024+75	80.98' LT
SWP3	TIMBER POLE-35'	2026+15	86.00' LT
SWP4	TIMBER POLE-35'	2026+81	90.58' RT
SWP5	TIMBER POLE-35'	2025+21	95.66' RT



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NEW MEXICO PROJECT NO. A300280

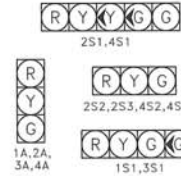
SHEET NO. 8 - 15

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 EMAIL: XXXXXXX@aecom.com

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TYPICAL SIGNAL FACE LENS ARRANGEMENTS



ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

	SIGNAL POLE OR CABINET ID
	CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

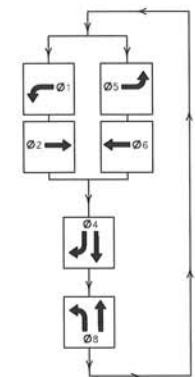
INITIALIZATION

STEADY ALL RED, THEN PHASES 2 & 6 GREEN

NOTES

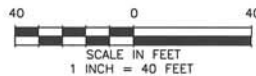
- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

SIGNAL PHASING



TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20	2024+13	91.23' LT
SWP2	TIMBER POLE-35	2024+75	80.96' LT
SWP3	TIMBER POLE-35	2026+15	86.00' LT
SWP4	TIMBER POLE-35	2026+81	90.58' RT
SWP5	TIMBER POLE-35	2025+21	95.66' RT



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
TEMPORARY SIGNAL PLAN PHASE 1B
BROADWAY BLVD & RIO BRAVO BLVD

As-Built

NEW MEXICO PROJECT CONTROL NO. A300280

TYPICAL SIGNAL FACE LENS ARRANGEMENTS



2S1,4S1



2S2,2S3,4S2,4S3



1S1,3S1



1S1,3S1



2S1,4S1

2S2,2S3,4S2,4S3,
1A,2A,3A,4A

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

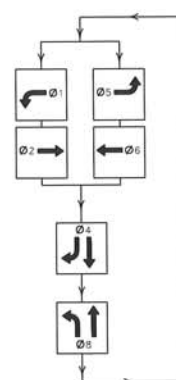
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
SA	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

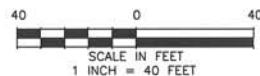
MA1	SIGNAL POLE OR CABINET ID
1	CONDUIT RUN ID

SIGNAL PHASING



TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20'	2024+13	91.23' LT
SWP2	TIMBER POLE-35'	2024+75	80.96' LT
SWP3	TIMBER POLE-35'	2026+15	86.00' LT
SWP4	TIMBER POLE-35'	2026+81	90.58' RT
SWP5	TIMBER POLE-35'	2025+21	95.66' RT



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 1C
BROADWAY BLVD & RIO BRAVO BLVD

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

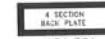
2S1,4S1



2S2,2S3,4S2,4S3



1S1,3S1



1S1,3S1

2S2,2S3,4S2,4S3,
1A,2A,3A,4A

2S1,4S1

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

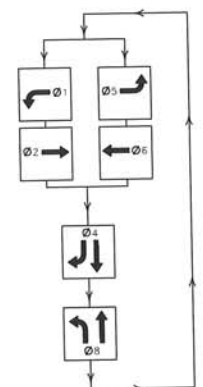
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
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3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1	SIGNAL POLE OR CABINET ID
1	CONDUIT RUN ID

SIGNAL PHASING



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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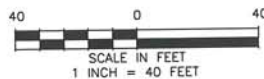
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

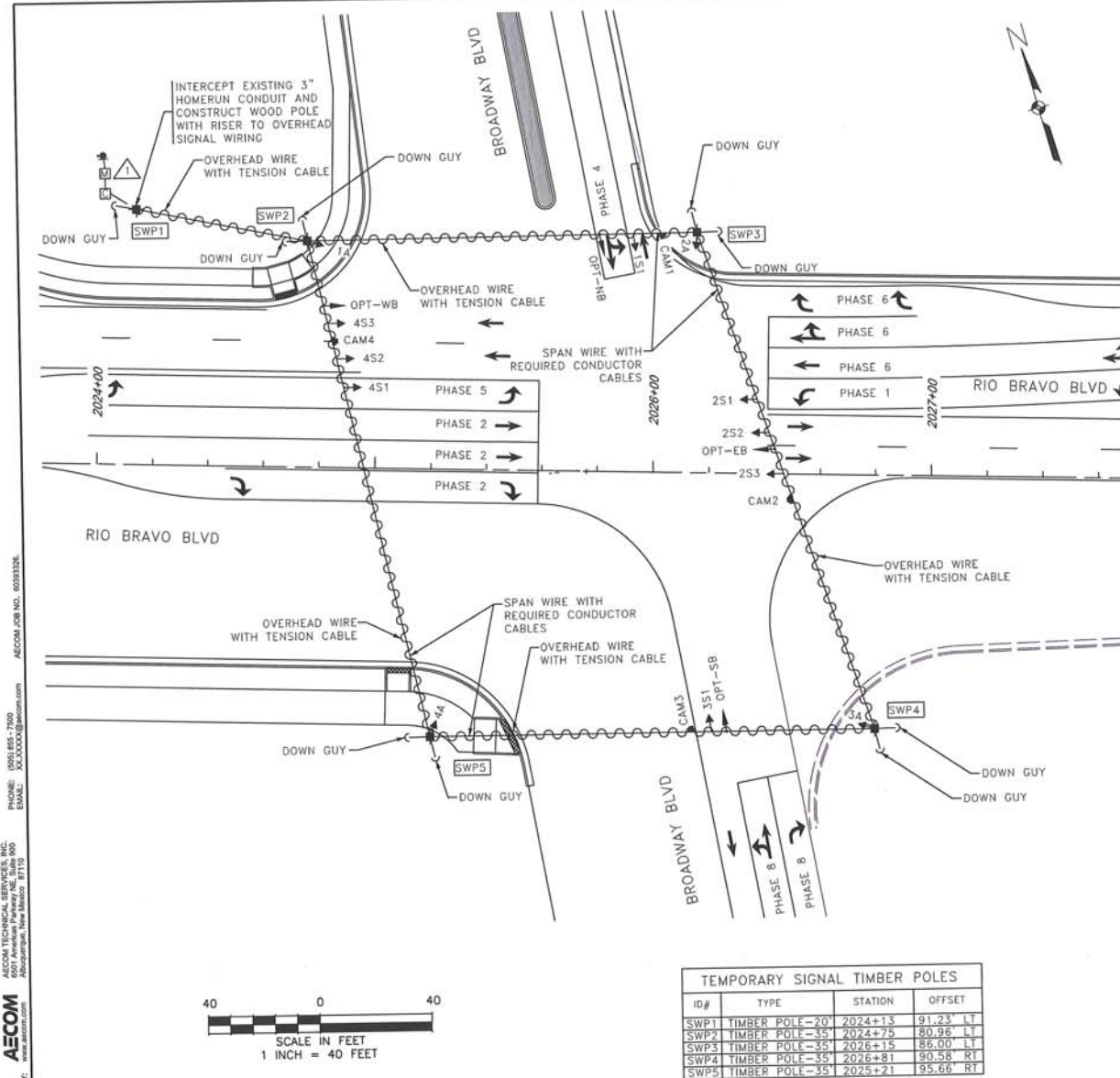
1-25/RIO BRAVO
INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 2A
BROADWAY BLVD & RIO BRAVO BLVD

TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20'	2024+13	91.23' LT
SWP2	TIMBER POLE-35'	2024+75	80.96' LT
SWP3	TIMBER POLE-35'	2026+15	86.00' LT
SWP4	TIMBER POLE-35'	2026+81	90.58' RT
SWP5	TIMBER POLE-35'	2025+21	95.66' RT



TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

RYYGG
2S1,4S1

RYG
2S2,2S3,4S2,4S3

RYGG
1S1,3S1

1A,2A,
3A,4A

4 SECTION
BACK PLATE
1S1,3S1

5 SECTION
BACK PLATE
2S1,4S1

7 SECTION
BACK PLATE
2S2,2S3,4S2,4S3,
1A,2A,3A,4A

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

1. CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
2. CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
3. LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
4. EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
5. EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
6. CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
7. ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

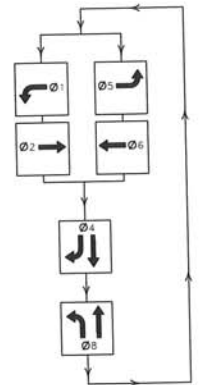
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

[MA]	SIGNAL POLE OR CABINET ID
[1]	CONDUIT RUN ID

SIGNAL PHASING



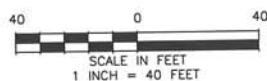
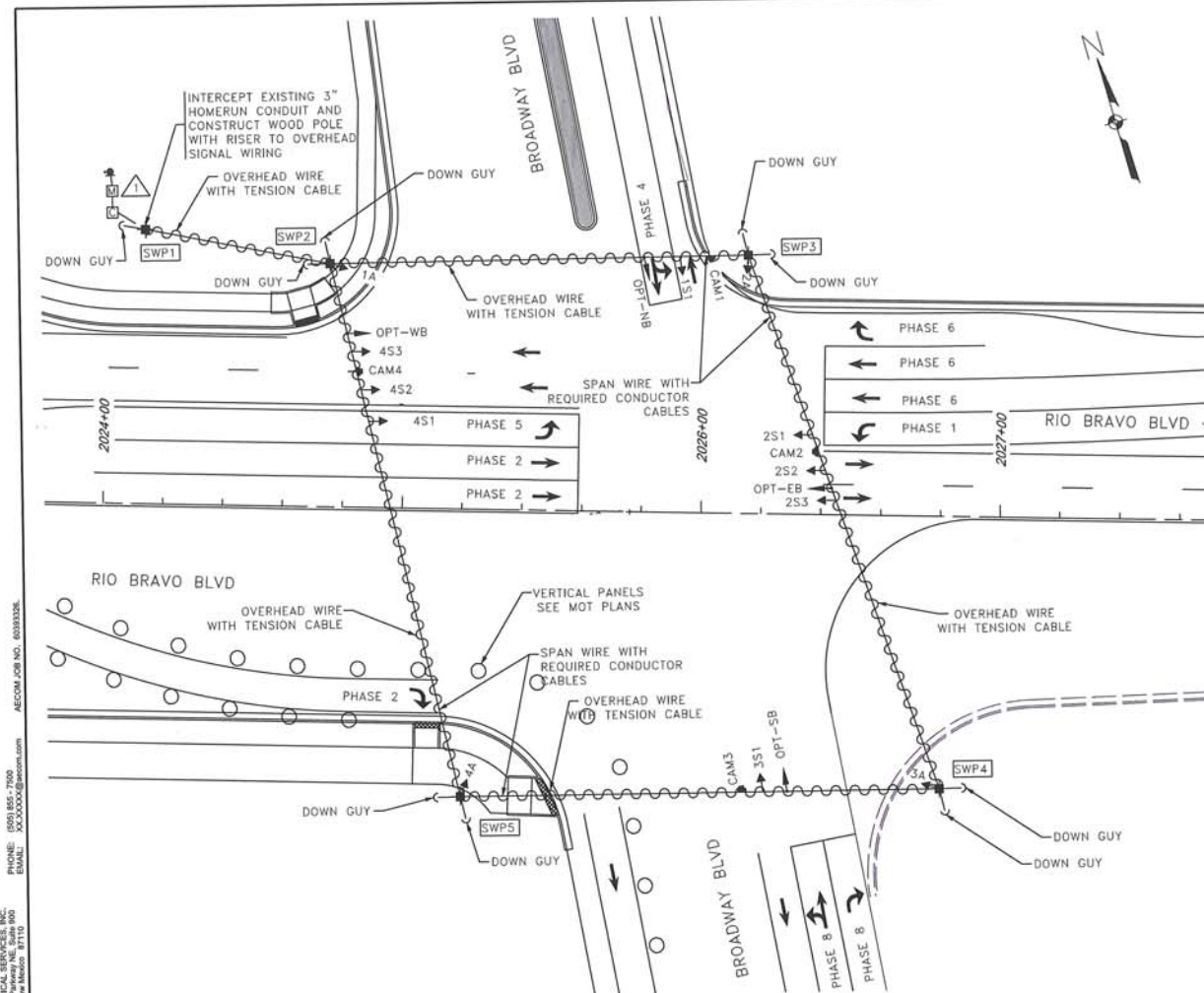
TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20'	2024+13	91.23 LT
SWP2	TIMBER POLE-35'	2024+75	80.96 LT
SWP3	TIMBER POLE-35'	2026+15	86.00 LT
SWP4	TIMBER POLE-35'	2026+81	90.58 RT
SWP5	TIMBER POLE-35'	2025+71	95.66 RT



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGETEMPORARY SIGNAL PLAN PHASE 2B
BROADWAY BLVD & RIO BRAVO BLVD



TEMPORARY SIGNAL TIMBER POLES				
ID#	TYPE	STATION	OFFSET	
SWP1	TIMBER POLE-20	2024+13	91.23	LT
SWP2	TIMBER POLE-35	2024+75	80.96	LT
SWP3	TIMBER POLE-35	2026+15	86.00	LT
SWP4	TIMBER POLE-35	2026+81	90.58	RT
SWP5	TIMBER POLE-35	2025+21	95.66	RT

TYPICAL SIGNAL FACE LENS ARRANGEMENTS



2S1,4S1



2S2,2S3,4S2,4S3



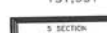
1A,2A,3A,4A



1S1,3S1



1S1,3S1



2S2,2S3,4S2,4S3,1A,2A,3A,4A

2S1,4S1

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES 2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER POLE SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

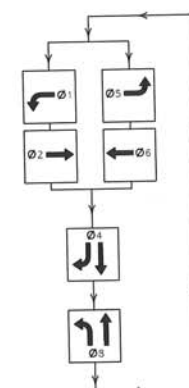
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1	SIGNAL POLE OR CABINET ID
1	CONDUIT RUN ID

SIGNAL PHASING



NO.	DESCRIPTION	DATE	BY
1			
2			
3			
4			
5			
6			

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 2C BROADWAY BLVD & RIO BRAVO BLVD



TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

R Y Y G G

2S1,4S1

R
Y
G

R Y G

2S2,2S3,4S2,4S3

1A,2A,
3A,4A

R Y G G

1S1,3S1

4 SECTION
BACK PLATE

1S1,3S1

5 SECTION
BACK PLATE

2S1,4S1

3 SECTION
BACK PLATE2S2,2S3,4S2,4S3,
1A,2A,3A,4A

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MAT	SIGNAL POLE OR CABINET ID
△	CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

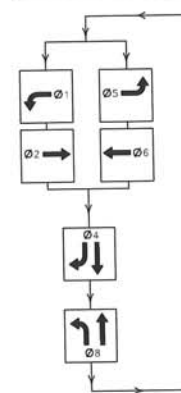
INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

SIGNAL PHASING

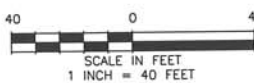
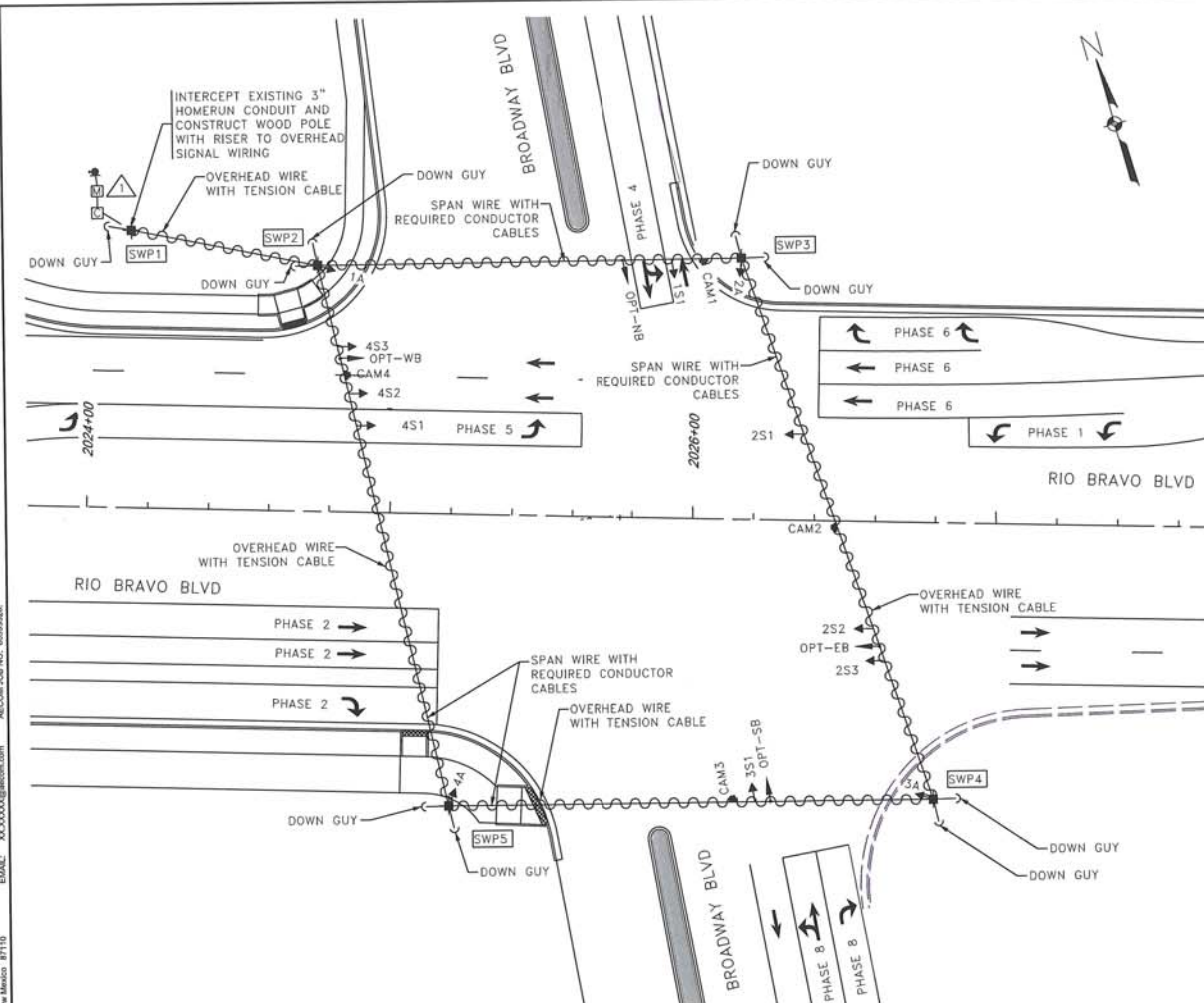


ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20	2024+13	91.23' L
SWP2	TIMBER POLE-35	2024+75	80.96' L
SWP3	TIMBER POLE-35	2026+15	86.00' L
SWP4	TIMBER POLE-35	2026+81	90.58' RT
SWP5	TIMBER POLE-35	2025+21	95.66' RT



NO.	DESCRIPTION	DATE	BY
6			
5			
4			
3			
2			
1			

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGETEMPORARY SIGNAL PLAN PHASE 3A
BROADWAY BLVD & RIO BRAVO BLVD

AECOM JOB NO. 6038326

AECOM JOB NO. 6038326

AECOM JOB NO. 6038326

AECOM

DESIGNED BY:

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TRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
601 Avenida Parkway NE, Suite 500
Albuquerque, New Mexico 87110
T 505.261.0700

AECOM

DESIGNED BY: AECOM

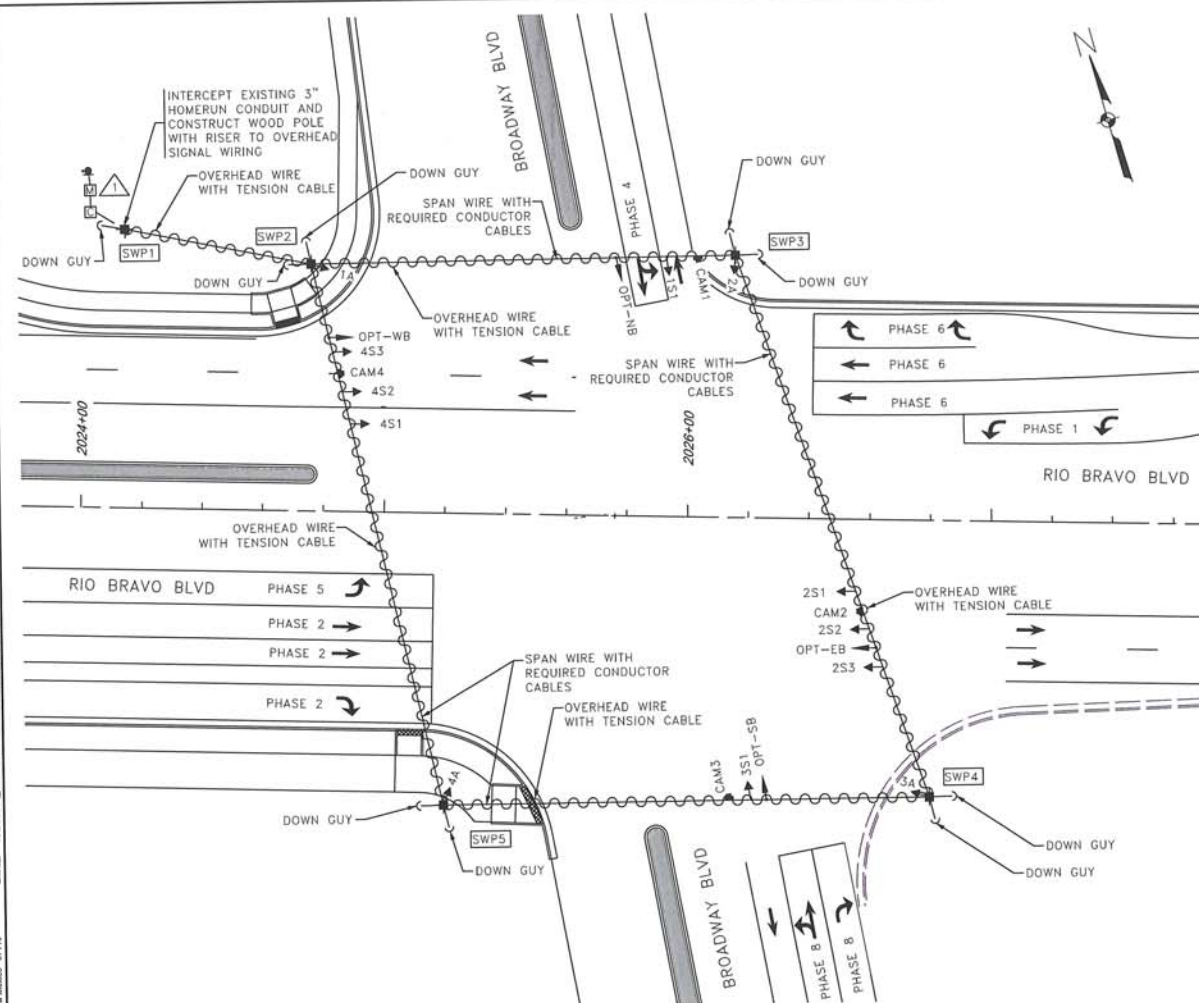
NEW MEXICO PROJECT NO. A30280

SHEET NO. 6 - 101

AECOM JOB NO. 60393326

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EMAIL: aecom@aecom.comAECOM TECHNICAL SERVICES, INC.
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40 0 40
SCALE IN FEET
1 INCH = 40 FEET

TEMPORARY SIGNAL TIMBER POLES			
ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE - 20'	2024+13	91.23' LT
SWP2	TIMBER POLE - 35'	2024+75	80.96' LT
SWP3	TIMBER POLE - 35'	2026+15	86.00' LT
SWP4	TIMBER POLE - 35'	2026+81	90.58' RT
SWP5	TIMBER POLE - 35'	2025+21	95.66' RT

TYPICAL SIGNAL FACE LENS ARRANGEMENTS

RYYG
2S1,4S1

RYG
2S2,2S3,4S2,4S3
1A,2A,3A,4A
1S1,3S1

4 SECTION
BACK PLATE
1S1,3S1

5 SECTION
BACK PLATE
2S1,4S1

3 SECTION
BACK PLATE
2S2,2S3,4S2,4S3,
1A,2A,3A,4A

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TEMPORARY SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

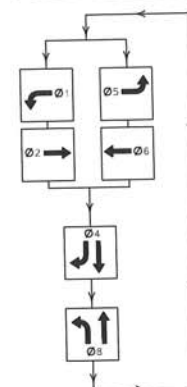
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE - FULLY TREATED

SYMBOL KEY

MAT	SIGNAL POLE OR CABINET ID
△	CONDUIT RUN ID

SIGNAL PHASING



NO.	DESCRIPTION	DATE	BY
1			
2			
3			
4			
5			
6			

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGETEMPORARY SIGNAL PLAN PHASE 3B
BROADWAY BLVD & RIO BRAVO BLVDTRANSPORTATION
AECOM TECHNICAL SERVICES, INC.
6001 American Parkway NE, Suite 100
Albuquerque, New Mexico 87110
1.905.855.7700AECOM
www.aecom.com

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300290

SHEET NO. 8 - 102

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

2S1,4S1



2S2,2S3,4S2,4S3



1A,2A, 3A,4A 1S1,3S1



1S1,3S1



2S2,2S3,4S2,4S3, 1A,2A,3A,4A



2S1,4S1

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

1. CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
2. CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
3. LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
4. EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
5. EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
6. CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
7. ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

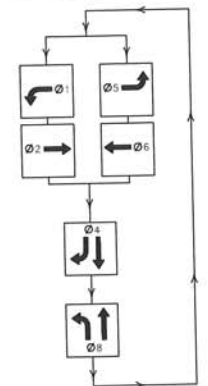
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

	SIGNAL POLE OR CABINET ID
	CONDUIT RUN ID

SIGNAL PHASING



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
4			
5			
6			

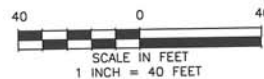
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 3C
BROADWAY BLVD & RIO BRAVO BLVD

TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20'	2024+13	91.23 LT
SWP2	TIMBER POLE-35	2024+75	80.96 LT
SWP3	TIMBER POLE-35	2026+15	86.00 LT
SWP4	TIMBER POLE-35	2026+81	90.58 RT
SWP5	TIMBER POLE-35	2025+21	95.66 RT



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 EMAIL: JOE.XXXXX@aecom.com
 AECOM JOB NO. 6030302E

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TRANSPORTATION
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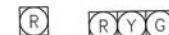
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 6 - 189

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

2S1,4S1



2S2,2S3,4S2,4S3



1S1,3S1



1S1,3S1



2S1,4S1

2S2,2S3,4S2,4S3,
1A,2A,3A,4A

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP5 WITH EQUIPMENT AS SHOWN.
- LEFT TURN PHASING ON RIO BRAVO BLVD SHOULD BE PROTECTED/PERMISSIVE, AND PHASING ON BROADWAY BLVD SHOULD BE SPLIT PHASED.
- EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

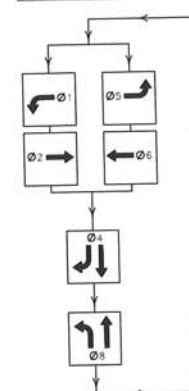
ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

	SIGNAL POLE OR CABINET ID
	CONDUIT RUN ID

SIGNAL PHASING



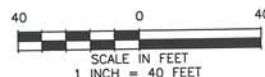
NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
3			
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6			
7			
8			
9			
10			

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 3D
BROADWAY BLVD & RIO BRAVO BLVD

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-20	2024+13	91.23' LT
SWP2	TIMBER POLE-35	2024+75	80.96' LT
SWP3	TIMBER POLE-35	2026+15	86.00' LT
SWP4	TIMBER POLE-35	2026+81	90.58' RT
SWP5	TIMBER POLE-35	2023+21	95.66' RT



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TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 Avenida Parkway NE, Suite 800
 Albuquerque, New Mexico 87113
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 www.aecom.com

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NEW MEXICO PROJECT NO. A300280

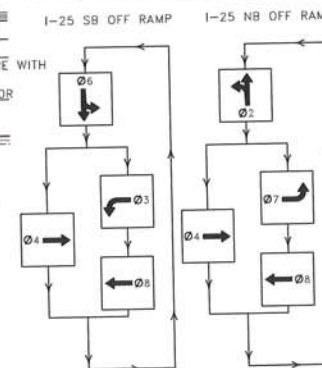
SHEET NO. 6 - 104



MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

NOTES

1. CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
2. CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWPB WITH EQUIPMENT AS SHOWN.
3. EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
4. CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
5. ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.



TEMPORARY SIGNAL TIMBER POLES			
ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' LT
SWP3	TIMBER POLE-35'	2050+08	134.01' R
SWP4	TIMBER POLE-35'	2049+76	66.76' LT
SWP5	TIMBER POLE-35'	2052+77	113.37' R
SWP6	TIMBER POLE-35'	2053+42	54.07' LT
SWP7	TIMBER POLE-35'	2053+97	52.25' LT
SWP8	TIMBER POLE-35'	2053+95	116.38' R

NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
TEMPORARY SIGNAL PLAN
PHASE 1
I-25 & RIO BRAVO BLVD

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

1S1,7S1

1S2,2S1,2S2,3S1,
3S2,3S3,5S1,5S2,
7S2,7S31S2,2S1,2S2,3S1,
3S2,3S3,5S1,5S2,
7S2,7S3

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1 SIGNAL POLE OR CABINET ID

1 CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

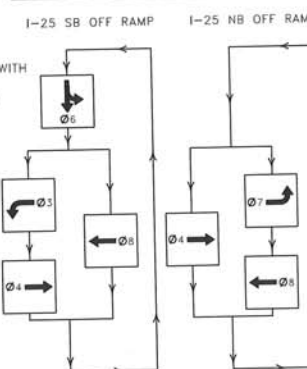
INITIALIZATION

STEADY ALL RED, THEN PHASES
4 & 8 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP8 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

SIGNAL PHASING



ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	66.76' LT
SWP5	TIMBER POLE-35'	2052+77	113.37' RT
SWP6	TIMBER POLE-35'	2053+42	54.07' LT
SWP7	TIMBER POLE-35'	2053+97	52.75' LT
SWP8	TIMBER POLE-35'	2053+95	116.38' RT



NEW MEXICO DEPARTMENT
OF TRANSPORTATION
I-25/RIO BRAVO
INTERCHANGE
TEMPORARY SIGNAL PLAN
PHASE 2
I-25 & RIO BRAVO BLVD

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

751



251, 252

151, 152, 351, 352,
551, 552, 752, 753151, 152, 251, 252,
351, 352, 551, 552,
752, 753

751

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1	SIGNAL POLE OR CABINET ID
1	CONDUIT RUN ID

FLASH CONDITION

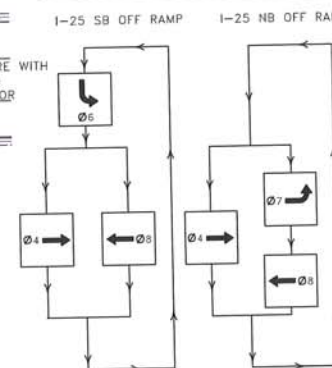
ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
4 & 8 GREEN

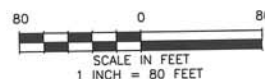
NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP8 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.



TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	66.76' LT
SWP5	TIMBER POLE-35'	2052+77	113.37' RT
SWP6	TIMBER POLE-35'	2053+42	54.07' LT
SWP7	TIMBER POLE-35'	2053+97	52.25' LT
SWP8	TIMBER POLE-35'	2053+95	116.38' RT

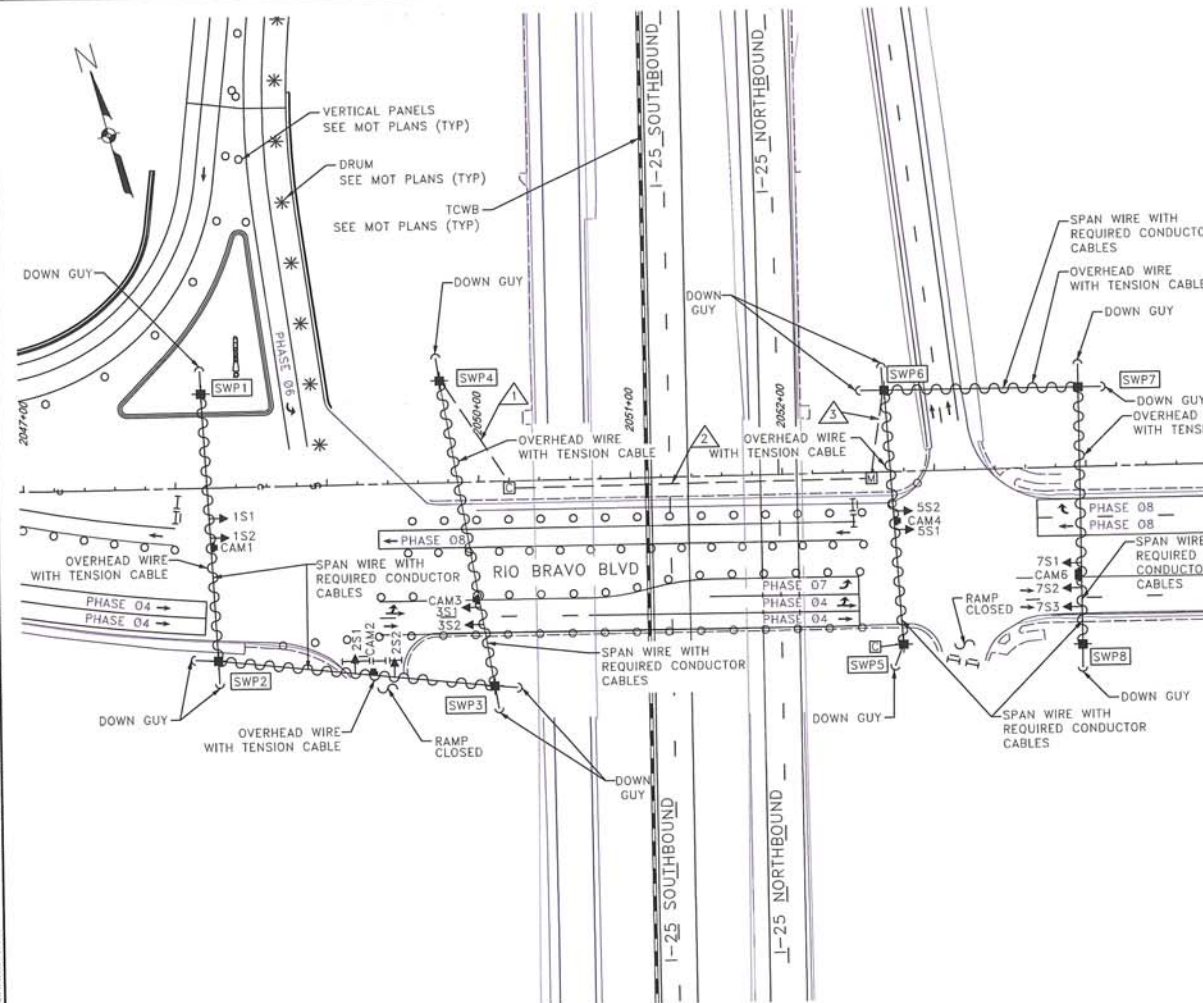


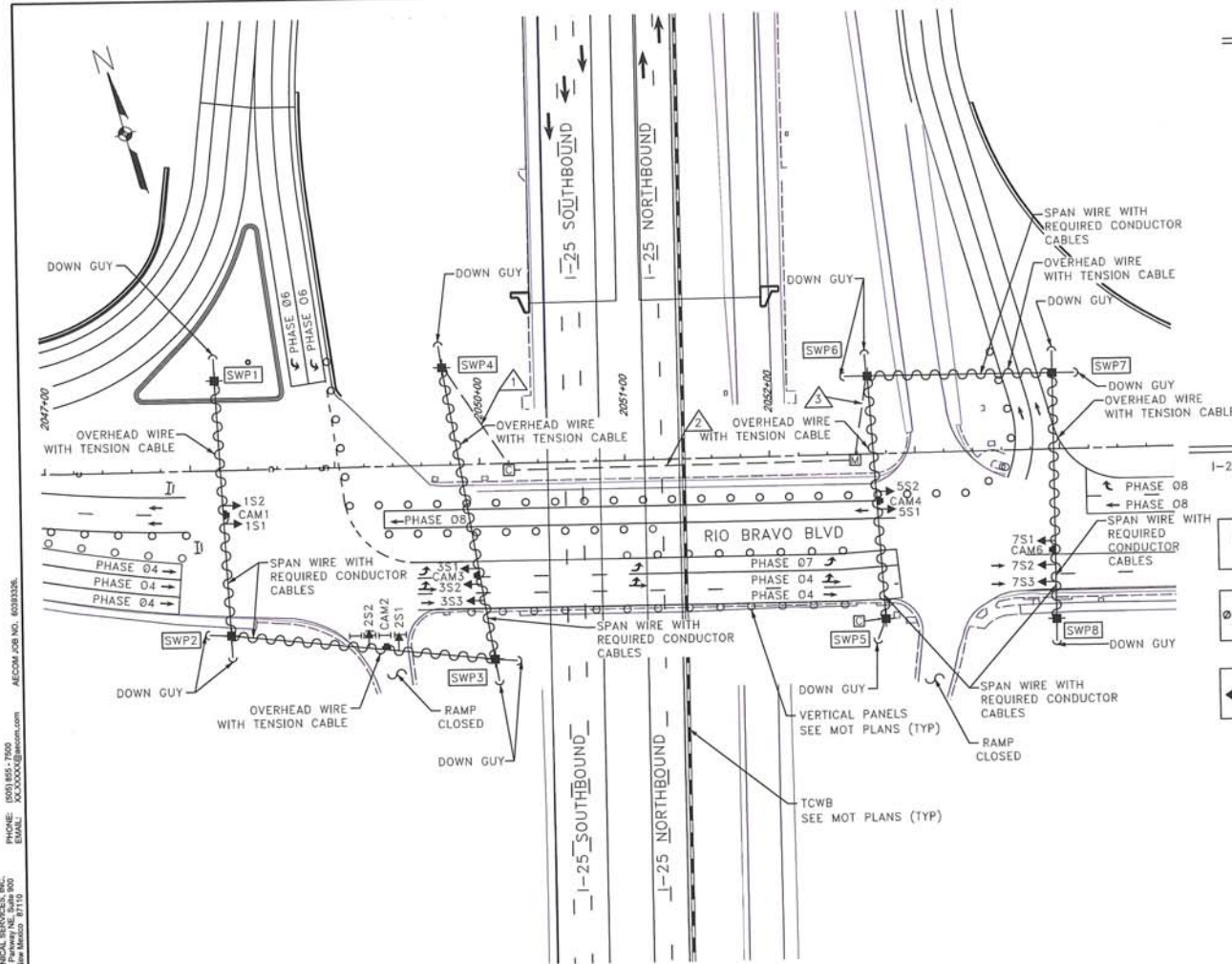
NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		

NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 3
I-25 & RIO BRAVO BLVD





TYPICAL SIGNAL FACE LENS ARRANGEMENTS



7S1,7S2



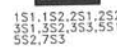
2S1,2S2



1S1,1S2,3S1,3S2,3S3,3S1,5S2,7S3

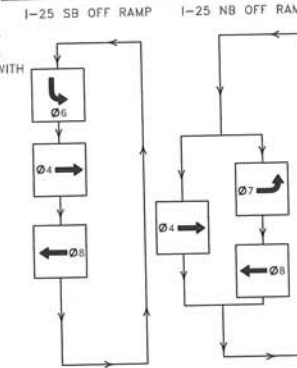


7S1,7S2



1S1,1S2,2S1,2S2,3S1,3S2,3S3,3S1,5S2,7S3

SIGNAL PHASING



ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1	SIGNAL POLE OR CABINET ID
△	CONDUIT RUN ID
FLASH CONDITION	

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES 4 & 8 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP8 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.31' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	66.76' LT
SWP5	TIMBER POLE-35'	2052+77	113.37' RT
SWP6	TIMBER POLE-35'	2053+42	54.07' LT
SWP7	TIMBER POLE-35'	2051+97	52.25' LT
SWP8	TIMBER POLE-35'	2053+95	116.38' RT



ACCOM JOB NO. 6033326

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DESIGNED BY: AECOM

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TRANSPORTATION
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DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25/RIO BRAVO INTERCHANGE
TEMPORARY SIGNAL PLAN PHASE 4
I-25 & RIO BRAVO BLVD

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
SA	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

SIGNAL POLE OR CABINET ID

CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

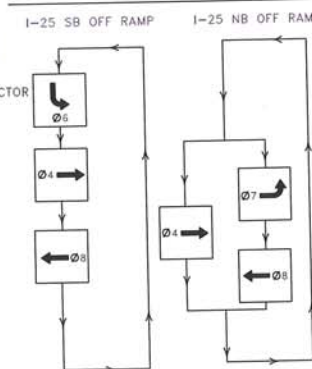
INITIALIZATION

STEADY ALL RED, THEN PHASES
4 & 8 GREEN

NOTES

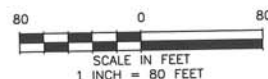
- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP8 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

SIGNAL PHASING



TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+26	65.76' LT
SWP5	TIMBER POLE-35'	2052+77	113.37' RT
SWP6	TIMBER POLE-35'	2053+42	54.07' LT
SWP7	TIMBER POLE-35'	2053+97	52.25' LT
SWP8	TIMBER POLE-35'	2053+95	116.38' RT

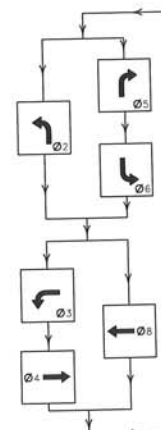


NO.	DESCRIPTION	DATE	BY
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	REVISIONS (OR CHANGE NOTICES)		
	NEW MEXICO DEPARTMENT OF TRANSPORTATION		
	I-25/RIO BRAVO INTERCHANGE		
	TEMPORARY SIGNAL PLAN PHASE 4A I-25 & RIO BRAVO BLVD		

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

SIGNAL PHASING

I-25 & RIO BRAVO BLVD



ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1 SIGNAL POLE OR CABINET ID

△ CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP4 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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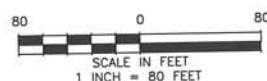
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25/RIO BRAVO
INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 5
I-25 & RIO BRAVO BLVD



ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	66.76' LT



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 EMAIL: AECOM@AECOM.COM

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TRANSPORTATION
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 1-800-875-7500

AECOM DESIGNED BY: AECOM
 www.aecom.com

NEW MEXICO PROJECT NO. A300280

SHEET NO. 8 - 119

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

NO SIGNALS REQUIRED IN PHASE 5A

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
SA	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

	SIGNAL POLE OR CABINET ID
	CONDUIT RUN ID
	FLASH CONDITION

SIGNAL PHASING

NO SIGNALS REQUIRED IN PHASE 5A

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES
2 & 6 GREEN

NOTES

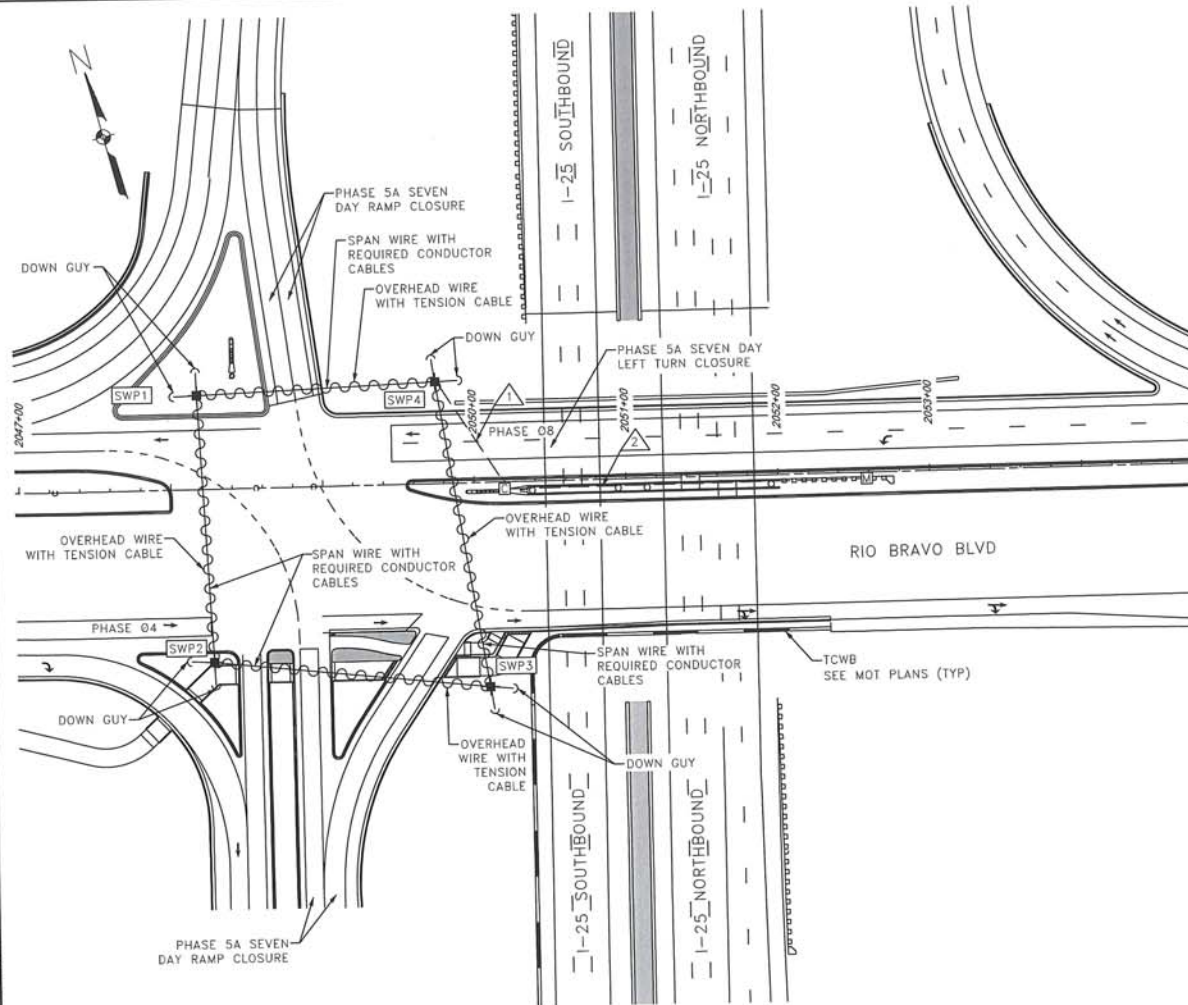
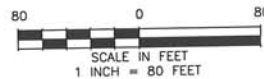
- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP4 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

NO.	DESCRIPTION	DATE	BY
1			
2			
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5			
6			

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGETEMPORARY SIGNAL PLAN PHASE 5A
I-25 & RIO BRAVO BLVD

TEMPORARY SIGNAL TIMBER POLES			
ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	66.76' LT



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 FAX: (505) 255-7501
 AECOM JOB NO. 6033326

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TRANSPORTATION
 AECOM TECHNICAL SERVICES, INC.
 6501 American Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 1/10/2017

DESIGNED BY: AECOM
 AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 8 - 111

TYPICAL SIGNAL FACE LENS ARRANGEMENTS



1S1



2S1, 2S2, 4S1, 4S2



1S2, 1S3, 2S3, 3S1, 3S2, 3S3, 3S4, 4S1, 4S2, 4S3



1S1



1S2, 1S3, 2S1, 2S2, 2S3, 3S1, 3S2, 3S3, 3S4, 4S1, 4S2, 4S3

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED

SYMBOL KEY

MA1	SIGNAL POLE OR CABINET ID
△	CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

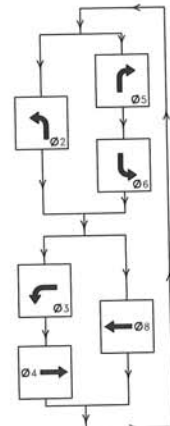
STEADY ALL RED, THEN PHASES 2 & 6 GREEN

NOTES

- CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP4 WITH EQUIPMENT AS SHOWN.
- EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
- CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
- ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.

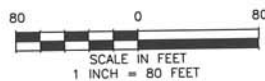
SIGNAL PHASING

I-25 & RIO BRAVO BLVD



TEMPORARY SIGNAL TIMBER POLES

ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35'	2048+18	60.91' LT
SWP2	TIMBER POLE-35'	2048+27	114.18' RT
SWP3	TIMBER POLE-35'	2050+08	134.01' RT
SWP4	TIMBER POLE-35'	2049+76	86.76' LT



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25/RIO BRAVO INTERCHANGE

TEMPORARY SIGNAL PLAN PHASE 6 I-25 & RIO BRAVO BLVD

DESIGNED BY: AECOM

NEW MEXICO PROJECT CONTROL NO. A300280

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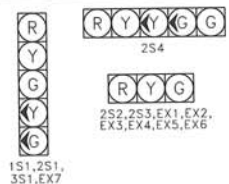
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1-800-666-2666

DESIGNED BY: AECOM

NEW MEXICO PROJECT NO. A300280

SHEET NO. 5 - 112

TYPICAL SIGNAL FACE
LENS ARRANGEMENTS

SYMBOL KEY

- MA1 SIGNAL POLE OR CABINET ID
- CONDUIT RUN ID

FLASH CONDITION

ALL PHASES - RED

INITIALIZATION

STEADY ALL RED, THEN PHASES 2 & 6 GREEN

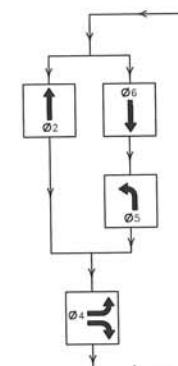
NOTES

1. CONTRACTOR SHALL MOVE TEMPORARY SIGNAL HEADS TO NECESSARY LOCATION AND MAINTAIN POWER FOR EACH NEW PHASE OF CONSTRUCTION.
2. CONTRACTOR SHALL INSTALL TIMBER SPAN WIRE POLES SWP1 THRU SWP3 WITH EQUIPMENT AS SHOWN.
3. EXISTING VIDEO DETECTION CAMERAS AND OPTICAL DETECTORS SHALL BE RELOCATED TO TEMPORARY POLES DURING CONSTRUCTION.
4. EXISTING 3-SECTION AND 5-SECTION TRAFFIC SIGNAL HEADS SHALL BE RE-USED DURING CONSTRUCTION.
5. CONTRACTOR SHALL RELOCATE EXISTING STREET NAME SIGNS TO SPAN WIRE.
6. ALL SPAN WIRE, SIGNS, AND TRAFFIC SIGNAL COMPONENTS SHALL MAINTAIN AT LEAST 20' OF VERTICAL CLEARANCE ABOVE ROADWAY.
7. CONTRACTOR SHALL INTERCEPT EXISTING CONDUIT (NOT SHOWN) FOR OPERATING EXISTING SIGNALS.

ABBREVIATIONS

MA	MAST ARM NUMBER
C	CONTROL CABINET NUMBER
SC	SPLICE CABINET NUMBER
PB	PULL BOX NUMBER (SIGNALS)
PBS	PULL BOX NUMBER (POWER)
PBC	PULL BOX NUMBER (INTERCONNECT)
3A	SIGNAL HEAD NUMBER
BP	BACK PLATE
CAM	VIDEO DETECTOR NUMBER
OPT.-EB	OPTICAL EMERGENCY DETECTOR - DIRECTION
MH	TRAFFIC MANHOLE NUMBER
MP	TRAFFIC METER PEDESTAL
SP	TRAFFIC SERVICE POLE
SWP	TIMBER POLE-FULLY TREATED
SN1	STREET NAME SIGN NUMBER

SIGNAL PHASING



ID#	TYPE	STATION	OFFSET
SWP1	TIMBER POLE-35	2060+43	69.11' LT
SWP2	TIMBER POLE-35	2060+39	89.76' RT
SWP3	TIMBER POLE-35	2062+06	80.33' RT

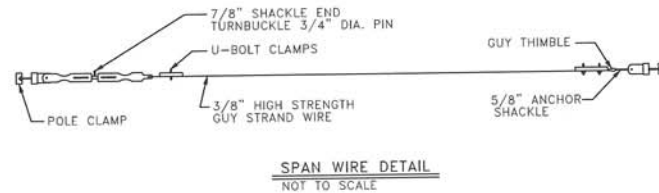


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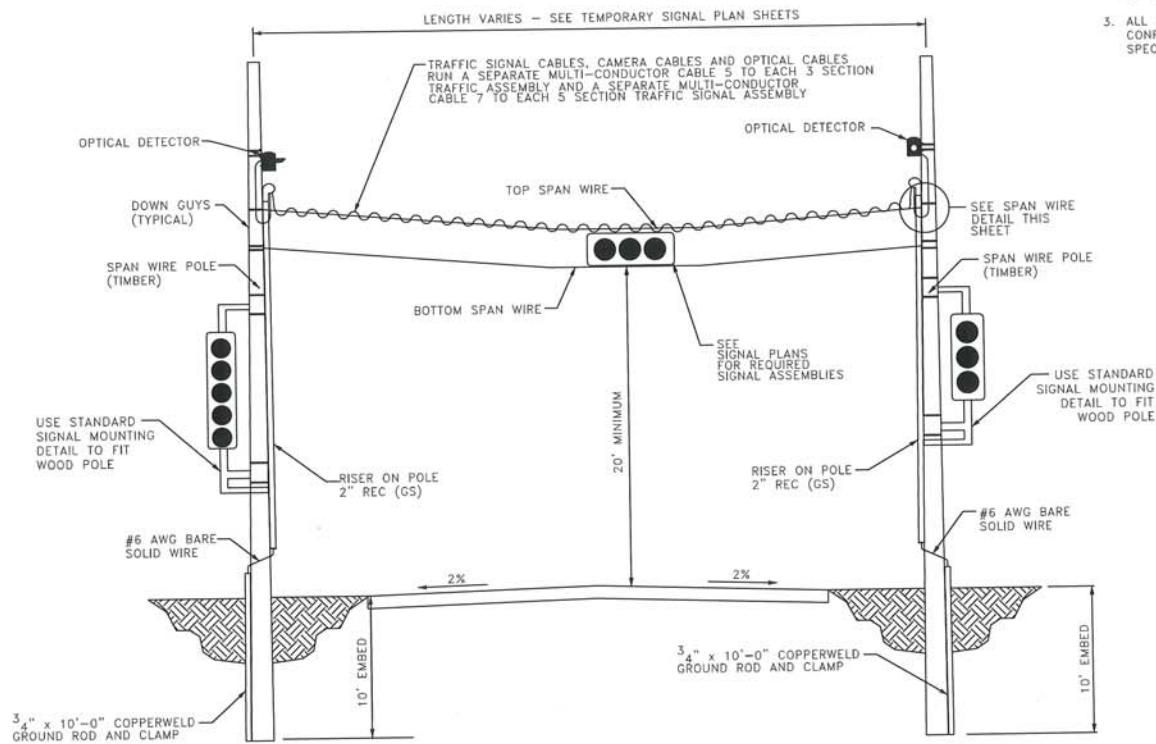
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25/RIO BRAVO
INTERCHANGETEMPORARY SIGNAL PLAN
PHASES 0 TO 6

UNIVERSITY BLVD & RIO BRAVO BLVD

**NOTES:**

1. ALL COSTS TO INSTALL, OPERATE, MAINTAIN AND REMOVE THE TEMPORARY SIGNAL SPAN EXCEPT AS NOTED HEREIN WERE ~~BE~~ INCLUDED IN UNIT PRICE BID FOR ITEM 702700 TEMPORARY SIGNAL SPAN. TEMPORARY SIGNAL SPAN MATERIALS MAY BE REUSED AT OTHER TEMPORARY SIGNAL SPAN LOCATIONS, BUT NOT FOR THE PERMANENT INSTALLATION. UPON COMPLETION OF THE TEMPORARY SIGNAL SPAN, THE CONTRACTOR SHALL REMOVE ALL TEMPORARY EQUIPMENT INCLUDING FOUNDATIONS, PULL BOXES AND TRAFFIC MANHOLES. CONDUITS, PULL BOXES AND TRAFFIC MANHOLES MAY BE ABANDONED IN PLACE ONLY WHEN SPECIFICALLY APPROVED BY THE PROJECT MANAGER. ALL TEMPORARY SIGNAL SPAN EQUIPMENT REMAINED THE PROPERTY OF THE CONTRACTOR.
2. THE INSTALLATION OF THE SIGNAL CONTROLLER CABINET, CONTROLLER, SIGNAL SPANS, HEADS, WIRING, MACHINE VISION VEHICLE DETECTION SYSTEM, VIDEO CAMERAS, EMERGENCY VEHICLE PRE-EMPTION EQUIPMENT AND ALL MATERIAL NOT SPECIFICALLY CALLED FOR ON THESE PLANS, BUT NECESSARY FOR AN OPERATING SYSTEM IN COMPLIANCE WITH THE NATIONAL ELECTRIC CODE WERE ~~BE~~ PROVIDED BY THE CONTRACTOR.
3. ALL MATERIAL AND INSTALLATION METHODS FOR THE SIGNAL WERE ~~BE~~ IN CONFORMANCE WITH THE APPLICABLE PARTS OF THE NMST&D STANDARD SPECIFICATIONS.



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)	
NO.	DESCRIPTION
1	NEW MEXICO DEPARTMENT OF TRANSPORTATION
2	I-25/RIO BRAVO INTERCHANGE
3	TEMPORARY SIGNAL PROFILE DETAILS



INDEX OF SHEETS

SHEET NO.	DESCRIPTION
7-1	PERMANENT STRIPING AND SIGNING - INDEX OF SHEETS AND QUANTITIES
7-2	PERMANENT STRIPING AND SIGNING - GENERAL NOTES
7-3	PERMANENT STRIPING AND SIGNING - LEGEND
7-4A	PERMANENT STRIPING AND SIGNING - SIGN INSTALLATION SCHEDULE
7-4B	PERMANENT STRIPING AND SIGNING - SIGN INSTALLATION SCHEDULE
7-4C	PERMANENT STRIPING AND SIGNING - SIGN INSTALLATION SCHEDULE
7-4D	PERMANENT STRIPING AND SIGNING - SIGN INSTALLATION SCHEDULE
7-4E	PERMANENT STRIPING AND SIGNING - SIGN INSTALLATION SCHEDULE
7-5	PERMANENT STRIPING AND SIGNING - I-25
7-6	PERMANENT STRIPING AND SIGNING - I-25
7-7	PERMANENT STRIPING AND SIGNING - I-25
7-8	PERMANENT STRIPING AND SIGNING - I-25
7-9	PERMANENT STRIPING AND SIGNING - I-25
7-10	PERMANENT STRIPING AND SIGNING - I-25
7-11	PERMANENT STRIPING AND SIGNING - I-25
7-12	PERMANENT STRIPING AND SIGNING - I-25
7-13	PERMANENT STRIPING AND SIGNING - I-25
7-14	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-15	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-16	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-17	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-18	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-19	PERMANENT SIGNING - RIO BRAVO BLVD.
7-19A	PERMANENT STRIPING - RIO BRAVO BLVD.
7-20	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-21	PERMANENT STRIPING AND SIGNING - RIO BRAVO BLVD.
7-22	PERMANENT STRIPING AND SIGNING - EN LOOP RAMP
7-23	PERMANENT STRIPING AND SIGNING - BROADWAY BLVD.
7-24	PERMANENT STRIPING AND SIGNING - DETAILS
7-25	I-BEAM SIGN DETAILS - SIGN 0501
7-26	I-BEAM SIGN DETAILS - SIGN 1302
7-27	OVERHEAD SIGN DETAILS - CANTILEVER SIGN 0703
7-28	OVERHEAD SIGN DETAILS - CANTILEVER SIGN 1104
7-29	OVERHEAD SIGN DETAILS - SIGN BRIDGE 1004-1006
7-30	OVERHEAD SIGN DETAILS - SIGN BRIDGE 1001-1003
7-31	OVERHEAD SIGN FACE DETAILS - SIGNS 1004, 1006, 1801, 1803, 1923
7-32	SIGN FACE DETAILS - SIGNS 0705, 0910, 1002, 1202, 1304, 1501, 1508, 1602, 1603, 1702, 1704, 1802, 1805, 1903, 1904
7-33	SIGN FACE DETAILS - SIGNS 1905, 1920, 1925, 2002, 2004, 2104, 2110, 2301, 2302

PERMANENT STRIPING & MARKING QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	UNIT 1 QTY. (COA)	UNIT 2 QTY. (BemCo)	PROJECT TOTAL
703100	ROAD DELINEATOR TYPE GUIDE	EA	102	0	102
703110	ROAD DELINEATOR TYPE HAZARD	EA	53	19	72
703205	TUBULAR FLEXIBLE TRAFFIC MARKER	EA	15	0	15
703400	1/10TH MILE DELINEATOR	EA	28	2	30
704700	HOT THERMOPLASTIC PAVEMENT MARKINGS 4"	LF	22,896	34,804	57,700
	4" SOLID WHITE	LF	5,728	15,593	21,321
	4" DASHED WHITE	LF	1,950	3,759	5,709
	4" DOTTED WHITE (BIKE LANE)	LF	0	459	459
	12" SOLID WHITE CHEVRON	LF	123	879	1,002
	24" SOLID WHITE STOP BAR	LF	1,908	9,108	11,016
	24" SOLID WHITE CROSSWALK	LF	1,080	4,020	5,100
	4" SOLID YELLOW	LF	3,769	8,540	12,309
	4" DASHED YELLOW (BIKE)	LF	230	554	784
704701	HOT THERMOPLASTIC PAVEMENT MARKINGS 6"	LF	62,550	4,314	66,864
	12" SOLID WHITE	LF	9,388	4,254	13,642
	6" SOLID WHITE	LF	17,378	4,036	21,414
	6" DASHED WHITE	LF	8,425	1,930	10,355
	6" SOLID YELLOW	LF	17,418	4,035	21,453
704715	HOT THERMOPLASTIC PAVEMENT MARKING COMBINATION (THRU AND RIGHT) ARROW	EA	1	2	3
704717	HOT THERMOPLASTIC PAVEMENT MARKING RIGHT ARROW	EA	12	23	35
704718	HOT THERMOPLASTIC PAVEMENT MARKING LEFT ARROW	EA	20	22	42
704719	HOT THERMOPLASTIC PAVEMENT MARKING THRU ARROW	EA	5	0	5
704720	HOT THERMOPLASTIC PAVEMENT MARKING WORD (ONLY)	EA	16	10	26
704730	HOT THERMOPLASTIC PAVEMENT MARKING LANE SYMBOL (INTERSTATE SHIELD)	EA	2	2	4
704731	HOT THERMOPLASTIC PAVEMENT MARKING LANE SYMBOL (BIKEWAY)	EA	3	8	11
704735	HOT THERMOPLASTIC PAVEMENT MARKING RIGHT LANE DROP ARROW	EA	11	3	14
704739	HOT THERMOPLASTIC PAVEMENT MARKING WRONG WAY ARROW	EA	6	0	6
704740	HOT THERMOPLASTIC PAVEMENT MARKING RAILROAD CROSSING	EA	0	7	7
704741	HOT THERMOPLASTIC PAVEMENT MARKING RAILROAD CROSSING (BIKEWAY)	EA	0	2	2
704859	PLOWABLE REFLECTIVE RAISED PAVEMENT MARKER TYPE PG	EA	231	0	231

PERMANENT SIGNING QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	UNIT 1 QTY. (COA)	UNIT 2 QTY. (BemCo)	PROJECT TOTAL
502048	DRILLED SHAFT FOUNDATION 48" DIAMETER	LF	0	52	52
502054	DRILLED SHAFT FOUNDATION 54" DIAMETER	LF	75	0	75
502066	DRILLED SHAFT FOUNDATION 66" DIAMETER	LF	85	0	85
540060	REINFORCING BARS GRADE 60	LB	22,792	8,658	31,450
701000	PANEL SIGNS	SF	1,250	694	1,944
701010	EXTRUDED PANEL SIGNS	SF	1,404	232	1,636
701030	REMOVE AND RESET PANEL SIGN	EA	0	1	1
701031	REMOVE AND RESET TRAFFIC SIGN	EA	1	3	4
701100	STEEL POST AND BASE POST FOR ALUMINUM PANEL SIGNS	LF	2,574	1,194	3,768
701121	STEEL I-BEAM POST TYPE W8X21	LF	0	30	30
701126	STEEL I-BEAM POST TYPE W10X26	LF	41	0	41
701221	BREAKAWAY BASE SYSTEM FOR STEEL I-BEAM POST W8X21	EA	0	2	2
701226	BREAKAWAY BASE SYSTEM FOR STEEL I-BEAM POST W10X26	EA	2	0	2
701452	OVERHEAD SIGN STRUCTURE, CANTILEVER TYPE, 36-INCH	EA	2	0	2
701545	OVERHEAD SIGN STRUCTURE, BRIDGE TYPE, 24-INCH	EA	0	1	1
701550	OVERHEAD SIGN STRUCTURE, BRIDGE TYPE, 30-INCH	EA	1	0	1



NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25 / RIO BRAVO BLVD. INTERCHANGE			
PERMANENT STRIPING AND SIGNING INDEX OF SHEETS AND QUANTITIES			

PERMANENT STRIPING NOTES

- MATERIAL:** ALL STRIPING AND MARKINGS SHALL BE HOT THERMOPLASTIC.
- LAYOUT:** THE CONTRACTOR SHALL MARK THE LAYOUT OF ALL STRIPING AND PAVEMENT MARKINGS AND SHALL OBTAIN CONCURRENCE FROM THE DISTRICT TRAFFIC ENGINEER OR HIS/her DESIGNEE BEFORE APPLYING THE FINAL STRIPING AND MARKINGS. THIS LAYOUT WILL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE PAVEMENT MARKINGS AND NO SEPARATE PAYMENT WILL BE MADE.
- REMOVALS:** WATER BLASTING IS THE ONLY APPROVED METHOD OF REMOVING STRIPING AND MARKINGS WITHIN DISTRICT THREE. THE USE OF BLACK PAINT TO COVER ANY STRIPING OR MARKINGS IS STRICTLY PROHIBITED. WHEN WATER BLASTING ON OPEN GRADED FRICTION COURSE (OGFC) OR ON THE FINAL LIFT OF PAVEMENT, THE CONTRACTOR SHALL APPLY A SEAL OF TACK COAT MATERIAL TO THE WATER BLASTED AREA. THIS SEAL SHALL BE INCIDENTAL TO THE REMOVAL OF PAVEMENT STRIPE AND NO SEPARATE PAYMENT WILL BE MADE.

PERMANENT DELINEATORS NOTES

- REMOVALS:** REMOVE ALL EXISTING DELINEATORS BETWEEN NB 1-25 BOP STA. 1666+62 AND EOP STA. 1752+60. EXISTING DELINEATORS (NOT SHOWN ON THESE PLANS) TO BE REMOVED INCLUDE 1/10TH MILE DELINEATORS, GUIDE DELINEATORS, AND A3 HAZARD MARKERS. REMOVAL OF EXISTING DELINEATORS WILL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE NEW DELINEATORS AND NO SEPARATE PAYMENT WILL BE MADE.
- STANDARD S:** CONSTRUCT NEW 1/10TH MILE DELINEATORS AT THE LOCATIONS SHOWN ON THESE PLANS PER STANDARD DRAWINGS 703 01-1/3 AND 703 02-1/2.
- ON BRIDGES:** FOR 1/10TH MILE DELINEATORS LOCATED ON BRIDGES, USE WHITE/8" WHITE SHUR-TITE CTE CUP MOUNT DELINEATORS (OR APPROVED EQUAL) MOUNTED ON TOP OF THE BRIDGE BARRIER.
- CULVERT ENDS:** CONSTRUCT NEW A3/A1 DELINEATORS TO MARK THE CULVERT END LOCATIONS SHOWN ON THESE PLANS PER STANDARD DRAWINGS 703 01-1/3 AND 703 01-3/3. THE STATION/OFFSET SHOWN INDICATES THE APPROXIMATE LOCATION OF THE CULVERT END. WHERE CULVERT ENDS ARE LOCATED AT THE BOTTOM OF HIGH SLOPES, THE DELINEATOR MAY BE PLACED AT THE SLOPE HINGE POINT DIRECTLY ABOVE THE CULVERT END, AS DIRECTED BY THE PROJECT MANAGER. THE A3 (TRIPLE AMBER REFLECTOR) SIDE IS TO FACE ONCOMING TRAFFIC, AND THE BACK SIDE HAS ONE AMBER REFLECTOR.

PERMANENT SIGNING NOTES

- STANDARD S:** ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE NEW MEXICO STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION 2014 EDITION, AND ANY APPLICABLE SPECIAL PROVISIONS AND/OR SUPPLEMENTAL SPECIFICATIONS. ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE FHWA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION.
- SIGN FACES:** EACH STANDARD SIGN FACE SHALL CONFORM TO THE SPECIFICATIONS IN THE FHWA STANDARD HIGHWAY SIGNS MANUAL, 2004 EDITION AND 2012 SUPPLEMENT. EACH SPECIAL SIGN FACE SHALL CONFORM TO THE DETAILS PROVIDED IN THIS PLAN SET. SUBMIT SHOP DRAWINGS OF EACH SPECIAL SIGN FACE DETAIL TO THE PROJECT MANAGER FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- SIGN SHEETING:** RETROREFLECTIVE SHEETING FOR ALL SIGNS SHALL BE TYPE IX OR TYPE XI FOR BOTH LEGEND AND BACKGROUND.
- OVERLAY FILM:** APPLY A CLEAR PROTECTIVE FILM OVERLAY (ANTI-GRAFFITI AND UV PROTECTION) TO ALL SIGNS. USE A PRODUCT FROM THE NMDOT APPROVED PRODUCTS LIST. THE FILM OVERLAY WILL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE PANEL SIGN AND NO SEPARATE PAYMENT WILL BE MADE.
- ALUMINUM PANELS:** ALL WARNING AND REGULATORY SIGN PANELS SHALL BE 0.125 INCH ALUMINUM.
- SIGN POSTS:** UNLESS OTHERWISE APPROVED BY THE PROJECT MANAGER OR THE DISTRICT TRAFFIC ENGINEER, ALL SIGN POSTS SHALL BE 2.25" X 2.25" 12 GA. STEEL SQUARE TUBING AND ALL BASE POSTS SHALL BE 2.50" X 2.50" X 42" LONG 10 GA. STEEL SQUARE TUBING.
- POST LENGTHS:** POST LENGTHS FOR ROADSIDE SIGNS ARE BASED ON THE 7-FOOT MINIMUM SIGN MOUNTING HEIGHT SHOWN FOR FREEWAYS ON STANDARD DRAWING 701-02-2/3. POST LENGTHS FOR MILEPOSTS ARE BASED ON THE 4-FOOT MINIMUM SIGN MOUNTING HEIGHT SHOWN ON STANDARD DRAWING 703-02-1/2. THE LENGTHS ARE MEASURED FROM THE BOTTOM OF THE SIGN TO NEAR EDGE OF THE DRIVING LANE.
- QUANTITIES:** QUANTITIES MAY VARY AS FIELD CONDITIONS DICTATE. THE CONTRACTOR WILL BE PAID FOR ACTUAL QUANTITIES USED.
- INCIDENTAL HARDWARE:** ALL SIGNING HARDWARE INCLUDING BUT NOT LIMITED BOLTS, BRACKETS, BANDING, BUCKLES, CLAMPS, CLIPS, FUSE PLATES, KEEPER PLATES, SHIMS, FASTENERS, SCREWS, WASHERS, ETC. ARE CONSIDERED AS INCIDENTAL TO THE COST OF THE SIGN POSTS AND NO SEPARATE PAYMENT WILL BE MADE.
- LATERAL CLEARANCE (INTERSTATE):** THE LATERAL CLEARANCE TO ALUMINUM PANEL SIGNS ON SQUARE TUBE POSTS ON THE INTERSTATE SHALL BE NO CLOSER THAN 6 FEET FROM THE EDGE OF SHOULDER OR 12 FEET FROM THE EDGE OF TRAVELED WAY. THE LATERAL CLEARANCE TO EXTRUDED PANEL SIGNS ON I-BEAM POSTS ON THE INTERSTATE SHALL BE NO CLOSER THAN 36 FEET FROM THE EDGE OF TRAVELED WAY.
- LATERAL CLEARANCE (URBAN STREETS):** THE LATERAL CLEARANCE TO ALUMINUM PANEL SIGNS ON SQUARE TUBE POSTS ON THE URBAN STREETS SHALL BE NO CLOSER THAN 2 FEET FROM THE FACE OF CURB. WHERE NO CURB IS PRESENT, SIGNS SHALL BE NO CLOSER THAN 6 FEET FROM THE EDGE OF SHOULDER OR 12 FEET FROM THE EDGE OF TRAVELED WAY.
- SIGN LOCATIONS:** THE CONTRACTOR SHALL STAKE OUT ALL NEW SIGN LOCATIONS FOR APPROVAL BY THE PROJECT MANAGER OR THE DISTRICT TRAFFIC ENGINEER PRIOR TO INSTALLATION. THIS LAYOUT WILL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE SIGNS AND NO SEPARATE PAYMENT WILL BE MADE.
- SALVAGE OF EXISTING SIGNS:** PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL CONTACT THE PROJECT MANAGER TO OBTAIN A DETERMINATION AS TO WHICH EXISTING SIGNS, SIGN POSTS, AND SIGN STRUCTURES SHOWN TO BE REMOVED SHALL BE SALVAGED. THE CONTRACTOR SHALL DELIVER SALVAGED SIGNAGE MATERIALS TO THE NMDOT SOUTH URBAN PATROL YARD 43-56 AT 3520 BROADWAY BLVD SE IN ALBUQUERQUE (505) 843-8598 DURING NORMAL WORKING HOURS AND HEAVILY STOCKPILE THEM AS DIRECTED BY THE YARD FOREMAN. THE CONTRACTOR SHALL RECYCLE OR PROPERLY DISPOSE ALL OTHER SIGNS, POSTS, STRUCTURES AND HARDWARE NOT TO BE SALVAGED. THE COSTS ASSOCIATED WITH THE REMOVAL, SALVAGE, DELIVERY, RECYCLING, AND DISPOSAL OF SIGN MATERIALS SHALL BE CONSIDERED AS INCIDENTAL TO THE SIGNAGE PAY ITEMS AND NO SEPARATE PAYMENT WILL BE MADE.



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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATIONI-25 / RIO BRAVO BLVD.
INTERCHANGEPERMANENT STRIPING AND SIGNING
GENERAL NOTES

NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 -

STRIPING AND MARKING LEGEND

(4W) 4" SOLID WHITE EDGE STRIPE	
(4L) 4" DASHED WHITE LANE STRIPE	10' STRIPE - 30' GAP
(4D) 4" WHITE LANE DROP STRIPE	3' STRIPE - 9' GAP
(4X) 4" DOTTED WHITE EXTENSION STRIPE	2' STRIPE - 4' GAP
(4Y) 4" SOLID YELLOW EDGE STRIPE	
(4YX) 4" DOTTED YELLOW EXTENSION STRIPE	2' STRIPE - 4' GAP
(4YB) 4" DASHED YELLOW BICYCLE PATH STRIPE	3' STRIPE - 9' GAP
(6W) 6" SOLID WHITE EDGE STRIPE	
(6L) 6" DASHED WHITE LANE STRIPE	10' STRIPE - 30' GAP
(6Y) 6" SOLID YELLOW EDGE STRIPE	
(8D) 8" WHITE LANE DROP STRIPE	3' STRIPE - 9' GAP
(12W) 12" SOLID WHITE STRIPE	
(12H) 12" SOLID WHITE HATCHING	45" @ 30' SPACING
(GM) GORE MARKINGS	PER STD. DWG. 704-05-1/1
(AR) ARROW MARKING - RIGHT TURN	
(AL) ARROW MARKING - LEFT TURN	
(AT) ARROW MARKING - STRAIGHT THROUGH	
(ART) ARROW MARKING - THROUGH / RIGHT TURN	
(ALT) ARROW MARKING - THROUGH / LEFT TURN	
(ALD) ARROW MARKING - LANE DROP	
(AW) ARROW MARKING - WRONG WAY	PER STD. DWG. 701-18-2/2
(ON) WORD MARKING - ONLY	
(RR) RAILROAD XING MARKING	PER STD. DWG. 704-02-1/1
(SB) 24" STOP BAR MARKING	
(XV) CROSSWALK MARKING	SEE DETAIL SHEET 7-24
(BL) BIKE LANE MARKING	SEE DETAIL SHEET 7-24
(RRB) RAILROAD XING MARKING FOR BIKE PATH	SEE DETAIL SHEET 7-24
(IS) INTERSTATE SHIELD MARKING	SEE DETAIL SHEET 7-24
(CNY) PAINT CURB NOSE YELLOW	SEE DETAIL SHEET 7-24

SIGNING LEGEND

(0000) CONSTRUCTED NEW SIGN, SEE SIGN INSTALLATION TABLE
(EX) EXISTING SIGN REMAINED - PROTECT IN PLACE
(REM) REMOVED EXISTING SIGN AND POST
(REL) REMOVED EXISTING SIGN PANEL AND RELOCATE TO NEW POSTS AT NEW LOCATION INDICATED
(RES) REMOVED AND RESET EXISTING SIGN PANEL ON NEW POSTS AT SAME STATION, NEW OFFSET

DESIGNED BY: AECOM
 PHONE: (505) 845-7000
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REV-SIGNS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25 / RIO BRAVO BLVD. INTERCHANGE			
PERMANENT STRIPING AND SIGNING LEGEND			

NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 -

PERMANENT SIGN INSTALLATION SCHEDULE

										ALUMINUM PANEL										SIGN POST QUANTITIES										MULTI-DIRECTION BASES REQUIRED	INSTALLATION NOTES
PLAN SHEET	UNIT	SIGN NO.	ALIGNMENT	STATION	OFFSET	FACING DIRECTION OF TRAFFIC	SIGN CODE	COLOR		PLAN QTY. EACH	SIGN DIMENSIONS				AREA EACH (SF)	AREA TOTAL (SF)	SIGN POST LENGTHS				BASE POSTS				TOTAL LENGTH (LF)						
								LEGEND	BACKGRD.		WIDTH (IN)	HEIGHT (IN)	ROUND (IN)	DIAMOND (IN)			LEFT (LF)	CENTER (LF)	RIGHT (LF)	TOTAL (LF)	QTY. EACH	LENGTH (LF)	TOTAL LENGTH (LF)								
																										RELOCATE EXTRUDED PANEL ON I-BEAM - SEE SHEET 7-25					
7-5	UNIT 2	0501	I-25 NB	1652+00	44.50 RT	FNBT		WHITE	GREEN																						
7-6	UNIT 2	0601	I-25 NB	1662+15	LT	FNBT	R2-1-48-65	BLACK	WHITE	1	48	60			20.00	20.00	13.8		13.3	27.2	2	3.5	34.2								
7-6	UNIT 2	0602	I-25 SB	1662+20	LT	FSBT	R2-1-48-75	BLACK	WHITE	1	48	60			20.00	20.00	13.8		13.3	27.2	2	3.5	34.2								
7-6	UNIT 2	0603	I-25 NB	1662+15	RT	FNBT	R2-1-48-65	BLACK	WHITE	1	48	60			20.00	20.00	13.3		13.8	27.2	2	3.5	34.2								
7-6	UNIT 2	0604	I-25 SB	1662+20	RT	FSBT	R2-1-48-75	BLACK	WHITE	1	48	60			20.00	20.00	13.3		13.8	27.2	2	3.5	34.2								
7-6	UNIT 2	0606	I-25 SB	1267+00	LT	FSBT	M1-1A-54-25	WHITE	GREEN	1	54	66			24.75	24.75	13.8		14.3	28.2	2	3.5	35.2								
7-7	UNIT 1	RESET	I-25 NB	1675+40	RT	FNBT	D10-3-12-220	WHITE	GREEN													1	3.5	3.5		SIGN FACE PER 701-15-1/2					
7-7	UNIT 2	RESET	I-25 SB	1275+45	LT	FSBT	D10-3-12-220	WHITE	GREEN													1	3.5	3.5		RESET EXISTING SIGN AND SIGN POST ON NEW BASE POST					
7-7	UNIT 1	0701	I-25 NB	1677+60	RT	FNBT	W13-2-36-40	BLACK	YELLOW	1	36	48			12.00	12.00	12.3		12.8	25.2	2	3.5	32.2								
7-7	UNIT 1	0703	I-25 NB	1678+50	46.25 RT	FNBT		WHITE	GREEN	1	204	120			170.00	170.00										EXTRUDED PANEL ON OH SIGN STRUCTURE - SEE SHEET 7-27					
7-7	UNIT 1	0703	I-25 NB	1678+50	46.25 RT	FNBT		BLACK	YELLOW	1	108	30			22.50	22.50										EXTRUDED PANEL ON OH SIGN STRUCTURE - SEE SHEET 7-27					
7-7	UNIT 1	0703	I-25 NB	1678+50	46.25 RT	FNBT		WHITE	GREEN	1	96	60			40.00	40.00	13.3	13.6	13.8	40.7	3	3.5	51.2			SIGN FACE PER SHEET 7-32					
7-7	UNIT 1	0705	I-25 NB	1683+34	RT	FNBT	E13-1-40-72	BLACK	YELLOW	1	72	24			12.00	12.00										PANEL ON SAME POST AS 0705					
7-7	UNIT 1	0706	I-25 NB	1683+34	RT	FNBT		BLACK	YELLOW	1				36	9.00	9.00	13.3		13.8	27.2	2	3.5	34.2								
7-7	UNIT 1	0707	NB EXIT RAMP	105+80	RT	FNBT	W1-2-36	BLACK	YELLOW	1	30	30			6.25	6.25										PANEL ON SAME POST AS 0707					
7-7	UNIT 1	0709	NB EXIT RAMP	105+80	RT	FNBT	W13-1L-30-35	BLACK	YELLOW	1	30	30					13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0801	NB EXIT RAMP	110+00	RT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0802	SB ON RAMP	204+70	RT	FSBT	W4-1L-48	BLACK	YELLOW	1				48	16.00	16.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0803	NB EXIT RAMP	111+20	RT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0804	SB ON RAMP	205+94	LT	FSBT	R16-NM-2-30	BLACK	WHITE	1	30	42			8.75	8.75		12.3			1	3.5	15.8								
7-8	UNIT 1	0804	SB ON RAMP	205+94	LT	FSBT	R16-NM-2-30	BLACK	WHITE	1	30	42			12.00	12.00	13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0805	NB EXIT RAMP	112+40	RT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			8.75	8.75	11.3		10.8	22.2	2	3.5	29.2								
7-8	UNIT 1	0806	NB EXIT RAMP	119+60	LT	FSBT	R5-1A-42	WHITE	RED	1	42	30			12.00	12.00	13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0807	NB EXIT RAMP	113+60	RT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			8.75	8.75	10.8		11.3	22.2	2	3.5	29.2								
7-8	UNIT 1	0808	NB EXIT RAMP	119+60	RT	FSBT	R5-1A-42	WHITE	RED	1	42	30			16.00	16.00	13.3		13.8	27.2	2	3.5	34.2								
7-8	UNIT 1	0809	I-25 NB	1694+39	RT	FNBT	W4-3R-48	BLACK	YELLOW	1				48	12.00	12.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0811	EB-NB LOOP	315+20	LT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0813	EB-NB LOOP	316+00	LT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0815	EB-NB LOOP	316+80	LT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0817	EB-NB LOOP	317+20	LT	FNBT	W4-6-48	BLACK	YELLOW	1				48	16.00	16.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0819	EB-NB LOOP	317+60	LT	FNBT	W1-8-36	BLACK	YELLOW	1	36	48			12.00	12.00	13.8		13.3	27.2	2	3.5	34.2								
7-8	UNIT 1	0821	NB EXIT RAMP	118+62	RT	FNBT	R3-8-36	BLACK	WHITE	1	36	30			7.50	7.50		10.8			1	3.5	14.3								
7-8	UNIT 1	0821	NB EXIT RAMP	118+62	RT	FNBT	R3-8-36	BLACK	WHITE	1	36	30			7.50	7.50		10.8			1	3.5	14.3								
7-8	UNIT 1	0823	NB EXIT RAMP	118+60	RT	FNBT	R3-8-36	BLACK	WHITE	1	36	30			7.50	7.50										PANEL ON SAME POST AS 0806					
7-8	UNIT 1	0825	NB EXIT RAMP	119+60	LT	FNBT	W3-3-48	BLACK	YELLOW	1				48	16.00	16.00										PANEL ON SAME POST AS 0808					
7-8	UNIT 1	0827	NB EXIT RAMP	119+60	RT	FNBT	W3-3-48	BLACK	YELLOW	1				48	16.00	16.00															
7-9	UNIT 1	0901	I-25 NB	1702+00	RT	FSBT	W4-2R-48	BLACK	YELLOW	1				48	16.00	16.00	13.8		13.3	27.2	2	3.5	34.2								
7-9	UNIT 1	0902	SB EXIT RAMP	504+41	RT	FSBT	W3-3-48	BLACK	YELLOW	1				48	16.00	16.00	13.3		13.8	27.2	2	3.5	34.2								
7-9	UNIT 1	0903	NB ON RAMP	413+02	RT	FNBT	R16-NM-2-30	BLACK	WHITE	1	30	42			8.75	8.75		12.3			1	3.5	15.8								
7-9	UNIT 1	0904	SB EXIT RAMP	504+41	LT	FSBT	W3-3-48	BLACK	YELLOW	1				48	16.00	16.00	13.3		13.8	27.2	2	3.5	34.2			PANEL ON SAME POST AS 0902					
7-9	UNIT 1	0905	SB EXIT RAMP	504+41	RT	FSBT	R5-1A-42	WHITE	RED	1	42	30			8.75	8.75															
7-9	UNIT 1	0906	SB EXIT RAMP	509+00	RT	FSBT	R3-8-36	BLACK	WHITE	1	36	30			7.50	7.50		10.8		10.8	1	3.5	14.3								
7-9	UNIT 1	0907	SB EXIT RAMP	504+41	LT	FSBT	R5-1A-42	WHITE	RED	1	42	30			8.75	8.75										PANEL ON SAME POST AS 0904					
7-9	UNIT 1	0908	SB EXIT RAMP	509+00	LT	FSBT	R3-8-36	BLACK	WHITE	1	36	30			7.50	7.50		10.8		10.8	1	3.5	14.3								
7-9	UNIT 1	0909	SB EXIT RAMP	508+00	RT	FSBT	R5-1A-42	WHITE	RED	1	42	30			8.75	8.75										PANEL ON SAME POST AS 0906					
7-9	UNIT 1	0909	SB EXIT RAMP	508+00	RT	FSBT	R5-1A-42	WHITE	RED	1	42	30			8.75	8.75										SIGN FACE PER SHEET 7-32					
7-9	UNIT 1	0910	SB EXIT RAMP	511+00	LT	FSBT		WHITE	GREEN	1	84	84			49.00	49.00	15.3	15.6	15.8	46.7	3	3.5	57.2			PANEL ON SAME POST AS 0908					
7-9	UNIT 1	0911	SB EXIT RAMP	508+00	LT	FSBT	R5-1A-42	WHITE	RED	1	42	30			8.75	8.75															
7-10	UNIT 1	1001	NB ON RAMP	415+41	LT	FNBT	W4-1L-48	BLACK	YELLOW	1				48	16.00	16.00	13.8		13.3	27.2	2	3.5	34.2			SIGN FACE PER SHEET 7-32					
7-10	UNIT 1	1002	I-25 SB	1319+38	LT	FSBT		WHITE	GREEN	1	84	84			49.00	49.00	15.3	15.6	15.8	46.7	3	3.5	57.2			EXTRUDED PANEL ON OH SIGN STRUCTURE - SEE SHEETS 7-29 & 7-31					
7-10	UNIT 1	1004	I-25 SB	1325+25	43.25 RT	FSBT		WHITE	GREEN	1	360	120			300.00	300.00															

AECOM JOB NO. 0303326

PHONE: (505) 845-7500
FAX: (505) 845-7501
EMAIL: jml@aecom.comDESIGNED BY: AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 400
Albuquerque, New Mexico 87113
www.aecom.comDesign File: c:\pwworking\dm31771\A300280\7P5044.dgn
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1/10/2017

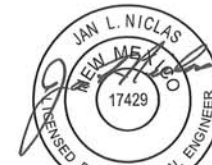
NEW MEXICO PROJECT NO. A300280

SHEET NO. 7

8					
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NO.	DESCRIPTION	DATE			
REV-SIGNS (OR CHANGE NOTICES)					
NEW MEXICO DEPARTMENT OF TRANSPORTATION					
I-25 / RIO BRAVO BLVD. INTERCHANGE					
PERMANENT STRIPING AND SIGNING SIGN INSTALLATION SCHEDULE					

PERMANENT SIGN INSTALLATION SCHEDULE

PLAN SHEET	UNIT	SIGN NO.	ALIGNMENT	STATION	OFF SET	FACING DIRECTION OF TRAFFIC	SIGN CODE	COLOR LEGEND	BACKGROUND	PLAN QTY, EACH	ALUMINUM PANEL				AREA EACH (SF)	AREA TOTAL (SF)	SIGN POST QUANTITIES					MULTI-DIRECTION BASES REQUIRED	INSTALLATION NOTES
											WIDTH (IN)	HEIGHT (IN)	ROUND (IN)	DIAMOND (IN)			SIGN POST LENGTHS		TOTAL (LF)	BASE POSTS QTY, EACH	LENGTH (LF)		
7-17	UNIT 2	1714	RIO BRAVO BLVD	2033+70	LT	FWBT	W10-1-36	BLACK	YELLOW	1		24	24		7.07	7.07	11.3		11.3	1	3.5	14.8	
7-17	UNIT 2	1715	RIO BRAVO BLVD	2038+31	RT	FEBT	R5-3-24	BLACK	WHITE	1	24				4.00	4.00	10.3		10.3	1	3.5	13.8	
7-17	UNIT 2	1716	RIO BRAVO BLVD	2033+70	LT	FWBT	W10-1a-24	BLACK	YELLOW	1	24	12			2.00	2.00							PANEL ON SAME POST AS 1714
7-17	UNIT 2	1717	RIO BRAVO BLVD	2038+60	RT	FEBT	R3-8-48	BLACK	WHITE	1	48	48			16.00	16.00	12.3	12.9	25.2	2	3.5	32.2	
7-17	UNIT 2	1718	RIO BRAVO BLVD	2037+69	LT	FWBT	R3-2-36	BLACK	WHITE	1	36	36			9.00	9.00							PANEL ON SAME POST AS 1719
7-17	UNIT 2	1719	RIO BRAVO BLVD	2037+69	LT	FWBT	R6-1R-54	BLACK	WHITE	1	54	12			4.50	4.50	12.3	12.3	24.7	2	3.5	31.7	
7-17	UNIT 2	1720	RIO BRAVO BLVD	2031+79	LT	FWBT	R15-1	BLACK	WHITE	2	9	48			3.00	6.00							MOUNT ON GATE POLE - SIGN FACE & INSTALLATION PER 701-21-1/5, 2/5, 4/5
7-17	UNIT 2	1721	RIO BRAVO BLVD	2031+53	RT	FEBT	R15-1	BLACK	WHITE	2	9	48			3.00	6.00							MOUNT ON GATE POLE - SIGN FACE & INSTALLATION PER 701-21-1/5, 2/5, 4/5
7-17	UNIT 2	1722	RIO BRAVO BLVD	2031+79	LT	FWBT	R1-1	RED	WHITE	1	36	36			7.10	7.10							MOUNT ON GATE POLE AS 1720
7-17	UNIT 2	1723	RIO BRAVO BLVD	2031+53	RT	FEBT	R1-1	RED	WHITE	1	36	36			7.10	7.10							MOUNT ON GATE POLE AS 1721
7-17	UNIT 2	1724	RIO BRAVO BLVD	2031+79	LT	FWBT		WHITE	BLUE	1	24	18			3.00	3.00							MOUNT ON GATE POLE AS 1720 - SIGN FACE PER 701-21-4/5 (X-ING # 019433N, MP 1.45)
7-17	UNIT 2	1725	RIO BRAVO BLVD	2031+53	RT	FEBT		WHITE	BLUE	1	24	18			3.00	3.00							MOUNT ON GATE POLE AS 1721 - SIGN FACE PER 701-21-4/5 (X-ING # 019433N, MP 1.45)
7-17	UNIT 2	1726	RIO BRAVO BLVD	2031+79	LT	FWBT	R15-1	BLACK	WHITE	2	9	48			3.00	6.00							MOUNT ON GATE POLE - SIGN FACE & INSTALLATION PER 701-21-1/5, 2/5, 4/5
7-17	UNIT 2	1727	RIO BRAVO BLVD	2031+53	RT	FEBT	R15-1	BLACK	WHITE	2	9	48			3.00	6.00							MOUNT ON GATE POLE - SIGN FACE & INSTALLATION PER 701-21-1/5, 2/5, 4/5
7-17	UNIT 2	1728	RIO BRAVO BLVD	2031+79	LT	FWBT	R1-1	RED	WHITE	1	36	36			7.10	7.10							MOUNT ON GATE POLE AS 1726
7-17	UNIT 2	1729	RIO BRAVO BLVD	2031+53	RT	FEBT	R1-1	RED	WHITE	1	36	36			7.10	7.10							MOUNT ON GATE POLE AS 1727
7-17	UNIT 2	1730	RIO BRAVO BLVD	2031+79	LT	FWBT		WHITE	BLUE	1	24	18			3.00	3.00							MOUNT ON GATE POLE AS 1726 - SIGN FACE PER 701-21-4/5 (X-ING # 019433N, MP 1.45)
7-17	UNIT 2	1731	RIO BRAVO BLVD	2031+53	RT	FEBT		WHITE	BLUE	1	24	18			3.00	3.00							MOUNT ON GATE POLE AS 1727 - SIGN FACE PER 701-21-4/5 (X-ING # 019433N, MP 1.45)
7-17	UNIT 2	1733	RIO BRAVO BLVD	2031+45	RT	FEBT	R15-1	BLACK	WHITE	2	24	4.5			0.75	1.50	12.0		12.0	1	4.0	16.0	
7-17	UNIT 2	1735	RIO BRAVO BLVD	2031+45	RT	FEBT	R1-2	RED	WHITE	1	18				11.69	11.69							SIGN FACE & INSTALLATION PER 701-22-1/5, 2/5, 4/5
7-17	UNIT 2	1737	RIO BRAVO BLVD	2031+45	RT	FEBT		WHITE	BLUE	1	12	9			0.75	0.75							PANEL ON SAME POST AS 1733
7-17	UNIT 2	1739	RIO BRAVO BLVD	2032+06	RT	FWBT	R15-1	BLACK	WHITE	2	24	4.5			0.75	1.50	12.0		12.0	1	4.0	16.0	
7-17	UNIT 2	1741	RIO BRAVO BLVD	2032+06	RT	FWBT	R1-2	RED	WHITE	1	18				11.69	11.69							SIGN FACE & INSTALLATION PER 701-22-1/5, 2/5, 4/5
7-17	UNIT 2	1743	RIO BRAVO BLVD	2032+06	RT	FWBT		WHITE	BLUE	1	12	9			0.75	0.75							PANEL ON SAME POST AS 1739
7-18	UNIT 2	1801	RIO BRAVO BLVD	2041+50	11.00' RT	FEBT		WHITE	GREEN	1	192	84			112.00	112.00							PANEL ON SAME POST AS 1739
7-18	UNIT 2	1802	RIO BRAVO BLVD	2043+40	LT	FWBT		WHITE	GREEN	1	84	18			10.50	10.50	9.3		9.3	2	3.5	25.7	
7-18	UNIT 2	1803	RIO BRAVO BLVD	2041+50	76.25' RT	FEBT		WHITE	GREEN	1	180	96			120.00	120.00							EXTRUDED PANEL ON OH SIGN STRUCTURE - SEE SHEETS 7-30 & 7-31
7-18	UNIT 2	1804	RIO BRAVO BLVD	2044+46	LT	FWBT	R2-1-36-45	BLACK	WHITE	1	36	48			12.00	12.00	12.3	12.3	24.6	2	3.5	31.6	
7-18	UNIT 2	1805	RIO BRAVO BLVD	2044+20	RT	FEBT		WHITE	GREEN	1	60	48			20.00	20.00	12.3	12.8	25.2	2	3.5	32.2	
7-18	UNIT 2	1806	RIO BRAVO BLVD	2044+49	LT	FWBT	R2-1-36-45	BLACK	WHITE	1	36	48			12.00	12.00	12.3	12.3	24.6	2	3.5	31.6	
7-18	UNIT 2	1808	RIO BRAVO BLVD	2045+57	LT	FWBT	M1-SA-54-600	WHITE	GREEN	1	54	66			24.75	24.75	12.3	12.8	25.2	2	3.5	32.2	
7-18	UNIT 2	1810	RIO BRAVO BLVD	2045+60	LT	FWBT	M1-SA-54-600	WHITE	GREEN	1	54	66			24.75	24.75	12.3	12.8	25.2	2	3.5	32.2	
7-18	UNIT 2	1811	RIO BRAVO BLVD	2064+20	RT	FEBT	R10-15R	BLACK	WHITE	1	30	30			6.25	6.25	11.3		11.3	1	3.5	14.8	
7-19	UNIT 2	1901	RIO BRAVO BLVD	2047+03	RT	FEBT	R3-7R-30	BLACK	WHITE	1	30	30			6.25	6.25	11.3		11.3	1	3.5	14.8	
7-19	UNIT 1	1902	SB ON RAMP	215+17	LT	FSBT	W4-2R-48	BLACK	YELLOW	1				48	16.00	16.00	12.3	12.8	25.2	2	3.5	32.2	
7-19	UNIT 1	1903	RIO BRAVO BLVD	2047+49	RT	FEBT		WHITE	GREEN	1	72	36			18.00	18.00	11.3	11.8	23.2	2	3.5	30.2	
7-19	UNIT 1	1904	SB ON RAMP	216+53	LT	FSBT		WHITE	GREEN	1	36	48			12.00	12.00	12.3	12.8	25.2	2	3.5	32.2	
7-19	UNIT 1	1905	RIO BRAVO BLVD	2047+55	RT	FEBT		WHITE	GREEN	1	36	48			12.00	12.00	12.3	12.8	25.2	2	3.5	32.2	
7-19	UNIT 1	1906	SB ON RAMP	216+76	RT	FSBT	R4-7-36	BLACK	WHITE	1	36	48			12.00	12.00	12.3	12.3	24.7	2	3.5	31.7	
7-19	UNIT 1	1907	RIO BRAVO BLVD	2047+86	RT	FEBT	W11-15-36	BLACK	YELLOW	1				36	9.00	9.00	12.3	12.3	24.7	2	3.5	31.7	
7-19	UNIT 1	1908	SB ON RAMP	216+51	RT	FSBT	R5-1-36	WHITE	RED	1	36	36			9.00	9.00	11.3		11.3	1	3.5	14.8	
7-19	UNIT 1	1909	RIO BRAVO BLVD	2047+86	RT	FEBT	W16-7PR-24	BLACK	YELLOW	1	24	12			2.00	2.00							PANEL ON SAME POST AS 1907
7-19	UNIT 2	1910	RIO BRAVO BLVD	2047+19	RT	FWBT	RS-1A-42	WHITE	RED	1	42	30			8.75	8.75	11.3	10.8	22.2	2	3.5	29.2	
7-19	UNIT 1	1911	RIO BRAVO BLVD	2048+07	RT	FEBT	W11-15-36	BLACK	YELLOW	1				36	9.00	9.00	12.3	12.3	24.7	2	3.5	31.7	
7-19	UNIT 2	1912	RIO BRAVO BLVD	2047+21	RT	FWBT	RS-1A-42	WHITE	RED	1	24	12			2.00	2.00							PANEL ON SAME POST AS 1911
7-19	UNIT 1	1913	RIO BRAVO BLVD	2048+07	RT	FEBT	W16-7PL-24	BLACK	YELLOW	1	36	36			9.00	9.00	11.3	11.3	22.7	2	3.5	29.7	
7-19	UNIT 1	1915	RIO BRAVO BLVD	2049+09	RT	FEBT	R3-3-36	BLACK	WHITE	1	36	36			9.00	9.00	11.3	11.3	22.7	2	3.5	29.7	
7-19	UNIT 2	1916	RIO BRAVO BLVD	2047+75	RT	FWBT	R5-1-36	WHITE	RED	1	36	36			9.00	9.00							MOUNTED ON TRAFFIC SIGNAL MAST ARM
7-19	UNIT 1	1917	RIO BRAVO BLVD	2049+50	RT	FEBT	R3-3-36	BLACK	WHITE	1	36	36			9.00	9.00	11.3	11.3	22.7	2	3.5	29.7	
7-19	UNIT 2	1918	RIO BRAVO BLVD	2047+75	RT	FWBT	R5-1-36	WHITE	RED	1	36	36			9.00	9.00	11.3	11.3	22.7	2	3.5	29.7	



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NO.	DESCRIPTION	DATE	
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25 / RIO BRAVO BLVD. INTERCHANGE			
PERMANENT STRIPING AND SIGNING SIGN INSTALLATION SCHEDULE			

PERMANENT SIGN INSTALLATION SCHEDULE

SIGN POST QUANTITIES											MULTI-DIRECTION BASES REQUIRED		INSTALLATION NOTES												
PLAN SHEET	UNIT	SIGN NO.	ALIGNMENT	STATION	OFFSET	FACING DIRECTION OF TRAFFIC	SIGN CODE	COLOR	LEGEND	BACKGRD.	PLAN QTY. EACH	SIGN DIMENSIONS				AREA EACH (SF)	AREA TOTAL (SF)	SIGN POST LENGTHS				BASE POSTS QTY. EACH	LENGTH (LF)	TOTAL LENGTH (LF)	
												WIDTH (IN)		HEIGHT (IN)	ROUND (IN)			DIAMOND (IN)	LEFT (LF)	CENTER (LF)	RIGHT (LF)				TOTAL (LF)

7-19	UNIT 1	1919	RIO BRAVO BLVD	2049+70	RT	FEBT	R3-3-36	BLACK	WHITE		1	36	36			9.00	9.00		11.3			11.3	1	3.5	14.8		SIGN FACE PER SHEET 7-33	
7-19	UNIT 1	1920	RIO BRAVO BLVD	2047+95	RT	FWBT		WHITE	GREEN		1	36	48			12.00	12.00	12.3			12.8	25.2	2	3.5	32.2			
7-19	UNIT 1	1921	RIO BRAVO BLVD	2049+08	RT	FEBT	R3-3-36	BLACK	WHITE		1	36	36			9.00	9.00		11.3			11.3	1	3.5	14.8			
7-19	UNIT 1	1922	RIO BRAVO BLVD	2047+95	RT	FWBT	R4-7-36	BLACK	WHITE		1	36	48			12.00	12.00	12.3			12.3	24.7	2	3.5	31.7		EXTRUDED PANEL ON BRIDGE - SIGN FACE PER SHEET 7-31	
7-19	UNIT 1	1923	RIO BRAVO BLVD	2050+35	RT	FEBT		WHITE	GREEN		1	180	78			97.50	97.50											
7-19	UNIT 1	1924	SB EXIT RAMP	500+74	LT	FSBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2		EXTRUDED PANEL ON SQUARE TUBING SIGN POSTS - SEE SHEET 7-33	
7-19	UNIT 1	1925	RIO BRAVO BLVD	2049+75	RT	FNBT		WHITE	GREEN		1	120	60			50.00	50.00	13.3	13.3		13.3	40.0	3	3.5	50.5			
7-19	UNIT 1	1926	SB EXIT RAMP	501+03	LT	FSBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-19	UNIT 1	1926	SB EXIT RAMP	501+03	RT	FSBT	W1-8-36	BLACK	WHITE		1	36	18			4.50	4.50		9.8			9.8	1	3.5	13.3			
7-19	UNIT 1	1927	RIO BRAVO BLVD	2049+78	RT	FNBT	R5-1R-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-19	UNIT 1	1928	SB EXIT RAMP	501+38	LT	FSBT	W1-8-36	BLACK	WHITE		1	36	36			9.00	9.00	11.3			11.3	22.7	2	3.5	29.7		PANEL ON SAME POST AS 1929	
7-19	UNIT 1	1929	RIO BRAVO BLVD	2049+06	LT	FNBT	R5-1-36	WHITE	RED		1	36	36			7.25	7.25										PANEL ON SAME POST AS 1929	
7-19	UNIT 1	1930	RIO BRAVO BLVD	2049+06	LT	FWBT	R5-1L-58	BLACK	WHITE		1	58	18			7.25	7.25											
7-19	UNIT 1	1931	RIO BRAVO BLVD	2049+06	LT	FEBT	R5-1R-58	BLACK	WHITE		1	58	18			9.00	9.00	11.3			11.3	22.7	2	3.5	29.7		PANEL ON SAME POST AS 1932	
7-19	UNIT 1	1932	RIO BRAVO BLVD	2048+55	LT	FNBT	R5-1-36	WHITE	RED		1	36	36			7.25	7.25										PANEL ON SAME POST AS 1932	
7-19	UNIT 1	1933	RIO BRAVO BLVD	2048+55	LT	FEBT	R5-1R-58	BLACK	WHITE		1	58	18			7.25	7.25											
7-19	UNIT 1	1934	RIO BRAVO BLVD	2048+55	LT	FWBT	R5-1L-58	BLACK	WHITE		1	58	18			9.00	9.00		11.3			11.3	1	3.5	14.8			
7-19	UNIT 1	1935	RIO BRAVO BLVD	2049+18	LT	FWBT	R3-1-36	BLACK	WHITE		1	36	36			7.50	7.50		11.3			11.3	1	3.5	14.8			
7-19	UNIT 1	1936	SB ON RAMP	217+34	RT	FSBT	R3-5L-30	BLACK	WHITE		1	30	36			9.00	9.00										MOUNTED ON TRAFFIC SIGNAL MAST ARM	
7-19	UNIT 1	1937	RIO BRAVO BLVD	2049+19	RT	FEBT	R3-3-36	BLACK	WHITE		1	36	36			7.50	7.50		11.3			11.3	1	3.5	14.8			
7-19	UNIT 1	1938	SB ON RAMP	217+35	RT	FSBT	R3-5L-30	BLACK	WHITE		1	30	36			3.00	3.00										MOUNTED ON TRAFFIC SIGNAL POLE	
7-19	UNIT 1	1939	RIO BRAVO BLVD	2049+95	RT	FNBT	R10-10b	BLACK	WHITE		1	18	24			3.00	3.00										MOUNTED ON TRAFFIC SIGNAL POLE	
7-19	UNIT 1	1940	RIO BRAVO BLVD	2050+35	RT	FNBT	R10-10b	BLACK	WHITE		1	18	24			3.00	3.00											
7-20	UNIT 1	2001	RIO BRAVO BLVD	2053+51	RT	FEBT	W1-15-36	BLACK	YELLOW		1				36	9.00	9.00	11.3			10.8	22.2	2	3.5	29.2		PANEL ON SAME POST AS 2001	
7-20	UNIT 1	2002	RIO BRAVO BLVD	2053+51	RT	FEBT	W13-1-24-30	BLACK	YELLOW		1	24	24			4.00	4.00										SIGN FACE PER SHEET 7-33	
7-20	UNIT 1	2003	RIO BRAVO BLVD	2054+40	RT	FWBT		WHITE	GREEN		1	36	48			12.00	12.00	12.3			12.8	25.2	2	3.5	32.2		SIGN FACE PER SHEET 7-33	
7-20	UNIT 1	2004	RIO BRAVO BLVD	2054+41	LT	FWBT		WHITE	GREEN		1	36	48			12.00	12.00	12.3			12.8	25.2	2	3.5	32.2			
7-20	UNIT 1	2005	NB ON RAMP	401+22	RT	FNBT	R5-10A-30	BLACK	WHITE		1	30	36			7.50	7.50		11.3			11.3	1	3.5	14.8			
7-20	UNIT 1	2006	NB ON RAMP	401+22	RT	FNBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-20	UNIT 1	2008	NB ON RAMP	401+35	LT	FNBT	W1-8-36	BLACK	YELLOW		1	36	48		48	16.00	16.00	13.8			13.3	27.2	2	3.5	34.2			
7-20	UNIT 1	2010	NB ON RAMP	402+00	RT	FNBT	W4-2R-48	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-20	UNIT 1	2012	NB ON RAMP	402+14	LT	FNBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-20	UNIT 1	2014	NB ON RAMP	402+92	LT	FNBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-20	UNIT 1	2016	NB ON RAMP	403+74	LT	FNBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	25.2	2	3.5	32.2			
7-21	UNIT 1	2101	EB-NB LOOP	306+60	LT	FNBT	W1-6R-48	BLACK	YELLOW		1	48	24			8.00	8.00	13.8			13.3	27.2	2	3.5	34.2			
7-21	UNIT 1	2102	RIO BRAVO BLVD	2057+81	LT	FWBT	R3-8-54	BLACK	WHITE		1	54	30			11.25	11.25	10.3			9.8	20.2	2	3.5	27.2		PANEL ON SAME POST AS 2101	
7-21	UNIT 1	2103	EB-NB LOOP	306+60	LT	FNBT	W13-1-24-35	BLACK	YELLOW		1	24	24			4.00	4.00										SIGN FACE PER SHEET 7-33	
7-21	UNIT 1	2104	RIO BRAVO BLVD	2058+90	LT	FWBT		WHITE	GREEN		1	60	48			20.00	20.00	12.3			12.8	25.2	2	3.5	32.2			
7-21	UNIT 1	2105	EB-NB LOOP	305+01	LT	FEBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			12.3	24.7	2	3.5	31.7			
7-21	UNIT 1	2106	RIO BRAVO BLVD	2059+79	LT	FWBT	R2-1-36-45	BLACK	WHITE		1	36	48			12.00	12.00	12.3			12.3	25.2	2	3.5	32.2			
7-21	UNIT 1	2107	EB-NB LOOP	305+61	LT	FEBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			10.3	10.3	1	3.5	13.8			
7-21	UNIT 1	2108	RIO BRAVO BLVD	2060+47	RT	FEBT	R5-3-24	BLACK	WHITE		1	24	24			4.00	4.00		12.8		12.3	25.2	2	3.5	32.2			
7-21	UNIT 1	2109	EB-NB LOOP	306+41	LT	FEBT	W1-8-36	BLACK	YELLOW		1	36	48			12.00	12.00	12.8			9.3	9.3	1	3.5	12.8		SIGN FACE PER SHEET 7-33	
7-21	UNIT 1	2110	RIO BRAVO BLVD	2060+47	RT	FSBT		WHITE	GREEN		1	18	12			1.50	1.50		9.3			9.8	1	3.5	13.3			
7-21	UNIT 1	2111	RIO BRAVO BLVD	2056+81	LT	FEBT	R3-8L-36	BLACK	WHITE		1	36	30			7.50	7.50				9.8		9.8	1	3.5	13.3		
7-21	UNIT 1	2113	RIO BRAVO BLVD	2057+49	RT	FEBT	R3-8R-36	BLACK	WHITE		1	36	30			7.50	7.50											

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 AECOM JOB NO. 8039332

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 www.aecom.com

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NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 -

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NO.	DESCRIPTION	DATE	
REV-SIGNS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25 / RIO BRAVO BLVD. INTERCHANGE			
PERMANENT STRIPING AND SIGNING SIGN INSTALLATION SCHEDULE			

PERMANENT SIGN INSTALLATION SCHEDULE

PERMANENT SIGN INSTALLATION SCHEDULE										ALUMINUM PANEL										SIGN POST QUANTITIES								MULTI-DIRECTION BASES REQUIRED	INSTALLATION NOTES
PLAN SHEET	UNIT	SIGN NO.	ALIGNMENT	STATION	OFFSET	FACING DIRECTION OF TRAFFIC	SIGN CODE	COLOR		PLAN QTY. EACH	SIGN DIMENSIONS				AREA EACH (SF)	AREA TOTAL (SF)	SIGN POST LENGTHS				BASE POSTS		TOTAL LENGTH (LF)						
											WIDTH (IN)	HEIGHT (IN)	ROUND (IN)	DIAMOND (IN)			LEFT (LF)	CENTER (LF)	RIGHT (LF)	TOTAL (LF)	QTY. EACH	LENGTH (LF)							
7-22	UNIT 1	2201	EB-NB LOOP	307+21	LT	FEBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2203	EB-NB LOOP	308+01	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2205	EB-NB LOOP	308+61	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2207	EB-NB LOOP	309+61	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2209	EB-NB LOOP	310+41	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2211	EB-NB LOOP	311+21	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2213	EB-NB LOOP	312+01	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2215	EB-NB LOOP	312+81	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2217	EB-NB LOOP	313+61	LT	FSBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-22	UNIT 1	2219	EB-NB LOOP	314+41	LT	FWBT	W1-8-36	BLACK	YELLOW	1	36	48		12.00	12.00	12.8		12.3	25.2	2	3.5	32.2							
7-23	UNIT 2	2301	RIO BRAVO BLVD	2025+59	RT	FSBT		WHITE	GREEN	1	102	18		12.75	12.75									MOUNTED ON TRAFFIC SIGNAL MAST ARM - SEE SHEET 7-33					
7-23	UNIT 2	2302	RIO BRAVO BLVD	2025+90	LT	FWBT		WHITE	GREEN	1	102	18		12.75	12.75									MOUNTED ON TRAFFIC SIGNAL MAST ARM - SEE SHEET 7-33					
UNIT 1 TOTALS											701000 PANEL SIGNS (SF)				1250		701100 STEEL POST AND BASE POST (LF)				2574		0						
UNIT 2 TOTALS											701000 PANEL SIGNS (SF)				694		701100 STEEL POST AND BASE POST (LF)				1194		0						
UNIT 1 TOTALS											701010 PANEL SIGNS (SF)				1404														
UNIT 2 TOTALS											701010 PANEL SIGNS (SF)				232														

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NO.	DESCRIPTION	DATE	
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION			
I-25 / RIO BRAVO BLVD. INTERCHANGE			
PERMANENT STRIPING AND SIGNING SIGN INSTALLATION SCHEDULE			

NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 -

511
TRAVEL
INFO
CALL 511
EX

(47) Broadway 4
Los Lunas 16
Las Cruces 216
EX



MATCHLINE I-25 NB STA. 1658+00.00
SEE SHEET 7-6

168'
FROM END OF BRIDGE RAIL

EXIT 220

500

Rio Bravo Blvd
1/2 MILE

FOOD GAS
BURGER KING
TACO BELL PIZZA HUT VALERO
EXISTING EXTRUDED PANEL I-BEAM SIGN
TO BE REMOVED BY OTHERS

(0501) CONSTRUCT NEW I-BEAM POSTS AND FOUNDATIONS
RELOCATE EXTRUDED SIGN PANEL FROM STA. 1661+70
SEE SHEET 7-25

SPEED
LIMIT
65
EX

NO.	DESCRIPTION	DATE
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REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25 / RIO BRAVO BLVD. INTERCHANGE		
PERMANENT STRIPING AND SIGNING I-25		



1/10/2017

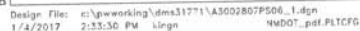
NEW MEXICO PROJECT NO. A300280

SHEET NO. 7

DESIGNED BY: AECOM

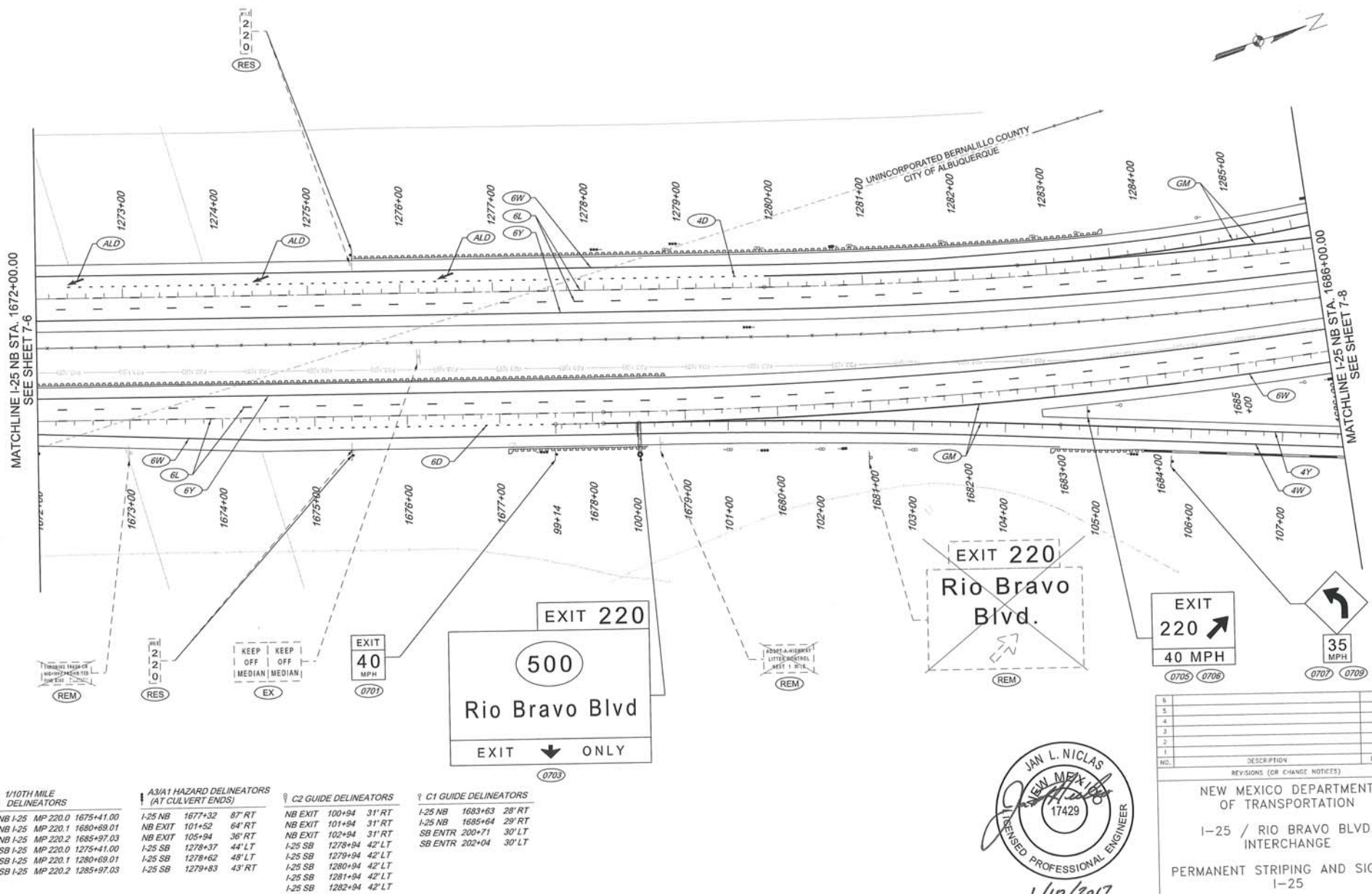
ASGAM TECHNICAL SERVICES, INC.
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FAX: (505) 845-7601
EMAIL: jlm@asgamt.com

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NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25 / RIO BRAVO BLVD. INTERCHANGE

PERMANENT STRIPING AND SIGNING I-25

AECOM JOB NO. 6039326

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DESIGNED BY:

Design File: c:\pwworking\dms31771\A300280\7P509_1.dgn
1/4/2017 2:53:28 PM User: hvdot_pjt.PLTGFGMATCHLINE I-25 NB STA. 1700+00.00
SEE SHEET 7-8MATCHLINE I-25 NB STA. 1714+00.00
SEE SHEET 7-101/10TH MILE
DELINEATORS

NB I-25	MP 220.5	1701+81.06
NB I-25	MP 220.6	1707+09.08
NB I-25	MP 220.7	1712+37.09
SB I-25	MP 220.5	1301+81.06
SB I-25	MP 220.6	1307+09.08
SB I-25	MP 220.7	1312+37.09

A3/A1 HAZARD DELINEATORS
(AT CULVERT ENDS)

I-25 SB	1310+07	68' LT
I-25 SB	1312+32	75' LT
SB EXIT	512+62	117' LT
WN ENTR	407+74	55' RT
WN ENTR	412+92	35' RT
WN ENTR	413+19	14' LT

C1 GUIDE DELINEATORS

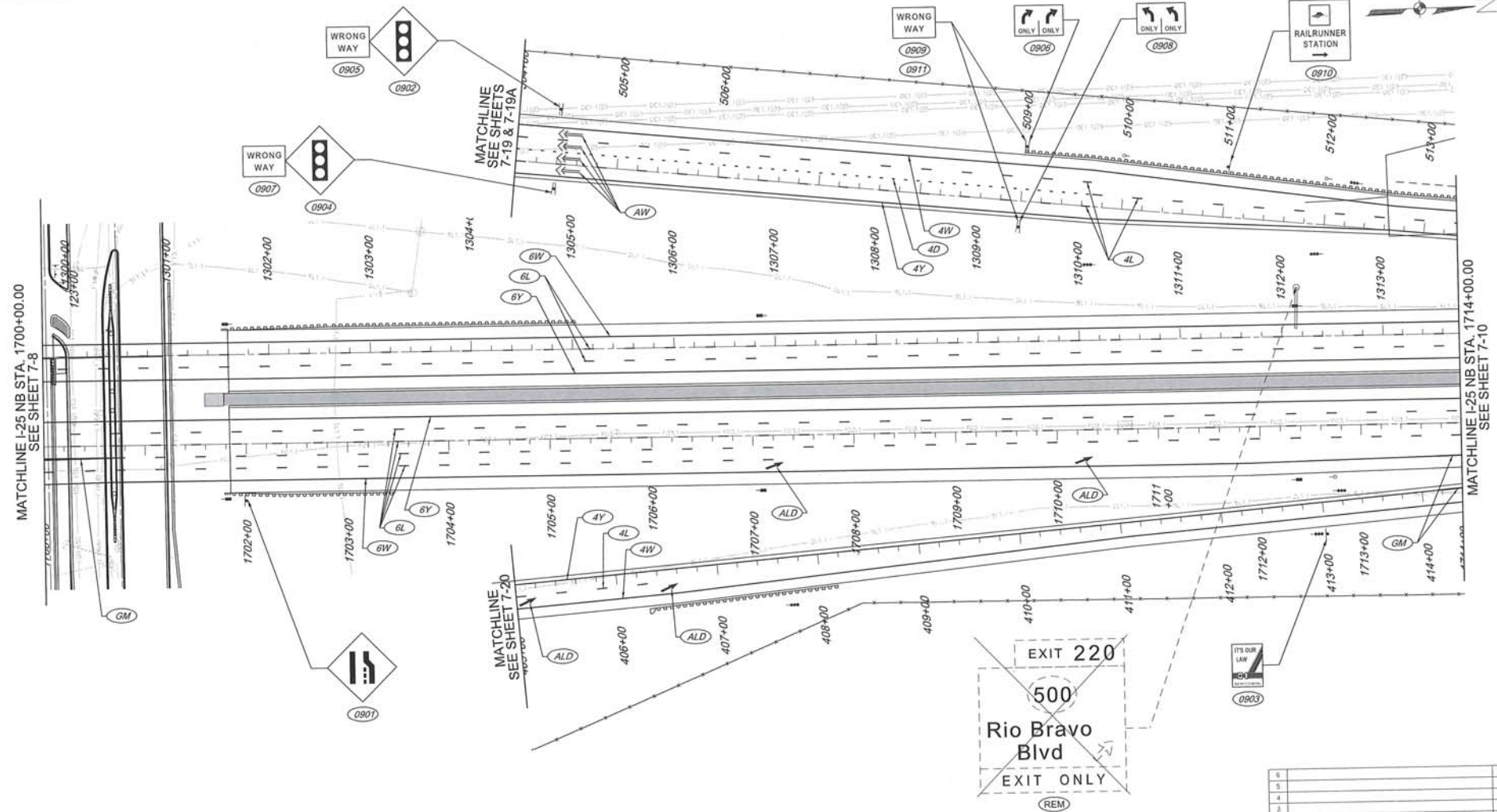
SB EXIT	510+04	53' LT
SB EXIT	512+05	45' LT
WN ENTR	1712+67	51' RT

C2/R2 DELINEATORS

SB EXIT	504+08	53' LT
SB EXIT	504+33	53' LT

A2/R2 DELINEATORS

SB EXIT	504+08	22' RT
SB EXIT	504+33	22' RT



1/10/2017

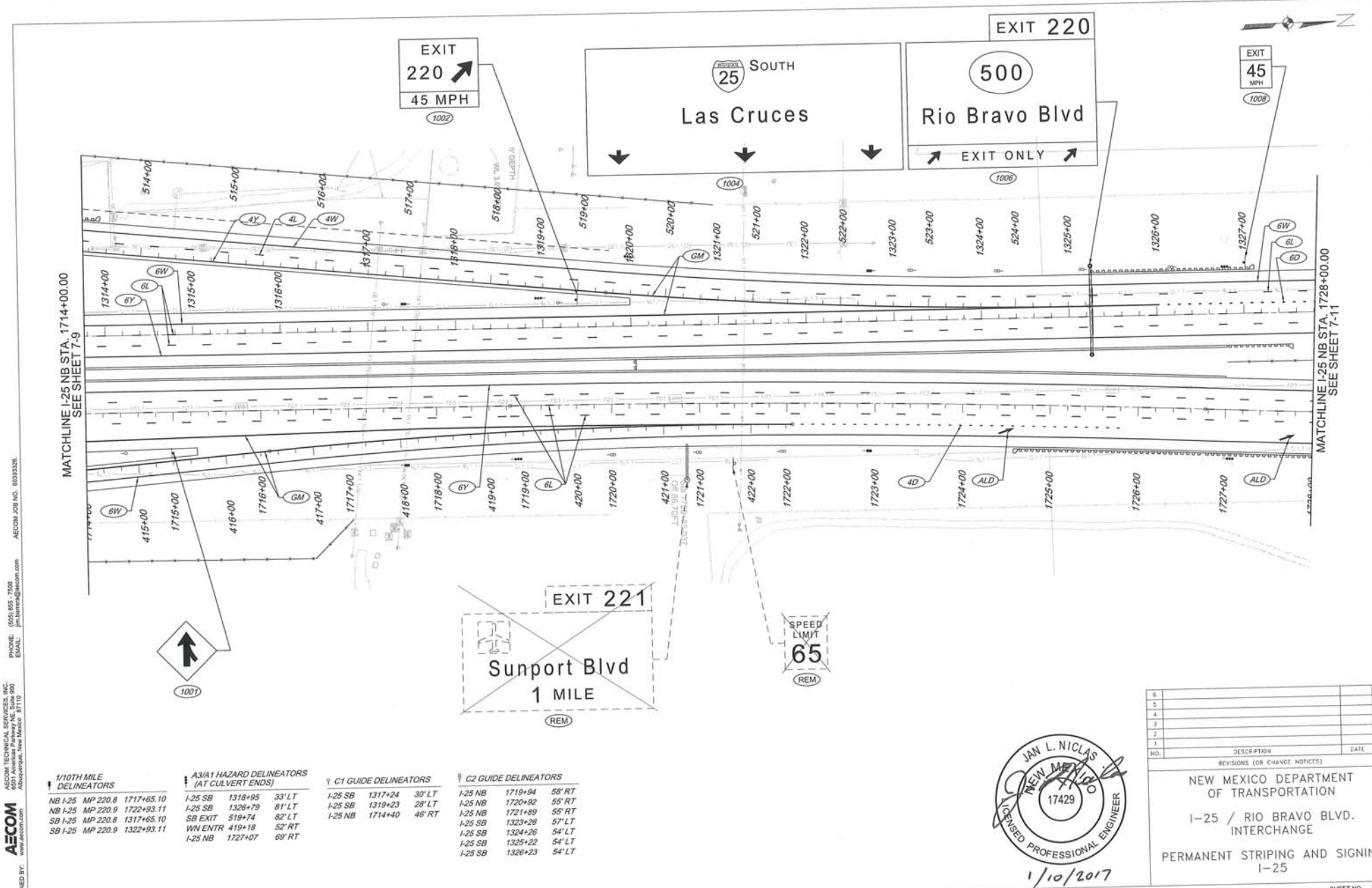
NEW MEXICO PROJECT NO. A300280

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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25 / RIO BRAVO BLVD.
INTERCHANGE

PERMANENT STRIPING AND SIGNING
I-25



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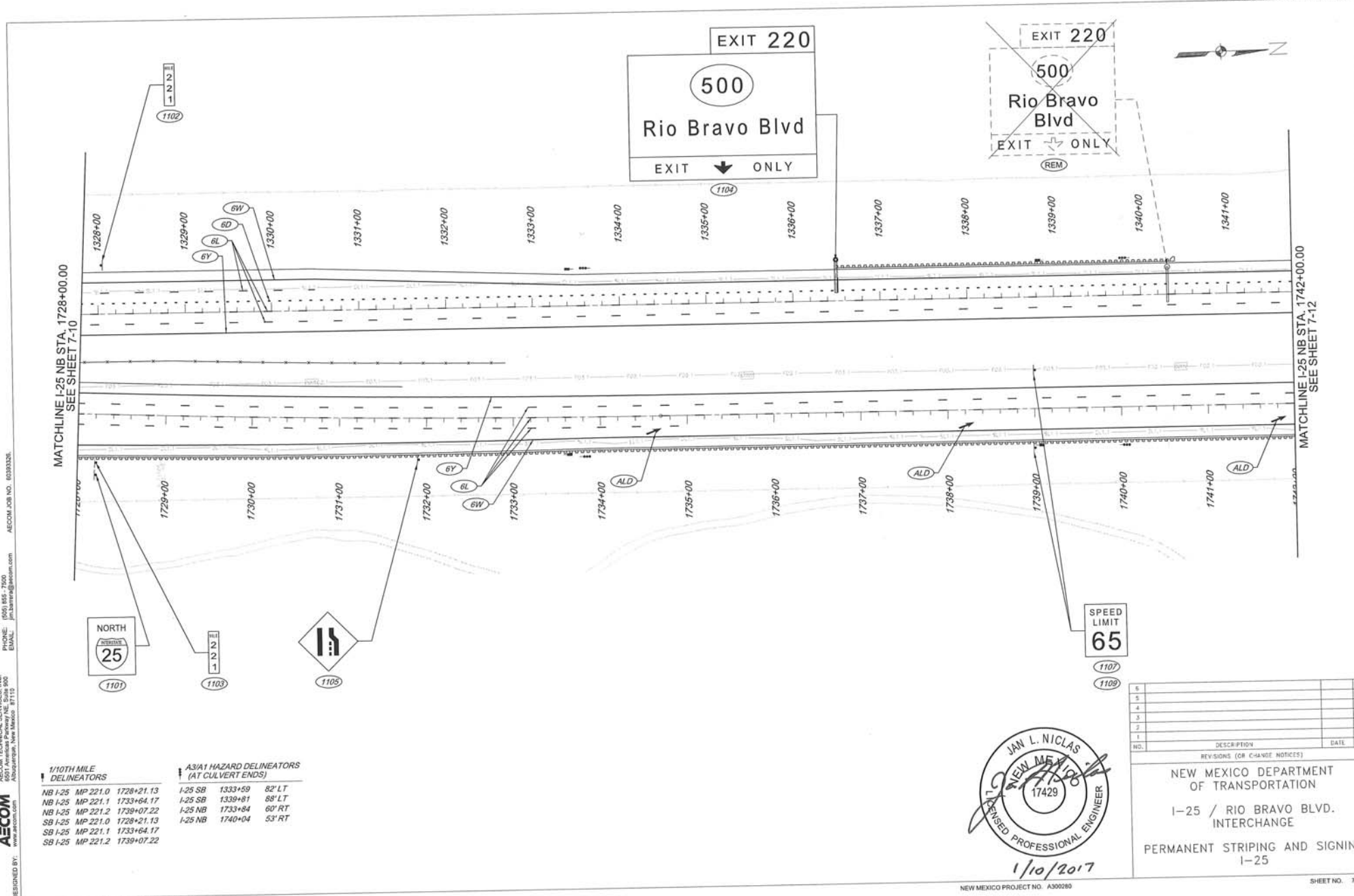
1/10TH MILE DELINEATORS	A3/A1 HAZARD DELINEATORS (AT CULVERT ENDS)	C1 GUIDE DELINEATORS	C2 GUIDE DELINEATORS
NB I-25 MP 220.8 1717+65.10	I-25 SB 1318+95 33' LT	I-25 SB 1317+24 30' LT	I-25 NB 1719+94 58' RT
NB I-25 MP 220.9 1722+93.11	I-25 SB 1326+79 81' LT	I-25 SB 1319+23 28' LT	I-25 NB 1720+92 55' RT
SB I-25 MP 220.8 1317+65.10	SB EXIT 519+74 82' LT	I-25 NB 1714+40 46' RT	I-25 NB 1721+89 55' RT
SB I-25 MP 220.9 1322+93.11	WN ENTR 419+18 52' RT		I-25 SB 1323+26 57' LT
	I-25 NB 1727+07 69' RT		I-25 SB 1324+26 54' LT
			I-25 SB 1325+22 54' LT
			I-25 SB 1326+23 54' LT



1/10/2017

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NEW MEXICO DEPARTMENT
 OF TRANSPORTATION
 I-25 / RIO BRAVO BLVD.
 INTERCHANGE
 PERMANENT STRIPING AND SIGNING
 I-25



ACCOM JOB NO. 03030326

PHONE: (505) 885-7500
EMAIL: jlm@accon.com

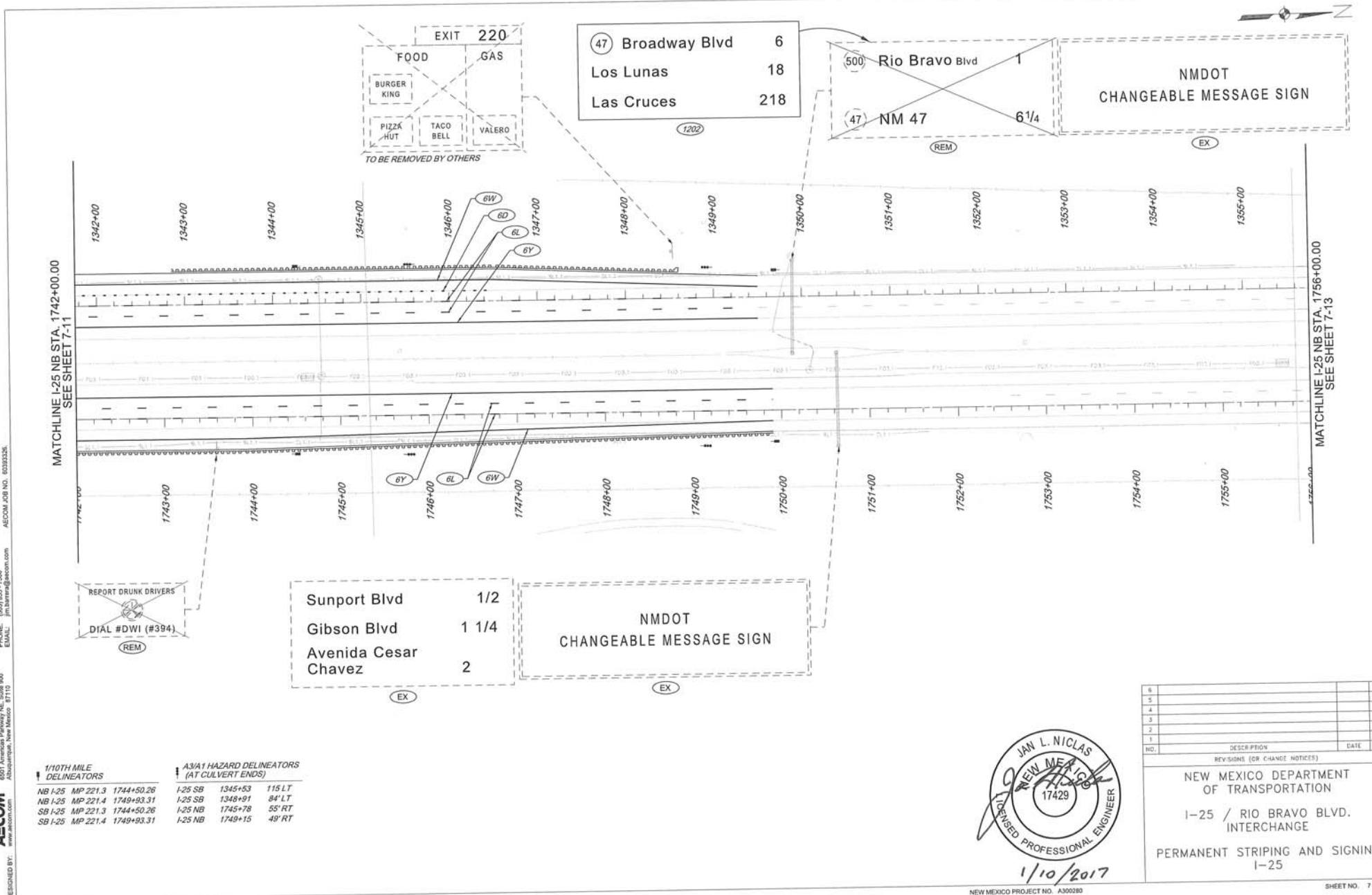
ACCON TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 900
Albuquerque, New Mexico 87110

DESIGNED BY:

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NEW MEXICO PROJECT NO. A300280

SHEET NO. 7



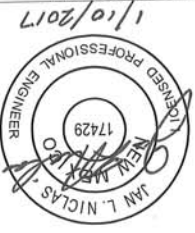
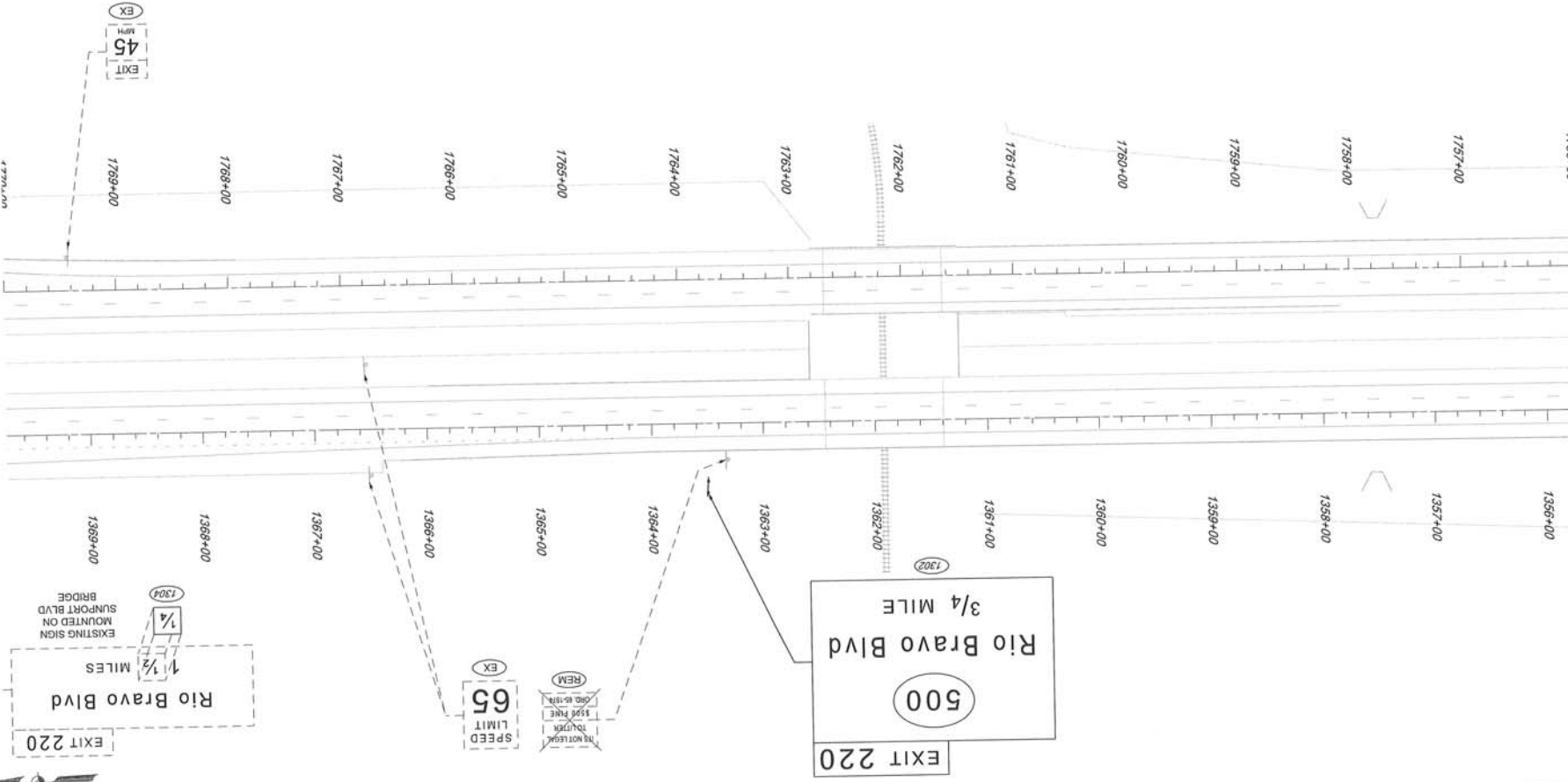
DESIGNED BY: AECOM
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 PHONE: (505) 855-7500
 FAX: (505) 855-7500
 AECOM JOB NO. 60303326
 PROJECT NO. 1742+00.00



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
 I-25 / RIO BRAVO BLVD. INTERCHANGE
 PERMANENT STRIPING AND SIGNING I-25

MATCHLINE 1-25 NB STA. 1756+00.00
SEE SHEET 7-12



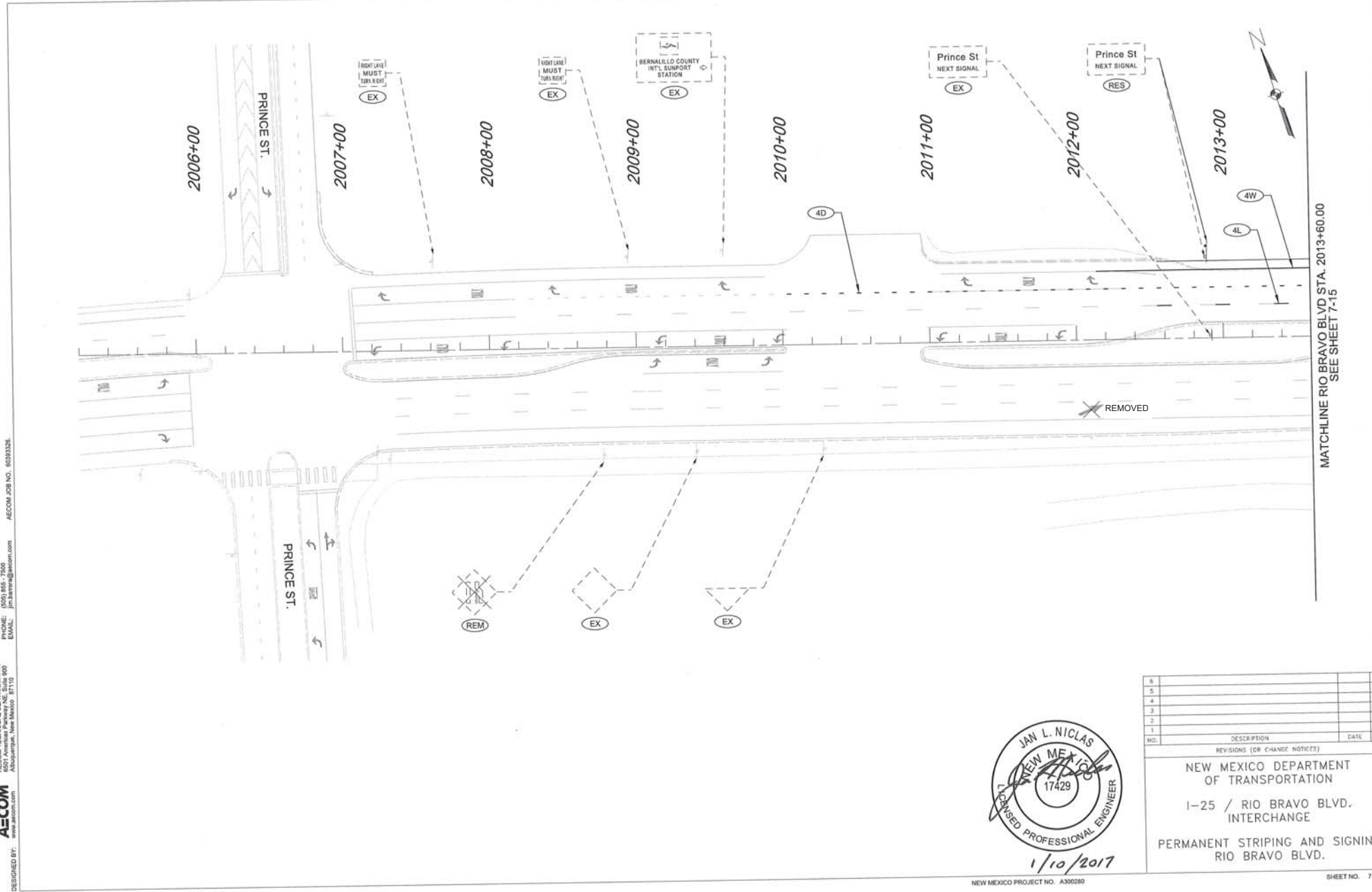
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NEW MEXICO PROJECT NO. A302080

PERMANENT STRIPING AND SIGNING
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION
1-25 / RIO BRAVO BLVD.
INTERCHANGE

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AECOM



1/10/2017

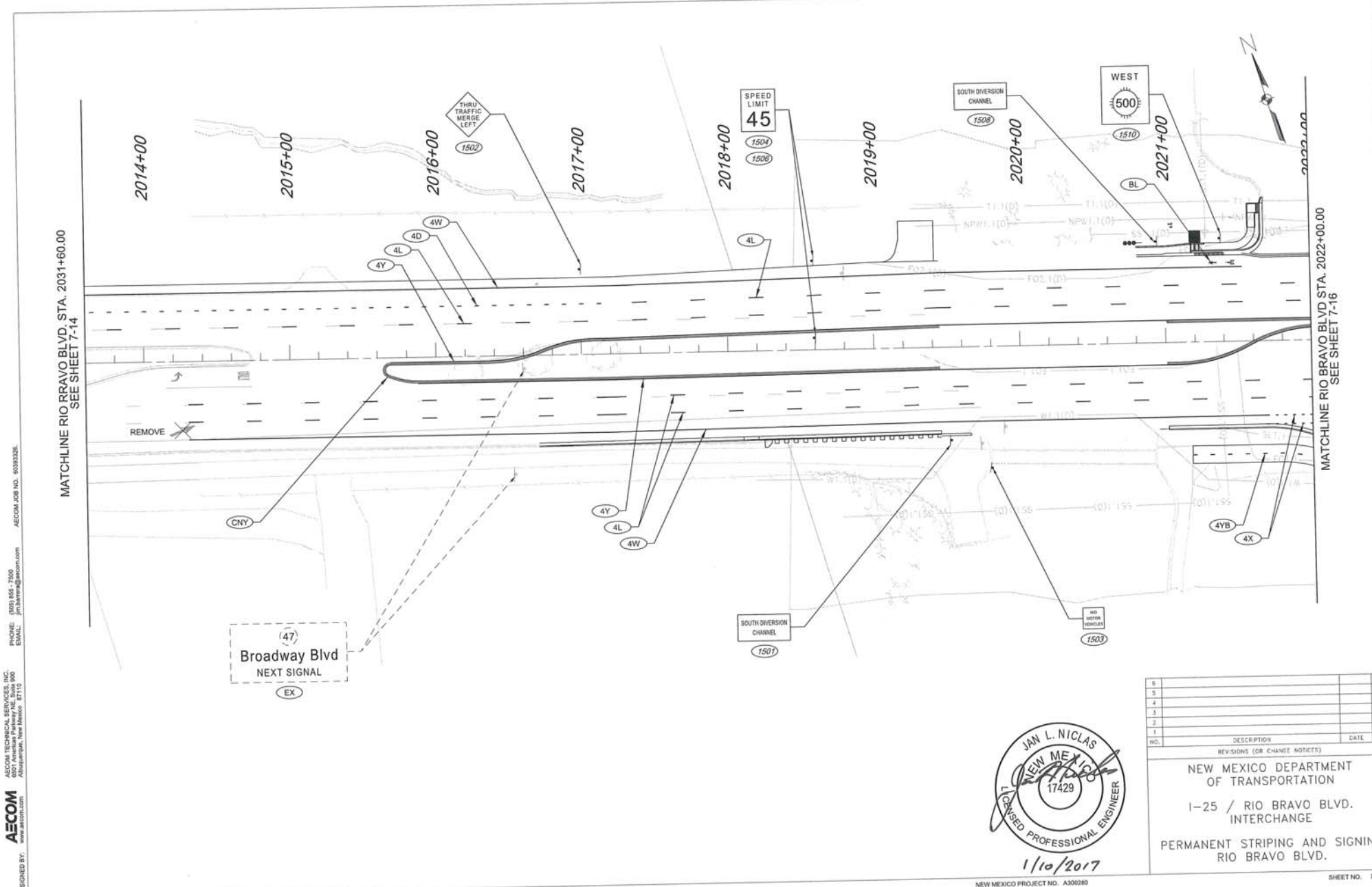
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REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25 / RIO BRAVO BLVD. INTERCHANGE

PERMANENT STRIPING AND SIGNING RIO BRAVO BLVD.



MATCHLINE RIO BRAVO BLVD. STA. 2031+60.00
SEE SHEET 7-14

MATCHLINE RIO BRAVO BLVD STA. 2022+00.00
SEE SHEET 7-16

DESIGNED BY: AECOM
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REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25 / RIO BRAVO BLVD. INTERCHANGE		
PERMANENT STRIPING AND SIGNING RIO BRAVO BLVD.		

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AECOM JOB NO. 6033326

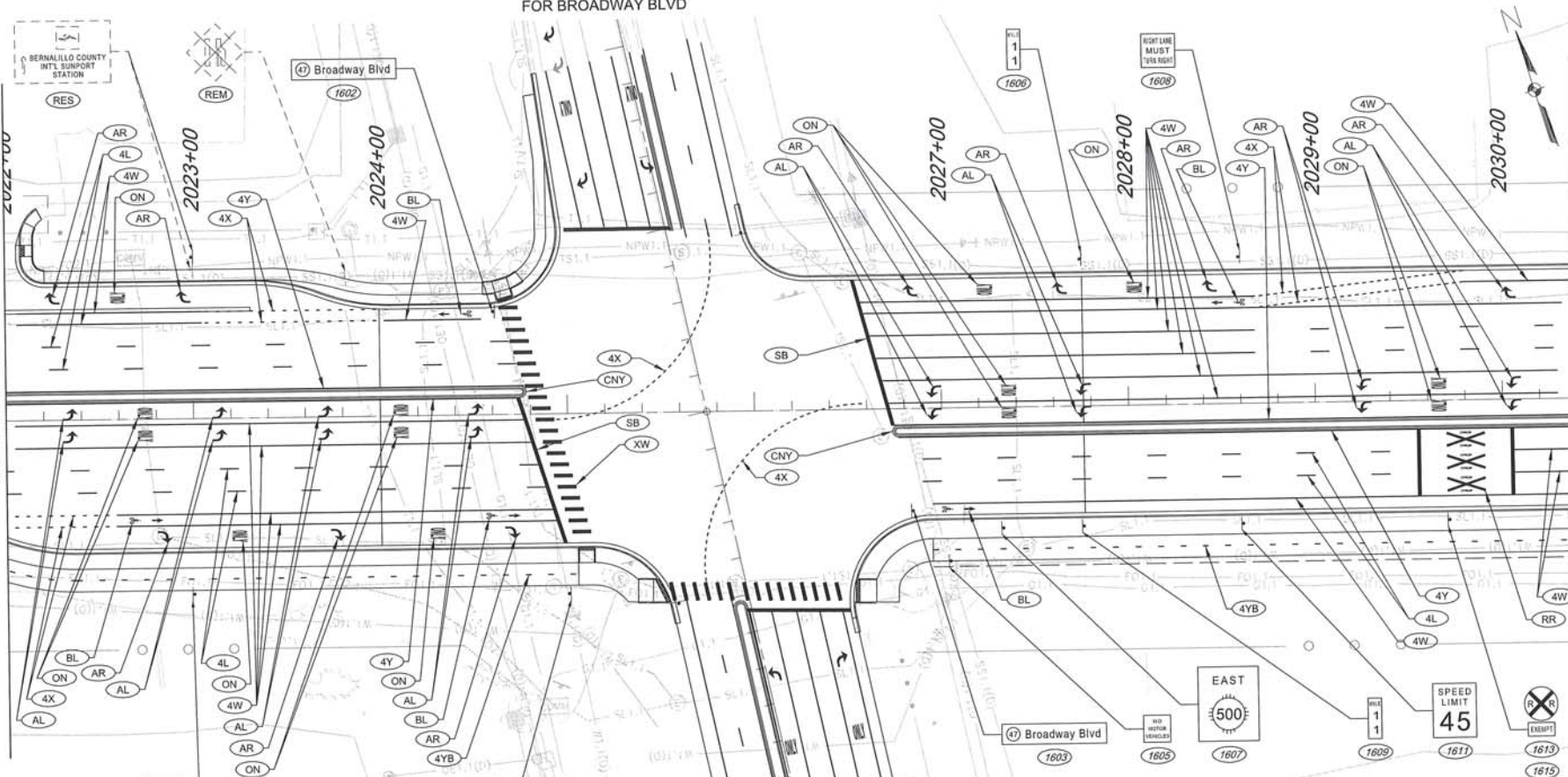
1 A3/A1 HAZARD DELINEATORS
(AT CULVERT ENDS)
RIO BRAVO 2026+30 96' LT

MATCHLINE RIO BRAVO BLVD STA. 2022+00.00
SEE SHEET 7-15

SEE SHEET 7-23
FOR BROADWAY BLVD

SEE SHEET 7-23
FOR BROADWAY BLVD

MATCHLINE RIO BRAVO BLVD STA. 2030+40.00
SEE SHEET 7-17



NO.	DESCRIPTION	DATE	BY
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NO.	DESCRIPTION	DATE	BY
	REVISIONS (OR CHANGE NOTICES)		
	NEW MEXICO DEPARTMENT OF TRANSPORTATION		
	1-25 / RIO BRAVO BLVD. INTERCHANGE		
	PERMANENT STRIPING AND SIGNING RIO BRAVO BLVD.		





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65501 Americas Parkway NE, Suite 900
Shoreline, WA 98148, U.S.A. Mexico: 87110
PHONE: (505) 855-7500
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1/10/2017

NEW MEXICO PROJECT NO. A300281

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REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25 / RIO BRAVO BLVD. INTERCHANGE		
PERMANENT STRIPING AND SIGNING RIO BRAVO BLVD.		

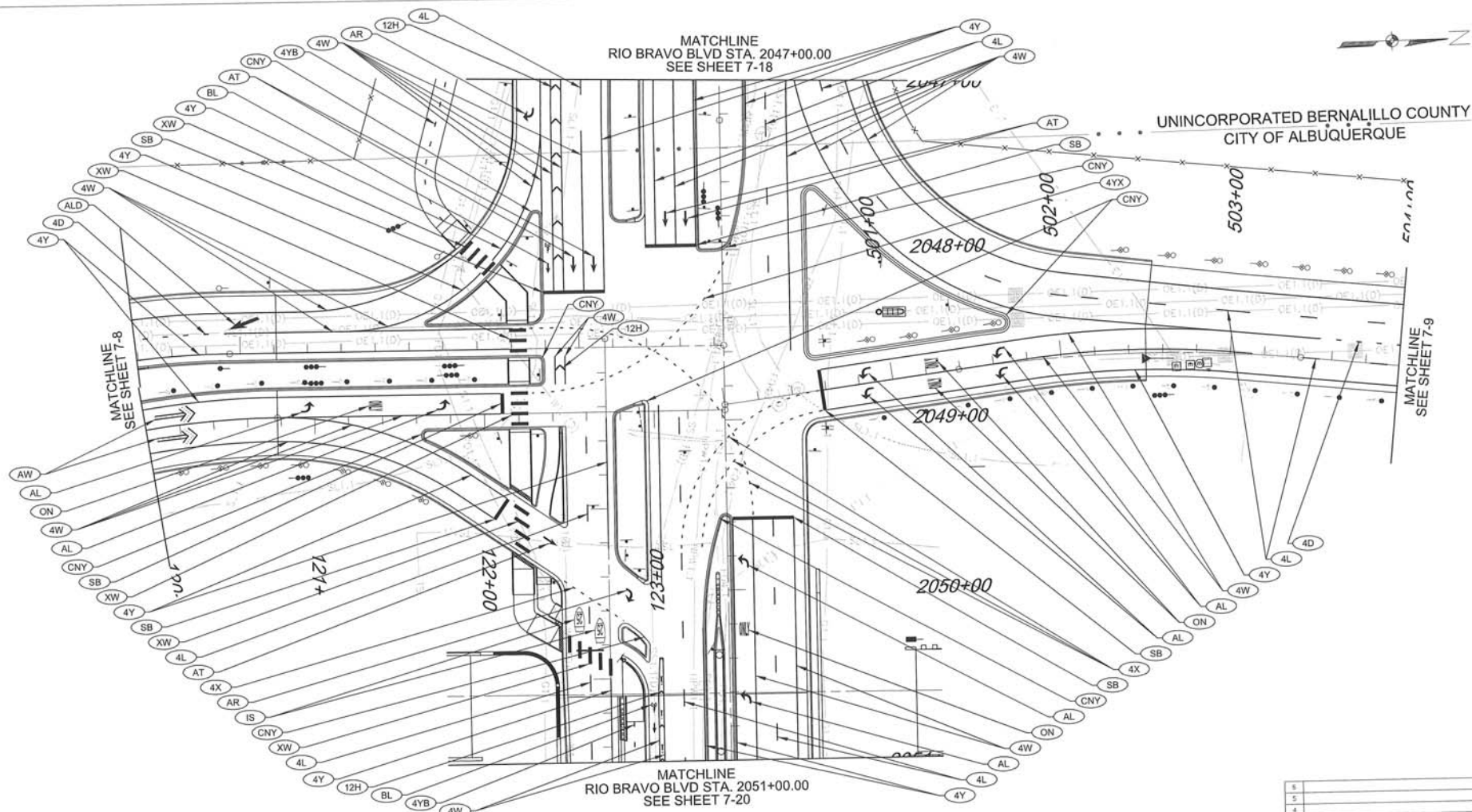
SHEET NO. 7



PHONE: (505) 855-7500
EMAIL: im.barrera@acconr.com

AECOM
AECOM TECHNICAL SERVICES, INC.
6501 Americas Parkway NE, Suite 900
Atlanta, Georgia 30328-4000
404.521.8710

Design File: c:\pwworking\dms31771\A3002807PS19_1.dgn
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A3/A1 HAZARD DELINEATORS (AT CULVERT ENDS)		
NB EXIT	121+00	25' LT
NB EXIT	121+83	26' LT
SB ENTR	215+43	13' RT
SB ENTR	216+27	12' RT
SB ENTR	215+95	59' LT
RIO BRAVO	2047+66	2' RT
SB EXIT	503+04	22' RT

C1 GUIDE DELINEATORS		
SB ENTR	214+94	39' LT
SB ENTR	216+22	58' LT

48" WHITE/WHITE TUBULAR FLEXIBLE MARKERS		
RIO BRAVO	2050+50	40' RT
RIO BRAVO	2050+70	40' RT
RIO BRAVO	2050+90	40' RT

C2/R2 DELINEATORS		
NB EXIT	120+11	28' RT
NB EXIT	120+36	28' RT
NB EXIT	120+61	28' RT
NB EXIT	120+86	28' RT
NB EXIT	121+11	31' RT
NB EXIT	121+34	37' RT
NB EXIT	121+57	47' RT
NB EXIT	121+86	13' RT

C2/R2 DELINEATORS		
SB EXIT	500+83	25' LT
SB EXIT	501+08	24' LT
SB EXIT	501+33	24' LT
SB EXIT	501+58	24' LT
SB EXIT	502+33	59' LT
SB EXIT	502+58	55' LT
SB EXIT	502+83	53' LT
SB EXIT	503+08	53' LT
SB EXIT	503+33	53' LT
SB EXIT	503+58	53' LT
SB EXIT	503+83	53' LT

A2/R2 DELINEATORS		
NB EXIT	120+11	21' LT
NB EXIT	120+36	21' LT
NB EXIT	120+61	21' LT
NB EXIT	120+86	21' LT
NB EXIT	121+11	21' LT
NB EXIT	121+36	21' LT
NB EXIT	121+61	21' LT

A2/R2 DELINEATORS		
SB EXIT	500+83	22' RT
SB EXIT	501+08	22' RT
SB EXIT	501+33	22' RT
SB EXIT	501+58	22' RT
SB EXIT	501+83	22' RT
SB EXIT	502+08	22' RT
SB EXIT	502+33	22' RT
SB EXIT	502+58	22' RT
SB EXIT	502+83	22' RT
SB EXIT	503+08	22' RT
SB EXIT	503+33	22' RT
SB EXIT	503+58	22' RT
SB EXIT	503+83	22' RT



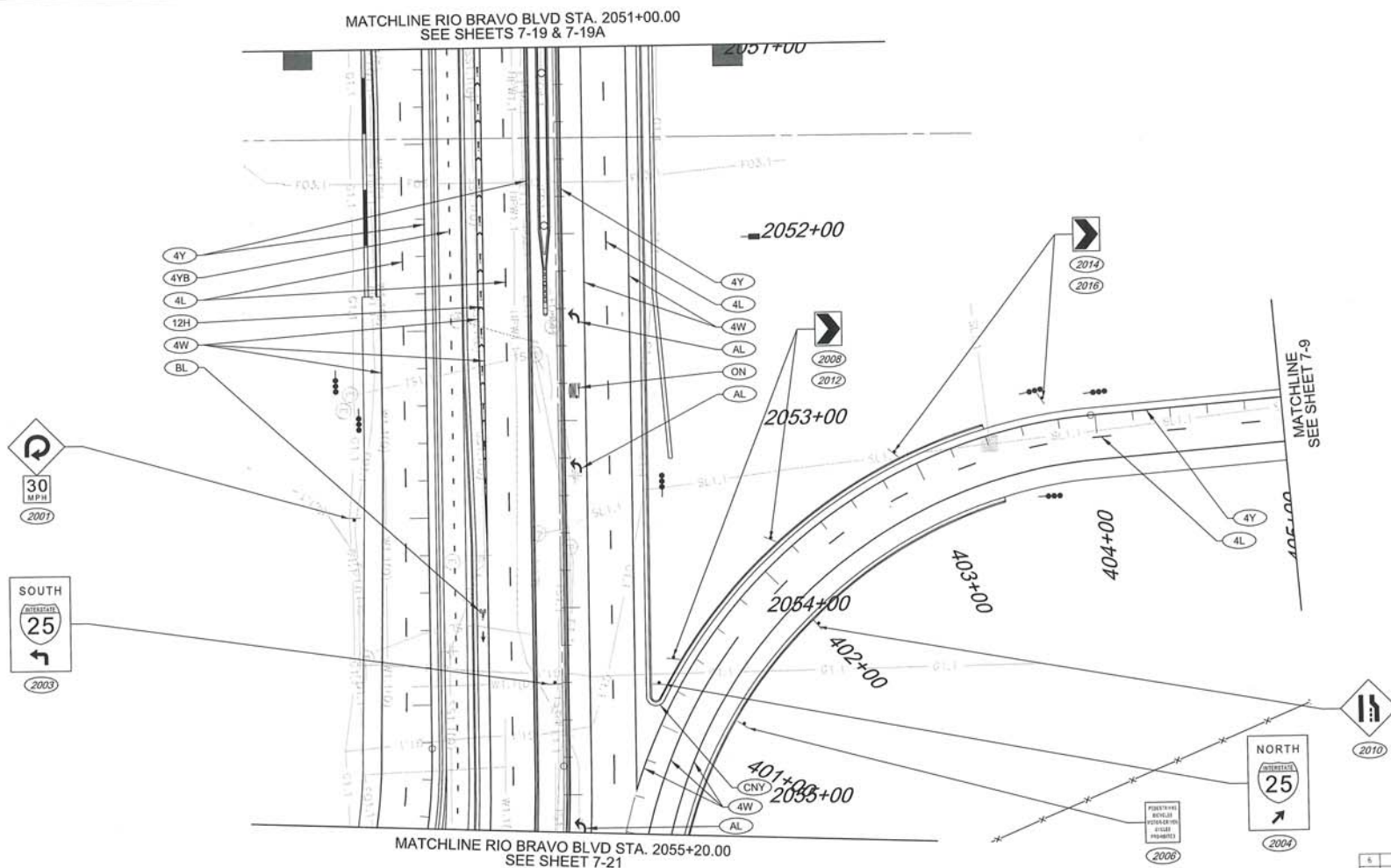
1/10/2017

NO.	DESCRIPTION	DATE
1	REVISIONS (OR CHANGE NOTICES)	
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25 / RIO BRAVO BLVD.
INTERCHANGE

PERMANENT STRIPING
RIO BRAVO BLVD.

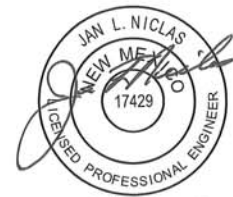


48" WHITE/WHITE TUBULAR FLEXIBLE MARKERS

RIO BRAVO 2051+10	40' RT
RIO BRAVO 2051+30	40' RT
RIO BRAVO 2051+50	40' RT
RIO BRAVO 2051+70	40' RT
RIO BRAVO 2051+90	40' RT
RIO BRAVO 2052+10	40' RT
RIO BRAVO 2052+30	40' RT
RIO BRAVO 2052+50	40' RT
RIO BRAVO 2052+70	40' RT
RIO BRAVO 2052+90	40' RT
RIO BRAVO 2053+10	40' RT
RIO BRAVO 2053+30	39' RT

A3/A1 HAZARD DELINEATORS (AT CULVERT ENDS)

EN LOOP	302+29	53' RT
EN LOOP	302+75	49' RT
RIO BRAVO	2053+10	61' LT
WN ENTR	403+57	10' LT
WN ENTR	403+70	53' RT
WN ENTR	403+99	10' LT



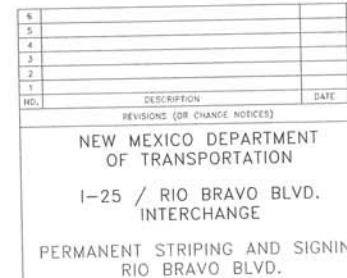
NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		

NEW MEXICO DEPARTMENT OF TRANSPORTATION

1-25 / RIO BRAVO BLVD. INTERCHANGE

PERMANENT STRIPING AND SIGNING RIO BRAVO BLVD.

DESIGNED BY: AECOM
AECOM TECHNICAL SERVICES, INC.
6501 Avenida Parkway NE, Suite 900
Albuquerque, New Mexico 87110
PHONE: (505) 855-7500
FAX: (505) 855-7501
www.aecom.com
AECOM JOB NO. 0303326



NEW MEXICO PROJECT NO. A300280

SHEET NO. 7

AECOM
AECOM TECHNICAL SERVICES, INC.,
6501 Americas Parkway NE, Suite 900
Albuquerque, New Mexico 87110

Design File: c:\pwworking\dms31771\A30D2807PS21_1.dgn
 1/1/2017 4:31:18 PM NVDOT.pdf,PLT,CFG

MATCHLINE EN LOOP RAMP STA. 315+00
SEE SHEET 7-8

MATCHLINE SEE SHEET 7-21

314+00

313+00

312+00

311+00

4W
4L
4Y

310+00

309+00

308+00

307+00

2201
2203
2205
2207

2209
2211
2213

2215
2217
2219

! A31A1 HAZARD DELINEATORS
(AT CULVERT ENDS)

LOOP EN 308+63 20' LT
LOOP EN 307+38 72' LT

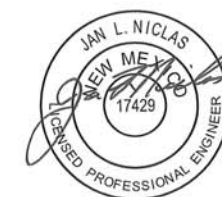
AECOM TECHNICAL SERVICES, INC.
6501 American Parkway NE, Suite 900
Albuquerque, New Mexico 87110

PHONE: (505) 854-7500
FAX: (505) 854-7500
WWW.AECOM.COM

DESIGNED BY: *[Signature]*

www.aecom.com

Design File: c:\pwworking\dms31771\A300280\PS22_1.dgn
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NO.	DESCRIPTION	DATE	BY
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REVISIONS (OR CHANGE NOTICES)

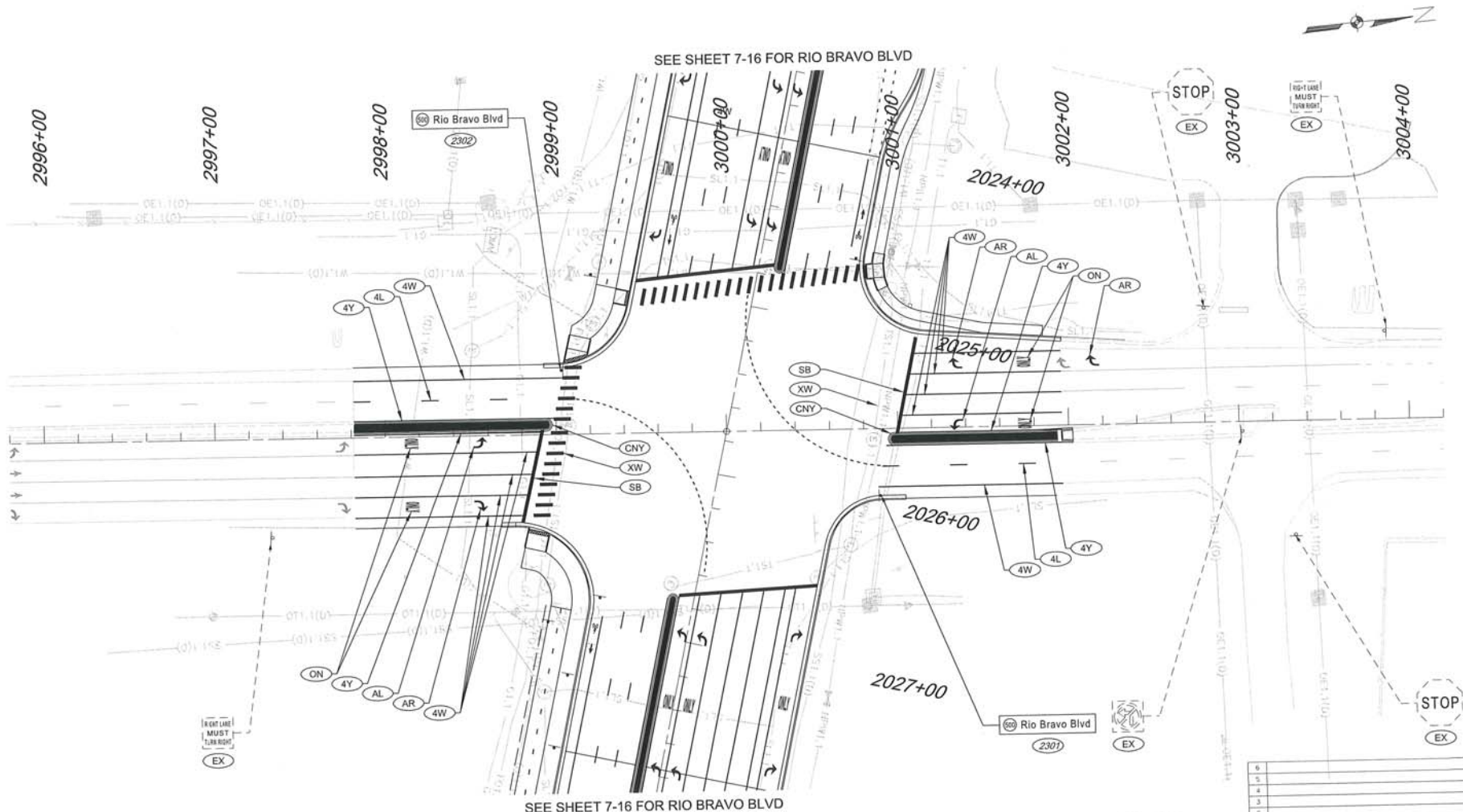
NEW MEXICO DEPARTMENT
OF TRANSPORTATION

I-25 / RIO BRAVO BLVD.
INTERCHANGE

PERMANENT STRIPING AND SIGNING
EN LOOP RAMP

NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 -



SEE SHEET 7-16 FOR RIO BRAVO BLVD



1/10/2017

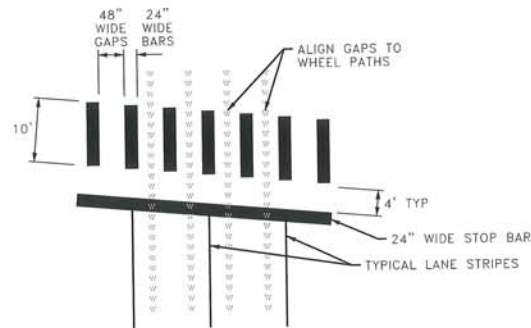
NO.	DESCRIPTION	DATE
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REVISIONS (OR CHANGE NOTICES)

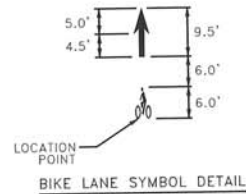
NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25 / RIO BRAVO BLVD. INTERCHANGE

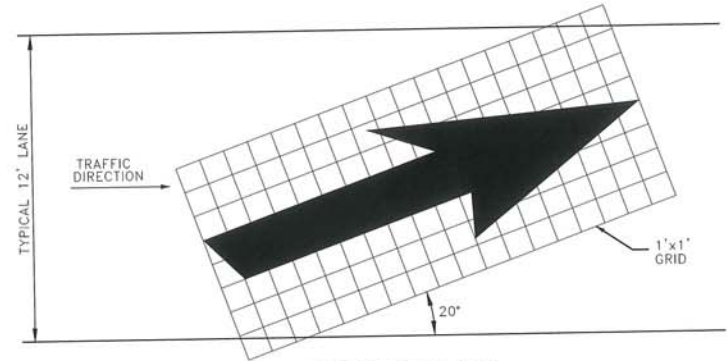
PERMANENT STRIPING AND SIGNING BROADWAY BLVD.



TYPICAL CROSSWALK AND STOP BAR DETAILS



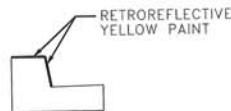
BIKE LANE SYMBOL DETAIL



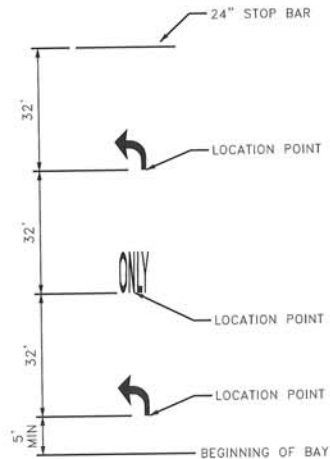
LANE DROP ARROW DETAIL

LANE DROP ARROW NOTES:

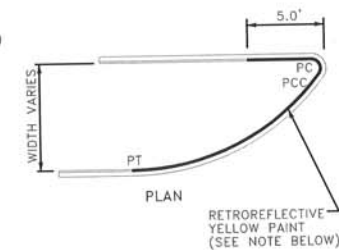
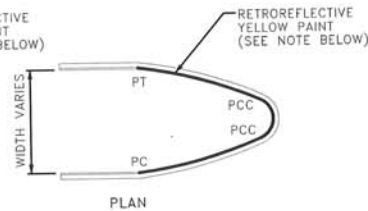
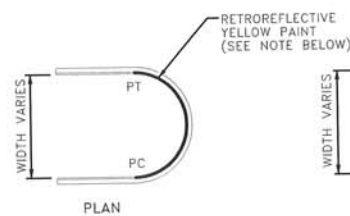
1. CALTRANS 18' TYPE VI ARROW IS SHOWN.
2. FHWA 18' LANE DROP ARROW (SHS PG. 10-9) IS ALSO ACCEPTABLE.
3. USE OF THE SYMMETRIC THROUGH ARROW FOR LANE DROPS WILL NOT BE ACCEPTED.
4. THE POINTY SIDE OF THE ARROWHEAD GOES TOWARD ONCOMING TRAFFIC. ARROWS INSTALLED WITH THE TEMPLATE UPSIDE DOWN OR BACKWARDS SHALL BE REMOVED AND REINSTALLED AT THE CONTRACTOR'S EXPENSE.



PAINTED CURB DETAIL



TYPICAL TURN LANE DETAILS

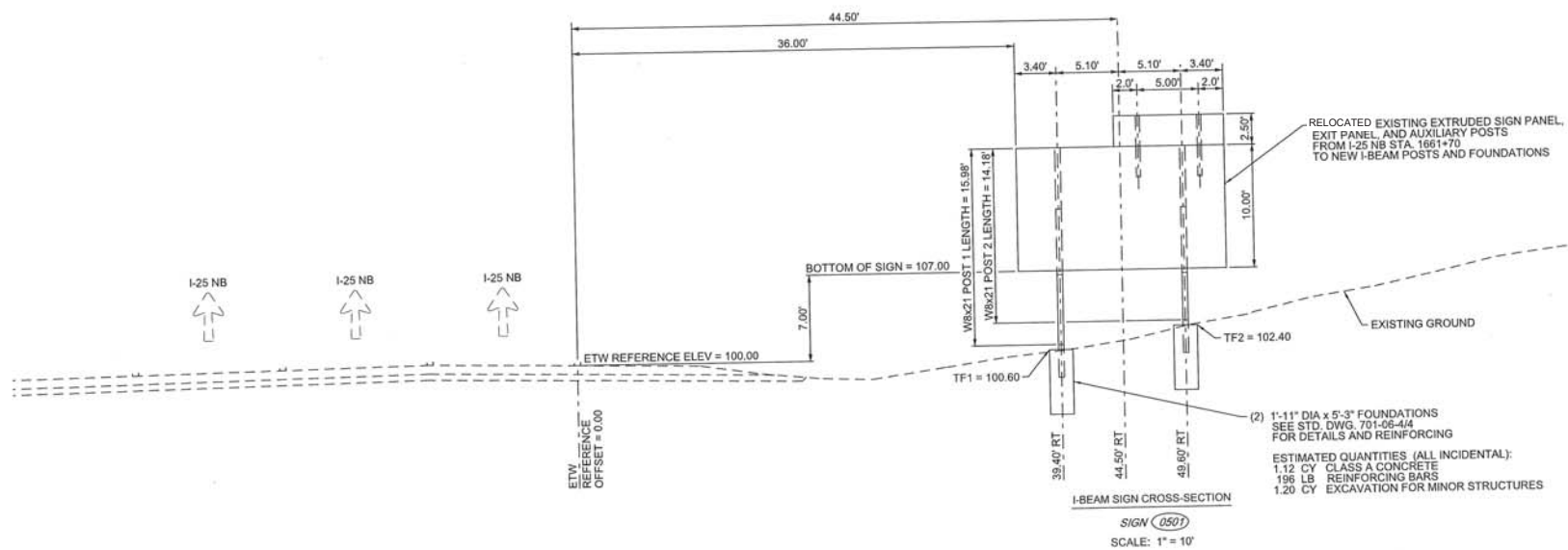


NOTE: MEDIAN NOSE(S) AND ENTIRE REFUGE ISLAND(S) SHALL BE PAINTED WITH RETROREFLECTIVE YELLOW PAINT. THE MEDIAN NOSE SHALL BE PAINTED FROM NOSE MIDPOINT BACK 5 FEET OR TO THE PC, WHICHEVER IS GREATER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO STRIPING & SIGNING.

MEDIAN NOSE & REFUGE ISLAND PAINTING TYPICAL DETAILS



NO.	DESCRIPTION	DATE
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REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25 / RIO BRAVO BLVD. INTERCHANGE		
PERMANENT STRIPING AND SIGNING DETAILS		



DESIGNED BY: AECOM
 AECOM TECHNICAL SERVICES, INC.
 6501 America Parkway NE, Suite 900
 Albuquerque, New Mexico 87110
 PHONE: (505) 955-7500
 EMAIL: jprohman@aecocom.com
 AECOM JOB NO. 03033326



NO.	DESCRIPTION	DATE
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REVISIONS (OR CHANGE NOTICES)

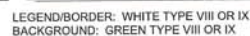
NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25 / RIO BRAVO BLVD. INTERCHANGE

I-BEAM SIGN DETAILS
SIGN 0501



SIGN 1302
SCALE: 1" = 10'



SIGN FACE DETAIL

SIGN 1302

SCALE: 1" = 5'



1/10/2017

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NO.	DESCRIPTION	DATE	

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO DEPARTMENT
OF TRANSPORTATION

1-25 / RIO BRAVO BLVD.
INTERCHANGE

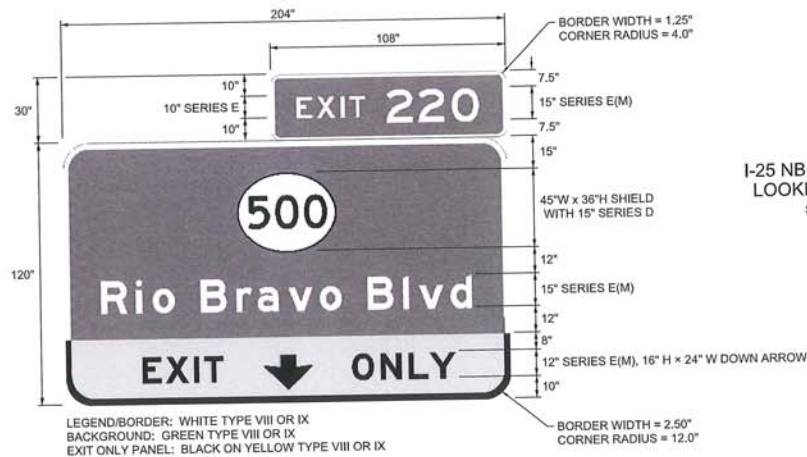
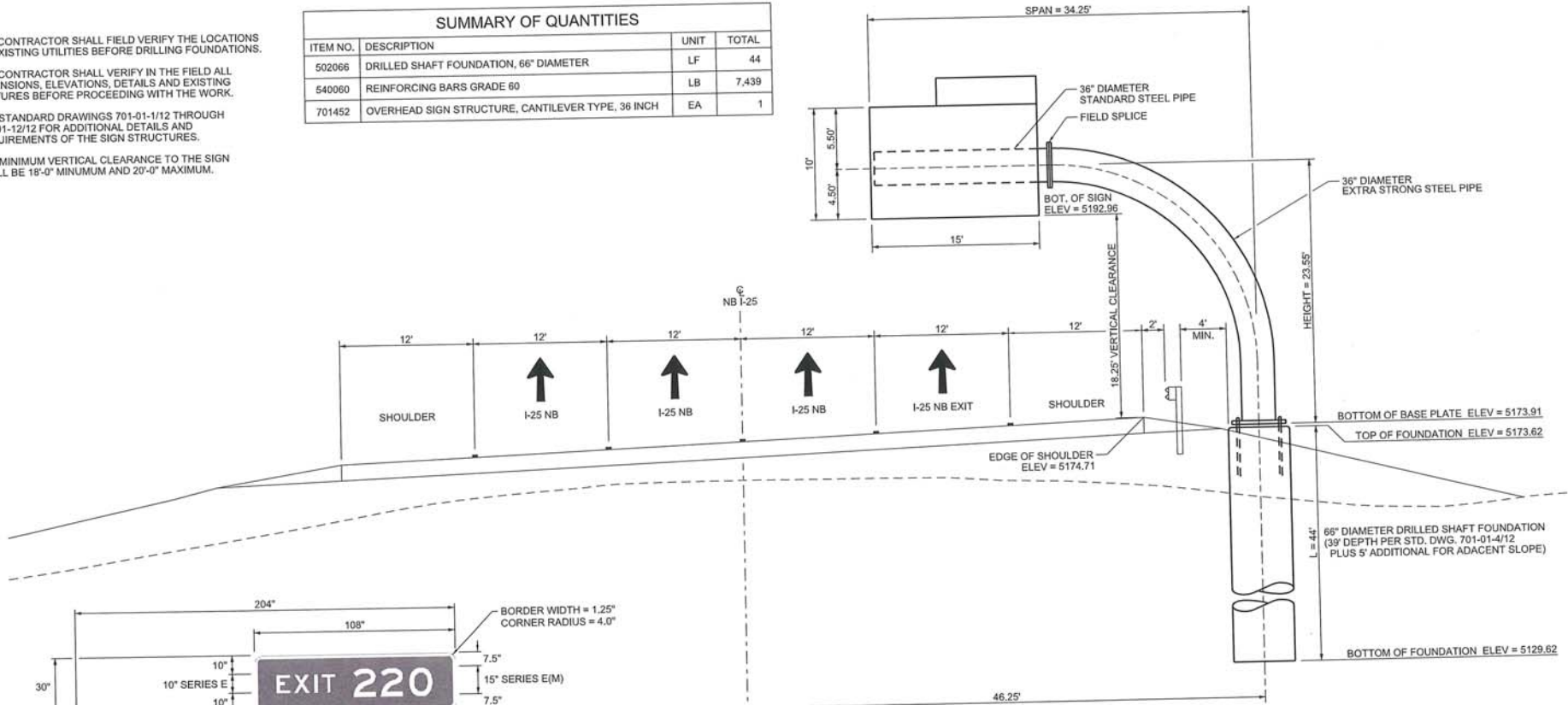
I-BEAM SIGN DETAILS
SIGN 1302

NOTES

1. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF EXISTING UTILITIES BEFORE DRILLING FOUNDATIONS.
2. THE CONTRACTOR SHALL VERIFY IN THE FIELD ALL DIMENSIONS, ELEVATIONS, DETAILS AND EXISTING FEATURES BEFORE PROCEEDING WITH THE WORK.
3. SEE STANDARD DRAWINGS 701-01-1/12 THROUGH 701-01-12/12 FOR ADDITIONAL DETAILS AND REQUIREMENTS OF THE SIGN STRUCTURES.
4. THE MINIMUM VERTICAL CLEARANCE TO THE SIGN SHALL BE 18'-0" MINIMUM AND 20'-0" MAXIMUM.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL
502066	DRILLED SHAFT FOUNDATION, 66" DIAMETER	LF	44
540060	REINFORCING BARS GRADE 60	LB	7,439
701452	OVERHEAD SIGN STRUCTURE, CANTILEVER TYPE, 36 INCH	EA	1



LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX
EXIT ONLY PANEL: BLACK ON YELLOW TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 0703

SCALE: 1" = 5'

I-25 NB STA. 1678+50.00
LOOKING UPSTATION
SCALE: 1" = 10'



NO.	DESCRIPTION	DATE
1	REVISIONS (OR CHANGE NOTICES)	
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25 / RIO BRAVO BLVD. INTERCHANGE
OVERHEAD SIGN DETAILS
CANTILEVER SIGN 0703

DRAWING SCALE: 1" = 50'
NEW MEXICO PROJECT NO. A300280

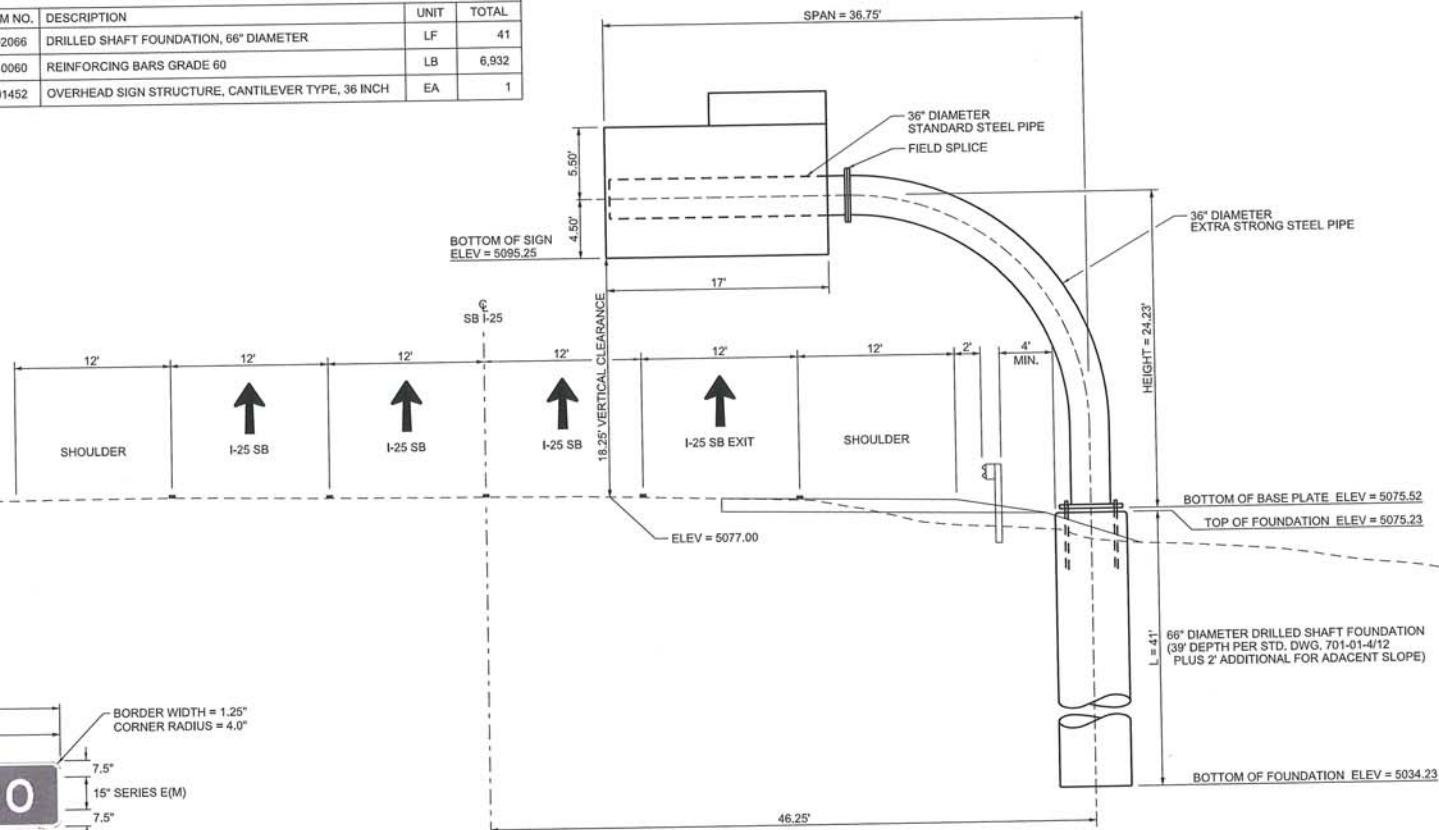
SHEET NO.

NOTES

1. THE CONTRACTOR **SHALL** FIELD VERIFY THE LOCATIONS OF EXISTING UTILITIES BEFORE DRILLING FOUNDATIONS.
2. THE CONTRACTOR **SHALL** VERIFY IN THE FIELD ALL DIMENSIONS, ELEVATIONS, DETAILS AND EXISTING FEATURES BEFORE PROCEEDING WITH THE WORK.
3. SEE STANDARD DRAWINGS 701-01-1/12 THROUGH 701-01-12/12 FOR ADDITIONAL DETAILS AND REQUIREMENTS OF THE SIGN STRUCTURES.
4. THE MINIMUM VERTICAL CLEARANCE TO THE SIGN WAS ~~18'-0"~~ 18'-0" MINIMUM AND 20'-0" MAXIMUM.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL
502066	DRILLED SHAFT FOUNDATION, 66" DIAMETER	LF	41
540060	REINFORCING BARS GRADE 60	LB	6,932
701452	OVERHEAD SIGN STRUCTURE, CANTILEVER TYPE, 36 INCH	EA	1



I-25 SB STA. 1336+50.00
LOOKING BACKSTATION
SCALE: 1" = 10'



NO.	DESCRIPTION	DATE
1	REVISIONS (OR CHANGE NOTICES)	
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NEW MEXICO DEPARTMENT OF TRANSPORTATION
I-25 / RIO BRAVO BLVD. INTERCHANGE
OVERHEAD SIGN DETAILS
CANTILEVER SIGN 1104

DRAWING SCALE: 1" = 50'
NEW MEXICO PROJECT NO. A300280

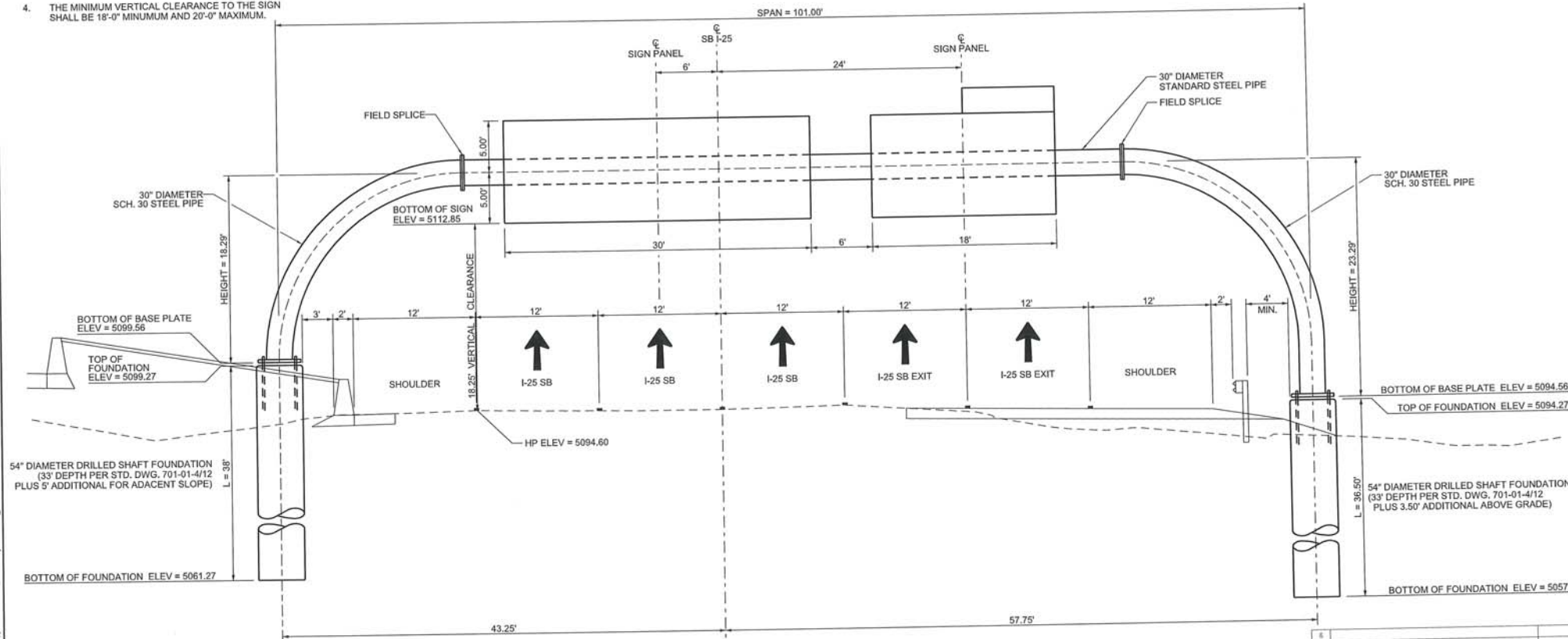
SHEET NO.

NOTES

1. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF EXISTING UTILITIES BEFORE DRILLING FOUNDATIONS.
2. THE CONTRACTOR SHALL VERIFY IN THE FIELD ALL DIMENSIONS, ELEVATIONS, DETAILS AND EXISTING FEATURES BEFORE PROCEEDING WITH THE WORK.
3. SEE STANDARD DRAWINGS 701-01-1/12 THROUGH 701-01-12/12 FOR ADDITIONAL DETAILS AND REQUIREMENTS OF THE SIGN STRUCTURES.
4. THE MINIMUM VERTICAL CLEARANCE TO THE SIGN SHALL BE 18'-0" MINIMUM AND 20'-0" MAXIMUM.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL
502054	DRILLED SHAFT FOUNDATION, 54" DIAMETER	LF	74.5
540060	REINFORCING BARS GRADE 60	LB	8,421
701550	OVERHEAD SIGN STRUCTURE, BRIDGE TYPE, 30 INCH	EA	1



SEE SHEET NO. 7-31 FOR SIGN PANEL DETAILS



NO.	DESCRIPTION	DATE
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NEW MEXICO DEPARTMENT OF TRANSPORTATION

I-25 / RIO BRAVO BLVD. INTERCHANGE

OVERHEAD SIGN DETAILS
SIGN BRIDGE 1004-1006DRAWING SCALE: 1:50
NEW MEXICO PROJECT NO. A300280

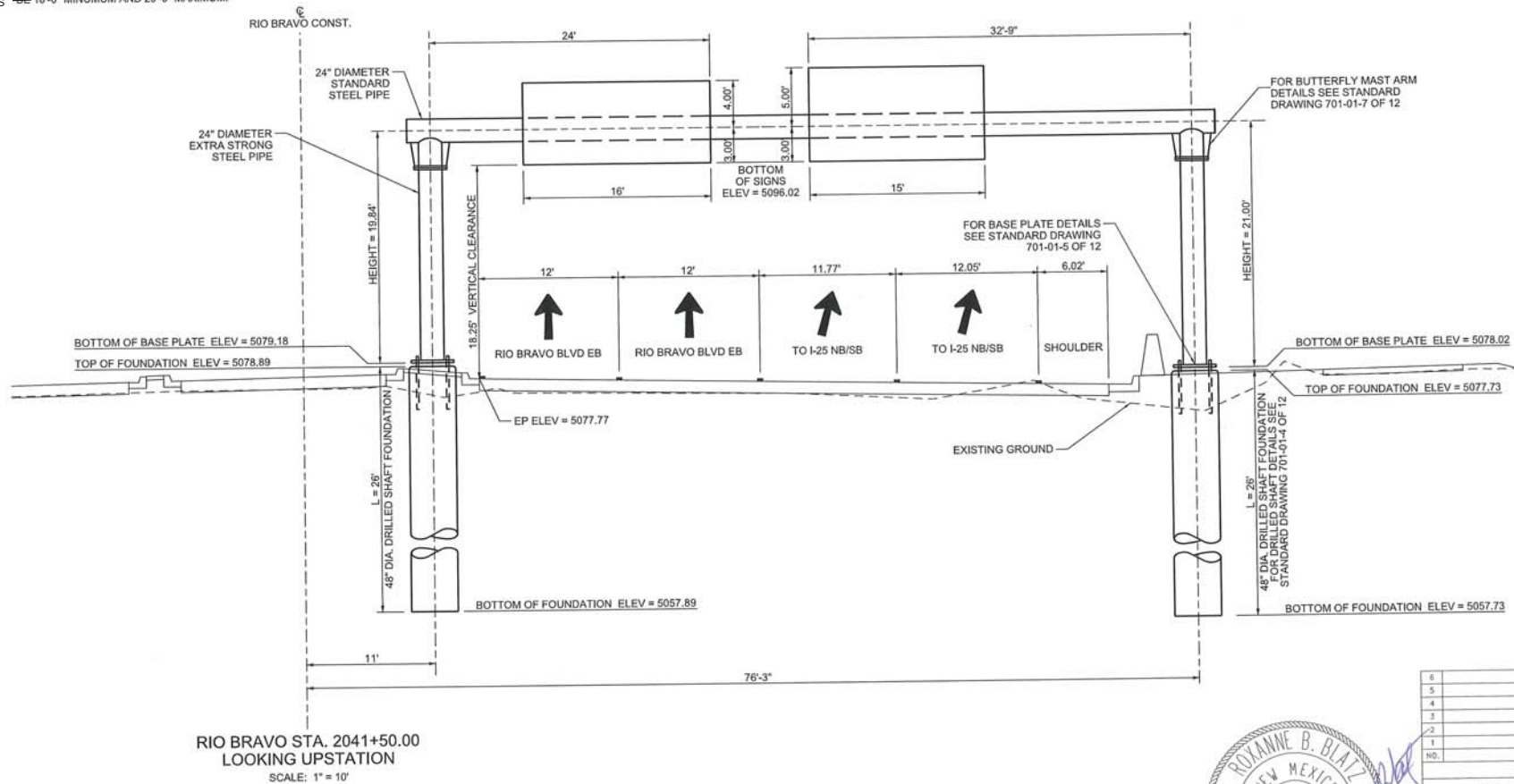
SHEET NO.

NOTES

1. THE CONTRACTOR **SHALL** FIELD VERIFY THE LOCATIONS OF EXISTING UTILITIES BEFORE DRILLING FOUNDATIONS.
2. THE CONTRACTOR **SHALL** VERIFY IN THE FIELD ALL DIMENSIONS, ELEVATIONS, DETAILS AND EXISTING FEATURES BEFORE PROCEEDING WITH THE WORK.
3. SEE STANDARD DRAWINGS 701-01-1/12 THROUGH 701-01-12/12 FOR ANY ADDITIONAL DETAILS AND REQUIREMENTS OF THE SIGN STRUCTURES. PROVIDED MINIMUM 2 1/2" THICK SPLICE PLATES FOR THE FIELD SPLICE AND BUTTERFLY SPLICES SHOWN ON STANDARD DRAWING 701-01-7/12.
4. THE MINIMUM VERTICAL CLEARANCE TO THE SIGN WAS ~~BE~~ 18'-0" MINIMUM AND 20'-0" MAXIMUM.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL
502048	DRILLED SHAFT FOUNDATION, 48" DIAMETER	LF	52
540060	REINFORCING BARS GRADE 60	LB	8,658
701545	OVERHEAD SIGN STRUCTURE, BRIDGE TYPE, 24-INCH	EA	1



NO.	DESCRIPTION	DATE
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REVISIONS (OR CHANGE NOTICE)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION		
I-25 / RIO BRAVO BLVD. INTERCHANGE		
OVERHEAD SIGN DETAILS		
BUTTERFLY SIGNS 1801 & 1803		

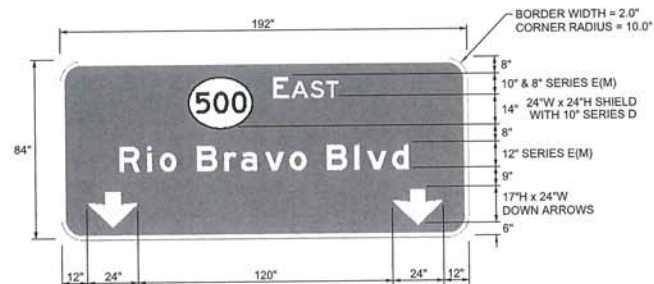


SEE SHEET NO. 7-31 FOR SIGN PANEL DETAILS

DRAWING SCALE: 1" = 50'

NEW MEXICO PROJECT NO. A300280

SHEET NO. 1

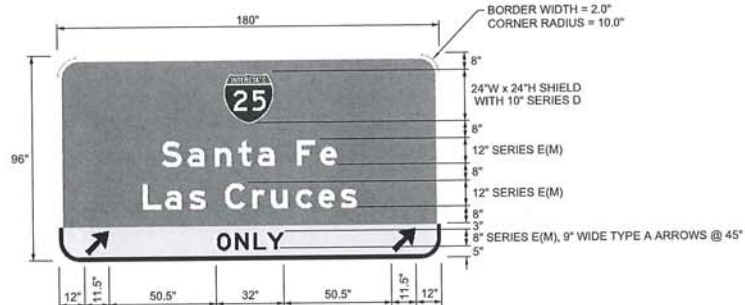


LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 1801

SCALE: 1" = 5'



LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX
EXIT ONLY PANEL: BLACK ON YELLOW TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 1803

SCALE: 1" = 5'

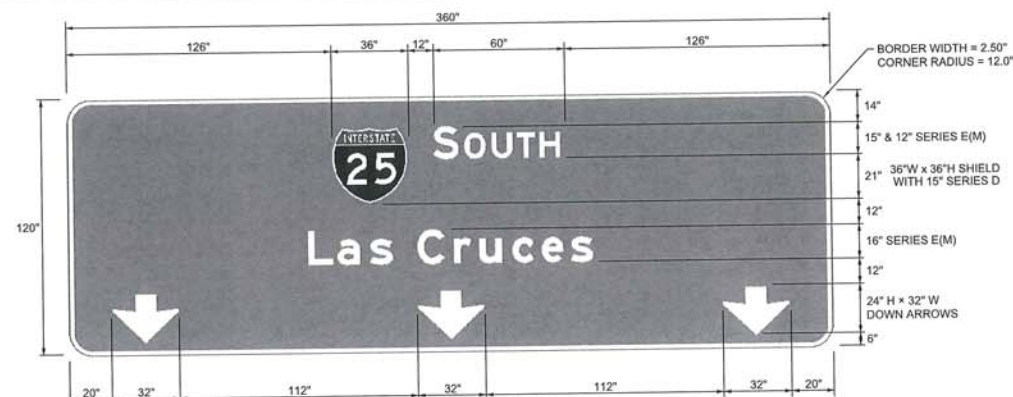


LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 1923

SCALE: 1" = 5'

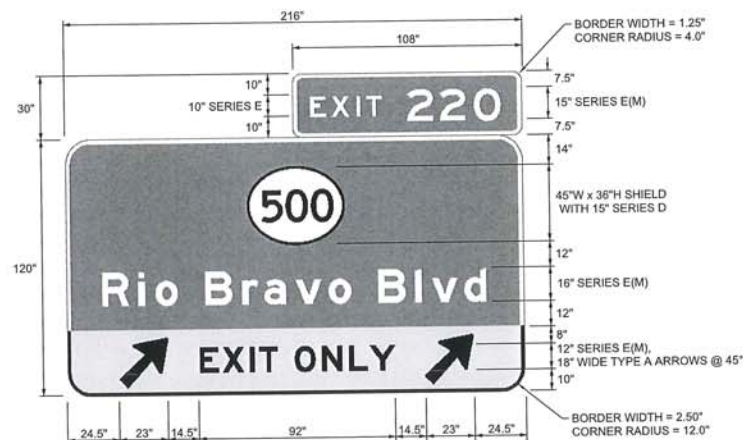


LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 1004

SCALE: 1" = 5'



LEGEND/BORDER: WHITE TYPE VIII OR IX
BACKGROUND: GREEN TYPE VIII OR IX
EXIT ONLY PANEL: BLACK ON YELLOW TYPE VIII OR IX

SIGN FACE DETAIL

SIGN 1006

SCALE: 1" = 5'



NO.	DESCRIPTION	DATE	BY
1	DESIGN		
2	REVISIONS (OR CHANGE NOTICES)		
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

1-25 / RIO BRAVO BLVD.
INTERCHANGE

OVERHEAD SIGN FACE DETAILS
SIGNS 1004 1006 1801 1803 1923

AECOM JOB NO. 6030328

PHONE: (505) 855-7500
FAX: (505) 855-7501
WWW.AECOM.COMDESIGNED BY: AECOM TECHNICAL SERVICES, INC.
6601 American Parkway NE, Suite 600
Albuquerque, New Mexico 87112

Design File: c:\pwworking\dms31771\A3002807P531.dgn
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NEW MEXICO PROJECT NO. A300280

SHEET NO. 7 - 1



Sign 0705;
6" Radius, 1" Border, White on Green;
[EXIT] E; [220] E;
Arrow 13.33UC-12CAP-A - 29° 45';



Sign 0910;
6" Radius, 1" Border, White on Green;
Railrunner Logo;
[RAILRUNNER] E;
[STATION] E;
Arrow 8" Type D4 - 32° 0';



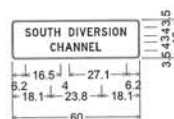
Sign 1002;
6" Radius, 1" Border, White on Green;
[EXIT] E; [220] E;
Arrow 13.33UC-12CAP-A - 29° 45';
6" Radius, 1" Border, Black on Yellow;
[45 MPH] E;



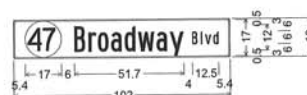
Sign 1202; 12.00" Radius, 2.00" Border, White on Green;
47 Shield; [Broadway Blvd] E; [6] E; [Los Lunas] E; [18] E; [Las Cruces] E; [218] E;



Sign 1304;
0" Radius, 0" Border, White on Green;
[1/4] E;



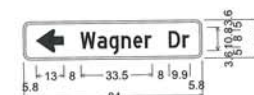
Sign 1501; 1508;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH DIVERSION] D;
[CHANNEL] D;



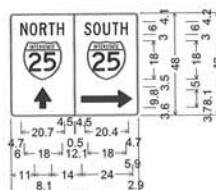
Sign 1602; 1603;
1.5" Radius, 0.5" Border, White on Green;
47 Shield;
[Broadway] B 50% spacing;
[Blvd] B;



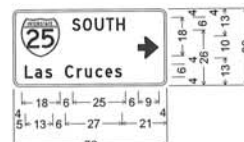
Signs 1702; 1704;
3.0" Radius, 1.0" Border, White on Green;
[JCT] D; Shield;
[Broadway] D;
[Blvd] D;



Sign 1802;
3.0" Radius, 1.0" Border, White on Green;
Arrow 8" Type D1 - 13.0° 180°;
[Wagner Dr] D 50% spacing;



Sign 1805;
1.5" Radius, 0.5" Border, White on Green;
[NORTH] C 95% spacing;
Arrow 6" Type D1 - 9.8° 90°;
[SOUTH] C 95% spacing;
Arrow 6" Type D4 - 24.0° 0°;



Sign 1903;
3" Radius, 1" Border, White on Green;
[SOUTH] D;
[Las Cruces] D;
Arrow 6" Type D3 - 10° 0°;



Sign 1904;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH] C 95% spacing;
Arrow 6" Type D1 - 9.8° 90°;



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
2			
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NEW MEXICO DEPARTMENT
OF TRANSPORTATION

1-25 / RIO BRAVO BLVD.
INTERCHANGE

SIGN FACE DETAILS



Sign 1905;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH] C 95% spacing;
Arrow 6" Type D2 - 13.5" 45°;



Sign 1920;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH] C 95% spacing;
Arrow 6" Type D4 - 24.0" 180°;



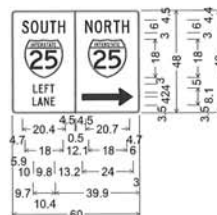
Sign 1925;
6" Radius, 1" Border, White on Green;
[Rio] E 50% spacing;
[Bravo] E 50% spacing;
[Blvd] E 50% spacing;
90 Deg Advance Turn Arrow Custom 16" X 25°;
[NORTH] E: Arrow 8" Type D2 - 18" 330°;



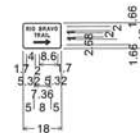
Sign 2003;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH] C 95% spacing;
90 Deg Advance Turn Arrow 12.0" X 12.0°;



Sign 2004;
1.5" Radius, 0.5" Border, White on Green;
[NORTH] C 95% spacing;
Arrow 6" Type D2 - 13.5" 45°;



Sign 2104;
1.5" Radius, 0.5" Border, White on Green;
[SOUTH] C 95% spacing;
[LEFT] C; [LANE] C;
[NORTH] C 95% spacing;
Arrow 6" Type D4 - 24.0" 0°;



Sign 2110;
1.00" Radius, 0.25" Border, White on Green;
[RIO BRAVO] D;
[TRAIL] D;
Arrow 2" Type D4 - 8.00" 0°;



Signs 2301, 2302;
1.5" Radius, 0.5" Border, White on Green;
47 Shield; [Rio] B 50% spacing;
[Bravo] B 50% spacing; [Blvd] B;



NO.	DESCRIPTION	DATE	BY
1	REVISIONS (OR CHANGE NOTICES)		
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