

RECORD DRAWING

PUBLIC IMPROVEMENT PLANS FOR TERIYAKI CHICKEN BOWL 6101 SAN MATEO BLVD. N.E. ALBUQUERQUE, NEW MEXICO

FEBRUARY 2001

PREPARED FOR:
TWIN DRAGONS, INC.
2110 EUBANK BLVD. N.E.
ALBUQUERQUE, N.M. 87112

PREPARED BY:
ISAACSON & ARFMAN, P.A.
128 MONROE ST. NE
ALBUQUERQUE, NM 87108

INDEX TO DRAWINGS

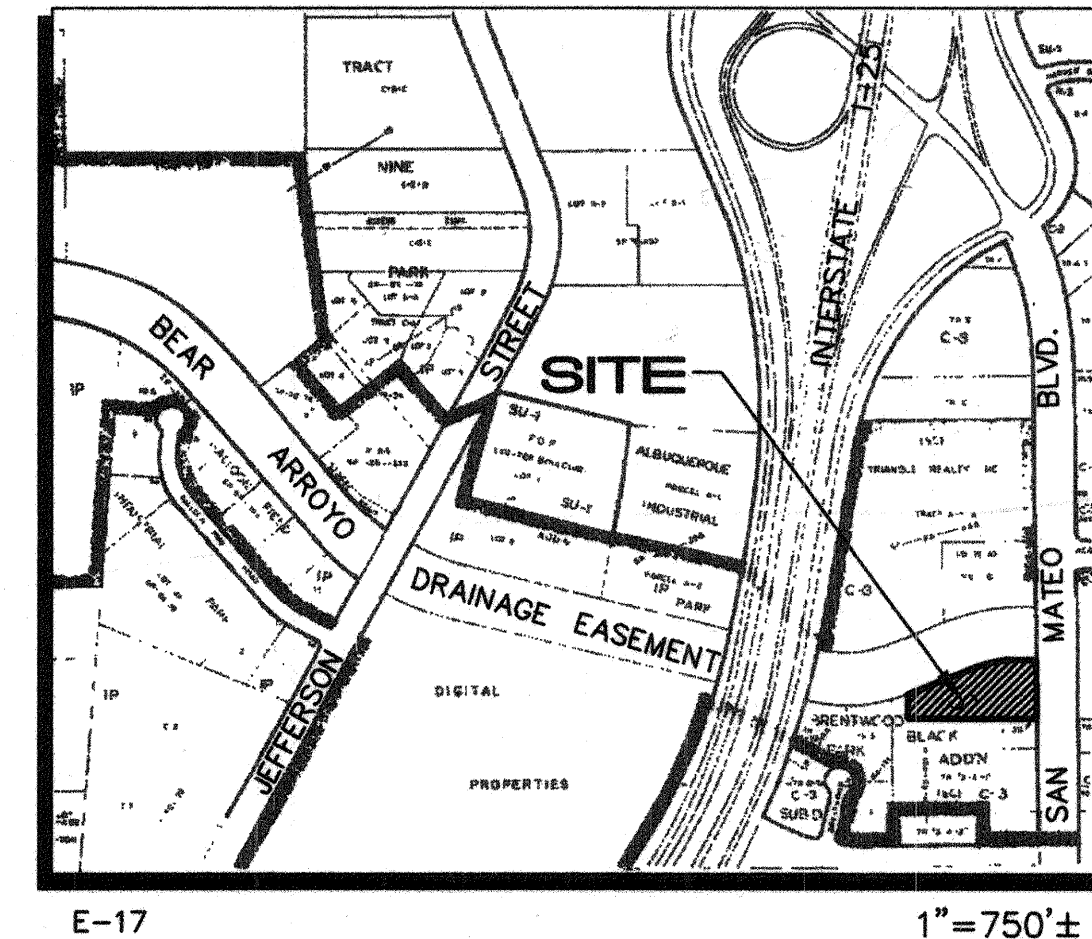
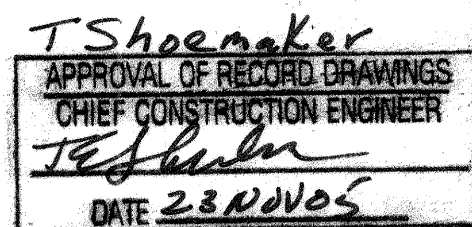
SHEET NO.	TITLE
1	COVER SHEET
2	SITE PLAN (BY SEPARATE CONTRACT)
3	SAN MATEO BOULEVARD PAVING PLAN, SECTIONS & DETAILS
4	SAN MATEO BOULEVARD UTILITY PLAN, SECTIONS & DETAILS
5	SIGNING & CONSTRUCTION TRAFFIC CONTROL STANDARDS
6	TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES

SURVEYOR'S CERTIFICATION

I, Charles G. Cala, Jr., a duly qualified Registered Professional Land Surveyor under the laws of the State of New Mexico, do hereby certify that the "as-built" information shown on these drawings was obtained from field construction and "as-built" surveys performed by me or under my supervision, that the "as-built" information shown on these drawings was added by me or under my supervision, and that this "as-built" information is true and correct to the best of my knowledge and belief and that I am not responsible for any of the design concepts, calculations, engineering, or intent of the record drawings.



Charles G. Cala, Jr.
Charles G. Cala, Jr., NMPS #1184
10-09-2001
Date



VICINITY MAP

GENERAL NOTES

- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 1986 EDITION AS AMENDED WITH UPDATE NO. 6.
- ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, RULES, AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE & VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH MINIMUM DELAY.
- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT LINE LOCATING SERVICE @ 280-1990 FOR LOCATION OF EXISTING UTILITIES.
- CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN A MANNER WHICH WILL MINIMIZE INTERFERENCE WITH LOCAL TRAFFIC. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND ORDERS OF ANY PUBLIC BODY HAVING JURISDICTION FOR THE SAFETY OF PERSONS OR PROPERTY, AND TO PROTECT THEM FROM DAMAGE, INJURY, OR LOSS. CONTRACTOR SHALL ERECT AND MAINTAIN, AS REQUIRED BY THE CONDITIONS AND PROGRESS OF THE WORK, ALL NECESSARY SAFEGUARDS FOR SAFETY CONTINUOUSLY AND NOT LIMITED TO NORMAL WORKING HOURS, THROUGHOUT THE DURATION OF THE PROJECT. CONTRACTOR SHALL ADHERE TO SECTION 19 OF THE GENERAL CONDITIONS OF THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986, AS AMENDED WITH UPDATE NO. 6.
- THE CONTRACTOR AGREES THAT HE SHALL ASSUME THE SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD HARMLESS THE OWNER & ENGINEER FROM ANY AND ALL LIABILITY REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.
- TRAFFIC CONTROL: FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONSTRUCTION COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (788-2551) PRIOR TO OCCUPYING AN INTERSECTION. SEE SECTION 19 OF THE SPECIFICATIONS. ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED WITH PLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR TO LOCATION AS EXISTING OR AS SHOWN IN THIS PLAN SET.
- WHEN ABUTTING EXISTING PAVEMENT TO NEW, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER, REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED.
- EXISTING CURB AND GUTTER NOT TO BE REMOVED UNDER THE CONTRACT WHICH IS DAMAGED OR DISPLACED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
- ALL FINAL BACKFILL FOR TRENCHES SHALL BE COMPACTED TO A MINIMUM 90% MAXIMUM DENSITY PER ASTM D-1557 AND AS DIRECTED BY SECTION 701.14.2 AND STANDARD DRAWING NUMBER 2315.
- THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY OR PRIVATE ROADWAY EASEMENTS SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET OR INTO ANY PUBLIC DRAINAGE FACILITY.
- PROPOSED WATERLINE MATERIALS SHALL BE EITHER PVC PIPE MEETING AWWA C900 REQUIREMENTS (6"-12") OR DUCTILE IRON PIPE, THICKNESS CLASS 50 (6"-16").
- ALL SANITARY SEWER LINE STATIONING REFERS TO SANITARY SEWER CENTERLINE STATIONING.
- ALL FITTINGS ON WATERLINES SHALL HAVE RESTRAINED JOINTS AS NOTED ON THE PLANS.
- CONTRACTOR SHALL SUPPORT ALL EXISTING, UNDERGROUND UTILITY LINES WHICH BECOME EXPOSED DURING CONSTRUCTION. PAYMENT FOR SUPPORTING WORK SHALL BE INCIDENTAL TO WATER-LINE AND/OR SEWERLINE COSTS.
- CONTRACTOR SHALL ASSIST THE ENGINEER/INSPECTOR IN THE RECORDING OF DATA ON ALL UTILITY LINES AND ACCESSORIES AS REQUIRED BY THE CITY OF ALBUQUERQUE FOR THE PREPARATION OF RECORD DRAWINGS. CONTRACTOR SHALL NOT COVER UTILITY LINES AND ACCESSORIES UNTIL ALL DATA HAS BEEN RECORDED.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE ENGINEER AND SHALL NOTIFY THE ENGINEER AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
- PNM WILL PROVIDE AT NO COST TO THE CITY OR THE CONTRACTOR THE REQUIRED PERSONNEL FOR INSPECTION OR OBSERVATION DURING CONSTRUCTION. THE CONTRACTOR SHALL BE CHARGED THE TOTAL COST ASSOCIATED WITH REPAIRS TO ANY DAMAGED CABLES OR FOR ANY COST ASSOCIATED WITH SUPPORTING OR RELOCATING THE POLES AND CABLES DURING CONSTRUCTION.
- WARNING--EXISTING UTILITY LINE LOCATIONS ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. THE LOCATION OF ANY SUCH EXISTING LINES IS BASED UPON INFORMATION PROVIDED BY THE UTILITY COMPANY, THE OWNER, OR BY OTHERS, AND THE INFORMATION MAY BE INCOMPLETE OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES.

THE ENGINEER HAS UNDERTAKEN NO FIELD VERIFICATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UNDERGROUND UTILITY LINES, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. THE CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY, AND PRESERVE ANY AND ALL EXISTING UTILITIES. THE CONTRACTOR SHALL COMPLY WITH STATE STATUTES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND FACILITIES, IN PLANNING AND CONDUCTING EXCAVATION, WHETHER BY CALLING OR NOTIFYING THE UTILITIES, COMPLYING WITH "BLUE STAKES" PROCEDURES, OR OTHERWISE.

- ANY WORK OCCURRING WITHIN AN ARTERIAL ROADWAY REQUIRES 24 HR. CONSTRUCTION.
- ALL EXCAVATION, TRENCHING AND SHORING ACTIVITIES MUST BE ACCOMPLISHED IN ACCORDANCE WITH OSHA 29CFR 1926.650 SUBPART P.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15
26 65898.01 05

PROJECT NO.

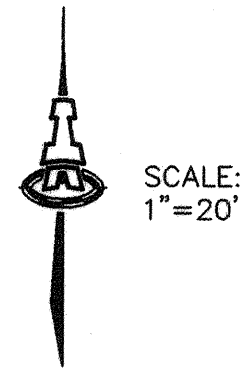
ISAACSON & ARFMAN, P.A.
Consulting Engineering Associates
128 Monroe Street N.E.
Albuquerque New Mexico
125CVR.DWG 05/02/01

REV.	1,2,3,4	CITY ENGINEER	DATE	10-07-2001	DATE	10-07-2001
ENGINEERS STAMP & SIGNATURE						<p>APPROVED FOR CONSTRUCTION</p> <p>City Engineer</p> <p>Date</p>
APPROVALS	<p>DRC Chairman: <i>[Signature]</i> 5-17-01</p> <p>Transportation: <i>[Signature]</i> 5-17-01</p> <p>Water/Wastewater: <i>[Signature]</i> 5/17/01</p> <p>Hydrology: <i>[Signature]</i> 5/17/01</p> <p>AMAFCA: <i>[Signature]</i> 5/17/01</p> <p>Constr. Mngmt. <i>[Signature]</i> 5-17-01</p> <p>Constr. Coord. <i>[Signature]</i> 5-17-01</p>					
City Project No.	658981					Sheet 1 of 6

JMA # 201.045.1

LEGEND

- 5200 — EXISTING CONTOUR
- 52 — PROPOSED CONTOUR
- ◆ 78.3 PROPOSED SPOT ELEVATION
- FLOW ARROW
- FF=6881.0 FINISH FLOOR ELEVATION
- S — SIDEWALK CULVERT
- 4.20 — TOP OF CURB ELEVATION
- 3.70 — FLOWLINE
- PROPOSED RETAINING WALL
- EROSION CONTROL BERM



BEAR ARROYO DRAINAGE EASEMENT

Δ = 21'00"48"
R = 425.00'
L = 155.87' (156.27')
CH = N79°41'17"E
155.00'

Found rebar
3.8'± East
(disregard)

Set capped
rebar "7002"

SURVEYOR'S CERTIFICATION

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Charles G. Cala, Jr. 10-09-2001
Charles G. Cala, Jr., NMPS 11184 Date



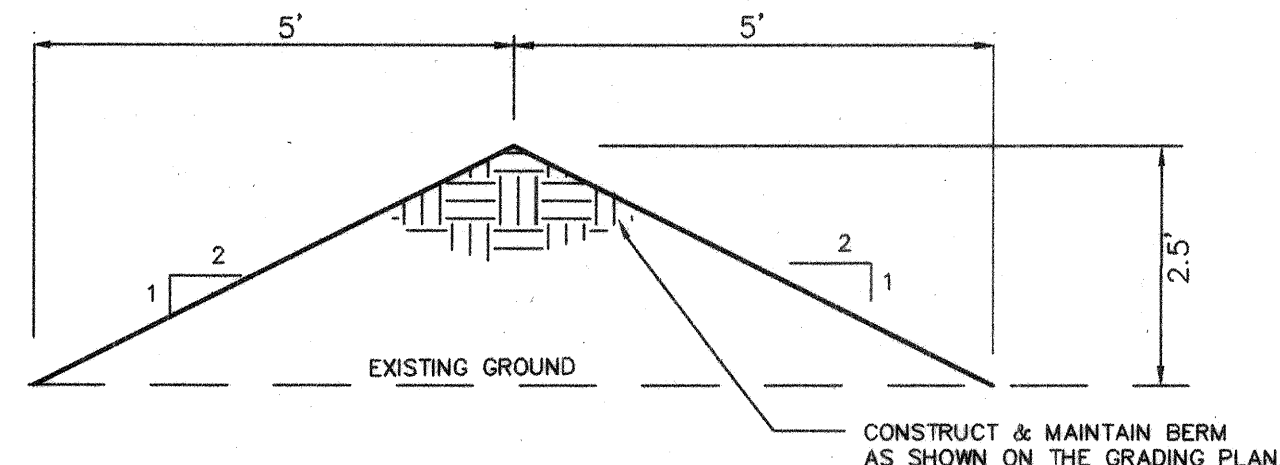
TRACT 1
88,981 sq.ft.
2.0427 acres

**TRACT 3-A-1
BLACK ADDITION**
Filed 8/11/86, Vol. C31, folio 60

**TRACT 3-B
BLACK ADDITION**
Filed 5/24/79, Vol. B16, folio 117

GRADING NOTES:

- ALL TRASH, DEBRIS, & SURFACE VEGETATION SHALL BE CLEARED AND LEGALLY DISPOSED OFFSITE.
- ALL SUBGRADE AND FILL SHALL BE COMPACTED TO A MINIMUM OF 90% ASTM D-1557.
- EXCAVATION IS UNCLASSIFIED AND INCLUDES EXCAVATION TO SUBGRADE ELEVATIONS INDICATED, REGARDLESS OF CHARACTER OF MATERIALS ENCOUNTERED.
- CONFORM TO ELEVATIONS AND DIMENSIONS SHOWN ON PLANS WITHIN A TOLERANCE OF 0.3± FEET.
- SCARIFY AND COMPACT SUBGRADE FOR FILLS. PLACE FILL MATERIALS IN LAYERS NOT MORE THAN 8" IN LOOSE DEPTH. MOISTEN AS NECESSARY TO PROVIDE OPTIMUM MOISTURE (±2%) CONTENT.
- UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING AS SHOWN ON PLAN. SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCE, COMPACT WITH UNIFORM SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE INDICATED.
- MAXIMUM SLOPES SHALL BE 3:1 MINIMUM SLOPES SHALL BE 1%.
- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM, 260-1990, FOR LOCATION OF EXISTING UTILITIES.
- IF ANY UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES ARE SHOWN ON THESE DRAWINGS, THEY ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. IF ANY SUCH EXISTING LINES ARE SHOWN, THE LOCATION IS BASED ON INFORMATION PROVIDED BY THE OWNER OF SAID UTILITY, AND THE INFORMATION MAY BE INCOMPLETE, OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES. THE ENGINEER HAS CONDUCTED ONLY PRELIMINARY INVESTIGATION OF THE LOCATION, DEPTH, SIZE OR TYPE OF EXISTING UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES. THESE INVESTIGATIONS IS NOT CONCLUSIVE, AND MAY NOT BE COMPLETE. THEREFORE, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. THE CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. IN PLANNING AND CONDUCTING EXCAVATION, THE CONTRACTOR SHALL COMPLY WITH STATE STATUTES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES.
- THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY SO THAT THE EXCAVATED MATERIAL IS NOT ERODED AND WASHED DOWN THE STREET.
- OWNER WILL PROVIDE SOIL TESTING AND INSPECTION SERVICES DURING EARTHWORK OPERATIONS. ALLOW TESTING SERVICE TO INSPECT AND APPROVE COMPACTED SUBGRADES AND FILL LAYERS BEFORE FURTHER CONSTRUCTION WORK IS DONE. SHALL COMPACTION TESTS INDICATE INADEQUATE DENSITY, CONTRACTOR SHALL PROVIDE ADDITIONAL COMPACTION AND TESTING AT NO ADDITIONAL EXPENSE.
- OWNER HAS ESTABLISHED SUBDIVISION BOUNDARY CORNERS. CONTRACTOR SHALL PROVIDE ALL OTHER CONSTRUCTION STAKING INCLUDING TRACT CORNERS. CONTRACTOR SHALL LOCATE AND PRESERVE ALL BOUNDARY CORNERS AND REPLACE ANY LOST OR DISTURBED CORNERS.



EROSION CONTROL BERM

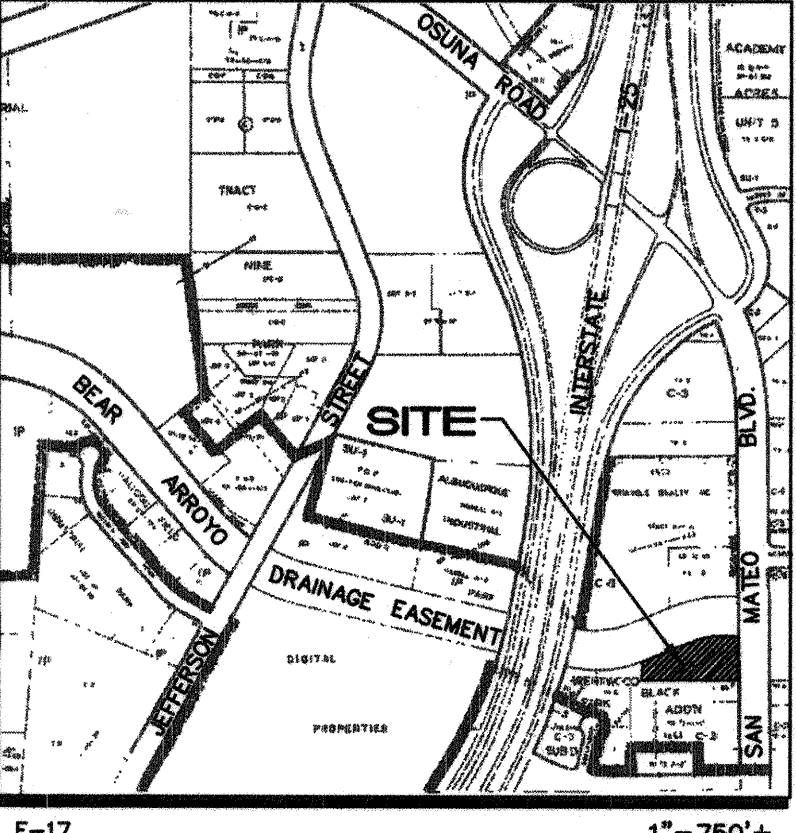
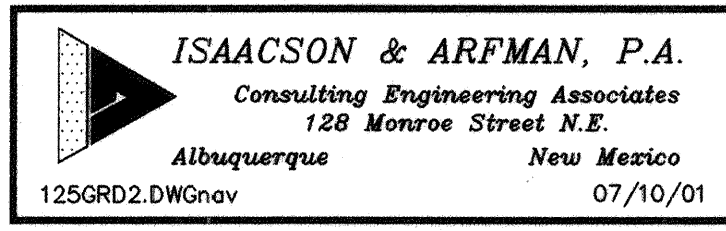
SCALE: 1"=2'

EROSION CONTROL

- THE CONTRACTOR SHALL ENSURE THAT NO SOIL ERODES FROM THE SITE INTO THE PUBLIC RIGHT-OF-WAY OR ONTO PRIVATE PROPERTY. THIS CAN BE ACHIEVED BY CONSTRUCTING EROSION CONTROL BERMS (AS DETAILED AT RIGHT) AS SHOWN ON THE PLAN AND WETTING THE SOIL TO KEEP IT FROM BLOWING.
- THE CONTRACTOR SHALL SECURE "TOPSOIL DISTURBANCE PERMIT" FROM THE CITY ENVIRONMENTAL HEALTH DEPARTMENT PRIOR TO BEGINNING CONSTRUCTION. AN EXCAVATION PERMIT IS REQUIRED FOR ALL WORK WITHIN PUBLIC RIGHT-OF-WAY.

KEYED CONSTRUCTION NOTES

- REMOVE EXISTING PARKING SPACE STRIPING.
- SAW-CUT & REMOVE EASTERLY 4 FT. OF PCC PATIO AND REPLACE WITH ASPHALT PAVING PER ARCHITECTURAL SITE PLAN.
- SAW-CUT & REMOVE EXISTING PCC PATIO. CONSTRUCT PCC WHEELCHAIR RAMP PER ARCHITECTURAL SITE PLAN.
- CONSTRUCTION PLANS FOR BOTH ENTRANCES AND DECELERATION LANE ARE FOUND IN THE PUBLIC WORK ORDER DRAWINGS.
- MATCH EXISTING ASPHALT GRADES TO PROVIDE FOR CONTINUOUS DRAINAGE.



VICINITY MAP

LEGAL DESCRIPTION:
TRACT NUMBERED ONE (1) OF THE BLACK ADDITION OF ALBUQUERQUE, NEW MEXICO, AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT OF SAID ADDITION, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO ON JANUARY 15, 1974.

PROJECT AREA:
2.0427 ACRES

BENCHMARK:
NMSHC BRASS TABLET STAMPED "NM-367-1", LOCATED IN THE MEDIAN ON SAN MATEO BLVD JUST NORTH OF OSUNA RD. ELEVATION=5210.60

FLOOD ZONE DESIGNATION:
FEMA PANEL NO. 139 INDICATES THAT THE SITE IS NOT WITHIN ZONE AO OF THE 100-YR FLOOD PLAIN.

EXISTING CONDITIONS:
THE SITE HAD BEEN PARTIALLY DEVELOPED AS A FAST FOOD RESTAURANT CONSISTING OF A SINGLE BUILDING, ASPHALT PARKING AND LANDSCAPING ALONG THE FRONTAGE. ALL CONSTRUCTED ON IMPORTED FILL MATERIAL. THE SITE HAS AN EAST TO WEST SLOPE OF APPROXIMATELY 1%-2% WITH 17% SIDE-SLOPE TRANSITIONS BETWEEN THE DEVELOPED AND UNDEVELOPED AREAS.

EXISTING HYDROLOGY:
PRECIPITATION ZONE: 2
LAND TREATMENTS:
TYPE C = 73.6% = 1.5034 AC
TYPE D = 26.4% = 0.5393 AC
Q₁₀₀ = (3.14)(1.5034) + (4.70)(0.5393)
Q₁₀₀ = 7.26 CFS

PROPOSED CONDITIONS:
THIS SITE IS PROPOSED TO BE CONTINUED AS A COMMERCIAL TRACT WITH ASSOCIATED PARKING. ALL DEVELOPED RUNOFF ON THE SITE WILL BE CONVEYED TO THE BEAR ARROYO ADJACENT TO THE SITE ALONG THE NORTHWEST PROPERTY LINE.

PROPOSED HYDROLOGY:
PRECIPITATION ZONE: 2
LAND TREATMENTS:
TYPE C = 55.4% = 1.1317 AC
TYPE D = 44.6% = 0.9110 AC
Q₁₀₀ = (3.14)(1.1317) + (4.70)(0.9110)
Q₁₀₀ = 7.84 CFS

ALL RUNOFF WILL BE CONVEYED VIA HISTORICAL DRAINAGE PATTERNS TO THE BEAR ARROYO. THE BEAR ARROYO HAS ADEQUATE CAPACITY TO ACCEPT THE ADDITIONAL 0.6 CFS OF STORM WATER FLOW.

RECORD DRAWING

10-2001 AS-BUILT & CERTIFY C.G.C. # 2001-049.1

revisions	job title TERIYAKI CHICKEN BOWL 6100 BLOCK SAN MATEO NE ALBUQUERQUE, NM	job no 20007	date 4/3/2000
	PROJECT MANAGER ALICEN STROMBECK	by	
	sheets title GRADING & DRAINAGE PLAN		

de la torre architects, p.a.
2400 louisiana blvd ne
building 3 suite 110
albuquerque nm 87110 / 505-883-7918

sheet-
2
of-

RECORD DRAWING

FOR GRADES WITHIN PROPERTY, SEE
TERIYAKI CHICKEN BOWL GRADING PLAN.

TC	FL
1 7.26	7.36
2 7.26	7.23
3 7.26	7.09

SCALE:
1"=20'

TC	FL
1 7.45	6.82
2 7.45	7.03
3 7.45	7.24

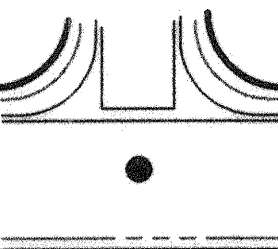
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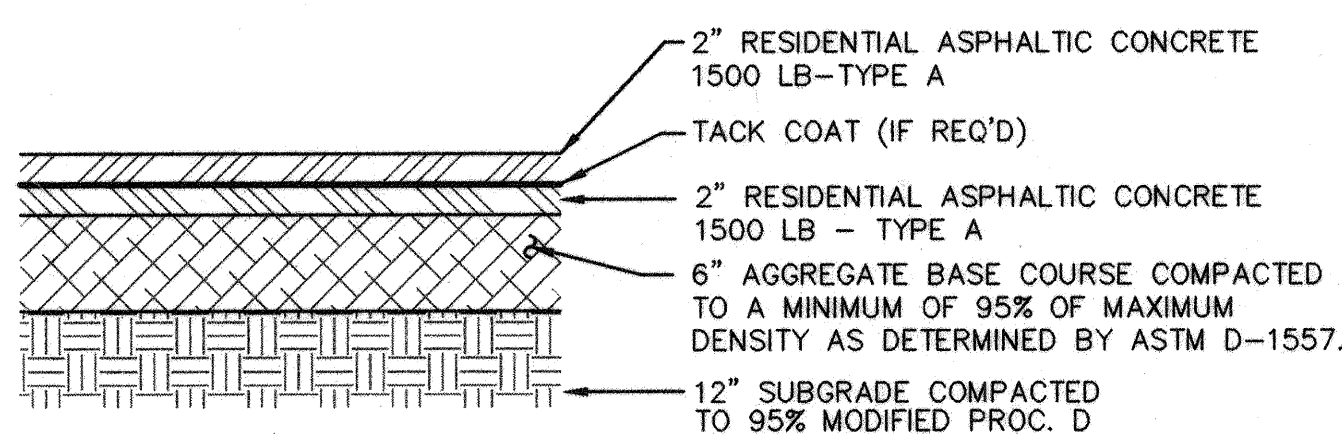
Charles G. Cala, Jr. 10-09-2001
Charles G. Cala, Jr., NMP 11184 Date

LEGEND

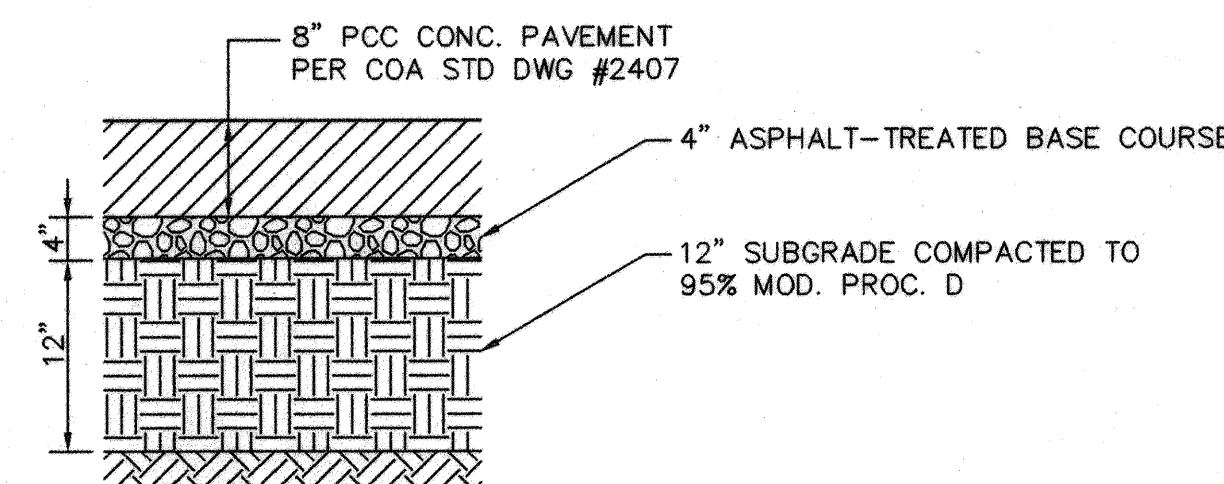
- EXISTING STREET IMPROVEMENTS
- PROPOSED STREET IMPROVEMENTS
- FUTURE STREET IMPROVEMENTS
- GUTTER LINE ELEV. (BASE OF CURB)
- VALLEY GUTTER
- STORM DRAIN MANHOLE
- SIDEWALK BY OTHERS
- SINGLE WATER METER & BOX
- DOUBLE WATER METER & BOX
- EXISTING WATER METER
- FIRE HYDRANT
- STORM DRAIN CATCH BASIN
- STORM DRAIN MANHOLE
- EXISTING SAS MANHOLE
- PROPOSED SAS MANHOLE



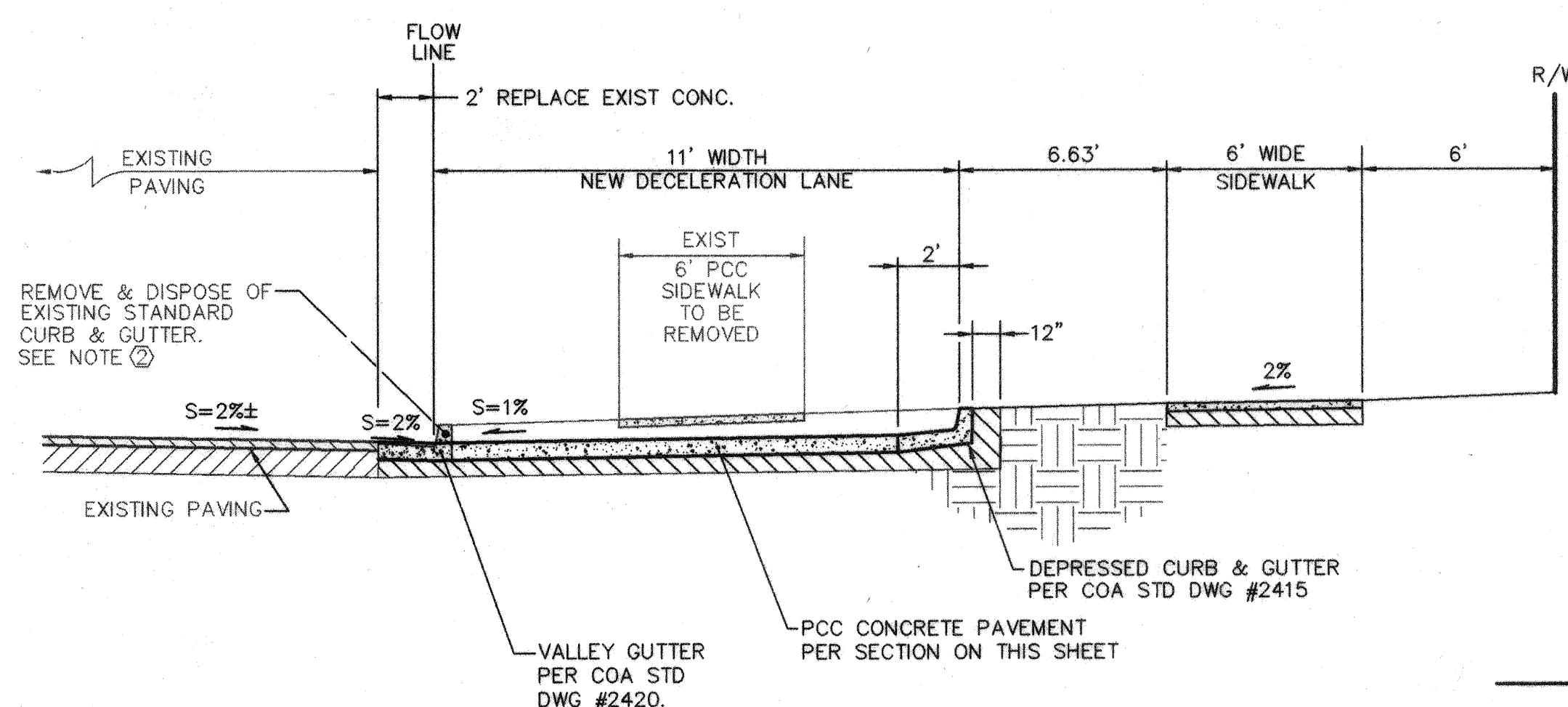
SAN MATEO BOULEVARD N.E.



ENTRANCE PAVEMENT SECTION
SCALE
1"=1'-0"



CONCRETE PAVEMENT SECTION
1"=1' (PER COA STD. DWG. #2407)



**SECTION A-A
DECELERATION LANE**
N.T.S.

WHEELCHAIR RAMP
SCALE: 1"=5'

KEYED CONSTRUCTION NOTES

- REMOVE AND DISPOSE OF (65 LF) EXISTING MEDIAN CURB & GUTTER.
- CAREFULLY REMOVE AND DISPOSE OF (190 LF) EXISTING STANDARD CURB & GUTTER. THE CONTRACTOR SHALL REMOVE EXISTING CURB & GUTTER SO AS TO PREVENT DAMAGE TO EXISTING ASPHALT. THE CITY INSPECTOR MUST APPROVE THE ASPHALT CONDITION PRIOR TO PLACING NEW CONCRETE. IF THE CITY INSPECTOR DOES NOT APPROVE, 1' WIDTH OF EXISTING ASPHALT MUST BE SAWCUT, REMOVED & REPLACED.
- REMOVE AND DISPOSE OF (177 SY) EXISTING PCC SIDEWALK.
- STANDARD CURB & GUTTER PER COA STD DWG #2415.
- MEDIAN CURB & GUTTER PER COA STD DWG #2415.
- DEPRESSED CURB & GUTTER PER COA STD DWG #2415.
- NOT USED.
- 6' PCC SIDEWALK PER COA STD DWG # 2430.
- WHEELCHAIR RAMP PER DETAIL ON THIS SHEET.
- PCC VALLEY GUTTER PER COA STD DWG #2420.
- 4" THICK TEXTURED PCC CONCRETE PER COA STD DWG #2408.
- REMOVE AND DISPOSE OF EXISTING DRIVEPAD.
- 16 SY ARTERIAL PAVING AT THICKNESS OF EXISTING PAVEMENT, WITH AN ADDITIONAL 2" THICKNESS.
- PCC CONCRETE DECELERATION LANE PER PAVEMENT SECTION ON THIS SHEET. CONTROL JOINTS REQUIRED AT 10' SPACING PER COA STD DWG #2450.
- 60 LF 4" WIDE, THERMOPLASTIC, SOLID WHITE STRIPING.
- FUTURE PCC SIDEWALK PER DEVELOPMENT PLAN.
- RESIDENTIAL PAVING PER ENTRANCE PAVEMENT SECTION ON THIS SHEET.
- RIGHT TURN ONLY ARROW.

FACE OF CURB CURVE TABLE

CURVE	RADIUS	LENGTH	DELTA
C1	25.00'	39.26'	89°58'39"
C2	25.00'	39.30'	90°03'48"
C3	150.00'	36.82'	14°03'48"
C4	50.00'	12.26'	14°03'16"
C5	25.00'	39.28'	90°00'43"
C6	25.00'	39.21'	89°52'15"
C7	2.87'	9.05'	28°06'29"

GENERAL NOTES:

- BASELINE OF STATIONING IS THE WEST RIGHT-OF-WAY LINE OF SAN MATEO BLVD. STA 1+00.00 IS THE SOUTHEAST CORNER OF TRACT 3-B.

ISAACSON & ARFMAN, P.A.
Consulting Engineering Associates
128 Monroe Street N.E.
Albuquerque, New Mexico 87101
12SPV1.DWG.rth 05/01/01

CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING GROUP

TITLE:
**TERIYAKI CHICKEN BOWL
SAN MATEO BOULEVARD
PAVING PLAN**

Design Review Committee	City Engineer Approval	Mo./Day/Yr.	Mo./Day/Yr.
APPROVED MAY 17 2001 DESIGN REVIEW COMMITTEE	APPROVED JUL 10 2001 CITY ENGINEER		
DESIGNED BY FCA	DRAWN BY ANW	CHECKED BY FCA	
DATE 1/01	DATE 1/01	DATE 1/01	

658981

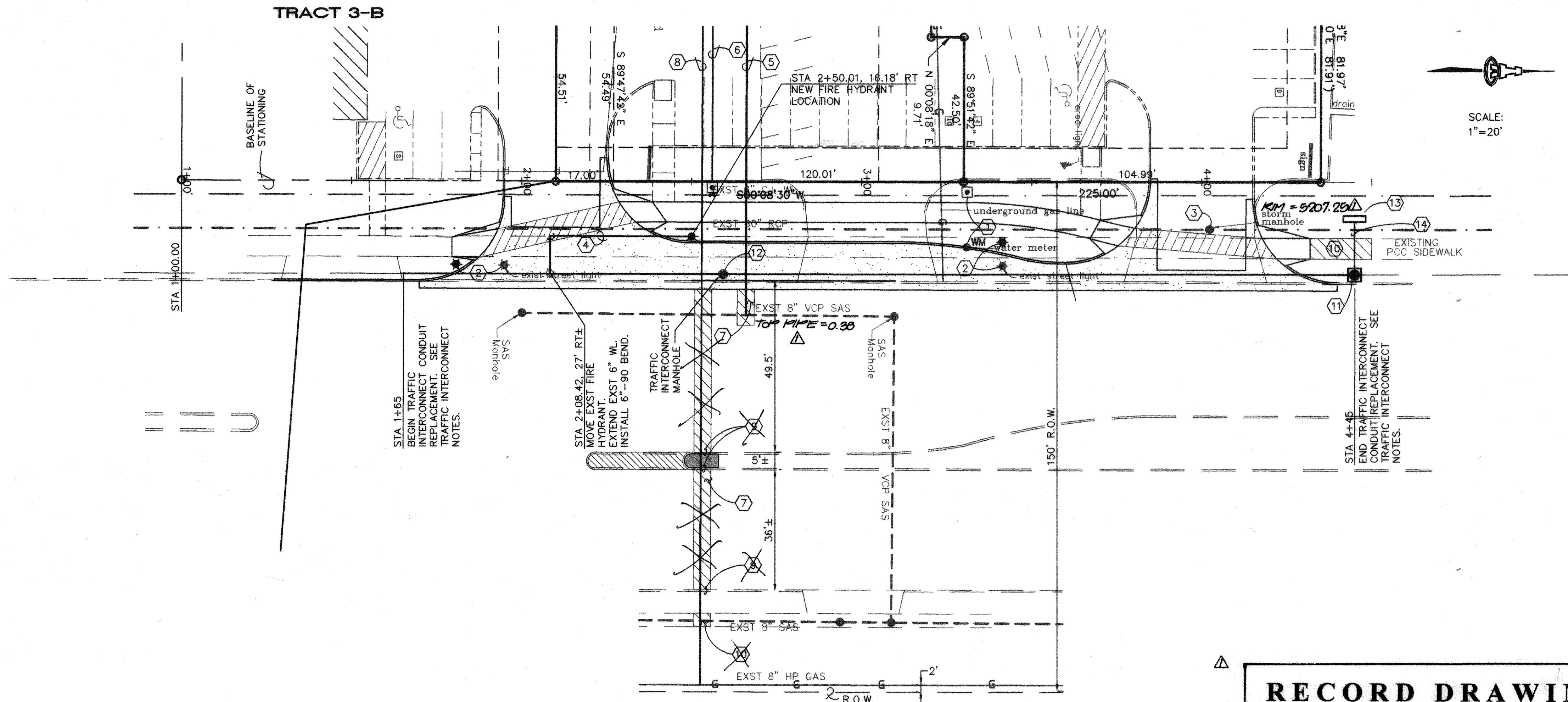
Zone Map No.

Sheet

3

Of

6



SAN MATEO BOULEVARD N.E.

RESTRAINED JOINT LENGTHS, L _r , FOR HORIZONTAL BENDS, VALVES, AND DEAD ENDS, (FT.)						
SIZE	HORZ. BENDS					VALVES & DEAD ENDS
	90°	45°	22 1/2°	11 1/4°		
12	30	12	6	3		85
10	26	11	5	3		72
8	22	9	4	2		60
6	17	7	3	2		46
4	12	5	2	1		32

THIS TABLE IS BASED UPON THE FOLLOWING CRITERIA:

DEPTH OF BURY 3.0 FT. MINIMUM
 FACTOR OF SAFETY 1.25
 MATERIAL PVC PIPE
 SOIL TYPE GM/SM - SILTY GRAVELS AND SILTY SANDS,
 GRAVEL-SAND-SILT MIXTURES.
 TEST PRESSURE 150 PSI
 TRENCH TYPE 4: PIPE BEDDED IN SAND, GRAVEL, OR CRUSHED
 STONE TO DEPTH OF 1/8 PIPE DIAMETER, 4 INCH
 MINIMUM; BACKFILL COMPACTED TO TOP OF PIPE.

DIFFERENT CRITERIA, E.G., DUCTILE IRON PIPE, GREATER DEPTH OF BURY,
 ETC., WILL REQUIRE DIFFERENT RESTRAINED LENGTHS. THESE MUST BE
 CALCULATED BY A QUALIFIED PROFESSIONAL ENGINEER AND APPROVED
 BY THE PUBLIC WORKS DEPARTMENT.

RESTRAINED JOINT LENGTHS FOR WATERLINE FITTINGS

TRAFFIC INTERCONNECT NOTES

1. REPLACE EXISTING CONDUIT WITH NEW 2" PVC CONDUIT A MINIMUM OF 2' BELOW PROPOSED GRADE.
2. EXISTING CABLE IS TO REMAIN IN PLACE. CONTRACTOR SHALL PROTECT THE CABLE DURING RELOCATION AND CONSTRUCTION. IF THE CABLE IS DAMAGED DURING CONSTRUCTION, THE CONTRACTOR SHALL REPLACE THE ENTIRE LENGTH OF THE INTERCONNECT CABLE FROM OSUNA RD. TO ACADEMY RD.
3. COORDINATE ALL WORK ON TRAFFIC CONTROL RELATED ITEMS WITH THE CITY OF ALBUQUERQUE TRAFFIC ENGINEERING DIVISION, FIELD OPERATIONS. CONTACT EDWARD TOLEDO OF THAT DEPARTMENT AT 857-8004 AT LEAST 3 DAYS PRIOR TO CONSTRUCTION.
4. EXTEND EXISTING CABLE TO NEW SPLICE CABINET.

LEGEND

- EXISTING STREET IMPROVEMENTS
- PROPOSED STREET IMPROVEMENTS
- FUTURE STREET IMPROVEMENTS
- VALLEY GUTTER
- STORM DRAIN MANHOLE
- SIDEWALK BY OTHERS
- SINGLE WATER METER & BOX
- DOUBLE WATER METER & BOX
- GATE VALVE W/ TYPE 'A' VALVE BOX
- FIRE HYDRANT
- WATER LINE W/ FITTING
- STORM DRAIN CATCH BASIN
- STORM DRAIN MANHOLE
- EXISTING SAS MANHOLE
- PROPOSED SAS MANHOLE

RECORD DRAWING

KEYED CONSTRUCTION NOTES

1. RELOCATE WATER METER BEHIND CURB PER COA STD DWG # 2360.
2. CONTRACTOR TO RELOCATE COA STREET LIGHT BEHIND CURB BY THIS CONTRACT. COORDINATE WITH PNM ELECTRICAL SERVICES.
3. ADJUST STORM DRAIN MANHOLE. RIM ELEV=5207.28.9
4. EXTEND 6" PVC FIRE LINE 53 LF TO NEW FIRE HYDRANT LOCATION. INCLUDE 6"-90° BEND. RESTRAIN JOINTS PER TABLE ON THIS SHEET.
5. 4" SAS SERVICE. INV =03.3, PER COA STD DWG #2125.
6. WATER SERVICE LINE WITH 1" METER PER COA STD DWG #2362.
7. SAWCUT, REMOVE & REPLACE ARTERIAL PAVING PER COA STD DWG #2465.
8. GAS SERVICE LINE.
9. SAWCUT, REMOVE & REPLACE CURB & GUTTER FOR GAS SERVICE TRENCH PER COA STD. DWG #2415.
10. SAWCUT, REMOVE & REPLACE 4" THICK PCC SIDEWALK PER COA STD. DWG #2430.
11. TRAFFIC SIGNAL MANHOLE TYPE I PER COA STD. DWG #2524.
12. REMOVE EXST. TRAFFIC INTERCONNECT MANHOLE. PLACE NEW 2" PVC CONDUIT.
13. TRAFFIC INTERCONNECT SPLICE CABINET.
14. EXTEND EXST. TRAFFIC INTERCONNECT CABLE TO NEW SPLICE CABINET. INSTALL NEW 2" CONDUIT FROM MANHOLE TO CABINET.

SURVEYOR'S CERTIFICATION

I, Charles G. Cala, Jr., a duly qualified Registered Professional Land Surveyor under the laws of the State of New Mexico, do hereby certify that the "as-built" information shown on these drawings was obtained from field construction and "as-built" surveys performed by me or under my supervision, that the "as-built" information shown on these drawings was added by me or under my supervision, and that this "as-built" information is true and correct to the best of my knowledge and belief and that I am not responsible for any of the design concepts, calculations, engineering, or intent of the record drawings.

Charles G. Cala, Jr., NMPS 11184
 Date 10-07-2001

GENERAL NOTES:

1. BASELINE OF STATIONING IS THE WEST RIGHT-OF-WAY LINE OF SAN MATEO BLVD. STA 1+00.00 IS THE SOUTHEAST CORNER OF TRACT 3-B.

ISAACSON & ARFMAN, P.A.
 Consulting Engineering Associates
 Albuquerque New Mexico
 12501 L.D.W.G. 05/02/01

CITY OF ALBUQUERQUE
 PUBLIC WORKS DEPARTMENT
 ENGINEERING GROUP

TITLE: TERIYAKI CHICKEN BOWL
 SAN MATEO BOULEVARD
 UTILITY PLAN

Design Review Committee	City Engineer Approval	Mo./Day/Yr.	Mo./Day/Yr.
APPROVED MAY 17 2001 DESIGN REVIEW COMMITTEE	APPROVED JUL 10 2001 CITY ENGINEER		
City Project No. 658981	Zone Map No.	Sheet 4	Of 6

CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

1. CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.
2. CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPERATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.
3. THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.
4. CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.
5. CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.
6. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
7. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET-UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR EQUIVALENT.
8. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
9. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.
10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.
11. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.
13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.
14. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.
15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.
16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.
17. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.
18. ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
19. 48 HOURS PRIOR TO OCCUPANCY OR CLOSING OF A RIGHT-OF-WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.
20. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.
21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.

22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING:
1. STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
 2. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION.
 3. SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.

23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.

24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION, WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.

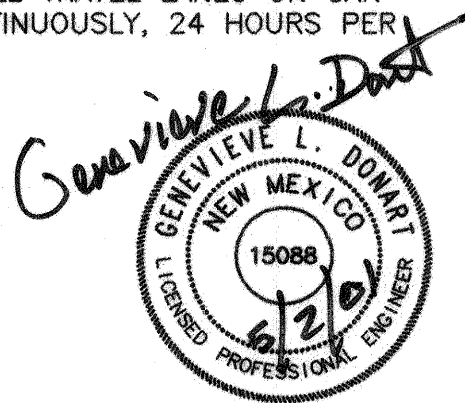
25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.

26. ADVANCE WARNING SIGNS SHALL BE 36"x36" MIN. WITH SUPER ENGINEERING GRADE SHEETING OR BETTER. MOUNTING HEIGHT AT TOP OF SIGN SHALL BE THE SAME AS FOR A 48" SIGN AS INDICATED IN THE M.U.T.C.D.

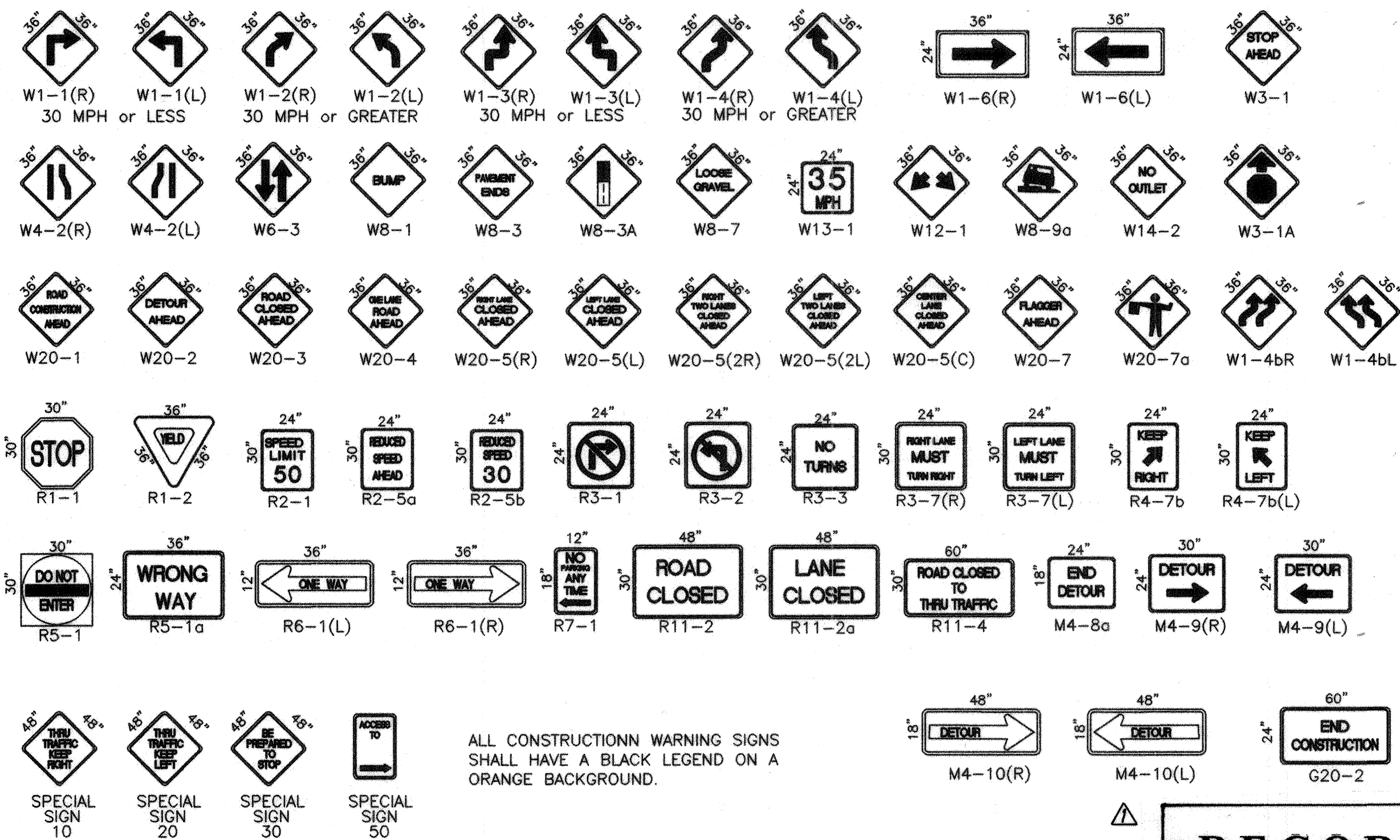
27. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORKSITE. ALL GRAFFITI SHALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND TEMPORARY.

SPECIAL PROJECT TRAFFIC CONTROL NOTES:

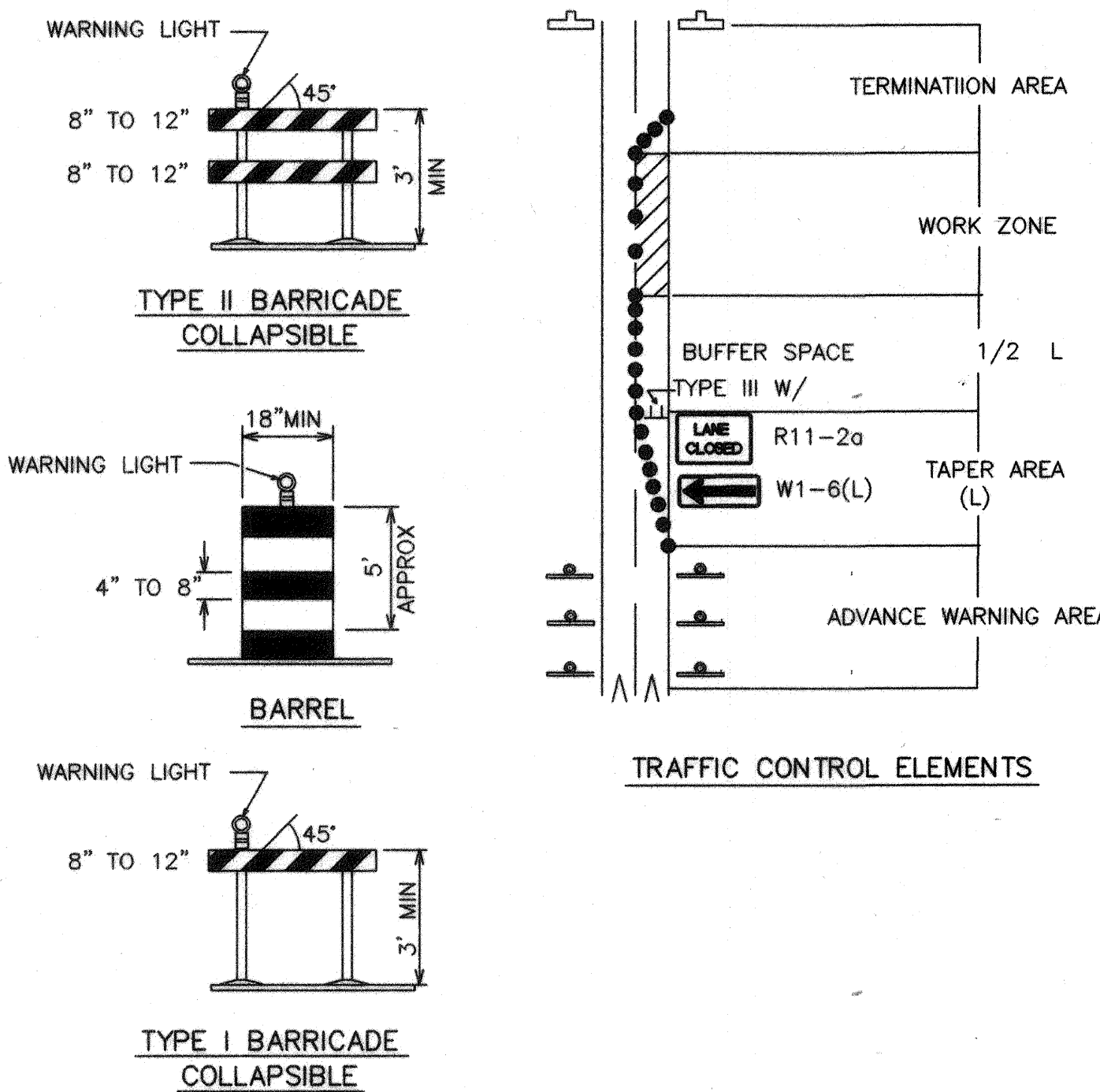
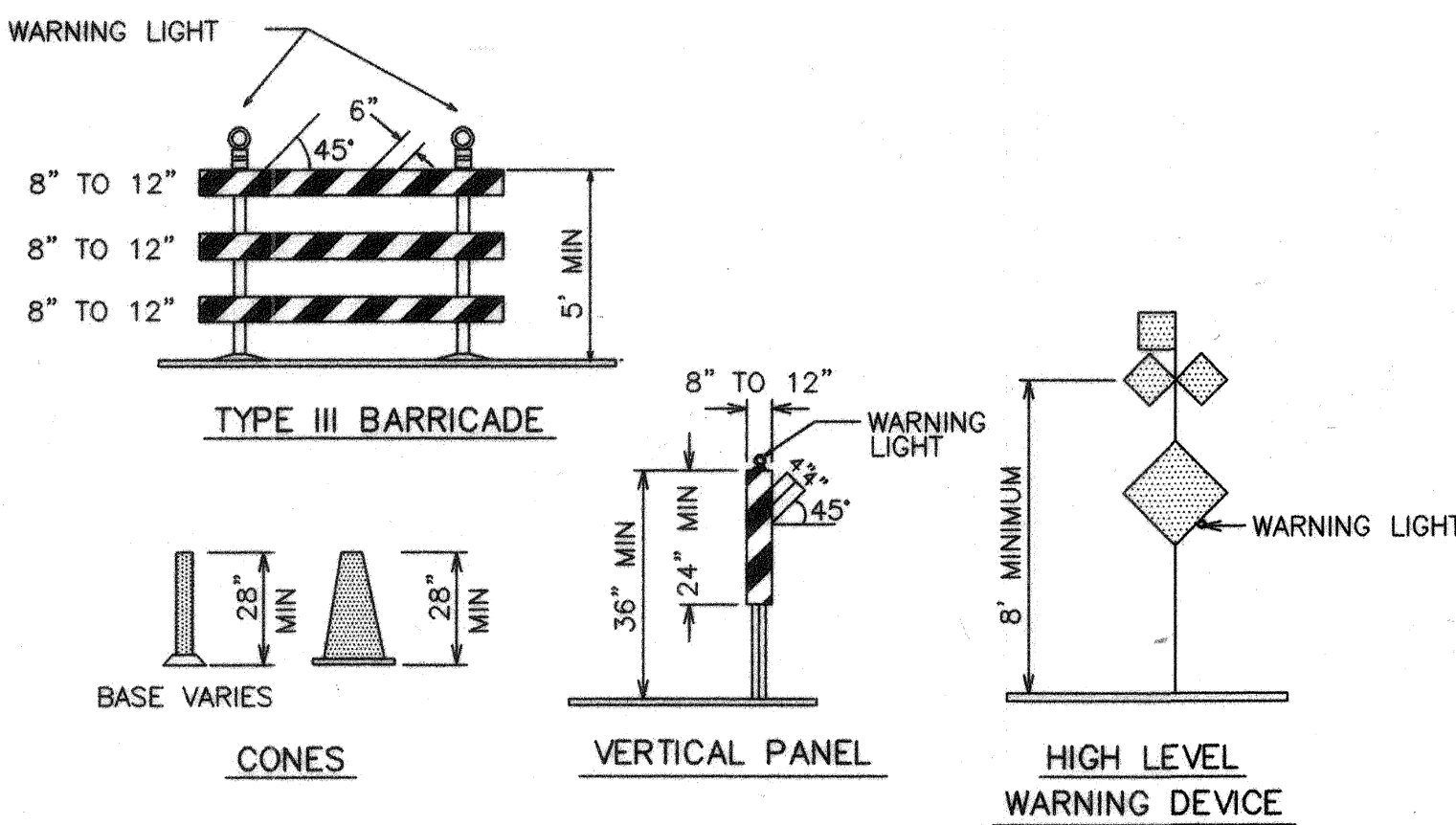
1. CONTRACTOR SHALL PERFORM AS MUCH WORK AS POSSIBLE BEHIND THE EXISTING CURB AND GUTTER PRIOR TO EFFECTING TRAFFIC.
2. ALL LANE CLOSURES ON SAN MATEO BLVD. ARE SUBJECT TO ARTERIAL / COLLECTOR ROAD USAGE FEES.
3. NO LANE CLOSURES WILL BE ALLOWED ON SAN MATEO BLVD. FROM 6:00 a.m. TO 9:00 a.m., AND FROM 3:00 p.m. TO 7:00 p.m., MONDAY THROUGH FRIDAY.
4. IF LANE CLOSURES ARE NEEDED FOR SAFETY CONCERNS, OR THERE ARE OPEN EXCAVATIONS IN TRAVEL LANES; CONTRACTOR SHALL WORK CONTINUOUSLY, 24 HOURS PER DAY UNTIL ALL LANES ARE OPENED TO TRAFFIC.
5. CONTRACTOR IS ALLOWED NO MORE THAN FIVE DAYS OF LANE CLOSURES TO PERFORM ALL WORK ASSOCIATED WITH THE SOUTHBOUND RIGHT CURB.
6. A MINIMUM OF TWO LANES MUST BE OPENED ON SAN MATEO BLVD. IN EACH DIRECTION AT ALL TIMES.
7. IF THE CONTRACTOR IS UNABLE TO FULLY OPEN ALL TRAVEL LANES ON SAN MATEO BLVD., THE CONTRACTOR SHALL WORK CONTINUOUSLY, 24 HOURS PER DAY, UNTIL ALL LANES ARE OPEN TO TRAFFIC.



SIGN FACE DETAILS



ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.



LEGEND

- WORK AREA
- BARRICADE - TYPE I, TYPE II, OR BARREL
 - BARRICADE - TYPE III
 - VERTICAL PANEL
 - WARNING SIGN
 - DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET
 - FLAGMAN POSITION
 - SPACING BETWEEN BARRICADES - A DISTANCE MEASURED IN FEET EQUAL TO THE SPEED LIMIT OF THE STREET
 - TAPER LENGTH - SEE CHART BELOW

THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

TAPER REQUIREMENT

SPEED LIMIT (MPH)	TAPER LENGTH(L) (FEET)			MINIMUM NUMBER OF DEVICES FOR TAPER	MAXIMUM DEVICE SPACING IN FEET	
	10' LANE	11' LANE	12' LANE		ALONG TAPER	AFTER TAPER
20	70	75	80	5	20	20
25	105	115	125	6	25	25
30	150	165	180	7	30	30
35	205	225	245	8	35	35
40	270	295	320	9	40	40
45	450	495	540	13	45	45
50	500	550	600	13	50	50
55	550	605	660	13	55	55

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

SPEED LIMIT (MPH)	MINIMUM DISTANCE IN FEET BETWEEN SIGNS	FROM LAST SIGN TO TAPER
0-20	10 X SPEED LIMIT	10 X SPEED LIMIT
25-30	10 X SPEED LIMIT	10 X SPEED LIMIT
30-35	10 X SPEED LIMIT	10 X SPEED LIMIT
40-45	10 X SPEED LIMIT	10 X SPEED LIMIT
50-60	10 X SPEED LIMIT	10 X SPEED LIMIT

TAPER CRITERIA

TYPE OF TAPER	TAPER LENGTH
UPSTREAM TAPER:	
MERGING TAPER	L MINIMUM
SHIFTING TAPER	1/2 L MINIMUM
SHOULDER TAPER	1/2 L MINIMUM
TWO-WAY TRAFFIC TAPER	100 FEET MAXIMUM
DOWNSTREAM TAPERS	100 FEET PER LANE

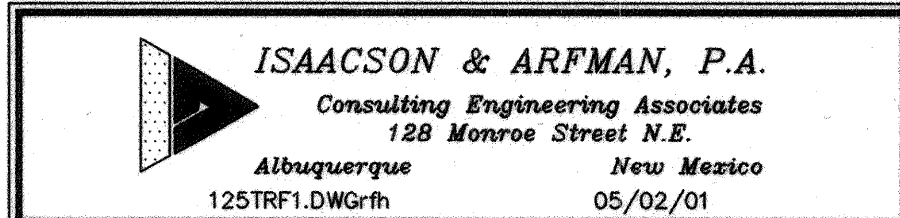
TAPER LENGTH COMPUTATION

SPEED LIMIT

40 MPH OR LESS $L = \frac{WS^2}{60}$

40 MPH OR GREATER $L = W \times S$

L = TAPER LENGTH
W = WIDTH OF OFFSET IN FEET
S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

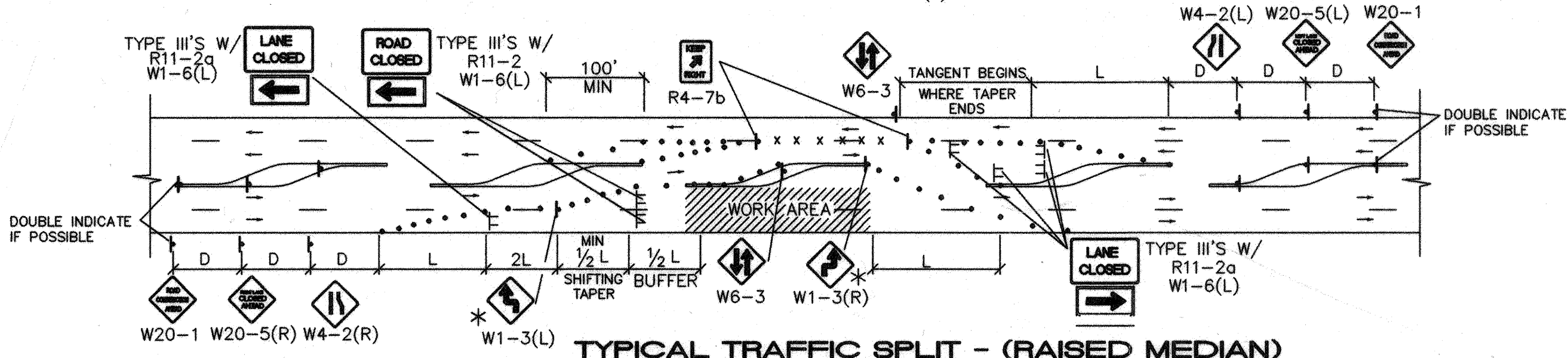
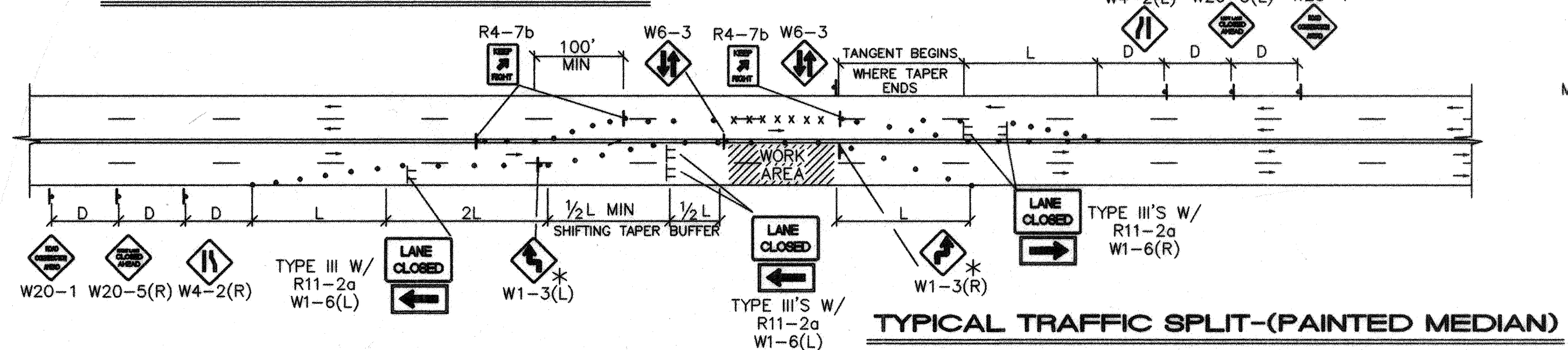
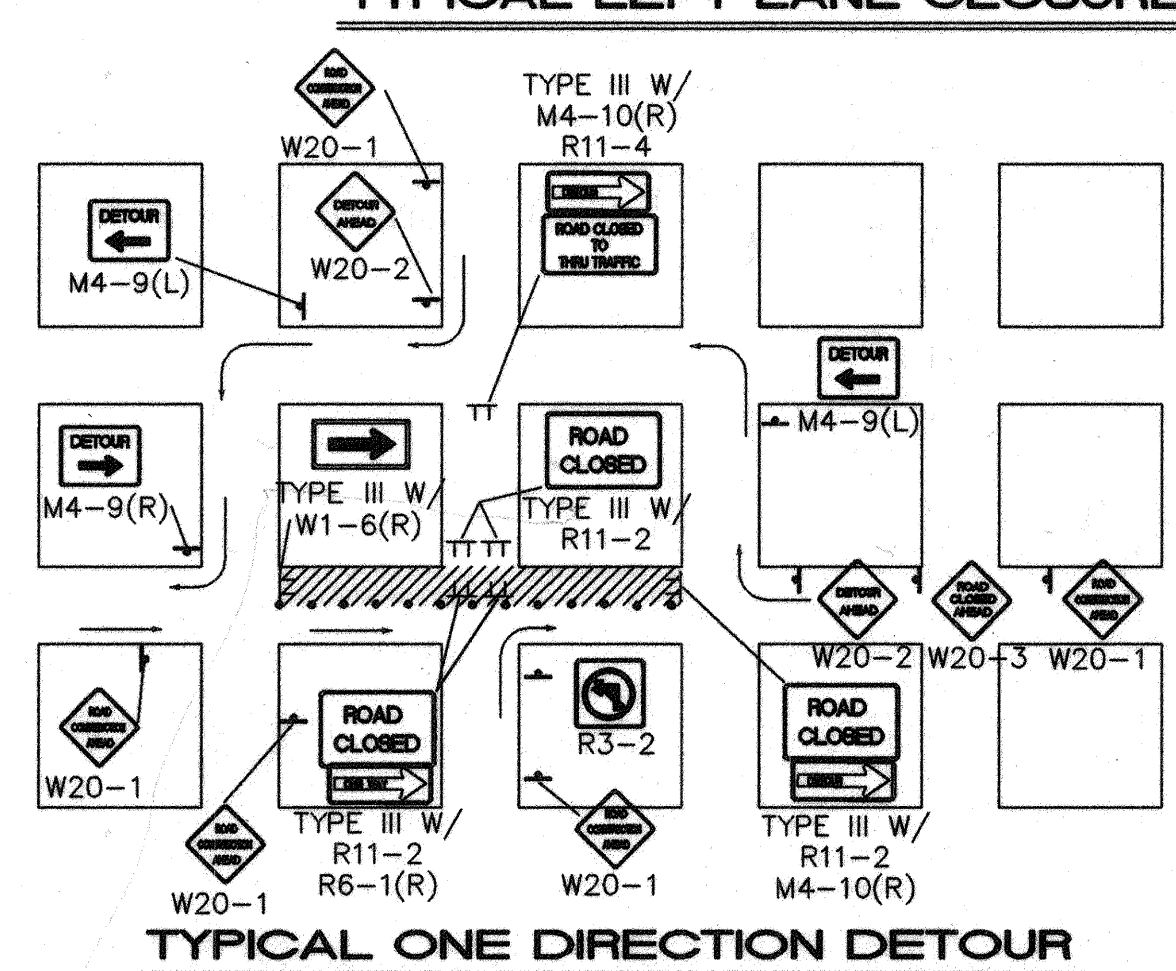
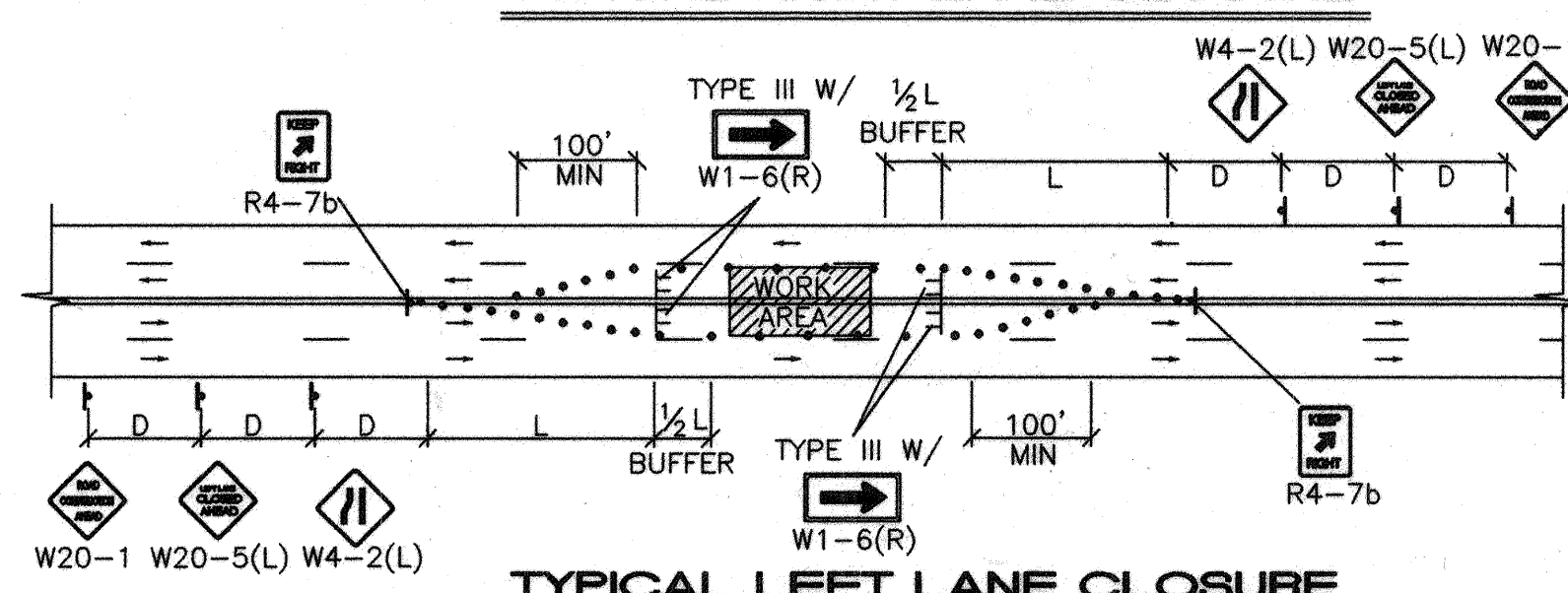
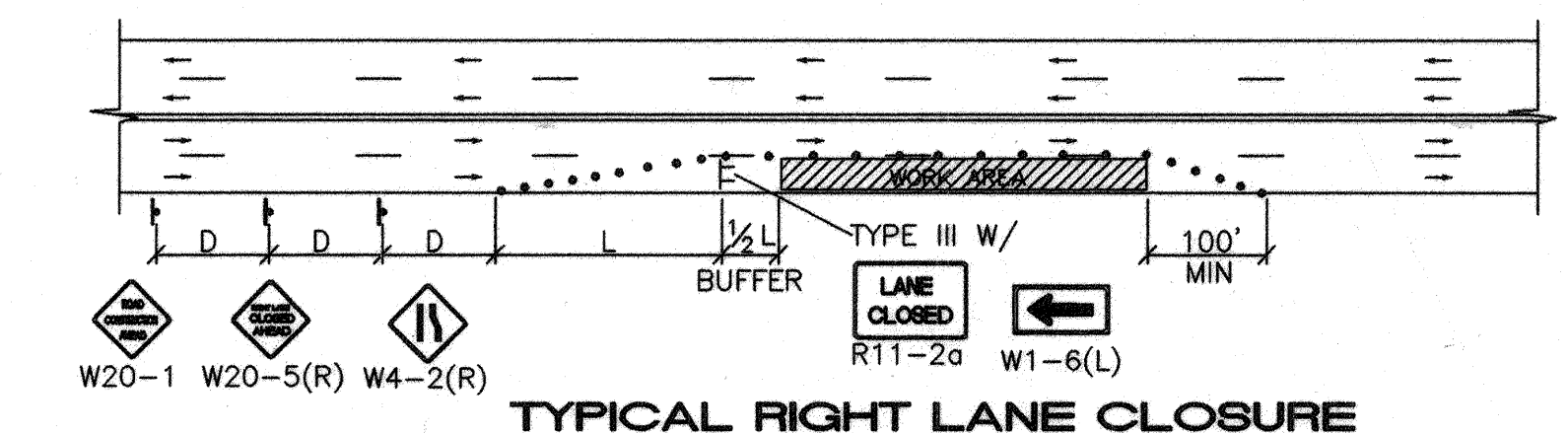


CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING GROUP

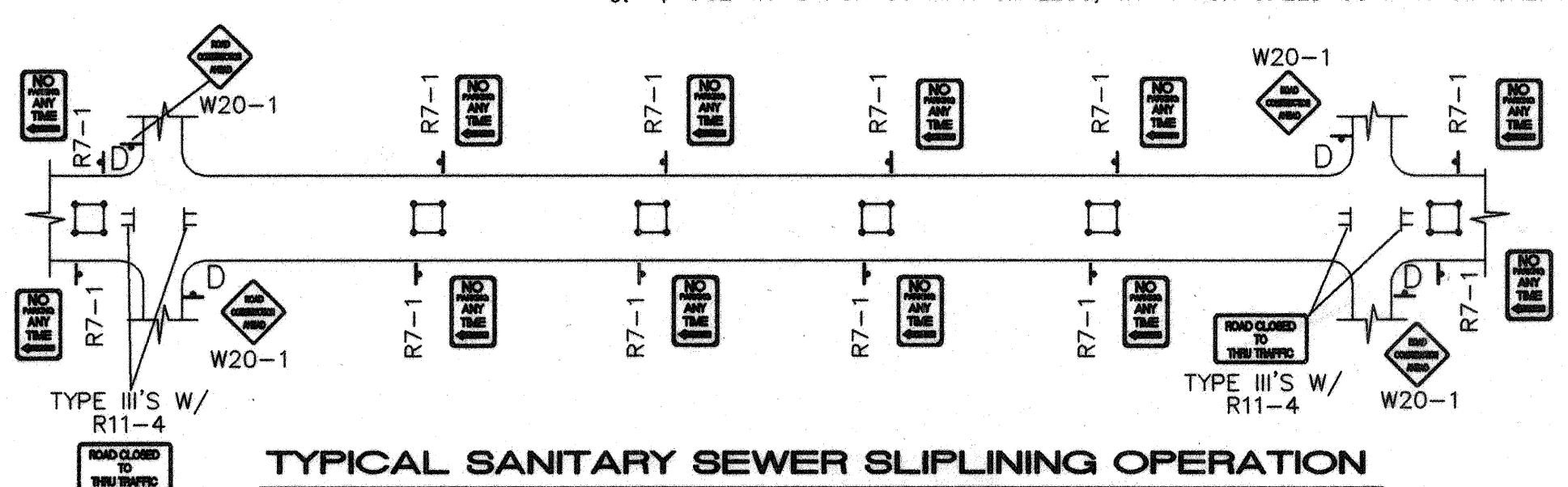
TITLE: **TERYAKI CHICKEN BOWL TRAFFIC CONTROL**
SIGNING & CONSTRUCTION TRAFFIC CONTROL STANDARDS

Design Review Committee	City Engineer Approval	Mo./Day/Yr.	Mo./Day/Yr.
COA	STD		
City Project No.	658981	Zone Map No.	Sheet 5 of 6

RECORD DRAWING

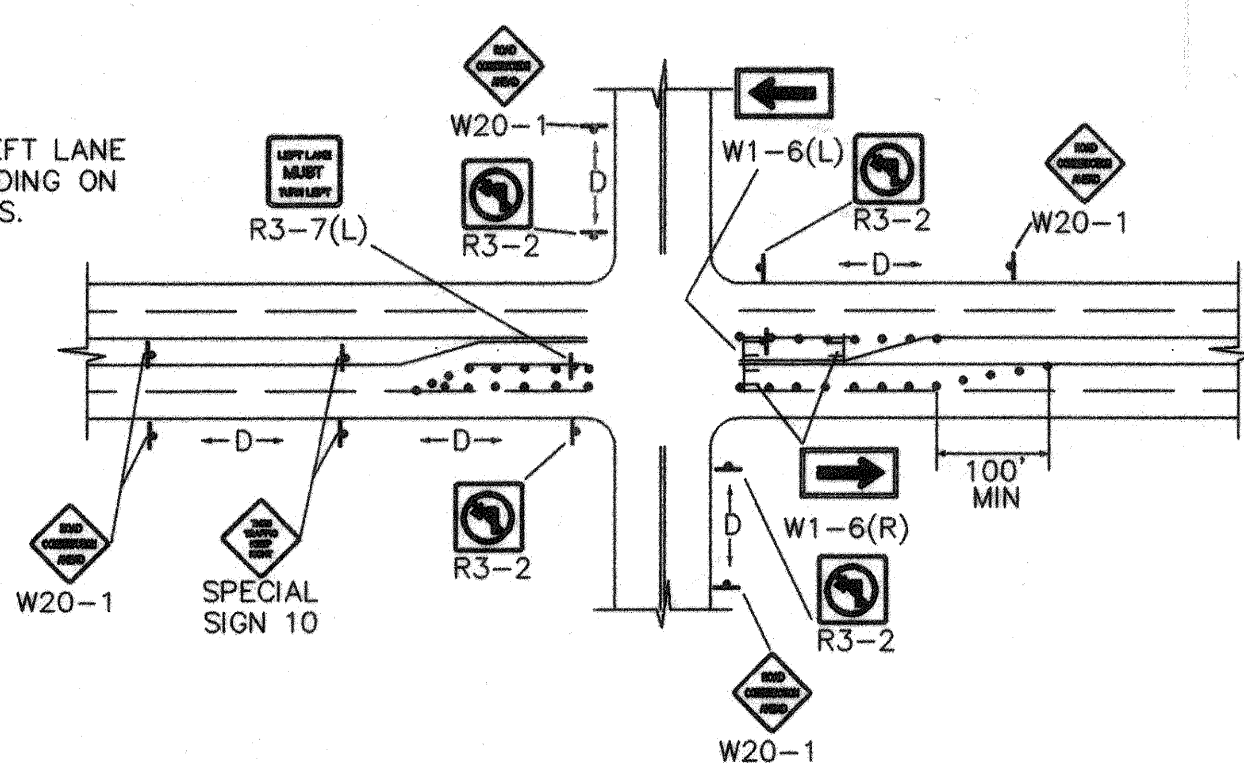


- TRAFFIC SPLIT NOTES:**
1. THE OFFSET DISTANCE MUST BE CALCULATED IN ALL SHIFTING TAPERS. THE OFFSET DISTANCES SHALL INCLUDE LANE WIDTHS PLUS MEDIAN WIDTHS.
 2. 1/2 L IS THE MINIMUM DISTANCE FOR SHIFTING TAPERS.
 3. REVERSE CURVES MAY BE IMPLEMENTED. ALL CURVE DATA SHALL BE CALCULATED.
 4. MEDIAN REMOVAL SHALL BE REQUIRED IF 1/2 L OR REVERSE CURVE IS NOT SUFFICIENT. MEDIAN REMOVAL SHALL TAKE PLACE BEFORE SPLITS.
 5. REDUCED SPEED MAY BE CONSIDERED.
 6. * USE W1-3 FOR 30 MPH OR LESS, W1-4 FOR SPEED 35 MPH OR GREATER.

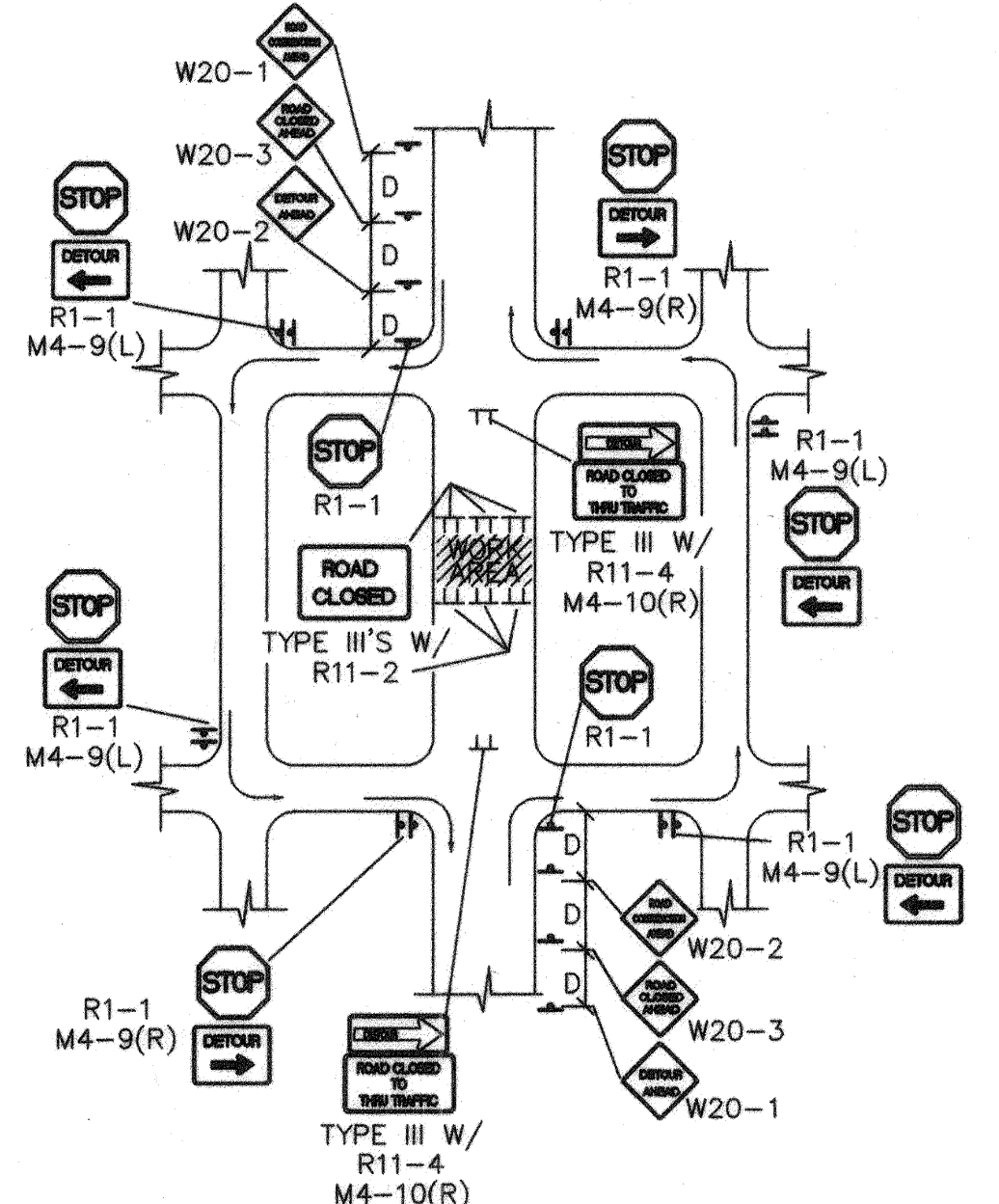


- ALL PITS TO HAVE TYPE-A FLASHING LIGHTS
- ALL PITS TO HAVE FLAGLINE

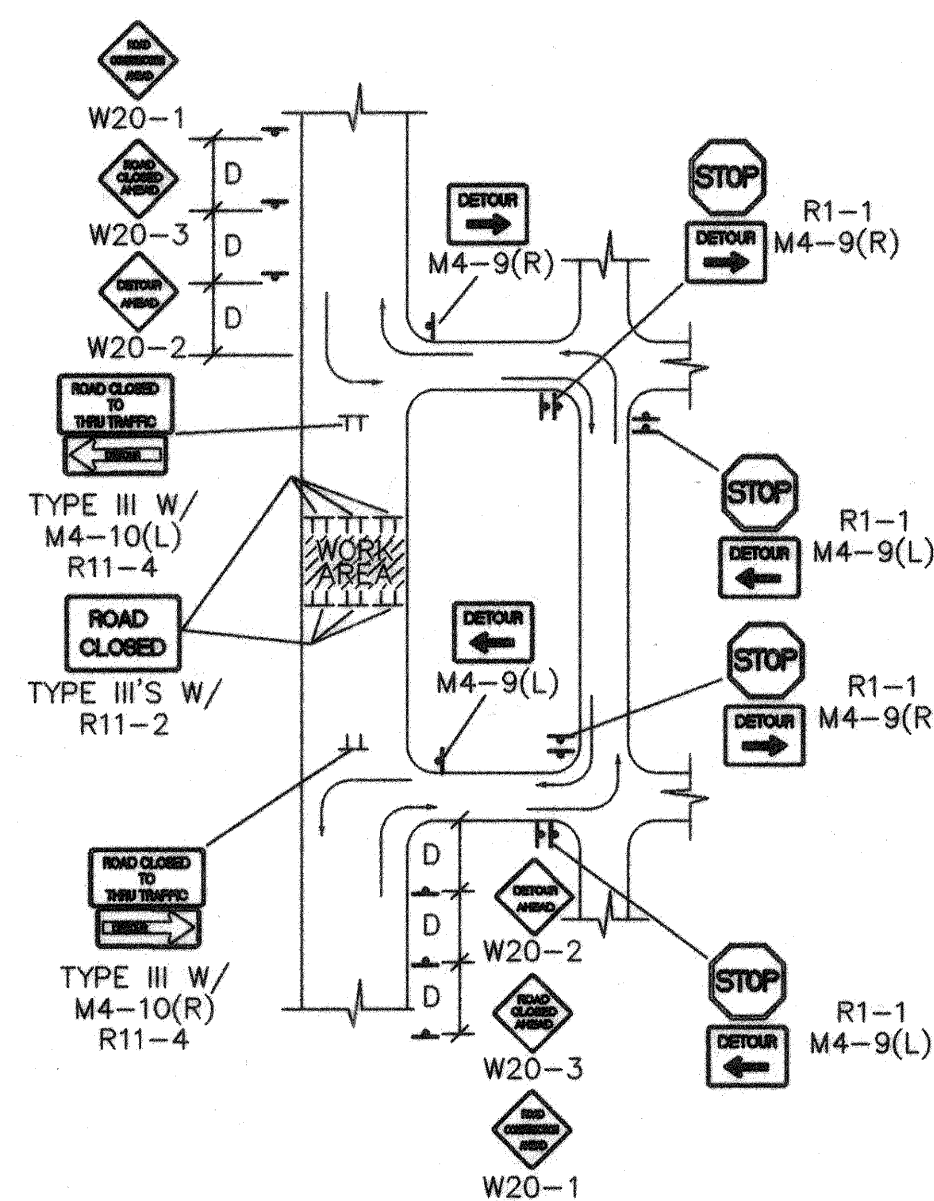
NOTE:
MAY REQUIRE LEFT LANE
CLOSURE DEPENDING ON
FIELD CONDITIONS.



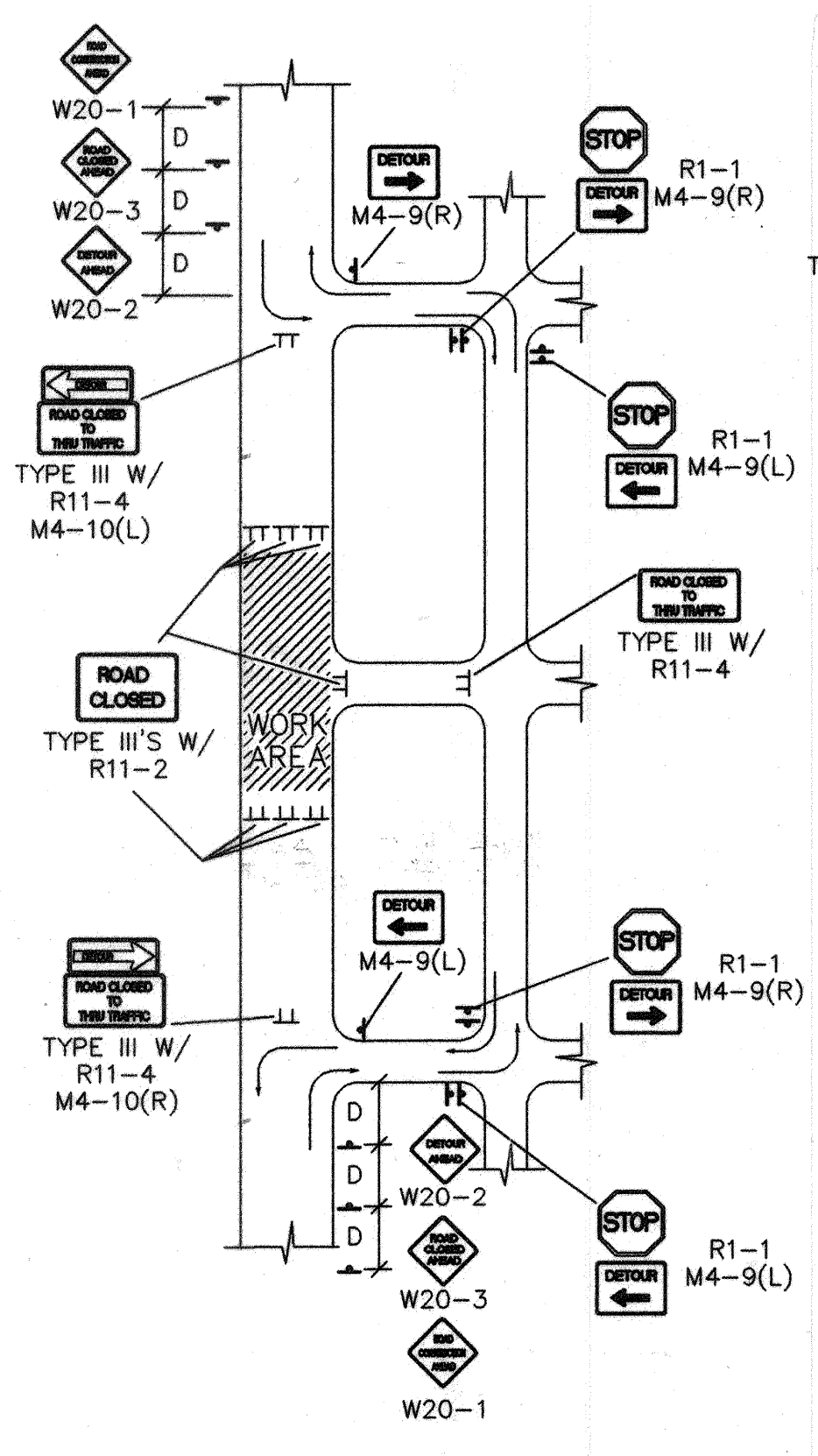
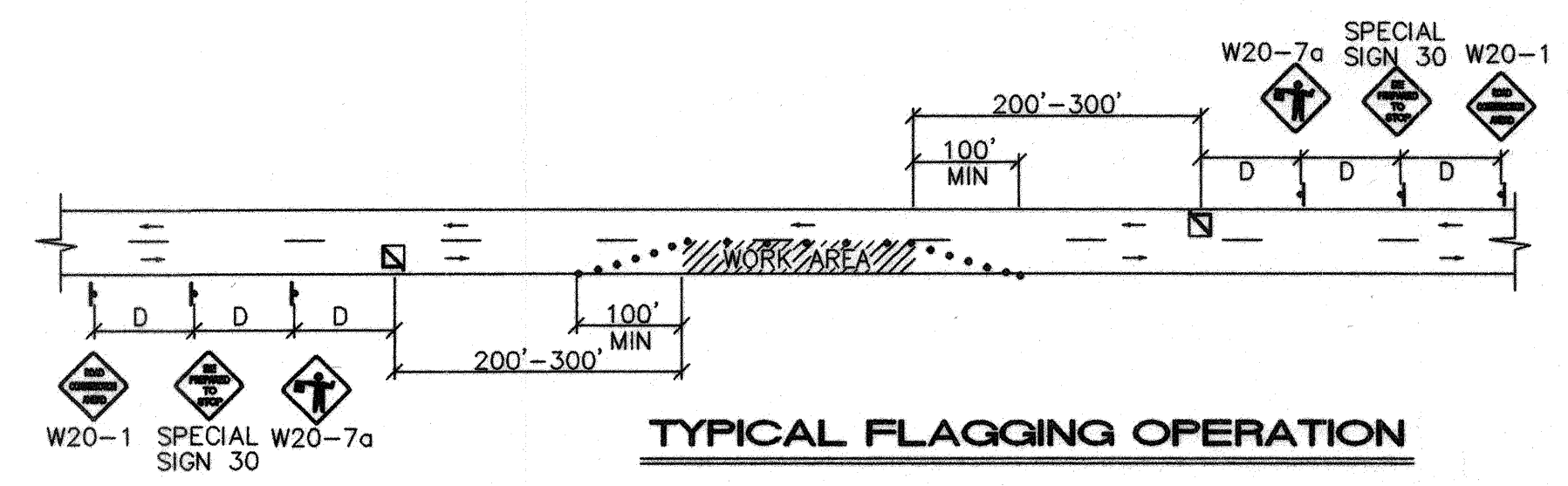
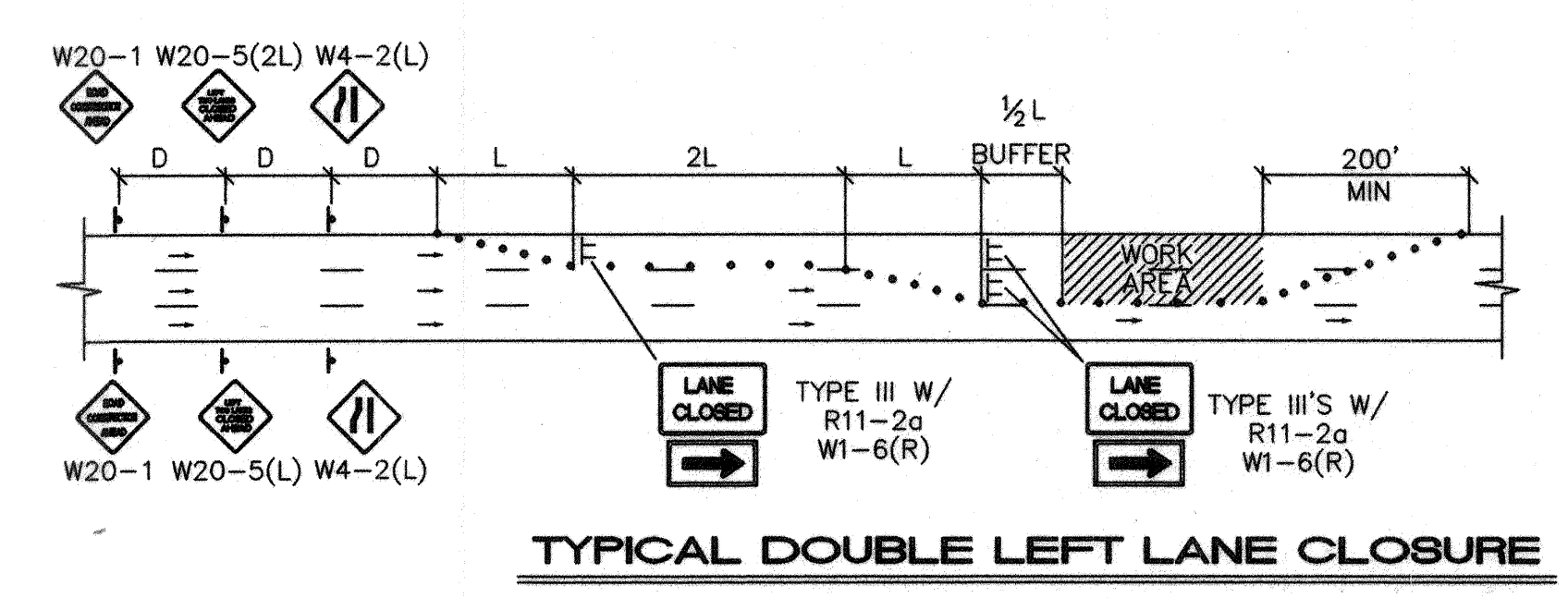
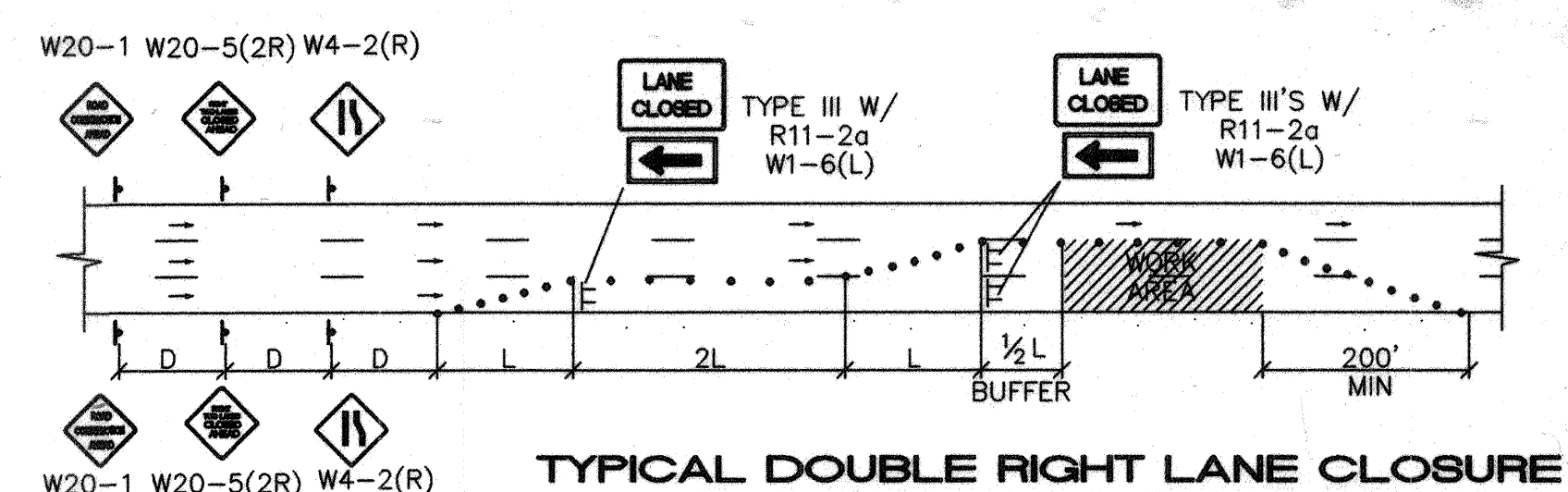
NOTE: DEPENDING ON WORK ZONE LOCATION.



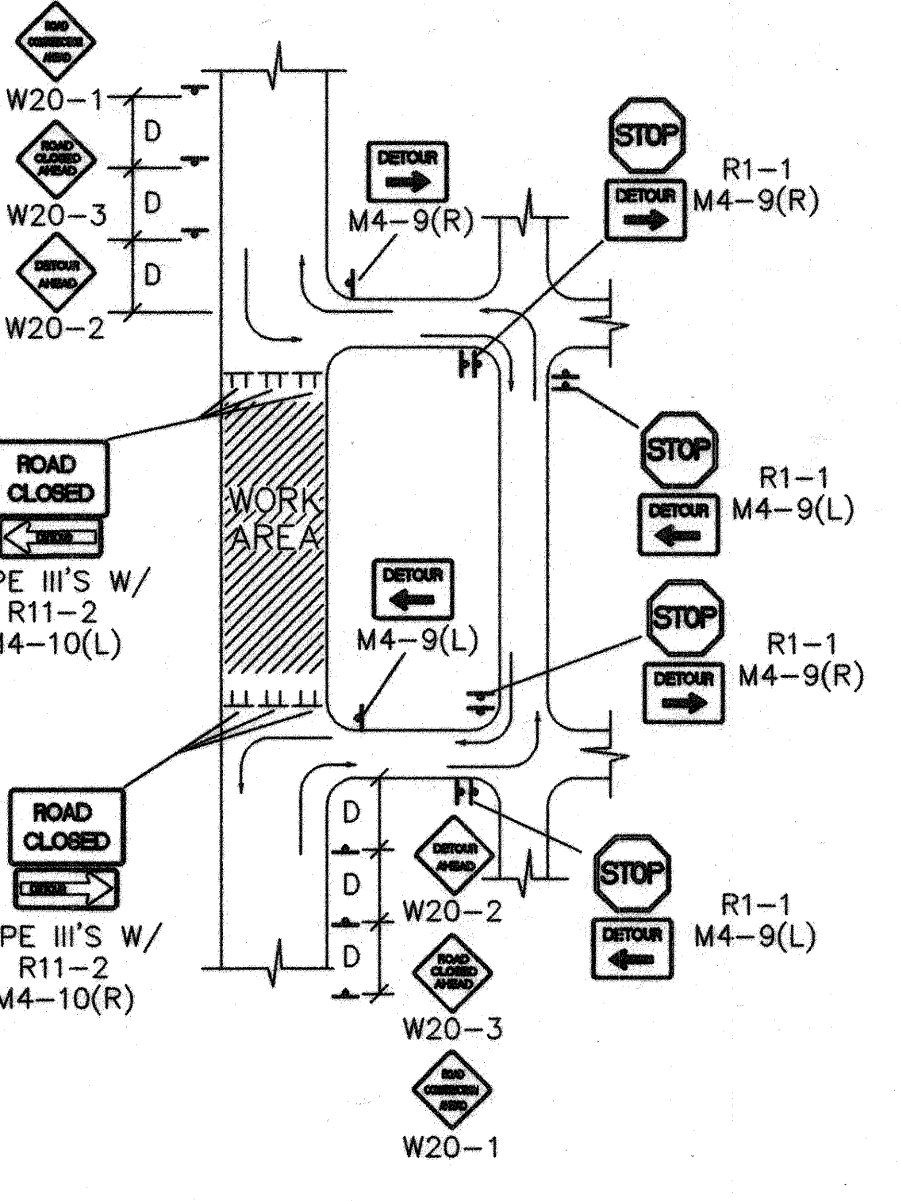
NOTE: (R1-1) BASED ON FIELD CONDITIONS.



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Consulting Engineering Associates
128 Monroe Street N.E.
Albuquerque, New Mexico 87102
125TRF2.DW07h 05/02/01

**CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING GROUP**

**TITLE: TERIYAKI CHICKEN BOWL
TRAFFIC CONTROL & SIGNING EXAMPLES**

Design Review Committee City Engineer Approval

COA STD

City Project No. 658981 Zone Map No. Sheet 6 Of 6

RECORD DRAWING