

SCANNED BY PLANNING

CONSTRUCTION PLANS

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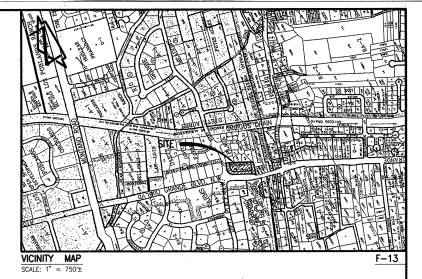
for

FLYING STAR - RIO GRANDE PARKING IMPROVEMENTS

ALBUQUERQUE, NEW MEXICO AUGUST, 2001

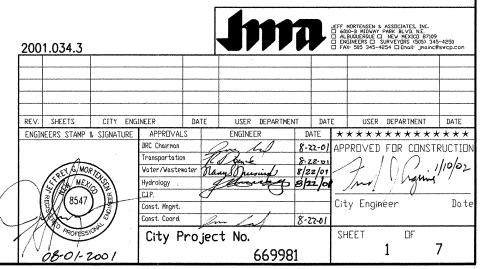
INDEX OF DRAWINGS

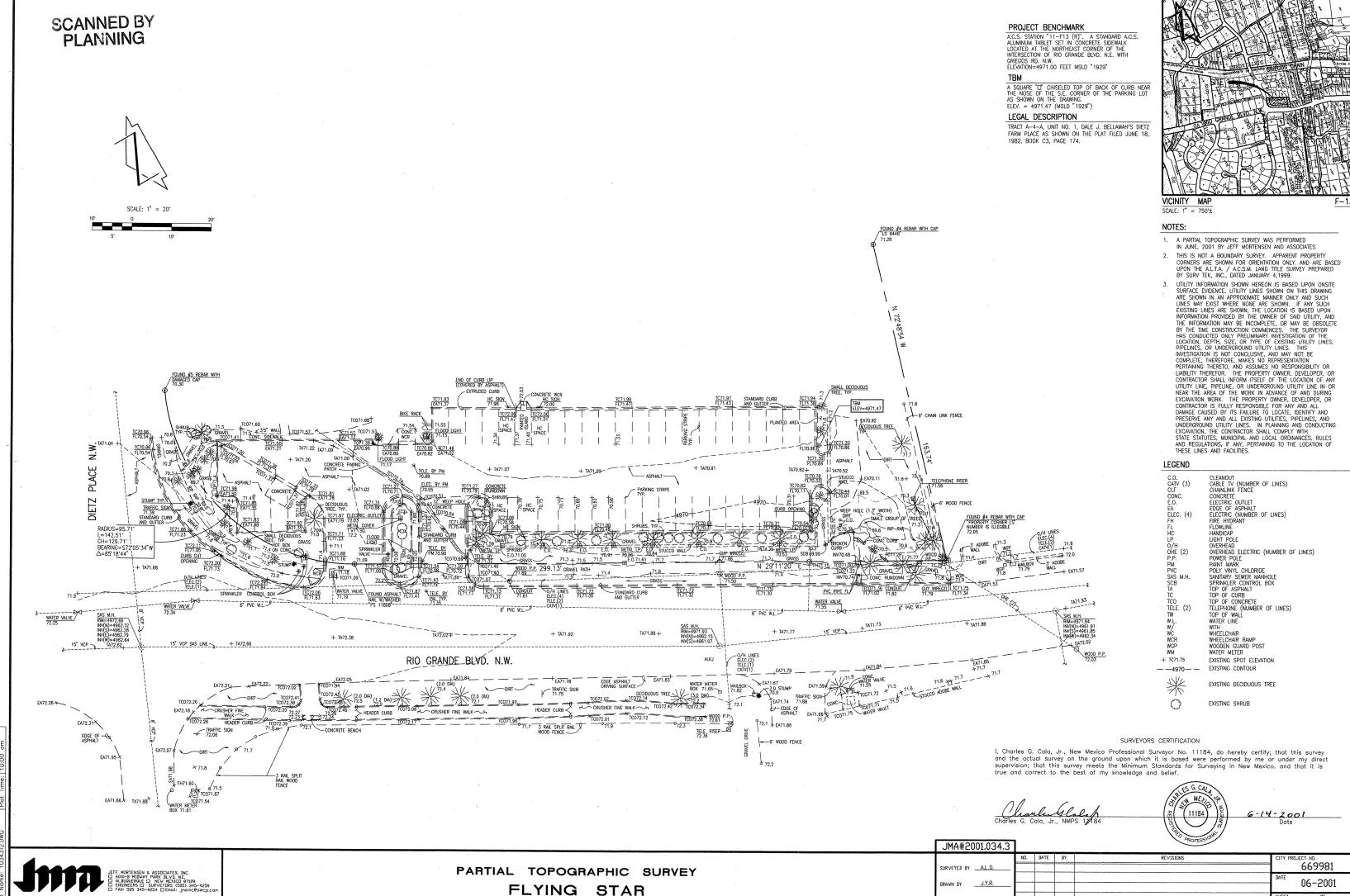
SHEET	DESCRIPTION
1	COVER SHEET, VICINITY MAP, GENERAL NOTES, LEGEND AND INDEX OF DRAWINGS
2	PARTIAL TOPOGRAPHIC SURVEY
3	DEMOLITION PLAN
4	RIO GRANDE BLVD. N.W. PARKING IMPROVEMENTS PLAN & PROFILE
5	SECTIONS AND DETAILS
6	SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS
7	TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES



GENERAL NOTES:

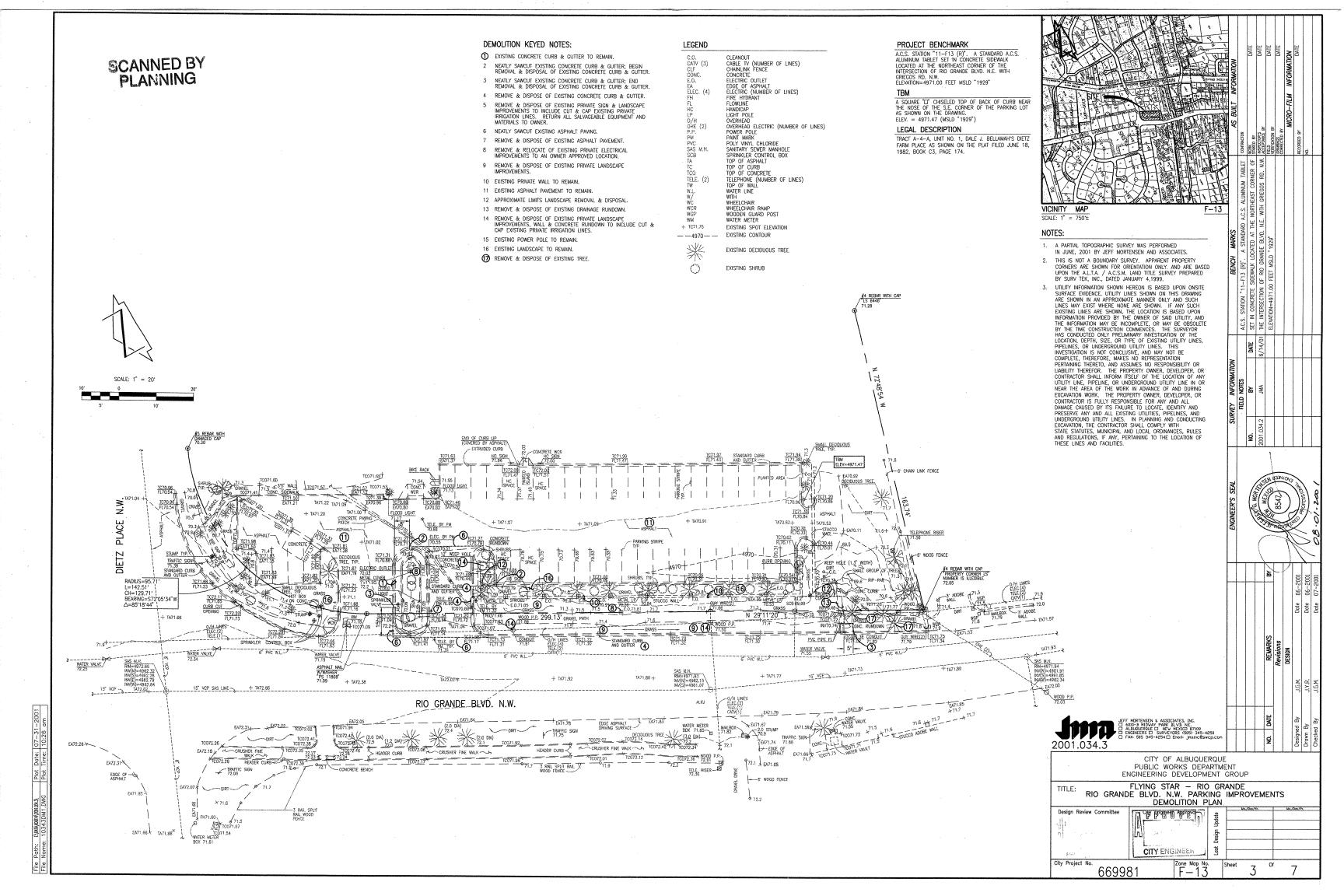
- . ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREON. BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS—PUBLIC WORKS CONSTRUCTION—1986 UPDATE NO. 6.
- . TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM, 260-1990, (ALBUQUERQUE AREA) 1-800-321-ALERT (2537) (STATEWIDE) FOR LOCATION OF EXISTING PUBLIC UTILITIES.
- IF ANY UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES ARE SHOWN ON THESE DRAWINGS, THEY ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. IF ANY SUCH EXISTING LINES ARE SHOWN, THE LOCATION IS BASED UPON INFORMATION PROVIDED BY THE OWNER OF SAID UTILITY, AND THE INFORMATION MAY BE INCOMPLETE, OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES. THE ENGINEER HAS CONDUCTED REASONBLE INVESTIGATION OF THE LOCATION DEPTH, SIZE OR TYPE OF EXISTING UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES. THE INVESTIGATION MAY NOT BE CONCLUSIVE, AND MAY NOT BE COMPLETE, THEREFORE, MAKES NO REPRESINATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. THE CONTRACTOR SHALL INFORM INSELF OF THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY TS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. IN PLANNING AND CONDUCTING EXCAVATION, THE CONTRACTOR SHALL COMPLY WITH STET STATURES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES.
- 4. SHOULD A CONFLICT EXIST BETWEEN THESE PLANS AND ACTUAL FIELD CONDITIONS, THE CONTRACTOR SHALL PROMPTL' NOTIFY THE ENGINEER IN WRITING SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY FO ALL PARTIES.
- 5. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ADJACENT PROPERTIES DURING CONSTRUCTION.
- ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING SAFETY AND HEALTH.
- 7. ALL UTILITIES AND UTILITY SERVICE LINES SHALL BE INSTALLED PRIOR TO PAVING
- 8. BACKFILL COMPACTION SHALL BE ACCORDING TO SPECIFIED STREET USE.
- 9. TACK COAT REQUIREMENTS SHALL BE DETERMINED DURING CONSTRUCTION BY THE PROJECT ENGINEER.
- SIDEWALKS AND WHEELCHAIR RAMPS WITHIN THE CURB RETURNS SHALL BE CONSTRUCTED WHEREVER A NEW CURB RETURN IS CONSTRUCTED.
- If CURB IS DEPRESSED FOR A DRIVEPAD OR A HANDICAP RAMP, THE DRIVEPAD OR RAMP SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF THE CURB AND GUITER.
- 12. ALL STORM DRAINAGE FACILITIES SHALL BE COMPLETED PRIOR TO FINAL ACCEPTANCE.
- 13. CONTRACTOR SHALL COORDINATE WITH THE WATER SYSTEM DIVISION FOR THE EXECUTION OF THE VALVE SHUT OFF PLAN, NOT LESS THAN FIVE (5) WORKING DAYS IN ADVANCE OF ANY WORK THAT MAY AFFECT THE EXISTING PUBLIC WATER UTILITIES. ONLY WATER SYSTEM DIVISION PERSONNEL SHALL OPERATE EXISTING VALVES. REFER TO SECTION 18 OF THE SPECIFICATION. THE WATER SYSTEM DIVISION PHONE NUMBER IS 857-8200.
- 14. CONTRACTOR SHALL NOTIFY THE CITY SURVEYOR NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATION OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED. CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE SPECIFICATIONS.
- 15. FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION CO-ORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL OBTAIN A BARRICADIE REPERT FROM THE CONSTRUCTION CO-ORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (768-2551) PRIOR TO OCCUPYING AN INTERSECTION. CONTRACTOR MUST REFER TO SECTION 19 OF THE STANDARD SPECIFICATION FOR TRAFFIC CONTROL.
- ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED WITH PLASTIC REFLECTORIZED STRIPING BY CONTRACTOR TO EXISTING LOCATION OR AS INDICATED BY THIS PLAN SET.
- 17. CAUTION: THESE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR CONSTRUCTION SAFETY WHICH SHALL REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.
- 18. ANY WORK OCCURRING WITHIN AN ARTERIAL ROADWAY REQUIRES TWENTY-FOUR HOUR CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.

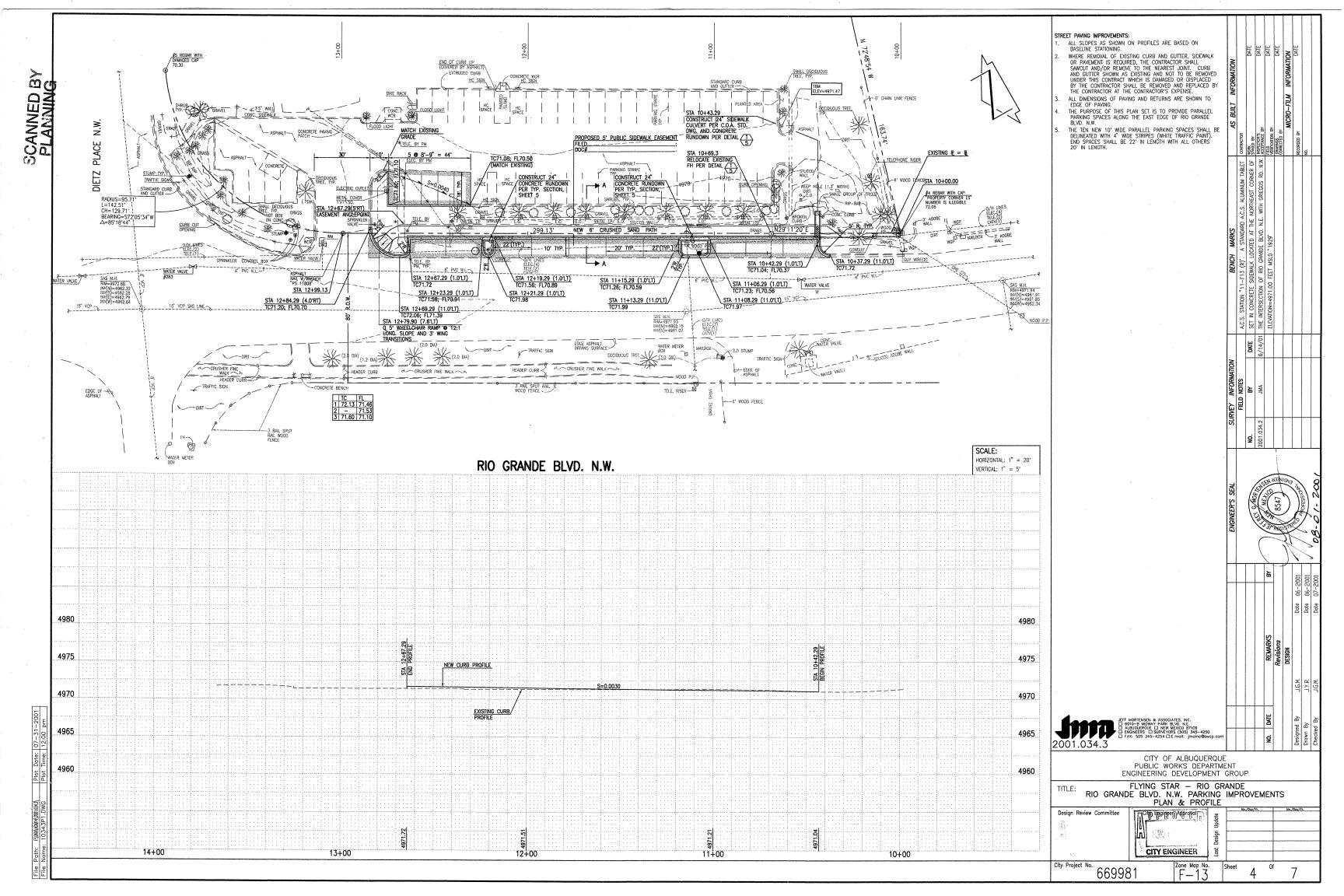




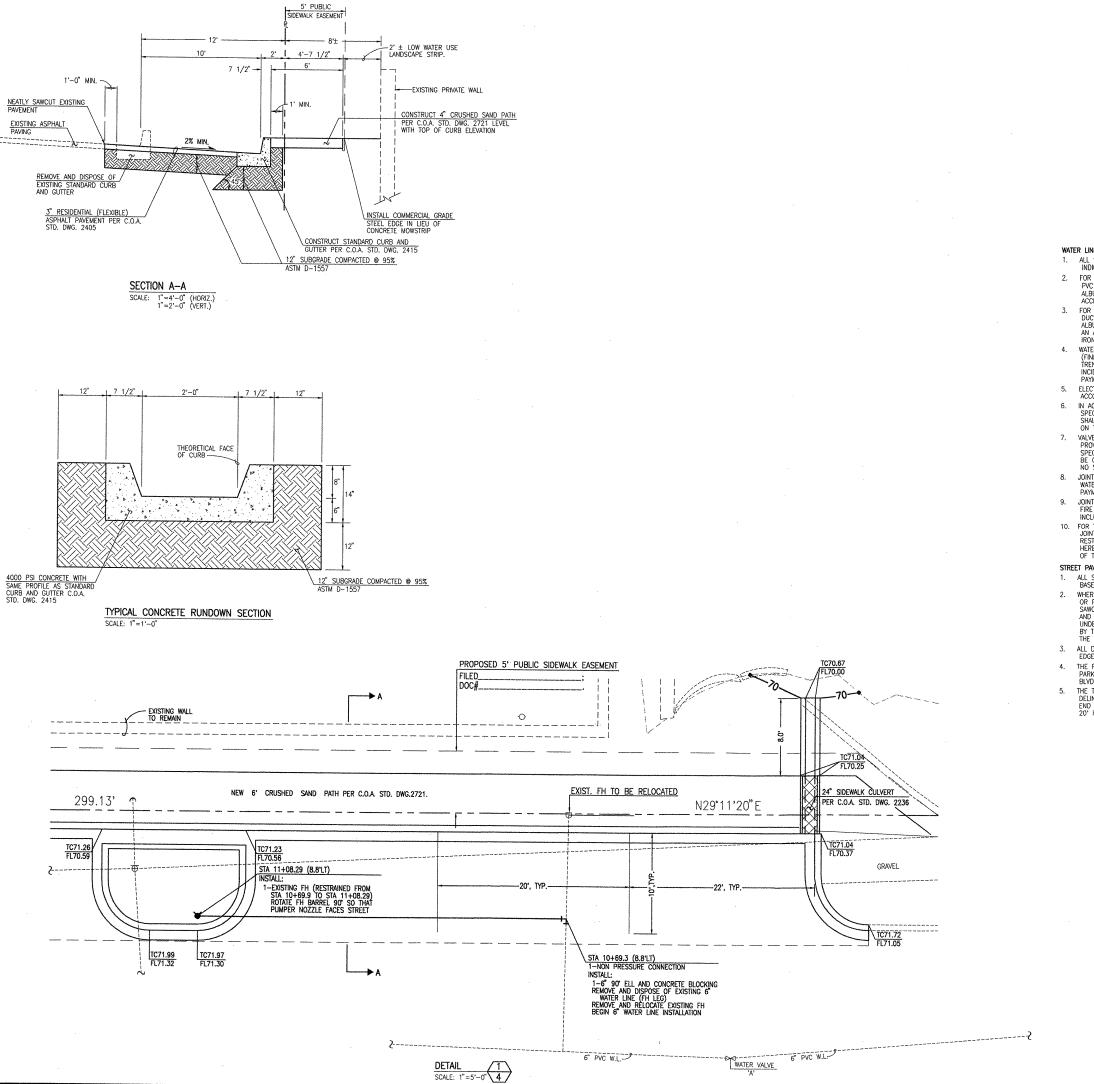
FLYING STAR

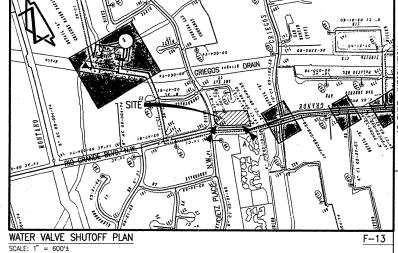
APPROVED BY C.G.C.











WATER LINE CONSTRUCTION NOTES

- ALL STATIONING IS BASED ON PROPERTY LINE UNLESS INDICATED OTHERWISE.
- INDICATED OTHERWISE.

 FOR ALL LINES 12" AND SMALLER, WATER MAIN SHALL BE PVC C-900 PIPE OR AS APPROVED BY THE CITY OF ALBUQUEROUE CITY ENGINEER. DUCTILE IRON IS AN ACCEPTABLE PIPE MATERIAL IN LIEU OF PVC.

 FOR ALL LINES 14" AND LARGER, WATER MAIN SHALL BE DUCTILE IRON OR AS APPROVED BY THE CITY OF ALBUQUERQUE CITY ENGINEER. CONCRETE CYLINDER IS AN ACCEPTABLE PIPE MATERIAL IN LIEU OF DUCTILE IRON.
- IRON.

 WATERINE SHALL HAVE A MINIMUM COVER OF 4'0"

 (FINISHED GRADE TO TOP OF PIPE). EXTRA DEPTH
 TRENCHING, IF REQUIRED, SHALL BE CONSIDERED
 INCIDENTAL TO CONSTRUCTION, THEREFORE, NO SEPARATE
 PAYMENT WILL BE MADE.
- ELECTRONIC MARKER DISCS (EMD) SHALL BE INSTALLED IN ACCORDANCE WITH THE STANDARD SPECIFICATION.
- IN ACCORDANCE WITH SECTION 801 OF THE "STANDARD SPECIFICATIONS", METALIZED DETECTABLE WARNING TAPE SHALL BE INSTALLED 18" ABOVE ALL PVC PIPE INSTALLED ON THIS PROJECT.
- ON THIS PROJECT.

 VALVE STEM AND FIRE HYDRANT EXTENSIONS SHALL BE
 PROVIDED IN ACCORDANCE WITH THE STANDARD
 SPECIFICATIONS. THE COST OF EACH EXTENSION SHALL
 BE CONSIDERED INCIDENTAL TO CONSTRUCTION THEREFORE
 NO SEPARATE PAYMENT WILL BE MADE.
- NO SEPARATE PAINGENT WILL BE CONSIDERED INCIDENTAL TO WATERLINE CONSTRUCTION THEREFORE NO SEPARATE PAYMENT WILL BE MADE.

 JOINT RESTRAINT SHALL BE PROVIDED ON ALL JOINTS OF FIRE HYDRANT LEGS FROM TEE TO FIRE HYDRANT INCLUDING THE TEE AND FIRE HYDRANT.
- INVECTORING THE TEL AND FIRE HTDRANT.

 10. FOR THE PURPOSES OF THIS PROJECT, ALL RESTRAINED JOINTS AND JOINT RESTRAINT SHALL BE MECHANICALLY RESTRAINED. JOINT RESTRAINT LENGTHS SPECIFIED HEREON ARE THE LENGTHS TO BE RESTRAINED EACH SIDE OF THE FITTING.

STREET PAVING IMPROVEMENTS:

- ALL SLOPES AS SHOWN ON PROFILES ARE BASED ON BASELINE STATIONING.
 WHERE REMOVAL OF EXISTING CURB AND CUTTER, SIDEWALK OR PAYEMENT IS REQUIRED, THE CONTRACTOR SHALL SAWCUT AND/OR REMOVE TO THE NEAREST JOINT. CURB AND GUTTER SHOWN AS EXISTING AND NOT TO BE REMOVED UNDER THIS CONTRACT WHICH IS DAMAGED OR DISPLACED BY THE CONTRACTOR ALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT THE CONTRA
- ALL DIMENSIONS OF PAVING AND RETURNS ARE SHOWN TO EDGE OF PAVING.
- EDGE OF PAYING.

 THE PURPOSE OF THIS PLAN SET IS TO PROVIDE PARALLEL PARKING SPACES ALONG THE EAST EDGE OF RIO GRANDE BLYD. N.W.

 THE TEN NEW 10' WIDE PARALLEL PARKING SPACES SHALL BE DELINEATED WITH 4" WIDE STRIPES (WHITE TRAFFIC PAINT). END SPACES SHALL BE 22' IN LENGTH WITH ALL OTHERS 20' IN LENGTH.

WATER VALVE SHUTOFF PLAN NOTES:

- AT A MINIMUM. THE FOLLOWING VALVES SHALL BE CLOSED TO FACILITATE THE PROPOSED NON-PRESSURE CONNECTIONS
- VALVES# 283 & 'A'
- WATER SYSTEMS PERSONNEL SHALL PERFORM ALL SHUTOFFS. THE CONTRACTOR SHALL NOT OPERATE ANY VALVES THAT MAY AFFECT SYSTEM OPERATIONS.
- 3. THE CONTRACTOR SHALL REQUEST A WATER SHUTOFF IN WRITING, A MINIMUM OF SEVEN (7) DAYS IN ADVANCE PER STANDARD CITY OF ALBUQUERQUE PROCEDURES.
- WITHIN FIVE (5) DAYS AFTER RECEIPT OF CONTRACTOR'S SHUTOFF REQUEST, TRIAL SHUTOFFS WILL BE CONDUCTED CITY TO FURNISH LABOR AND APPURTENANCES NECESSAR' TO VERIFY COMPLETENESS OF SHUTOFFS.
- 5. THE CONTRACTOR SHALL HAVE ALL FITTINGS, MATERIALS AND EQUIPMENT ON SITE PRIOR TO VALVE SHUTOFF. ALL EXCAVATION SHALL BE COMPLETE EXCEPT AS NECESSARY TO MAINTAIN SAFE SYSTEM OPERATION WHILE UNDER PRESSURE.
- EXISTING WATER VALVE AND WATER LINE INFORMATION OBTAINED FROM CITY OF ALBUQUERQUE FACILITIES MAP F-13.
- 8. AT THE TIME OF WATER SHUTOFF APPLICATION, THE CONTRACTOR SHALL REQUEST THAT WATER SYSTEMS DIVISION NOTIFY ALL AFFECTED PROPERTY OWNERS AT LEAST TWENTY-FOUR (24) HOURS IN ADVANCE OF THE RESPECTIVE SHUTOFFS.

6. THE CONTRACTOR SHALL PROVIDE ALL RESTRAINTS AND THRUST BLOCKS NECESSARY TO EFFECT ALL NECESSARY SHUTOFFS AND CONNECTIONS.

Date Date

2001.034.3

CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP FLYING STAR - RIO GRANDE RIO GRANDE BLVD. N.W. PARKING IMPROVEMENTS SECTION AND DETAILS TO PORCE APPRECIA

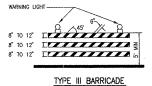
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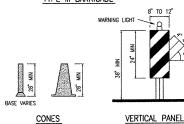
CITY ENGINEER

CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

- CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.
- 2. CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.
- 3. THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL
- CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.
- 5. CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN DEFLICTION SLICH CHANGES REFLECTING SUCH CHANGES.
- 6. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES, TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION PER SECTION 54 4 OF THE MITCH. CONSTRUCTION COORDINATION, PER SECTION 5A 4 OF THE MUTCD, LATEST
- 7. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET—UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR
- 8. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
- 9. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.
- 10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACES APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.
- 11. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
- 12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.
- 13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.
- 14. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE
- 15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.
- 16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.
- 17. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGHS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.
- 18. ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- 19. 48 HOURS PRIOR TO OCCUPYING OR CLOSING OF A RIGHT-OF-WAY. CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITAL, TRANSIT AUTHORITY, BUSINESSES AN/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.
- 20. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.

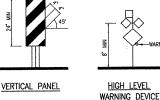
- 21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHES OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.
- 22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING: 1. STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION.
 SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER
- 23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WO WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.
- 24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.
- 25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.
- 26. ADVANCE WARNING SIGNS SHALL BE $36^{\circ}\times36^{\circ}$ min. With super engineering grade sheeting or better. Mounting height at top of sign shall be the same as for a 48° sign as indicated in the m.u.t.c.d.
- 27. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FRFF WORKSITE. ALL GRAFFITI ALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND

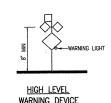




BARREL

TYPE I BARRICADE





LEGEND

DATE DATE DATE DATE

TAKED BY
CCEPTANCE
ELD
RAWINGS
DRRECTED B
DRRECTED B

COA

WORK AREA BARRICADE - TYPE I. TYPE II OR BARREL BARRICADE - TYPE III

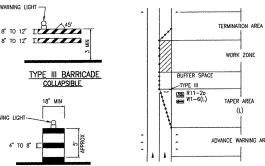
VERTICAL PANEL

D FLAGMAN POSITION

SPACING BETWEEN BARRICADES – A DISTANCE MEASURED IN FEET EQUAL TO THE SPEED LIMIT OF THE STREET

TAPER LENGTH - SEE CHART BELOW

THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH. FOR A GIVEN STREET



ADVANCE WARNING AREA

TRAFFIC CONTROL ELEMENTS

TAPER REQUIREMENTS

SPEED LIMIT	TAPI	ER LENGTH (FEET)	(L)	MINIMUM NUMBER OF	MAXIMUM DEVICE SPACING IN FEET	
(MPH)	10' LANE	11' LANE	12' LANE	DEVICES FOR TAPER	ALONG TAPER	AFTER TAPER
20	70	75	80	5	20	20
25	105	115	125 .	6	25	25
30	150	165	180	7	30	30
35	205	225	245	8	35	35
40	270	295	320	9	40	40
45	450	495	540	13	45	45
50	500	550	600	13	50	50
55	550	605	660	13	55	55

RECOMMENDED SIGN SPACING(D) FOR

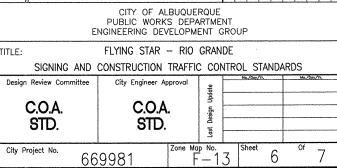
ADVANCE	WARNING SIGN SERIES
SPEED	MINIMUM DISTANCE IN FEET
MILES PER HOUR	BETWEEN FROM LAST SIGNS SIGN TO TAPER
0-20	10 x SPEED LIMIT 10 x SPEED LIMIT
25-30	10 x SPEED LIMIT 10 x SPEED LIMIT
30-35	10 x SPEED LIMIT 10 x SPEED LIMIT
40-45	10 x SPEED LIMIT 10 x SPEED LIMIT
EO EO	10 v SPEED LIMIT 10 v SPEED LIMIT

TAPER CRITERIA

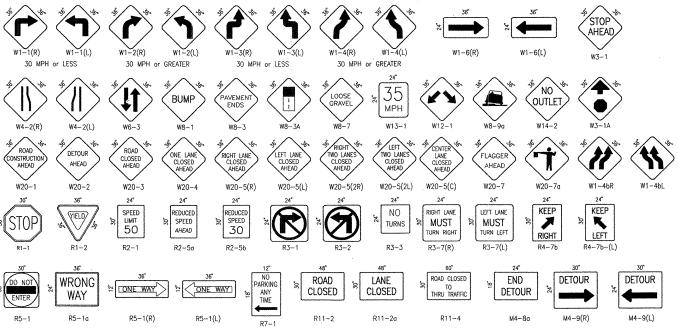
TYPE OF TAPER	TAPER LENGTH
UPSTREAM TAPER	
MERGING TAPER	L MINIMUM
SHIFTING TAPER	1/2 L MINIMUM
SHOULDER TAPER	1/2 L MINIMUM
TWO-WAY TRAFFIC TAPER	100 FEET MAXIMUM
DOWNSTREAM TAPERS	100 FEET PER LANE

TAPER LENGTH COMPUTATION	UN
SPEED LIMIT	
40 MPH OR LESS	WS ²
45 MPH OR GREATER	L = W x S
L = TAPER LENGTH W = WIDTH OF OFFSET IN FEET S = POSTED SPEED OR OFF-PEAK 85 PERCENTILE SPEED IN MPH	

JMA#2001.034.3



SIGN FACE DETAILS



TRAFFIC KEEP LEFT SPECIAL

SPECIAL SPECIAL SIGN

ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A

© DETOUR \ M4-10(R)

M4 - 10(1)

© DETOURI CONSTRUCTION

END

G20-2