SCANNED BY PLANNING

# **CONSTRUCTION PLANS FOR**

# FIVE POINTS ROAD DRAINAGE IMPROVEMENTS (PHASE I)

BERNALILLO COUNTY, NEW MEXICO

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APPROVED AS RECORD DRAWINGS DESIGN REVIEW SECTION CITY CONSTRUCTION ENGINEER

DATE:02-09-2012

TInformation Only-No As Boilt Surrey or Construction Information Provided

ALIE O COUNTY PUBLIC WORKS DIVISION

- 1. THE CONTRACTOR SHALL ABIDE BY ALL LOCAL, STATE, AND FEDERAL LAWS, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS
- 2. ALL ELECTRICAL, TELEPHONE, CABLE TV, GAS AND OTHER UTILITY ALL ELECTRICAL, TELEPHONE, CABLE TV, GAS AND OTHER UTILITY LINES, CABLES, AND APPURTENANCES ENCOUNTERED DURING CONSTRUCTION THAT REQUIRE RELOCATION, SHALL BE COORDINATED WITH THAT UTILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL NECESSARY UTILITY ADJUSTMENTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR DELAYS OR INCCNVENIENCES CAUSED BY UTILITY COMPANY WORK CREWS. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE HIS ACTIVITIES TO ALLOW UTILITY CREWS TO PERFORM THEIR REQUIRED WORK.
- 3 A DISPOSAL SITE FOR ALL EXCESS EXCAVATION MATERIAL, AND A DISPOSAL SITE FOR ALL EXCESS EXCAVATION MATERIAL, AND UNSUITABLE MATERIAL SHALL BE OBTAINED BY THE CONTRACTOR IN COMPLIANCE WITH APPLICABLE ENVIRONMENTAL REGULATIONS AND APPROVED BY THE CONSTRUCTION OBSERVER. ALL COSTS INCURRED IN OBTAINING A DISPOSAL SITE AND HAUL THERETO SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EXISTING UTILITY LINES WITHIN THE CONSTRUCTION AREA. ANY DAMAGE TO EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE AND APPROVED BY THE PROJECT ENGINEER.

- 5. CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT PROPERTIES RESULTING FROM THE CONSTRUCTION PROCESS IS THE RESPONSIBILITY OF THE CONTRACTOR. ANY COSTS INCURRED FOR REPAIRS SHALL BE THE COST OF THE CONTRACTOR.
- 6. OVERNIGHT PARKING OF CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS OR DESIGNATED TRAFFIC LANES. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL WITHIN THE PUBLIC RIGHT OF WAY.
- THE CONTRACTOR SHALL OBTAIN ALL THE NECESSARY PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION (I.E. BARRICADING, SURFACE DISTURBANCE)
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE AT HIS EXPENSE ANY AND ALL PROPERTY CORNERS DESTROYED DURING CONSTRUCTION. ALL PROPERTY CORNERS MUST BE RESET BY A PROFESSIONAL SURVEYOR.
- ALL PERMANENT PAVEMENT MARKING AND TRAFFIC SIGNING SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR PER PLAN.
- 10. THE CONTRACTOR SHALL FOLLOW THE CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN PROVIDED HEREIN. THE CONTRACTOR SHALL COORDINATE WITH THE BERNALILLO COUNTY PUBLIC WORKS DIVISION TRAFFIC ENGINEER PRIOR TO BEGINNING ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.

- 11. ALL BARRICADES AND CONSTRUCTION SIGNING SHALL CONFORM TO APPLICABLE SECTIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), U.S. DEPARTMENT OF TRANSPORTATION, LATEST EDITION.
- 12. THE CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION BARRICADES AND SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
- 13. ALL SAWOUT PAVEMENT SHALL HAVE A UNIFORM EDGE AND BE SPRAYED WITH TACK.
- 14. WHEN ABUTTING NEW CURB AND GUTTER TO EXISTING PAVEMENT, A 1' WIDE SECTION OF EXISTING PAVEMENT ADJACENT TO THE CL AND GUTTER SHALL BE SAWCUT, REMOVED, AND REPLACED PER THE STANDARD SPECIFICATIONS.
- 15. THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENTS, PAVEMENT MARKINGS, CURB & GUTTER, DRIVE PADS, WHEELCHAIR RAMPS, AND SIDEWALK DURING CONSTRUCTION, APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS AND SHALL REPAIR OR REPLACE PER BERNALILLO COUNTY STANDARDS, AT HIS OWN EXPENSE.
- 16. CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THE TRAFFIC CONTROL STANDARD DETAILS AND PLAN ON SHEETS 11, 12 AND 13.

17. THE CONTRACTOR SHALL REPLACE IN KIND ANY ADJACENT PROPERTY OWNER LANDSCAPING DISTURBED DURING CONSTRUCTION. COST SHALL BE INCIDENTAL TO THE PROJECT.

#### NOTICE TO CONTRACTORS

- . ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH NEW MEXICO STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION 2000 EDITION AS REVISED FOR THIS PROJECT.
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL DESTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR SURVEYOR IMMEDIATELY SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONST. COORDINATION DIVISION. CONTRACTOR SHALL NOTICE DEFINITION TRAFFIC ENGINEERING (848—1540) PRIOR TO OCCUPYING AN INTERSECTION.
- 5. ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED W/ PLASTIC REFLECTORIZED PAVEMENT MARKING BY CONTRACTOR TO LOCATION AS EXISTING OR AS INDICATED BY THIS PLAN SET.

#### THE FOLLOWING NOTES ALSO APPLY WHEN CHECKED

- $\fbox{X}$  all utilities and utility service lines shall be installed prior to paving.
- BACKFILL COMPACTION SHALL BE ACCORDING TO SPECIFIED STREET USE.
- TACK COAT REQUIREMENTS SHALL BE DETERMINED BY ENGINEER.
- SIDEWALKS AND WHEELCHAIR RAMPS WITHIN THE CURB RETURNS SHALL BE CONSTRUCTED WHEREVER A NEW CURB RETURN IS CONSTRUCTED.
- IX IF CURB IS DEPRESSED FOR A DRIVEPAD, THE DRIVEPAD SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB AND GUTTER. X ALL STORM DRAINAGE FACILITIES SHALL BE COMPLETED PRIOR TO FINAL ACCEPTANCE.
- THE REQUESTOR OR DEVELOPER SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT OF ALL CURB AND GUTTER OR SIDEWALK DAMAGED AFTER APPROVAL BY THE CITY ENGINEER OF WORK COMPLETED BY THE CONTRACTOR.

REV. SHEETS CITY ENGINEER DATE USER DEPARTMENT DATE USER DEPARTMENT DATE APPROVALS ENGINEER DATE \*\*\*\*\*\*\*\*\*\*



# **Bohannan** ▲ **Huston** ⊭

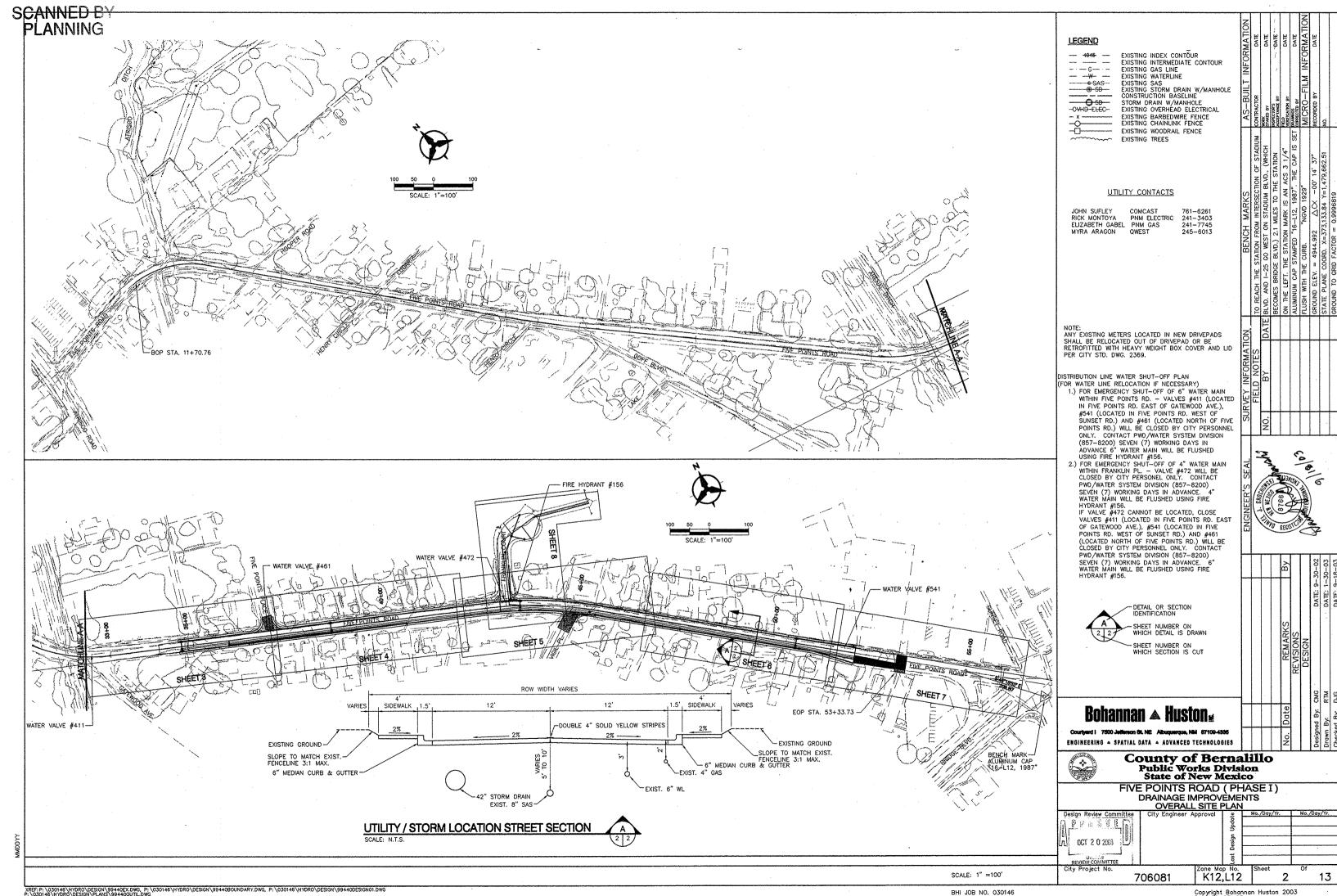
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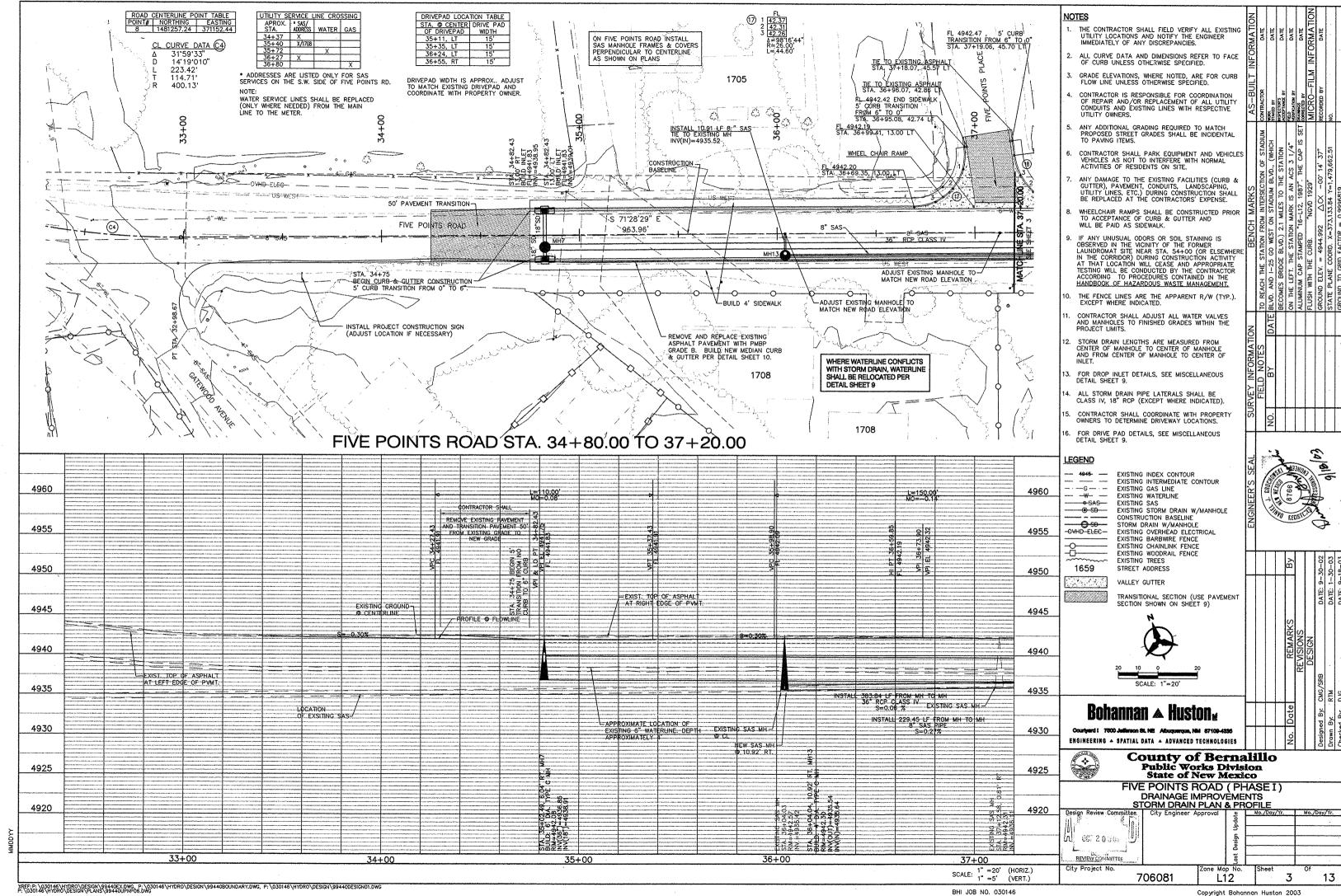
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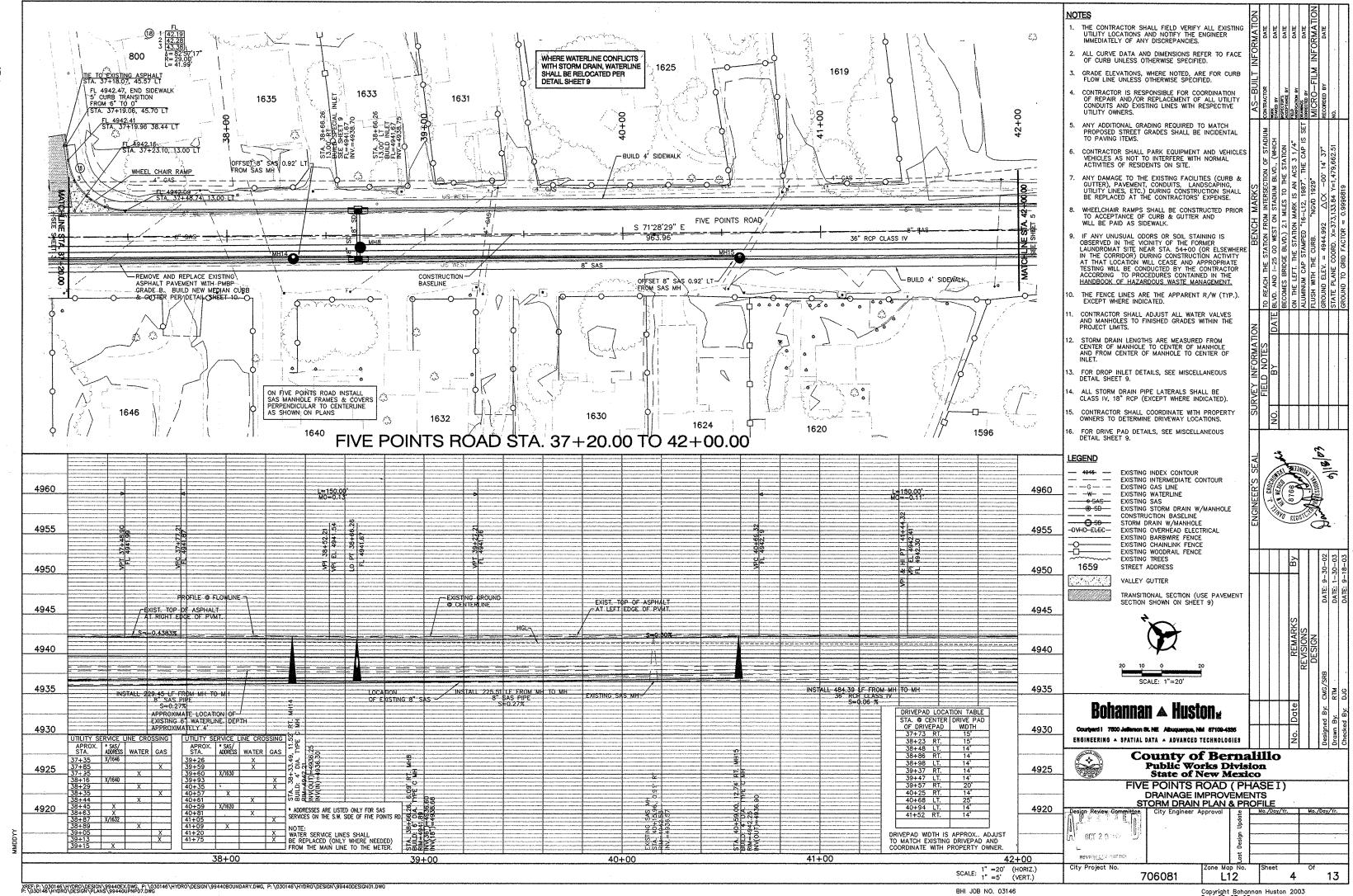
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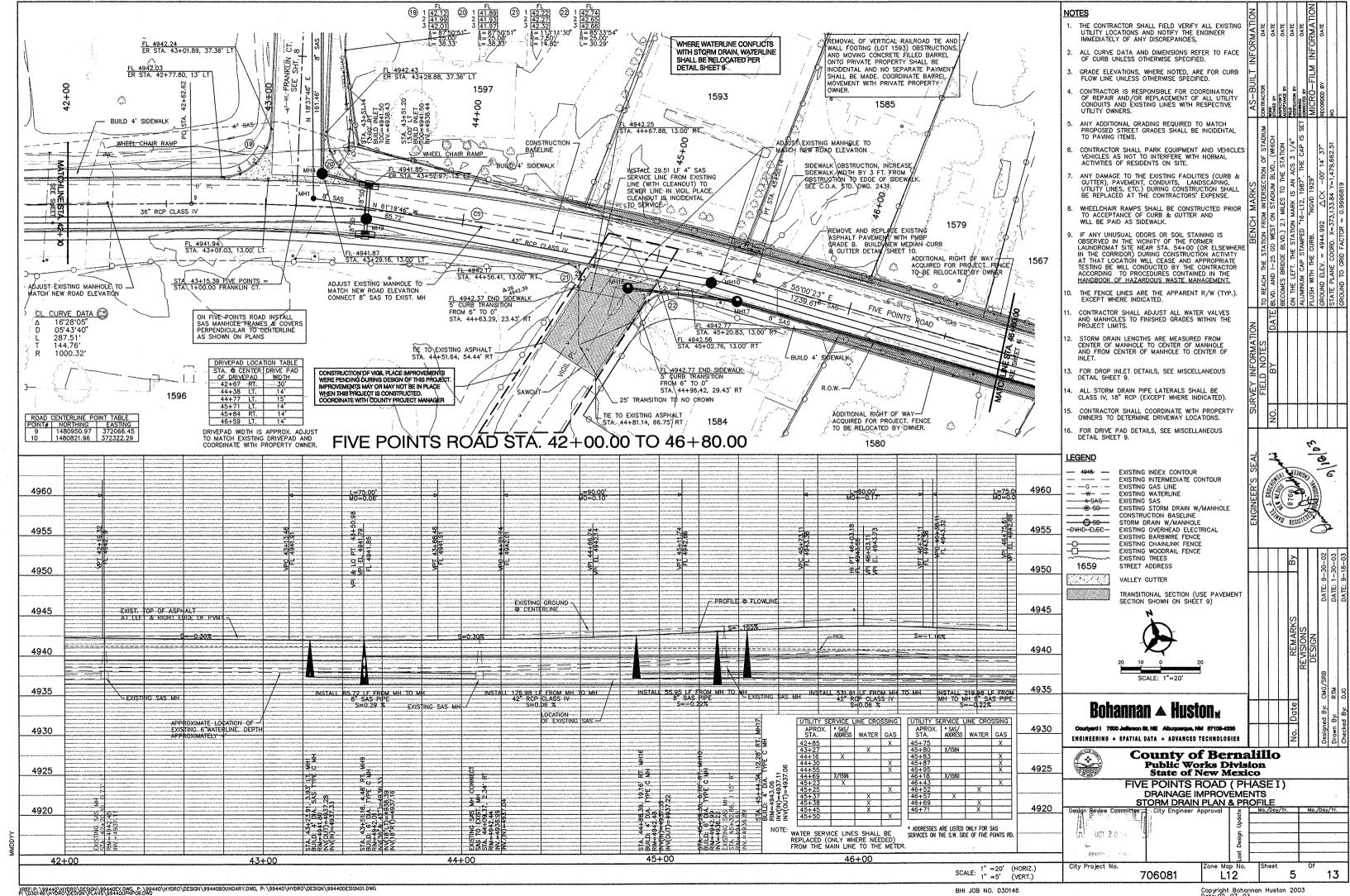
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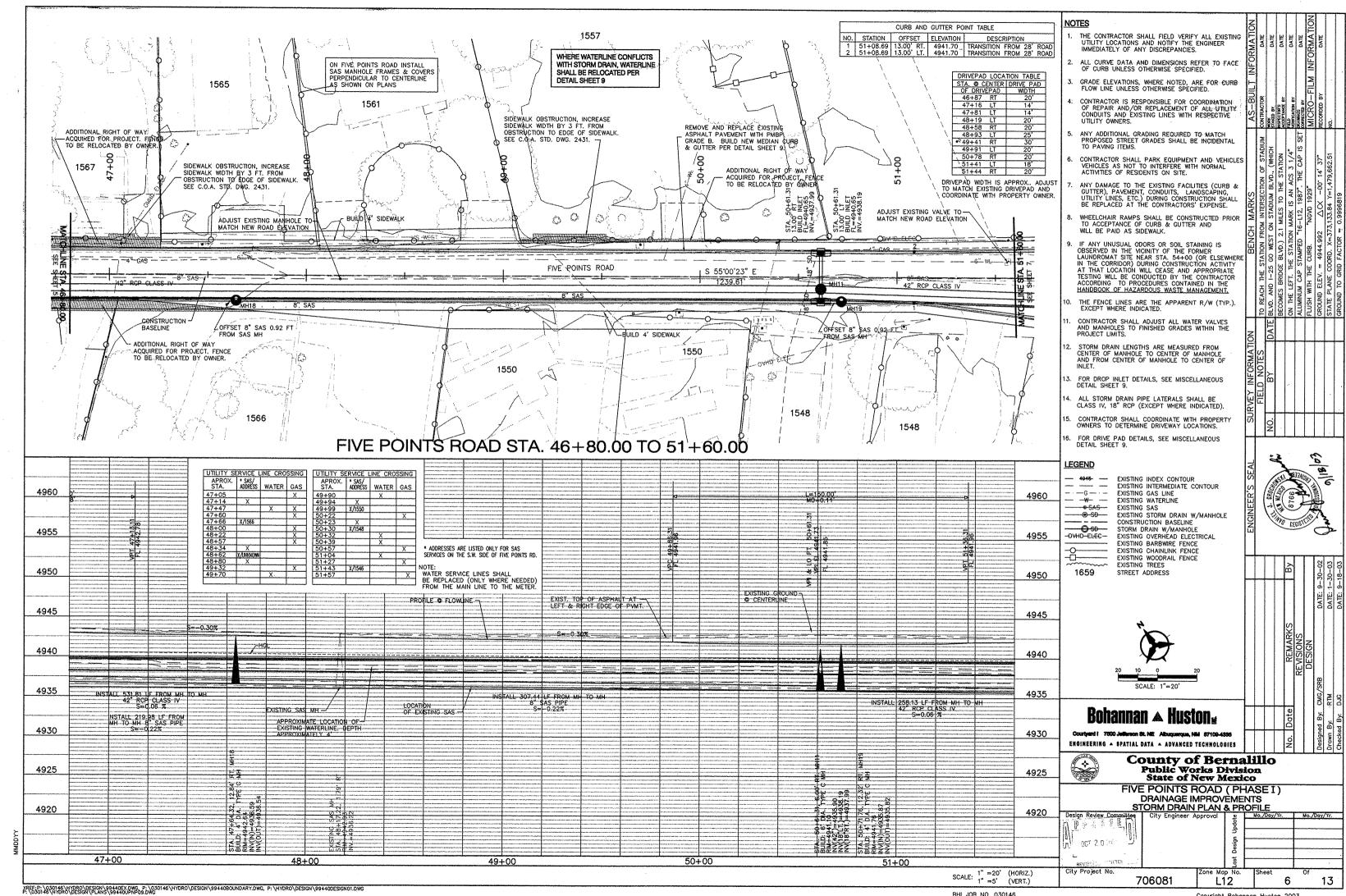
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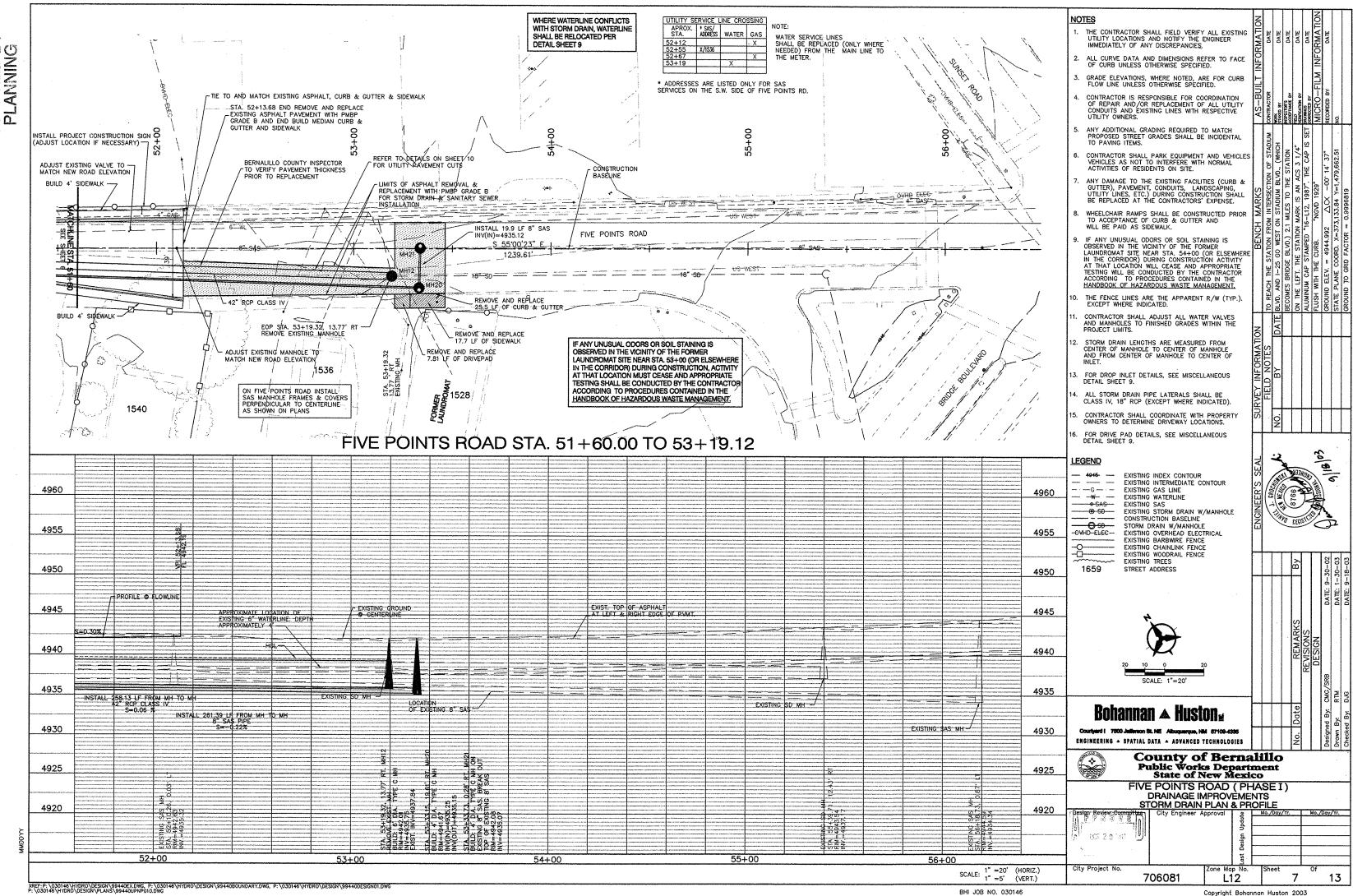


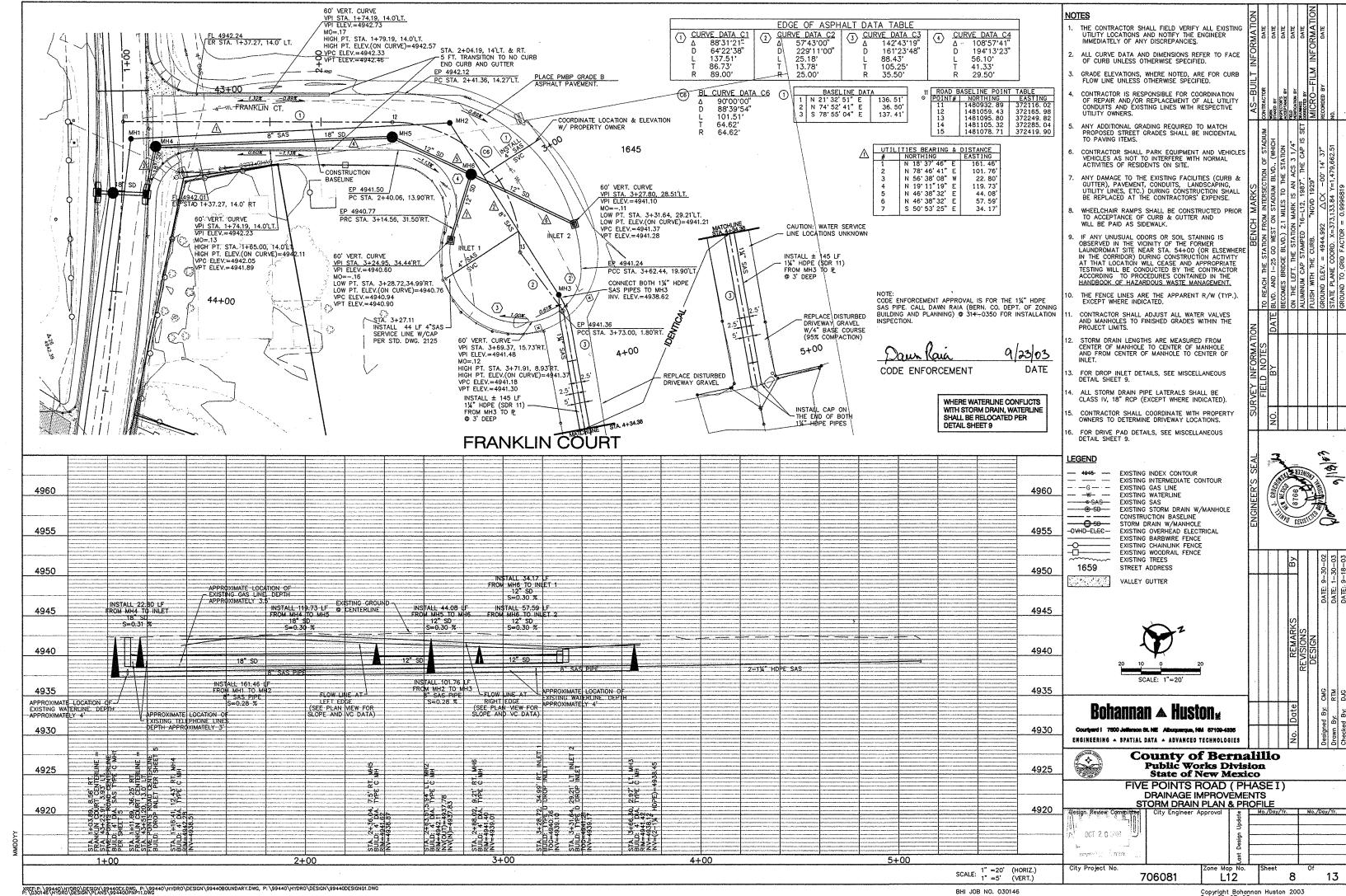


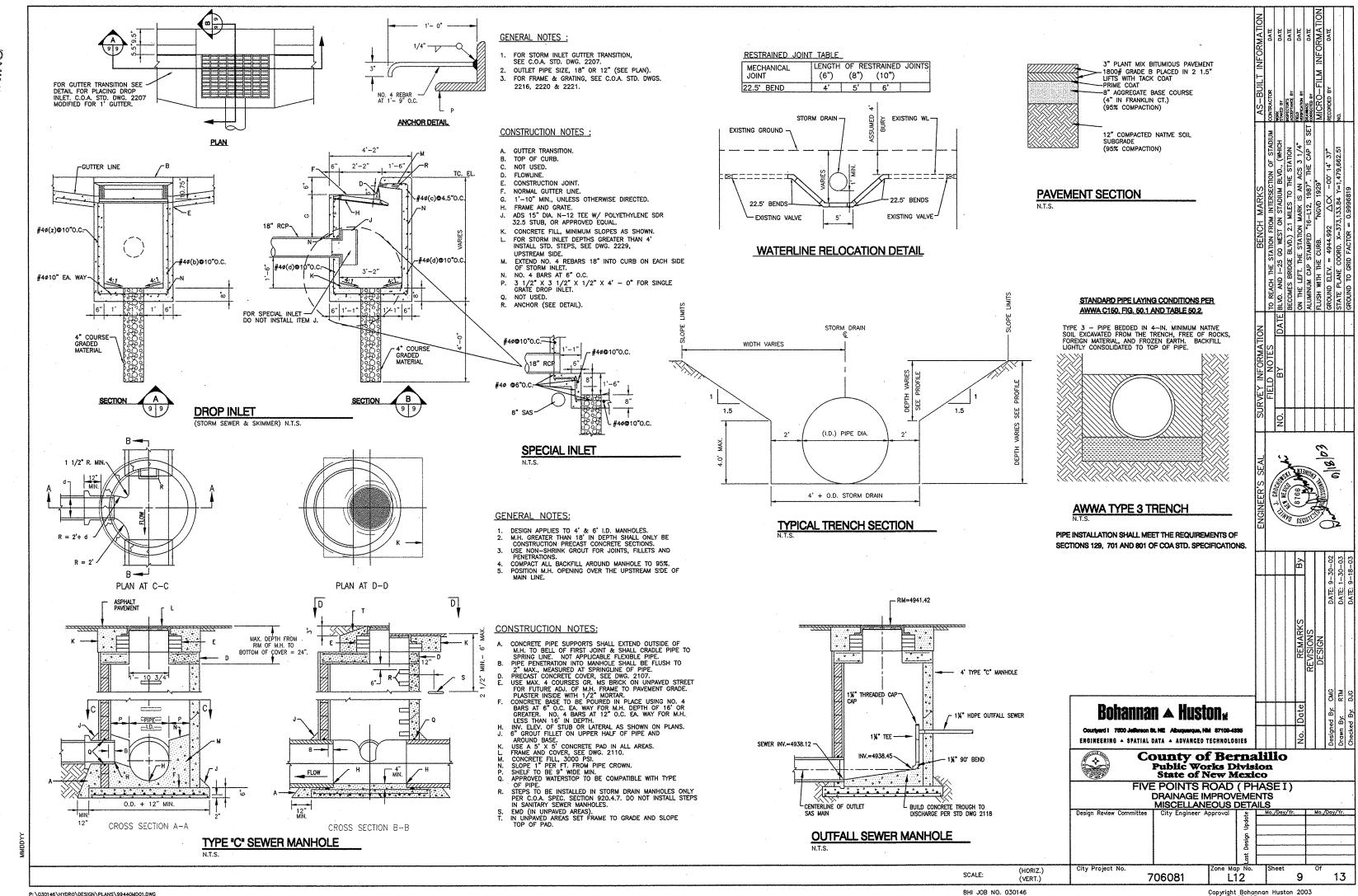


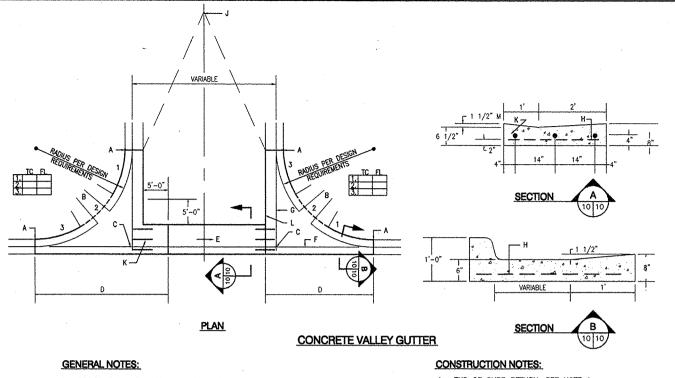












- DESIGN ELEVATIONS TO BE GIVEN AT EACH END OF THE CURB RETUREN (TOP OF CURB ELEV.) AND AT INTERSECTIONS OF PROJECTED FLOWLINES (FLOWLINE ELEV.).
- ON UPSTREAM AND DOWNSTREAM ENDS OF THE INTERSECTION, VALLEY GUTTER CONSTRUCTION SHALL EXTEND TO THE END OF THE RETURNS.
- 3. THE VALLEY GUTTER TO BE REINFORCED WITH 6"x6"x NO.6 GA. WIRE MESH.
- INVERT OF VALLEY GUTTER TO EXTEND FROM FLOWLINE OF UPSTREAM CURB RETURN TO FLOWLINE OF DOWNSTREAM CURB RETURN.
- CURB FLOWLINE AND TOP OF CURB ELEY. SHOWN IN THE BOX CORRESPOND TO QUARTERPOINTS INDICATED ON THE CURB RETURN IN THE CLOCKWISE DIRECTION.
- DENOTES 1/2" EXPANSION JOINT.
- FOR NEW CONSTRUCTION, VALLEY GUTTER SHALL BE CONSTRUCTED PRIOR TO ADJACENT PAVEMENT. ASPHALT CONC. SHALL BE INSTALLED MONOLITHICALLY TO MEET NEW VALLEY GUTTER.
- PRIOR TO CONSTRUCTION OF NEW VALLEY GUTTER ON EXISTING ACCEPTED STREETS, PAVEMENT SHALL BE REMOVED AS SHOWN ON PLANS.

- A. END OF CURB RETURN, SEE NOTE 1.
- B. FOR RAMP SEE DETAILS THIS SHEET.
- C. INTERSECTION OF FLOWLINES, SEE NOTE 1

- G. PROJECTED FLOWLINE OF 1 1/2" INVERT, SEE NOTE 2.
- BEGIN CROWN WARP TO STRAIGHT SECTION WHERE SPECIFIED ON PLANS, OR INDICATED BY THE ENGR.
- K. NO. 4 BARS 3'-0" LONG AT 16" O.C.
- L. ALTERNATE B. NO FILLET AS PER PLANS.
- THE 1 1/2" INVERT DEPTH MAY BE REDUCED TO IMPROVE RIDEABILITY WITH APPROVAL OF ENGINEER.

# BACK OF SIDEWALK AT CASE I MINIMAL R.O.W. FOR SIDEWALK AT BACK OF CURB



#### **GENERAL NOTES:**

- CURB ACCESS RAMPS ARE NORMALLY TO BE LOCATED AT THE CENTER OF THE RETURN OR AS DIRECTED BY THE COUNTY TRAFFIC ENGINEER.
- SURFACE TEXTURE OF CURB ASSESS RAMPS SHALL BE OBTAINED BY HEAVY BROOMING. (TEXTURE DEPTH .035"), TRANSVERSE TO THE SLOPE OF THE RAMP.
- GUTTER FLOW-LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP. DRAINAGE CATCH BASIN STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS.
- WIDTH OF SIDEWALK AND RAMP MUST BE MAINTAINED AT A MINIMUM OF 5'-0" THROUGH ENTIRE RAMP LENGTH.

#### **CONSTRUCTION NOTES:**

- A. SLOPE OF GUTTER DEPENDENT ON REQUIREMENTS FOR VALLEY GUTTER.
- 1/4" LIP ABOVE FLOW LINE (VERTICAL). SEE CURB AT FLOWLINE DETAIL.
- C. CURB & GUTTER. (SEE GUTTER AT WHEELCHAIR CURB ACCESS RAMP).
- 1/2" EXPANSION JOINT.
- 12:1 SLOPE AT FACE OF CURB
- G. CONTRACTION (DUMMY) JOINT.
- VARIES WITH AVAILABLE R.O.W.
- HEADER CURB, SEE DETAIL BELOW.

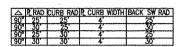
- BACK OF SIDEWALK.

  BACK OF SIDEWALK.

  BACK OF SIDEWALK RADIUS TO BE ESTABLISHED SO AS TO MAINTAIN A 5'-0" RAMP WIDTH

  (MINIMUM) OR MORE THROUGHOUT. SEE C.O.A. STD. DWG 2440 (NOTE S) IF LESS THAN

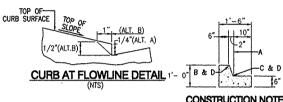
  5'-0" IS AVAILABLE DUE TO UNTIMELY UNRESOLVABLE CONSTRAINTS.
- 4 1/2" MAY
- 12:1 SLOPE MAXIMUM ON RAMP.
- ANY PRIVATE LANDSCAPING AND OR IRRIGATION SHALL BE RESTORED TO ORIGINAL CONDITION. SHOULD ANY PRIVATE IMPROVEMENT NEED TO BE REMOVED, OWNER MUST BE NOTIFIED.



1 1/2"-

**HEADER CURB** 

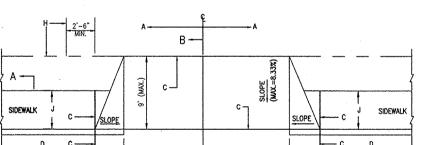
ALT, A-1/4" VERT, RISE THEN SLOPE UP TO BACK OF CURB (SEE TYPICAL LOCATION OF SIDEWALKS AND RAMPS BELOW)
ALT. B-1/2" RISE 1" HORIZ. THEN SLOPE UP TO BACK OF CURB. (SEE TYPICAL LOCATION OF SIDEWALKS AND RAMPS BELOW)



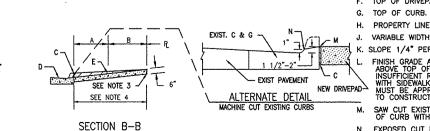
### **CONSTRUCTION NOTES:**

THEORETICAL FACE OF CURB OR FLOWLINE. 3/4" RADIUS.

MEDIAN CURB & GUTTER



PLAN NOMINAL WIDTH **ELEVATION** 



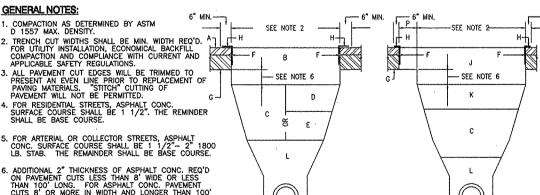
PAVING DRIVEPADS

#### **GENERAL NOTES:**

- USE 1/2" EXP. JT. WHERE SIDEWALK OR DRIVEPAD ABUTS BLDGS., FENCES, WALLS, OR OTHER IMMOVABLE OBJECTS.
- DRIVEPADS WIDER THAN 18' (NOMINAL) SHALL HAVE 1/2" EXP. JT. AT MIDPOINT DRIVEPADS WIDER THAN 36' SHALL HAVE 1/2" EXP. JT. 18' MAX. BETWEEN JT. EQUALLY SPACED.
- CURB TYPE SIDEWALK SHALL BE USED ONLY WHEN VARIANCE IS APPROVED.
- FOR SIDEWALK WIDTH, SEE SHEET 2.
- SUBGRADE UNDER SIDEWALK & DRIVEPAD SHALL BE COMPACTED TO 90% MAX. DENSITY TO A DEPTH OF 6'.

## CONSTRUCTION NOTES:

- A. CURB TYPE SIDEWALK.
- C. 1/2" EXP. JT. ADJUST TO FIELD CONDITIONS ON REPLACEMENT WORK.
- D. CURB & GUTTER.
- E. SLOPE TO BE ADJUSTED TO PROVIDE A UNIFORM TRANSITION BETWEEN SIDEWALK & DRIVEPAD.
- F. TOP OF DRIVEPAD.
- G. TOP OF CURB.
- VARIABLE WIDTH.
- K. SLOPE 1/4" PER FT.
- SAW CUT EXISTING CONCRETE FROM BACKSIDE OF CURB WITH SLOPE TOWARD FLOWLINE.
- N. EXPOSED CUT EDGES TO BE GROUND SMOOTH/ ROUNDED TO REMOVE SHARP EDGE.



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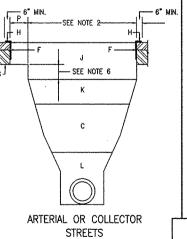
CONCRETE PAVEMENT

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6. ADDITIONAL 2" THICKNESS OF ASPHALT CONC. REQ'D ON PAVEMENT CUTS LESS THAN 8' WIDE OR LESS THAN 100' LONG. FOR ASPHALT CONC. PAVEMENT CUTS 8' OR MORE IN WIDTH AND LONGER THAN 100' SHALL BE PLACED WITH LAYDOWN MACHINE TO A DEPTH EQUAL TO THAT OF ASPHALT CONC. REMOVED OR TO THE DEPTH SHOWN ON DWG. 2405, 2407 & 2408 FOR APPROPRIATE PAVEMENT SECTION, WHICH EVER IS GREATER. RESIDENTIAL STREETS CONSTRUCTION NOTES: - SEE NOTE 2 -

- A. 2"- 4" EXIST. ASPHALT SURFACE COURSE.
  B. 4"- 6" ASPHALT CONC. TO BE 2" THICKER
  THAN EXIST. PAVEMENT, SEE NOTES 4 & 6.
  C. 12" COMPACTED SUBBASE MATERIAL, PLACED
  IN 2"- 6" LIFTS, 95% COMPACTION.
  D. 6" SUBBASE MATERIAL, 95% COMPACTION.
- D. 6" SUBBASE MATERIAL, 95% COMPACTION.
  E. 6" SUBGRADE MATERIAL, 95% COMPACTION.
  F. SAWCUT OR BLADECUT ASPHALT PAVEMENT. SAWCUT ONLY ONE THIRD CONC. PAVEMENT DEPTH.
  G. EXIST. ASPHALT PAVEMENT.
  H. TACK COAT.
  J. 4"— 6" ASPHALT CONC. TO BE 2" THICKER THAN EXIST. PAVEMENT. SEE NOTES 5 & 6.
  K. 8" BASECOURSE 95% COMPACTION.
  L. SUBGRADE MATERIAL, 90% COMPACTION.
  M. EXIST. CONC. PAVEMENT.
  N. JOINTS TO BE TOOLED & SEALED IN ACCORDANCE WITH ENGINEER'S REQUIREMENTS.
  C. TO MATCH EXIST. THICKNESS. 6" MIN. 4000 P.S.I.

- O. TO MATCH EXIST. THICKNESS, 6" MIN. 4000 P.S.I.
  P. 12" CUT-BACK, ONLY APPLICABLE TO FED. HWY. AND
  NMSHD FUNDED PROJECTS.



**PAVEMENT CUTS** FOR ALL UTILITES **Bohannan** ▲ **Huston** ⊭

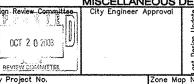
CURB & GUTTER TAPER DETAIL

TO NO CURB

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ENGINEERING . SPATIAL DATA . ADVANCED TECHNOLOGIES County of Bernalillo Public Works Division State of New Mexico

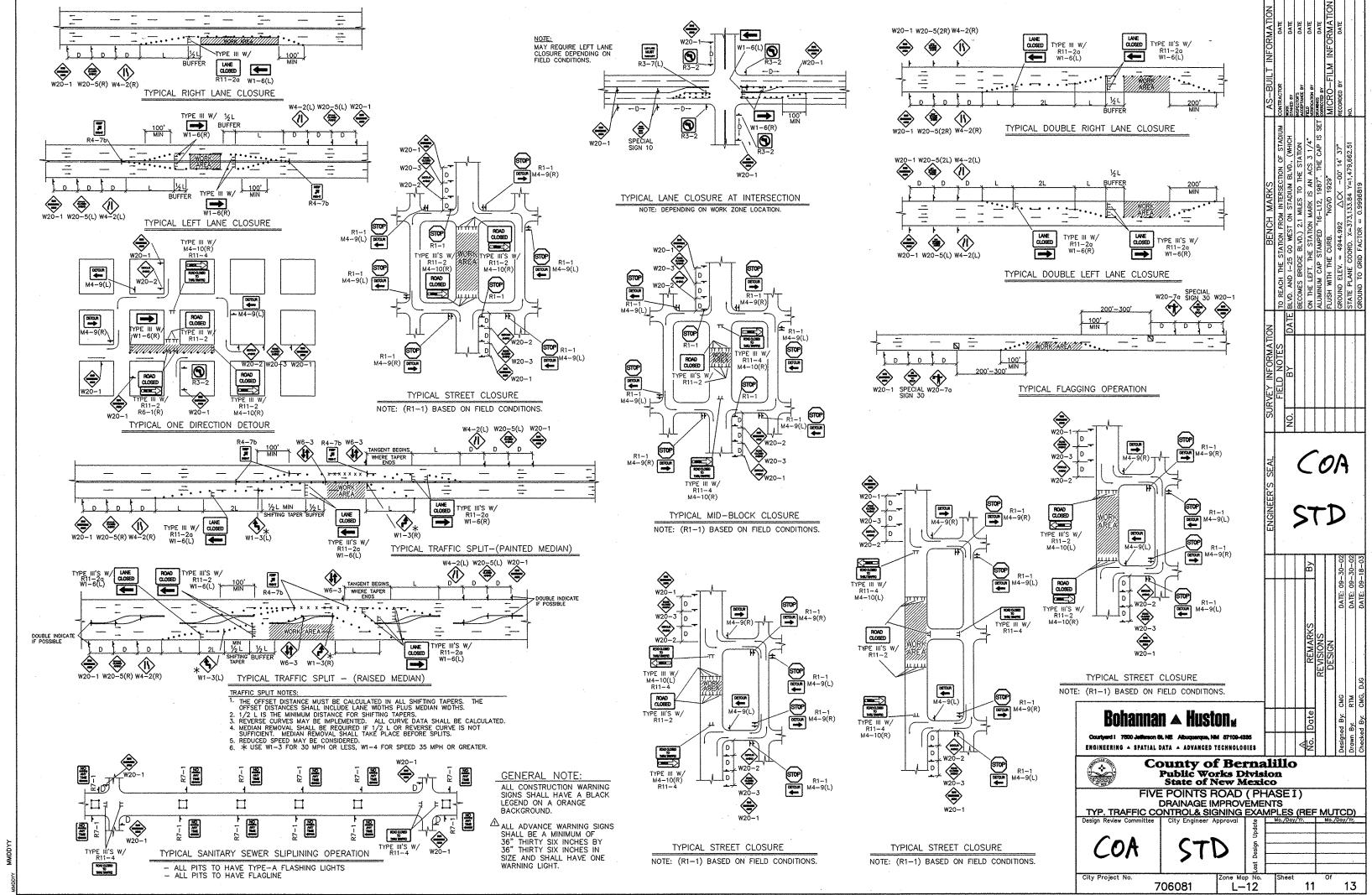
FIVE POINTS ROAD (PHASE I) DRAINAGE IMPROVEMENTS MISCELLANEOUS DETAILS



706081 L12 Copyright Bohannan Huston 2003 DATE: 02-07-03

SEE NOTE

SECTION A-A



#### CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

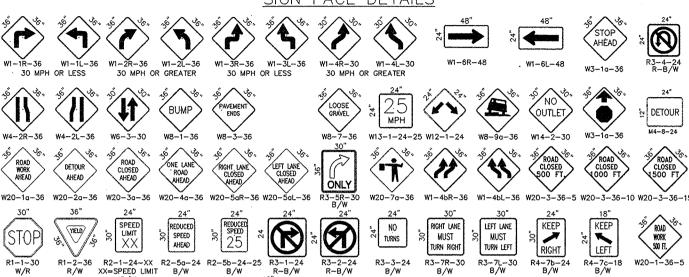
- 1. PROJECT IS SUBJECT TO THE REQUIREMENTS OF COUNTY ORDINANCE NO. 95–12 AND COUNTY ORDINANCE NO. 96–3. THE CONTRACTOR IS RESPONSIBLE FOR ACQUIRING ALL PERMITS NECESSARY. CONTRACTOR SHALL CALL 848-1504 FOR
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE COUNTY TRAFFIC CONTROL ADMINISTRATOR AT LEAST 72 HOURS PRIOR TO COMMENCEMENT OF WORK. TRAFFIC CONTROL PLANS SHALL BE PREPARED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION AND BY CERTIFIED PERSONNEL AND MUST BE APPROVED PRIOR TO COMMENCEMENT OF WORK ON THE PROJECT. CALL 848-1504 FOR FURTHER INFORMATION.
- 3. A DAILY TRAFFIC CONTROL LOG SHALL BE MAINTAINED BY THE CONTRACTOR REFLECTING THE TYPES AND LOCATIONS OF ALL TRAFFIC CONTROL DEVICES, SIGNS, BARRICADES, ETC. BEING USED ON THE PROJECT IN COMPLIANCE WITH THE APPROVED TRAFFIC CONTROL PLAN. THIS LOG SHALL BE SUBMITTED TO THE BERNALILLO COUNTY TRAFFIC CONTROL ADMINISTRATOR UPON COMPLETION OF THE PROJECT CONSTRUCTION OR WHEN SO REQUESTED BY THE PROJECT MANAGER OR THE BERNALILLO COUNTY TRAFFIC CONTROL ADMINISTRATOR
- 4. CONTRACTOR SHALL PROVIDE A WEEKLY CONSTRUCTION ACTIVITY SCHEDULE TO BERNALILLO COUNTY PUBLIC WORKS DIVISION ATTN: PROJECT MANAGER
- 5. THE CONTRACTOR SHALL FILE A WEEKLY WRITTEN REPORT TO THE TRAFFIC CONTROL ADMINISTRATOR DESCRIBING THE TRAFFIC CONTROL PLAN THAT WILL BE IN EFFECT FOR THE FOLLOWING WEEK.
- SUBJECT TO THE APPROVED TRAFFIC CONTROL PLAN, AT LEAST ONE LANE SHALL BE OPEN TO TRAFFIC AT ALL TIMES. CONTRACTOR SHALL PROVIDE PROPER SIGNAGE AND FLAGMAN AND SHALL MAINTAIN THE TRAFFIC LANE IN SUCH A MANNER AS TO ASSURE PROPER SAFETY TO THE TRAVELING PUBLIC AT ALL TIMES, EXCEPT WHEN GRADING, EXCAVATION AND BACKFILL OPERATIONS ARE BEING CONDUCTED IMMEDIATELY IN FRONT OF THE PROPERTY, IN WHICH CASE ACCESS WILL NOT BE DENIED FOR MORE THAN 4 HOURS WITHOUT APPROVAL BY THE COUNTY.
- 7. TRAFFIC LANES PROVIDED DURING CONSTRUCTION SHALL BE MAINTAINED, IN SUCH A CONDITION UNDER ALL WEATHER CONDITIONS, SO AS TO PERMIT THE REASONABLE PASSAGE OF PASSENGER VEHICLES, AND SHALL BE KEPT GRADED AND SMOOTH, AND WATERED SEVERAL TIMES DAILY TO CONTROL DUST.
- TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.
- CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY BERNALILLO COUNTY TRAFFIC
- 10. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF THE COUNTY TRAFFIC CONTROL ADMINISTRATOR, PER SECTION 6A-4 OF THE MUTCD,
- 11. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET—UP FOR EACH PHASE SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORKSITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR
- 12. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
- 13. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.

- 14. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY BERNALILLO COUNTY TRAFFIC ENGINEERING PER MUTCD SECTION 6A-4.
- 15. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
- 16. FOUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF BERNALILLO COUNTY TRAFFIC ENGINEERING.
- 17. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.
- 18. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.
- 19. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.
- 20. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF BERNALILLO COUNTY TRAFFIC ENGINEERING. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE LETTERING ON BLUE BACKGROUND. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.
- 48 HOURS PRIOR TO OCCUPYING OR CLOSING OF A RIGHT-OF-WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.
- 22. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY BERNALILLO COUNTY TRAFFIC ENGINEERING.
- 23. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET, SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.

#### SPECIAL PROJECT TRAFFIC CONTROL NOTES

- 1. CONTRACTOR TO DEFINE CENTERLINE OF ROADWAY WITHIN THE CONSTRUCTION LIMITS, BUT OUTSIDE OF THE CURRENT EXCAVATION LIMITS, WITHIN VERTICAL PANELS. THESE VERTICAL PANELS WILL GUIDE LOCAL TRAFFIC THROUGH THE
- 2. AT INTERVALS OF 200 FEET, CONTRACTOR SHALL PLACE A TYPE "H" DRUM WITH "KEEP RIGHT" SIGN ATTACHED TO POSITIVELY DELINEATE THE CENTERLINE.

#### SIGN FACE DETAILS



ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.

WRONG

WAY

CONE WAY

DO NOT ENTER



ONE WAY



ROAD

CLÓSED

LANE

CLOSED

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ROAD CLOSED

THRU TRAFFIC

END

DETOUR

END ROAD WORK

DETOUR

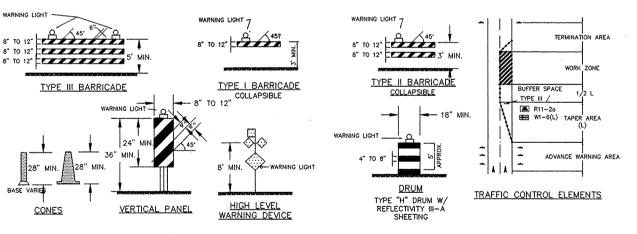
M4-9R-30



**DETOUR** 

M4-91-30





#### **LEGEND**

WORK AREA

- BARRICADE TYPE I, TYPE II, OR TYPE "H" DRUM W/ REFLECTIVITY III-A SHEETING
- BARRICADE TYPE III
- VERTICAL PANEL
- WARNING SIGN
- DISTANCE BETWEEN SIGNS A DISTANCE MEASURED IN FEET EQUAL
- TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET
- D FLAGMAN POSITION
- SPACING BETWEEN BARRICADES- A DISTANCE MEASURED IN FEET
- EQUAL TO THE SPEED LIMIT OF THE STREET TAPER LENGTH - SEE CHART BELOW
- THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

Bohannan 🛦 Huston,

ENGINEERING . SPATIAL DATA . ADVANCED TECHNOLOGIES

## County of Bernalillo Public Works Division State of New Mexico

706081

FIVE POINTS ROAD (PHASE I) DRAINAGE IMPROVEMENTS SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS

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