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26-7060181-2012

SCANNED BY
PLANNING

CONSTRUCTION PLANS FOR FIVE POINTS ROAD DRAINAGE IMPROVEMENTS (PHASE I)

BERNALILLO COUNTY, NEW MEXICO

INDEX

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4	PLAN & PROFILE STA. 37+20.00 TO STA. 42+00.00
5	PLAN & PROFILE STA. 42+00.00 TO STA. 46+80.00
6	PLAN & PROFILE STA. 46+80.00 TO STA. 51+60.00
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8	FRANKLIN COURT PLAN & PROFILE
9	MISCELLANEOUS DETAILS
10	MISCELLANEOUS DETAILS
11	TRAFFIC CONTROL
12	TRAFFIC CONTROL
13	TRAFFIC CONTROL

APPROVED AS RECORD DRAWINGS[†]
DESIGN REVIEW SECTION
CITY CONSTRUCTION ENGINEER
Paul Olsen
DATE: 02-09-2012

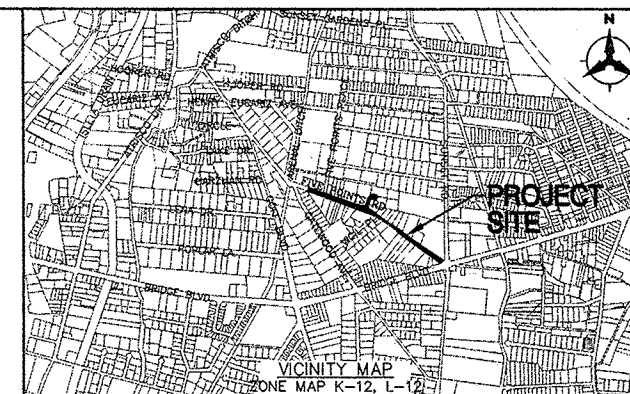
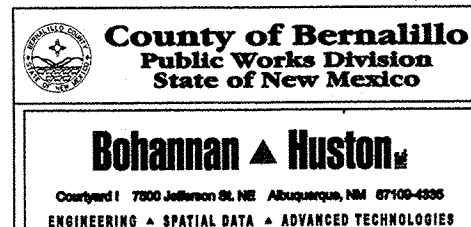
*† Information Only - No As Built
Survey or Construction Information
Provided*

TZ Paul
BERNALILLO COUNTY PUBLIC
WORKS DIVISION

10/1/03
DATE

CONSTRUCTION NOTES:

- THE CONTRACTOR SHALL ABIDE BY ALL LOCAL, STATE, AND FEDERAL LAWS, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS.
- ALL ELECTRICAL, TELEPHONE, CABLE TV, GAS AND OTHER UTILITY LINES, CABLES, AND APPURTENANCES ENCOUNTERED DURING CONSTRUCTION THAT REQUIRE RELOCATION, SHALL BE COORDINATED WITH THAT UTILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL NECESSARY UTILITY ADJUSTMENTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR DELAYS OR INCONVENIENCES CAUSED BY UTILITY COMPANY WORK CREWS. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE HIS ACTIVITIES TO ALLOW UTILITY CREWS TO PERFORM THEIR REQUIRED WORK.
- A DISPOSAL SITE FOR ALL EXCESS EXCAVATION MATERIAL, AND UNSUITABLE MATERIAL SHALL BE OBTAINED BY THE CONTRACTOR IN COMPLIANCE WITH APPLICABLE ENVIRONMENTAL REGULATIONS AND APPROVED BY THE CONSTRUCTION OBSERVER. ALL COSTS INCURRED IN OBTAINING A DISPOSAL SITE AND HAUL THERETO SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.
- THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EXISTING UTILITY LINES WITHIN THE CONSTRUCTION AREA. ANY DAMAGE TO EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE AND APPROVED BY THE PROJECT ENGINEER.
- CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT PROPERTIES RESULTING FROM THE CONSTRUCTION PROCESS IS THE RESPONSIBILITY OF THE CONTRACTOR. ANY COSTS INCURRED FOR REPAIRS SHALL BE THE COST OF THE CONTRACTOR.
- OVERNIGHT PARKING OF CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS OR DESIGNATED TRAFFIC LANES. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL WITHIN THE PUBLIC RIGHT OF WAY.
- THE CONTRACTOR SHALL OBTAIN ALL THE NECESSARY PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION (I.E. BARRICADING, SURFACE DISTURBANCE).
- THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE AT HIS EXPENSE ANY AND ALL PROPERTY CORNERS DESTROYED DURING CONSTRUCTION. ALL PROPERTY CORNERS MUST BE RESET BY A PROFESSIONAL SURVEYOR.
- ALL PERMANENT PAVEMENT MARKING AND TRAFFIC SIGNING SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR PER PLAN.
- THE CONTRACTOR SHALL FOLLOW THE CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN PROVIDED HEREIN. THE CONTRACTOR SHALL COORDINATE WITH THE BERNALILLO COUNTY PUBLIC WORKS DIVISION TRAFFIC ENGINEER PRIOR TO BEGINNING ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.
- ALL BARRICADES AND CONSTRUCTION SIGNING SHALL CONFORM TO APPLICABLE SECTIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), U.S. DEPARTMENT OF TRANSPORTATION, LATEST EDITION.
- THE CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION BARRICADES AND SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
- ALL SAWCUT PAVEMENT SHALL HAVE A UNIFORM EDGE AND BE SPRAYED WITH TACK.
- WHEN ABUTTING NEW CURB AND GUTTER TO EXISTING PAVEMENT, A 1" WIDE SECTION OF EXISTING PAVEMENT ADJACENT TO THE CURB AND GUTTER SHALL BE SAWCUT, REMOVED, AND REPLACED PER THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENTS, PAVEMENT MARKINGS, CURB & GUTTER, DRIVE PADS, WHEELCHAIR RAMPS, AND SIDEWALK DURING CONSTRUCTION, APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS AND SHALL REPAIR OR REPLACE PER BERNALILLO COUNTY STANDARDS, AT HIS OWN EXPENSE.
- CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THE TRAFFIC CONTROL STANDARD DETAILS AND PLAN ON SHEETS 11, 12 AND 13.
- THE CONTRACTOR SHALL REPLACE IN KIND ANY ADJACENT PROPERTY OWNER LANDSCAPING DISTURBED DURING CONSTRUCTION. COST SHALL BE INCIDENTAL TO THE PROJECT.



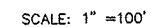
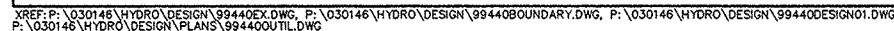
NOTICE TO CONTRACTORS

- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, BE CONSTRUCTED IN ACCORDANCE WITH NEW MEXICO STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION 2000 EDITION AS REVISED FOR THIS PROJECT.
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR SURVEYOR IMMEDIATELY SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONST. COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BERNALILLO COUNTY PUBLIC WORKS DIVISION TRAFFIC ENGINEERING (848-1540) PRIOR TO OCCUPYING AN INTERSECTION.
- ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED W/ PLASTIC REFLECTORIZED PAVEMENT MARKING BY CONTRACTOR TO LOCATION AS EXISTING OR AS INDICATED BY THIS PLAN SET.
- CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI - FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY GRAFFITI FROM ALL EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.

THE FOLLOWING NOTES ALSO APPLY WHEN CHECKED

- ☒ ALL UTILITIES AND UTILITY SERVICE LINES SHALL BE INSTALLED PRIOR TO PAVING.
- ☐ BACKFILL COMPACTION SHALL BE ACCORDING TO SPECIFIED STREET USE.
- ☐ TACK COAT REQUIREMENTS SHALL BE DETERMINED BY THE ENGINEER.
- ☒ SIDEWALKS AND WHEELCHAIR RAMPS WITHIN THE CURB RETURNS SHALL BE CONSTRUCTED WHEREVER A NEW CURB RETURN IS CONSTRUCTED.
- ☒ IF CURB IS DEPRESSED FOR A DRIVEPAD, THE DRIVEPAD SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB AND GUTTER.
- ☒ ALL STORM DRAINAGE FACILITIES SHALL BE COMPLETED PRIOR TO FINAL ACCEPTANCE.
- ☐ THE REQUESTOR OR DEVELOPER SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT OF ALL CURB AND GUTTER OR SIDEWALK DAMAGED AFTER APPROVAL BY THE CITY ENGINEER OF WORK COMPLETED BY THE CONTRACTOR.

REV.	SHEETS	CITY ENGINEER	DATE	USER DEPARTMENT	DATE	USER DEPARTMENT	DATE
ENGINEERS STAMP & SIGNATURE		APPROVALS		ENGINEER		DATE	
		DRC Chairman		<i>Paul Olsen</i>		10-20-03	
		Transportation		N/A		10/7/03	
		Water/Wastewater		<i>Paul Olsen</i>		10/9/03	
		Hydrology		<i>Paul Olsen</i>		10-20-03	
		Parks		N/A		10-20-03	
City Project No.		706081		Sheet		1 of 13	



Sheet Of

Sheet 2 of 13

BHI JOB NO. 030146

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Date: 02-07-03

















STA. @ CENTER OF DRIVEPAD	DRIVE PAD WIDTH
35+11, LT	15'
35+35, LT	15'
36+24, LT	15'
36+55, RT	15'

DRIVEPAD WIDTH IS APPROX.. ADJUST
TO MATCH EXISTING DRIVEPAD AND
COORDINATE WITH PROPERTY OWNER.

1705

FL 4942.47, 5' CURB
TRANSITION FROM 6" TO 0"
STA. 37+19.06, 45.70 LT 7

1. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
2. ALL CURVE DATA AND DIMENSIONS REFER TO FACE OF CURB UNLESS OTHERWISE SPECIFIED.
3. GRADE ELEVATIONS, WHERE NOTED, ARE FOR CURB FLOW LINE UNLESS OTHERWISE SPECIFIED.
4. CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF REPAIR AND/OR REPLACEMENT OF ALL UTILITY CONDUITS AND EXISTING LINES WITH RESPECTIVE UTILITY OWNERS.
5. ANY ADDITIONAL GRADING REQUIRED TO MATCH PROPOSED STREET GRADES SHALL BE INCIDENTAL TO PAVING ITEMS.
6. CONTRACTOR SHALL PARK EQUIPMENT AND VEHICLES AS NOT TO INTERFERE WITH NORMAL ACTIVITIES OF RESIDENTS ON SITE.
7. ANY DAMAGE TO THE EXISTING FACILITIES (CURB & GUTTER), PAVEMENT, CONDUITS, LANDSCAPING, UTILITY LINES, ETC.) DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTORS' EXPENSE.
8. WHEELCHAIR RAMPS SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB & GUTTER AND WILL BE PAID AS SIDEWALK.
9. IF ANY UNUSUAL ODORS OR SOIL STAINING IS OBSERVED IN THE VICINITY OF THE FORMER LAUNDROMAT SITE NEAR STA. 54+00 (OR ELSEWHERE IN THE CORRIDOR) DURING CONSTRUCTION ACTIVITY AT THAT LOCATION WILL CEASE AND APPROPRIATE TESTING WILL BE CONDUCTED BY THE CONTRACTOR ACCORDING TO PROCEDURES CONTAINED IN THE HANDBOOK OF HAZARDOUS WASTE MANAGEMENT.
10. THE FENCE LINES ARE THE APPARENT R/W (TYP.), EXCEPT WHERE INDICATED.
11. CONTRACTOR SHALL ADJUST ALL WATER VALVES AND MANHOLES TO FINISHED GRADES WITHIN THE PROJECT LIMITS.
12. STORM DRAIN LENGTHS ARE MEASURED FROM CENTER OF MANHOLE TO CENTER OF MANHOLE AND FROM CENTER OF MANHOLE TO CENTER OF INLET.
13. FOR DROP INLET DETAILS, SEE MISCELLANEOUS DETAIL SHEET 9.
14. ALL STORM DRAIN PIPE LATERALS SHALL BE CLASS IV, 18" RCP (EXCEPT WHERE INDICATED).
15. CONTRACTOR SHALL COORDINATE WITH PROPERTY OWNERS TO DETERMINE DRIVEWAY LOCATIONS.
16. FOR DRIVE PAD DETAILS, SEE MISCELLANEOUS DETAIL SHEET 9.

	EXISTING INDEX CONTOUR
	EXISTING INTERMEDIATE CONTOUR
	EXISTING GAS LINE
	EXISTING WATERLINE
	EXISTING SAs
	EXISTING STORM DRAIN W/MANHOLE
	CONSTRUCTION BASELINE
	STORM DRAIN W/MANHOLE
	EXISTING OVERHEAD ELECTRICAL
	EXISTING BARBWIRE FENCE
	EXISTING CHAINLINK FENCE
	EXISTING WOODRAIL FENCE
	EXISTING TREES
	STREET ADDRESS
	VALLEY GUTTER
	TRANSITIONAL SECTION (USE PAVEMENT SECTION SHOWN ON SHEET 9)

Bohannon ▲ Huston
 Courtyard I 7000 Jefferson St. NE Albuquerque, NM 87106
 ENGINEERING ▲ SPATIAL DATA ▲ ADVANCED TECHNOLOGY

County of Bernalillo
Public Works Division
State of New Mexico

**FIVE POINTS ROAD (PHASE I)
DRAINAGE IMPROVEMENTS
STORM DRAIN PLAN & PROFILE**

City Engineer Approval

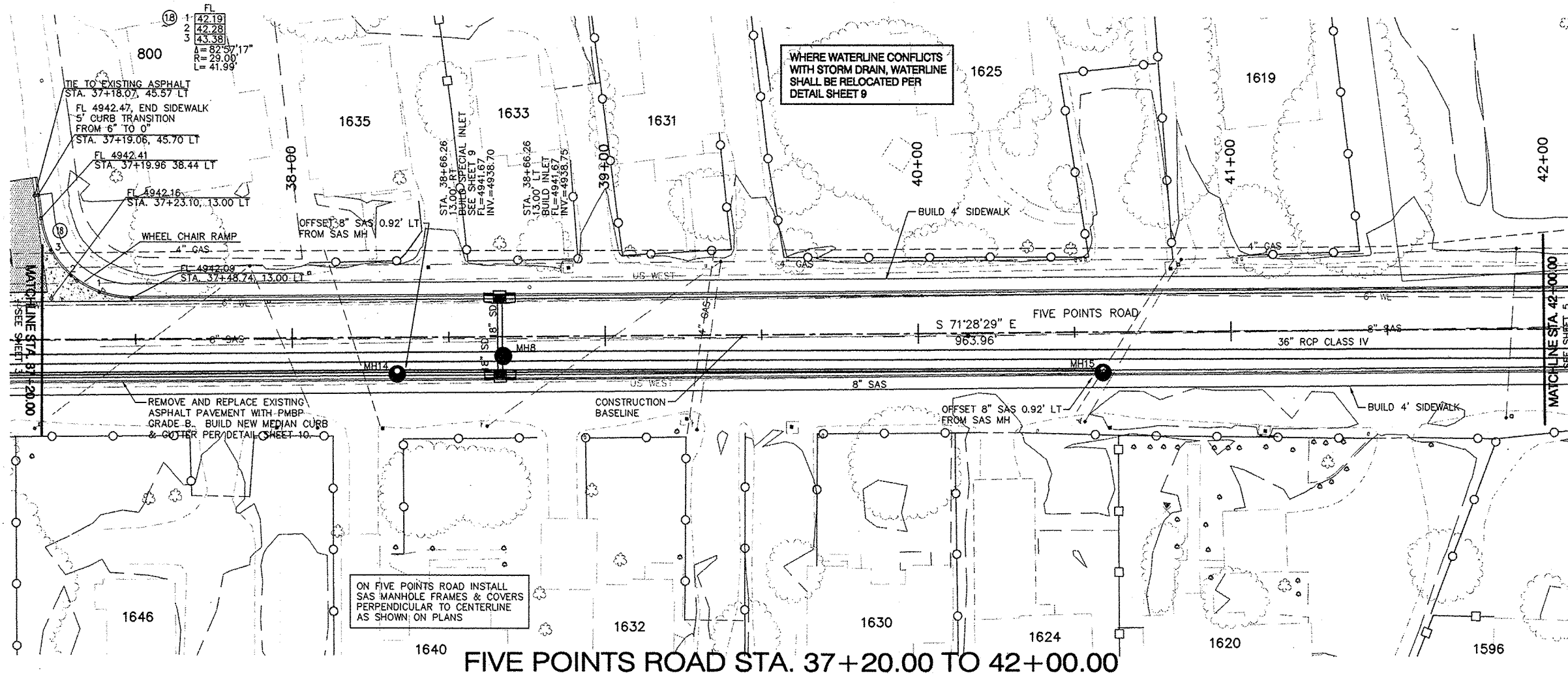
ENGINEER'S SEAL

No.	Date	REMARKS	By
REVISIONS			
DESIGN			
Designed By: CMG/SRB		DATE: 9-30-02	
Drawn By: RTM	DATE: 1-30-03		

City Project No.	
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Zone Map No.
L12

Sheet	3	Of	13
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LEGEND

- 4045- EXISTING INDEX CONTOUR
- EXISTING INTERMEDIATE CONTOUR
- G- EXISTING GAS LINE
- W- EXISTING WATERLINE
- S- EXISTING SAS
- SD EXISTING STORM DRAIN W/MANHOLE
- CONSTRUCTION BASELINE
- STORM DRAIN W/MANHOLE
- EXISTING OVERHEAD ELECTRICAL
- EXISTING BARB WIRE FENCE
- EXISTING CHAINLINK FENCE
- EXISTING WOODRAIL FENCE
- EXISTING TREES
- 1659 STREET ADDRESS
- VALLEY GUTTER
- TRANSITIONAL SECTION (USE PAVEMENT SECTION SHOWN ON SHEET 9)

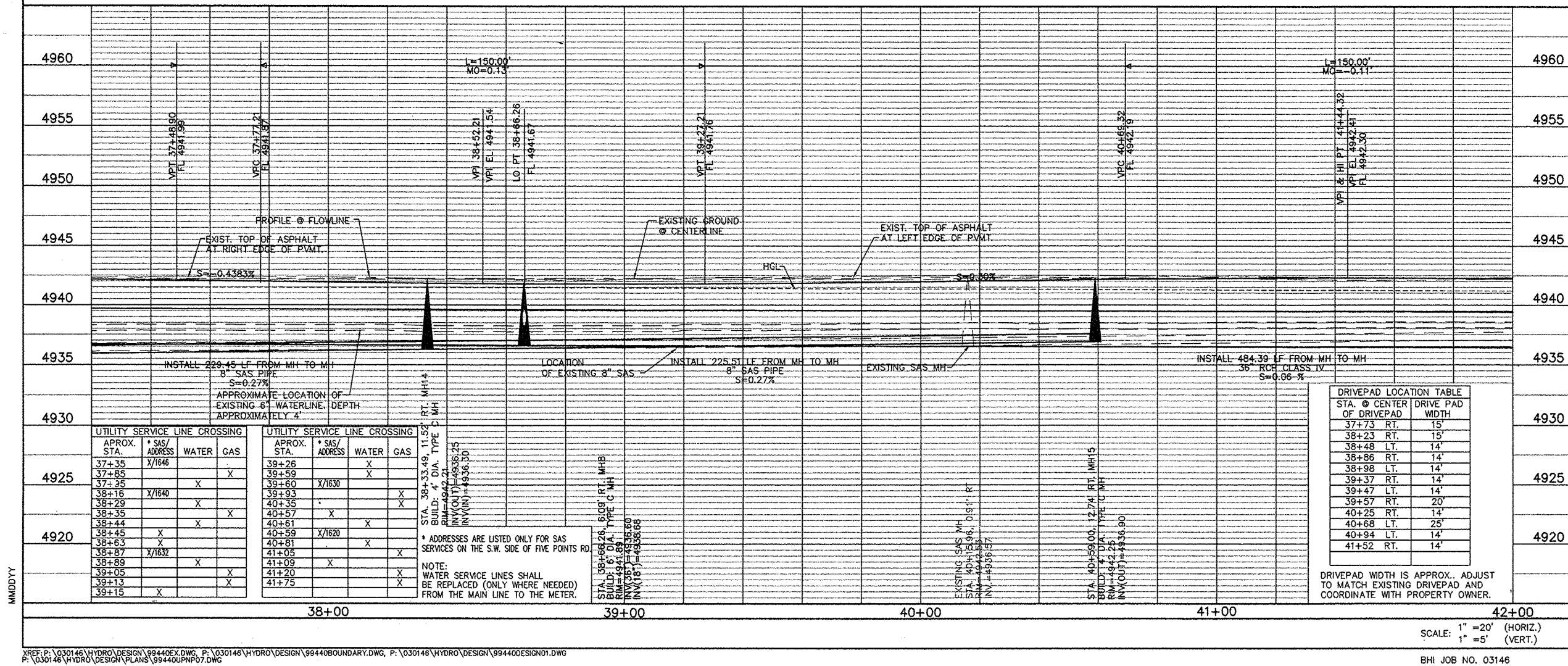
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Engineering & Spatial Data & Advanced Technologies
7000 Jefferson St. NE Albuquerque, NM 87109-4395

County of Bernalillo
Public Works Division
State of New Mexico

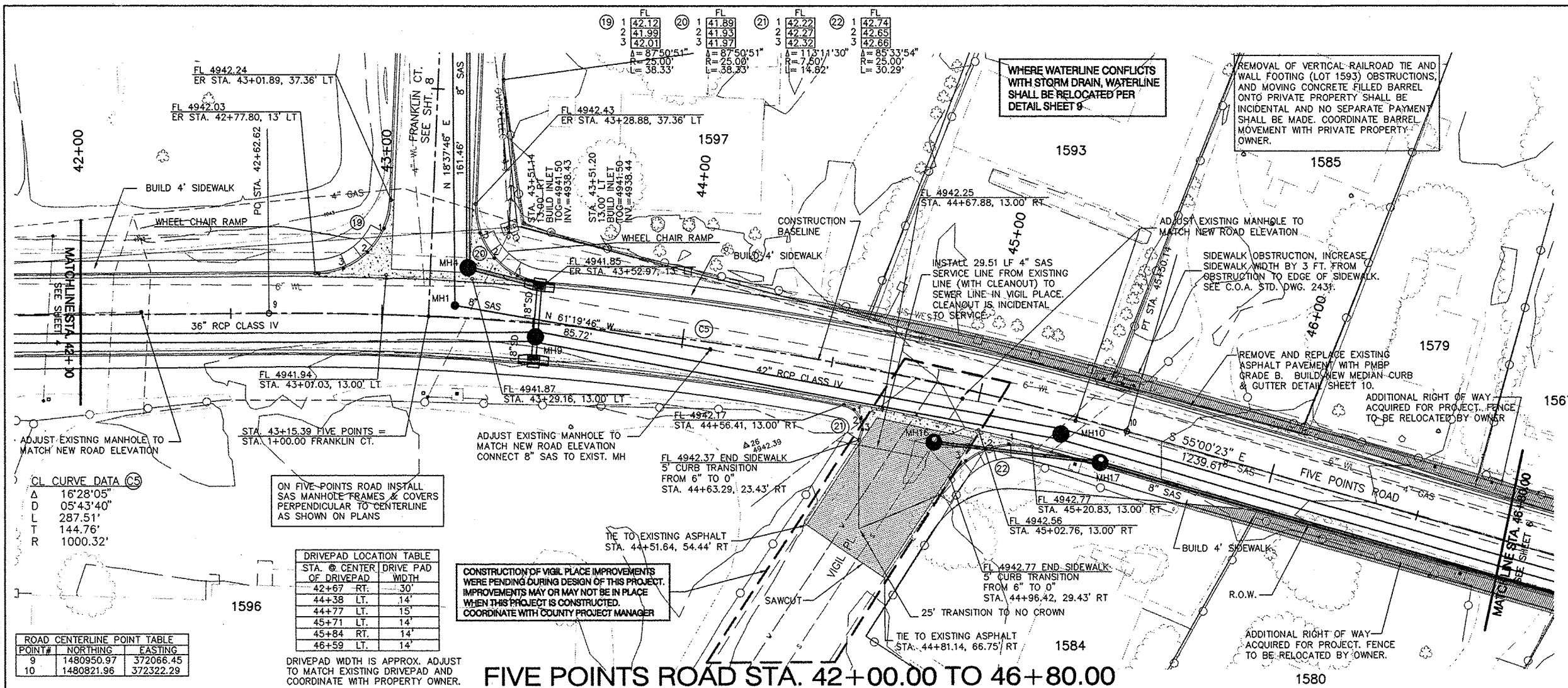
FIVE POINTS ROAD (PHASE I)
DRAINAGE IMPROVEMENTS
STORM DRAIN PLAN & PROFILE

City Project No. 706081 Zone Map No. L12 Sheet 4 of 13

City Engineer Approval: [Signature]
Design Review Committee: [Signature]
Mo./Day/Yr. []/ []/ []



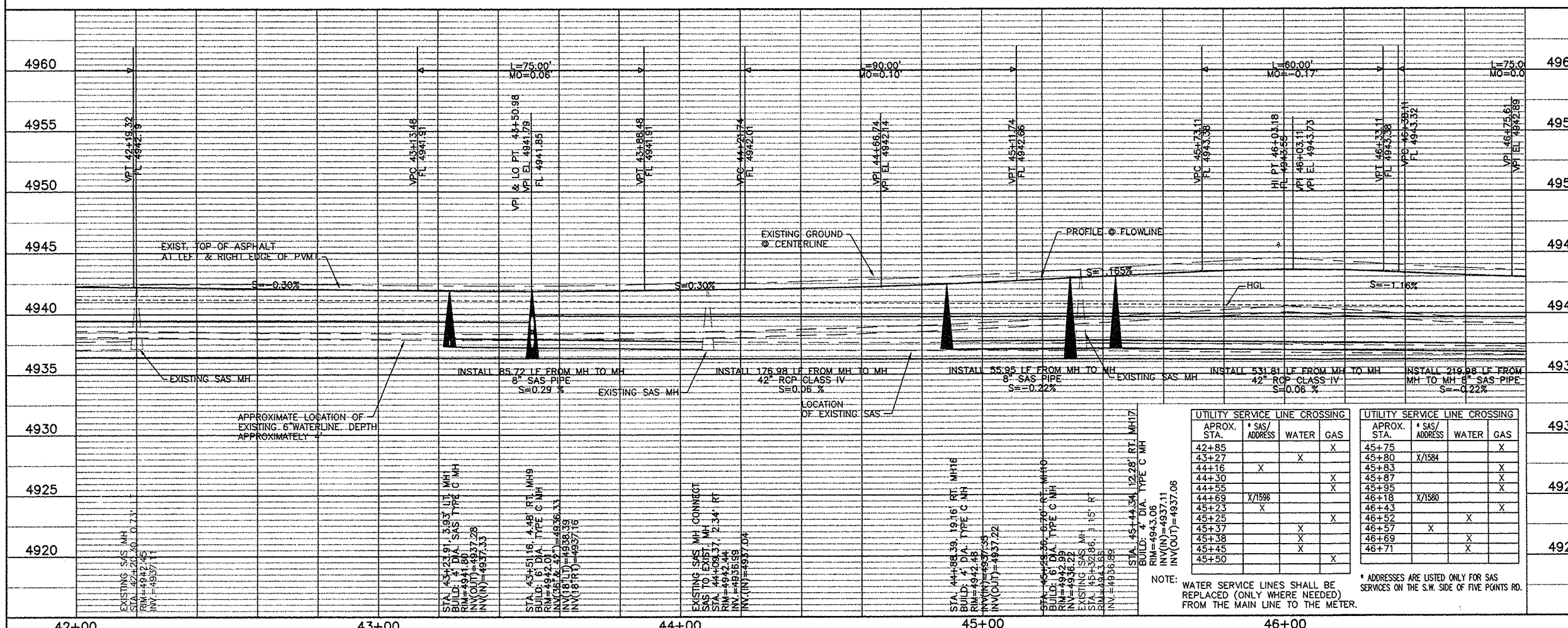
SCANNED BY:
PLANNING



ROAD CENTERLINE POINT TABLE		
POINT#	NORTHING	EASTING
9	1480950.97	372066.45
10	1480821.96	372322.29

DRIVEPAD LOCATION TABLE	
STA. @ CENTER OF DRIVEPAD	DRIVE PAD WIDTH
42+67 RT.	30'
44+38 LT.	14'
44+77 LT.	15'
45+71 LT.	14'
45+84 RT.	14'
46+59 LT.	14'

CONSTRUCTION OF VIGIL PLACE IMPROVEMENTS
WAS PENDING DURING DESIGN OF THIS PROJECT.
IMPROVEMENTS MAY OR MAY NOT BE IN PLACE
WHEN THIS PROJECT IS CONSTRUCTED.
COORDINATE WITH COUNTY PROJECT MANAGER



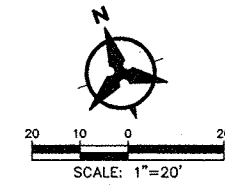
UTILITY SERVICE LINE CROSSING			
APPROX. STA.	* SAS/ ADDRESS	WATER	GAS
42+85			X
43+27		X	
44+16	X		
44+30			X
44+55			X
44+69	X/1596		
45+23	X		
45+25			X
45+37		X	
45+38		X	
45+45		X	
45+50			X

UTILITY SERVICE LINE CROSSING			
APPROX. STA.	* SAS/ ADDRESS	WATER	GAS
45+75			X
45+80	X/1584		
45+83			X
45+87			X
45+95			X
46+18	X/1580		
46+43			X
46+52		X	
46+57	X		
46+69		X	
46+71		X	

NOTE: WATER SERVICE LINES SHALL BE REPLACED (ONLY WHERE NEEDED) FROM THE MAIN LINE TO THE METER.

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- | | |
|----------------|--|
| LEGEND | |
| 4945- --- | EXISTING INDEX CONTOUR |
| --- --- | EXISTING INTERMEDIATE CONTOUR |
| --- G --- | EXISTING GAS LINE |
| --- W --- | EXISTING WATERLINE |
| --- @ SAS --- | EXISTING SAS |
| --- (C) SD --- | EXISTING STORM DRAIN W/MANHOLE |
| --- (C) SD --- | CONSTRUCTION BASELINE |
| --- (C) SD --- | STORM DRAIN W/MANHOLE |
| OVHD-ELEC | EXISTING OVERHEAD ELECTRICAL |
| --- --- | EXISTING BARBWARE FENCE |
| --- --- | EXISTING CHAINLINK FENCE |
| --- --- | EXISTING WOODRAIL FENCE |
| --- --- | EXISTING TREES |
| 1659 | STREET ADDRESS |
| | VALLEY GUTTER |
| | TRANSITIONAL SECTION (USE PAVEMENT SECTION SHOWN ON SHEET 9) |



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 Courtyard | 7800 Jefferson St. NE Albuquerque, NM 87109-43
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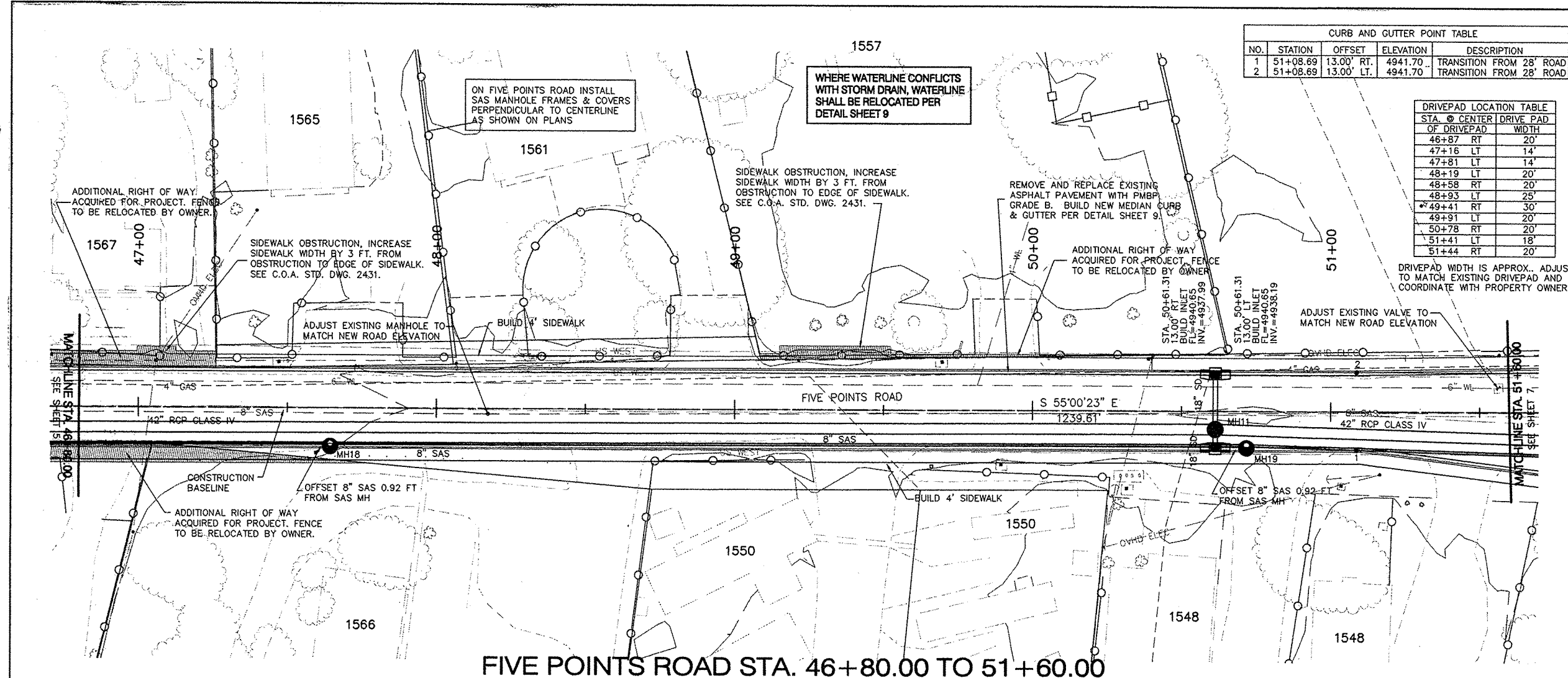
County of Bernalillo
Public Works Division
State of New Mexico

**FIVE POINTS ROAD (PHASE I)
DRAINAGE IMPROVEMENTS
STORM DRAIN PLAN & PROFILE**

Design Review Committee City Engineer Approval

REVIEWED: OCT 20 1964

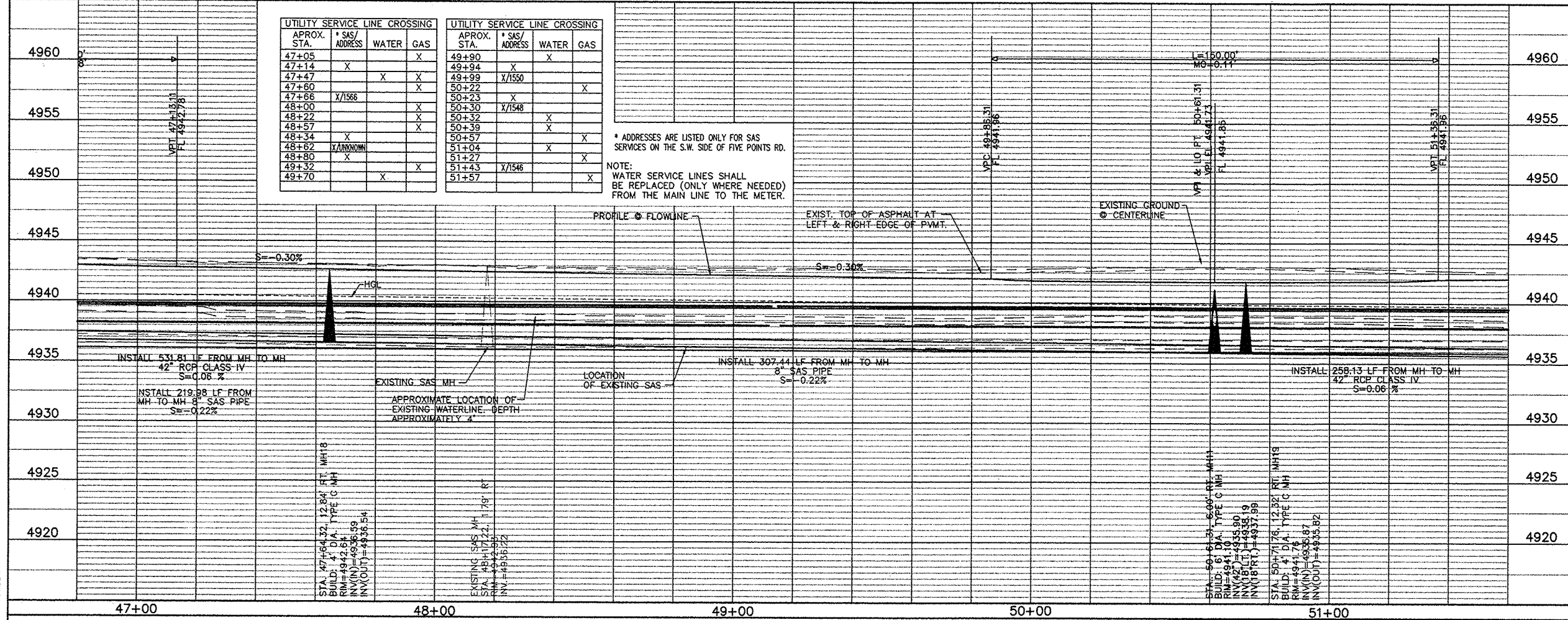
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CURB AND GUTTER POINT TABLE				
NO.	STATION	OFFSET	ELEVATION	DESCRIPTION
1	51+08.69	13.00' RT.	4941.70	TRANSITION FROM 28' ROAD
2	51+08.69	13.00' LT.	4941.70	TRANSITION FROM 28' ROAD

DRIVEPAD LOCATION TABLE		
STA. @ CENTER DRIVE PAD	WIDTH	
46+87 RT	20'	
47+16 LT	14'	
47+81 LT	14'	
48+19 LT	20'	
48+58 RT	20'	
48+93 LT	25'	
49+41 RT	30'	
49+91 LT	20'	
50+78 RT	20'	
51+41 LT	18'	
51+44 RT	20'	

- ### NOTES
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 - CONTRACTOR SHALL PARK EQUIPMENT AND VEHICLES AS NOT TO INTERFERE WITH NORMAL ACTIVITIES OF RESIDENTS ON SITE.
 - ANY DAMAGE TO THE EXISTING FACILITIES (CURB & GUTTER), PAVEMENT, CONDUITS, LANDSCAPING, UTILITY LINES, ETC.) DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
 - WHEELCHAIR RAMPS SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB & GUTTER AND WILL BE PAID AS SIDEWALK.
 - IF ANY UNUSUAL ODORS OR SOIL STAINING IS OBSERVED IN THE VICINITY OF THE FORMER LAUNDROMAT SITE NEAR STA. 54+00 (OR ELSEWHERE IN THE CORRIDOR) DURING CONSTRUCTION ACTIVITY AT THAT LOCATION WILL CEASE AND APPROPRIATE TESTING WILL BE CONDUCTED BY THE CONTRACTOR ACCORDING TO PROCEDURES CONTAINED IN THE HANDBOOK OF HAZARDOUS WASTE MANAGEMENT.
 - THE FENCE LINES ARE THE APPARENT R/W (TYP.) EXCEPT WHERE INDICATED.
 - CONTRACTOR SHALL ADJUST ALL WATER VALVES AND MANHOLES TO FINISHED GRADES WITHIN THE PROJECT LIMITS.
 - STORM DRAIN LENGTHS ARE MEASURED FROM CENTER OF MANHOLE TO CENTER OF MANHOLE AND FROM CENTER OF MANHOLE TO CENTER OF INLET.
 - FOR DROP INLET DETAILS, SEE MISCELLANEOUS DETAIL SHEET 9.
 - ALL STORM DRAIN PIPE LATERALS SHALL BE CLASS IV, 18" RCP (EXCEPT WHERE INDICATED).
 - CONTRACTOR SHALL COORDINATE WITH PROPERTY OWNERS TO DETERMINE DRIVEWAY LOCATIONS.
 - FOR DRIVE PAD DETAILS, SEE MISCELLANEOUS DETAIL SHEET 9.



UTILITY SERVICE LINE CROSSING			
APPROX. STA.	SAS ADDRESS	WATER	GAS
47+05		X	X
47+14	X		
47+47		X	X
47+60		X	
47+66	X/1568		
48+00		X	
48+22		X	
48+57		X	
48+57	X		
48+62	X/1568		
48+80	X		
49+32		X	
49+70		X	

UTILITY SERVICE LINE CROSSING			
APPROX. STA.	SAS ADDRESS	WATER	GAS
49+90		X	
49+94	X		
49+99	X/1550		
50+22			X
50+23	X		
50+30	X/1548		
50+32		X	
50+39		X	
50+57		X	X
51+04		X	
51+27		X	
51+43	X/1546		
51+57			X

* ADDRESSES ARE LISTED ONLY FOR SAS SERVICES ON THE S.W. SIDE OF FIVE POINTS RD.
NOTE: WATER SERVICE LINES SHALL BE REPLACED (ONLY WHERE NEEDED) FROM THE MAIN LINE TO THE METER.

LEGEND

- 4945- EXISTING INDEX CONTOUR
- EXISTING INTERMEDIATE CONTOUR
- G- EXISTING GAS LINE
- W- EXISTING WATERLINE
- SAS- EXISTING SAS
- SD- EXISTING STORM DRAIN W/MANHOLE
- CON- CONSTRUCTION BASELINE
- SD- STORM DRAIN W/MANHOLE
- OVHD-ELEC- EXISTING OVERHEAD ELECTRICAL
- EXISTING BARB WIRE FENCE
- EXISTING CHAINLINK FENCE
- EXISTING WOODRAIL FENCE
- EXISTING TREES
- 1659 STREET ADDRESS

SCALE: 1"=20'

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 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

County of Bernalillo
Public Works Division
State of New Mexico

FIVE POINTS ROAD (PHASE I)
DRAINAGE IMPROVEMENTS
STORM DRAIN PLAN & PROFILE

Design Review Committee: [Signature]
 City Engineer Approval: [Signature]
 Last Design Update: [Date]

City Project No. **706081** Zone Map No. **L12** Sheet **6** Of **13**

Designed By: CMG/SRB DATE: 9-30-02
 Drawn By: RTM DATE: 1-30-03
 Checked By: DJG DATE: 9-18-03

XXXXXX

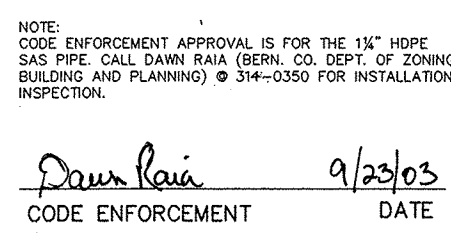
14. *Quelques points relatifs aux points de vue suivants*

BHI JOB NO. 030146

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Date: 02-07-03

alillo tment ico HASE I) ENTS PROFILE	
Mo./Day/Yr.	Mo./Day/Yr.
Sheet	Of
7	13

MMDDYY



BASELINE DATA			
1	N	21° 32' 51"	E 136.51'
2	N	74° 52' 41"	E 36.50'
3	S	78° 55' 04"	E 137.41'


UTILITIES BEARING & DISTANCE				
#	NORTHING		EASTING	
1	N 18° 37' 46"	E	161.46'	
2	N 78° 46' 41"	E	101.76'	
3	N 56° 38' 08"	W	22.80'	
4	N 19° 11' 19"	E	119.73'	
5	N 46° 38' 32"	E	44.08'	
6	N 46° 38' 32"	E	57.59'	
7	S 50° 53' 25"	E	34.17'	

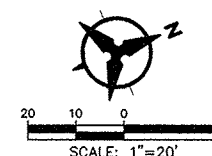
NOTE:
CODE ENFORCEMENT APPROVAL IS FOR THE 1 1/4" HDPE
SAS PIPE. CALL DAWN RAI (BERN. CO. DEPT. OF ZONING
BUILDING AND PLANNING) @ 314-0350 FOR INSTALLATION
INSPECTION.

Dawn Rain 9/23/03
CODE ENFORCEMENT DATE

WHERE WATERLINE CONFLICTS
WITH STORM DRAIN, WATERLINE
SHALL BE RELOCATED PER
DETAIL SHEET 9

1. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
2. ALL CURVE DATA AND DIMENSIONS REFER TO FACE OF CURB UNLESS OTHERWISE SPECIFIED.
3. GRADE ELEVATIONS, WHERE NOTED, ARE FOR CURB FLOW LINE UNLESS OTHERWISE SPECIFIED.
4. CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF REPAIR AND/OR REPLACEMENT OF ALL UTILITY CONDUITS AND EXISTING LINES WITH RESPECTIVE UTILITY OWNERS.
5. ANY ADDITIONAL GRADING REQUIRED TO MATCH PROPOSED STREET GRADES SHALL BE INCIDENTAL TO PAVING ITEMS.
6. CONTRACTOR SHALL PARK EQUIPMENT AND VEHICLES AS NOT TO INTERFERE WITH NORMAL ACTIVITIES OF RESIDENTS ON SITE.
7. ANY DAMAGE TO THE EXISTING FACILITIES (CURB & GUTTER), PAVEMENT, CONDUITS, LANDSCAPING, UTILITY LINES, ETC.) DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTORS' EXPENSE.
8. WHEELCHAIR RAMPS SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB & GUTTER AND WILL BE PAID AS SIDEWALK.
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4445-	EXISTING INDEX CONTOUR
---	EXISTING INTERMEDIATE CONTOUR
- G -	EXISTING GAS LINE
- W -	EXISTING WATERLINE
- SAS -	EXISTING STORM DRAIN W/MANHOLE
⊙ - SD	CONSTRUCTION BASELINE
⊙ - SD	STORM DRAIN W/MANHOLE
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---	EXISTING CHAINLINK FENCE
---	EXISTING WOODRAIL FENCE
---	EXISTING TREES
1659	STREET ADDRESS
	VALLEY GUTTER



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**FIVE POINTS ROAD (PHASE I)
DRAINAGE IMPROVEMENTS
STORM DRAIN PLAN & PROFILE**

Design Review Committee City Engineer Approval

Mo./Day/Yr.	Mo./Day/Yr.

City Project No.	706081	Zone Map No.	L12	Sheet	8	Of	13
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SCALE: 1" = 20' (HORIZ.)
1" = 5' (VERT.)

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