

FILENET FORM FOR RECORD CENTER

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|-------------------------------|---|
| SECURITY | <input type="checkbox"/> RESTRICTED OR <input checked="" type="checkbox"/> UNRESTRICTED |
| PROJECT # | 7337.81 |
| PROJECT TITLE | Coors Blvd / Namaste Rd - Western Trail Traffic Signal Installation |
| ATLAS ZONE | F-11 |
| DATE OF MAP & RECORD STAMP | 4/8/2005 |
| SERIES | 26 |
| NUMBER OF SHEETS | 14 |

HOW TO FILL OUT THE FORM EXAMPLE

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|-------------------------------|---|
| SECURITY | <input type="checkbox"/> RESTRICTED OR <input checked="" type="checkbox"/> UNRESTRICTED |
| PROJECT # | 6777.81 |
| PROJECT TITLE | BLACK ARROYO MASTER STORM DRAIN |
| ATLAS ZONE | A-11, A-12 |
| DATE OF MAP & RECORD STAMP | 2/6/2003 |
| SERIES | 26 |
| NUMBER OF SHEETS | 13 |

CONSTRUCTION PLANS
FOR
COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
TRAFFIC SIGNAL INSTALLATION

ALBUQUERQUE, NEW MEXICO

INDEX

| SHEET NUMBER | DESCRIPTION |
|--------------|--|
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| 2 | CONTROL DATA PLAN |
| 3 | INTERSECTION PLAN |
| 4 | TRAFFIC SIGNAL NOTES & LEGEND |
| 5 | TRAFFIC SIGNAL EQUIPMENT REQUIREMENTS & INCIDENTAL ITEMS |
| 6 | TRAFFIC SIGNAL ESTIMATED QUANTITIES |
| 7 | TRAFFIC SIGNAL PLAN |
| 8-9 | TRAFFIC SIGNAL CABLES & CONDUITS |
| 10 | TRAFFIC SIGNAL FUNCTIONS & DETECTORS |
| 11 | INTERSECTION LIGHTING PLAN |
| 12 | SIGNING & STRIPING PLAN |
| 13 | TRAFFIC CONTROL SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS |
| 14 | TYPICAL TRAFFIC CONTROL & SIGNING EXAMPLES |

AS BUILT CERTIFICATION

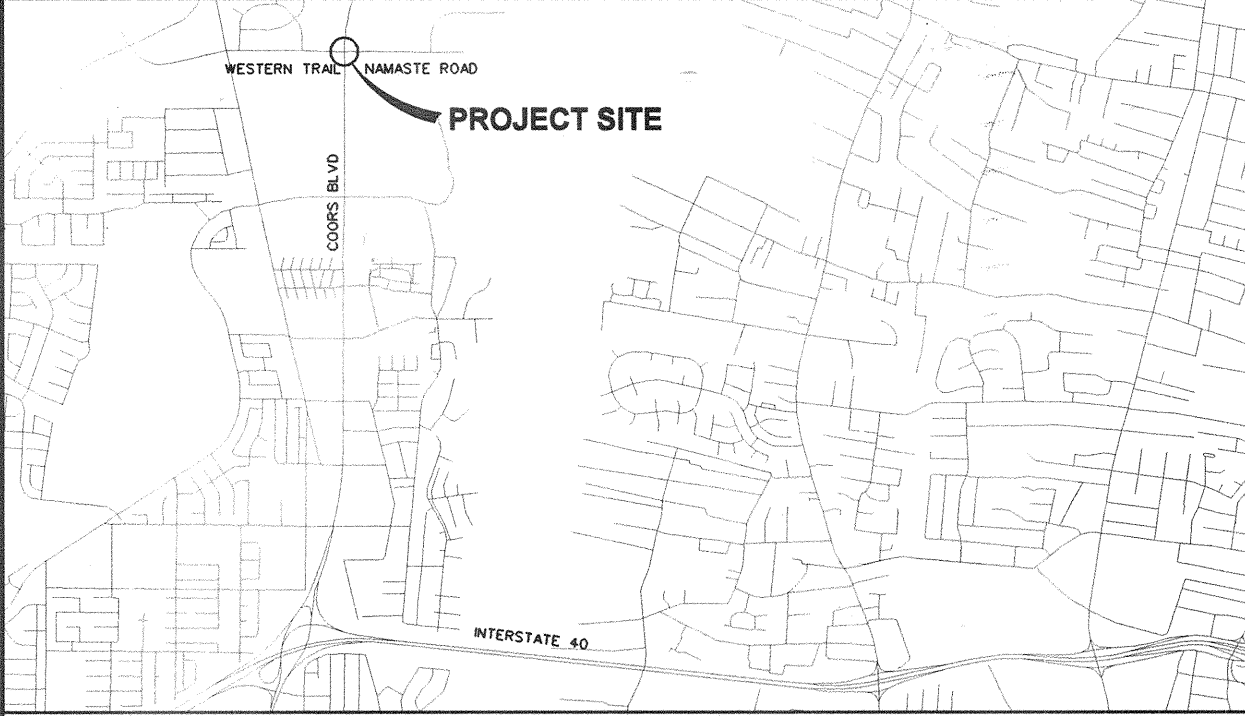
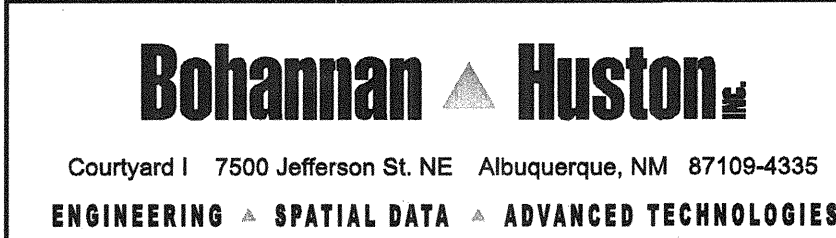
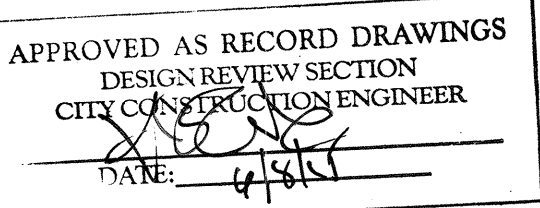
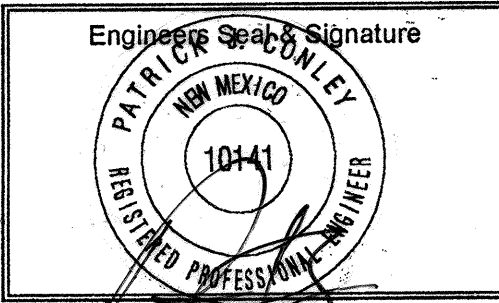
I, DAVID VIGIL, DO HEREBY ATTEST TO THE FACT THAT THE AS-BUILT INFORMATION SHOWN HEREON IS THE RESULT OF A FIELD SURVEY PERFORMED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT THE SAME ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

DAVID VIGIL DATE: 01/04/05

NORTHSTAR SURVEYING



I, Patrick J. Conley of the firm of SMITH ENGINEERING & CO. A Registered Professional Engineer in the State of New Mexico, do hereby certify, to the best of my knowledge and belief, that the infrastructure installed as part of this project has been inspected by me or by a qualified person under my direct supervision and has been constructed in accordance with the plans and specifications, while someone from Smith Engineering Company was onsite, approved by the City Engineer and that the original design intent of the approved plans has been met, except as noted by me on the as-built construction drawings. This Certification is based on site inspections by me or personnel under my direction and survey information provided by DAVID VIGIL (Surveyor Name) NMPS number 8911 (PS #).



VICINITY MAP: NOT TO SCALE ZONE MAP NO. F-11

NOTICE TO CONTRACTORS

- AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN THE CITY RIGHT-OF-WAY.
- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION AS REVISED THROUGH UPDATE No. 7.
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (280-1990) FOR LOCATION OF EXISTING UTILITIES. UTILITIES SHOWN IN PLANS ARE APPROXIMATE AND MAY NOT REFLECT ALL UTILITIES WITHIN THE INTERSECTION.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR SURVEYOR IMMEDIATELY SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT, TO THE CONSTRUCTION COORDINATION DIVISION, A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONST. COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY CONSTRUCTION COORDINATION ENGINEER (924-3400) PRIOR TO OCCUPYING AN INTERSECTION. REFER TO SECTION 19 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
- ALL WORK AFFECTING ARTERIAL ROADWAYS REQUIRES TWENTY-FOUR HOUR CONSTRUCTION.
- ALL STREET STRIPING ALTERED OR DESTROYED SHALL BE REPLACED W/ PLASTIC REFLECTORIZED PAVEMENT MARKING BY CONTRACTOR TO THE SAME LOCATION AS EXISTING, OR AS INDICATED BY THIS PLAN SET.
- CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
- CONTRACTOR SHALL RECORD DATA ON ALL UTILITY LINES AND ACCESSORIES AS REQUIRED BY THE CITY OF ALBUQUERQUE FOR THE PREPARATION OF "AS CONSTRUCTED" DRAWINGS. CONTRACTOR SHALL NOT COVER UTILITY LINES AND ACCESSORIES UNTIL ALL DATA HAS BEEN RECORDED.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY GRAFFITI FROM ALL EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.
- CONTRACTOR SHALL COORDINATE WITH THE CITY OF ALBUQUERQUE WATER SYSTEMS DIVISION (857-8200) SEVEN (7) WORKING DAYS IN ADVANCE OF ANY WORK THAT MAY AFFECT EXISTING PUBLIC WATER OR SEWER UTILITIES. EXISTING VALVES TO BE OPERATED BY CITY PERSONNEL ONLY. CONTRACTOR SHALL CONTACT THE WATER SYSTEMS DIVISION SEVEN (7) DAYS PRIOR TO NEEDING VALVES TURNED ON OR OFF.

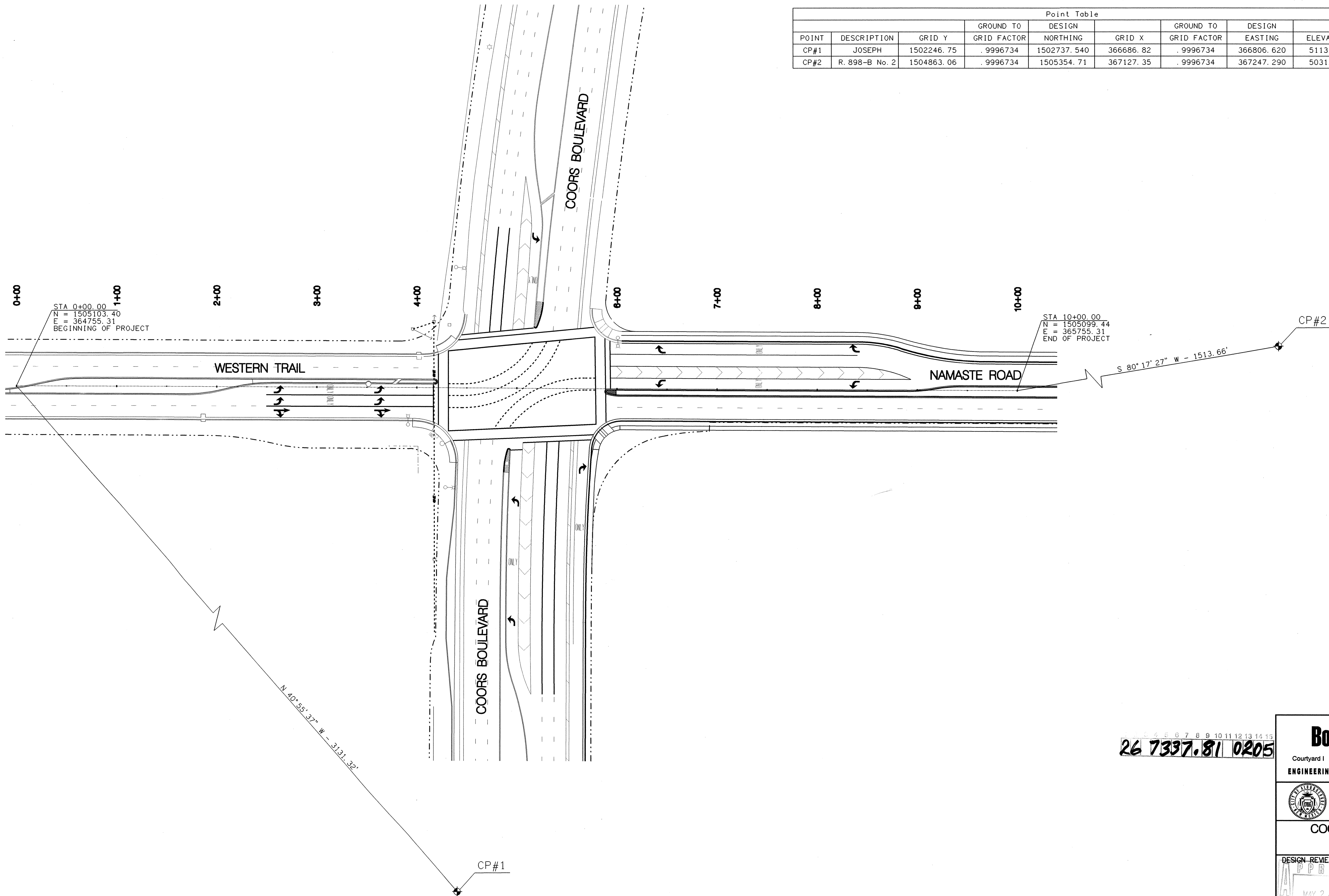
THE FOLLOWING NOTES ALSO APPLY WHEN CHECKED

- ☐ ALL UTILITIES AND UTILITY SERVICE LINES SHALL BE INSTALLED PRIOR TO PAVING.
- ☒ BACKFILL COMPACTION SHALL BE ACCORDING TO SPECIFIED STREET USE.
- ☐ TACK COAT REQUIREMENTS SHALL BE DETERMINED BY THE ENGINEER.
- ☐ SIDEWALKS AND WHEELCHAIR RAMPS WITHIN THE CURB RETURNS SHALL BE CONSTRUCTED WHEREVER A NEW CURB RETURN IS CONSTRUCTED.
- ☐ IF CURB IS DEPRESSED FOR A DRIVEPAD, THE DRIVEPAD SHALL BE CONSTRUCTED PRIOR TO ACCEPTANCE OF CURB AND GUTTER.
- ☐ ALL STORM DRAINAGE FACILITIES SHALL BE COMPLETED PRIOR TO FINAL ACCEPTANCE.
- ☒ THE REQUESTOR OR DEVELOPER SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT OF ALL CURB AND GUTTER OR SIDEWALK DAMAGED AFTER APPROVAL BY THE CITY ENGINEER OF WORK COMPLETED BY THE CONTRACTOR.


"AS-BUILTS"

26 7337.81 01/05



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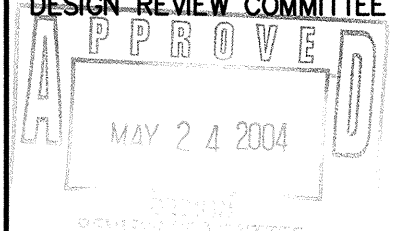
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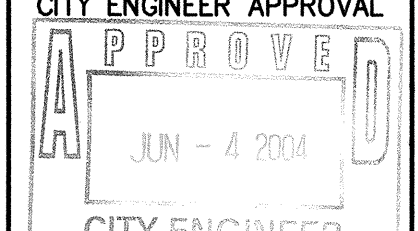
Courtyard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335

ENGINEERING  SPATIAL DATA  ADVANCED TECHNOLOGIES

**CITY OF ALBUQUERQUE**
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
CONTROL DATA PLAN

DESIGN REVIEW COMMITTEE

MAY 24 2004

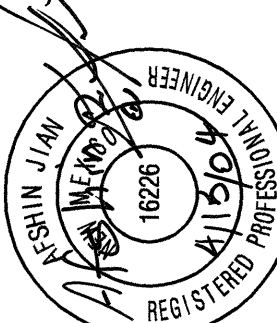
CITY ENGINEER APPROVAL

JUN - 4 2004

LAST DESIGN UPDATE

CITY PROJECT NO.
733781

ZONE MAP NO.
F-11

SHEET
2 OF
14

| ENGINEER'S SEAL | | SURVEY INFORMATION | | BENCH MARKS | | AS-BUILT INFORMATION | | | | |
|--|--|--------------------|----|-------------|---|-----------------------------|-------------------------------------|--|--|--|
|  | | NO. | BY | DATE | USC & GS BRASS TABLET STAMPED "JOSEPH 1968" GEOGRAPHIC POSITION (NAD 1927) N.M. State Plane Coordinates (Central Zone) X = 366,806.62 Y = 1,502,737.54 Ground-to-Grid Factor = 0.9996734 AQ = -00°15'24" SLD 1929 Elevation = 5113.852 | CONTRACTOR MW1 | STAKED BY NORTHSTAR SURV. | | | |
| | | | | | | | | | | |
| REVISIONS | | NO. | | DATE | | INSPECTOR'S VERIFICATION BY | | | | |
| DESIGN | | | | | | FIELD VERIFICATION BY | | | | |
| | | | | | | CORRECTED BY | | | | |
| | | | | | | MICRO-FILM INFORMATION | | | | |
| | | | | | | RECORDED BY | | | | |
| | | | | | | NO. | | | | |

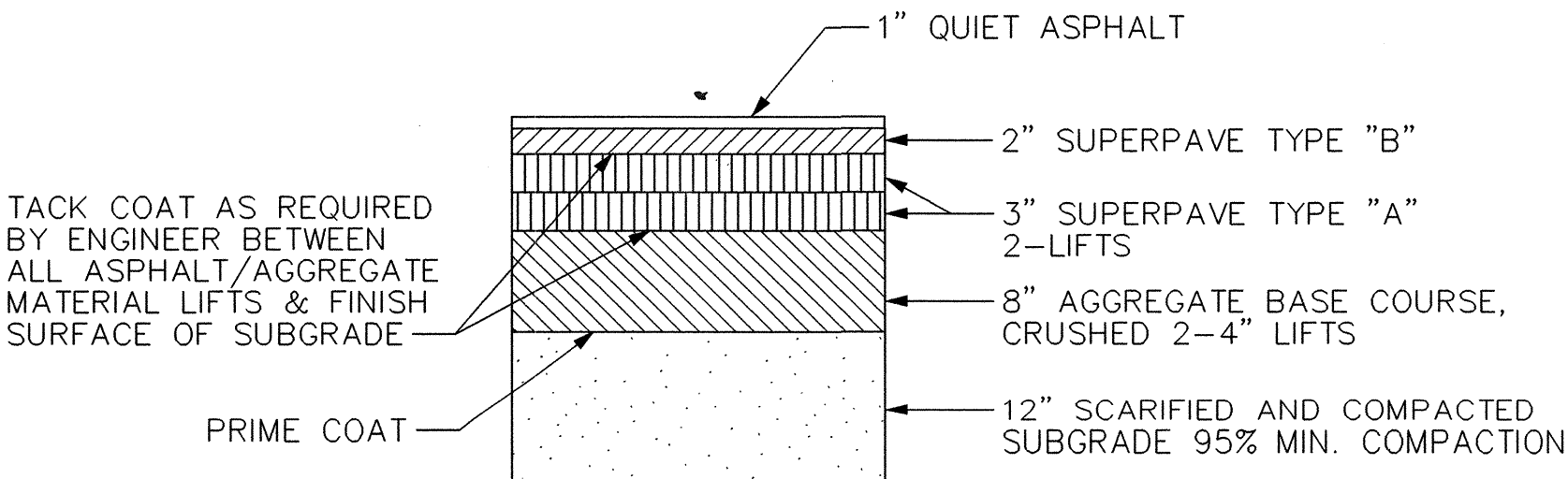
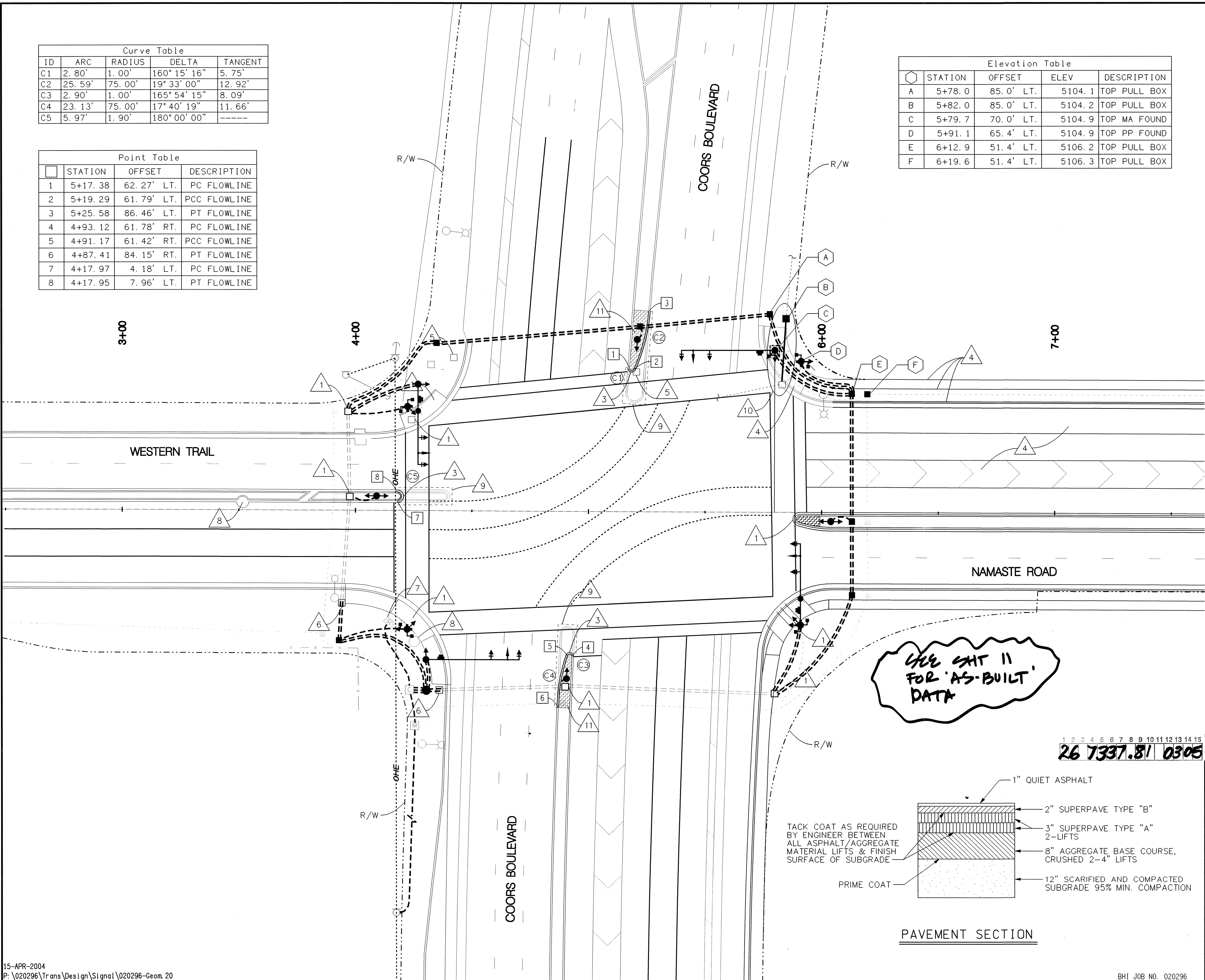
| Curve Table | | | | |
|-------------|--------|--------|--------------|---------|
| ID | ARC | RADIUS | DELTA | TANGENT |
| C1 | 2.80' | 1.00' | 160° 15' 16" | 5.75' |
| C2 | 25.59' | 75.00' | 19° 33' 00" | 12.92' |
| C3 | 2.90' | 1.00' | 165° 54' 15" | 8.09' |
| C4 | 23.13' | 75.00' | 17° 40' 19" | 11.66' |
| C5 | 5.97' | 1.90' | 180° 00' 00" | ----- |

| Point Table | | | |
|-------------|---------|-------------|--------------|
| STATION | OFFSET | DESCRIPTION | |
| 1 | 5+17.38 | 62.27' LT. | PC FLOWLINE |
| 2 | 5+19.29 | 61.79' LT. | PCC FLOWLINE |
| 3 | 5+25.58 | 86.46' LT. | PT FLOWLINE |
| 4 | 4+93.12 | 61.78' RT. | PC FLOWLINE |
| 5 | 4+91.17 | 61.42' RT. | PCC FLOWLINE |
| 6 | 4+87.41 | 84.15' RT. | PT FLOWLINE |
| 7 | 4+17.97 | 4.18' LT. | PC FLOWLINE |
| 8 | 4+17.95 | 7.96' LT. | PT FLOWLINE |

| Elevation Table | | | | |
|-----------------|--------|-----------|-------------|--------------|
| STATION | OFFSET | ELEV | DESCRIPTION | |
| A | 5+78.0 | 85.0' LT. | 5104.1 | TOP PULL BOX |
| B | 5+82.0 | 85.0' LT. | 5104.2 | TOP PULL BOX |
| C | 5+79.7 | 70.0' LT. | 5104.9 | TOP MA FOUND |
| D | 5+91.1 | 65.4' LT. | 5104.9 | TOP PP FOUND |
| E | 6+12.9 | 51.4' LT. | 5106.2 | TOP PULL BOX |
| F | 6+19.6 | 51.4' LT. | 5106.3 | TOP PULL BOX |

KEYED NOTES

- EXISTING TO REMAIN
- NOT USED
- CONSTRUCT NEW MEDIAN NOSE
- TO BE CONSTRUCTED BY OTHERS
- REMOVE AND DISPOSE EXISTING PULL BOX
- REMOVE AND DISPOSE EXISTING PULL BOX. INTERCEPT EXISTING CONDUIT AND EXTEND TO NEW LOCATION. SEE SIGNAL PLAN FOR LOCATION.
- SPLICE CABINET TO REMAIN
- MANHOLE TO REMAIN
- SAWCUT EXISTING PAVEMENT 1' FROM LIP OF GUTTER. REMOVE MEDIAN AND CURB & GUTTER. INSTALL NEW PAVEMENT PER PAVEMENT SECTION THIS SHEET.
- LOCATE AND INTERCEPT EXISTING INTERCONNECT CONDUIT NORTH OF EXISTING PULL BOX LOCATION. PULL EXISTING INTERCONNECT CABLE BACK FROM SPLICE CABINET. INSTALL NEW LARGE SIZE PULL BOX. INSTALL NEW CONDUIT FROM EXISTING PULL BOX LOCATION TO NEW PULL BOX. PULL INTERCONNECT CABLE BACK TO SPLICE CABINET.
- CONSTRUCT MEDIAN PATTERNED CONCRETE PER COA STD DWG 2408



PAVEMENT SECTION

| | | | | | | | |
|------------------------|-----------|---|------|--------------------|-----|-----------------|---------|
| AS-BUILT INFORMATION | | BENCH MARKS | | SURVEY INFORMATION | | ENGINEER'S SEAL | |
| CONTRACTOR | INSPECTOR | USC & GS BRASS TABLE STAMPED | DATE | NO. | BY | DATE | REMARKS |
| N/S | N/S | 1969, GEOGRAPHIC POSITION (NAD 1927) | | | | | |
| DATE | DATE | N.M. State Plane Coordinates (Central Zone) | | | | | |
| 2/05 | 2/05 | X = 366,806.62 Y = 1,502,737.54 | | | | | |
| | | Ground-to-Grid Factor = 0.9996734 | | | | | |
| | | AD = -001'5"24" | | | | | |
| MICRO-FILM INFORMATION | | RECORDED BY | | DESIGNED BY | | CHECKED BY | |
| | | | | AG | LAH | 2/2004 | 2/2004 |
| | | | | | KT | 2/2004 | 2/2004 |

Bohannon

Huston

Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335

ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

CITY OF ALBUQUERQUE

PUBLIC WORKS DEPARTMENT

ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL

INTERSECTION PLAN

DESIGN REVIEW COMMITTEE

CITY ENGINEER APPROVAL

LAST DESIGN UPDATE

DESIGN REVIEW COMMITTEE

MAY 2 4 2004

CITY ENGINEER

JUN - 4 2004

CITY PROJECT NO.

733781

ZONE MAP NO.

F-11

SHEET

3

OF

14

SIGNAL NOTES

1.

ALL WORK ON THESE PLANS TO BE PERFORMED UNDER THIS CONTRACT SHALL CONFORM TO THE CURRENT NATIONAL ELECTRIC CODE, THE STANDARDS OF THE NATIONAL BOARD OF FIRE UNDERWRITERS FOR ELECTRICAL WIRING AND APPARATUS AND THE CITY'S TRAFFIC ENGINEERING OPERATIONS THIRD DRAFT SPECIFICATIONS (JUNE 1994).
2.

LOCATIONS OF CONDUITS, FOUNDATIONS, METER PEDESTALS, CONTROL CABINETS, POLES, PULL BOXES, MANHOLES, AND SPLICE CABINETS SHOWN ON THE PLANS ARE SCHEMATIC AND SHALL BE ADJUSTED IN THE FIELD TO MAXIMIZE CLEAR SPACE AVAILABLE FOR PEDESTRIANS AND WHEELCHAIRS TO COMPLY WITH THE AMERICAN WITH DISABILITIES ACT. THE CONTRACTOR SHALL MEET WITH THE CITY'S TRAFFIC ENGINEERING OPERATIONS PERSONNEL IN THE FIELD AT ALL LOCATIONS TO SPOT EQUIPMENT BEFORE BEGINNING THE WORK. ALL SUCH EQUIPMENT SHALL BE INSTALLED WITHIN THE RIGHT-OF-WAY.
3.

THE CONTRACTOR IS WARNED THAT EXISTING CONDUITS MAY CONTAIN AC POWER AND CAUTION SHALL BE EXERCISED IN INTERCEPTING OR INSTALLING CABLE IN EXISTING CONDUIT.
4.

THE CONTRACTOR SHALL BORE, DRILL OR PUSH WHEN CROSSING EXISTING PAVEMENTS AND ANY DRIVEWAYS FOR SIDE STREET CROSSINGS. BEFORE CONDUIT CAN BE BORED, DRILLED OR PUSHED THE CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES. THE CONTRACTOR SHALL LOCATE AND EXPOSE GAS LINES WHICH CROSS ANY PROPOSED BORES. THESE EXCAVATIONS SHALL REMAIN OPEN UNTIL AFTER THE BORE IS COMPLETE. CONTRACTOR SHALL REMOVE AND REPLACE IN KIND ANY SIDEWALK OR PAVEMENT REQUIRED TO EXPOSE SUCH LINES. THE CONTRACTOR MAY CUT, TRENCH, AND REPLACE EXISTING PAVEMENT ONLY WHEN APPROVED BY THE PROJECT MANAGER.
5.

SPLICING OF COMMUNICATIONS CABLE WILL NOT BE PERMITTED IN PULL BOXES. SPLICING OF COMMUNICATIONS CABLE (CONNECTIONS) WILL BE PERMITTED ONLY AT SPLICE CABINETS OR CONTROLLER CABINETS WITH SPLICE BARS, SPLICING OF TRAFFIC SIGNALS MCC WILL BE PERMITTED IN LARGE PULL BOXES INCLUDING LARGE MEDIAN PULL BOXES. SPLICING OF VIDEO DETECTION COAXIAL CABLE WILL NOT BE PERMITTED FROM THE MASTARM BASE TO THE CONTROLLER CABINET. SPLICING OF OPTICAL DETECTOR CABLE WILL NOT BE PERMITTED FROM THE DETECTOR TO THE CONTROLLER CABINET.
6.

ALL VIDEO DETECTION COAXIAL AND POWER CABLES SHALL BE TAGGED AT THE CONTROL CABINET TO IDENTIFY EACH CABLE BY CAMERA NUMBER AND LOCATION. ALL OPTICAL DETECTOR CABLE SHALL BE TAGGED AT THE CONTROL CABINET TO IDENTIFY EACH BY DIRECTION AND LOCATION.
7.

ALL PULL BOXES SHALL BE REINFORCED POLYMER MORTAR HEAVY DUTY TYPE WITH REINFORCED POLYMER MORTAR HEAVY DUTY COVERS. CONCRETE COVERS, STEEL COVERS, AND CONCRETE PULL BOXES WILL NOT BE ACCEPTABLE.
8.

THE CONTRACTOR SHALL NOTIFY THE CITY OF ALBUQUERQUE TRAFFIC ENGINEERING OPERATIONS (857-8000) THREE WORKING DAYS IN ADVANCE OF ANY ANTICIPATED WORK ON SIGNALS, LIGHTING, AND POWER SERVICES. TRAFFIC ENGINEERING OPERATIONS PERSONNEL WILL ASSIST THE CONTRACTOR IN FIELD LOCATION OF EQUIPMENT, COLOR, CODING OF WIRING, AND MUST BE PRESENT WHEN SIGNALS AND LIGHTING ARE SHUT-OFF OR TURNED ON. THE CONTRACTOR SHALL ALSO NOTIFY THE CITY OF ALBUQUERQUE TRAFFIC ENGINEERING OPERATIONS EACH TIME A TRAFFIC SIGNAL CONTROL DOOR IS OPEN.
9.

THE CONTRACTOR SHALL NOTIFY PNM 30 DAYS IN ADVANCE OF ANTICIPATED POWER SERVICE CONNECTIONS. THE CONTRACTOR SHALL COORDINATE WITH PUBLIC SERVICE COMPANY TO ESTABLISH ELECTRICAL SERVICE IN THE CITY'S NAME. THE CONTRACTOR SHALL PAY THE LOCAL POWER COMPANY ALL COSTS TO PROVIDE ELECTRICAL SERVICE. THIS WORK IS CONSIDERED INCIDENTAL TO CONSTRUCTION.
10.

THE CONTRACTOR SHALL REMOVE ALL CONFLICTING SIGNING AND DELIVER TO THE CITY TRAFFIC ENGINEERING YARDS WHEN TRAFFIC SIGNALS ARE PUT INTO OPERATION.
11.

LIVE UNUSED CONDUCTORS WILL NOT BE ALLOWED AT MASTARM POLES AND PEDESTAL POLES. ALL SUCH UNUSED CONDUCTORS SHALL BE DISCONNECTED AT THE LARGE PULL BOX ADJACENT TO THE POLE.
12.

IF TRENCH WIDTHS LESS THAN 12" ARE PROPOSED BY THE CONTRACTOR, APPROVED COMPACTION METHODS SHALL BE USED DURING BACKFILL TO PREVENT LATENT TRENCH FAILURES. THE CONTRACTOR SHALL USE GROUT OR LEAN FILL AS APPROVED BY THE PROJECT MANAGER IN LIEU OF EARTH BACKFILL.
13.

FOR CONDUITS CONTAINING ONLY LOW VOLTAGE COMMUNICATIONS CABLES, THE REQUIREMENTS FOR A SINGLE CONDUCTOR BARE COPPER #8 AWG MAY BE WAIVED WHERE PERMITTED BY THE NATIONAL ELECTRIC CODE.
14.

THE CITY OF ALBUQUERQUE TRAFFIC ENGINEERING OPERATIONS PERSONNEL WILL PROVIDE TRAFFIC SIGNAL TIMING PLANS AND WILL PROGRAM TRAFFIC SIGNAL CONTROLLERS.
15.

EXISTING CONDUITS TO BE REMOVED OR ABANDONED SHALL HAVE ALL WIRING REMOVED.
16.

EXISTING CONDUITS SHALL BE REPAIRED, ADJUSTED, OR REPLACED AS DIRECTED BY THE PROJECT MANAGER WHERE ELECTRICAL PULL BOXES OR TRAFFIC MANHOLES ARE INSTALLED OR REPLACED.
17.

THE CONTRACTOR SHALL PROVIDE OFF-DUTY POLICE OFFICERS TO DIRECT TRAFFIC WHEN SIGNALS ARE TURNED OFF.
18.

ALL DATA SHOWN HEREIN CONCERNING EXISTING UTILITIES HAS BEEN OBTAINED FROM "AS-BUILT" DRAWINGS AND FROM FIELD OBSERVATIONS WHICH MAY OR MAY NOT BE ACCURATE. THE CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING ALL EXISTING UTILITIES BY CONTACTING NM ONE CALL SYSTEM AT 260-1990. EXPLORATORY TRENCHING, IF NECESSARY, TO MORE SPECIFICALLY LOCATE UTILITY LINES WILL BE COMPLETED AT THE DISCRETION OF THE CONTRACTOR. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY CONFLICTS IDENTIFIED THROUGH THIS PROCESS. COST OF LOCATING UTILITY LINES INCLUDING EXPLORATORY TRENCHING WILL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
19.

THE CONTRACTOR SHALL REPAIR OR REPLACE ANY LANDSCAPE AREAS OR MEDIANS WITH PATTERNED CONCRETE WHICH HAVE BEEN DAMAGED AS THE RESULT OF DIGGING, TRENCHING AND BORING FOR THE PURPOSE OF INSTALLING CONDUITS, PULL BOXES, MANHOLES, AND FOUNDATIONS FOR CABINETS, MASTARMS AND PEDESTAL POLES.

20. THE FOLLOWING CITY OF ALBUQUERQUE STANDARD DRAWINGS SHALL BE USED FOR THIS PROJECT:
- 2550

- TRAFFIC SIGNAL PULL BOX DETAILS
- 2551

- TRAFFIC SIGNAL MANHOLE DETAILS
- 2555

- TRAFFIC SIGNAL CONTROLLER CABINET & PEDESTRIAN FOUNDATION DETAILS
- 2557

- TRAFFIC SIGNAL SPLICE CABINET GROUND MOUNT (LARGE)
- 2558

- TRAFFIC SIGNAL FOUNDATION DETAILS TYPE II AND TYPE III STANDARDS
- 2560

- TRAFFIC SIGNAL MISCELLANEOUS DETAILS
- 2562C

- TRAFFIC SIGNAL MASTRM DETAILS, TYPE III STANDARD
- 2562D

- TRAFFIC SIGNAL TYPE III STANDARD MISC. DETAILS
- 2568

- TRAFFIC SIGNAL MACHINE VISION VEHICLE DETECTOR SYSTEM
- 2569

- TRAFFIC SIGNAL OPTICAL DETECTOR INSTALLATION DETAILS
- 2570

- TRAFFIC SIGNAL ELECTRICAL SERVICE DETAILS
- 2571

- TRAFFIC SIGNAL METER PEDESTAL DETAILS FOR SIGNAL

TRAFFIC SIGNAL LEGEND

| NEW | EXISTING | ITEM |
|-----|----------|---|
| | | PULL BOX |
| | | SERVICE POLE |
| | | METER PEDESTAL |
| | | CONTROLLER CABINET |
| | | CONDUIT RUN (SIGNALS) |
| | | CONDUIT RUN (LIGHTING) |
| | | TRAFFIC SIGNAL PEDESTAL POLE |
| | | CONDUIT RUN NUMBER (SIGNAL) |
| | | CONDUIT RUN NUMBER (POWER SERVICE) |
| | | TYPE II STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACK PLATE, AND OPTICAL DETECTOR |
| | | TYPE III STANDARD WITH MASTARM, TRAFFIC SIGNAL, BACK PLATE, OPTICAL DETECTOR, LUMINAIRE, AND VIDEO CAMERA |
| | | PEDESTRIAN PUSH BUTTON (MOUNTED TO SIDE OF POLE WHERE INDICATED) |
| | | PEDESTRIAN SIGNALS (MOUNTED TO SIDE OF POLE WHERE INDICATED) |
| | | SPLICE CABINET |
| | | TRAFFIC MANHOLE |
| | | VIDEO CAMERA |

DEFINITIONS

1. "ENGINEER" – FOR THE PURPOSES OF THIS PROJECT, THE TERM "ENGINEER" IS SYNONYMOUS WITH THE TERM "PROJECT MANAGER".

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15
26 7337.81 0405

| AS-BUILT INFORMATION | |
|-----------------------------------|---------|
| CONTRACTOR | MMI |
| STARTED BY | MS |
| INSPECTOR'S FIELD VERIFICATION BY | MS |
| DATE | 2/20/04 |
| FIELD VERIFICATION BY | MS |
| DATE | " |
| CORRECTED BY | MS |
| DATE | " |
| MICRO-FILM INFORMATION | |
| RECORDED BY | |
| DATE | |
| NO. | |

| BENCH MARKS | |
|---|--------------------------------|
| USC & GS BRASS TABLET STAMPED | JOSEPH |
| 1969" | GEOGRAPHIC POSITION (NAD 1927) |
| N.M. State Plane Coordinates (Central Zone) | |
| X= 366,806.82 | Y= 1,502,737.54 |
| Ground-to-Grid Factor = 0.9996734 | |
| ΔG = -00'15"24" | |
| SLD 1929 Elevation = 5113.852 | |
| NO. | |

| SURVEY INFORMATION | |
|--------------------|------|
| FIELD NOTES | |
| BY | DATE |
| NO. | |

| ENGINEER'S SEAL | |
|-----------------|---------|
| | |
| REMARKS | BY |
| REVISIONS | |
| DESIGN | |
| DATE | 2/20/04 |
| DRAWN BY | LH |
| CHECKED BY | KT |

Bohannon **Huston**

Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335

ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL

TRAFFIC SIGNAL
NOTES & LEGEND

| | | | |
|-------------------------|------------------------|--------------------|-------------|
| DESIGN REVIEW COMMITTEE | CITY ENGINEER APPROVAL | MO./DAY/YR. | MO./DAY/YR. |
| | | | |
| DESIGN REVIEW COMMITTEE | CITY ENGINEER | LAST DESIGN UPDATE | |

| | | | |
|------------------|--------------|-------|----|
| CITY PROJECT NO. | ZONE MAP NO. | SHEET | OF |
| 733781 | F-11 | 4 | 14 |

1. THIS PROJECT IS AN EXTENSION OF AN EXISTING SIGNAL SYSTEM.
THEREFORE THE CONTRACTOR SHALL FURNISH AND INSTALL THE FOLLOWING:

- A. ALL TRAFFIC SIGNAL CONTROLLERS SUPPLIED FOR THIS PROJECT SHALL BE AS APPROVED BY THE CITY OF ALBUQUERQUE.

B. ALL TRAFFIC SIGNAL CONTROLLER CABINETS SUPPLIED FOR THIS PROJECT SHALL BE EIGHT PHASE DUAL RING CABINETS WIRED FOR EIGHT PHASE SYSTEM OPERATION WITH TELEMETRY BOARD, IT TRANSIENT VOLTAGE SUPPRESSOR AND ALL CONNECTING HARNESSSES. THIS CABINET SHALL BE CAPABLE OF VIDEO DETECTION.

- EMERGENCY VEHICLE PRE-EMPT EQUIPMENT SHALL BE 3M "OPTICOM" MODEL 562 OR LATEST MODEL PHASE SELECTORS MOUNTED ON 3M "OPTICOM" MODEL 560 RACKS, OR APPROVED EQUAL. ALL RACKS SHALL BE CAPABLE OF PROVIDING FOUR CHANNELS OF DETECTION. PHASE SELECTOR MODULES SHALL BE CAPABLE OF TWO CHANNELS OF DETECTION EACH. OPTICAL DETECTORS SHALL BE 3M "OPTICOM" MODEL 711, ONE (1) CHANNEL, ONE (1) DIRECTION OR APPROVED EQUAL. OPTICAL DETECTOR CABLE SHALL BE 3M "OPTICOM" MODEL 138 OR EQUAL. A MANUFACTURER'S REPRESENTATIVE SHALL ASSIST THE CONTRACTOR IN THE FIELD AS WORK PROGRESSES TO COMPLETE THE INSTALLATION OF ALL PRE-EMPTION EQUIPMENT AND ASSIST IN SETTING UP, TURNING ON, PROGRAMMING AND FIELD TESTING PRE-EMPTION EQUIPMENT INCLUDING EMITTERS TO INSURE THAT THE EQUIPMENT IS OPERATIONAL.

- A. EITHER AN ECONOLITE SOLOPRO II, ITERIS VANTAGE PLUS, OR TRAFCON VIDEO VEHICLE DETECTION SYSTEM UNIT, ALL NECESSARY CABLES, HARNESSSES, MATERIALS, FITTINGS, AND MISCELLANEOUS COMPONENTS NECESSARY TO PROVIDE A COMPLETE AND OPERATING SYSTEM AND ONE (1) SIGNALIZED INTERSECTION.

B. VIDEO CAMERAS WITH ZOOM LENSES.

C. VIDEO POWER CABLE AS RECOMMENDED BY THE MANUFACTURER.

4. ALL SIGNAL INDICATORS AND PEDESTRIAN SIGNAL HEADS SHALL BE LED, AS APPROVED BY THE CITY. PEDESTRIAN SIGNALS SHALL BE COUNTDOWN AS APPROVED BY THE CITY.

1. REMOVAL OF EXISTING PULL BOXES, CONDUITS, OR OTHER SIGNAL EQUIPMENT FOR INSTALLATION OF NEW SIGNAL EQUIPMENT.

2. CABLE TESTING AND DIAGRAMS.
3. BORING, DRILLING, PUSHING, AND TRENCHING, INCLUDING REMOVAL AND REPLACEMENT OF PAVEMENT, SIDEWALKS, DRIVE PADS, VALLEY GUTTERS, WHEELCHAIR RAMPS, CURB & GUTTER, AND LANDSCAPING (INCLUDING SPRINKLERS). FOR INSTALLATION OF PULL BOXES, CONDUITS, AND SIGNAL FOUNDATIONS, EXCEPT AS NOTED ON THE PLANS.

4. LOCATION OF UTILITY LINES INCLUDING EXPLORATORY TRENCHING AND EXPOSING OF GAS LINES WHEN BORING.

5. DESIGN, MATERIALS, INSTALLATION AND REMOVAL OF SAFETY BARRIER FOR SHIELDING EQUIPMENT OR MATERIAL.

6. APPRISING PUBLIC THROUGH THE LOCAL NEWS MEDIA.

7. HAULING OF MATERIAL TO BE DISPOSED TO CITY LANDFILL.

8. LEAN FILL FOR CONDUIT TRENCHES.


9. PULL BOX ADJUSTMENT TO GRADE.

10. OFF-DUTY POLICE OFFICER FOR TRAFFIC CONTROL.

11. REMOVAL AND REPLACEMENT IN KIND OR BETTER OF LANDSCAPING INCLUDING SPRINKLERS, FOR INSTALLATION OF PULL BOXES, CONDUITS AND SIGNAL FOUNDATIONS.

12. COST FOR PUBLIC SERVICE COMPANY TO PROVIDE ELECTRICAL SERVICE.

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 **CITY OF ALBUQUERQUE**
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
TRAFFIC SIGNAL
EQUIPMENT REQUIREMENTS & INCIDENTAL ITEMS





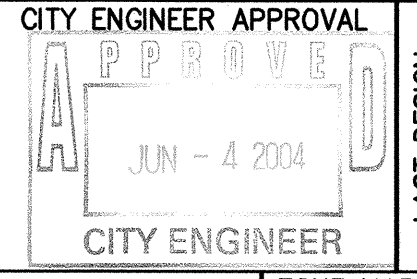
| DESIGN REVIEW COMMITTEE | | CITY ENGINEER APPROVAL | MO./DAY/YR. | MO./DAY/YR. |
|-------------------------|---------------|------------------------|-------------|-------------|
| APPROVED | APPROVED | LAST DESIGN UPDATE | | |
| MAY 24 2004 | JUN - 4 2004 | | | |
| DESIGN REVIEW COMMITTEE | CITY ENGINEER | | | |

| | | | |
|-----------------------------------|-----------------------------|-------------------|-----------------|
| CITY PROJECT NO. 733781 | ZONE MAP NO. F-11 | SHEET 5 | OF 14 |
|-----------------------------------|-----------------------------|-------------------|-----------------|

| TRAFFIC SIGNAL ESTIMATED QUANTITIES | | | | | |
|-------------------------------------|--|--------|-----------------------------|----------|-------|
| ITEM NO | ITEM DESCRIPTION | UNIT | COORS/NAMASTE-WESTERN TRAIL | | |
| | | | SIGNAL | LIGHTING | TOTAL |
| 0421.005 | SERVICE RISER (SIGNAL) | EACH | 1 | | 1 |
| 0421.011 | METER PEDESTAL (SIGNAL) | EACH | 1 | | 1 |
| 0422.004 | TRAFFIC SIGNAL PEDESTAL POLE, 15 FOOT | EACH | 7 | | 7 |
| 0422.019 | TRAFFIC SIGNAL MASTARM, 35 FOOT ARM, TYPE III, TROMBONE | EACH | 2 | | 2 |
| 0422.021 | TRAFFIC SIGNAL MASTARM, 40 FOOT ARM, TYPE III, TROMBONE | EACH | 2 | | 2 |
| 0422.030 | STREET LIGHT STANDARD, SINGLE ARM, 30' (TYPE V DAVIT), CIP | EACH | 2 | | 2 |
| 0423.001 | TRAFFIC SIGNAL FOUNDATION FOR PEDESTAL POLE | EACH | 7 | | 7 |
| 0423.002 | TRAFFIC SIGNAL MASTARM FOUNDATION | EACH | 4 | | 4 |
| 0423.003 | TRAFFIC CONTROLLER FOUNDATION (TYPE M & P CABINETS) | EACH | 1 | | 1 |
| 0423.020 | LUMINAIRE FOUNDATION FOR LUMINAIRE HEIGHT OF 40' OR LESS, CIP | EACH | 2 | | 2 |
| 0424.006 | RIGID ELECTRICAL CONDUIT, 2" INCLUDING TRENCH, BACKFILL, PATCHING, PUSHING, BORING & JACKING | LIN FT | 1170 | 685 | 1855 |
| 0424.011 | RIGID ELECTRICAL CONDUIT, 3" INCLUDING TRENCH, BACKFILL, PATCHING, PUSHING, BORING & JACKING | LIN FT | 1215 | | 1215 |
| 0425.002 | ELECTRICAL PULL BOX, (STANDARD) | EACH | | 9 | 9 |
| 0425.003 | ELECTRICAL PULL BOX, (LARGE) | EACH | 7 | | 7 |
| 0425.020 | TRAFFIC SIGNAL MANHOLE | EACH | 1 | | 1 |
| 0425.101 | ELECTRICAL PULL BOX, ANY SIZE, REMOVE AND DISPOSE | EACH | 5 | | 5 |
| 0426.001 | SINGLE CONDUCTOR, 2 | LIN FT | 390 | 1490 | 1880 |
| 0426.004 | SINGLE CONDUCTOR, 8 | LIN FT | 880 | | 880 |
| 0426.005 | SINGLE CONDUCTOR, 10 | LIN FT | | 480 | 480 |
| 0426.010 | MULTI-CONDUCTOR CABLE 5 | LIN FT | 2975 | | 2975 |
| 0426.011 | MULTI-CONDUCTOR CABLE 7 | LIN FT | 140 | | 140 |
| 0426.014 | MULTI-CONDUCTOR CABLE 20 | LIN FT | 1760 | | 1760 |
| 0426.020 | COMMUNICATION CABLE 6 PAIR | LIN FT | 45 | | 45 |
| 0427.002 | 3 SECTION TRAFFIC SIGNAL ASSEMBLY | EACH | 20 | | 20 |
| 0427.005 | 5 SECTION TRAFFIC SIGNAL ASSEMBLY | EACH | 4 | | 4 |
| 0427.031 | 3 SECTION BACKPLATE | EACH | 8 | | 8 |
| 0428.010 | PEDESTRIAN PUSH BUTTON STATION | EACH | 4 | | 4 |
| 0428.071 | PHASE SELECTOR MODULE, 2 CHANNEL | EACH | 2 | | 2 |
| 0428.075 | OPTICAL DETECTOR 1D/1C | EACH | 4 | | 4 |
| 0428.078 | OPTICAL DETECTOR CABLE | LIN FT | 1195 | | 1195 |
| 0428.XXX | VIDEO DETECTION SYSTEM | EACH | 1 | | 1 |
| 0428.092 | VIDEO COAXIAL CABLE | LIN FT | 1225 | | 1225 |
| 0428.093 | VIDEO POWER CABLE | LIN FT | 1225 | | 1225 |
| 0428.094 | VIDEO CAMERA | EACH | 4 | | 4 |
| 0429.001 | TRAFFIC ACTUATED CONTROLLER | EACH | 1 | | 1 |
| 0429.021 | EIGHT PHASE DUAL RING CONTROLLER CABINET | EACH | 1 | | 1 |
| 0432.002 | ROADWAY LUMINAIRE, TYPE 400S | EACH | 4 | | 4 |
| 0450.001 | ALUMINUM PANEL SIGN | EACH | 4 | | 4 |

FOR INFORMATION ONLY

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15
26 7337.81 0605

| | |
|---|---|
| Bohannon  Huston  | |
| Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES | |
|  CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP | |
| COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL TRAFFIC SIGNAL ESTIMATED QUANTITIES | |
| DESIGN REVIEW COMMITTEE  MAY 24 2004 | CITY ENGINEER APPROVAL  JUN - 4 2004 |
| LAST DESIGN UPDATE | MO./DAY/YR. |
| CITY PROJECT NO. 733781 | ZONE MAP NO. F-11 |
| SHEET 6 | OF 14 |

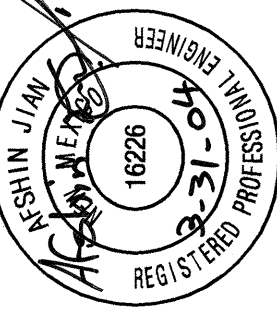
| ENGINEER'S SEAL | | SURVEY INFORMATION | | BENCH MARKS | | AS-BUILT INFORMATION | |
|---|--|--------------------|--|---|--|--|--|
|  | | NO. BY DATE | | USC & GS BRASS TABLET STAMPED "JOSEPH 1969" GEOGRAPHIC POSITION (NAD 1927) N.M. State Plane Coordinates (Central Zone) X= 366,806.62 Y= 1,502,737.54 Ground-to-to-Grid Factor = 0.9996734 Δα = -00'15"24" | | CONTRACTOR MW1 STAKED BY PS INSPECTOR'S FIELD VERIFICATION BY GAC DATE 4/05 CORRECTED BY DEC DATE 4/05 MICRO-FILM INFORMATION RECORDED BY DATE | |
| REMARKS | | BY | | DATE | | DATE | |
| DESIGN | | DATE 2/2004 | | DATE 2/2004 | | DATE 2/2004 | |
| DRAWN BY LAH | | DATE 2/2004 | | DATE 2/2004 | | DATE 2/2004 | |
| CHECKED BY KT | | DATE 2/2004 | | DATE 2/2004 | | DATE 2/2004 | |

Diagram illustrating the proposed bus rapid transit system for the 10th Avenue Corridor, showing four travel directions and their respective stations and lane configurations.

Northbound Coors Blvd:

- Stations: MA3 (Main Avenue), PP4 (Parkway), PP3 (Parkway).
- Lane Configuration: 3B (Leftmost lane), 3C (Center turn lane), 3M1 (Bus lane), 3M2 (Bus lane), 3A (Rightmost lane).

Southbound Coors Blvd:

- Stations: MA4 (Main Avenue), PP7 (Parkway), MA1 (Main Avenue).
- Lane Configuration: 1B (Leftmost lane), 1C (Center turn lane), 1M1 (Bus lane), 1M2 (Bus lane), 1A (Rightmost lane).

Eastbound Namaste Road:

- Stations: MA3 (Main Avenue), PP6 (Parkway), PP2 (Parkway), MA4 (Main Avenue), PP1 (Parkway).
- Lane Configuration: 4B (Leftmost lane), 4C (Center turn lane), 4D (Center turn lane), 4M1 (Bus lane), 4M2 (Bus lane), 4A (Rightmost lane), 4E (Far right lane).

Westbound Western Trail:

- Stations: PP1 (Parkway), PP2 (Parkway), PP6 (Parkway), MA2 (Main Avenue), PP5 (Parkway).
- Lane Configuration: 2B (Leftmost lane), 2C (Center turn lane), 2D (Center turn lane), 2M1 (Bus lane), 2M2 (Bus lane), 2A (Rightmost lane), 2E (Far right lane).

← WESTERN TR
NAMASTE RD →

MOUNT ON ⁵⁻²MASTARM 3

S-3

SEE SHEET 12 FOR SIGN FACE DETAILS

PHASE 7

PHASE 7

PHASE 4

RED BALL ALL SIGNAL HEADS

ALL RED, THEN PHASE 2 AND 6 GREEN

| | |
|------|-------------------------------|
| MAI | MASTARM NUMBER |
| PP1 | PEDESTAL POLE NUMBER |
| PPB1 | PEDESTRIAN PUSH BUTTON NUMBER |
| CC1 | CONTROL CABINET NUMBER |
| SC1 | SPLICE CABINET NUMBER |
| PB1 | PULL BOX NUMBER (SIGNALS) |
| PBS1 | PULL BOX NUMBER (POWER) |
| 3A | SIGNAL HEAD NUMBER |
| P1 | PEDESTRIAN SIGNAL NUMBER |
| MH1 | TRAFFIC MANHOLE NUMBER |

| | |
|-----|--------------------------------|
| MAX | SIGNAL & CABINET ID |
| X | CONDUIT RUN ID (SIGNALS) |
| SX | CONDUIT RUN ID (POWER SERVICE) |

15-APR-2004
P: \020296\Trans\Design\Signal\020296-SGPL.20

522 SHT 11
FOR 'AS-BUILT'
DATA

| FROM | TO | CONDUIT TRACE | TOTAL LENGTH |
|------|-----|-------------------------|--------------|
| MA1 | CC1 | MA1-28-2 | 110' |
| MA2 | CC1 | MA2-30-8-6-4-2 | 305' |
| MA3 | CC1 | MA3-32-18-20-22-24-26-2 | 485' |
| MA4 | CC1 | MA4-34-24-26-2 | 295' |
| | | TOTAL | 1195' |

| FROM | TO | CONDUIT TRACE | TOTAL LENGTH |
|------|-----|-------------------------|--------------|
| MA1 | CC1 | MA1-28-2 | 115' |
| MA2 | CC1 | MA2-30-8-6-4-2 | 305' |
| MA3 | CC1 | MA3-32-18-20-22-24-26-2 | 490' |
| MA4 | CC1 | MA4-34-24-26-2 | 305' |
| | | TOTAL | 1225' |

```

graph TD
    Start(( )) --> P1[PHASE 1]
    P1 --> P2[PHASE 2]
    P2 --> P3[PHASE 3]
    P3 --> P4[PHASE 4]
    P4 --> P5[PHASE 5]
    P5 --> P6[PHASE 6]
    P6 --> P7[PHASE 7]
    P7 --> P8[PHASE 8]
    P8 --> Start
  
```

The diagram illustrates a closed-loop system with eight phases, each represented by a box containing a directional arrow and a label. The flow is as follows:

- PHASE 1:** A black arrow pointing left, turning 90 degrees downward.
- PHASE 2:** A black arrow pointing straight up.
- PHASE 3:** A black arrow pointing right, turning 90 degrees upward.
- PHASE 4:** A black arrow pointing straight right.
- PHASE 5:** A black arrow pointing right, turning 90 degrees upward.
- PHASE 6:** A black arrow pointing straight down.
- PHASE 7:** A black arrow pointing left, turning 90 degrees downward.
- PHASE 8:** A black arrow pointing straight left.

The flow starts at the top, proceeds through PHASE 1 to PHASE 2, then PHASE 3 to PHASE 4, then PHASE 5 to PHASE 6, then PHASE 7 to PHASE 8, and finally returns to the start.

2B, 2C, 2D, 4B, 4C, 4D

1A, 1B, 1C, 1D, 2E, 4E

P1, P2, P3, P4, P5, P6, P7, P8




1M1, 1M2, 2M1, 2M2, 3M1, 3M2, 4M1, 4M2


1B, 1C, 3B, 3C

1. PULL BOXES SHOWN IN PARENTHESES ARE EXISTING, AND SHALL REMAIN.
2. PULL BOXES PB1 AND PB4 THRU PB9 ARE LARGE SIZE.
3. REMOVE EXISTING PULL BOX. EXTEND EXISTING CONDUIT TO NEW PULL BOX OR MANHOLE LOCATION.
4. LOCATION OF PP1 MAY BE ADJUSTED TO AVOID MANHOLE, SPLICE CABINET AND PULL BOX AS APPROVED BY THE COA TRAFFIC OPERATIONS DEPARTMENT.
5. INSTALL 3" CONDUIT AND COMMUNICATION CABLE, 6 PAIR FROM CONTROLLER TO EXISTING SPLICE CABINET (45').
6. LOCATE MA4 TO AVOID OBSTRUCTING VIEW OF EXISTING SUBDIVISION SIGN.

| ID# | TYPE | STATION | OFFSET | LUMINAIRE MOUNTING HEIGHT | LUMINAIRE ARM |
|-----|----------------------|---------|----------|---------------------------------|------------------|
| MA1 | MASTARM-40' TYPE III | 4+30.4 | 63.5' RT | 40' | 20' |
| MA2 | MASTARM-35' TYPE III | 4+26.7 | 54.8' RT | 40' | 20' |
| MA3 | MASTARM-40' TYPE III | 5+79.7 | 70.0' LT | 40' | 20' |
| MA4 | MASTARM-35' TYPE III | 5+91.1 | 47.8' RT | 40' | 20' |
| PP1 | PEDESTAL-15' TYPE I | 4+22.3 | 50.5' RT | - | - |
| PP2 | PEDESTAL-15' TYPE I | 4+09.0 | 6.2' LT | - | - |
| PP3 | PEDESTAL-15' TYPE I | 4+22.3 | 45.3' LT | - | - |
| PP4 | PEDESTAL-15' TYPE I | 5+20.7 | 74.3' LT | - | - |
| PP5 | PEDESTAL-15' TYPE I | 5+91.1 | 65.4' LT | - | - |
| PP6 | PEDESTAL-15' TYPE I | 6+03.9 | 3.8' RT | - | - |
| PP7 | PEDESTAL-15' TYPE I | 4+90.8 | 71.7' RT | - | - |
| CC1 | CONTROLLER | 4+23.5 | 76.8' RT | - | - |

BHI JOB NO. 020296

Bohannon  **Huston** INC.
 Courtyard | 7500 Jefferson St. NE Albuquerque, NM 87109-4
ENGINEERING  **SPATIAL DATA**  **ADVANCED TECHNOLOGY**


CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP
 COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL

| DESIGN REVIEW COMMITTEE | | CITY ENGINEER APPROVAL | | MO./DAY/YR. | | MO./DAY/YR. | |
|---|--|---|---|-------------|--|-------------|--|
| <div style="border: 1px solid black; padding: 5px; text-align: center;"> APPROVED MAY 24 2004 DESIGN REVIEW COMMITTEE </div> | | <div style="border: 1px solid black; padding: 5px; text-align: center;"> APPROVED JUN - 4 2004 CITY ENGINEER </div> | <div style="border: 1px solid black; padding: 5px; text-align: center;"> LAST DESIGN UPDATE </div> | | | | |
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|------------------|--------------|-------|----|
| CITY PROJECT NO. | ZONE MAP NO. | SHEET | OF |
| 733781 | F-11 | 7 | 14 |

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|----|---|---|---|---|---|---|---|---|----|----|----|----|----|----|---|
| 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | |
| 26 | | | 7 | 3 | 3 | 7 | . | 8 | 1 | | | 0 | 7 | 0 | 5 |

| CONDUIT AND CONDUCTOR REQUIREMENTS | | | | | | | | | | | | | | |
|------------------------------------|-------------|-------|----|------|----------------------|--|-------------------|--------------------|--------------------|--------------------|----------------|---------------------|-------------------------|------------------------|
| CONDUIT LENGTH, SIZE, AND TYPE | | | | | | CONDUIT FILL BY CONTRACTOR LENGTH AND TYPE | | | | | | | | |
| RUN ID ## | SIZE/LENGTH | | | TYPE | REMARKS | MCC 5 (# @ FT) | MCC 7 (# @ FT) | MCC 20 (# @ FT) | SCC #2 (# @ FT) | SCC #8 (# @ FT) | PS (# @ FT) | OPTICOM (# @ FT) | VIDEO POWER (# @ FT) | VIDEO COAX (# @ FT) |
| | 1" | 2" | 3" | | | | | | | | | | | |
| S1 | | 100 | | REC | TRANSFORMER TO METER | | | | 3 @ 105 | | | | | |
| S2 | | 20 | | REC | METER TO CC1 | | | | 3 @ 25 | | | | | |
| 1 | | | 25 | REC | CC1 TO MH1 | 1 @ 30 | | 2 @ 30 | | 1 @ 30 | | | | |
| 2 | | | 25 | REC | CC1 TO MH1 | | | | | | | 4 @ 30 | 4 @ 30 | 4 @ 30 |
| 3 | | | 25 | REC | CC1 TO MH1 | 1 @ 30 | | 2 @ 30 | | 1 @ 30 | | | | |
| 4 | | 55 | | REC | MH1 TO PB1 | | | | | | | 1 @ 60 | 1 @ 60 | 1 @ 60 |
| 5 | | 55 | | REC | MH1 TO PB1 | 1 @ 60 | | 2 @ 60 | | 1 @ 60 | | | | |
| 6 | 104 | 65 | | REC | PB1 TO (PB2) | | | | | | | 1 @ 70 | 1 @ 70 | 1 @ 70 |
| 7 | | 1065 | | REC | PB1 TO (PB2) | 1 @ 70 | | 2 @ 70 | | 1 @ 70 | | | | |
| 8 | | 45 | | REC | (PB2) TO (PB3) | | | | | | | 1 @ 50 | 1 @ 50 | 1 @ 50 |
| 9 | | 45 | | REC | (PB2) TO (PB3) | 1 @ 50 | | 2 @ 50 | | 1 @ 50 | | | | |
| 10 | | 10855 | | REC | (PB3) TO PB4 | | | | | | | | | |
| 11 | | 10855 | | REC | (PB3) TO PB4 | 1 @ 60 | | 2 @ 60 | | 1 @ 60 | | | | |
| 12 | | 95 | | REC | PB4 TO PB5 | | | | | | 1 @ 100 | | | |
| 13 | | | 95 | REC | PB4 TO PB5 | 1 @ 100 | | 2 @ 100 | | 1 @ 100 | | | | |
| 14 | | 7065 | | REC | PB5 TO PB6 | | | | | | 1 @ 70 | | | |
| 15 | | 7065 | | REC | PB5 TO PB6 | 1 @ 70 | | 2 @ 70 | | 1 @ 70 | | | | |
| 16 | | 1155 | | REC | PB6 TO PB7 | | | | | | | | | |
| 17 | | 1155 | | REC | PB6 TO PB7 | 1 @ 60 | | 2 @ 60 | | 1 @ 60 | | | | |
| 18 | | 10560 | | REC | PB7 TO PB8 | | | | | | | 1 @ 65 | 1 @ 65 | 1 @ 65 |
| 19 | | 10560 | | REC | PB7 TO PB8 | 1 @ 65 | | 2 @ 65 | | 1 @ 65 | | | | |
| 20 | | 10440 | | REC | PB8 TO PB9 | | | | | | | 1 @ 45 | 1 @ 45 | 1 @ 45 |
| 21 | | 10440 | | REC | PB8 TO PB9 | 1 @ 45 | | 2 @ 45 | | 1 @ 45 | | | | |
| 22 | | 10800 | | REC | PB9 TO (PB10) | | | | | | | 1 @ 65 | 1 @ 65 | 1 @ 65 |
| 23 | | 10800 | | REC | PB9 TO (PB10) | 1 @ 65 | | 2 @ 65 | | 1 @ 65 | | | | |
| 24 | | 100 | | REC | (PB10) TO (PB11) | | | | | | | 2 @ 105 | 2 @ 105 | 2 @ 105 |
| 25 | | 100 | | REC | (PB10) TO (PB11) | 1 @ 105 | | 2 @ 105 | | 1 @ 105 | | | | |
| 26 | | 10465 | | REC | PB11 TO MH1 | | | | | | | 2 @ 70 | 2 @ 70 | 2 @ 70 |
| 27 | | 1085 | | REC | PB11 TO MH1 | 1 @ 70 | | 2 @ 70 | | 1 @ 70 | | | | |
| 28 | | 20 | | REC | MA1 TO MH1 | | | | | | | 1 @ 25 | 1 @ 25 | 1 @ 25 |
| 29 | | 20 | | REC | MA1 TO MH1 | 3 @ 25 | | | | | | | | |
| 30 | | 2640 | | REC | MA2 TO (PB3) | | | | | | | 1 @ 45 | 1 @ 45 | 1 @ 45 |
| 31 | | 2640 | | REC | MA2 TO (PB3) | 3 @ 45 | | | | | | | | |
| 32 | | 3345 | | REC | MA3 TO PB7 | | | | | | | 1 @ 50 | 1 @ 50 | 1 @ 50 |
| 33 | | 3345 | | REC | MA3 TO PB7 | 4 @ 50 | | | | | | | | |
| 34 | | 35 | | REC | MA4 TO (PB10) | | | | | | | 1 @ 40 | 1 @ 40 | 1 @ 40 |
| 35 | | 35 | | REC | MA4 TO (PB10) | 8 @ 40 | | | | | | | | |
| 36 | | 35 | | REC | PP1 TO PB1 | 6 @ 40 | | | | | | | | |
| 37 | | 15 | | REC | PP2 TO (PB2) | 1 @ 20 | 1 @ 20 | | | | | | | |
| 38 | | 4030 | | REC | PP3 TO (PB3) | 5 @ 35 | | | | | | | | |
| 39 | | 515 | | REC | PP4 TO PB5 | | 1 @ 20 | | | | | | | |
| 40 | | 4025 | | REC | PP5 TO PB7 | 5 @ 30 | | | | | | | | |
| 41 | | 615 | | REC | PP6 TO PB8 | 1 @ 20 | 1 @ 20 | | | | | | | |
| 42 | | 515 | | REC | PP7 TO (PB11) | | 1 @ 20 | | | | | | | |
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ABBREVIATIONS

| | |
|-------|-----------------------------|
| CC 6 | COMMUNICATION CABLE-6 PAIR |
| CC 12 | COMMUNICATION CABLE-12 PAIR |
| DLIC | DETECTOR LOOP LEAD IN CABLE |
| MA | MASTARM |
| MCC | MULTI CONDUCTOR CABLE |
| PB | PULL BOX |
| PP | PEDESTAL POLE |
| PPB | PEDESTRIAN PUSH BUTTON |
| REC | RIGID ELECTRIC CONDUIT |
| SCC | SINGLE CONDUCTOR CABLE |

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ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

CITY OF ALBUQUERQUE

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COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL

TRAFFIC SIGNAL

CABLES & CONDUITS

DESIGN REVIEW COMMITTEE

CITY ENGINEER APPROVAL

MO./DAY/YR.

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APPROVED

APPROVED

DESIGN REVIEW COMMITTEE

CITY ENGINEER

LAST DESIGN UPDATE

CITY PROJECT NO.

733781

ZONE MAP NO.

F-11

SHEET

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31-MAR-2004
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BHI JOB NO. 020296

| CONDUIT AND CONDUCTOR REQUIREMENTS | | | | | | | | | | | | | | | | | | | | | | | |
|------------------------------------|-------------|------|-----|------|----------------------|--|-----|-------------------|-----|--------------------|-----|--------------------|------|--------------------|--|----------------|--|---------------------|----|-------------------------|----|------------------------|----|
| CONDUIT LENGTH, SIZE, AND TYPE | | | | | | CONDUIT FILL BY CONTRACTOR LENGTH AND TYPE | | | | | | | | | | | | | | | | | |
| RUN ID ## | SIZE/LENGTH | | | TYPE | REMARKS | MCC 5 (# @ FT) | | MCC 7 (# @ FT) | | MCC 20 (# @ FT) | | SCC #2 (# @ FT) | | SCC #8 (# @ FT) | | PS (# @ FT) | | OPTICOM (# @ FT) | | VIDEO POWER (# @ FT) | | VIDEO COAX (# @ FT) | |
| | 1" | 2" | 3" | | | | | | | | | | | | | | | | | | | | |
| MA1 | | | | | BASE TO 1A | 1 @ | 15 | | | | | | | | | | | | | | | | |
| MA1 | | | | | BASE TO 1M1 | 1 @ | 60 | | | | | | | | | | | | | | | | |
| MA1 | | | | | BASE TO 1M2 | 1 @ | 50 | | | | | | | | | | | | | | | | |
| MA1 | | | | | BASE TO CAM3 | | | | | | | | | | | | | | | 1 @ | 60 | 1 @ | 60 |
| MA1 | | | | | BASE TO OPTICOM SB | | | | | | | | | | | | | 1 @ | 55 | | | | |
| MA2 | | | | | BASE TO 2A | 1 @ | 15 | | | | | | | | | | | | | | | | |
| MA2 | | | | | BASE TO 2M1 | 1 @ | 55 | | | | | | | | | | | | | | | | |
| MA2 | | | | | BASE TO 2M2 | 1 @ | 45 | | | | | | | | | | | | | | | | |
| MA2 | | | | | BASE TO CAM4 | | | | | | | | | | | | | | | 1 @ | 60 | 1 @ | 60 |
| MA2 | | | | | BASE TO OPTICOM WB | | | | | | | | | | | | | 1 @ | 50 | | | | |
| MA3 | | | | | BASE TO 3A | 1 @ | 15 | | | | | | | | | | | | | | | | |
| MA3 | | | | | BASE TO 4B | 1 @ | 15 | | | | | | | | | | | | | | | | |
| MA3 | | | | | BASE TO 3M1 | 1 @ | 60 | | | | | | | | | | | | | | | | |
| MA3 | | | | | BASE TO 3M2 | 1 @ | 50 | | | | | | | | | | | | | | | | |
| MA3 | | | | | BASE TO CAM1 | | | | | | | | | | | | | | | 1 @ | 60 | 1 @ | 60 |
| MA3 | | | | | BASE TO OPTICOM NB | | | | | | | | | | | | | 1 @ | 55 | | | | |
| MA4 | | | | | BASE TO 4A | 1 @ | 15 | | | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO 1B | | | 1 @ | 15 | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO 4M1 | 1 @ | 55 | | | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO 4M2 | 1 @ | 45 | | | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO P4A, P2B | 2 @ | 15 | | | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO PPB4A, PPB2B | 2 @ | 5 | | | | | | | | | | | | | | | | |
| MA4 | | | | | BASE TO CAM2 | | | | | | | | | | | | | | | 1 @ | 60 | 1 @ | 60 |
| MA4 | | | | | BASE TO OPTICOM EB | | | | | | | | | | | | | 1 @ | 50 | | | | |
| PP1 | | | | | BASE TO 2B | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP1 | | | | | BASE TO 4E | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP1 | | | | | BASE TO P4B, P6A | 2 @ | 15 | | | | | | | | | | | | | | | | |
| PP1 | | | | | BASE TO PPB4B, PPB6A | 2 @ | 5 | | | | | | | | | | | | | | | | |
| PP2 | | | | | BASE TO 2C | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP2 | | | | | BASE TO 4D | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP3 | | | | | BASE TO 3B | | | 1 @ | 15 | | | | | | | | | | | | | | |
| PP3 | | | | | BASE TO P6B, P8A | 2 @ | 15 | | | | | | | | | | | | | | | | |
| PP3 | | | | | BASE TO PPB6B, PPB8A | 2 @ | 5 | | | | | | | | | | | | | | | | |
| PP4 | | | | | BASE TO 3C | | | 1 @ | 15 | | | | | | | | | | | | | | |
| PP5 | | | | | BASE TO 2E | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP5 | | | | | BASE TO P2A, P8B | 2 @ | 15 | | | | | | | | | | | | | | | | |
| PP5 | | | | | BASE TO PPB2A, PPB8B | 2 @ | 5 | | | | | | | | | | | | | | | | |
| PP6 | | | | | BASE TO 4C | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP6 | | | | | BASE TO 2D | 1 @ | 15 | | | | | | | | | | | | | | | | |
| PP7 | | | | | BASE TO 1C | | | 1 @ | 15 | | | | | | | | | | | | | | |
| TOTAL | 0 | 1170 | 975 | | | 2975 | 140 | 1760 | 390 | 880 | 170 | 1195 | 1225 | 1225 | | | | | | | | | |
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ABBREVIATIONS


CC 6 COMMUNICATION CABLE-6 PAIR
CC 12 COMMUNICATION CABLE-12 PAIR
DLLIC DETECTOR LOOP LEAD IN CABLE
MA MASTARM
MCC MULTI CONDUCTOR CABLE
PB PULL BOX
PP PEDESTAL POLE
PPB PEDESTRIAN PUSH BUTTON
REC RIGID ELECTRIC CONDUIT
SCC SINGLE CONDUCTOR CABLE

26 7337.81 0905

Bohannon  **Huston**

Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335

ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**CITY OF ALBUQUERQUE**
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
TRAFFIC SIGNAL
CABLES & CONDUITS

DESIGN REVIEW COMMITTEE
APPROVED
MAY 2 & 2004
DESIGN REVIEW COMMITTEE

CITY ENGINEER APPROVAL
APPROVED
JUN - 4 2004
CITY ENGINEER

LAST DESIGN UPDATE

CITY PROJECT NO.
733781

ZONE MAP NO.
F-11

SHEET
9 OF
14

DETECTOR RACK ASSIGNMENTS

| UNIT NUMBER → | POWER SUPPLY | 16 | 17 | 18 |
|----------------------------|--------------|--|-----------|-----------|
| CHANNEL 1 → | | P E D I S O L A T I O N | OPTICOM 1 | OPTICOM 3 |
| CHANNEL 2 → | | | OPTICOM 2 | OPTICOM 4 |
| DETECTOR MODULE REQUIRED → | * | | ✓ | ✓ |

* INCIDENTAL TO CONSTRUCTION

NOTES:

- 1/ IDENTIFY CONDUCTORS LISTED AS "115 VOLTS"
- 2/ WRAP RING 2 CABLE AT EACH SPLICE POINT WITH COLORED ELECTRICAL TAPE. THE IDENTIFICATION MARKING SHALL BE PROVIDED ON EACH RING 2 CABLE AT EACH SPLICE BOX AND LOCATED 6" BACK FROM THE END.
- 3/ IDENTIFY CONDUCTORS LISTED AS "PPB - LOW VOLTAGE" AT EACH SPLICE POINT. FIVE (5) CONDUCTOR CABLE SHALL BE 24 VOLTS AND USED FOR PUSH BUTTONS ONLY.

VIDEO DETECTION IS
REQUIRED FOR
THIS INTERSECTION

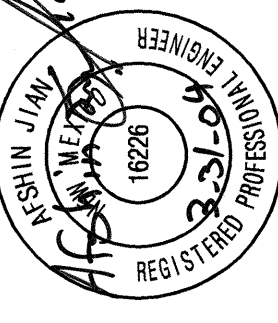
FUNCTION CHART – 115 VOLT CIRCUIT 1/

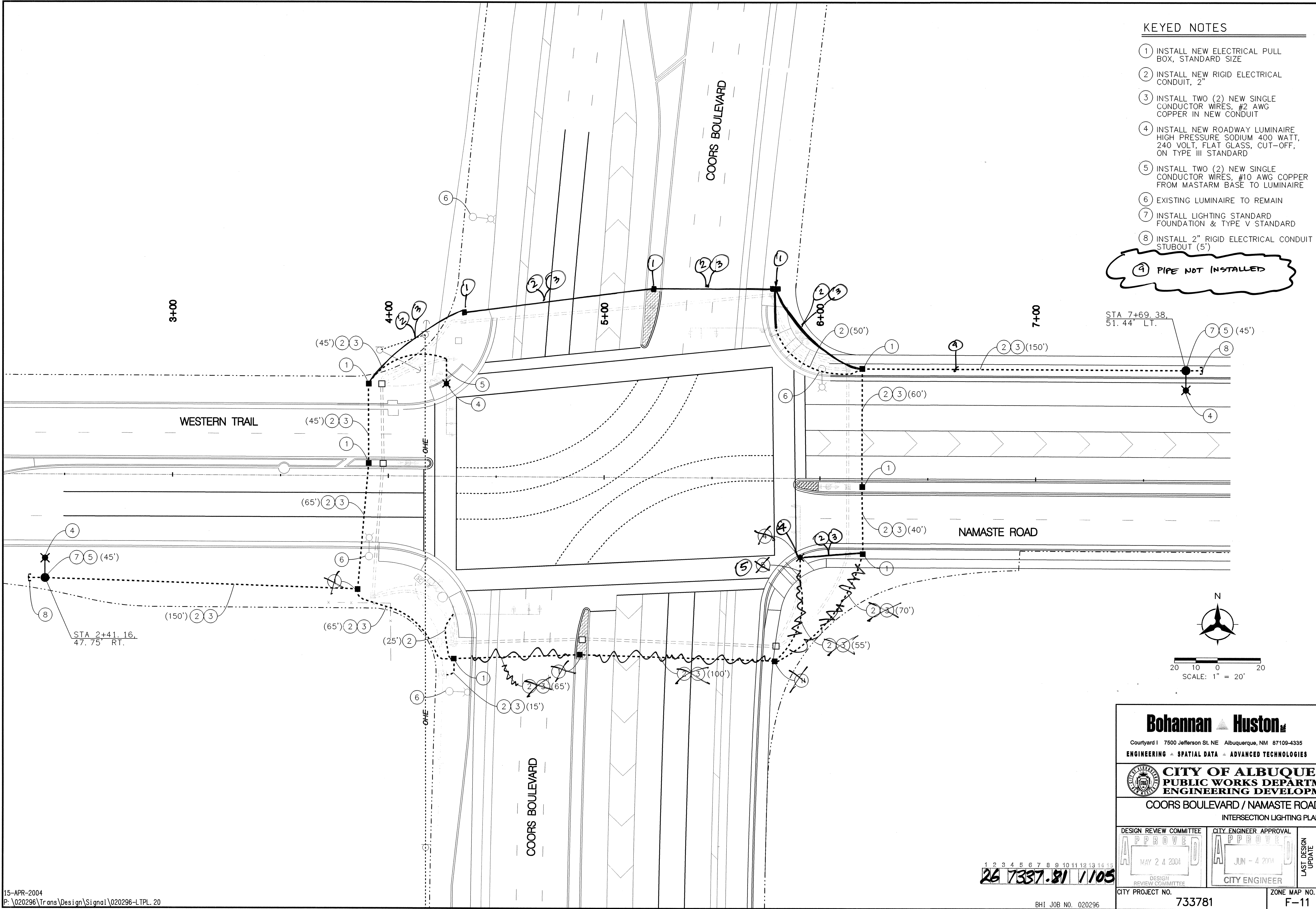
| CONDUCTOR | | | RING 1 – MULTI CONDUCTOR CABLE 20 | | RING 2 – MULTI CONDUCTOR CABLE 20 2/ | |
|------------------|------------|--------|-----------------------------------|---------------------------------------|--------------------------------------|---------------------------------------|
| CONDUCTOR NUMBER | BASE COLOR | TRACER | FUNCTION | FIELD CONNECTION | FUNCTION | FIELD CONNECTION |
| 1 | BLACK | – | SPARE | SPARE | SPARE | SPARE |
| 2 | WHITE | – | SPARE | SPARE | SPARE | SPARE |
| 3 | RED | – | PHASE 1 RED | RED LEFT TURN ARROW 1B, 1C | PHASE 5 RED | RED LEFT TURN ARROW 3B, 3C |
| 4 | GREEN | – | PHASE 1 GREEN | GREEN LEFT TURN ARROW 1B, 1C | PHASE 5 GREEN | GREEN LEFT TURN ARROW 3B, 3C |
| 5 | ORANGE | – | PHASE 1 YELLOW | YELLOW LEFT TURN ARROW 1B, 1C | PHASE 5 YELLOW | YELLOW LEFT TURN ARROW 3B, 3C |
| 6 | BLUE | – | SPARE | SPARE | SPARE | SPARE |
| 7 | WHITE | BLACK | SPARE | SPARE | SPARE | SPARE |
| 8 | RED | BLACK | PHASE 2 RED | RED BALL 3A, 3M1, 3M2 | PHASE 6 RED | RED BALL 1A, 1M1, 1M2 |
| 9 | GREEN | BLACK | PHASE 2 GREEN | GREEN BALL 3A, 3M1, 3M2 | PHASE 6 GREEN | GREEN BALL 1A, 1M1, 1M2 |
| 10 | ORANGE | BLACK | PHASE 2 YELLOW | YELLOW BALL 3A, 3M1, 3M2 | PHASE 6 YELLOW | YELLOW BALL 1A, 1M1, 1M2 |
| 11 | BLUE | BLACK | PHASE 2 WALK | PEDESTRIAN WALK P2A, P2B | PHASE 6 WALK | PEDESTRIAN WALK P6A, P6B |
| 12 | BLACK | WHITE | PHASE 2 DON'T WALK | PEDESTRIAN DON'T WALK P2A, P2B | PHASE 6 DON'T WALK | PEDESTRIAN DON'T WALK P6A, P6B |
| 13 | RED | WHITE | PHASE 3 RED | RED LEFT TURN ARROW 2B, 2C, 2D, 2E | PHASE 7 RED | RED LEFT TURN ARROW 4B, 4C, 4D, 4E |
| 14 | GREEN | WHITE | PHASE 3 GREEN | GREEN LEFT TURN ARROW 2B, 2C, 2D, 2E | PHASE 7 GREEN | GREEN LEFT TURN ARROW 4B, 4C, 4D, 4E |
| 15 | BLUE | WHITE | PHASE 3 YELLOW | YELLOW LEFT TURN ARROW 2B, 2C, 2D, 2E | PHASE 7 YELLOW | YELLOW LEFT TURN ARROW 4B, 4C, 4D, 4E |
| 16 | BLACK | RED | PHASE 4 RED | RED BALL 4A, 4M1, 4M2 | PHASE 8 RED | RED BALL 2A, 2M1, 2M2 |
| 17 | WHITE | RED | PHASE 4 GREEN | GREEN BALL 4A, 4M1, 4M2 | PHASE 8 GREEN | GREEN BALL 2A, 2M1, 2M2 |
| 18 | ORANGE | RED | PHASE 4 YELLOW | YELLOW BALL 4A, 4M1, 4M2 | PHASE 8 YELLOW | YELLOW BALL 2A, 2M1, 2M2 |
| 19 | BLUE | RED | PHASE 4 WALK | PEDESTRIAN WALK P4A, P4B | PHASE 8 WALK | PEDESTRIAN WALK P8A, P8B |
| 20 | RED | GREEN | PHASE 4 DON'T WALK | PEDESTRIAN DON'T WALK P4A, P4B | PHASE 8 DON'T WALK | PEDESTRIAN DON'T WALK P8A, P8B |

FUNCTION CHART – 24 VOLT CIRCUIT 3/

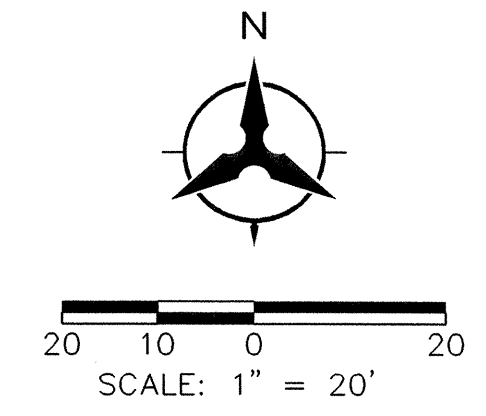
| | | RING 1 – MULTI CONDUCTOR CABLE 5 | | RING 2 – MULTI CONDUCTOR CABLE 5 | |
|------------------|------------|----------------------------------|------------------|----------------------------------|------------------|
| CONDUCTOR NUMBER | BASE COLOR | FUNCTION | FIELD CONNECTION | FUNCTION | FIELD CONNECTION |
| 1 | BLACK | PHASE 2 PPB | PPB2A, PPB2B | SPARE | |
| 2 | WHITE | COMMON | PPB 2A,2B,4A,4B | COMMON | PPB 6A,6B,8A,8B |
| 3 | RED | PHASE 4 PPB | PPB4A, PPB4B | SPARE | |
| 4 | GREEN | SPARE | | PHASE 6 PPB | PPB6A, PPB6B |
| 5 | ORANGE | SPARE | | PHASE 8 PPB | PPB8A, PPB8B |

| | |
|---|--|
| Bohannon & Huston Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES | |
| CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL TRAFFIC SIGNAL FUNCTIONS & DETECTORS | |
| DESIGN REVIEW COMMITTEE APPROVED MAY 24 2004 | CITY ENGINEER APPROVAL APPROVED JUN - 4 2004 |
| DESIGN REVIEW COMMITTEE | CITY ENGINEER |
| CITY PROJECT NO. 733781 | ZONE MAP NO. F-11 |
| SHEET 10 | OF 14 |

| | |
|--|--|
| ENGINEER'S SEAL  | |
| SURVEY INFORMATION FIELD NOTES NO. BY DATE | |
| BENCH MARKS USC & GS BRASS TABLET STAMPED "JOSEPH 1969" GEOGRAPHIC POSITION (NAD 1927) N.M. State Plane Coordinates (Central Zone) X= 366,806.62 Y= 1,502,737.54 Ground-to-Grid Factor = 0.9996734 Δα = -00'15"24" SLD 1929 Elevation = 5113.852 | |
| AS-BUILT INFORMATION CONTRACTOR INSPECTOR'S DATE VERIFICATION BY DATE CORRECTED BY DATE MICRO-FILM INFORMATION RECORDED BY NO. DATE | |



- KEYED NOTES**
- 1. INSTALL NEW ELECTRICAL PULL BOX, STANDARD SIZE
 - 2. INSTALL NEW RIGID ELECTRICAL CONDUIT, 2"
 - 3. INSTALL TWO (2) NEW SINGLE CONDUCTOR WIRES, #2 AWG COPPER IN NEW CONDUIT
 - 4. INSTALL NEW ROADWAY LUMINAIRE, HIGH PRESSURE SODIUM 400 WATT, 240 VOLT, FLAT GLASS, CUT-OFF, ON TYPE III STANDARD
 - 5. INSTALL TWO (2) NEW SINGLE CONDUCTOR WIRES, #10 AWG COPPER FROM MASTARM BASE TO LUMINAIRE
 - 6. EXISTING LUMINAIRE TO REMAIN
 - 7. INSTALL LIGHTING STANDARD FOUNDATION & TYPE V STANDARD
 - 8. INSTALL 2" RIGID ELECTRICAL CONDUIT STUBOUT (5')
9. PIPE NOT INSTALLED



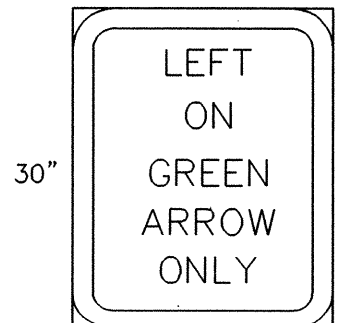
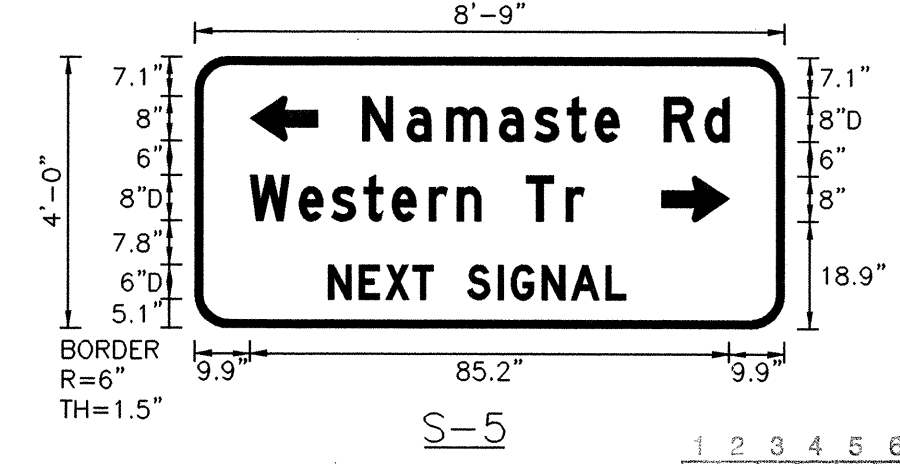
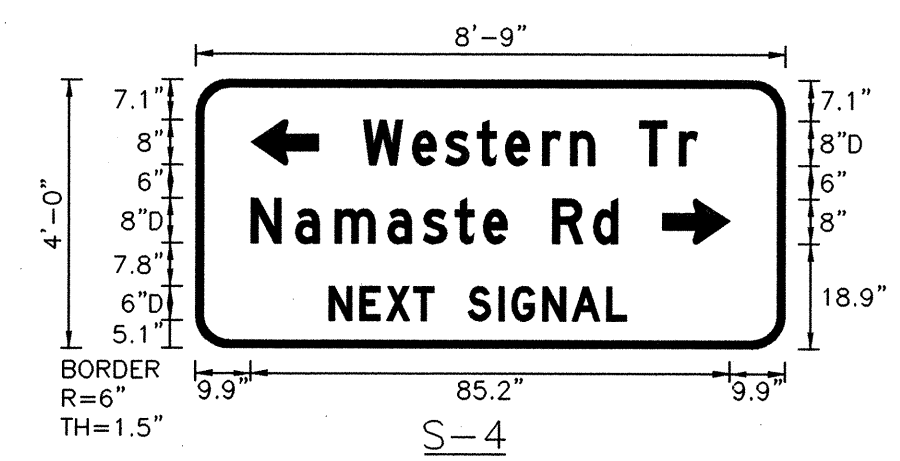
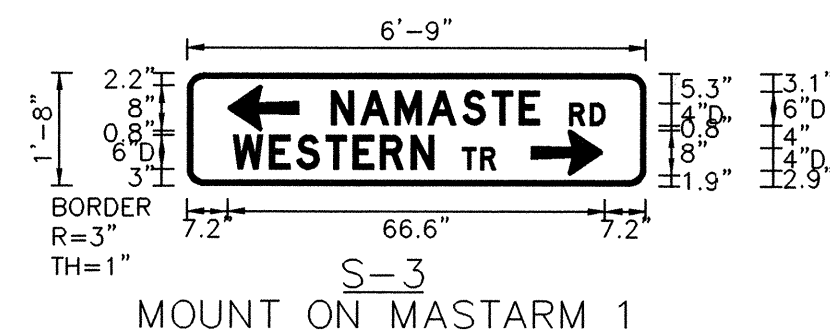
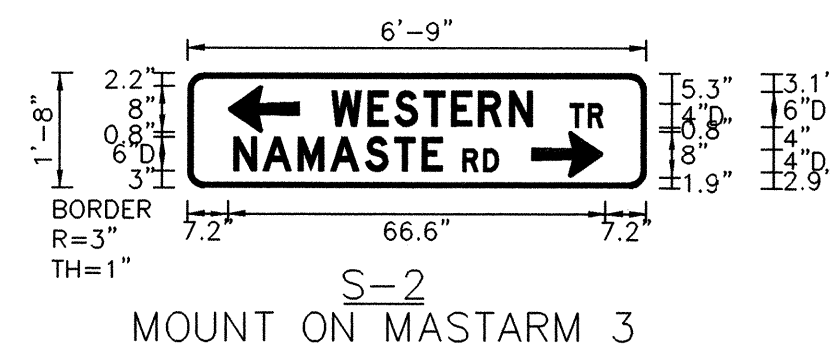
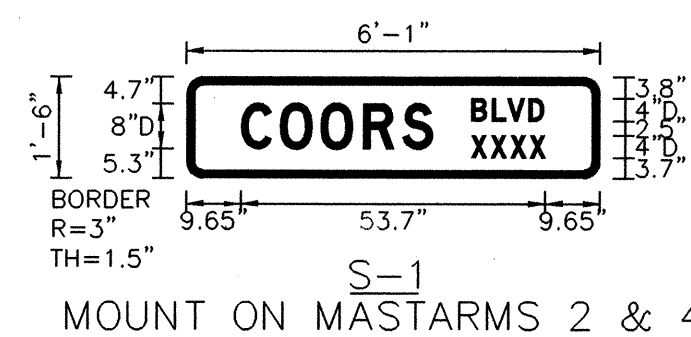
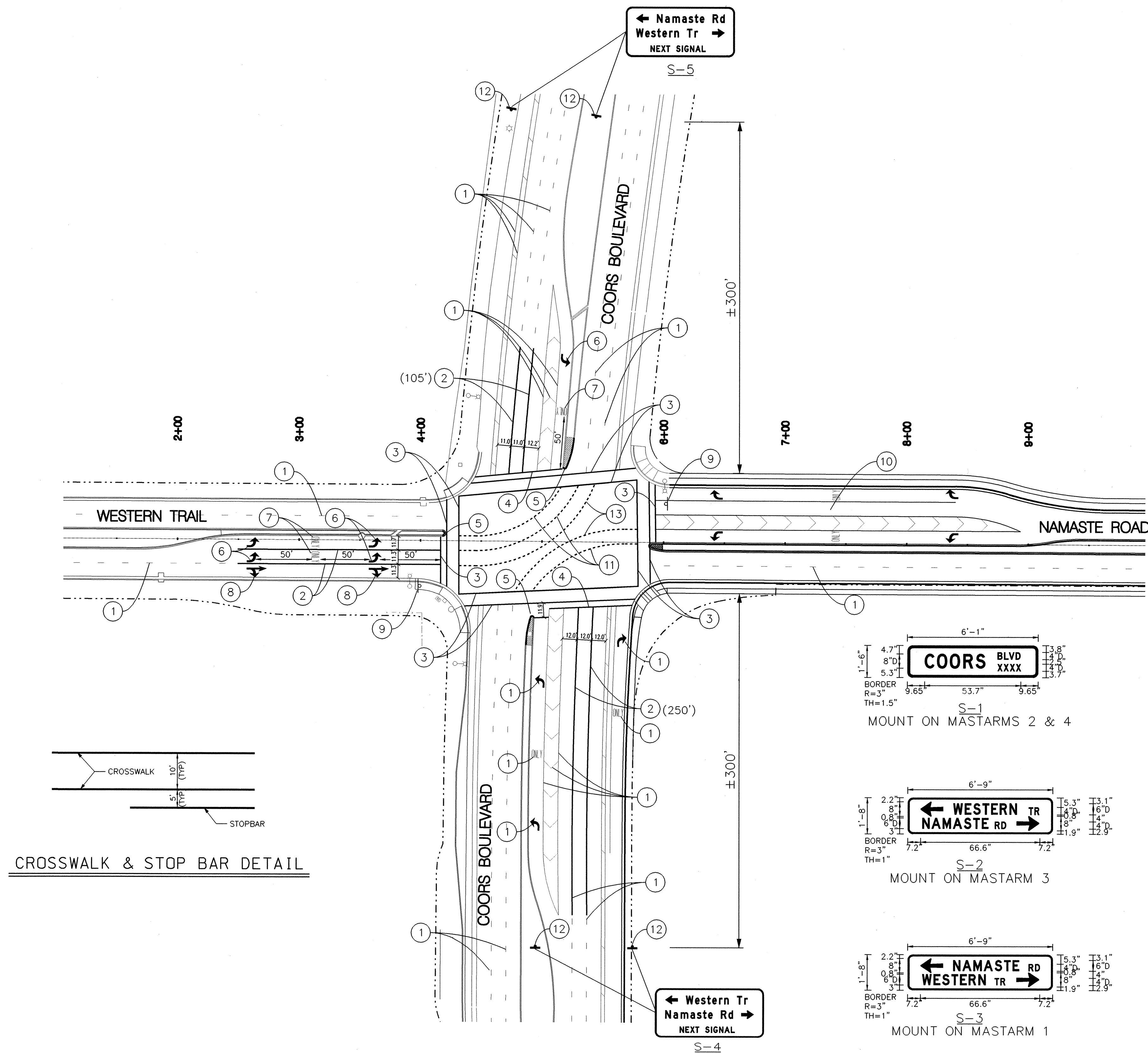
Bohannon & Huston
Court yard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335
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CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP
COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
INTERSECTION LIGHTING PLAN

| | | | |
|--------------------------|--------------------------|-------------|-------------|
| DESIGN REVIEW COMMITTEE | CITY ENGINEER APPROVAL | MO./DAY/YR. | MO./DAY/YR. |
| APPROVED MAY 2 4 2004 | APPROVED JUN - 4 2004 | | |
| DESIGN REVIEW COMMITTEE | CITY ENGINEER | | |

CITY PROJECT NO. **733781** ZONE MAP NO. **F-11** SHEET **11** OF **14**

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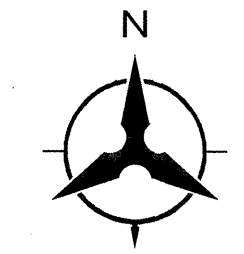
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- REMOVE & SALVAGE EXISTING SIGN AND POST. DELIVER TO THE CITY TRAFFIC ENGINEERING YARDS.
- WESTBOUND NAMASTE ROAD STRIPING TO BE COMPLETED WITH COA PROJECT #730881
- INSTALL 4" DASHED WHITE STRIPE (2' STRIPE, 6' GAP) PER MUTCD
- INSTALL SQUARE TUBE STEEL POST & BASE POSTS FOR ALUMINUM SIGN, CIP. LOCATE SIGN SUCH THAT IT IS NOT OBSTRUCTED BY EXISTING LANDSCAPING
- INSTALL 4" DASHED YELLOW STRIPE (2' STRIPE, 6' GAP) PER MUTCD

NOTES

- CONTRACTOR SHALL REMOVE EXISTING STRIPING THROUGH INTERSECTION AND ANY OTHER CONFLICTING STRIPING.
- ALL STRIPING ERADICATION SHALL BE ACCOMPLISHED BY WATER BLASTING OR SURFACE PLANING TO A MINIMUM DEPTH OF .25 INCHES (MAX) AND FOR A WIDTH EQUAL TO TWO TIMES THE WIDTH OF THE STRIPE OR MARKING TO BE REMOVED.



SCALE: 1" = 50'

26 7337.81 / 1205

Bohannon Huston
 Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335
 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

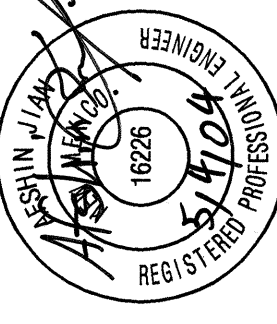
CITY OF ALBUQUERQUE
 PUBLIC WORKS DEPARTMENT
 ENGINEERING DEVELOPMENT GROUP

COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL
 SIGNING & STRIPING PLAN

| DESIGN REVIEW COMMITTEE | CITY ENGINEER APPROVAL | MO./DAY/YR. | MO./DAY/YR. |
|--|---|-------------|-------------|
| APPROVED MAY 24 2004 DESIGN REVIEW COMMITTEE | APPROVED JUN - 4 2004 CITY ENGINEER | | |
| | LAST DESIGN UPDATE | | |

| | | | |
|------------------|--------------|-------|----|
| CITY PROJECT NO. | ZONE MAP NO. | SHEET | OF |
| 733781 | F-11 | 12 | 14 |

| AS-BUILT INFORMATION | | BENCH MARKS | | SURVEY INFORMATION | | ENGINEER'S SEAL | |
|------------------------|------------|---|------|--------------------|----|-----------------|----|
| CONTRACTOR | STARTED BY | USC & GS BRASS TABLET STAMPED | DATE | NO. | BY | REMARKS | BY |
| NW1 | NS | 1969" GEOGRAPHIC POSITION (NAD 1927) | | | | | |
| INSPECTOR'S | DATE | N.M. State Plane Coordinates (Central Zone) | | | | REVISIONS | |
| BY | | X= 366,806.62 Y= 1,502,737.54 | | | | DESIGN | |
| FIELD | | Ground-to-Grid Factor = 0.9996734 | | | | | |
| VERIFICATION BY | | ΔX = -0015'24" | | | | | |
| CORRECTED BY | | SLD 1929 Elevation = 5113.852 | | | | | |
| MICRO-FILM INFORMATION | | | | | | | |
| RECORDED BY | | | | | | | |
| NO. | | | | | | | |



CONSTRUCTION TRAFFIC CONTROL GENERAL NOTES

1. CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS-OF-WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING, THAT A PERMIT SHALL BE OBTAINED WITHIN 24 TO 48 HOURS.
2. CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.

3. THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.

4. CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.

5. CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR, THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.

6. ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.

7. THE CONSTRUCTION TRAFFIC CONTROL INITIAL SET-UP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED WORK SITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORKSITE TRAFFIC SUPERVISOR OR EQUIVALENT.

8. CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.

9. ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI-LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.

10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.

11. CONTRACTOR SHALL NOT BEGIN WORK BEFORE 7:00 A.M. OR END WORK AFTER 7:00 P.M. WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.

12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION.

13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON-WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.

14. CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.

15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.

16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENTS AT ALL TIMES.

17. CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.

18. ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.

19. 24 HOURS PRIOR TO OCCUPYING OR CLOSING OF A RIGHT-OF-WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.

20. ANY FIELD ADJUSTMENTS SHALL BE APPROVED BY CONSTRUCTION COORDINATION.

21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS.

22. CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING:

1. STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
2. THE CITY OF ALBUQUERQUE TRAFFIC CODE, LATEST EDITION.
3. SECTION 19 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.

23. FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED, WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.

24. ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW-CLEAN CONDITION, WASHING OF EQUIPMENT IS INCIDENTAL TO IT'S PLACEMENT AND MAINTENANCE.

25. TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE CONSTRUCTION TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.

26. ADVANCE WARNING SIGNS SHALL BE 36"x36" WITH SUPER ENGINEERING GRADE SHEETING OR BETTER.

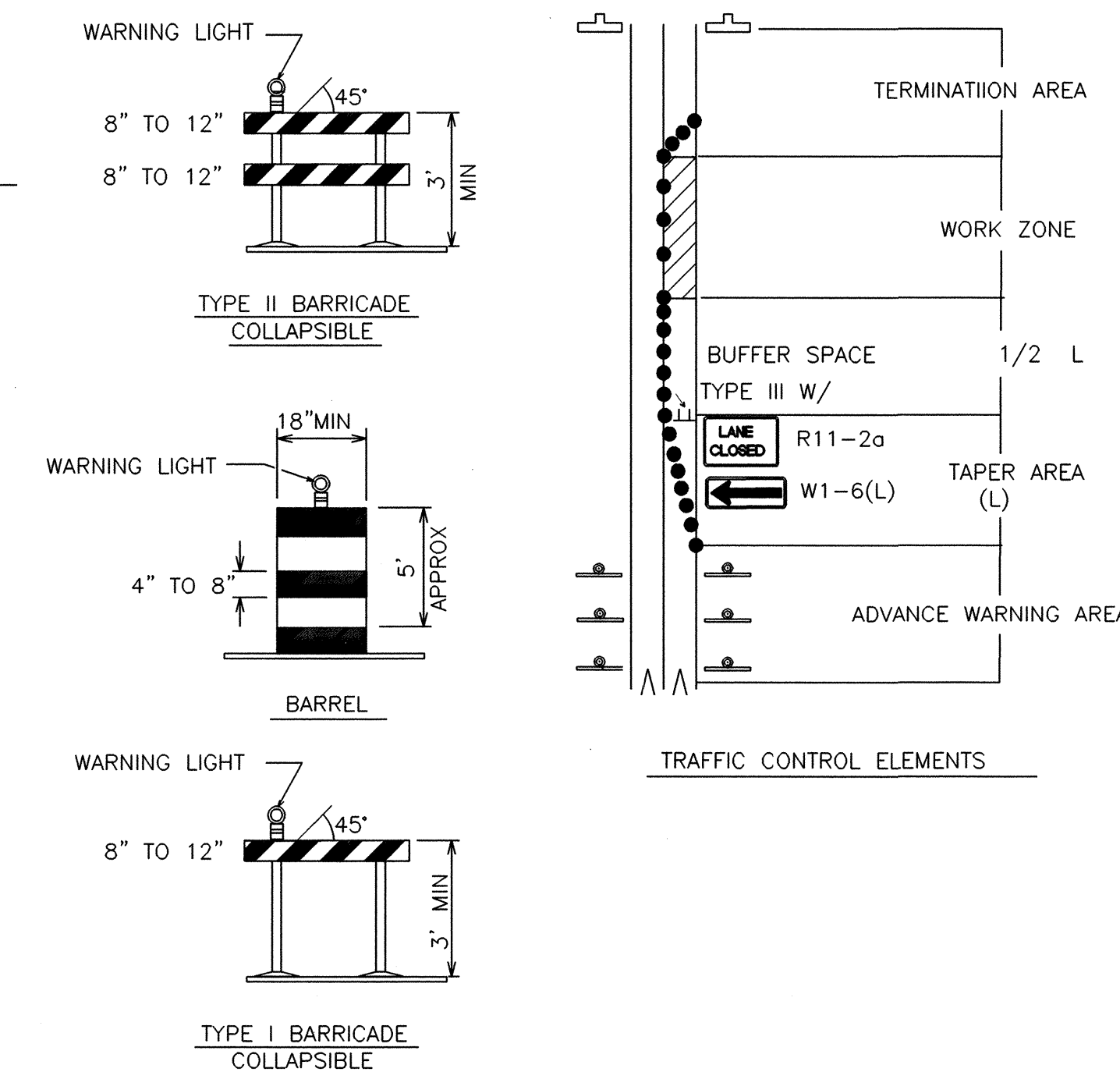
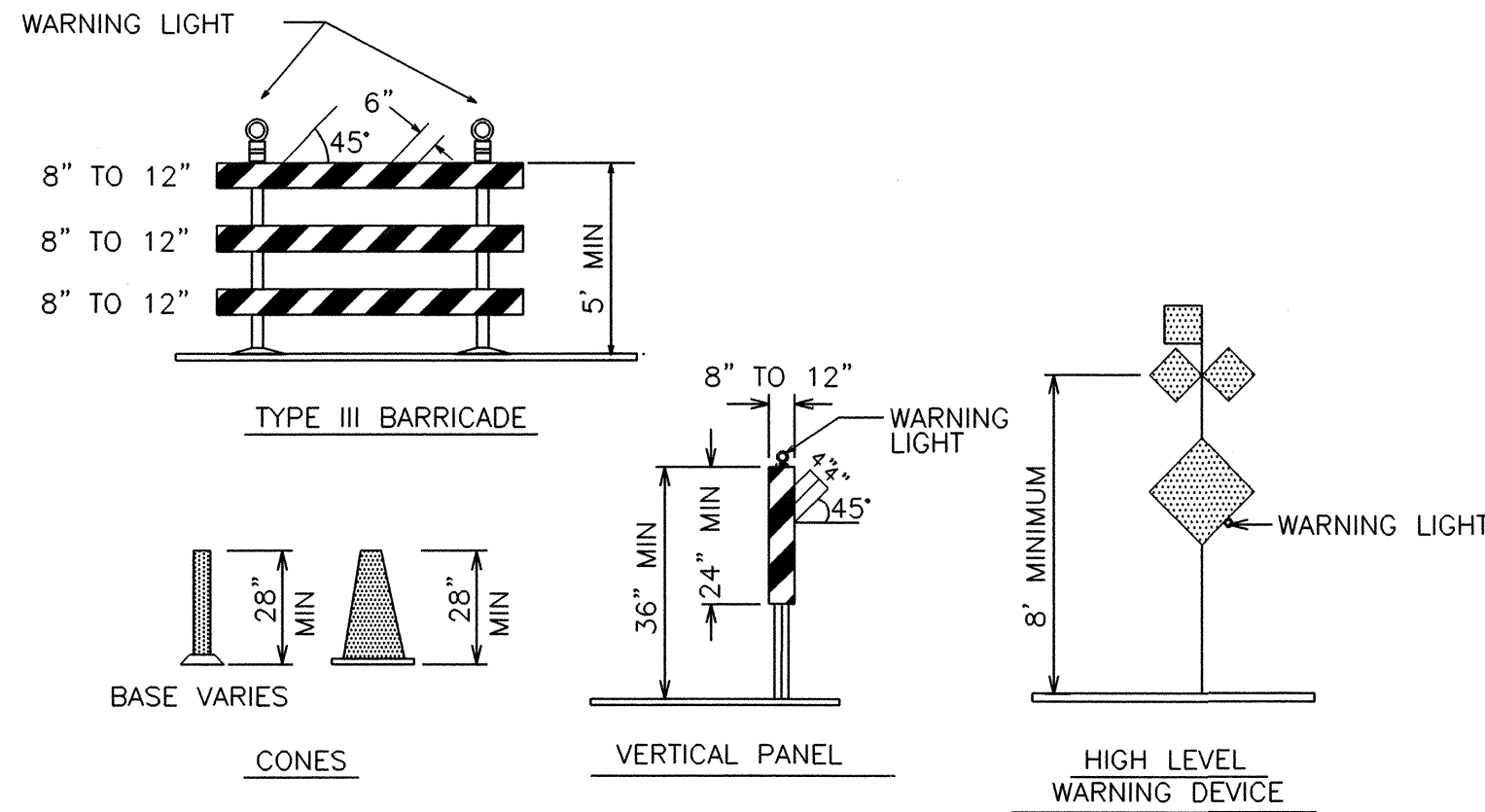
SPECIAL PROJECT TRAFFIC CONTROL NOTES

1. ALL CONDUITS MUST BE PUSHED OR BORED ACROSS COORS BLVD. NO OPEN CUTS WILL BE ALLOWED.

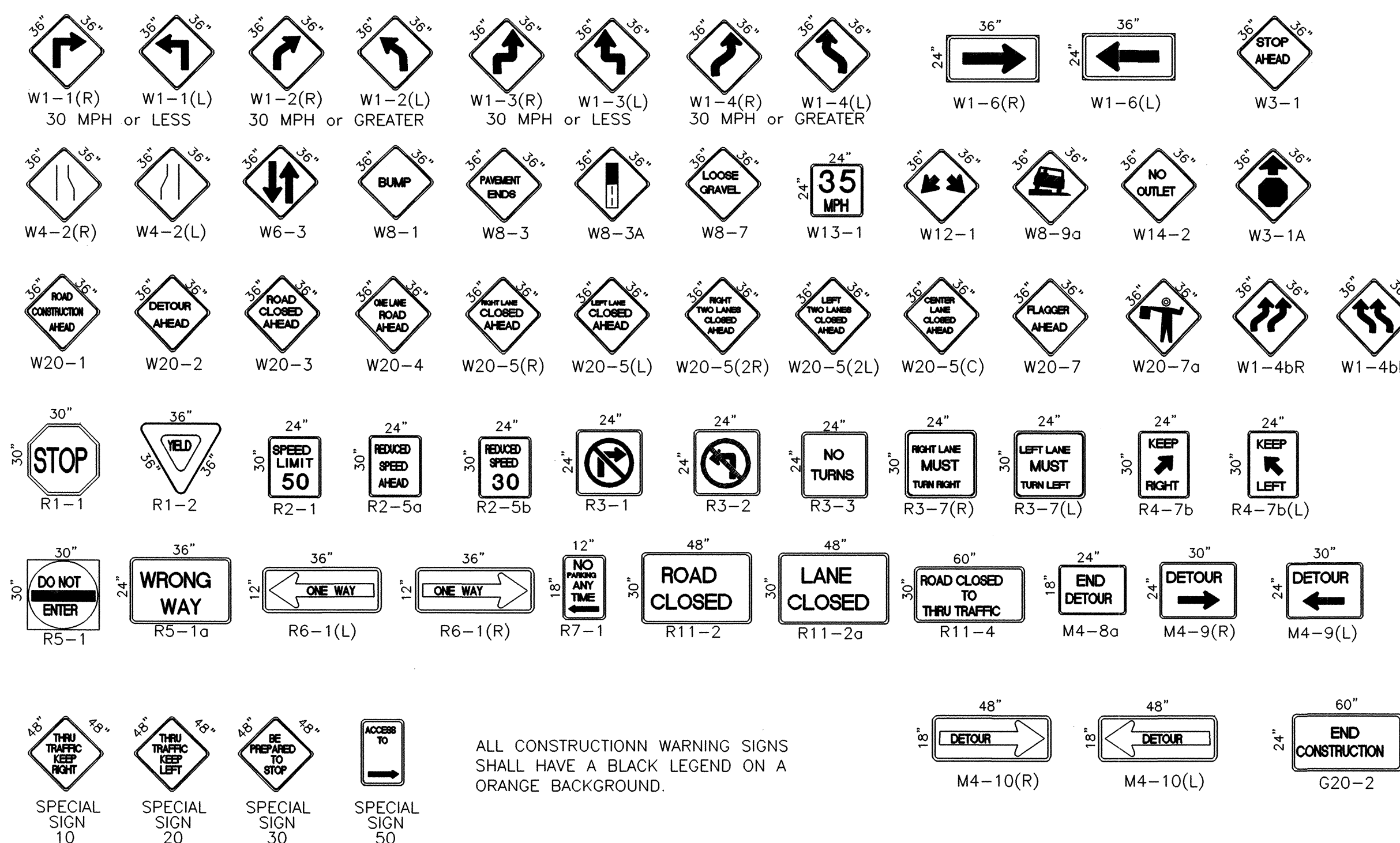
2. ONLY SINGLE LANE CLOSURES IN EACH DIRECTION WILL BE ALLOWED ON COORS BLVD.

3. CLOSURES OF THROUGH TRAFFIC LANES ON COORS BLVD. WILL NOT BE ALLOWED, IN ANY DIRECTION, FROM 6:00 AM TO 9:00 AM, AND FROM 3:00 PM TO 6:00 PM, MONDAY THROUGH FRIDAYS. IF THROUGH LANE CLOSURES ON COORS BLVD. MUST REMAIN DUE TO SAFETY CONCERNS, THEN THE CONTRACTOR SHALL WORK CONTINUOUSLY, 24-HOURS PER DAY UNTIL ALL LANES ARE OPEN TO TRAFFIC.

4. ARTERIAL/COLLECTOR ROAD USEAGE FEES OF \$0.01 PER SQUARE FOOT OF BARRICADED AREA PER DAY WILL BE ASSESSED ON BOTH COORS BLVD. AND WESTERN TRAIL.



SIGN FACE DETAILS



ALL CONSTRUCTION WARNING SIGNS SHALL HAVE A BLACK LEGEND ON A ORANGE BACKGROUND.

LEGEND

- WORK AREA
- BARRICADE - TYPE I, TYPE II, OR BARREL
 - BARRICADE - TYPE III
 - VERTICAL PANEL
 - WARNING SIGN
 - DISTANCE BETWEEN SIGNS - A DISTANCE MEASURED IN FEET EQUAL TO A VALUE OF TEN TIMES THE SPEED LIMIT OF THE STREET
 - FLAGMAN POSITION
 - SPACING BETWEEN BARRICADES - A DISTANCE MEASURED IN FEET EQUAL TO THE STREET LIMIT OF THE STREET
 - TAPER LENGTH - SEE CHART BELOW

THE TANGENT LENGTH IS EQUAL TO THE TAPER LENGTH FOR A GIVEN STREET.

TAPER REQUIREMENT

| SPEED LIMIT (MPH) | TAPER LENGTH(L) (FEET) | | | MINIMUM NUMBER OF DEVICES FOR TAPER | MAXIMUM DEVICE SPACING IN FEET | |
|-------------------|------------------------|----------|----------|-------------------------------------|--------------------------------|-------------|
| | 10' LANE | 11' LANE | 12' LANE | | ALONG TAPER | AFTER TAPER |
| 20 | 70 | 75 | 80 | 5 | 20 | 20 |
| 25 | 105 | 115 | 125 | 6 | 25 | 25 |
| 30 | 150 | 165 | 180 | 7 | 30 | 30 |
| 35 | 205 | 225 | 245 | 8 | 35 | 35 |
| 40 | 270 | 295 | 320 | 9 | 40 | 40 |
| 45 | 450 | 495 | 540 | 13 | 45 | 45 |
| 50 | 500 | 550 | 600 | 13 | 50 | 50 |
| 55 | 550 | 605 | 660 | 13 | 55 | 55 |

RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES

| SPEED MILES PER HOUR | MINIMUM DISTANCE IN FEET BETWEEN SIGNS | FROM LAST SIGN TO TAPER |
|----------------------|--|-------------------------|
| 0-20 | 10 X SPEED LIMIT | 10 X SPEED LIMIT |
| 25-30 | 10 X SPEED LIMIT | 10 X SPEED LIMIT |
| 30-35 | 10 X SPEED LIMIT | 10 X SPEED LIMIT |
| 40-45 | 10 X SPEED LIMIT | 10 X SPEED LIMIT |
| 50-60 | 10 X SPEED LIMIT | 10 X SPEED LIMIT |

TAPER CRITERIA

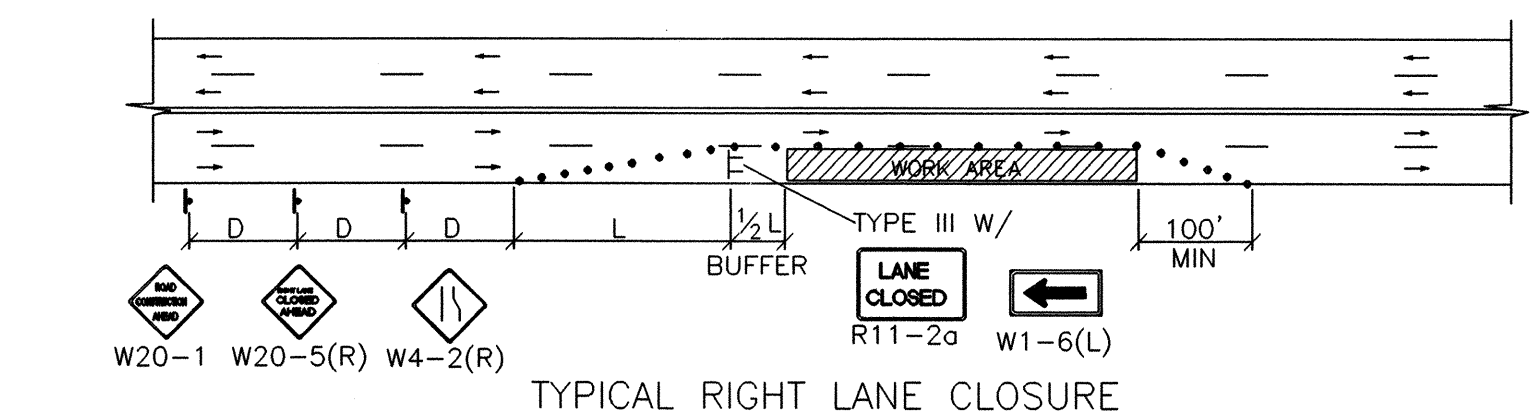
| TYPE OF TAPER | TAPER LENGTH |
|-----------------------|-------------------|
| UPSTREAM TAPER: | |
| MERGING TAPER | L MINIMUM |
| SHIFTING TAPER | 1/2 L MINIMUM |
| SHOULDER TAPER | 1/2 L MINIMUM |
| TWO-WAY TRAFFIC TAPER | 100 FEET MAXIMUM |
| DOWNSTREAM TAPERS | 100 FEET PER LANE |

TAPER LENGTH COMPUTATION

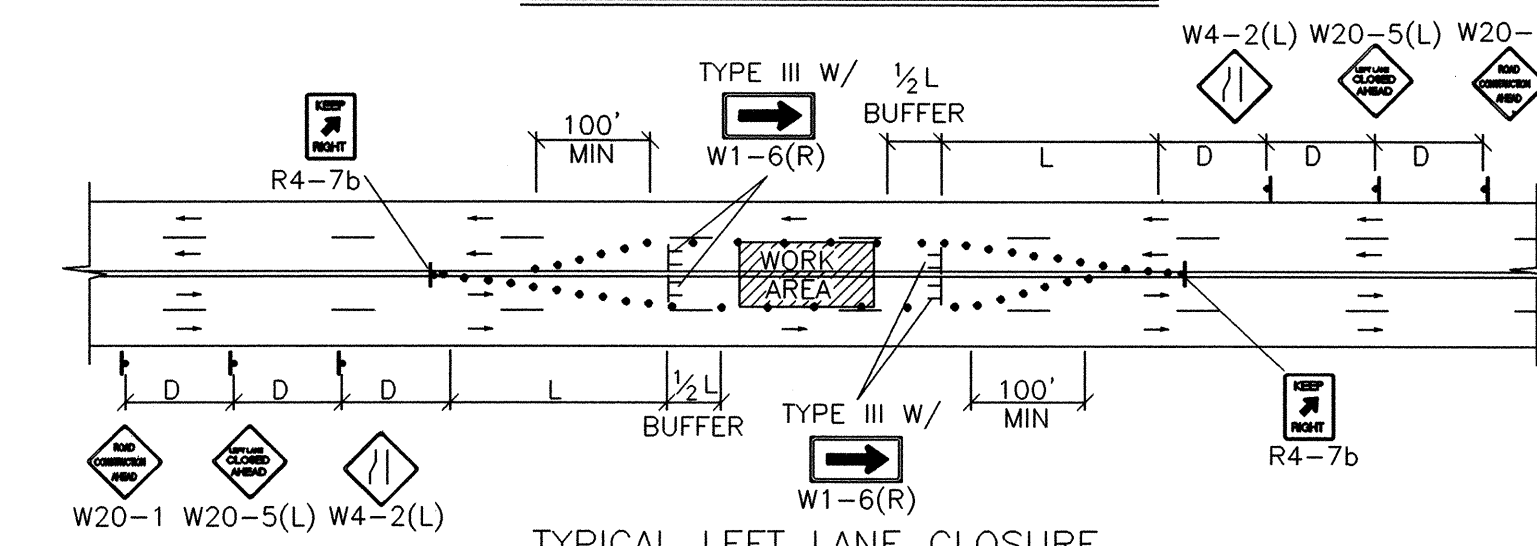
| SPEED LIMIT | |
|-------------------|-----------------------|
| 40 MPH OR LESS | $L = \frac{WS^2}{60}$ |
| 40 MPH OR GREATER | $L = W \times S$ |

L = TAPER LENGTH
W = WIDTH OF OFFSET IN FEET
S = POSTED SPEED OR OFF-PEAK 85-PERCENTILE SPEED IN MPH

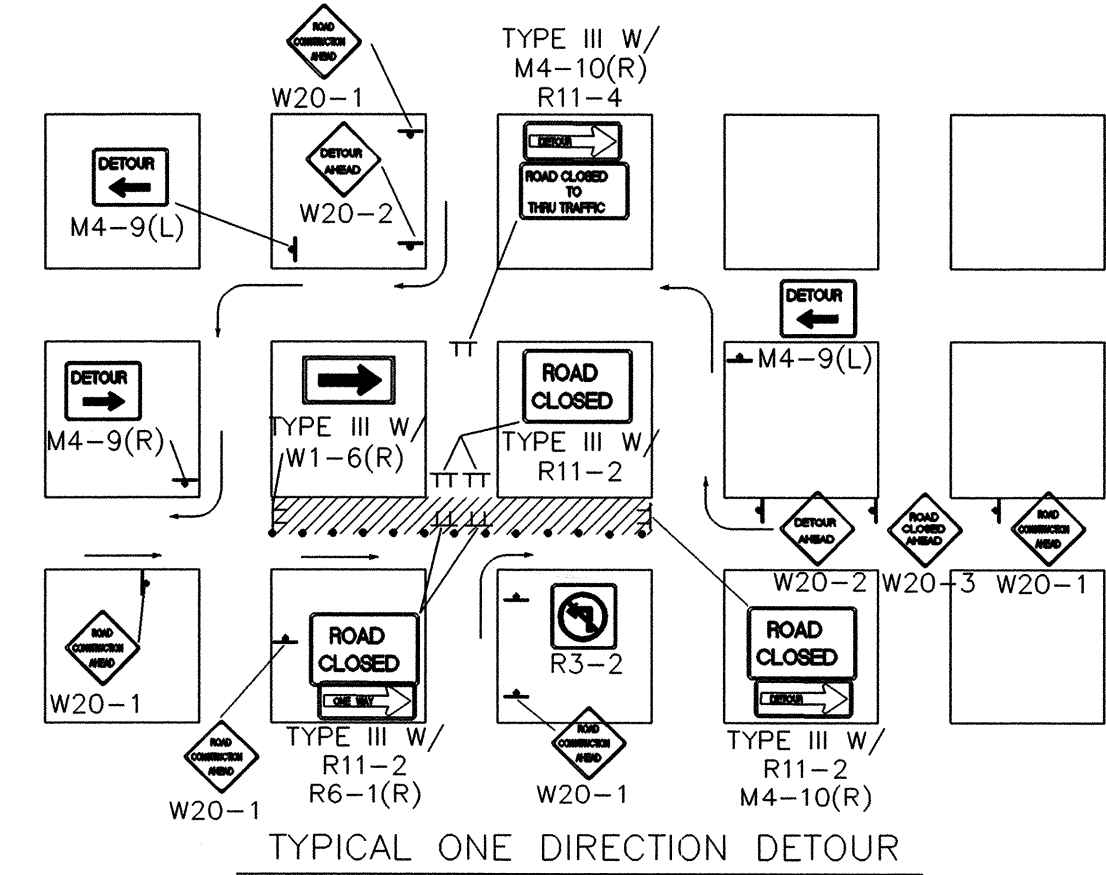
| | |
|---|------------------------|
| Bohannon & Huston Courtney I 7500 Jefferson St. NE Albuquerque, NM 87109-4335 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES | |
| CITY OF ALBUQUERQUE PUBLIC WORKS DEPARTMENT ENGINEERING DEVELOPMENT GROUP | |
| COORS BOULEVARD / NAMASTE ROAD-WESTERN TRAIL | |
| SIGNING AND CONSTRUCTION TRAFFIC CONTROL STANDARDS | |
| DESIGN REVIEW COMMITTEE | CITY ENGINEER APPROVAL |
| MO./DAY/YR. | MO./DAY/YR. |
| COA | STD |
| CITY PROJECT NO. 733781 | ZONE MAP NO. F-11 |
| SHEET 13 | OF 14 |



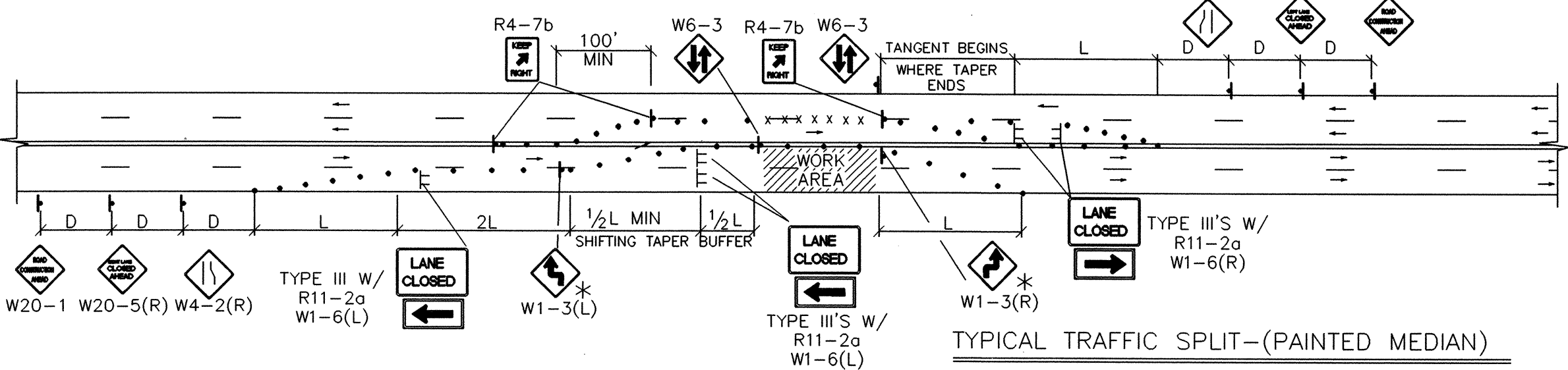
TYPICAL RIGHT LANE CLOSURE



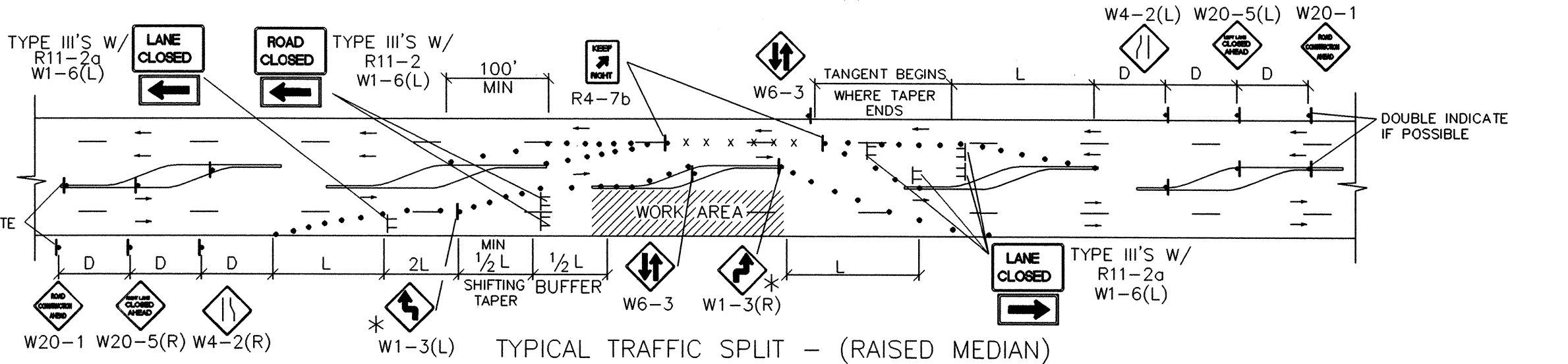
TYPICAL LEFT LANE CLOSURE



TYPICAL ONE DIRECTION DETOUR

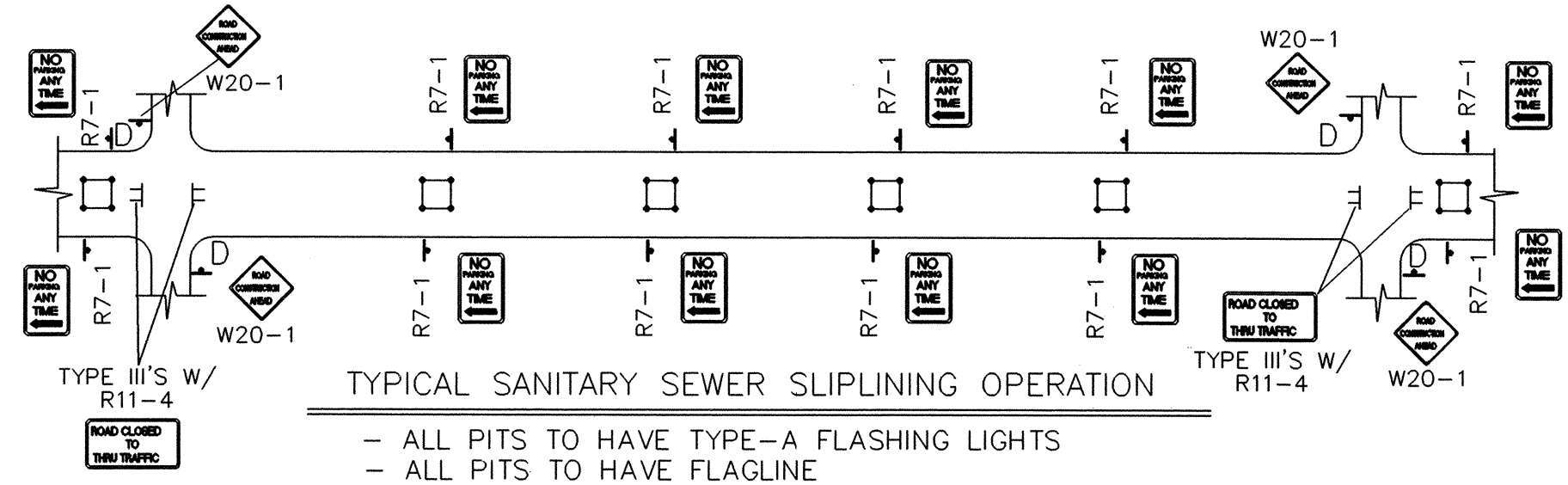


TYPICAL TRAFFIC SPLIT - (PAINTED MEDIAN)



TYPICAL TRAFFIC SPLIT - (RAISED MEDIAN)

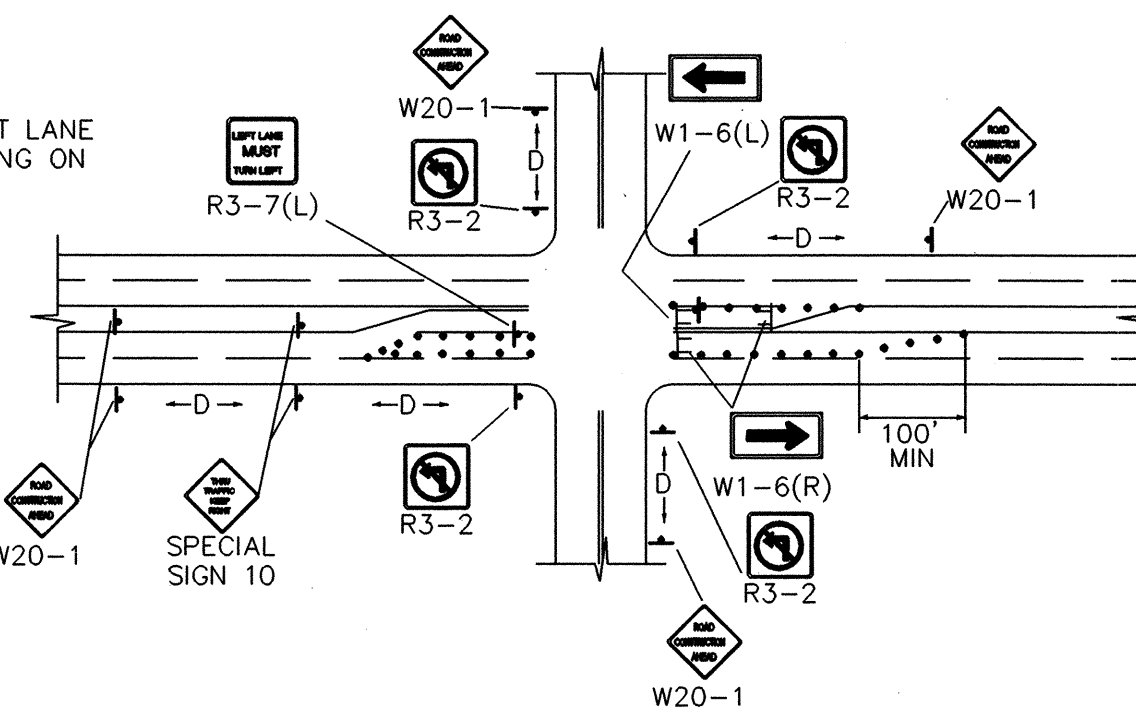
- TRAFFIC SPLIT NOTES:
1. THE OFFSET DISTANCE MUST BE CALCULATED IN ALL SHIFTING TAPERS. THE OFFSET DISTANCES SHALL INCLUDE LANE WIDTHS PLUS MEDIAN WIDTHS.
 2. 1/2 L IS THE MINIMUM DISTANCE FOR SHIFTING TAPERS.
 3. REVERSE CURVES MAY BE IMPLEMENTED. ALL CURVE DATA SHALL BE CALCULATED.
 4. MEDIAN REMOVAL SHALL BE REQUIRED IF 1/2 L OR REVERSE CURVE IS NOT SUFFICIENT. MEDIAN REMOVAL SHALL TAKE PLACE BEFORE SPLITS.
 5. REDUCED SPEED MAY BE CONSIDERED.
 6. * USE W1-3 FOR 30 MPH OR LESS, W1-4 FOR SPEED 35 MPH OR GREATER.



TYPICAL SANITARY SEWER SLIPLINING OPERATION

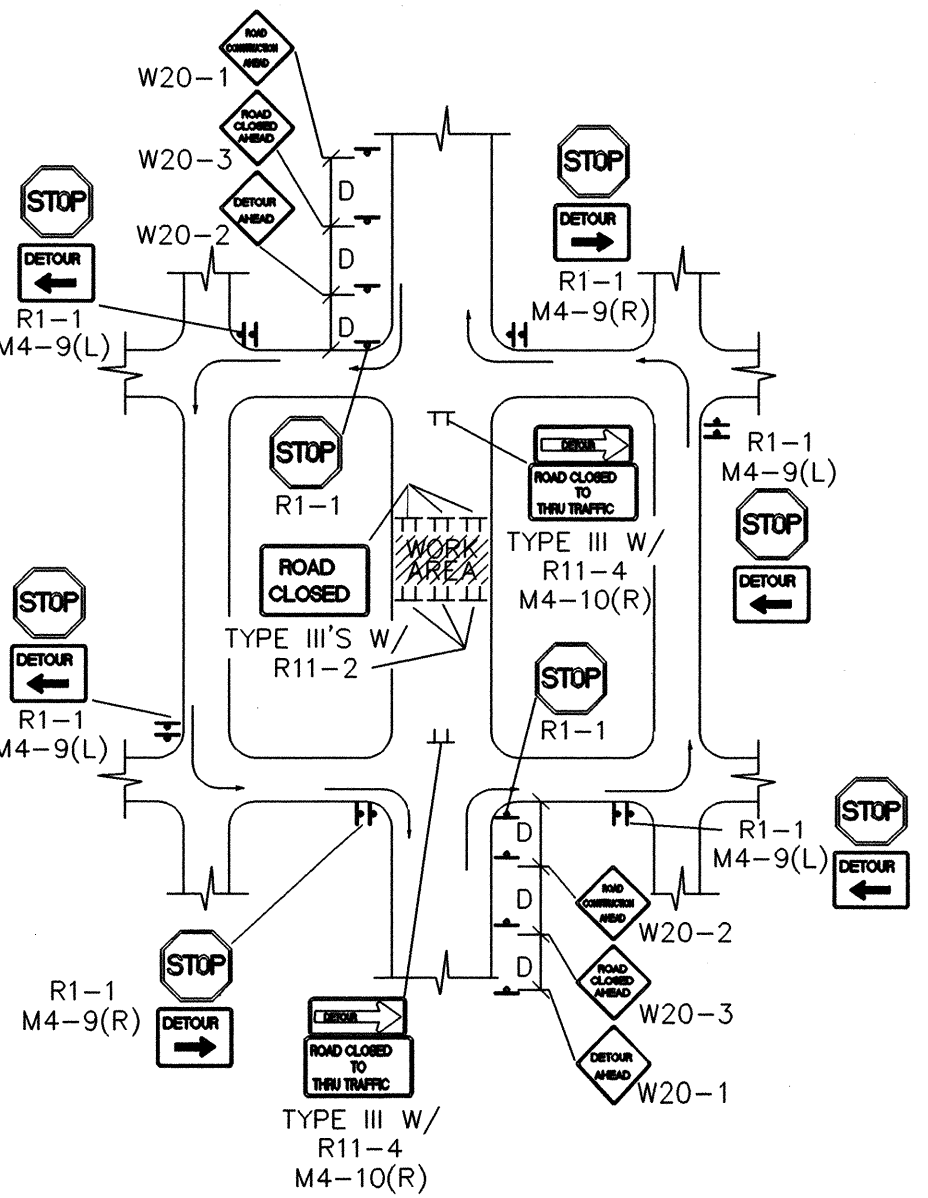
- ALL PITS TO HAVE TYPE-A FLASHING LIGHTS
- ALL PITS TO HAVE FLAGLINE

NOTE:
MAY REQUIRE LEFT LANE
CLOSURE DEPENDING ON
FIELD CONDITIONS.



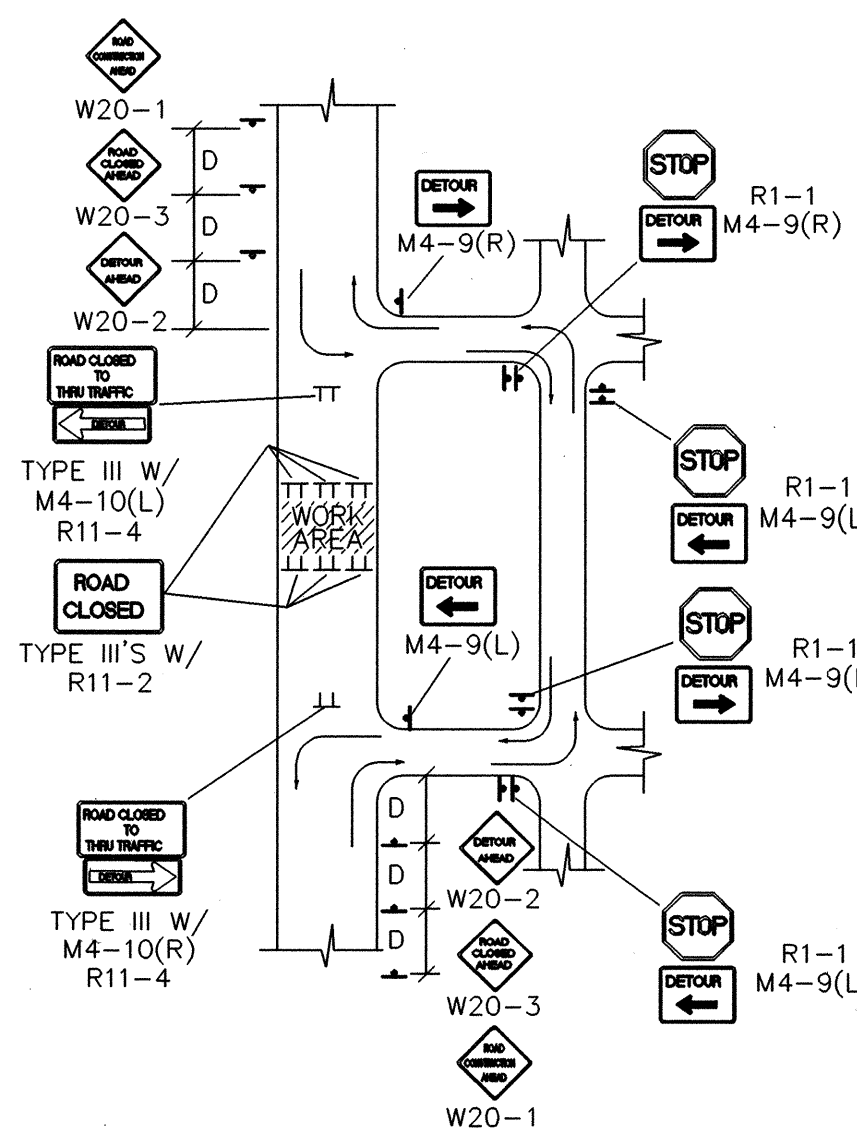
TYPICAL LANE CLOSURE AT INTERSECTION

NOTE: DEPENDING ON WORK ZONE LOCATION.



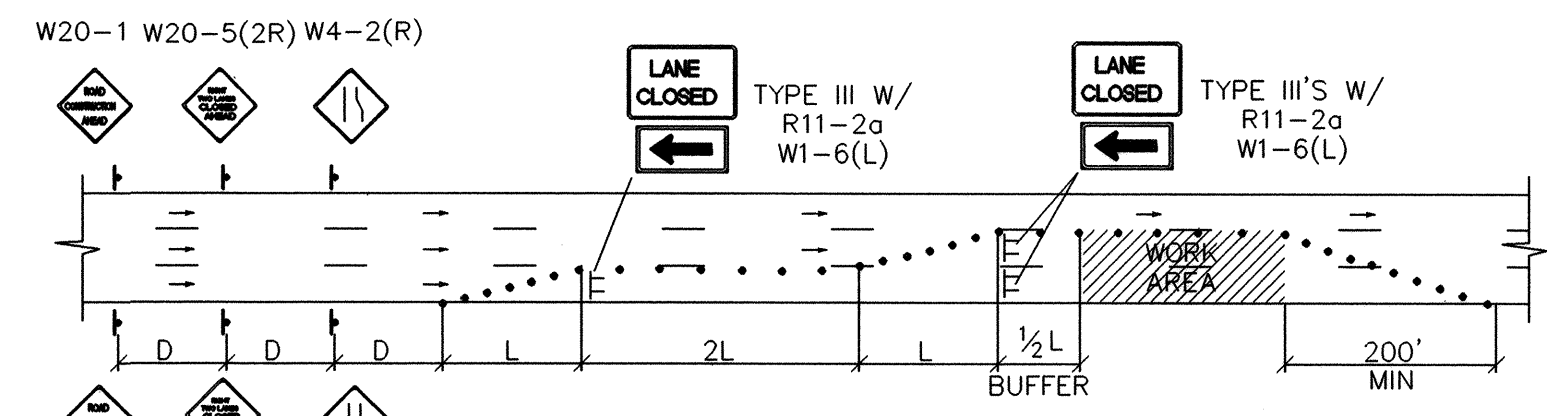
TYPICAL MID-BLOCK CLOSURE

NOTE: (R1-1) BASED ON FIELD CONDITIONS.

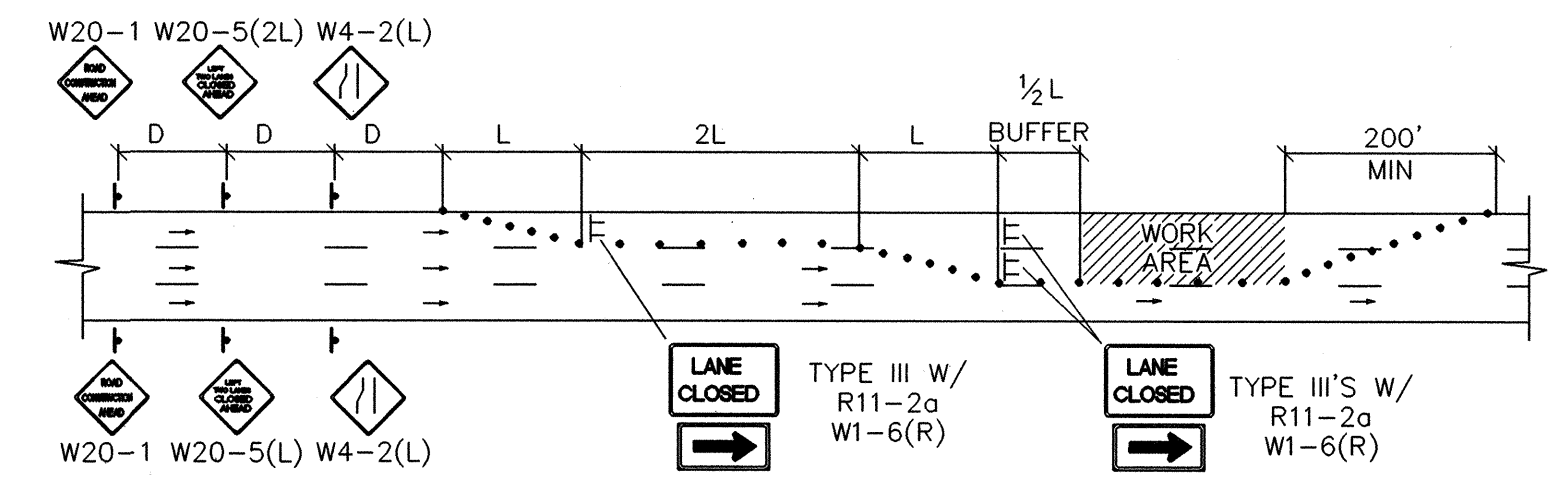


TYPICAL STREET CLOSURE

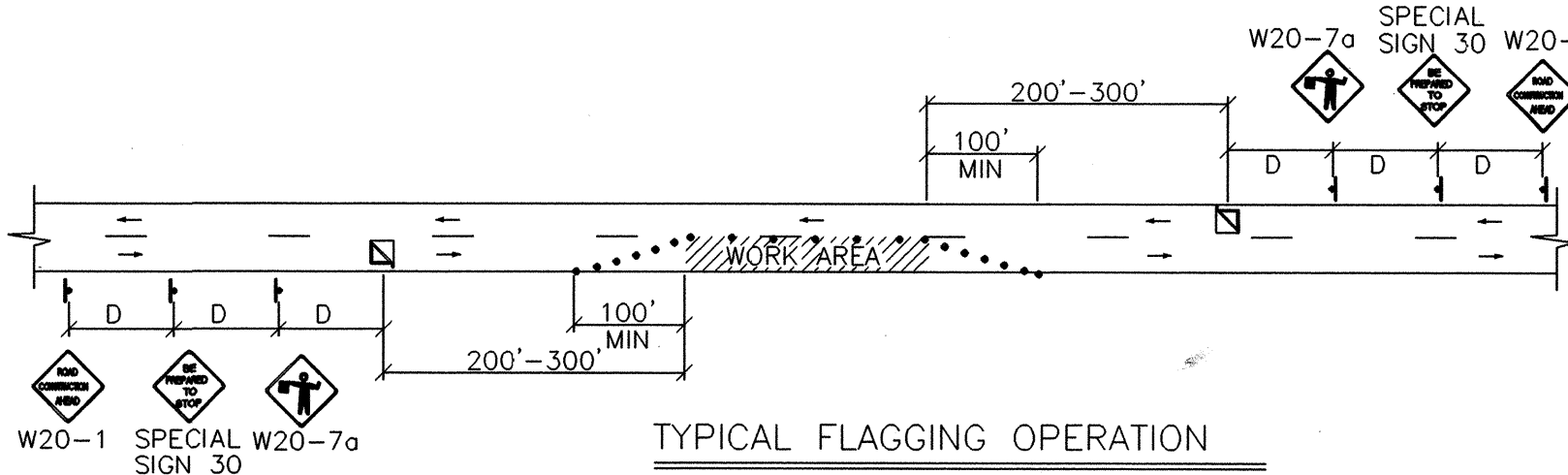
NOTE: (R1-1) BASED ON FIELD CONDITIONS.



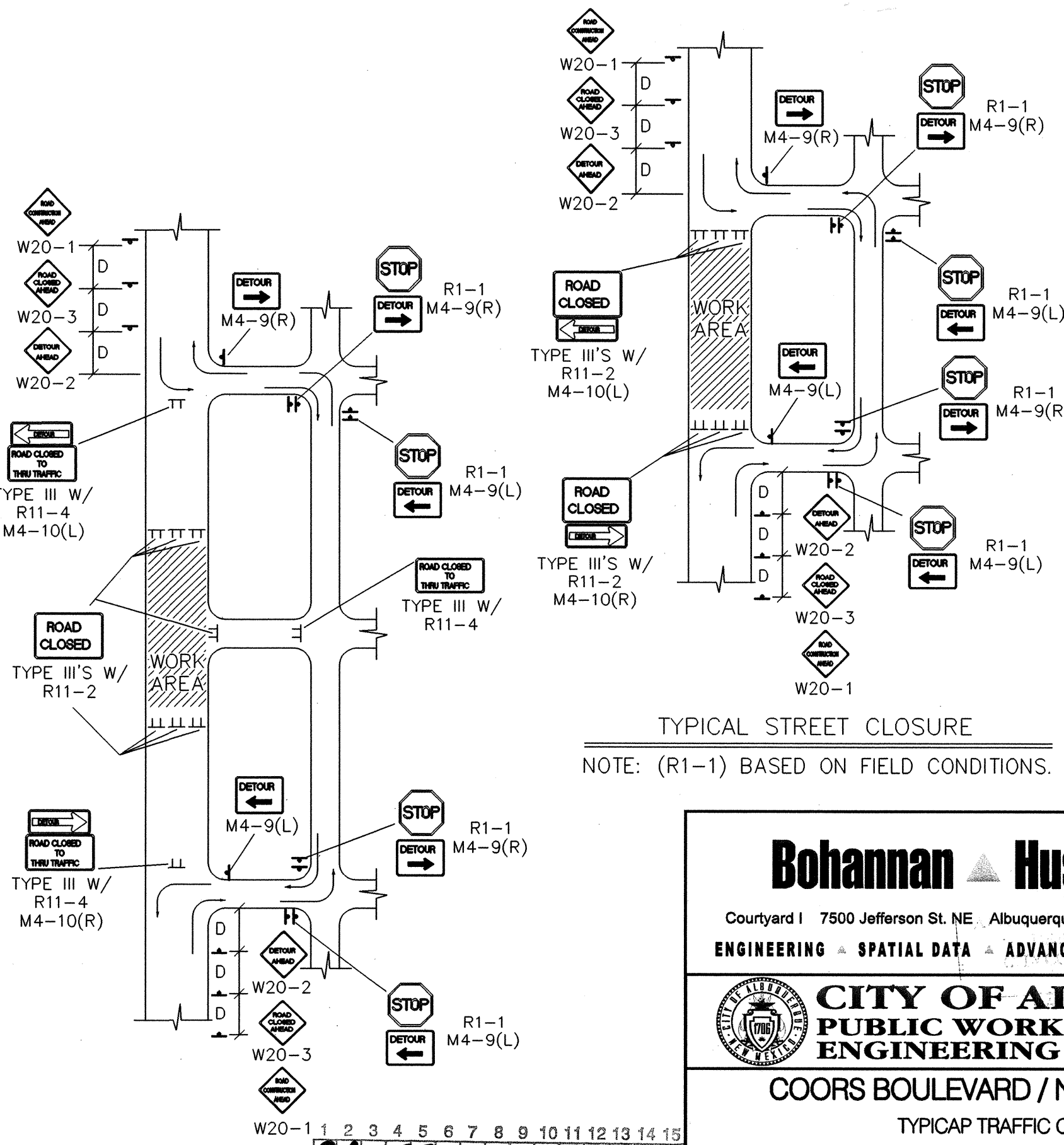
TYPICAL DOUBLE RIGHT LANE CLOSURE



TYPICAL DOUBLE LEFT LANE CLOSURE



TYPICAL FLAGGING OPERATION



TYPICAL STREET CLOSURE

NOTE: (R1-1) BASED ON FIELD CONDITIONS.

GENERAL NOTE:

ALL CONSTRUCTION WARNING
SIGNS SHALL HAVE A BLACK
LEGEND ON AN ORANGE
BACKGROUND.

ALL ADVANCE WARNING SIGNS
SHALL BE A MINIMUM OF
FORTY EIGHT (48) INCHES BY
FORTY EIGHT (48) INCHES IN
SIZE AND SHALL HAVE ONE
WARNING LIGHT.

| | | | |
|---|--|----------------------|--|
| CONTRACTOR | | AS-BUILT INFORMATION | |
| JOSEPH | | DATE | |
| 1969 | | DATE | |
| INSPECTOR'S POSITION (NAD 1927) | | DATE | |
| N.M. State Plane Coordinates (Central Zone) | | DATE | |
| X = 366,806.62 Y = 1,502,337.54 | | DATE | |
| Ground-to-Grid Factor = 0.9996734 | | DATE | |
| ΔX = -001'5.74" | | DATE | |
| SLD 1929 Elevation = 5113.852 | | DATE | |
| NO. | | NO. | |
| BY | | BY | |
| DATE | | DATE | |
| REMARKS | | REVISIONS | |
| DESIGN | | DESIGN | |
| NO. | | NO. | |
| DATE | | DATE | |
| 2/2004 | | 2/2004 | |
| DRAWN BY | | CHECKED BY | |
| COA | | COA | |
| DATE | | DATE | |
| 2/2004 | | 2/2004 | |
| CITY PROJECT NO. | | ZONE MAP NO. | |
| 733781 | | F-11 | |
| SHEET | | OF | |
| 14 | | 14 | |