

MID-REGION COUNCIL OF GOVERNMENTS

ALBUQUERQUE, NEW MEXICO

CONSTRUCTION PLANS

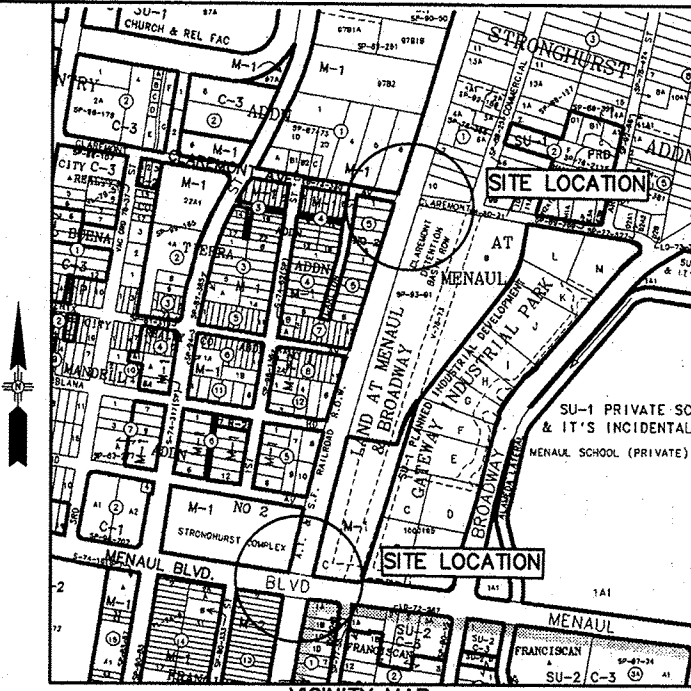
FOR

CLAREMONT AVE.

STA 10+00 TO STA 15+92.26

MENAU BLVD.

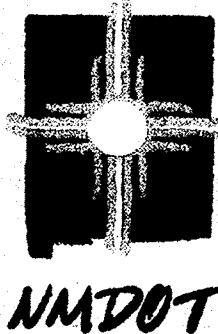
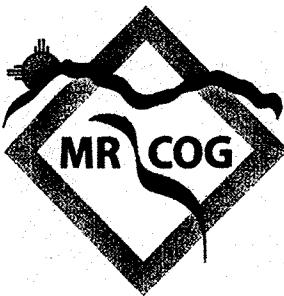
STA 10+00 TO STA 15+68.56



RAIL CROSSING IMPROVEMENTS

RAILROAD QUIET ZONE

COA PROJECT NO. 7839.81



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FOR INFORMATION ONLY

As-builts and Closeout package not submitted.  
Construction verified by aerial imagery.

By: Jim Roeder Date: 12/31/2019

**WILSON & COMPANY**  
4900 LANG AVE., NE  
SUITE 100  
ALBUQUERQUE, NEW MEXICO 87109

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SYMBOL LEGEND		
PROPOSED	EXISTING	DESCRIPTION
		CONCRETE
		COLORED PATTERNED CONCRETE
---		SLOPE LIMITS
	----	RIGHT-OF-WAY
		BOLLARD
		CONTROL POINT
		MONITORING WELL
		CANTILEVER SIGNAL
		SIGNAL POLE w/ARM
		SINGLE POST SIGN
		DOUBLE POST SIGN
		GUY WIRE
	—COMM—	COMMUNICATION LINE, OVERHEAD
		CORROSION TEST POINT
		BUSH
		POWER POLE
		SPRINKLER
		LIGHT POLE
		J-BOX
		WATER VALVE
		UNKNOWN VALVE
		REFLECTOR
		ELECTRIC METER
		TRAFFIC LIGHT
		BENCH
		TRAFFIC CONNECTION BOX
		TRAFFIC CONTROL BOX
		TELEPHONE RISER
		ELECTRIC BOX
		ELECTRIC SPLICE BOX
		CABLE TV BOX
		SPRINKLER CONTROL BOX
		WATER METER
		SIGNAL LIGHT JUNCTION BOX
		FIRE HYDRANT
		DROP INLET
		GATE
● ST		STORM DRAINAGE MANHOLE
● SS		SANITARY SEWER MANHOLE
		TELEPHONE MANHOLE
		ELECTRIC MANHOLE
		CURB AND GUTTER
		SIDEWALK
	—G—	UNDERGROUND GAS LINE
	—UGT—	UNDERGROUND TELEPHONE LINE
	—FO—	UNDERGROUND FIBER OPTICS LINE
	—V—	UNDERGROUND WATERLINE
	—CATV—	UNDERGROUND CABLE TELEVISION LINE
	—UGE—	UNDERGROUND ELECTRIC LINE
	—DHE—	OVERHEAD ELECTRIC LINE
	—SAS—	UNDERGROUND SANITARY SEWER LINE
—SD—	—SD—	UNDERGROUND STORM DRAIN LINE
	—	WIRE FENCE
	—	PIPE FENCE
	—	WOOD FENCE
	—	BLOCK WALL
	—3"—	UNDERGROUND CONDUIT

## UTILITY OWNERS

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## RAILROAD CONTACTS

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Wiley, Matthew	Office Phone: (505)247-1750	Mid-Region Council of Governments

**WILSON  
& COMPANY**

<b>MID-REGION COUNCIL OF GOVERNMENTS</b>	
<b>CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS</b>	
<b>SYMBOL LEGEND AND UTILITY CONTACT LIST</b>	
Design Review Committee  DESIGN REVIEW COMMITTEE	City Engineer Approval  CITY ENGINEER
City Project No. <b>7839.81</b>	Zone Map No. <b>H-14, H-15</b>
Sheet <b>2 of 13</b>	Last Design Update Mo./Day/Yr.

GENERAL NOTES:

1. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER THIS CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION, AS AMENDED THROUGH UPDATE NO. 7 AND AMENDMENT NO. 1 DATED 08/19/2005.
2. THE CONTRACTOR SHALL PROVIDE INGRESS AND EGRESS TO LOCAL BUSINESSES AND RESIDENCES FOR THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL ADVISE OF AND SCHEDULE ACCESS CLOSURES, AT LEAST 24 HOURS IN ADVANCE, WITH PROPERTY OWNERS AND THE CONSTRUCTION PROJECT MANAGER.
3. FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE CONSTRUCTION COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (924-3400) PRIOR TO OCCUPYING AN INTERSECTION.
4. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT N.M. ONE-CALL SYSTEM, (505) 260-1990, FOR LOCATION OF EXISTING UTILITIES.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING, AT THEIR OWN EXPENSE, ALL PERMITS NECESSARY FOR CONSTRUCTION.
6. ALL WORK IMPACTING AN ARTERIAL ROADWAY WILL REQUIRE 24-HOUR CONSTRUCTION IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE'S ORANGE BARREL POLICY, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON.
7. CRITICAL INTERSECTION WORK SHALL NOT START UNTIL THE CONTRACTOR HAS ALL MATERIAL, EQUIPMENT, AND NECESSARY PERSONNEL ON-SITE. TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED PREMATURELY.
8. OVERNIGHT PARKING OF CONSTRUCTION VEHICLES ON PRIVATE PROPERTY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
9. THE CONSTRUCTION CLEAR ZONE FOR THIS PROJECT IS 16 FEET FROM THE EDGE OF DRIVING LANE WITH A REDUCED SPEED LIMIT (25 MPH.) THE CONTRACTOR SHALL NOT STORE EQUIPMENT OR MATERIAL WITHIN THE CONSTRUCTION CLEAR ZONE UNLESS THE EQUIPMENT OR MATERIAL IS PROPERLY SHIELDED UTILIZING CURRENT SAFETY DESIGN AND INSTALLATION METHODS. THE SAFETY DESIGN FOR SHIELDING SHALL BE PROVIDED BY THE CONTRACTOR AND MUST BE APPROVED BY THE PROJECT MANAGER BEFORE IMPLEMENTING. THIS WORK, INCLUDING DESIGN, INSTALLATION, AND REMOVAL OF THE SHIELDING, SHALL BE CONSIDERED INCIDENTAL TO THE COMPLETION OF THE PROJECT AND NO ADDITIONAL PAYMENT WILL BE MADE.
10. THE CONTRACTOR SHALL NOTIFY THE ALBUQUERQUE TRAFFIC ENGINEERING DIVISION THREE (3) WORKING DAYS IN ADVANCE OF ANY WORK REQUIRED REGARDING ALL EXISTING REGULATORY SIGNS AND SIGNALS THAT NEED TO BE REMOVED, RELOCATED, OR REINSTALLED. CALL 857-8680. REFER TO SECTION 18.4.4 OF THE SPECIFICATIONS.
11. WHEN EXISTING TRAFFIC SIGNALS ARE TURNED OFF, THE CONTRACTOR SHALL HAVE AN OFF-DUTY POLICE OFFICER DIRECT TRAFFIC. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION AND NO ADDITIONAL PAYMENT WILL BE MADE.
12. THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE ENGINEER MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE ENGINEER AND SHALL NOTIFY THE ENGINEER AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. REPLACEMENT SHALL BE DONE ONLY BY THE CITY SURVEYOR. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATION OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT THE CONTRACTOR'S EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO SECTION 4.4 OF THE GENERAL CONDITIONS OF THE STANDARD SPECIFICATIONS.
13. THE CONTRACTOR WILL BE REQUIRED TO CONFINE THEIR WORK WITHIN THE CONSTRUCTION LIMITS AND/OR RIGHT OF WAY TO PRESERVE EXISTING VEGETATION AND PRIVATE PROPERTY. OVERNIGHT PARKING OF CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAY OPENINGS OR DESIGNATED TRAFFIC LANES.
14. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT BUILDING STRUCTURES ADJACENT TO THE PROJECT NOT BE DAMAGED DUE TO ANY CONSTRUCTION ACTIVITIES. DAMAGE CAUSED TO ANY BUILDING STRUCTURE WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR, INCLUDING ALL COSTS INCURRED IN RESTORING/REPAIRING SAID DAMAGE.
15. THE CONTRACTOR SHALL SUBMIT A PROPOSED WORK PLAN FOR PEDESTRIAN IMPROVEMENTS TO THE PROJECT MANAGER FOR REVIEW AND APPROVAL PRIOR TO INITIATING ANY WORK AFFECTING PEDESTRIANS. THIS PLAN SHALL INCLUDE THE METHOD PROPOSED TO MAINTAIN PEDESTRIAN ACCESS TO BUSINESSES, SCHOOLS, HOSPITALS, BUILDINGS, ETC. THROUGHOUT THE PEDESTRIAN IMPROVEMENTS CONSTRUCTION IN PARTICULAR. THE CONTRACTOR, AT MINIMUM, SHALL MAINTAIN A 36" CLEAR PATH FOR PEDESTRIANS SO AS TO MEET ADA ACCESSIBILITY REQUIREMENTS.
16. THE CONTRACTOR SHALL MAINTAIN AN UP TO DATE SET OF AS-BUILT PLANS FOR THE PROJECT. THESE PLANS SHALL BE KEPT CURRENT, WITHIN TWO WEEKS, AT ALL TIMES AND SHALL BE SUBJECT TO REVIEW BY THE PROJECT MANAGER THROUGHOUT THE PROJECT AND WILL BE REVISED BY THE PROJECT MANAGER FOR ACCURACY AND COMPLETENESS AT LEAST ONCE EVERY 30 DAYS. THE FINAL AS-BUILT PLANS SHALL BE SUBMITTED TO THE PROJECT MANAGER PRIOR TO FINAL PAYMENT.
17. THE CONTRACTOR SHALL BE RESTRICTED TO THE USE OF A 35 TON MAXIMUM NON-VIBRATORY ROLLER TO OBTAIN THE REQUIRED COMPACTION IN PAVEMENT STRUCTURE, ROADWAY BACKFILL, EMBANKMENT, AND SUBGRADES IN URBAN AREAS WHERE THE USE OF HEAVIER EQUIPMENT COULD DAMAGE UNDERGROUND UTILITIES OR OTHER PERMANENT STRUCTURES.
18. MANHOLE RIM ELEVATIONS SHOWN ON THESE PLANS ARE APPROXIMATE AND WILL VARY WITH THE FINISHED PAVEMENT ELEVATIONS.
19. ALL RADII OF CURB RETURNS ARE SHOWN TO THE FACE OF CURB AND, UNLESS OTHERWISE NOTED, ALL OFFSETS AND DIMENSIONS ARE TO THE FACE OF CURB. MATCH FLOWLINES OF GUTTERS WHEN CONNECTING NEW CURB AND GUTTER WITH EXISTING CURB AND GUTTER.
20. EXACT LOCATION OF DRIVEPADS AND WHEELCHAIR RAMPS WILL BE DETERMINED IN THE FIELD BY THE FIELD ENGINEER. LOCATIONS SHOWN ON PLAN AND PROFILE SHEETS ARE APPROXIMATE.
21. WHERE PULL BOXES ABUT THE BACK OF CURB OR ARE LOCATED IN A CONCRETE PAVED AREA, PROVIDE 3/4" EXPANSION MATERIAL AROUND THE PULL BOX. ALL PULL BOXES TO BE FLUSH WITH SURFACE OF CONCRETE.
22. WHEN ABUTTING NEW PAVEMENT TO EXISTING, CONTRACTOR SHALL SAW CUT, TO FULL PAVEMENT DEPTH, THE EXISTING PAVEMENT TO A NEAT VERTICAL STRAIGHT LINE AS REQUIRED TO REMOVE ANY BROKEN OR CRACKED PAVEMENT AND MATCH NEW TO EXISTING.
23. THE CONTRACTOR SHALL VERIFY ALL EXISTING FIRE HYDRANT FLANGES, PADS, VALVE BOXES, MANHOLE RIMS AND TOP OF PIPE ELEVATIONS IN THE FIELD. ELEVATIONS SHALL BE ADJUSTED TO COMPLY WITH THE REQUIREMENTS OF THE STANDARD CITY DETAILS.
24. WHEN APPEARING IN ANY PORTION OF THIS CONTRACT, THE WORDS "PLANT MIX BITUMINOUS PAVEMENT" (PMBP) WILL HAVE THE SAME MEANING AS AND MAY BE USED INTERCHANGEABLY WITH THE WORDS "ASPHALT CONCRETE".
25. THE CONTRACTOR SHALL WARP SLOPES WHERE NECESSARY TO STAY WITHIN THE RIGHT OF WAY OR CONSTRUCTION EASEMENT LIMITS. THE WORK SHALL BE AS DIRECTED BY THE PROJECT MANAGER.
26. THE SUBGRADE PREP SHALL EXTEND ONE (1) FOOT BEYOND THE FREE EDGE OF NEW SIDEWALK.
27. THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENTS, PAVEMENT MARKINGS, CURB AND GUTTER, DRIVEPADS, HANDICAP RAMPS, AND SIDEWALK DURING CONSTRUCTION APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS, AND SHALL REMOVE AND REPLACE AS PER COA STANDARDS AT THEIR OWN EXPENSE.
28. WHEN REMOVAL OF EXISTING CURB AND GUTTER OR SIDEWALK IS REQUIRED, REMOVE TO NEAREST JOINT.
29. ALL CONSTRUCTION DEBRIS, SPOIL AND NON-SALVAGEABLE ITEMS BECOME THE PROPERTY OF THE CONTRACTOR AND PROPER DISPOSAL OF THESE ITEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. ANY HAUL COSTS ASSOCIATED WITH THIS WORK IS CONSIDERED INCIDENTAL TO THE PROJECT.
30. ALL TRENCH EXCAVATION BACKFILL WITHIN THE ROADWAY PRISM SHALL BE COMPACTED TO 95% OF THE MODIFIED PROCTOR PER ASTM D-698 OR D-1557 AS APPLICABLE.
31. ANY EARTHWORK HAUL ON THIS PROJECT WILL BE CONSIDERED AS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE THEREFOR.
32. A SHRINKAGE FACTOR OF 10% SHALL BE APPLIED TO COMPENSATE FOR THE CHANGE IN MATERIAL VOLUME FOR CUT TO FILL SECTIONS. EARTHWORK QUANTITIES ARE BASED ON THIS SHRINKAGE FACTOR FOR ROADWAY EMBANKMENT.

33. STRUCTURAL FILL FOR EMBANKMENTS, PAVEMENTS, AND STRUCTURES SHALL BE PLACED IN LIFTS THAT, FOLLOWING COMPACTION, DO NOT EXCEED 8". FILLS SHALL BE COMPACTED AT MOISTURE CONTENTS WITHIN +/- 2% OF OPTIMUM MOISTURE CONTENT. SELECT STRUCTURAL FILL AROUND CULVERTS AND OTHER STRUCTURES SHOULD BE COMPACTED, USING HAND OPERATED COMPACTION EQUIPMENT AND IN LIFTS NOT EXCEEDING 4", TO THE DENSITY AND MOISTURE CONTENT SPECIFIED ABOVE. THE UPPER 12" OF ROADBEDS SHALL BE COMPACTED TO 95% OF MAXIMUM DRY DENSITY (ASTM 1557).
34. ALL EXISTING UTILITIES SHOWN HEREIN WERE TAKEN FROM RECORD DRAWINGS, FIELD SURVEYS, C.O.A. SYSTEMS UTILITIES MAPS AND INFORMATION PROVIDED BY THE UTILITY OWNERS AND ARE APPROXIMATE. IT WILL BE CONTRACTORS RESPONSIBILITY TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS AND TYPE OF EXISTING UTILITIES TO BE ADJUSTED OR EXTENDED AND TO PROVIDE PROTECTION FOR ALL UTILITIES WITHIN THE CONSTRUCTION AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR EXPLORATORY TRENCHING AND EXPOSING OF EXISTING UTILITIES (POTHOLING) TO CONFIRM SIZE, LOCATION, ETC. THIS WORK WILL BE CONSIDERED AS INCIDENTAL TO THE PROJECT.
35. CONTRACTOR IS TO EXERCISE DUE CARE TO AVOID DISTURBING ANY EXISTING UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE UTILITY COMPANIES IN ORDER TO PREVENT ANY SERVICE DISRUPTION THAT MIGHT RESULT FROM PROJECT CONSTRUCTION. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND PRESERVE UTILITY EQUIPMENT AFFECTED BY PROJECT CONSTRUCTION. SHOULD THE CONTRACTOR BREAK AN EXISTING UTILITY LINE DURING CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL BE RESPONSIBLE FOR UTILITY REPAIR COSTS.
36. THE CONTRACTOR SHALL NOTIFY THE LOCAL FIRE DEPARTMENT AT LEAST TWO (2) WORKING DAYS IN ADVANCE OF WHEN FIRE HYDRANTS WILL BE TAKEN OUT OF SERVICE AND RETURNED TO SERVICE.
37. THE ABCWUA (857-8200) WILL BE NOTIFIED BY THE CONTRACTOR SEVEN(7) WORKING DAYS IN ADVANCE OF ANY WORK WHICH MAY AFFECT THE EXISTING PUBLIC WATER FACILITIES. REFER TO SECTION 18 OF THE STANDARD SPECIFICATIONS.
38. THE CONTRACTOR SHALL ADJUST TO GRADE ALL WATER VALVE BOXES, SANITARY SEWER MANHOLES AND STORM DRAIN MANHOLES WHICH MAY BE AFFECTED BY THIS CONTRACT.
39. ALL EXCAVATION, TRENCHING, AND SHORING ACTIVITIES MUST BE CARRIED OUT IN ACCORDANCE WITH OSHA 29 CFR 1926.650 SUBPART P.
40. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSING OF DEBRIS, INCLUDING BUT NOT LIMITED TO HAZARDOUS WASTE AT DISPOSAL SITES APPROVED BY GOVERNMENTAL AGENCIES REGULATING THE DISPOSAL OF SUCH MATERIALS.
41. THE CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE AND SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.
42. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING ALL CONSTRUCTION SIGNING UNTIL THE PROJECT HAS BEEN ACCEPTED BY THE CITY.
43. ALL CONSTRUCTION WITHIN THE NMDOT RAILROAD RIGHT OF WAY SHALL BE COORDINATED WITH HENRY GONZALES (505-837-3219) AND MATTHEW WILEY (505-247-1750) PRIOR TO COMMENCING WORK. A FLAGMAN WILL BE REQUIRED FOR ALL WORK WITHIN THE NMDOT RAILROAD RIGHT OF WAY.

44. EXISTING VALVE CANS AND MANHOLES SHALL BE ADJUSTED TO FINAL GRADE.

AS-BUILT INFORMATION	
CONTRACTOR	DATE
WORK STARTED BY	DATE
WORK STOPPED BY	DATE
FIELD VERIFICATION BY	DATE
RECORDED BY	DATE
MICRO-FILM INFORMATION	
NO.	DATE

BENCH MARKS	
NMSHC/ACS STATION BEING A BRASS	DATE
TABLET STAMPED "STAIN47-13" SET IN	DATE
TOP OF A CONC. POST PROJECTING 0.1 ft.	DATE
ABOVE THE GROUND TO REACH THE STA.	DATE
GO NORTH FROM CENTRAL AVE. & 2nd ST	DATE
1.8 mi. TO THE STA. IN THE CENTER OF	DATE
2nd ST. MEDIAN STRIP JUST SOUTH OF	DATE
THE INTERSECTION. (1st-order).	DATE
ELEVATION = 4968.392 FT. (SID 1929)	DATE

SURVEY INFORMATION	
FIELD NOTES	DATE
BY	DATE
NO.	DATE

ENGINEER'S SEAL	
REVISIONS	BY
NO.	DATE
WILSON & COMPANY, ENGINEERS & ARCHITECTS	
DESIGNED BY	DATE
DRAWN BY	DATE
CHECKED BY	DATE



MID-REGION COUNCIL OF GOVERNMENTS			
CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS			
GENERAL NOTES			
Design Review Committee	City Engineer Approval	Mo./Day/Yr.	Mo./Day/Yr.
DESIGN REVIEW COMMITTEE	City Engineer		
City Project No.	Zone Map No.	Sheet	
7839.81	H-14, H-15	3 of 13	

SUMMARY OF QUANTITIES

Claremont Avenue  
Project No. 7839.81

6.	7.		9.	11.
Item No.	Description	Unit	Est. Quantity	As-Built Quantity
301.020	Subgrade Preparation 12"	SY	130	
336.082	Art Asp Conc, Type SPIII, 2" Thick, Material & Placement, cip.	SY	260	
336.120	Tack Coat	SY	130	
340.060	Curb & Gutter, Median, PCC	LF	282	
340.050	Curb & Gutter, Standard, PCC	LF	38	
340.025	Drivepad, 6" thick, PCC, incl. subgrade compaction, cip.	SY	15	
440.015	Reflectorized painting for median noses, cip.	SF	282	
441.001	Reflectorized thermoplastic pavement markings, 4" width, cip.	LF	600	
441.005	Reflectorized thermoplastic pavement markings, 24" width, cip.	LF	56	
441.011	Ref Plas RXR	EA	2	
443.101	Removal of pavement stripe, based on 4" width, painted or plastic, compl.	LF	300	
343.030	Asphalt concrete pavement, 4" or more, sawcut, remove & dispose, compl.	SY	85	
343.080	Existing curb and gutter, PCC, remove & dispose, compl.	LF	38	
343.085	Existing sidewalk, PCC, remove & dispose, compl.	SY	15	
450.001	Aluminum Panel Sign, cip.	SF	11	
450.010	Square tube steel posts & base posts for aluminum panel sign, cip	LF	26	
---	Remove and dispose of existing fence, compl.	LF	60	
---	New 18-ft wide gate, cip.	Each	1	
---	New fence, cip.	LF	50	
---	Landscape removal, cap irrigation lines, compl.	LS	1	

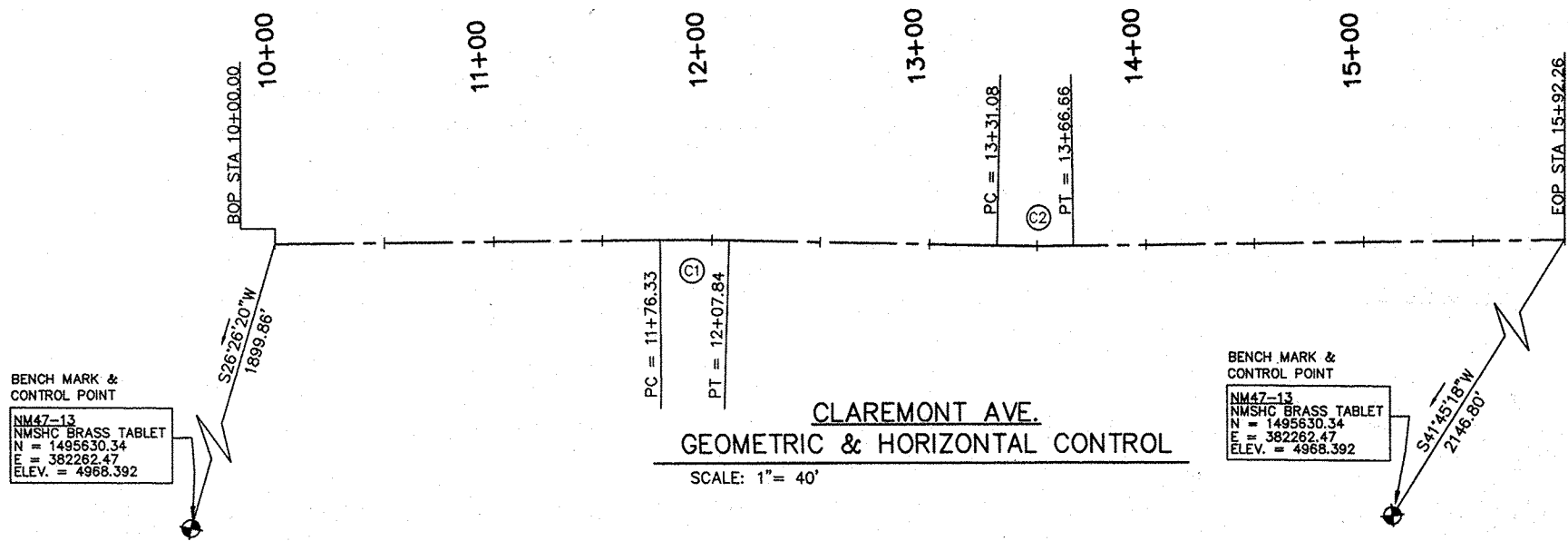
Menaul Boulevard  
Project No. 7839.81

6.	7.		9.	11.
Item No.	Description	Unit	Est. Quantity	As-Built Quantity
	No work within City of Albuquerque right of way.			

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS-BUILT INFORMATION	
		FIELD NOTES		NMSHC/ACS STATION BEING A BRASS		CONTRACTOR	
NO. DATE		BY DATE		TABLET STAMPED "STANM47-13" SET IN TOP OF A CONC. POST PROJECTING 0.1 ft ABOVE THE GROUND TO REACH THE STA. GO NORTH FROM CENTRAL AVE. & 2nd ST 1.8 mi. TO THE STA. IN THE CENTER OF 2nd ST. MEDIAN STRIP JUST SOUTH OF THE INTERSECTION. (1st-order) ELEVATION = 4968.392 FT. (SLD 1929)		WORK STARTED BY DATE	
REVISIONS		REMARKS				REVISIONS BY DATE	
WILSON & COMPANY, ENGINEERS & ARCHITECTS						REVISIONS BY DATE	
DESIGNED BY: SGC						REVISIONS BY DATE	
DRAWN BY: ELO						REVISIONS BY DATE	
CHECKED BY: DDM						REVISIONS BY DATE	

WILSON  
& COMPANY

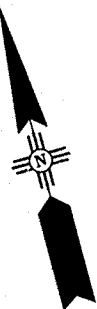
MID-REGION COUNCIL OF GOVERNMENTS	
CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS	
SUMMARY OF QUANTITIES	
Design Review Committee	City Engineer Approval
City Project No.	Zone Map No.
7839.81	H-14, H-15
Sheet	4 of 13



CENTERLINE OF CONSTRUCTION					PROJECT GROUND COORDINATES (FT)	
REF.	DESCRIPTION	STATION	BEARING	DISTANCE	NORTHING	EASTING
CLAREMONT AVE.						
8 of	BOP	10+00			1497331.4929	383108.3686
8 of	COURSE FROM BOP TO (NO HRZ. CURVE) PC CURVE NO. 1		S80°41'43"E	176.33'		
8 of	PC CURVE NO. 1	11+76.33			1497302.9872	383282.3822
8 of	PI	11+92.09			1497300.4348	383297.9293
8 of	PT CURVE NO. 1	12+07.84			1497297.3991	383313.3885
8 of	COURSE FROM PT TO (NO HRZ. CURVE) PC CURVE NO. 2		S78°53'24"E	123.24'		
8 of	PC CURVE NO. 2	13+31.08			1497273.6523	383434.3155
8 of	PI	13+48.87			1497270.2238	383451.7746
8 of	PT CURVE NO. 2	13+66.66			1497267.4185	383469.3445
8 of	COURSE FROM PT TO (NO HRZ. CURVE) EOP		S80°55'43"E	225.60'		
8 of	EOP	15+92.26			1497231.8491	383692.1263

CURVE TABLE CENTERLINE OF CONSTRUCTION						
CURVE	LENGTH	RADIUS	TANGENT	DELTA	EXTERNAL	MID-ORD
(C1)	31.51'	1000.00'	15.75'	01°48'19"	0.12'	0.12'
(C2)	35.58'	1000.00'	17.79'	02°02'19"	0.16'	0.16'

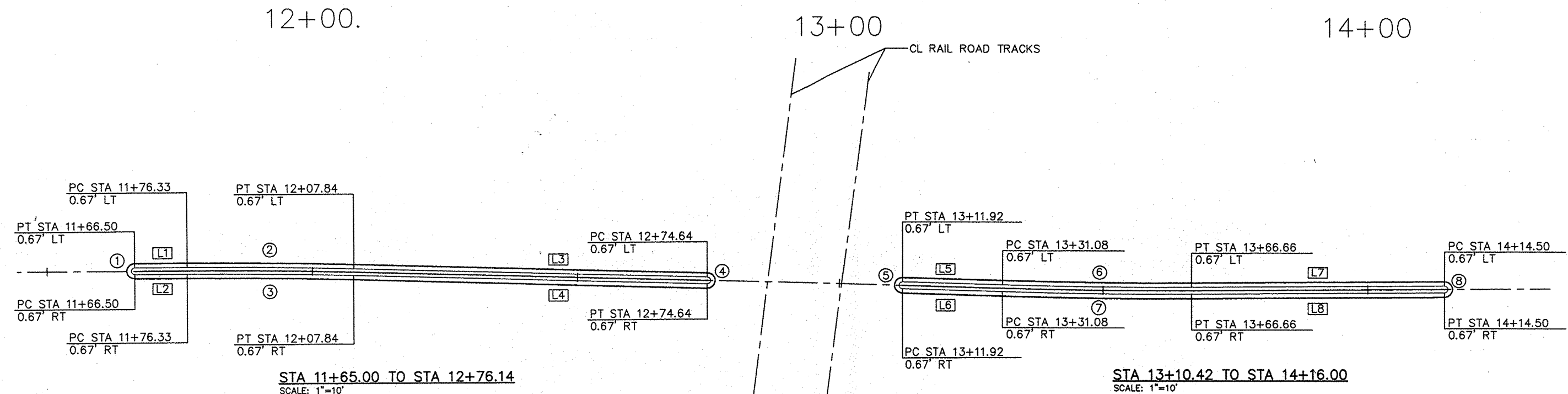
BENCHMARK:  
NMSHC/ACS CONTROL STATION "NM47-13" STANDARD NMSHC BRASS TABLET STAMPED  
"STANM47-13" SET IN TOP OF A CONCRETE POST 0.1 ft. ABOVE THE GROUND.  
NEW MEXICO STATE PLANE COORDINATES (CENTRAL ZONE)  
X=1495630.34 Y=382262.47 ELEV=4968.392 (SLD 1929)  
GROUND-TO-GRID FACTOR=0.99967825 DELTA ALPHA=(-)0°13'35" (NAD 1927)



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MID-REGION COUNCIL OF GOVERNMENTS	
CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS GEOMETRIC & HORIZONTAL CONTROL PLAN	
Design Review Committee <b>APPROVED</b> DEC 07 2007	City Engineer <b>APPROVED</b> JAN 03 2008
City Project No. 7839.81	Zone Map No. H-14, H-15
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① FL CURVE DATA	② FL CURVE DATA	③ FL CURVE DATA	④ FL CURVE DATA
$\Delta = 180^{\circ}00'00"$	$\Delta = 1^{\circ}48'19"$	$\Delta = 1^{\circ}48'19"$	$\Delta = 180^{\circ}00'00"$
R = 0.67'	R = 1000.67'	R = 999.33'	R = 0.67'
L = 2.10'	L = 31.53'	L = 31.49'	L = 2.10'
T = -----	T = 15.76'	T = 15.74'	T = -----
RADIUS POINT STA 11+66.50 0.00' RT	RADIUS POINT STA 12+07.84 1000.00' RT	RADIUS POINT STA 12+07.84 1000.00' RT	RADIUS POINT STA 12+74.64 0.00' LT

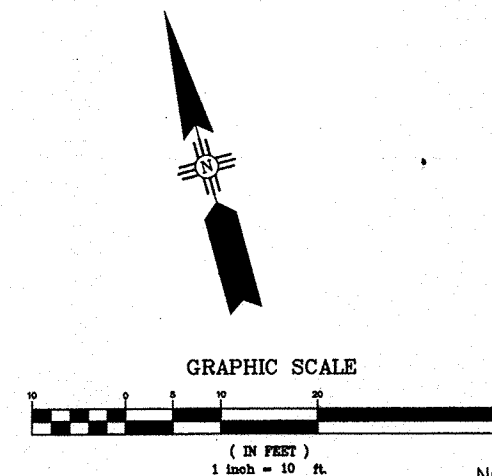
[L1] = S80°41'43"E - 9.83'  
[L2] = S80°41'43"E - 9.83'

[L3] = S78°53'24"E - 66.80'  
[L4] = S78°53'24"E - 66.80'

⑤ FL CURVE DATA	⑥ FL CURVE DATA	⑦ FL CURVE DATA	⑧ FL CURVE DATA
$\Delta = 180^{\circ}00'00"$	$\Delta = 2^{\circ}02'19"$	$\Delta = 2^{\circ}02'19"$	$\Delta = 180^{\circ}00'00"$
R = 0.67'	R = 999.33'	R = 1000.67'	R = 0.67'
L = 2.10'	L = 35.56'	L = 35.61'	L = 2.10'
T = -----	T = 17.78'	T = 17.80'	T = -----
RADIUS POINT STA 13+11.92 0.00' RT	RADIUS POINT STA 13+66.66 1000.00' LT	RADIUS POINT STA 13+66.66 1000.00' LT	RADIUS POINT STA 14+14.50 0.00' LT

[L5] = S78°53'24"E - 19.16'  
[L6] = S78°53'24"E - 19.16'

[L7] = S80°55'43"E - 47.84'  
[L8] = S80°55'43"E - 47.84'



NOTE: ALL STATIONS/OFFSETS AND CURVE DATA IS TAKEN ALONG THE FLOWLINE OF CURB AND GUTTER UNLESS OTHERWISE NOTED.

**WILSON & COMPANY**

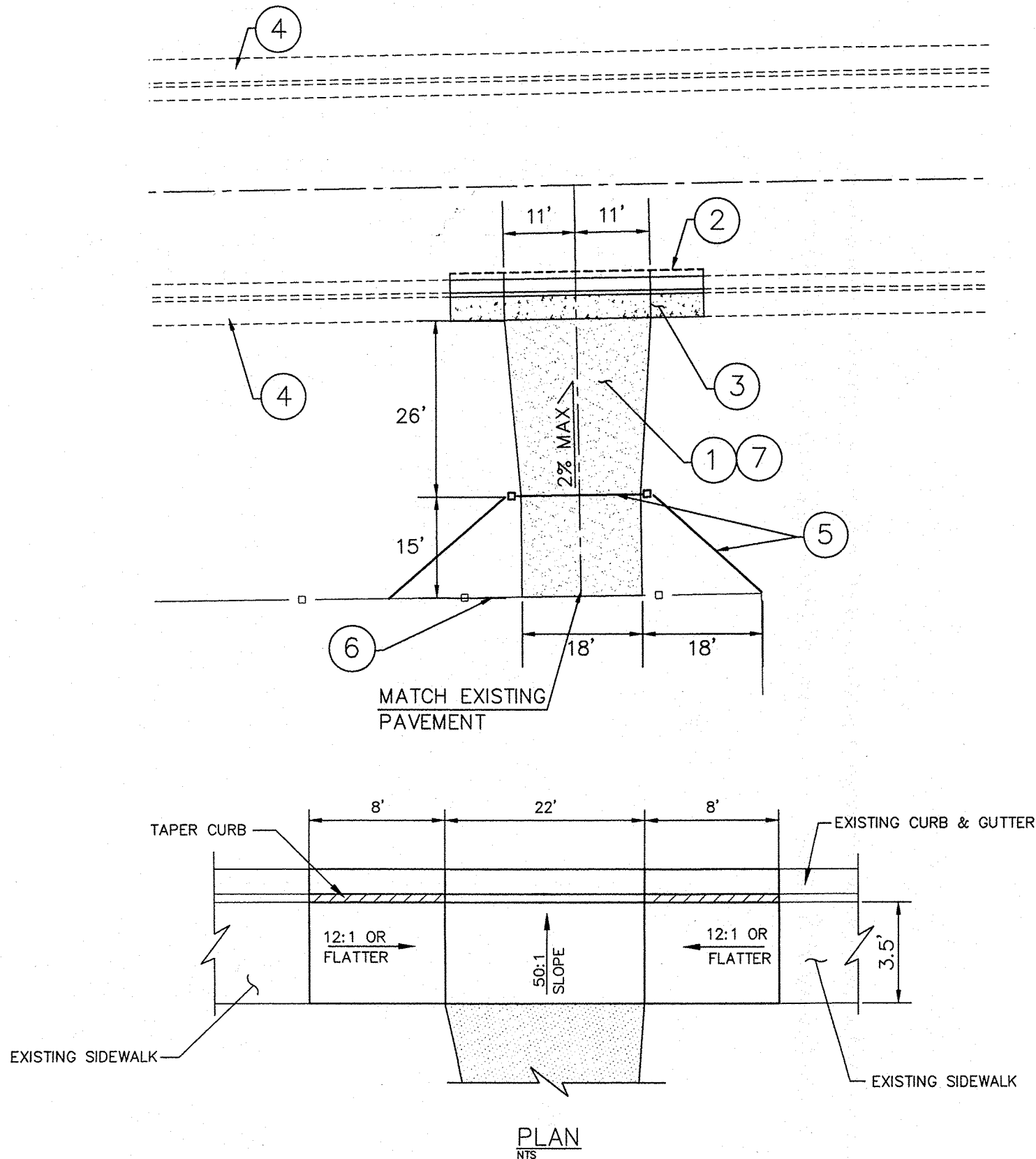
MID-REGION COUNCIL OF GOVERNMENTS	
CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS	
MEDIAN GEOMETRIC PLAN	
Design Review Committee APPROVED DEC 07 2007	City Engineer Approval APPROVED JAN 08 2008
City Project No. 7839.81	Zone Map No. H-14, H-15
Sheet 7 of 13	

SURVEY INFORMATION		BENCH MARKS		AS-BUILT INFORMATION	
NO.	DATE	DATE	CONTRACTOR	DATE	DATE
			ANSHC/ACS STATION BEING A BRASS		
			TABLET STAMPED "STANM47-13" SET IN		
			TOP OF A CONC. POST PROJECTING 0.1 ft		
			ABOVE THE GROUND TO REACH THE STA.		
			GO NORTH FROM CENTRAL AVE. & 2nd ST		
			1.8 mi. TO THE STA. IN THE CENTER OF		
			2nd ST. MEDIAN STRIP JUST SOUTH OF		
			THE INTERSECTION. (1st-order)		
			ELEVATION = 4988.392 FT. (SLD 1929)		


CLAREMONT AVE.

KEYED NOTES:

- 1 BUILD ASPHALT DRIVEWAY WITH 2-2" LIFTS OF SP III OVER 12" SUBGRADE PREP
- 2 SAWCUT LINE FOR EXISTING C&G AND ASPHALT PAVEMENT REMOVAL, 1 FT FROM LIP OF EXIST. C&G
- 3 CONSTRUCT NEW DRIVEWAY PER DETAIL THIS SHEET.
- 4 EXISTING SIDEWALK AND CURB & GUTTER TO REMAIN.
- 5 CONSTRUCT NEW WROUGHT IRON GATE AND FENCE MATCH EXISTING FENCING MATERIAL AND GATE
- 6 REMOVE & DISPOSE OF OLD FENCE
- 7 COORDINATE REMOVAL OF EXISTING LANDSCAPING WITH CITY OF ALBUQUERQUE



**GRAPHIC SCALE**



( IN FEET )  
1 inch = 10 ft.

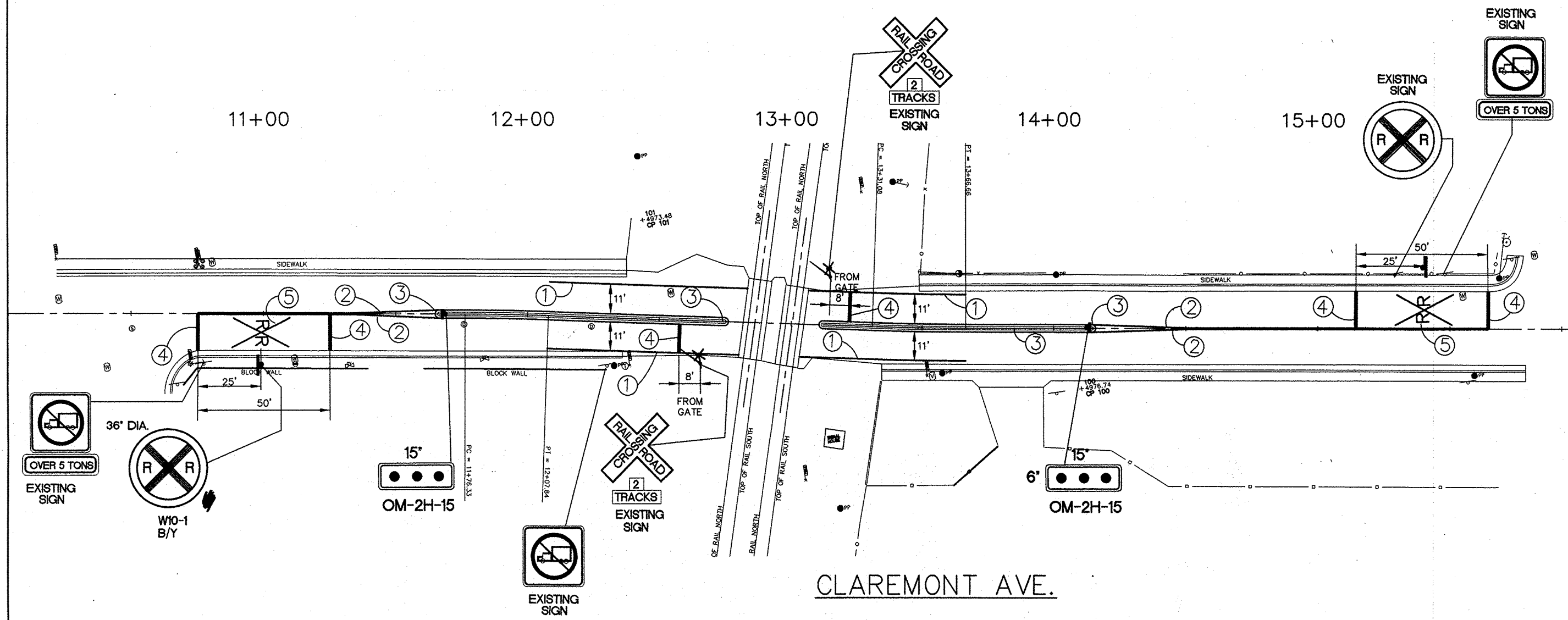
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MID-REGION COUNCIL OF GOVERNMENTS

CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS  
DRIVEWAY RELOCATION PLAN

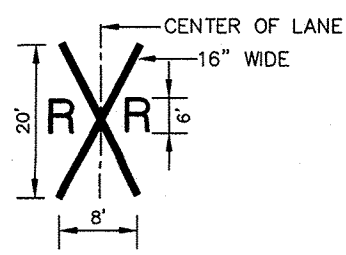
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City Project No.	Zone Map No.	Sheet
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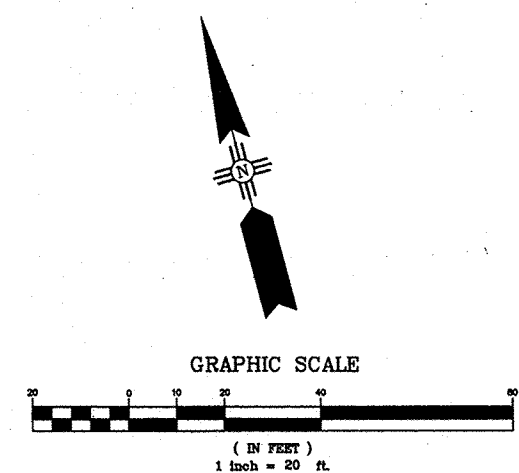


**STRIPING KEY**

- ① 4" SOLID WHITE REFLECTORIZED THERMOPLASTIC STRIPE (PREFORMED) *use 11/13/07*
- ② 4" SOLID YELLOW REFLECTORIZED THERMOPLASTIC STRIPE (PREFORMED) *use 11/13/07*
- ③ MEDIAN PAINTED SOLID YELLOW (FL TO FL, FULL LENGTH OF MEDIAN) *use 11/13/07*
- ④ 24" SOLID WHITE REFLECTORIZED THERMOPLASTIC STRIPE (PREFORMED)
- ⑤ WHITE REFLECTORIZED RR CROSSING PAVEMENT MARKINGS PER MUTCD (PREFORMED) *use 11/13/07*



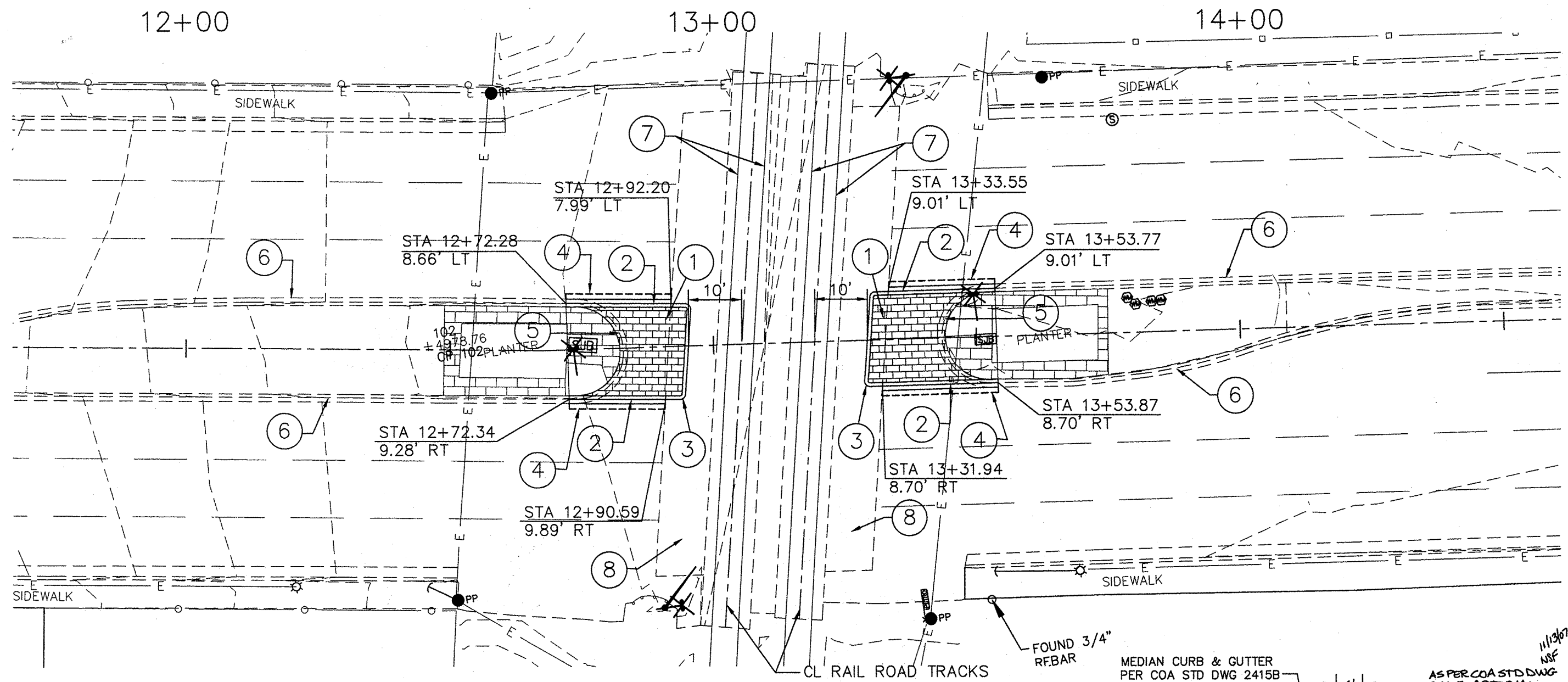
NOTE: ERADICATE EXISTING RXR STRIPING PRIOR TO REPLACING WITH NEW STRIPING.



AS-BUILT INFORMATION		BENCH MARKS		SURVEY INFORMATION		ENGINEER'S SEAL	
CONTRACTOR	DATE	NW5HC/ACS STATION BEING A BRASS	DATE	NO.	BY	REMARKS	NO. DATE
DESIGNED BY	DATE	TABLET STAMPED "STA. 1147-13" SET IN	DATE				
DRAWN BY	DATE	TOP OF A CONC. POST PROJECTING 0.1 ft.	DATE				
CHECKED BY	DATE	ABOVE THE GROUND TO REACH THE STA.	DATE				
	DATE	GO NORTH FROM CENTRAL AVE. & 2nd ST.	DATE				
	DATE	1.8 mi. TO THE CENTER OF	DATE				
	DATE	2nd ST. MEDIAN STRIP JUST SOUTH OF	DATE				
	DATE	THE INTERSECTION. (1st-order)	DATE				
	DATE	ELEVATION = 4968.392 FT. (SLD 1929)	DATE				

MID-REGION COUNCIL OF GOVERNMENTS	
CLAREMONT AVE. RAIL CROSSING IMPROVEMENTS STRIPING & SIGNING PLAN	
Design Review Committee	City Engineer Approval
APPROVED	APPROVED
DESIGNED BY	CITY ENGINEER
DRAWN BY	
CHECKED BY	
City Project No. 7839.81	Zone Map No. H-14, H-15
Sheet	9 of 13

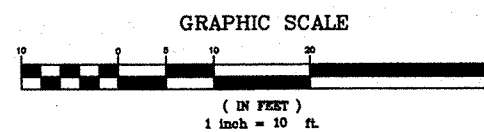
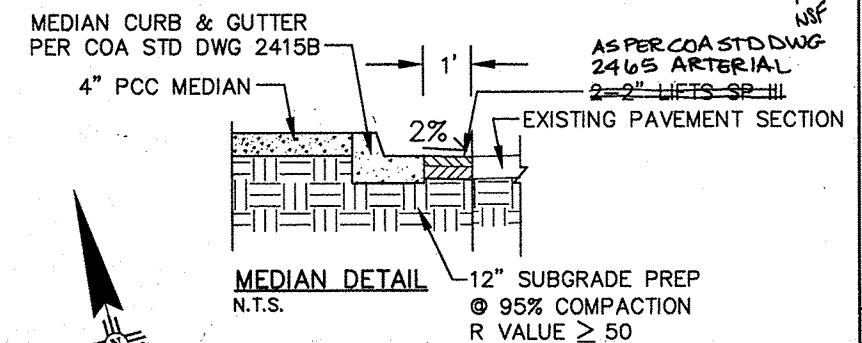




## KEYED NOTES:

- 1 BUILD MEDIAN PCC AS PER COA STD DWG 2408 MATCH EXIST. COLOR AND PATTERN
- 2 BUILD MEDIAN CURB & GUTTER AS PER COA STD DWG 2415B
- 3 BUILD TYPE II PINNED CURB AS PER COA STD DWG 2415B
- 4 SAWCUT LINE FOR EXISTING C&G AND ASPHALT PAVEMENT REMOVAL, 1 FT. FROM THE LIP OF PROPOSED MEDIAN C&G
- 5 HORIZONTAL SAWCUT AT FL OF EXISTING CURB AND REMOVE MEDIAN NOSE, GUTTER OF EXIST CURB TO REMAIN
- 6 EXISTING CURB & GUTTER TO REMAIN
- 7 EXISTING RAIL TO REMAIN
- 8 EXISTING CONCRETE APPROACH TO REMAIN

NOTE: SEE SHEET 12 OF 13 FOR MEDIAN GEOMETRICS



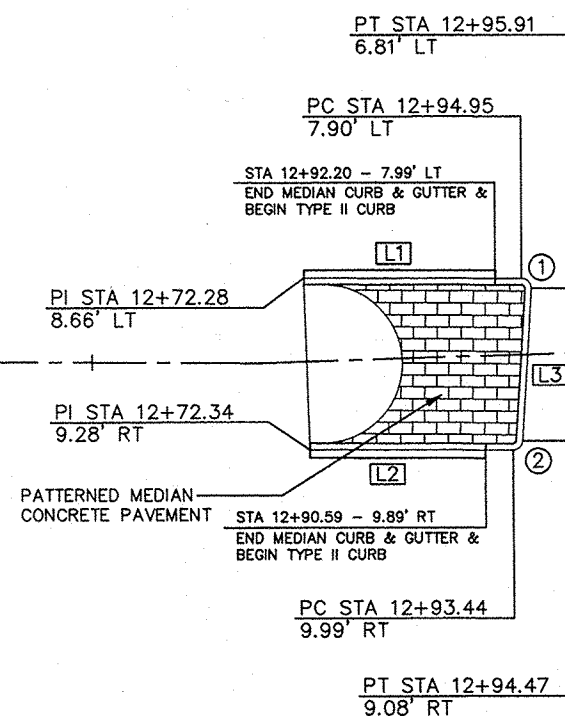
**WILSON & COMPANY**

MID-REGION COUNCIL OF GOVERNMENTS			
MENAUL BLVD. RAIL CROSSING IMPROVEMENTS CONSTRUCTION PLAN			
Design Review Committee	City Engineer Approval	Last Design Update	
		Mo./Day/Yr.	
City Project No. 7839.81	Zone Map No. H-14, H-15	Sheet 11 of 13	

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS-BUILT INFORMATION	
		NO.	DATE	CONTRACTOR	DATE	CONTRACTOR	DATE
REVISIONS		FIELD NOTES		NMSHC/ACS STATION BEING A BRASS		NMSHC/ACS STATION BEING A BRASS	
NO.	DATE	NO.	DATE	TABLET STAMPED "STANM47-13" SET IN TOP OF A CONC. POST PROJECTING 0.1 ft. ABOVE THE GROUND TO REACH THE STA.		TABLET STAMPED "STANM47-13" SET IN TOP OF A CONC. POST PROJECTING 0.1 ft. ABOVE THE GROUND TO REACH THE STA.	
REMARKS		BY		GO NORTH FROM CENTRAL AVE. & 2nd ST. 1.8 mi. TO THE STA. IN THE CENTER OF 2nd ST. MEDIAN STRIP JUST SOUTH OF THE INTERSECTION (1st-order)		GO NORTH FROM CENTRAL AVE. & 2nd ST. 1.8 mi. TO THE STA. IN THE CENTER OF 2nd ST. MEDIAN STRIP JUST SOUTH OF THE INTERSECTION (1st-order)	
DESIGNED BY		BY		ELEVATION = 4968.392 FT. (SLD 1929)		ELEVATION = 4968.392 FT. (SLD 1929)	
DRAWN BY		BY					
CHECKED BY		BY					
DATE		DATE					
DATE		DATE					
DATE		DATE					

$13+00$  $14+00$ 

RAILROAD TRACKS



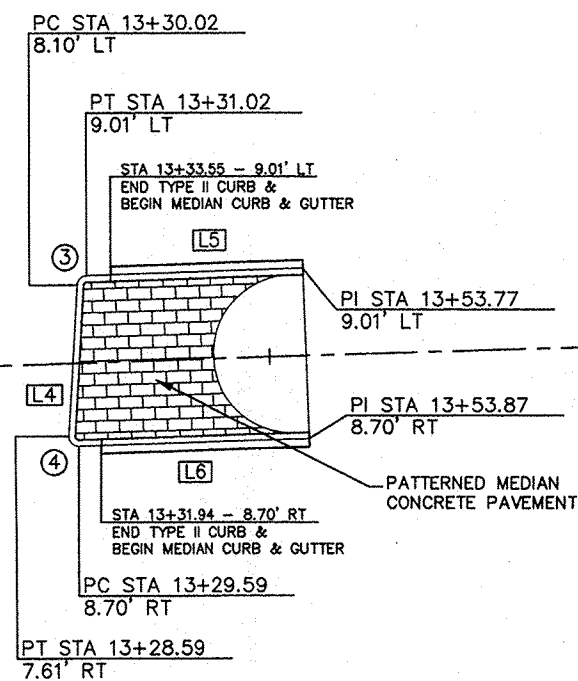
STA 12+72.28 TO STA 12+95.91  
SCALE: 1"=10'

<u>① FL CURVE DATA</u>	<u>② FL CURVE DATA</u>
$\Delta = 93^{\circ}14'01''$	$\Delta = 86^{\circ}45'59''$
$R = 1.00'$	$R = 1.00'$
$L = 1.63'$	$L = 1.51'$
$T = 1.06'$	$T = 0.95'$
<u>RADIUS POINT</u>	<u>RADIUS POINT</u>
STA 12+94.91	STA 12+93.48
6.90' LT	8.99' RT

**[L1]** = S 81°18'19"E - 22.68'

[L2] = S 81°18'19"E - 21.12'

$\boxed{L3} = S11^{\circ}55'41''W, -15.95'$



STA 13+28.59 TO STA 13+53.87  
SCALE: 1"=10'

③ <u>FL CURVE DATA</u>	④ <u>FL CURVE DATA</u>
$\Delta = 84^{\circ}48'08''$	$\Delta = 95^{\circ}11'52''$
$R = 1.00'$	$R = 1.00'$
$L = 1.48'$	$L = 1.66'$
$T = 0.91'$	$T = 1.10'$
<u>RADIUS POINT</u>	<u>RADIUS POINT</u>
STA 13+31.02	STA 13+29.59
8.01', LT	7.70', RT

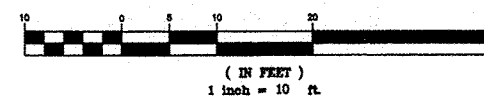
$\boxed{L4} = N11^{\circ}57'35''E, -15.78'$

[L5] = S83°14'16"E - 22.75'

**[L6] = S83°14'16"E - 24.28'**



GRAPHIC SCALE



NOTE: ALL STATIONS/OFFSETS AND CURVE DATA IS TAKEN ALONG THE FLOWLINE OF CURB AND GUTTER UNLESS OTHERWISE NOTED.

**WILSON  
& COMPANY**

MID-REGION COUNCIL OF GOVERNMENTS

MENAU BLVD. RAIL CROSSING IMPROVEMENTS  
MEDIAN GEOMETRIC PLAN

Design Review Committ

City Engineer Approval	
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Mo./Day/Yr	
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Yr.	Mo./Day/Yr.
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City Project No.

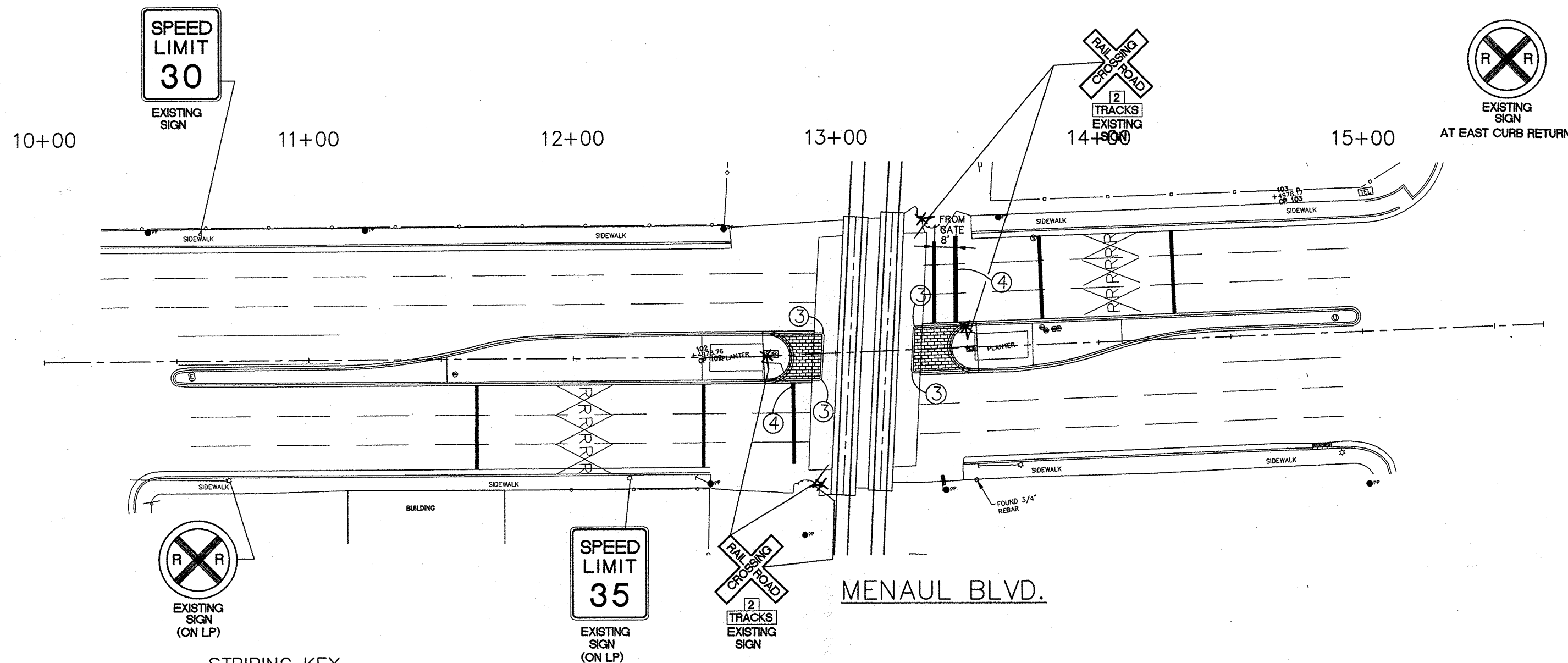
Zone Map No.

Sheet

7839.81

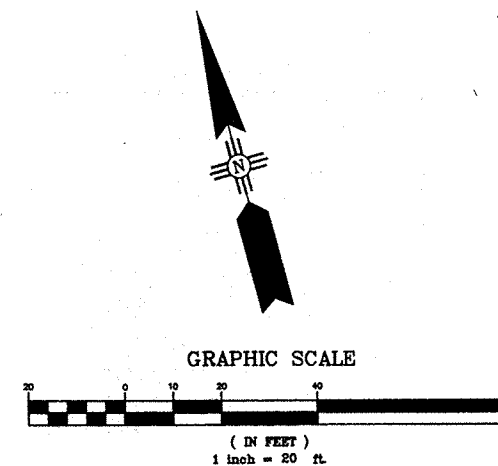
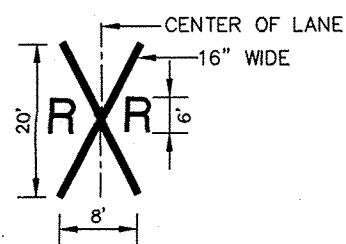
Zone Map No.  
H-14, H-15

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STRIPING KEY

- ① 4" SOLID WHITE REFLECTORIZED THERMOPLASTIC STRIPE (NOT USED)
- ② 4" SOLID YELLOW REFLECTORIZED THERMOPLASTIC STRIPE (NOT USED)
- ③ MEDIAN PAINTED SOLID YELLOW (FL TO FL, FULL LENGTH OF MEDIAN)
- ④ 24" SOLID WHITE REFLECTORIZED THERMOPLASTIC STRIPE
- ⑤ WHITE REFLECTORIZED RR CROSSING PAVEMENT MARKINGS PER MUTCD (NOT USED)



**WILSON & COMPANY**

MID-REGION COUNCIL OF GOVERNMENTS			
MENAUL BLVD. RAIL CROSSING IMPROVEMENTS STRIPING & SIGNING PLAN			
Design Review Committee	City Engineer Approval	Last Design Update	
		Mo./Day/Yr.	
City Project No.	Zone Map No.	Sheet	
7839.81	H-14, H-15	13 of 13	

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS-BUILT INFORMATION	
	NO.	DATE	BY	TABLET STAMPED	CONTRACTOR	DATE	DATE
				"STAN. 47-13" SET IN			
				TOP OF A CONC. POST PROJECTING 0.1 ft.			
				ABOVE THE GROUND TO REACH THE STA.			
				GO NORTH FROM CENTRAL AVE. & 2nd ST			
				1.8 mi. TO THE STA. IN THE CENTER OF			
				2nd ST. MEDIAN STRIP JUST SOUTH OF			
				THE INTERSECTION. (1st-order)			
				ELEVATION = 4968.392 FT. (SLD 1929)			