

AS-BUILT BLOCK INFORMATION

Contractor: TLC Company, Inc.

Work Staked by: Huitt Zollars

Acceptance by: COA \Bill Woodward, PE \ Mike Sedillo

Field Verified by: COA \ Same as above

Drawings Corrected by:

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
PLANS FOR CONSTRUCTION

CANDELARIA ROAD MEDIANS
RAILROAD TO EDITH BLVD
PROJECT #751701



NOTES:

1. CONTRACTOR SHALL WORK CONTINUOUSLY, 24 HOURS PER DAY, ON ALL ARTERIAL ROADWAYS WHEN TRAFFIC LANES ARE CLOSED TO TRAFFIC UNLESS THE WORK VIOLATES THE CITY'S NOISE ORDINANCE.
2. IF THE CONTRACTOR IS NOT ALLOWED TO WORK AT NIGHT DUE TO THE CITY'S NOISE ORDINANCE, THE CONTRACTOR SHALL OPEN ALL TRAFFIC LANES TO TRAFFIC WITH THE PROPER USE OF TRENCH PLATES DURING NON-WORKING HOURS, AND MUST WORK MINIMUM HOURS FROM 9:00 A.M. TO 3:00 P.M. MONDAY THROUGH SATURDAY.
3. ARTERIAL STREETS ARE AS INDICATED IN THE "LONG RANGE ROADWAY SYSTEM" MAP PUBLISHED BY THE MID-REGION COUNCIL OF GOVERNMENTS (MRCOG).

UTILITY COMPANY CONTACTS

PNM-ELECTRIC

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DENVER, CO.
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ABCWUA

(WATER & SEWER)

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McLeodUSA

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QWEST LONG DISTANCE

LARRY KELLY
400 TIJERAS AVE. NW
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RED FLEX

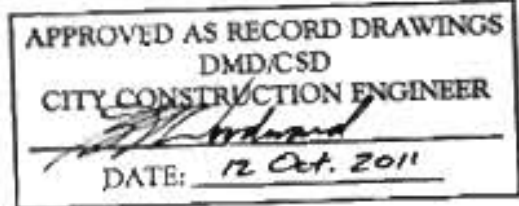
BRIAN KORITA
23751 N 23RD AVE.
PHOENIX, AZ 85085
(480) 226-7725



INDEX OF SHEETS

- 1 TITLE SHEET & GENERAL NOTES
- 2 SUMMARY OF QUANTITIES & MISC. SCHEDULES
- 3 SURVEY CONTROL SHEET
- 4 TYPICAL SECTIONS & MISC. DETAILS
- 5 PROPOSED MODIFICATIONS
- 6 SAN JUAN CHAMA WATERLINE NOTES

RECORD DRAWINGS



GENERAL NOTES

1. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION THROUGH UPDATE #7, INCLUDING AMENDMENT NO. 1, AND WILL BE REFERRED TO HEREIN AS "STANDARD SPECIFICATIONS".
 2. ALL CONSTRUCTION WITHIN CITY RIGHT-OF-WAY OR EASEMENTS MUST BE DONE FROM APPROVED WORK ORDER DOCUMENTS FROM THE CITY.
 3. ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, ORDINANCES, RULES, AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
 4. CONTRACTOR AGREES THAT HE SHALL ASSUME THE SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD HARMLESS THE OWNER AND ENGINEER FROM ANY AND ALL LIABILITY REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.
 5. ALL EXCAVATION, TRENCHING, AND SHORING ACTIVITIES MUST BE ACCOMPLISHED IN ACCORDANCE WITH OSHA 29CFR 1926.650 SUBPART P.
 6. AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN CITY RIGHT-OF-WAY.
 7. CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. ONLY THE CITY SURVEYOR SHALL REPLACE SURVEY MONUMENTS. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO STANDARD SPECIFICATIONS SECTION 4.4.
 8. SEVEN (7) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, CONTRACTOR SHALL SUBMIT TO DMD, CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE DMD, CONSTRUCTION COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (924-3400) PRIOR TO OCCUPYING AN INTERSECTION. REFER TO SECTION 19 OF STANDARD SPECIFICATIONS.
- PERMIT REQUESTS MAY BE DENIED OR DELAYED DUE TO CONFLICTS WITH OTHER PROJECTS IN THE AREA.
9. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
 10. CONTRACTOR SHALL ASSIST THE ENGINEER/INSPECTOR IN THE RECORDING OF DATA ON ALL UTILITY LINES AND ACCESSORIES AS REQUIRED BY THE CITY OF ALBUQUERQUE FOR THE PREPARATION OF "AS CONSTRUCTED" DRAWINGS. CONTRACTOR SHALL NOT COVER UTILITY LINES AND ACCESSORIES UNTIL ALL DATA HAS BEEN RECORDED.
 11. AT HIS OWN EXPENSE, CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENT, PAVEMENT MARKINGS, CURB AND GUTTER, WHEELCHAIR RAMPS, AND SIDEWALK DURING CONSTRUCTION APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS AND SHALL REPAIR OR REPLACE, PER STANDARD SPECIFICATIONS.
 12. ALL STREET STRIPING, ALTERED OR DESTROYED, SHALL BE REPLACED WITH THERMOPLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR TO SAME LOCATION AS EXISTING, OR AS INDICATED BY THIS PLAN SET. CONTRACTOR SHALL COORDINATE WITH CITY TRAFFIC OPERATIONS.
 13. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.
 14. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AND MAINTAIN ALL CONSTRUCTION SIGNING UNTIL THE PROJECT HAS BEEN ACCEPTED BY THE CITY.
 15. EXISTING UTILITY LINE LOCATIONS ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. THE LOCATION OF ANY SUCH EXISTING LINES IS BASED UPON INFORMATION PROVIDED BY THE UTILITY COMPANY, THE OWNER, OR BY OTHERS, AND THE INFORMATION MAY BE INCOMPLETE OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES.
 16. THE ENGINEER HAS UNDERTAKEN NO FIELD VERIFICATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UNDERGROUND UTILITY LINES, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY, AND PRESERVE ANY AND ALL EXISTING UTILITIES.
 17. REMOVALS SHALL BE DISPOSED OF OFF-SITE AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 18. WHEN ABUTTING EXISTING PAVEMENT TO NEW, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER. REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED.
 19. REMOVAL OF EXISTING CURB AND GUTTER OR SIDEWALK SHALL BE TO THE NEAREST JOINT OR SAW CUT.



REV.	SHEETS	CITY ENGINEER	DATE	USER DEPARTMENT	DATE	USER DEPARTMENT	DATE
ENGINEER'S STAMP & SIGNATURE		APPROVED	ENGINEER	DATE	APPROVED FOR CONSTRUCTION		
		DRC Chairman	<i>A. Woodall</i>	10-20-10	<i>Richard Port</i> 10-20-10 CITY ENGINEER DATE		
		Transportation	<i>P. Z...</i>	9-10-10			
		ABCWUA	<i>...</i>	9-10-10			
		Hydrology	<i>...</i>	9-13-10			
		CIP					
		AMAFCA					
		Constr. Coord.					
PROJECT NUMBER		ZONE ATLAS NO.		DRAWING NO. 1 OF 6			
751701		G-15					

Candelaria Road Medians - Railroad to Edith Boulevard

Summary of Quantities

DETECTABLE WARNING SURFACE

STATION	LOCATION/DESCRIPTION
12+00	NE Corner of Commercial & Candelaria
16+68	NE Corner of Armo & Candelaria

STRIPING SCHEDULE

STRIPING SCHEDULE		4" DASHED WHITE (LF) (10' STRIPE, 30' GAP)	4" SOLID WHITE (LF)	YELLOW MEDIAN NOSES (LF) *	REMOVE EX. STRIPING (LF)
9+07 TO 22+00		1293			1293
9+07 TO 22+00		1293			1293
10+86 TO 11+36			50	5	
12+15 TO 12+65			50	5	
15+62 TO 16+12			50	5	
16+97.5 TO 17+47.50			50	5	
19+50 TO 20+00			50	5	
20+78 TO 21+28			50	5	
	SUBTOTAL	2586	300	30	2586
	ADJUSTED	646.5 LF			

* THIS COST IS INCIDENTAL TO THE PAVEMENT STRIPING ITEM NO. 441.001

CURB AND GUTTER, MEDIAN

ITEM NO. 340.06

STATION	LOCATION/DESCRIPTION	UNITS	QUANTITY
10+00 TO 11+37.26	WEST OF COMMERCIAL ST.	L.F.	276
12+15.21 TO 16+13	BTWN COMMERCIAL ST AND ARNO ST.	L.F.	800
16+97 TO 19+97	BTWN ARNO ST AND FRANCISCAN ST.	L.F.	604
20+77.5 TO 22+22.15	BTWN FRANCISCAN ST AND EDITH BLVD	L.F.	213
TOTAL			1893.00
PROJECT TOTAL			1900

CURB AND GUTTER, REMOVE & DISPOSE

ITEM NO. 340.08

STATION	LOCATION/DESCRIPTION	UNITS	QUANTITY
10+00	WEST OF COMMERCIAL ST.	L.F.	12
20+69 TO 22+00	BTWN FRANCISCAN ST AND EDITH BLVD	L.F.	230
TOTAL			242.00
PROJECT TOTAL			245.00

ITEM NO.	DESCRIPTION	UNIT	ESTIMATE
	<u>EARTHWORK</u>		
205.01	FILL, BORROW, HAUL & COMPACT	C.Y.	270
	<u>PAVING</u>		
301.02	SUBGRADE PREP, 6"	S.Y.	150
302.01	AGGREGATE BASE COURSE, 6"	S.Y.	150
336.12	TACK COAT	S.Y.	300
336.02	ASPHALT CONCRETE, SUPERPAVE, 2-1/2"	S.Y.	150
336.03	ASPHALT CONCRETE, SUPERPAVE, 3" (2 LIFTS)	S.Y.	300
340.06	CURB & GUTTER, MEDIAN, PCC	L.F.	1,900
343.03	ASPHALT CONCRETE PAVEMENT, >4", SAWCUT, REMOVE AND DISPOSE	S.Y.	620
343.04	EXISTING MEDIAN PAVEMENT, UP TO 4" THICK, SAWCUT, REMOVE & DISPOSE	S.Y.	60
343.08	CURB & GUTTER, PCC, REMOVE AND DISPOSE	L.F.	245
346.11	4" MEDIAN PAVEMENT, COLORED PC CONCRETE	S.F.	1,832
441.001	REFLECTORIZED PLASTIC PAVEMENT MARKINGS, 4"	L.F.	947
443.101	REMOVE PAVEMENT STRIPE	L.F.	2,600
	<u>MISCELLANEOUS</u>		
XXXX	DETECTABLE WARNING SURFACE	S.F.	29
19.01	CONSTRUCTION TRAFFIC CONTROL AND BARRICADING	L.S.	1

ASPHALT PAVEMENT, REMOVE & DISPOSE

ITEM NO. 343.03


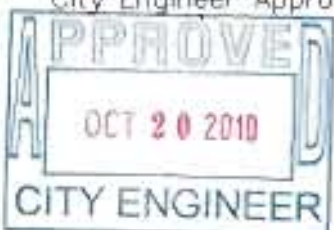
STATION	LOCATION/DESCRIPTION	UNITS	QUANTITY
10+00 TO 11+37.26	WEST OF COMMERCIAL ST.	S. Y.	57
12+15.21 TO 16+13	BTWN COMMERCIAL ST AND ARNO ST.	S. Y.	344
16+97 TO 19+97	BTWN ARNO ST AND FRANCISCAN ST.	S. Y.	198
20+77.5 TO 22+00	BTWN FRANCISCAN ST AND EDITH BLVD	S. Y.	19
		TOTAL	618
	PROJECT TOTAL		629

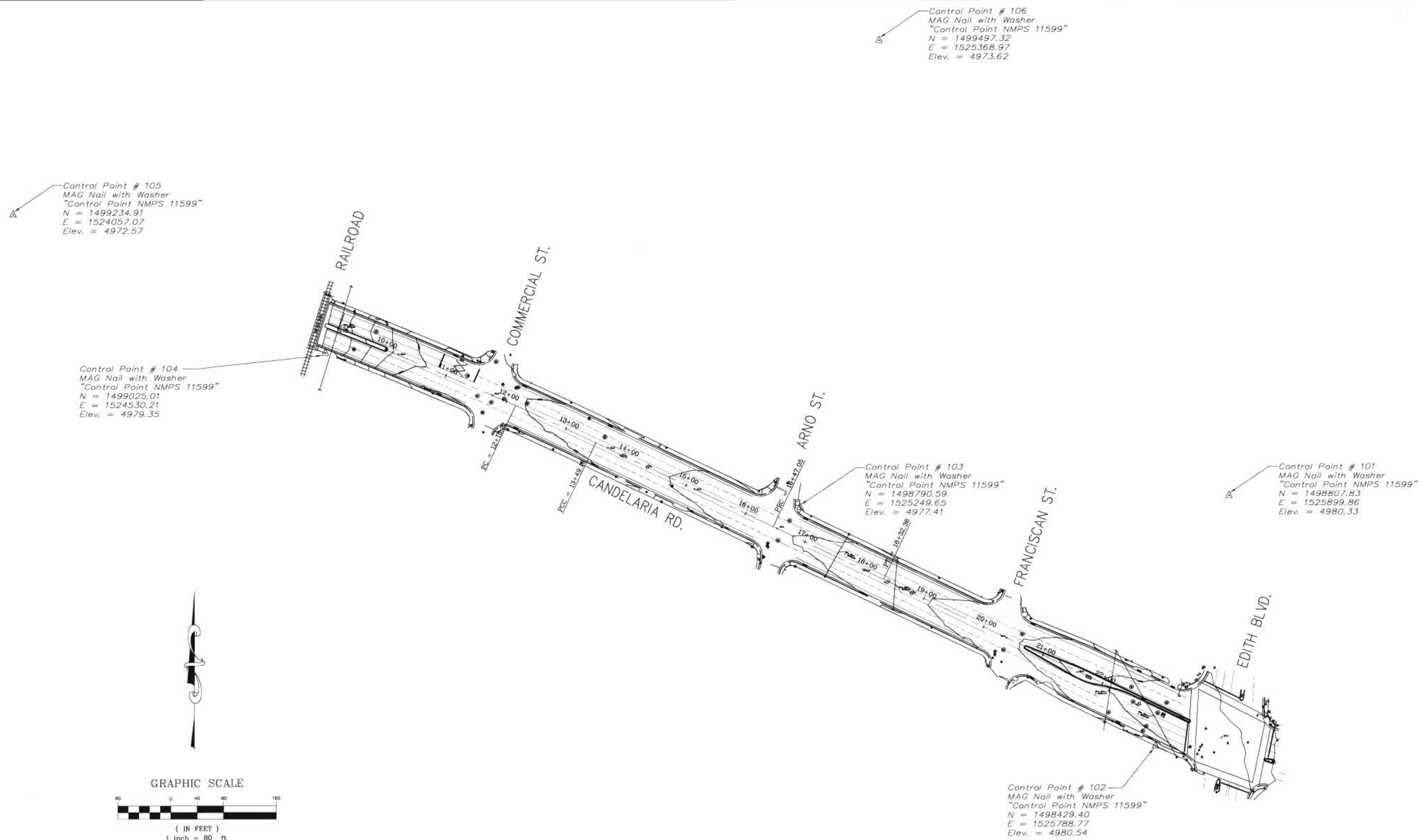
LANDSCAPE AREAS

STATION	LOCATION/DESCRIPTION	UNITS	QUANTITY
9+00 TO 10+40	BTWN RAILROAD AND COMMERCIAL ST.	S.F.	777
13+04 TO 15+23	BTWN COMMERCIAL ST AND ARNO ST.	S.F.	2339
17+76 TO 19+08	BTWN ARNO ST AND FRANCISCAN ST.	S.F.	1230
		TOTAL	4346


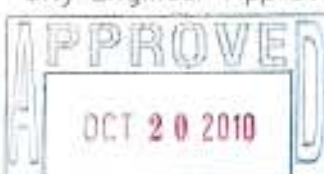
FILL FOR THE NEW LANDSCAPE AREAS WILL BE PAID FOR UNDER ITEM 205.01. REMOVAL OF ASPHALT AND DEBRIS FOR THE LANDSCAPE AREAS WILL BE PAID FOR UNDER ITEM 343.03 OR ITEM 343.04.

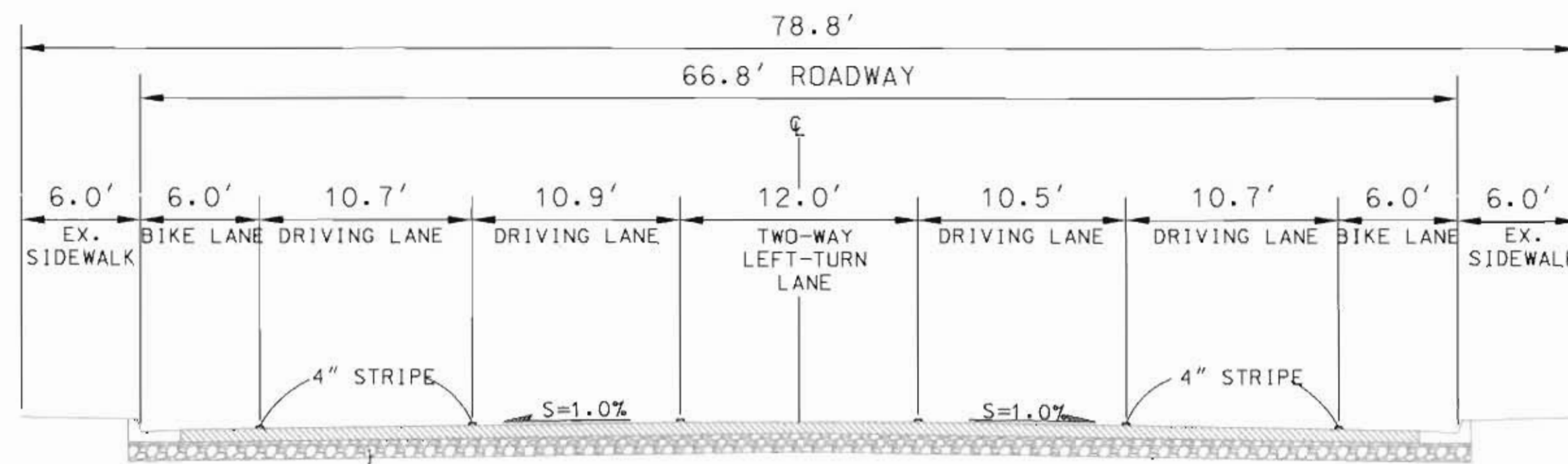


CITY OF ALBUQUERQUE DEPARTMENT OF MUNICIPAL DEVELOPMENT ENGINEERING DIVISION															
TITLE:		CANDELARIA ROAD MEDIANS RAILROAD TO EDITH BLVD SUMMARY OF QUANTITIES AND MISC. SCHEDULES													
Design Review Committee	City Engineer Approval	Lost Design update	<table border="1"> <tr> <td>Mo. / Day / Yr.</td> <td>Mo. / Day / Yr.</td> </tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>	Mo. / Day / Yr.	Mo. / Day / Yr.										
Mo. / Day / Yr.	Mo. / Day / Yr.														
		<table border="1"> <tr> <td>City Project No.</td> <td>Zone Map No.</td> <td>Sheet</td> <td>Of</td> </tr> <tr> <td>751701</td> <td>G-15</td> <td>2</td> <td>6</td> </tr> </table>		City Project No.	Zone Map No.	Sheet	Of	751701	G-15	2	6				
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751701	G-15	2	6												



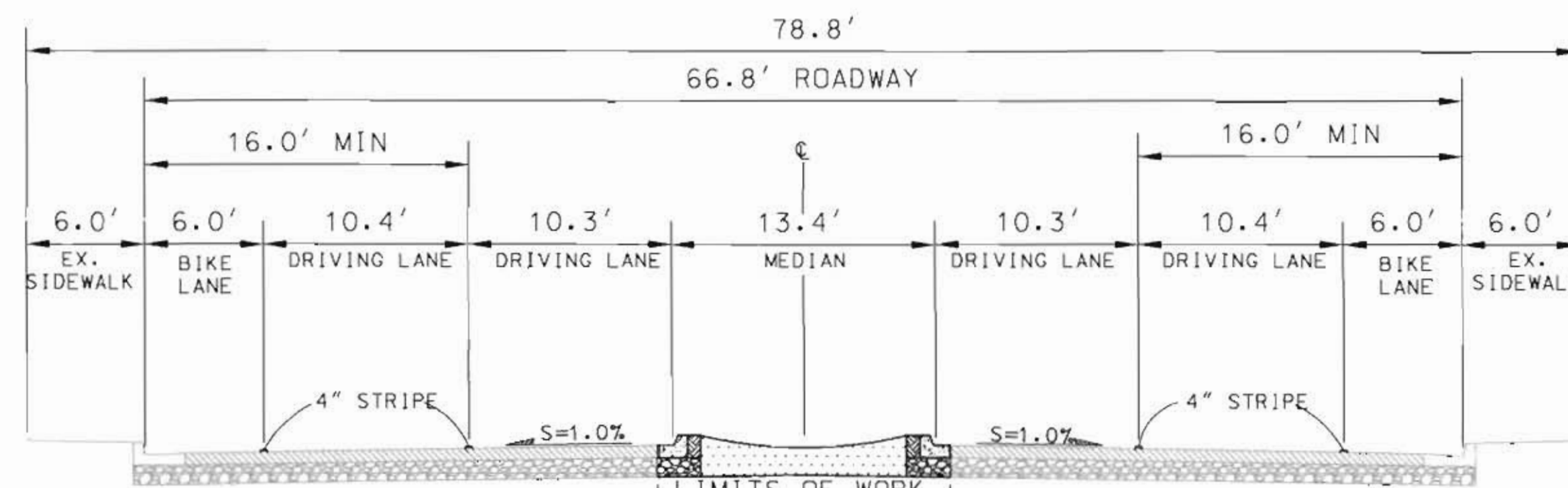
		<i>ENGINEER'S SEAL</i>		<i>SURVEY INFORMATION</i>		<i>BENCH MARKS</i>		<i>AS BUILT INFORMATION</i>	
				FIELD NOTES				CONTRACTOR	
				NO.	BY	DATE			WORKED BY
							NGS STAINLESS ROD SET BENEATH A 5 1/2"		INSPECTOR'S NAME
							ACCESS COVER STAMPED "B-438, 1984".		DATE
							NE QUADRANT OF CANDELARIA BLVD. NE		DATE
							AND THE BNSF RAILROAD TRACKS, 42.2 FT.		DATE
							EAST OF CENTERLINE OF THE TRACKS, 77.7		DATE
							FT. NORTH OF CENTERLINE OF CANDELARIA		<i>MICRO-FILM INFORMATION</i>
							BLVD., AND 1 FT. WEST OF A CHAIN		RECORDED BY
							LINK FENCE. DATUM NAVD 1988		N.O.
							ELEV. 4975.610		

CITY OF ALBUQUERQUE DEPARTMENT OF MUNICIPAL DEVELOPMENT ENGINEERING DIVISION				
TITLE:		CANDELARIA ROAD MEDIANS RAILROAD TO EDITH BLVD SURVEY CONTROL SHEET		
Design Review Committee	City Engineer Approval	Ms. / Day / Yr.	Ms. / Day / Yr.	
 <p>APPROVED OCT 20 2010 DESIGN REVIEW COMMITTEE</p>	 <p>APPROVED OCT 20 2010 CITY ENGINEER</p>	<div> <div>Ms. / Day / Yr.</div> <div></div> </div> <div> <div>Ms. / Day / Yr.</div> <div></div> </div> <div> <div>Ms. / Day / Yr.</div> <div></div> </div> <div> <div>Ms. / Day / Yr.</div> <div></div> </div>		
City Project No.	751701	Zone Map No.	Sheet	Of
		G-15	3	6

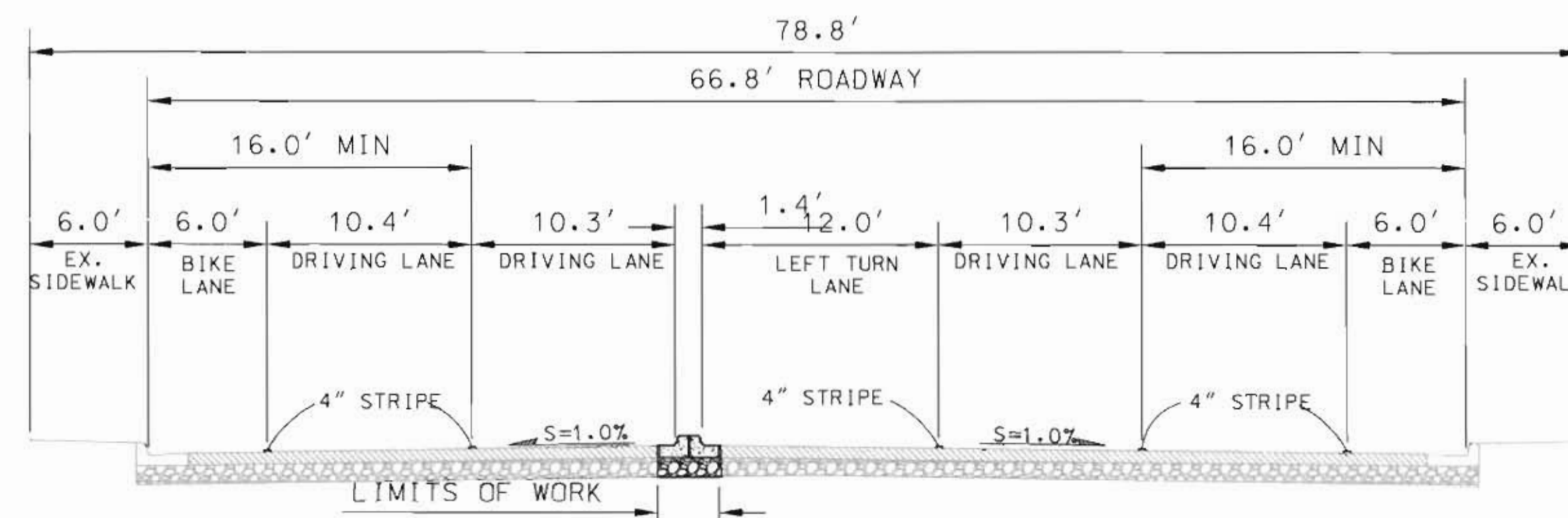


EXISTING PAVEMENT
SHOWN FOR INFORMATION ONLY

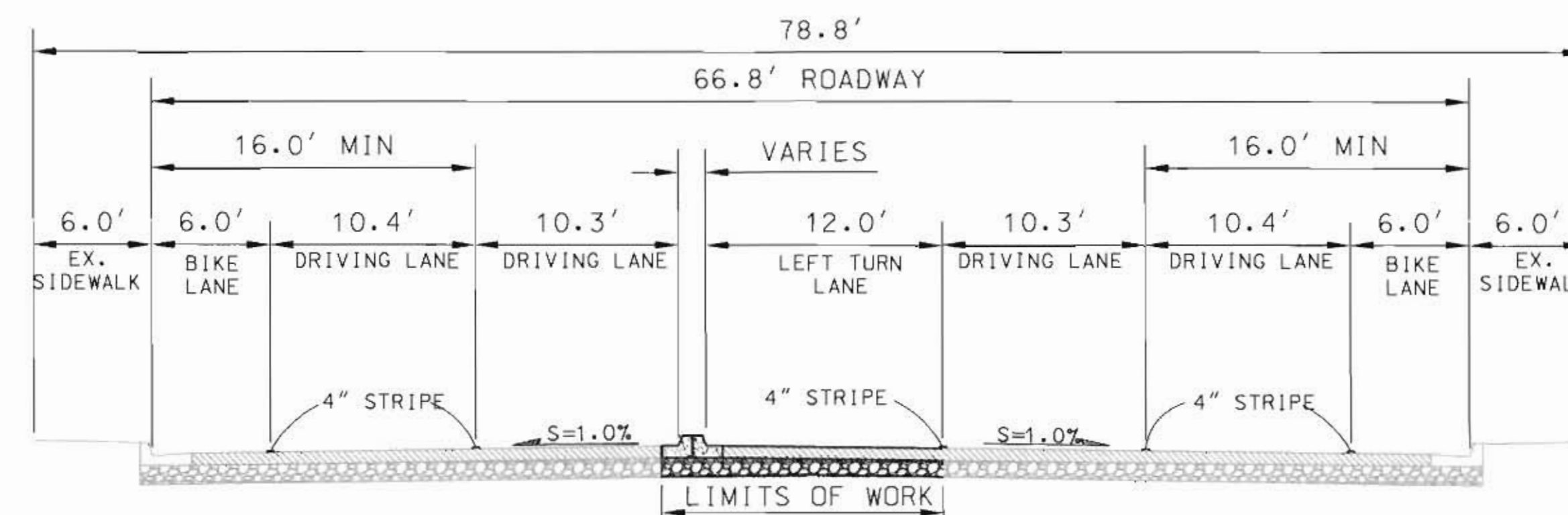
EXISTING TYPICAL SECTION
CANDELARIA BETWEEN RAILROAD
TRACKS AND EDITH BLVD



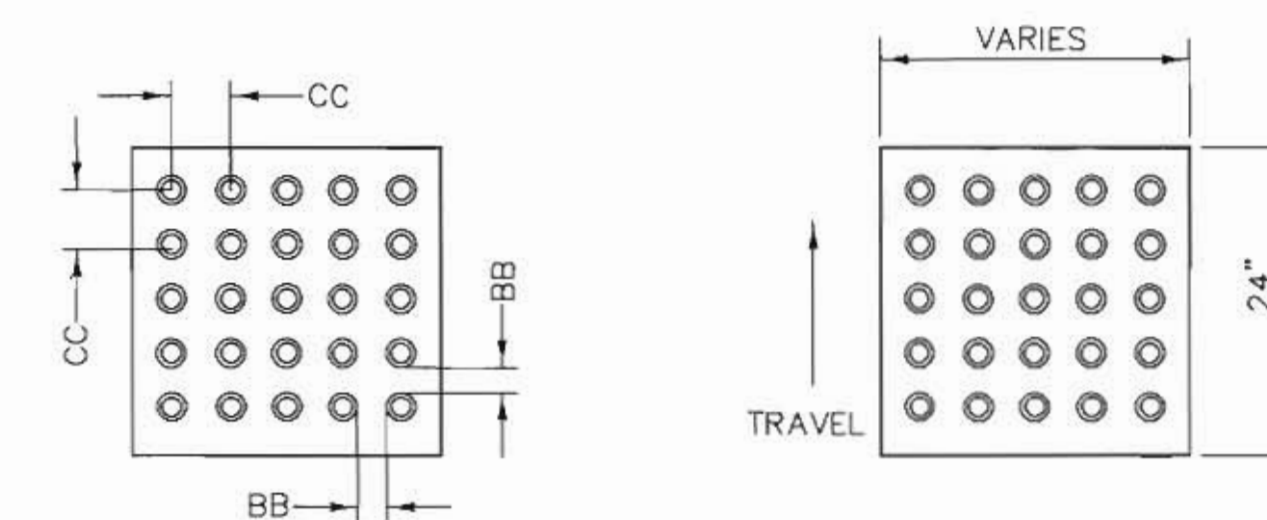
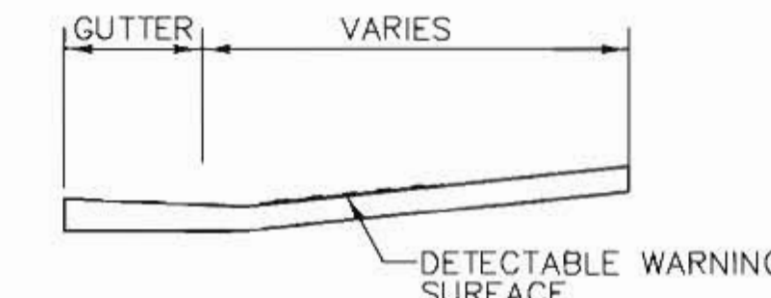
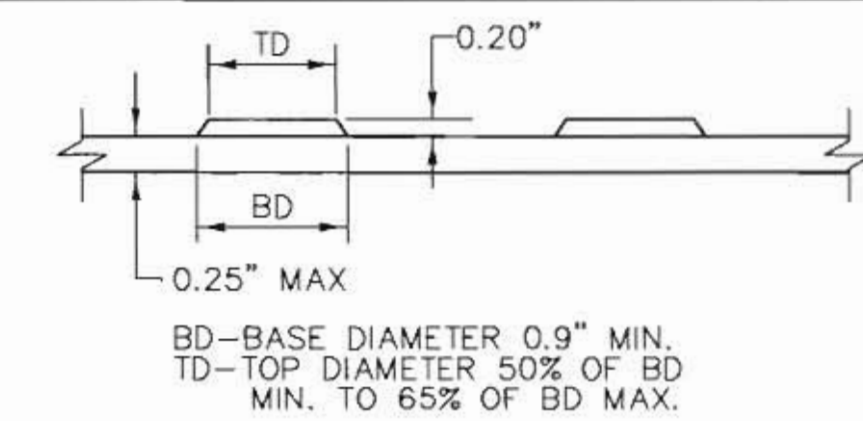
PROPOSED TYPICAL SECTION
AT FULL-WIDTH MEDIAN



PROPOSED TYPICAL SECTION
AT LEFT TURN LANE



PROPOSED TYPICAL SECTION
AT MEDIAN REMOVAL



CC-CENTER TO CENTER SPACING
2.35"
BB-BASE TO BASE SPACING
1.48" MIN.

1. DESIGN SHALL BE PER ADA GUIDELINE
2. SUBMIT SPECS TO COA CONSTRUCTION ENGINEER FOR EVALUATION PRIOR TO CONSTRUCTION

DETECTABLE WARNING SURFACE

SCALE: NONE TO BE USED AT EXIST. CURB RAMPS
AT STA. 12+00 LT. & 16+68 LT.

DEFINITIONS:

DETECTABLE WARNING SURFACE: A SURFACE FEATURE BUILT IN OR APPLIED TO WALKING SURFACES OR OTHER ELEMENTS TO WARN OF HAZARDS ON A CIRCULATION PATH TO AID PERSONS WITH VISUAL IMPAIRMENTS.

CURB LINE: A LINE AT THE FACE OF THE CURB THAT MARKS THE TRANSITION BETWEEN THE SIDEWALK AND THE GUTTER OF THE ROADWAY.

LOCATION:

- 1.) DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE A CURB RAMP OR LANDING CONNECTS TO A CROSSWALK AND/OR PEDESTRIAN ROUTE CROSSING A ROADWAY.
- 2.) DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6" MIN. AND 8" MAX. FROM THE CURB LINE.
- 3.) MEDIAN AND REFUGE ISLANDS SHALL HAVE A DETECTABLE WARNINGS. DETECTABLE WARNING AT CUT THROUGH ISLANDS SHALL BE SEPARATED BY A 24" MIN. LENGTH OF WALKWAY WITHOUT WARNINGS.

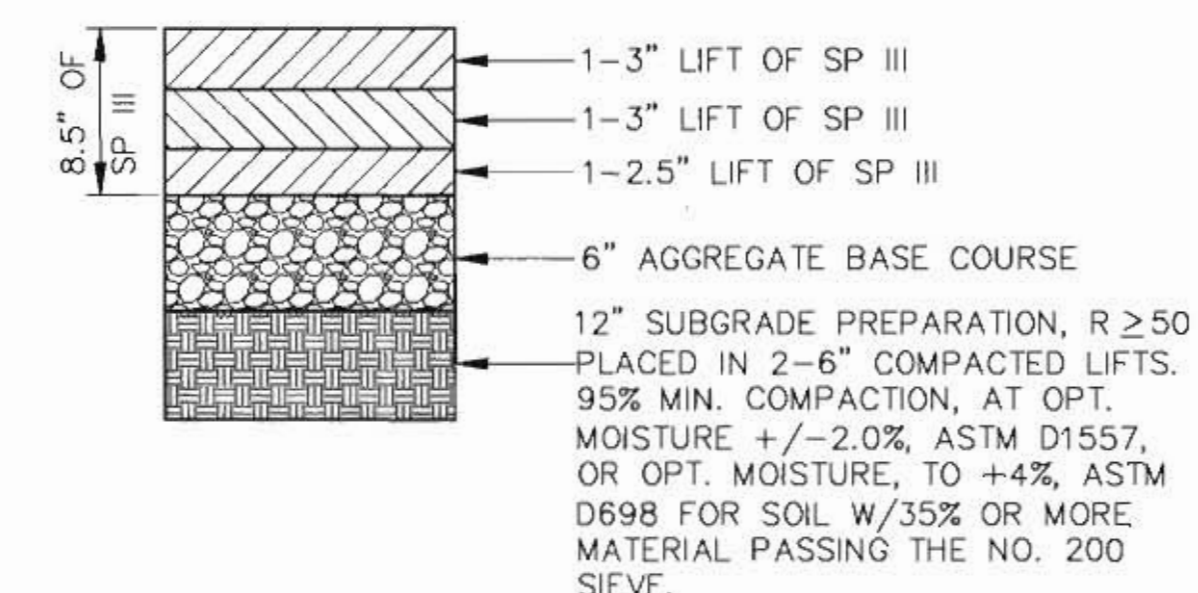
EXCEPTION:

DETECTABLE WARNINGS SHALL NOT BE REQUIRED ON CUT THROUGH ISLANDS WHERE THE CROSSINGS ARE CONTROLLED BY SIGNALS AND ARE TIMED FOR FULL CROSSING ON MEDIANS LESS THAN 7 FT.

NOTES:

- 1.) DETAILS SPECIFIED ON THIS PLAN APPLY TO ALL CONSTRUCTION OR RECONSTRUCTION OF STREETS, CURBS, OR SIDEWALKS BY ALL PUBLIC AGENCIES AND BY ALL PRIVATE ORGANIZATIONS CONSTRUCTING FACILITIES FOR PUBLIC USE.
- 2.) SIDEWALK RAMPS ARE TO BE LOCATED AS SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 3.) THE TOP OF THE JOINT FILLER FOR ALL RAMP TYPES SHALL BE FLUSH WITH THE ADJACENT CONCRETE.
- 4.) ALL PRODUCTS USED FOR DETECTABLE WARNING SURFACES SHALL BE ON THE CITY OF ALBUQUERQUE'S APPROVED PRODUCTS LIST.


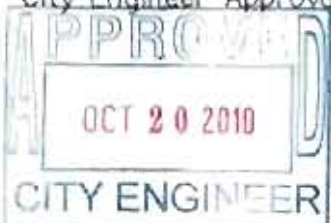
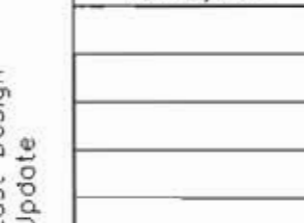
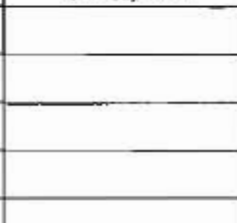
DOME ALIGNMENT:
 DETECTABLE WARNING SURFACES SHALL EXTEND 24" MIN. IN THE
 DIRECTION OF TRAVEL, AND FULL WIDTH OF THE CURB RAMP, LANDING,
 OR TRANSITION. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE
 PREDOMINANT DIRECTIONS OF THE CROSSWALK TO PERMIT WHEELS
 TO ROLL BETWEEN DOMES.

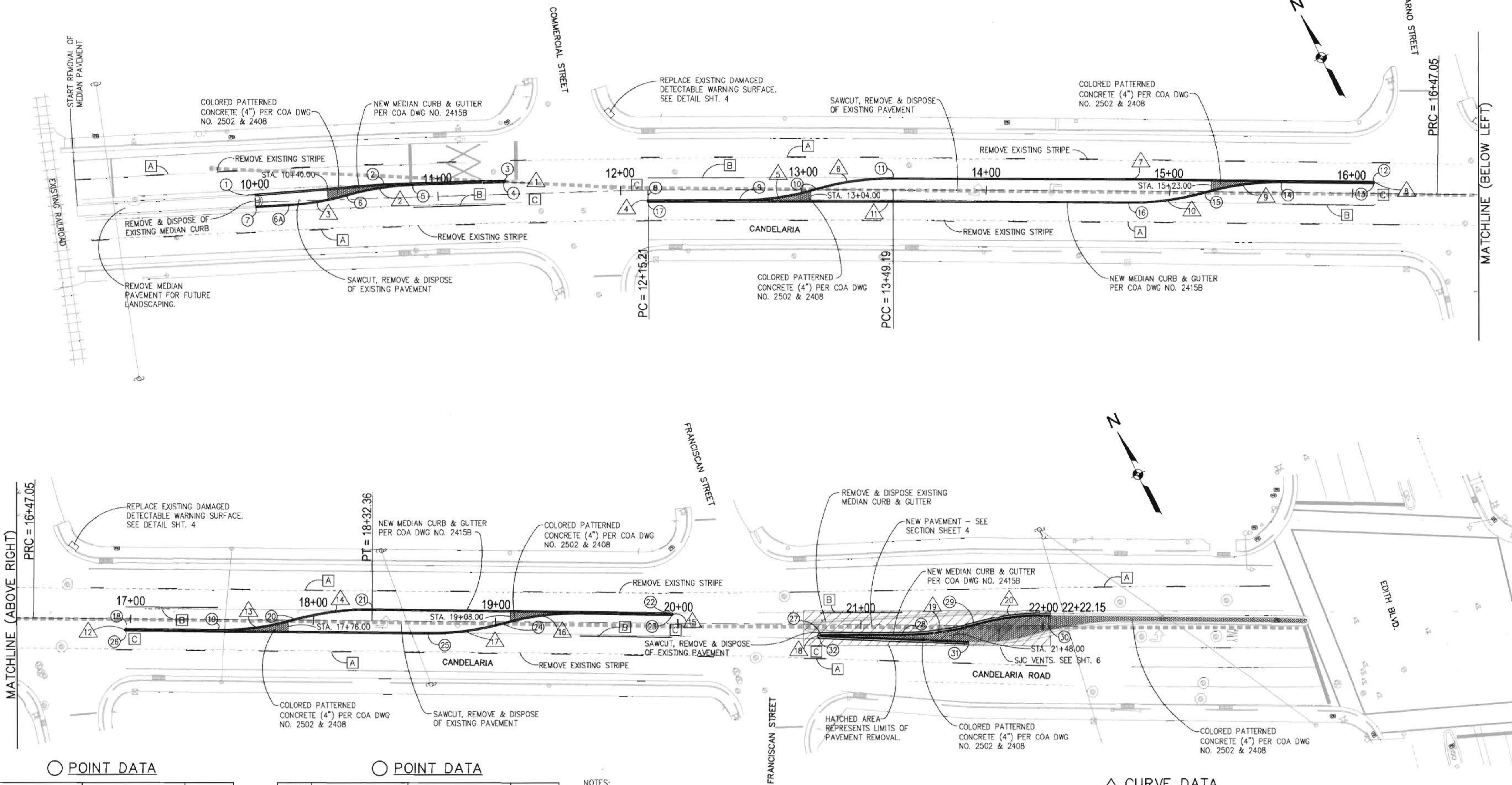


DETAIL-NEW PAVEMENT SECTION

NOTE: PAVEMENT SECTION MATCHES
CITY PROJECT #3203.

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS BUILT INFORMATION	
		FIELD NOTES		NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "B-438, 1984", NE QUADRANT OF CANDELARIA BLVD. NE AND THE BNSF RAILROAD TRACKS, 42.2 FT. EAST OF CENTERLINE OF THE TRACKS, 77.7 FT. NORTH OF CENTERLINE OF CANDELARIA BLVD., AND 1 FT. WEST OF A CHAIN LINK FENCE. DATUM NAVD 1988 ELEV. 4975.610		CONTRACTOR	
		NO.	BY			DATE	DATE
						WORKED BY	DATE
						INSPECTOR'S ACCEPTANCE BY	DATE
						FIELD VERIFICATION BY	DATE
						DRAWINGS CORRECTED BY	DATE
		NO. DATE REMARKS REVISIONS DESIGN		MICRO-FILM INFORMATION			
				DESIGNED BY SL DATE 9/2010			
				DRAWN BY BN DATE 9/2010			
CHECKED BY KA		DATE 9/2010		RECORDED BY NO. DATE			

CITY OF ALBUQUERQUE DEPARTMENT OF MUNICIPAL DEVELOPMENT ENGINEERING DIVISION			
TITLE: CANDELARIA ROAD MEDIANS RAILROAD TO EDITH BLVD TYPICAL SECTIONS & MISC. DETAILS			
Design Review Committee 	City Engineer Approval 	Lost Design Update 	
City Project No. 751701	Zone Map No. C-15	Sheet 4	Of 6



○ POINT DATA

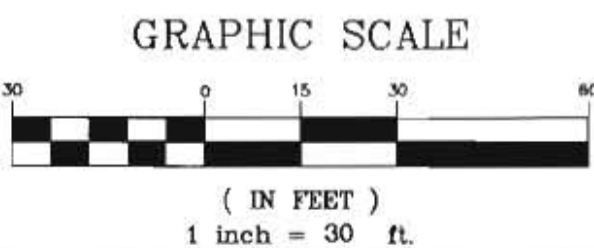
POINT NO.	X	Y	ELEV.
1	1524621.8400	1499034.7200	76.33
2	1524688.1872	1499009.9955	75.43
3	1524748.9078	1498983.9873	74.53
4	1524748.3803	1498982.7663	74.53
5	1524702.4190	1499002.4528	74.92
6	1524667.2369	1499012.6561	75.22
6A	1524636.0405	1499021.2197	75.97
7	1524619.3700	1499027.8000	76.39
8	1524815.9956	1498941.1270	75.05
9	1524861.3731	1498920.1066	75.12
10	1524901.7475	1498907.8973	75.30
11	1524942.5417	1498895.4924	75.55
12	1525180.2895	1498782.7947	76.54
13	1525179.7139	1498781.5957	76.48
14	1525179.7139	1498803.1894	76.51
15	1525093.9157	1498815.9107	76.28
16	1525053.6206	1498828.4387	76.08
17	1524815.4335	1498939.9106	75.02
18	1525252.0633	1498735.1381	77.25
19	1525297.1960	1498713.6198	77.28
20	1525337.7983	1498700.9428	77.45

○ POINT DATA

POINT NO.	X	Y	ELEV.
21	1525378.9613	1498687.9969	77.55
22	1525526.6116	1498616.8477	78.40
23	1525526.0463	1498615.6437	78.38
24	1525440.6661	1498650.1626	77.98
25	1525400.9019	1498733.9376	77.74
26	1525251.4909	1498733.9376	77.23
27	1525595.4813	1498571.6801	79.28
28	1525636.0612	1498552.9633	79.45
29	1525673.7808	1498541.2889	80.07
30	1525713.7040	1498528.5909	80.07
31	1525667.2127	1498533.7779	79.57
32	1525594.9502	1498570.6389	79.28

- NOTES:
- SEE SHEET 4 FOR TYPICAL SECTIONS AND DETAILS.
 - ALL CONFLICTING STRIPING SHALL BE ERADICATED BY WATER BLASTING OR SURFACE PLANING OF THE ROADWAY TO A MAXIMUM DEPTH OF .25 INCHES AND A WIDTH TO EQUAL 2 TIMES THE WIDTH OF THE STRIPE OR MARKING TO BE REMOVED. REFER TO SPEC SECTION 443.
 - ALL STRIPING SHALL CONFORM TO THE CURRENT EDITION OF THE M.U.T.C.D.
 - ALL STRIPING TO BE REFLECTIVE PREFORMED THERMOPLASTIC TAPE.
 - LANDSCAPING IN THE MEDIANS WILL BE PROVIDED BY OTHERS.
 - ELEVATIONS ARE FLOWLINE OF CURB, UNLESS NOTED OTHERWISE.
 - CONTRACTOR TO REMOVE DEBRIS (ROCK OVER 3", ASPHALT MILLINGS, CONCRETE, TRASH, WEEDS, AND ROOTS) TO A DEPTH OF 8" BELOW FINISH GRADE OR AS NECESSARY TO REMOVE DEBRIS. EXISTING CLEAN DIRT CAN REMAIN. CLEAN DIRT TO BE ADDED OR REMOVED AS NECESSARY TO ACHIEVE FINISH GRADE. CURB MUST BE SUPPORTED. FINISH GRADE TO BE 4" BELOW TOP OF CURB. THIS NOTE ONLY APPLIES TO THE 3 LANDSCAPING AREAS.

- LEGEND
- A 4" WHITE STRIPE (10' STRIPE 30" GAP)
 - B 4" SOLID WHITE STRIPE
 - C PAINT MEDIAN NOSE YELLOW (RETROREFLECTIVE)
 - LOCATION OF EXISTING SAN JUAN CHAMA WATERLINE, AS SHOWN ON AS-BUILT DRAWINGS (CPN 681005)



△ CURVE DATA

CURVE NO.	RADIUS	TANGENT LENGTH	ARC LENGTH
1	0.66'	--	2.07'
2	150.00'	18.45'	36.72'
3	150.00'	16.27'	32.41'
4	0.67'	--	2.11'
5	150.00'	21.30'	42.32'
6	150.00'	21.54'	42.78'
7	27197.39'	13155'	263.11'
8	0.67'	--	2.09'
9	150.00'	21.54'	42.79'
10	150.00'	21.31'	42.34'
11	27148.06'	131.49'	262.98'
12	0.67'	--	2.09'
13	150.00'	21.48'	42.68'
14	150.00'	21.80'	43.30'
15	0.67'	--	2.10'
16	150.00'	21.37'	42.45'
17	150.00'	21.05'	41.83'
18	0.58'	--	1.84'
19	150.00'	19.92'	39.60'
20	150.00'	21.20'	42.12'



CITY OF ALBUQUERQUE DEPARTMENT OF MUNICIPAL DEVELOPMENT ENGINEERING DIVISION			
TITLE: CANELARIA ROAD MEDIANS RAILROAD TO EDITH BLVD PROPOSED MODIFICATIONS			
Design Review Committee	City Engineer Approval	Last Design Update	
City Project No.	Zone Map No.	Sheet	Of
751701	G-15	5	6

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS BUILT INFORMATION	
		NO.	BY	DATE	NGS STAINLESS ROD SET BENEATH A 5 1/2"	CONTRACTOR	DATE
					ACCESS COVER STAMPED "B-438, 1984"	WORK STARTED BY	DATE
					NE QUADRANT OF CANELARIA BLVD. NE	ACCEPTANCE BY	DATE
					AND THE BNSF RAILROAD TRACKS, 42.2 FT.	FIELD CORRECTION BY	DATE
					EAST OF CENTERLINE OF THE TRACKS, 77.7	DRAWINGS CORRECTED BY	DATE
					FT. NORTH OF CENTERLINE OF CANELARIA	MICRO-FILM INFORMATION	
					BLVD., AND 1 FT. WEST OF A CHAIN	RECORDED BY	DATE
					LINK FENCE. DATUM NAVD 1988	NO.	
					ELEV. 4975.610		

The San Juan Chama (SJC) transmissions lines are critical to the function of the surface water treatment plant. Due to the lines' sizes and configuration, protection of the lines from damage or events that can interrupt their serviceability is paramount. The SJC transmission system does not possess the same degree of redundancy as the existing production and distribution system so any disruptions can have severe impacts on the surface water treatment plant operations and long-term water supply strategy. The SJC transmission lines vary in size from 30-inches up to 72 inches in diameter. They are the largest lines in the Albuquerque Metro water service area. Ground cover over the tops of these lines vary from 4 feet up to 13 feet or more. They can be the same depth as other water lines in the system or much deeper.



Designers, engineers, and construction contractors are required to take additional precautions when work is planned around any of these lines to minimize any potential for damage. Be aware of the following:

1. Use the most current "as built" information. Verify that the presence of the lines and appurtenances (valve vaults, air relief locations, corrosion protection, etc.) are clearly shown on all designs/utility plans. Heightened awareness can prevent poor planning and avoid negative outcomes (damage, schedule disruptions, litigation, etc.)
2. Not all line segments are constructed in the public right-of-way or well established streets. Some segments are constructed in undeveloped or other areas and these may be even more vulnerable since their presence may not be readily apparent.
3. Do NOT rely on the One-Call system for line locating requests. Problems with the One-Call system include: poor or vague location descriptions from the requestor, incomplete plans which do not show all the buried utilities, highly variable depths of cover, etc. If the presence of a large transmission line is suspected, specifically mark on the One-Call request: "San Juan Chama transmission line in the area; please have line locator contact and schedule field meeting with the Requestor". Allow extra time during planning and construction for the extra steps necessary to minimize any risk to these lines. Do not wait until the construction schedule is up against a 48 hour notice or use the "emergency" 2-hour response line location request. Such "short cuts" usually result in significant problems.
4. Use of boring tools/technology around these lines can be especially risky. The large pipe diameters and varying depths of cover make determination of the elevation dimension of the planned bore critical along with being able to carefully control the bore's progress. Designers and engineers should plan on using survey techniques and the as-built information to determine all coordinates and mitigate any potential three-dimensional conflicts. The ABCWUA does not perform surveying services and its existing line locating equipment may not be able to definitively determine location due to depth of cover and large diameter of some of the pipes. Again, do NOT rely on the One-Call system to solve this.
5. If any of these lines need to be taken out of service for either planned or emergency situations, the operational and engineering coordination, draining, repair, re-filling, etc. could take 10-30 days (or even longer), depending on the circumstances. These could be significant disruptions to work schedules. In planning to take a line out of service, allow for a minimum of 30 days into the scheduling. Emergency repairs could also take weeks so avoidance of these damages is in everyone's best interest.
6. Any damage to the SJC transmission lines has the potential for causing significant collateral damage, including loss of life, severe property damage, loss of other infrastructure, loss of business, short-and/or long-term water shortages throughout the Albuquerque Metro service area, etc. which could result in stiff penalties, astronomical claims and possible criminal prosecution for negligence.

7. In the event of damage to or a break of a SJC transmission line, Control Center Control should be contacted immediately at 342-3001 or 342-3002. Do **NOT** attempt to do anything until a Control Center or a Plant/Field Superintendent of the ABCWUA provides direction/instruction.
8. The large valve vaults on the SJC lines are confined spaces. Anyone entering these facilities is doing so at his/her own risk. Confined space entry involves many safety precautions including, but not limited to: air monitoring, active ventilation, buddy system, harnesses, etc. All confined space entry must be in compliance with OSHA requirements. The ABCWUA will not be held responsible or liable for contractors, engineers, and other non-ABCWUA persons that enter these vaults.
9. An ABCWUA Field Representative will need to be present at the jobsite during excavation around any SJC transmission lines.

ENGINEER'S SEAL		SURVEY INFORMATION		BENCH MARKS		AS BUILT INFORMATION	
		FIELD NOTES		NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "B-438, 1984".		CONTRACTOR	
		NO.	BY	DATE	NO.	WORKED BY	DATE
						INSPECTED BY	DATE
						ACCEPTANCE BY	DATE
						VERIFICATION BY	DATE
REMARKS		NO.		DRAWINGS		MICRO-FILM INFORMATION	
DESIGN				CORRECTED BY		RECORDED BY	
DESIGNED BY	SL	DATE	8/2010	FT. NORTH OF CENTERLINE OF CANDELARIA BLVD., AND 1 FT. WEST OF A CHAIN LINK FENCE. DATUM NAVD 1988		NO.	
DRAWN BY	BN	DATE	8/2010				
CHECKED BY	KA	DATE	8/2010				



CITY OF ALBUQUERQUE DEPARTMENT OF MUNICIPAL DEVELOPMENT ENGINEERING DIVISION			
TITLE: CANDELARIA ROAD MEDIANS RAILROAD TO EDITH BLVD SAN JUAN CHAMA WATERLINE NOTES			
Design Review Committee	City Engineer Approval	Mo. / Day / Yr.	Mo. / Day / Yr.
			
City Project No.	Zone Map No.	Sheet	Of
751701	G-15	6	6