

SITE DEVELOPMENT PLAN FOR SUBDIVISION

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Project Summary

Minimum GSF with 1/200 Parking	777,792
Total Maximum Bldg GSF	905,842
Total Site Acreage	59.6
Total parking required Per CABQ Zoning Code	2,877
Total Parking Provided	3,159
Difference	282
% Difference	10%

Land Use Summary

Land Use	GSF
Office (*MIN**MAX GSF)	525,200 - 653,250
Retail	18,000 MAX.
Restaurant	9,000 MAX.
Hotel	225,592 MAX.
Total Bldg. GSF (*MIN**MAX)	777,792 - 905,842

* MINIMUM GSF BASED ON 1/200 SF PARKING
 ** MAXIMUM GSF BASED ON CABQ MINIMUM PARKING REQUIREMENTS

NOTE: RETAIL USES LIMITED TO THOSE FIRST LISTED AS PERMISSIVE IN THE C-3 ZONE AND THOSE LISTED PERMISSIVE IN THE IP INDUSTRIAL PARK ZONE.

Total parking required pursuant to the Zoning Code (CABQ Parking Regulations 14-16-3-1)

Business NLSF (First Floor)*	192,825	Parking Req'd (1/200)	964
Business NLSF (Second through Fourth Floors)*	395,100	Parking Req'd (1/300)	1,317
Hotel Keys (Rooms) Tract # 1	250	Parking Req'd (1/Key)	250
Hotel Keys (Rooms) Tract # 3	155	Parking Req'd (1/Key)	155
NLSF Retail (Tract #2)	16,200	Parking Req'd (1/200 & 1/250 NLSF)	80
Restaurant (Tract #4) Occupants	167	Parking Req'd (1/3 Occupants)	56
Restaurant (Tract #6) Occupants	164	Parking Req'd (1/3 Occupants)	55
Total Parking Required			2,877

* Net Leasable Square Footage (NLSF) is estimated to be 90% of GSF

- Notes:**
- Tract uses and areas indicated on this sheet are consistent with those used in the traffic impact study for the North I-25 Corporate Center site, dated March 30th 2009, and prepared by Terry Brown. Intensity of development is to be maintained in accordance with the TIS (traffic impact study). Any proposal substantially exceeding the TIS may require a TIS amendment. Any revisions to the TIS shall require consultation with transportation staff and/or the city traffic engineer.
 - Building heights are limited to 120' maximum and as further defined on sheet A-704 design standards.
 - Subject site is zoned SU-2/M-1. parking requirements are pursuant to the zoning code 14-16-3-1. See parking table this sheet.
 - Any variation of access point location which differs from what is shown in these drawings shall require consultation with and approval by the city traffic engineer.
 - Private, blanket cross lot access, drainage, and dry utility easements granted on and for the benefit of tracts 1-15 and tracts 18 & 19. See sheet VS-101.
 - Minor variations to parking totals per tract are allowed.

NORTH I-25 CORPORATE CENTER

LEGAL DESCRIPTION:
 Signetics Albuquerque facility as the same is shown and designated on the corrected amendment plat of land in section 12, T11N, R3E, N.M.P.M. blocks 6, 7 and 8, tract A, unit B, North Albuquerque Acres, filed in the office of the county clerk of Bernalillo county, New Mexico, on December 17, 1981, in plat book C19, folio 48, and containing 59.6622 acres, more or less.

LAND USE SUMMARY:

This SU2/M-1 (light manufacturing zone) property is intended to be developed as a business park with a mix of commercial and office uses. Design standards are provided to integrate the mix of uses by establishing an architectural style, unified landscaping throughout, pedestrian linkages, lighting and signage standards, and compliance with the north I-25 sector development plan. The proposed mixture of commercial uses on this site is intended to appeal to surrounding areas, as well as provide some on-site amenities in support of the on-site office uses. Although zoned SU2/M-1, the marketing effort will be toward the mixed commercial and office uses described above. The site includes a large open space area designed to encourage outdoor interaction and uses on the site. Pedestrian linkages as indicated on the pedestrian plan are located throughout the site to help tie buildings to each other and to the large open space area. These linkages also provide internal access to the site from bounding streets. Key points of emphasis described within this site development plan for subdivision are:

- Strong pedestrian linkages
- Unified architecture
- Open space areas
- Unified landscaping

VEHICULAR ACCESS & CIRCULATION TO AND THROUGH THE SITE:

Two internal private ways (see legend) that run east and west between San Mateo Blvd. and Pan American freeway will provide vehicular access through the site. The larger of the two, and northernmost, is a private way 64' in width (40' curb to curb asphalt). It will serve as primary access to the site. The secondary, and southernmost, is a private way 51' in width (24' curb to curb asphalt). It will help to serve access needs on the southern portion of the site. Access to each individual tract from one or sometimes both of the two access streets, access to the site from Pan American freeway (single direction traffic going south) is improved by the addition of deceleration lanes 300 feet in length. Three access points along Modesto are anticipated. One is to be a shared access point for both tracts #9 & #11. The others will be individual access points serving tracts #14 & #3. While locations for the access points are shown, actual locations may vary based on actual site layout and access needs. From San Mateo there will be access to the two internal streets mentioned above. Existing south bound turn lanes in the median at San Mateo will serve these access points. North bound traffic along San Mateo will not require new turn lanes into the site. There is no vehicular access to the site from San Diego ave. Because of the La Cueva arroyo lying along the sites northern boundary, the 12' AMAFCA access drive along the southern edge of the arroyo will be paved for bicycle and pedestrian traffic. A 15' wide gravel access way for AMAFCA will be maintained along the northern edge of the arroyo. Two internal north south vehicular/pedestrian connections lie between tracts 13 & 10 and tracts 2 & 5.

PEDESTRIAN ACCESS & CIRCULATION TO AND THROUGH THE SITE:

Each of the two east/west streets within the site are to have 6' sidewalks along each side. New pedestrian sidewalks will also be provided along San Mateo Blvd and Modesto ave. Where pedestrians are most likely to approach the site. At the intersection of Modesto and San Mateo a new crosswalk made from colored, textured concrete shall be provided to both slow vehicular traffic and provide safer pedestrian access to the site. Each tract shall have a pedestrian connection to either San Mateo or Modesto or to at least one of the internal East/West streets that in turn provides pedestrian connection to San Mateo and the existing bus stop location located near the midpoint of the San Mateo boundary of the site. The two North/South vehicular access ways discussed above in vehicular access, are to accommodate at least one pedestrian walk-way of at least 6' in width. These two walkways will provide pedestrian connection to the two east west connectors within the site. A third north/south pedestrian circulation route shall also connect the north and south east/west connections between tracts #8 & #10 at the center of the site. A pedestrian route from tract #2 will run east and west to connect to tract #13. This route runs along the southern edges of tract #4 & #15 (the central commons/park area). Pedestrians will also be able to access the site from the bicycle/pedestrian path on the southern edge of the La Cueva arroyo at the northern boundary of the site. Pedestrian access to tracts #1, #7, & #12 shall connect to this pedestrian/bicycle path. All pedestrian connections shall be as described in the design standards for this site development plan detailed on subsequent sheets.

PHASING:

The first site plan for building permit (tract #13) shall be heard by the environmental planning commission for approval. Vehicular and pedestrian access from San Mateo sufficient to serve the needs of tract #13 will be constructed as a part of phase one. This will include the improvement of the existing bus stop on the East side of San Mateo. The central commons/park and remainder of the two main internal streets running East and West shall be constructed ahead of or concurrent with any next tracts development following the completion of tract #13. Development of other tracts will progress based on market conditions and demands.

NOTE: INFRASTRUCTURE LIST TO BE ESTABLISHED WITH FUTURE PLATTING OR SITE PLAN FOR BUILDING PERMIT

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LEGEND

---	PROPERTY LINES
---	NEW PRIVATE PRIMARY (INTERNAL) WAY.
---	NEW PRIVATE SECONDARY (INTERNAL) WAY.
---	PEDESTRIAN/VEHICULAR CONNECTION
---	PEDESTRIAN CONNECTION
---	PEDESTRIAN/BIKE PATH

PROJECT NUMBER 1000310

APPLICATION NUMBER

EPC SITE DEVELOPMENT PLAN SIGNOFF

APPROVAL:

<i>[Signature]</i>	03/10/10	DATE
TRAFFIC ENGINEERING, TRANSPORTATION DIVISION		
<i>[Signature]</i>	03/10/03	DATE
ABCWUA		
<i>[Signature]</i>	3/10/10	DATE
PARKS AND RECREATION DEPARTMENT		
<i>[Signature]</i>	3/10/10	DATE
CITY ENGINEER		
ENVIRONMENTAL HEALTH DEPARTMENT (CONDITIONAL)		DATE
SOLID WASTE MANAGEMENT		DATE
<i>[Signature]</i>	10/4/10	DATE
DRB CHAIRPERSON, PLANNING DEPARTMENT		
NORTH I-25 CORPORATE CENTER		

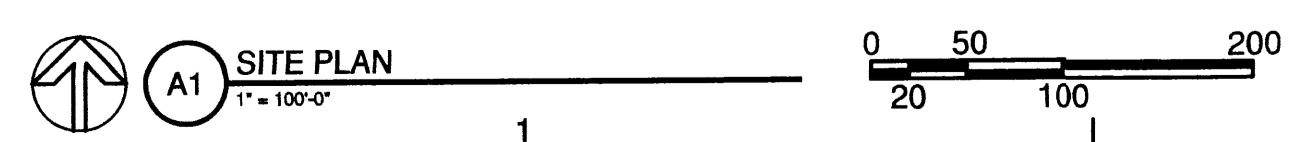
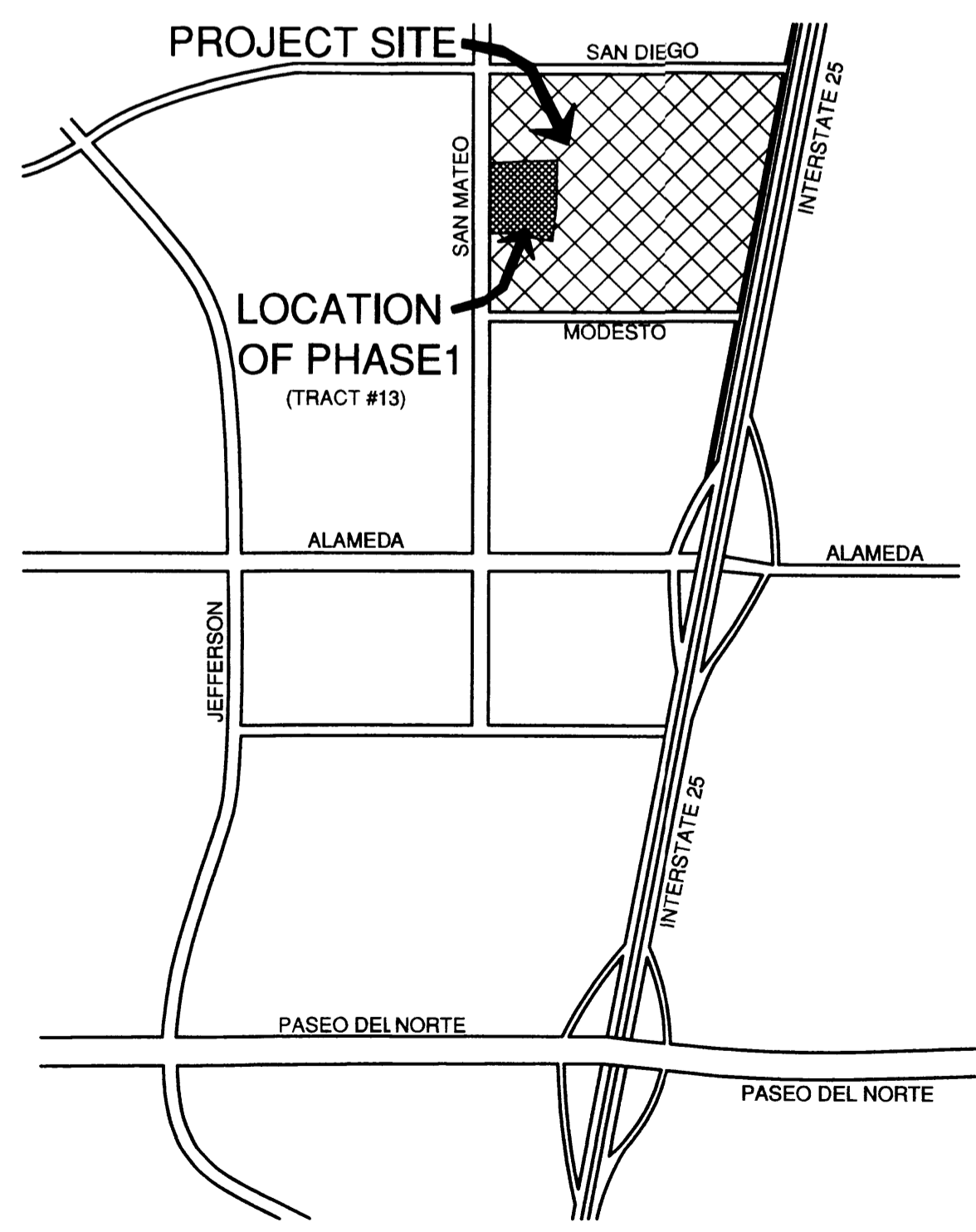
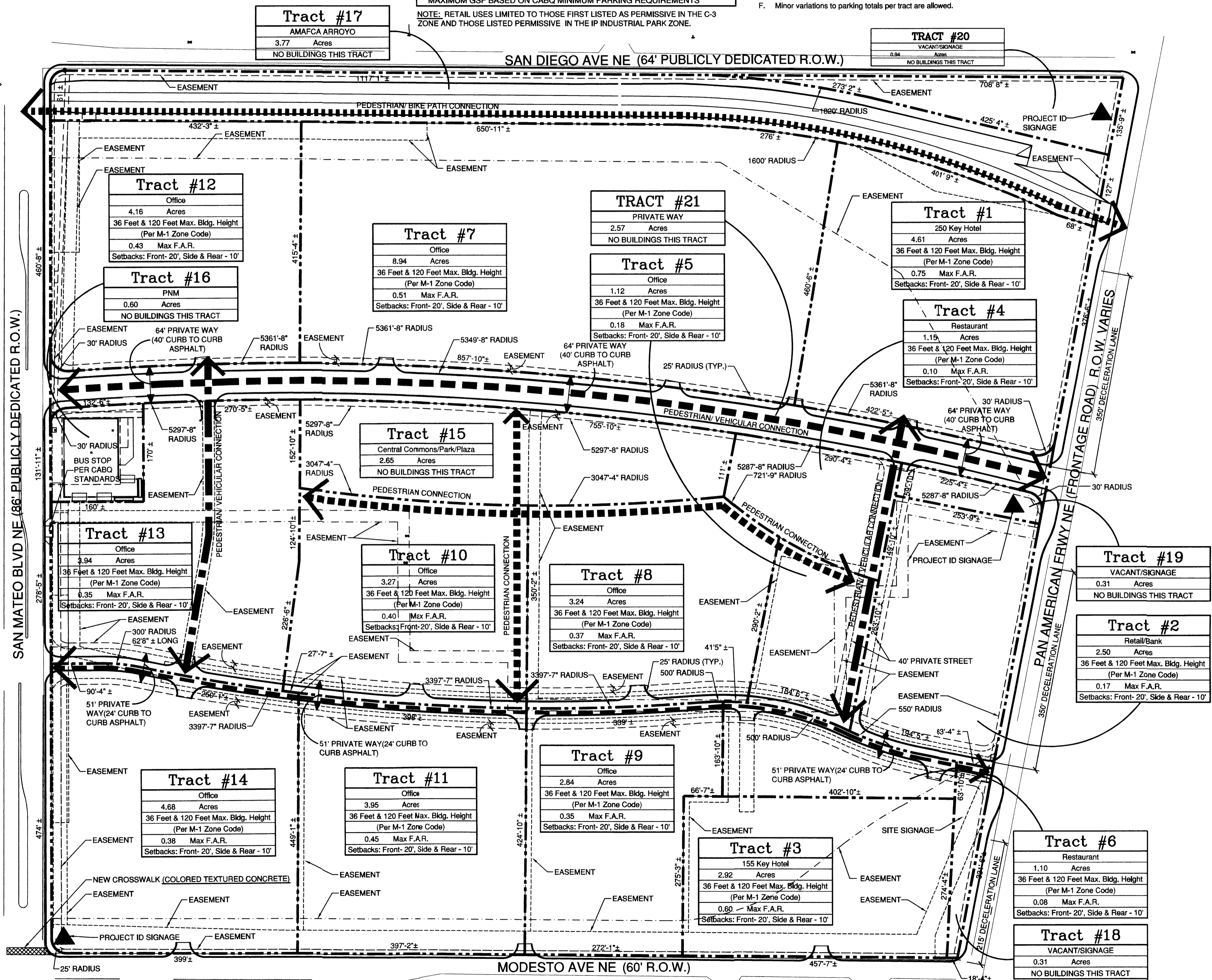
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	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO: N-25-CC
 CAD DWG FILE: AS-101 SITE DEVELOPMENT PLAN FOR SUBDIVISION & BUILDING PERMIT DWG
 DRAWN BY: JLL
 CHECKED BY: JLL

SHEET TITLE

SITE DEVELOPMENT PLAN FOR SUBDIVISION

AS-101



NOTE: SEE SHEET VS-101 FOR DETAILED INFORMATION ON ALL EASEMENTS, EXISTING AND PROPOSED.

VICINITY MAP
 SCALE: 1" = 1,000'-0"

PROJECT # 1000310

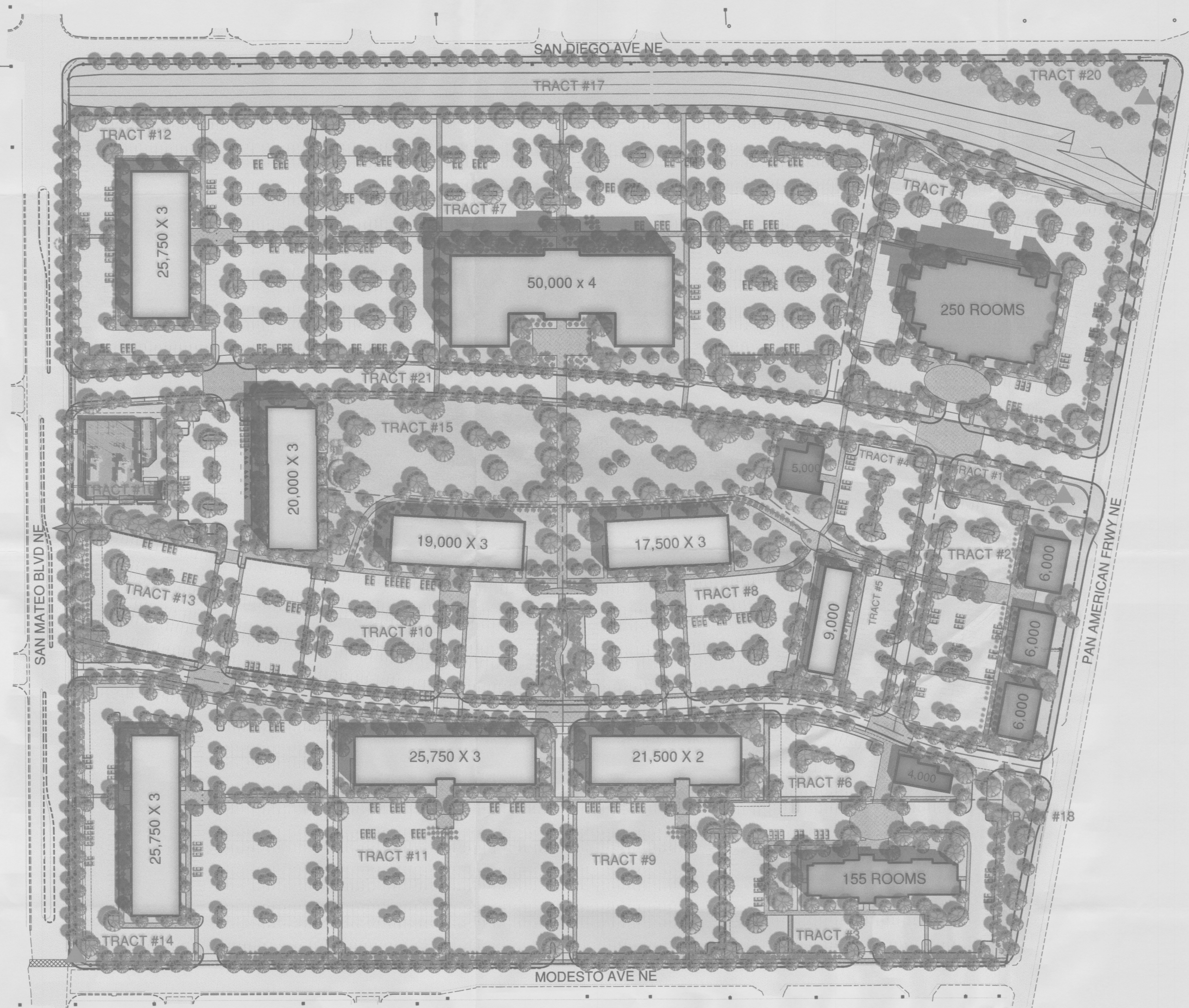


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LEGEND

- RETAIL
- HOTEL
- OFFICE
- RESTAURANT
- PEDESTRIAN CONNECTIONS

- LOT LINES
- SITE SIGNAGE (PROJECT IDENTIFICATION)

EXISTING BUS STOP WITH NEW RIGHT-OF-WAY AS DESCRIBED ON SHEET VS-101. NEW TYPE "C" BUS SHELTER WITH NEW BENCH & NEW CONCRETE SIDEWALK/ WAITING AREA. ALL PER COA STD. 2355.

NORTH I-25 CORPORATE CENTER

MARK	DATE	DESCRIPTION
	12/16/09	REVISED EPC SUBMITTAL
	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:	N-25-CC
CAD DWG FILE:	AS-102 CONCEPTUAL SITE PLAN.DWG
DRAWN BY:	JLL
CHECKED BY:	JLL

SHEET TITLE
CONCEPTUAL SITE PLAN

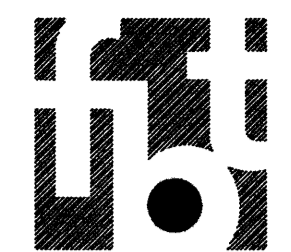
AS-102



A2

CONCEPTUAL SITE PLAN

SCALE: 1" = 100'-0"

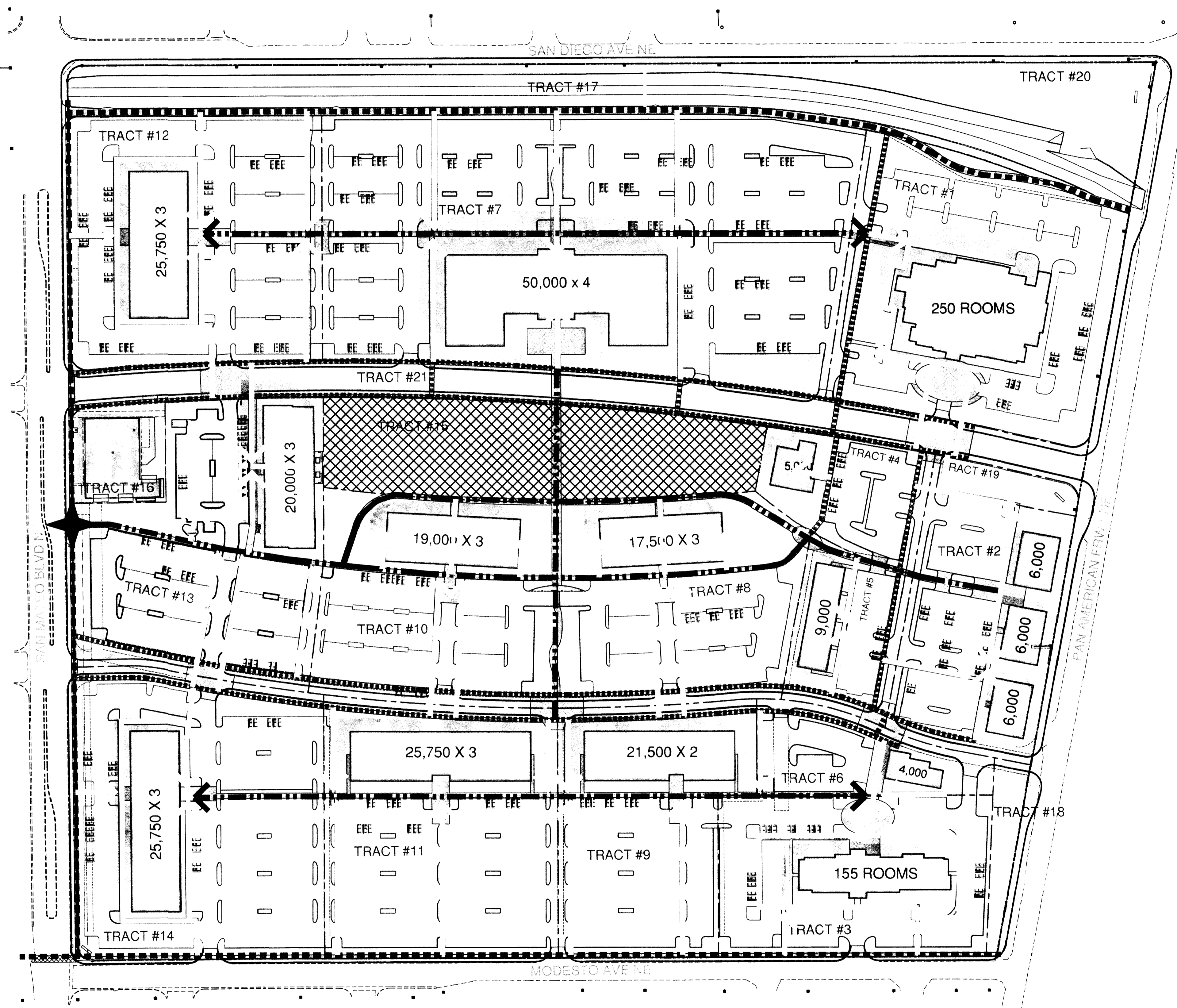


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NOTES:

- A. LOCATIONS INDICATED FOR PEDESTRIAN ACCESS ARE APPROXIMATIONS AND MAY VARY. QUANTITIES SHALL NOT BE REDUCED.
- B. PEDESTRIAN WALKWAYS INDICATED ON THIS SHEET ARE AS FURTHER DESCRIBED IN THE DESIGN STANDARDS FOR THIS SITE PLAN FOR SUBDIVISION.
- C. MINIMUM WIDTH FOR PEDESTRIAN WALKWAYS IS 8'-0". WHEN ADJACENT TO HEAD IN PARKING AREAS MINIMUM WIDTH IS 8'-0".
- D. PEDESTRIAN WALKWAYS SHALL BE CONSTRUCTED OF ENHANCED PAVING WHICH MAY INCLUDE CONCRETE OR OTHER PAVING MATERIALS BUT SHALL NOT BE ASPHALT. IN ADDITION, ENHANCED PAVING TREATMENTS OF TEXTURED OR COLORED CONCRETE OR THERMOPLASTIC OR SIMILAR MATERIAL SHALL BE PROVIDED AT CROSSWALKS CONNECTING PARKING AREAS AND AT WALKWAYS THAT CROSS STREETS.
- E. THE CROSSWALK AT THE INTERSECTION OF MODESTO AVE. AND SAN MATEO BLVD. SHALL BE COLORED, TEXTURED CONCRETE.
- F. PLAIN GREY OR BLACK CONCRETE, WHICH BLENDS IN WITH PARKING LOT ASPHALT COLORS, SHALL NOT BE USED. CONCRETE MUST BE TEXTURED AND PLAINLY VISIBLE AS IT CROSSES PARKING LOT AREAS.

LEGEND

- PRIMARY PEDESTRIAN WALKWAY NETWORK
- SECONDARY PEDESTRIAN WALKWAY NETWORK
- INDIVIDUAL BUSINESS PEDESTRIAN WALKWAY ACCESS
- CROSS TRACT PEDESTRIAN WALKWAY ACCESS
- CENTRAL COMMONS / PARK PLAZA - EXCLUSIVE PEDESTRIAN ENVIRONMENT
- EXISTING BUS STOP WITH NEW RIGHT-OF-WAY AS DESCRIBED ON SHEET VS-101. NEW TYPE 'C' BUS SHELTER WITH NEW BENCH & NEW CONCRETE SIDEWALK WAITING AREA, ALL PER COA STD. 2355.

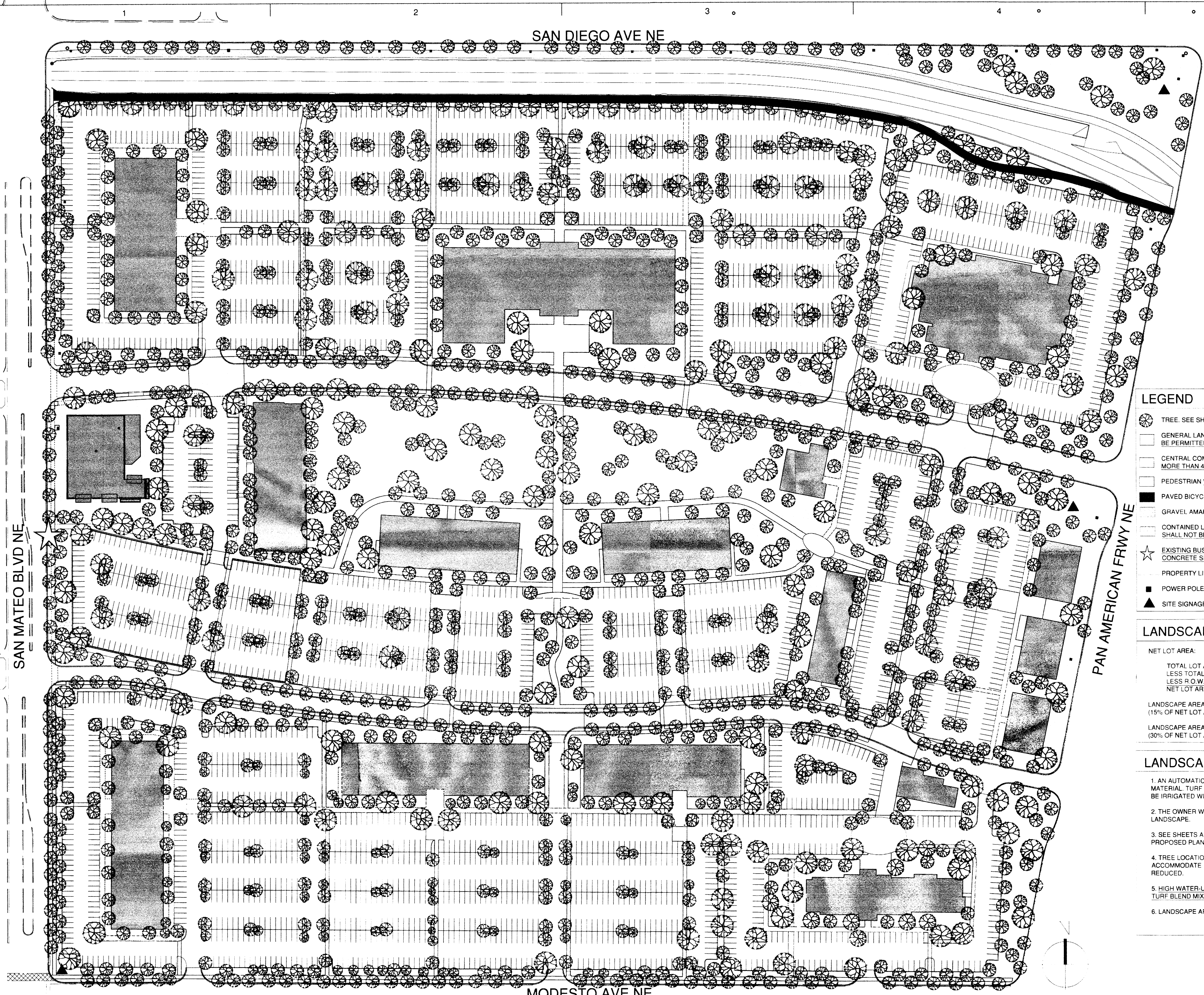
NORTH I-25 CORPORATE CENTER

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	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL
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PROJECT NO:	N425-CC
CAD DWG FILE:	AS-103 CONCEPTUAL SITE PLAN/PEDESTRIAN.DWG
DRAWN BY:	xxx
CHECKED BY:	xxx

SHEET TITLE
CONCEPTUAL PEDESTRIAN PLAN

AS-103



SAN DIEGO AVE NE

MODESTO AVE NE

SAN MATEO BLVD NE

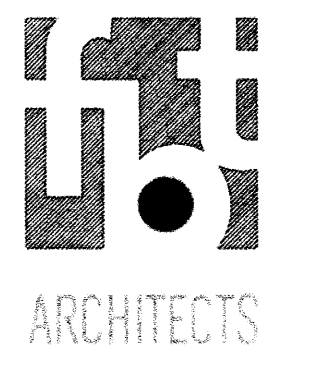
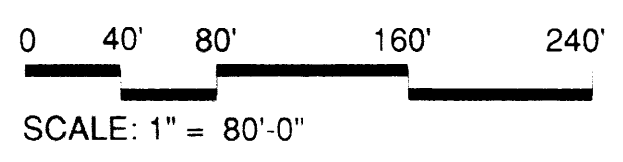
PAN AMERICAN FRWY NE

- LEGEND**
- TREE. SEE SHEET A-703 FOR PROPOSED PLANT PALETTE
 - GENERAL LANDSCAPE AREA (PLANTING BEDS). HIGH WATER USE TURF SHALL NOT BE PERMITTED.
 - CENTRAL COMMONS PARK/PLAZA. HIGH WATER USE TURF PERMITTED AS NO MORE THAN 40% OF A TURF BLEND MIX.
 - PEDESTRIAN WALKWAY
 - PAVED BICYCLE AND PEDESTRIAN PATH AND AMAFCA ACCESS ROAD (SHARED)
 - GRAVEL AMAFCA ACCESS ROAD
 - CONTAINED LANDSCAPE AT UNEXCAVATED LANDFILL. HIGH WATER USE TURF SHALL NOT BE PERMITTED.
 - EXISTING BUS STOP. NEW TYPE 'C' BUS SHELTER WITH NEW BENCH AND NEW CONCRETE SIDEWALK/WAITING AREA. ALL PER COA STD. 2355.
 - PROPERTY LINE
 - POWER POLE
 - SITE SIGNAGE LOCATION

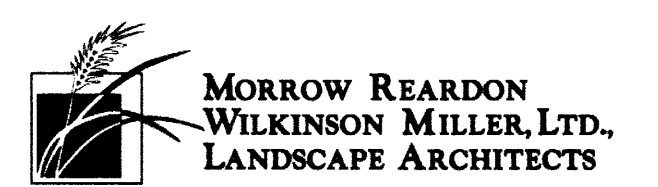
LANDSCAPE AREA CALCULATIONS

NET LOT AREA:	
TOTAL LOT AREA	2,598,885 SF
LESS TOTAL BUILDINGS AREA (306,158 SF)	
LESS P.O.W. LANDSCAPING (80,949 SF)	
NET LOT AREA	2,211,778 SF
LANDSCAPE AREA REQUIREMENT (15% OF NET LOT AREA)	
	331,767 SF
LANDSCAPE AREA PROVIDED (30% OF NET LOT AREA)	
	656,203 SF

- LANDSCAPE NOTES**
1. AN AUTOMATIC IRRIGATION SYSTEM WILL BE INSTALLED FOR ALL PLANT MATERIAL. TURF AREAS SHALL BE SPRAY IRRIGATED. TREES AND SHRUBS SHALL BE IRRIGATED WITH A BUBBLER SYSTEM.
 2. THE OWNER WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF THE LANDSCAPE.
 3. SEE SHEETS A-701 THROUGH A-703 FOR LANDSCAPE DESIGN STANDARDS AND PROPOSED PLANT PALETTE.
 4. TREE LOCATIONS ARE SHOWN SCHEMATICALLY AND MAY VARY TO ACCOMMODATE FINAL BUILDING FOOTPRINTS. QUANTITIES SHALL NOT BE REDUCED.
 5. HIGH WATER-USE TURF SHALL BE PERMITTED AS NO MORE THAN 40% OF A TURF BLEND MIX IN THE CENTRAL COMMONS PARK/PLAZA.
 6. LANDSCAPE AREAS SHALL BE COMPACTED TO 85% PER COA SPECIFICATIONS.



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MORROW REARDON
 WILKINSON MILLER, LTD.
 LANDSCAPE ARCHITECTS

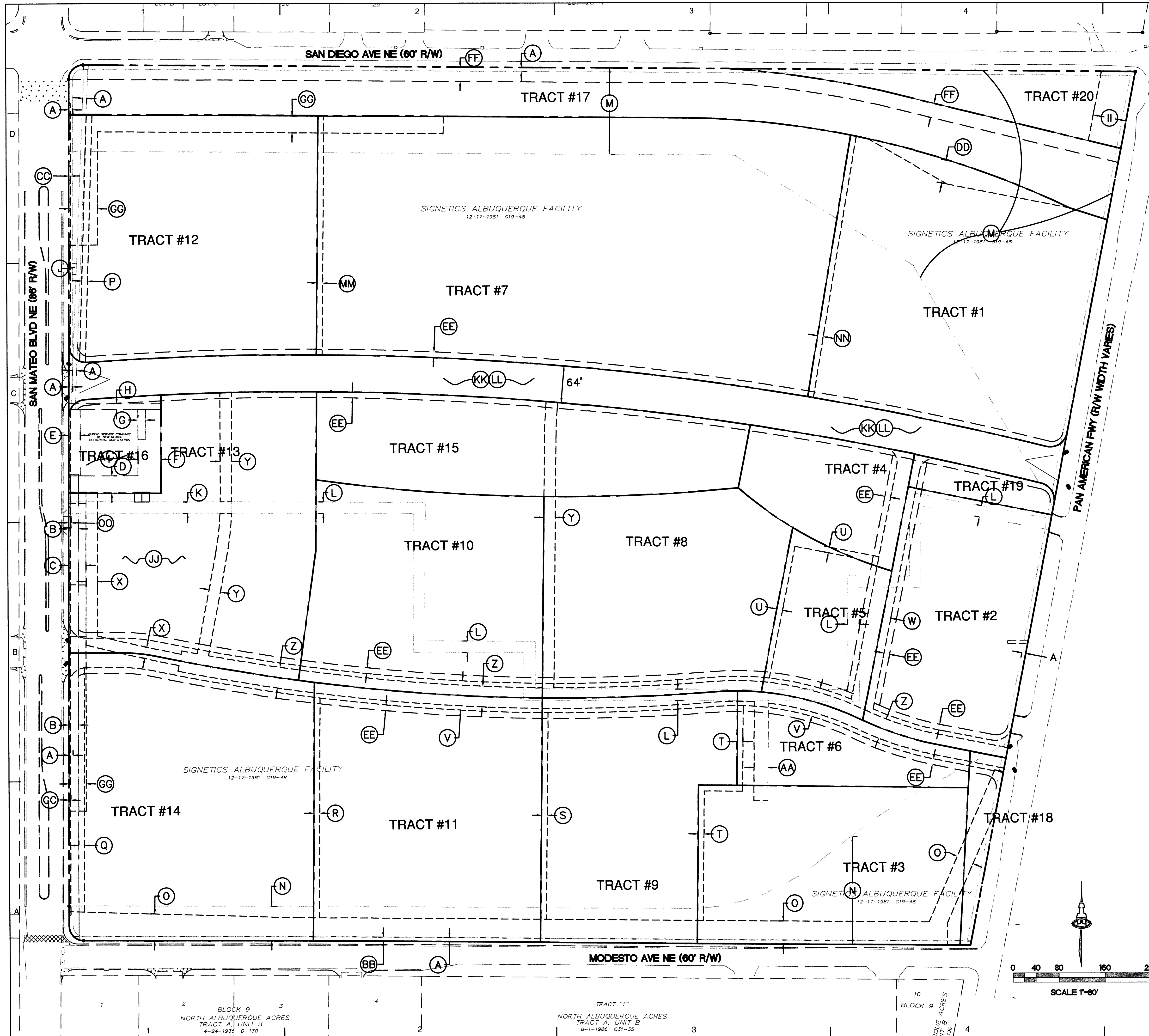
**NORTH I-25
 CORPORATE CENTER**

MARK	DATE	DESCRIPTION
	12/16/09	REVISED EPC SUBMITTAL
	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:
 CAD DWG FILE: 0409 - SITE - EPC
 DRAWN BY: AD
 CHECKED BY: ECR

SHEET TITLE
**CONCEPTUAL
 LANDSCAPE PLAN**

LS-101



EASEMENT NOTES

- A. EXISTING 7' UTILITY EASEMENT PER PLAT FILED 12-17-81, VOL. C19, FOLIO 48
- B. EXISTING 17' PNM & US WEST EASEMENT PER DOCUMENT FILED 5-31-94, BK. 9417, PG. 1431
- C. EXISTING 30' PNM ACCESS EASEMENT PER DOCUMENT FILED 9-12-2002, BK. A41, PG. 5281
- D. EXISTING 30' PNM & US WEST EASEMENT PER DOCUMENT FILED 9-12-2002, BK. A41, PG. 5255
- E. EXISTING 20' PNM & US WEST EASEMENT PER DOCUMENT FILED 5-31-94, BK. 9417, PG. 1431
- F. EXISTING 40' PNM & US WEST EASEMENT PER DOCUMENT FILED 9-12-2002, BK. A41, PG. 5255
- G. EXISTING 13.5' PNM & US WEST EASEMENT PER DOCUMENT FILED 7-27-99, BK. 9910, PG. 7348
- H. EXISTING 10' PNM & US WEST EASEMENT PER DOCUMENT FILED 9-12-2002, BK. A41, PG. 5255
- I. EXISTING PNM SUBSTATION EASEMENT AS SHOWN ON PLAT FILED 12-17-81, VOL. C19, FOLIO 48
- J. EXISTING VARIABLE WIDTH OWEST EASEMENT PER DOCUMENT FILED 4-3-2001, BK. A17, PG. 5292
- K. EXISTING 20' WATER LINE EASEMENT PER PLAT FILED 12-17-81, VOL. C19, FOLIO 48
- L. EXISTING 20' WATER LINE EASEMENT TO BE VACATED BY FINAL PLAT
- M. EXISTING DRAINAGE EASEMENT AS SHOWN ON PLAT FILED 12-17-81, VOL. C19, FOLIO 48 TO BE VACATED BY FINAL PLAT
- N. EXISTING DRAINAGE EASEMENT AS SHOWN ON PLAT FILED 12-17-81, VOL. C19, FOLIO 48 TO BE VACATED BY FINAL PLAT AT TIME OF CONSTRUCTION OF UNDERGROUND DRAINAGE STRUCTURE.
- O. PUBLIC DRAINAGE EASEMENT GRANTED TO THE CITY OF ALBUQUERQUE BY FINAL PLAT.
- P. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 13 & 21 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIAT.
- Q. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 13 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- R. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 10 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- S. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 8 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- T. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACTS 2, 5, & 6 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- U. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 2 & 5 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- V. 51' PRIVATE ACCESS & DRAINAGE EASEMENT GRANTED TO AND MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION. GRANTED BY FINAL PLAT.
- W. 40.5' PRIVATE ROADWAY & DRAINAGE EASEMENT GRANTED TO AND MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION. GRANTED BY FINAL PLAT.
- X. 20' PUBLIC SANITARY SEWER EASEMENT GRANTED TO ABCWUA BY FINAL PLAT
- Y. 20' PUBLIC WATER EASEMENT GRANTED TO ABCWUA BY FINAL PLAT
- Z. 36' PUBLIC SANITARY SEWER & WATER EASEMENT GRANTED TO ABCWUA BY FINAL PLAT
- AA. 25' PUBLIC SANITARY SEWER & WATER EASEMENT GRANTED TO ABCWUA BY FINAL PLAT
- BB. 7' PUBLIC PEDESTRIAN ACCESS EASEMENT GRANTED TO CITY OF ALBUQUERQUE BY FINAL PLAT.
- CC. 3' PUBLIC PEDESTRIAN ACCESS EASEMENT GRANTED TO CITY OF ALBUQUERQUE BY FINAL PLAT.
- DD. VARIABLE WIDTH PUBLIC PEDESTRIAN ACCESS EASEMENT GRANTED TO CITY OF ALBUQUERQUE BY FINAL PLAT.
- EE. 10' PUBLIC UTILITY EASEMENT GRANTED BY FINAL PLAT
- FF. 25' OVERHEAD UTILITY EASEMENT GRANTED TO PNM BY FINAL PLAT.
- GG. LANDFILL WASTE EASEMENT. STORM AND IRRIGATION WATERS MUST BE CONTAINED AND NOT BE ALLOWED TO INFILTRATE THE SOIL WITHIN THIS EASEMENT. SPRINKLERS AND IRRIGATION MAY ONLY OCCUR IN WATER CONTAINMENT AREAS LINED WITH MATERIALS APPROVED BY THE CERTIFYING LANDFILL GAS ENGINEER RESPONSIBLE FOR THE WASTE EXCAVATION PLAN TO REMOVE BURIED WASTE FROM THE SITE.
- HH. PRIVATE, BLANKET CROSS LOT ACCESS, DRAINAGE, AND DRY UTILITY EASEMENTS GRANTED ON AND FOR THE BENEFIT OF TRACTS 1 - 15 AND TRACTS 18 & 19.
- II. 58' WIDE PUBLIC DRAINAGE EASEMENT GRANTED TO AMAFCA BY FINAL PLAT
- JJ. BLANKET MAINTENANCE ACCESS EASEMENT COVERING TRACT 13 GRANTED TO PNM BY FINAL PLAT.
- KK. BLANKET PRIVATE ACCESS & DRAINAGE EASEMENT COVERING TRACT 21, GRANTED TO AND MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION. GRANTED BY FINAL PLAT.
- LL. BLANKET PUBLIC WATER & SANITARY SEWER EASEMENT COVERING TRACT 21, GRANTED TO THE ABCWUA BY FINAL PLAT.
- MM. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACTS 4, 8, 10, 15, & 21 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION.
- NN. 10' PRIVATE DRAINAGE EASEMENT GRANTED TO TRACT 2, 4, 19, & 21 BY FINAL PLAT AND TO BE MAINTAINED BY THE NORTH I-25 CORPORATE CENTER OWNER'S ASSOCIATION
- OC. 4' x 20' PUBLIC PEDESTRIAN ACCESS EASEMENT GRANTED TO CITY BY FINAL PLAT FOR BUS STOP.

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NORTH I-25
CORPORATE CENTER

MARK	DATE	DESCRIPTION
	7/29/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:
 CAD DWG FILE:
 DRAWN BY:
 CHECKED BY:

SHEET TITLE

EASEMENT EXHIBIT

VS-101



CONNECTION TO AMAFCA CHANNEL PER AMAFCA REQUIREMENTS

PUBLIC STORM DRAIN TO RE-ROUTE SOUTH LA CUEVA ARROYO FLOW THROUGH PROPERTY. FLOODPLAIN TO BE REMOVED.

VICINITY MAP



PROJECT DATA

PROPERTY: THE SITE IS A DEVELOPED COMMERCIAL PROPERTY LOCATED WITHIN C.O.A. VICINITY MAP B-18. THE SITE IS BOUND TO THE EAST BY I-25 FRONTAGE ROAD, TO THE WEST BY SAN MATEO BLVD., TO THE NORTH BY AN AMAFCA LINED ARROYO AND TO THE SOUTH BY MODESTO AVE. NE.

PROPOSED IMPROVEMENTS: THE PROPOSED IMPROVEMENTS INCLUDE OFFICE / RETAIL / RESTAURANT / HOTEL SITES WITH ASSOCIATED STREETS, PARKING AND LANDSCAPING.

LEGAL: SIGNETICS ALBUQUERQUE FACILITY, BLOCKS 6, 7 AND 8, TRACT A, UNIT B, NORTH ALBUQUERQUE ACRES, BERNILLO COUNTY, NM

ADDRESS: IRVING BLVD. N.E.

BENCHMARK: ALBUQUERQUE CONTROL SURVEY/NEW MEXICO STATE HIGHWAY COMMISSION MONUMENT "125-11" ELEVATION = 5206.936 (NGVD29)

OFF-SITE: NO OFF-SITE DRAINAGE AFFECTS THIS PROPERTY.

FLOOD HAZARD: PER BERNILLO COUNTY FIRM MAP #129, THE SITE IS LOCATED WITHIN FLOODZONE "X" DESIGNATED AS AREAS DETERMINED TO BE OUTSIDE 500-YEAR FLOODPLAIN.

DRAINAGE PLAN CONCEPT:

THE ENTIRE SITE IS PERMITTED FREE DISCHARGE TO THE ADJACENT PUBLIC STORM DRAIN SYSTEM TO THE SOUTH (A STORM DRAIN WILL REPLACE THE EXISTING SURFACE CHANNEL TO THE SOUTH) OR TO THE EXISTING AMAFCA ARROYO TO THE NORTH. AS EACH LOT DEVELOPS, A PRIVATE STORM DRAIN SYSTEM WILL BE REQUIRED TO COLLECT ON-SITE RUNOFF FOR ROUTING TO THE NORTH ARROYO OR TO STUBS PROVIDED TO THE SOUTH STORM DRAIN FOR THE PROPERTIES USE. NEW DEVELOPED RUNOFF IS BASED ON 15% GRASS AND GRAVEL, AND 85% IMPERVIOUS SURFACING. FREE DISCHARGE IS APPROPRIATE FOR THE SITE AS DOWNSTREAM CAPACITY IS AVAILABLE.

LEGEND

- PROPOSED PRIVATE STORM DRAIN
- PROPOSED PUBLIC STORM DRAIN
- PROPOSED PRIVATE STORM DRAIN INLET
- PROPOSED CONTOUR
- FF=5195.5 PROPOSED FINISH FLOOR ELEVATION
- ▨ EXISTING LANDFILL

CALCULATIONS: North I-25 Corporate Center :

Based on Drainage Design Criteria for City of Albuquerque Section 22.2, DPM, Vol 2, dated Jan., 1993

ON-SITE:		AREA OF SITE:	2377035.629 SF	SF	=	54.6	Ac.
HISTORIC FLOWS:		DEVELOPED FLOWS:		EXCESS PRECIP:		Precip. Zone 3	
On-Site Historic	Land Condition	On-Site Developed	Land Condition	Area A	=	0 SF	E _A = 0.66
Area B	=	Area B	=	237704 SF			E _B = 0.92
Area C	=	Area C	=	237704 SF			E _C = 1.29
Area D	=	Area D	=	1901629 SF			E _D = 2.36
Total Area	=	Total Area	=	2377036 SF			

On-Site Weighted Excess Precipitation (100-Year, 6-Hour Storm)

$$\text{Weighted } E = \frac{E_A A_A + E_B A_B + E_C A_C + E_D A_D}{A_A + A_B + A_C + A_D}$$

Historic E = 1.64 in. | Developed E = 2.11 in.

On-Site Volume of Runoff: V₃₆₀ = E*A / 12

Historic V₃₆₀ = 324862 CF | Developed V₃₆₀ = 417764 CF

On-Site Peak Discharge Rate: Q_p = Q_{ps}A_s + Q_{pd}A_d + Q_{pc}A_c + Q_{pd}A_d / 43,560

For Precipitation Zone 3

Q_{ps} = 1.87 | Q_{pc} = 3.45

Q_{pd} = 2.60 | Q_{pd} = 5.02

Historic Q_p = 207.9 CFS | Developed Q_p = 252.2 CFS



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NORTH I-25 CORPORATE CENTER

5/29/09	REVISED EPC SUBMITTAL
6/24/09	REVISED EPC SUBMITTAL
4/16/09	REVISED EPC SUBMITTAL
8/1/08	INITIAL EPC SUBMITTAL

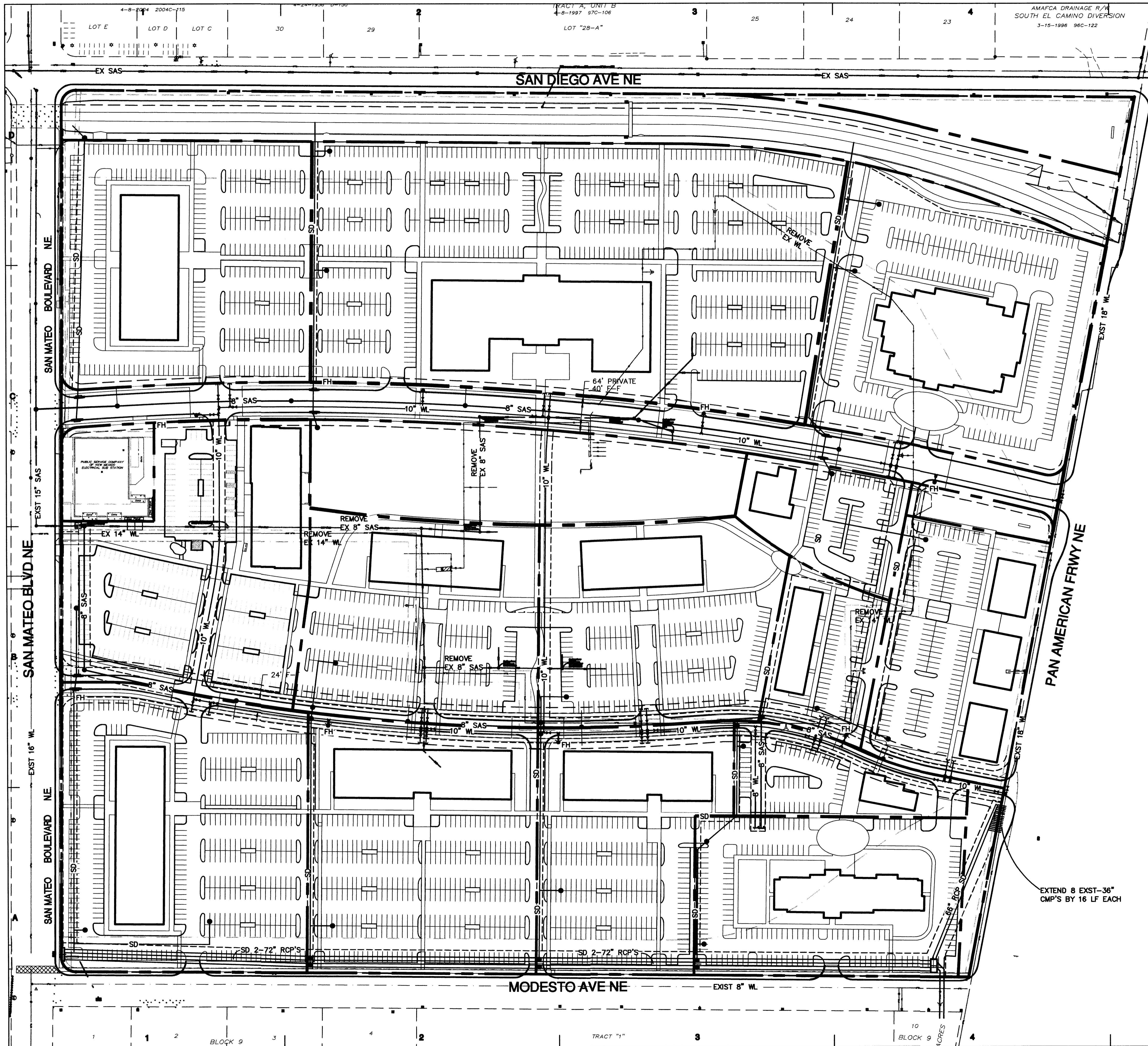
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PROJECT NO:	1686	
CAD DWG FILE:	1686 CG-101.dwg	
DRAWN BY:	DLP	
CHECKED BY:	GLD	

SHEET TITLE

CONCEPTUAL GRADING PLAN

CG-101

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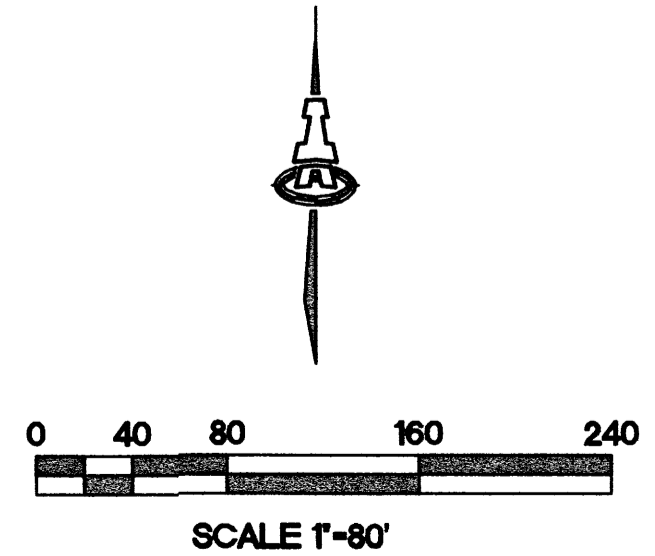


GENERAL NOTES

A. SEE SHEET V-101 FOR EXISTING & PROPOSED EASEMENTS.

LEGEND

- WATER METER & BOX
- GATE VALVE W/ VALVE BOX
- FIRE HYDRANT
- WATER LINE W/ FITTING
- SAS-
- SANITARY SEWER MANHOLE
- SD-
- EXISTING LANDFILL



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**NORTH I-25
CORPORATE CENTER**

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	4/16/09	REVISED EPC SUBMITTAL
	8/14/08	INITIAL EPC SUBMITTAL

PROJECT NO: 1688
CAD DWG FILE: 1688 CU-101.dwg
DRAWN BY: DLP
CHECKED BY: GLD

SHEET TITLE

**CONCEPTUAL
UTILITY PLAN**

CU-101

1- INTRODUCTION

The intent of these Design Standards is to provide a framework for future architects, landscape architects, designers, and developers that will help them understand the aesthetic and functional vision and goals for the property. The primary goal is to create a vibrant complex that responds to its surroundings, meets the needs of adjacent properties, improves environmental conditions, and complies with the city of Albuquerque zoning codes & ordinances. These standards shall be used to facilitate the design of buildings, landscapes, and facilities that respect their surroundings, maintain and highlight views, and improve the natural and built environment. As outlined in these Design Standards, innovative and sustainable design techniques along with the use of "green" building materials is required.

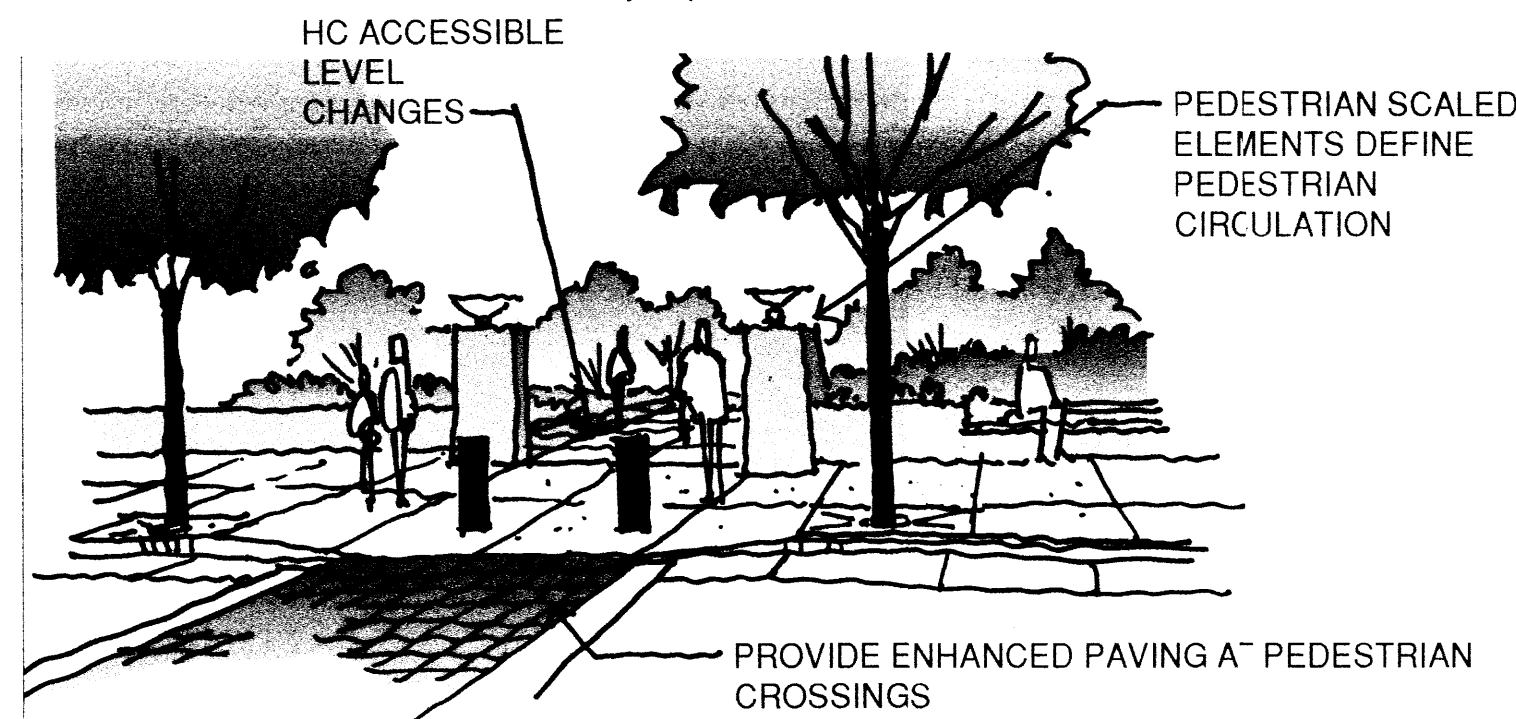
The purpose of these Design Standards is to provide guidance for future development with specific objectives that encourage innovative and creative solutions. The desired character of design features common to the master plan area such as pedestrian features, grading, landscape, signage, parking, lighting, and architecture are expressed in these standards.

A. LAND USE CONCEPT:

- The master developer shall maintain control of the site through the development process and asset ownership. Maintenance of the common area and each landscaped area on individual tracts will be under the purview and oversight of a management entity controlled by the master developer pursuant to the covenants, rules and restrictions established for the master development.
- This SU2/M-1 (light manufacturing zone) property is intended to be developed as a business park with a mix of commercial and office uses. Design Standards are provided to integrate the mix of uses by establishing an architectural style, unified landscaping throughout, pedestrian linkages, lighting and signage standards, and compliance with the North I-25 Sector Development Plan. The proposed mixture of commercial uses on the site is intended to appeal to surrounding areas, as well as provide some on-site amenities in support of the on-site office uses. This development will provide up to two (2) on-site restaurants, two hotel sites, retail/bank as permissive in the IP zone, and office uses. Although zoned SU2/M-1, the marketing effort will be toward the mixed commercial and office uses described above.
- The two drive isles providing internal access to the site in the east west direction are to be private drive isles maintained by the property owners. The site includes a large open space area designed to encourage outdoor interaction and uses on the site. Pedestrian linkages as indicated on the pedestrian plan are located throughout the site to help tie buildings to each other and to the large open space area. These linkages also provide internal access to the site from bounding streets.
- Key points of emphasis described within this site development plan for subdivision are:
 - Strong pedestrian linkages
 - Unified architecture
 - Open space areas
 - Unified landscaping
- No more than 3 uses with drive up service windows shall be permitted. Each shall adhere to the following restrictions:
 - Food and drink uses with drive up service windows are not allowed pursuant to the subject site's current zoning (SU-2/M-1) and will not be permitted unless allowed by a future, applicable sector development plan.
 - No drive up service window shall face Interstate 25
 - The drive up service window and stacking lane shall be screened from public view and shall be integrated with the architecture of the building it serves.
 - A pedestrian crossing across the stacking lane shall be demarcated and shall be clearly visible and highlighted with enhanced paving.
- Future development shall be in accordance with these design standards.
- Building outlines, parking areas, sidewalk layouts, and landscape areas shown on individual tracts in any of the sheets accompanying this site development plan for subdivision are conceptual in nature. Actual layouts and areas on individual tracts may differ from those shown herein, but must meet the intent and requirements of this site plan for subdivision.
- All tract building setbacks are as outlined in the IP zone pursuant to the North I-25 sector plan.
- Refer to sheet VS-101 for detailed information on all easements new and existing.
- Because the premises is zoned SU-2/M-1 and larger than five acres, it is considered a Shopping center site and therefore subject to the Shopping Center Regulations of the Zoning Code.

B. GENERAL SITE DESIGN:

- A primary focus in site design will be the creation of a site that is pedestrian friendly and encourages pedestrian activity throughout the site.
- All buildings shall be oriented to pedestrian movement and the public right-of-way except in cases where the development creates an interior pedestrian plaza. A number of pedestrian walkways shall be provided between the sidewalk adjacent to the roadway and building entries as indicated on the conceptual pedestrian plan (AS-103).
- Pedestrian walkways shall be constructed of enhanced paving which shall include concrete or other paving materials but shall not be asphalt. In addition, enhanced paving treatments of textured or colored concrete or thermoplastic or similar material shall be provided at crosswalks connecting parking areas and at walkways that cross streets. Plain grey or black concrete, which blends in with parking lot asphalt colors, shall not be used. Concrete must be textured and plainly visible as it crosses parking lot areas.
- Shared entries to commercial businesses are encouraged. The number of vehicular access points to parking lots shall be limited to the minimum necessary to provide adequate circulation.



- Breaking up large expansive areas of asphalt or concrete paving in parking lots with landscape islands, pedestrian walkways, and other design elements is required. Landscape islands and pedestrian walkways shall be as further described in these Design Standards.
- Ensure appropriate bicycle and pedestrian access to the plan area and adjacent trails.
- Ensure appropriate pedestrian access to Wildflower Park, west of the development.
- Main building entrances shall be visible to the pedestrian and directly accessible.
- Pedestrian connections shall link buildings to any small plaza areas on individual tracts and to the larger central commons park/plaza [NVAP]
- The pedestrian crossing leading to the wildflower neighborhood shall be made of colored, textured concrete.

2- PEDESTRIAN FEATURES

A. SITE FURNISHINGS

To enhance the pedestrian experience throughout the site, amenities such as benches, shade structures, bicycle racks, pedestrian scale lighting, enhanced landscape areas, enhanced paving, and other visual articulations shall be incorporated throughout the development. A cohesive palette of site furnishings will contribute to the visual continuity of the site.

B. SIDEWALKS

Sidewalks shall be provided along the east side of San Mateo Blvd. and the north side of Modesto Ave., and shall comply with the requirements of the COA Sidewalk Ordinance. There shall be no sidewalk along the south side of San Diego Ave. to encourage pedestrians to use the nearby paved bike and pedestrian path south of the La Cueva Arroyo. Pedestrian traffic shall be discouraged along the Pan American Freeway through the use of wide landscape buffers. See section 6.D1 of these standards.

- Sidewalks shall be a minimum width of six feet. When adjacent to head in parking areas minimum width is 8'-0"
- The street side of the sidewalk shall be set back a minimum of six feet from the back of the curb.

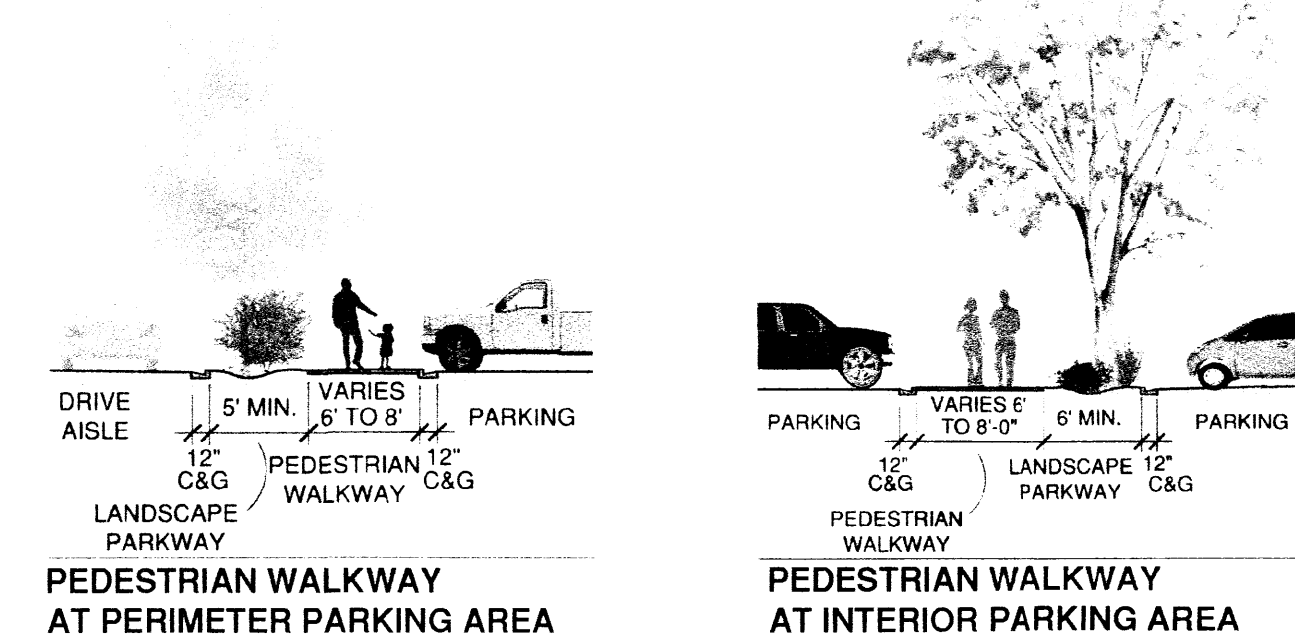
C. ACCESSIBILITY

Pedestrian circulation routes shall comply with the guidelines of the Americans With Disabilities Act of 1990.

D. PEDESTRIAN CONNECTIONS AT PARKING AREAS

Safe, convenient access to and from the site and ease of mobility within the site shall be provided for pedestrians. Each tract within the site shall be pedestrian, bicycle, and handicap accessible within the tract and to adjacent streets.

- Pedestrians shall have safe and accessible travel routes from parking areas to buildings.
- Pedestrian walkways shall be constructed of enhanced paving which shall include concrete or other paving materials but shall not be asphalt. In addition, enhanced paving treatments of textured or colored concrete or thermoplastic or similar material shall be provided at crosswalks connecting parking areas and at walkways that cross streets. Plain grey or black concrete, which blends in with parking lot asphalt colors, shall not be used. Concrete must be textured and plainly visible as it crosses parking lot areas.

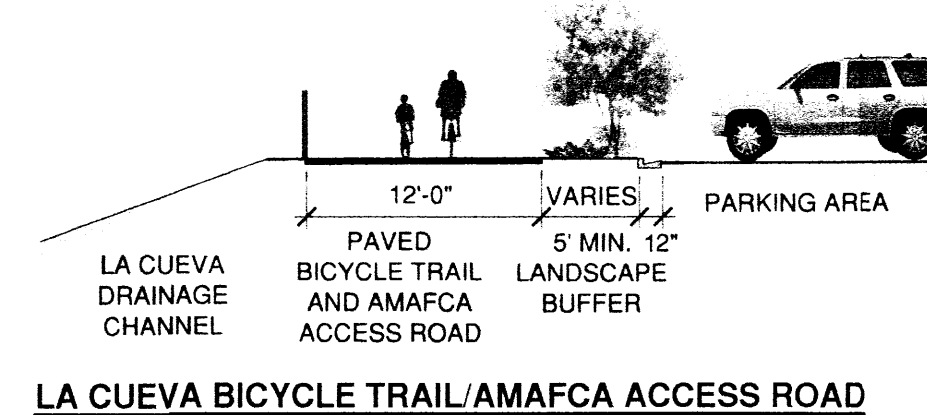


E. BICYCLE CONNECTIONS

1. Connections near Arroyos:

A paved bicycle path shall be provided on the southern side of the La Cueva Arroyo (North I-25 Sector Development Plan (NSDP) 1998 and Trails and Bikeways Facility Plan (TBFP). This trail is a portion of the COA "La Cueva/Alameda Wasteway Trail" and is part of the network of trails planned and described in the Trails and Bikeways Facility Plan.

- The paved bike & pedestrian path shall be 12 feet in width to ensure pedestrian and bicyclist safety (TBFP).
- The paved bike and pedestrian pathway along the southern side of the arroyo on the subject site's northern side shall have a separated 12' bicycle and pedestrian lane that indicates where the bicycles are supposed to go and where the pedestrians are supposed to go, and that does not create conflict between pedestrian and bicycle traffic.
- A guardrail shall be provided where trail is within five feet of the arroyo.
- The trail will be shared as an access road for AMAFCA.



- Connections throughout the site- Pedestrian networks identified on the Conceptual Pedestrian Plan (AS-103) will provide cyclists access and connections throughout the site.

F. BICYCLE STORAGE

- Outdoor bicycle parking (racks) must be provided on each tract as required by the City of Albuquerque. Bicycle parking does not include indoor bicycle storage as described below.
- Each tract must also provide indoor bicycle storage at the rate of 1 space for every 15,000 sq ft of GLSF.
- Each tract shall also provide shower and changing rooms for .5% of building occupants as determined by applicable building codes. Shower and changing room requirement does not apply to tracts having retail and restaurant uses.
-

G. RESTAURANTS

Restaurants shall have shaded outdoor patio seating and dining. Adequate shading shall be defined as a minimum of 35% of the outdoor seating area.

2- PEDESTRIAN FEATURES

H. TRANSIT

Accessibility is a key component of encouraging mass transit ridership. The transit stop shall be improved at the existing location along San Mateo as a part of phase 1. The bus stop improvements are required to meet City of Albuquerque Standards.

3- CENTRAL COMMONS PARK/PLAZA

The Central Commons Park/Plaza shall provide an inviting, flexible space that creates opportunities for a variety of uses for the enjoyment of all users. Paved areas shall connect to the surrounding pedestrian-ways at the perimeter of the Commons Area. The paved areas shall be 'interwoven' with landscape areas to provide visual interest and separation/buffering between the various use zones.

A. Paved areas shall include the following:

- Open gathering areas for large groups to attend formal events such as musical performances, wedding receptions, and parties.
- Intimate gathering areas for smaller groups to participate in informal events such as outdoor meetings, lunch groups, and informal social events.
- Benches, tables, and trash receptacles shall be strategically placed throughout the paved areas for the convenience and comfort of users. The site furnishings shall be coordinated with the furnishings palette for the balance of the project to ensure the visual and aesthetic continuity of the site.
- Pedestrian scale lighting shall be provided for night events and for general safety.
- Light poles and fixtures shall be aesthetically appealing and shall be coordinated with the furnishings palette for the balance of the project. Lighting shall comply with state and local illumination safety standards.
- Shade structures such as trees, pergolas & gazebos etc.

B. Landscape areas shall include the following:

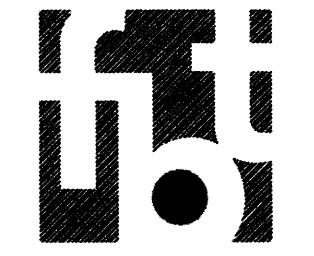
- Open turf areas for large group gatherings and informal recreation.
- Decorative landscape beds with trees and shrubs.
- Planting buffers between the various use zones.
- Shade trees installed in tree wells in selected paved areas to provide shade and an outdoor 'ceiling' to the space.
- Buffer plantings between the Commons Area and the private way to the north.



CENTRAL COMMONS AT PARK/GREEN SPACE AREA



CENTRAL COMMONS AT PLAZA/LARGE GATHERING AREA

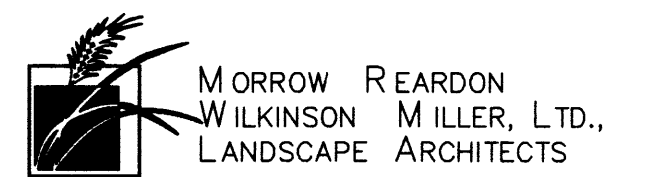


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NORTH I-25 CORPORATE CENTER

MARK	DATE	DESCRIPTION
	3/2/10	DRB SUBMITTAL
	12/16/09	REVISED EPC SUBMITTAL
	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:	N-125-CC
CAD DWG FILE:	A701 DESIGN GUIDELINES.DWG
DRAWN BY:	JLL
CHECKED BY:	JLL

SHEET TITLE

DESIGN STANDARDS

A-701

4- ART



PUBLIC ART/SCULPTURES

SCULPTURES OF VARYING SIZES LOCATED THROUGHOUT THE SITE

A. ART/SCULPTURE

- At various locations throughout the site art & sculptures will be used to highlight focal points, & encourage pedestrian interaction.
- Artwork shall not be used as signage to advertise a particular business or use.
- Artwork shall not have wayfinding graphics.
- Artwork elements shall not contain advertising of any kind.

5- PARKING / STREETS

A. INTRODUCTION

The intent is to provide flexibility for potential property owners who might require slightly more parking than that typically required in the City of Albuquerque minimum parking requirements. In all instances, the minimum parking requirement per City of Albuquerque parking ordinances is met. In the project summary table (Sheet AS-101) the conceptual building square footages are expressed in a range having an associated parking count of 1 space per 200 gsf of office building compared to the City of Albuquerque minimum standard of 1 space per 200 nlsf for the first floor and 1 per 300 for floors above and below the first floor.

B. CLEAR LINES OF SIGHT

Clear lines of sight shall be maintained at all exits of parking areas and street intersections.

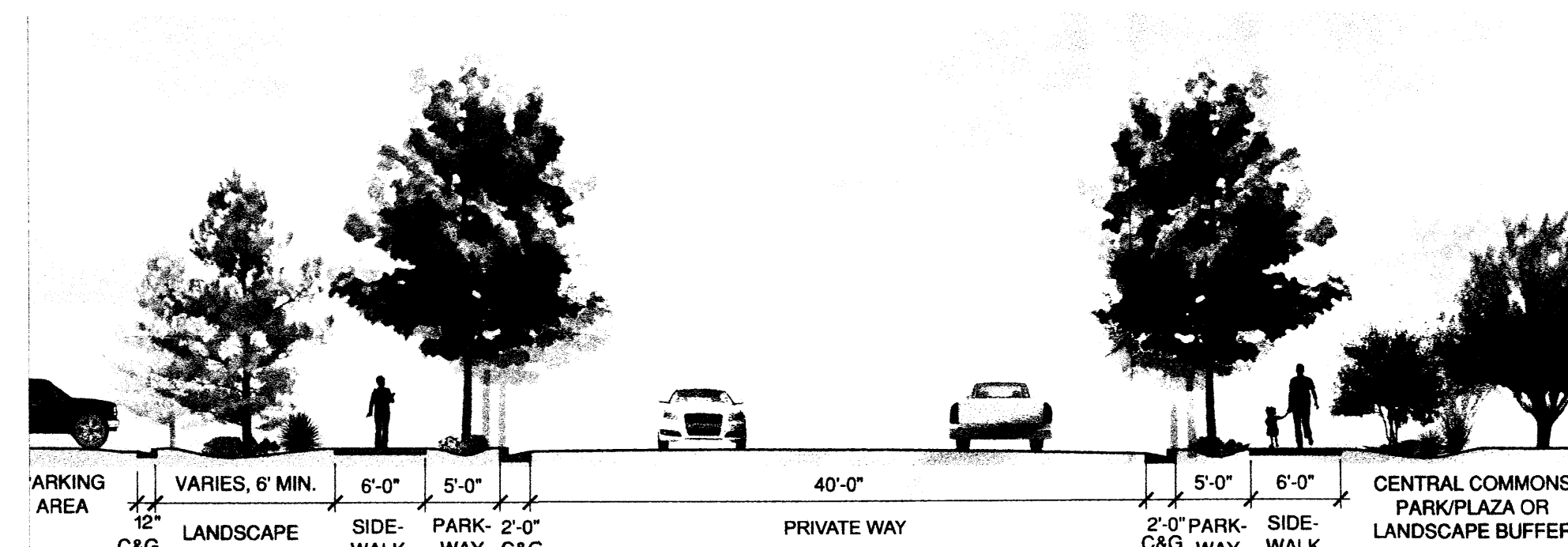
C. OFF-STREET PARKING AREA LANDSCAPING

- Trees are required in and around off-street parking areas to provide shade and relieve the adverse visual impact of pavement and parked cars (COA Landscape Ordinance). Parking lot trees shall be "Shade Trees" (see Plant Palette, Sheet A-703). Quantity and distribution of trees shall be as follows:
 - A minimum of one tree per ten parking spaces shall be provided (COA Landscape Ordinance).
 - All tree planter wells shall be a minimum of 64 square feet per tree.
 - A minimum of 75 percent of the required parking area trees shall be deciduous canopy-type shade trees, capable of achieving a mature canopy diameter of at least 25 feet (COA Landscape Ordinance).
 - No parking space shall be more than fifty feet from a tree (North I-25 Sector Development Plan 1998).
- Parking and drive areas shall have openings at curbs to adjacent landscape for the absorption of water run-off through bio-swales.

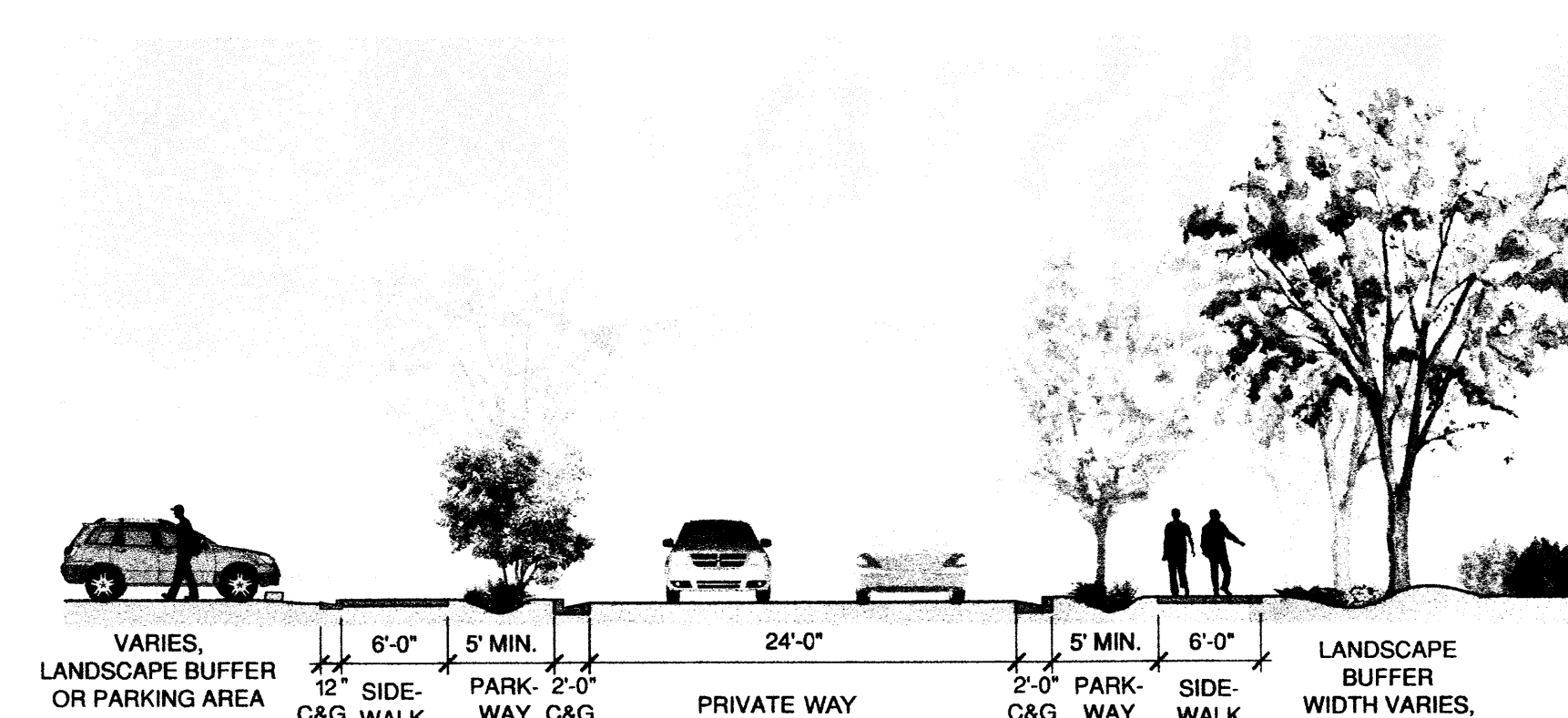
D. PRIVATE WAYS LANDSCAPING

Street trees, sidewalks, and screening of parking and maintenance areas shall be provided along private ways within the site as follows:

- Street trees meeting the requirements of the COA Street Tree Ordinance shall be provided along private way frontages.
- Sidewalks shall be a minimum width of six feet and shall be provided on both sides of the private way.
- Parking and maintenance areas adjacent to private ways shall be screened with landscaping, berming, or a combination thereof.



PRIMARY PRIVATE WAY - TYPICAL SECTION



SECONDARY PRIVATE WAY - TYPICAL SECTION

E. LOW EMISSION & FUEL EFFICIENT VEHICLE PARKING

Each tract shall provide preferred parking spaces for 5% of total vehicle parking. The meaning of "preferred" parking for low emission vehicles (LEV) and fuel efficient vehicles shall be defined as parking closest to a building's main entrance.

F. DRIVE ACCESS

Drive access cuts to lots from internal streets to be determined during individual site design, 24' wide typical.

G. RETAIL LOCATIONS ALONG PAN AMERICAN FREEWAY

There is to be no parking between building elevations and the Pan American Freeway. primary parking, service, storage, and loading areas are to be on the side opposite of Pan American freeway (west side).

6- LANDSCAPING

A. INTRODUCTION

The following Landscape Design Standards are intended to create places of a human scale that promote physical and emotional well-being for people at the North I-25 Corporate Center. In keeping with the North I-25 Sector Development Plan (1998), these Landscape Design Standards are intended to further the following objectives:

- Reduce adverse visual and noise conflicts from traffic and adjacent developments.
- Encourage an identifiable character by ensuring visual continuity within the plan area.
- Ensure shade for pedestrian areas and buildings during the summer months

Where the Landscape Design Standards are based on an applicable code or ordinance, the code or ordinance is cited.

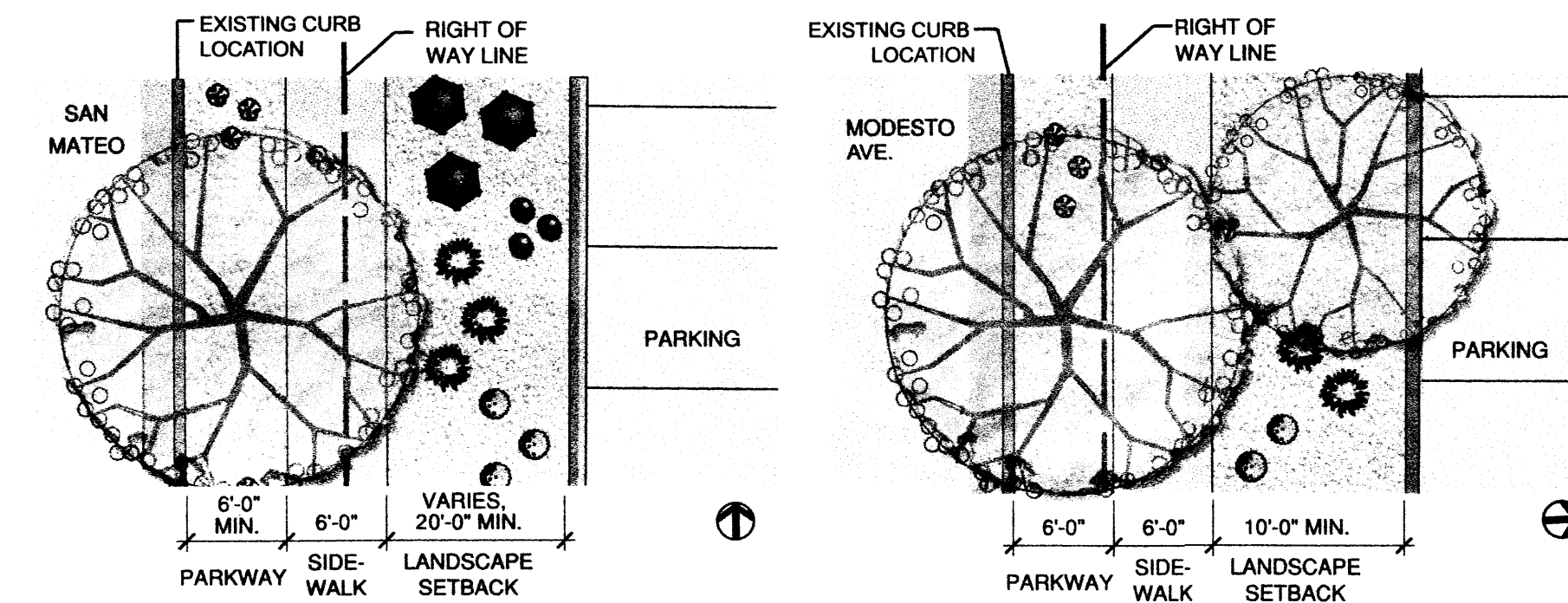
B. LANDSCAPE AREA REQUIREMENTS

- Entire Plan Area:
Landscape area for the entire plan area shall be a minimum of 20 percent of the net plan area.
- Individual Tracts:
Landscape area at individual tracts shall be a minimum of 20 percent of the net tract area.

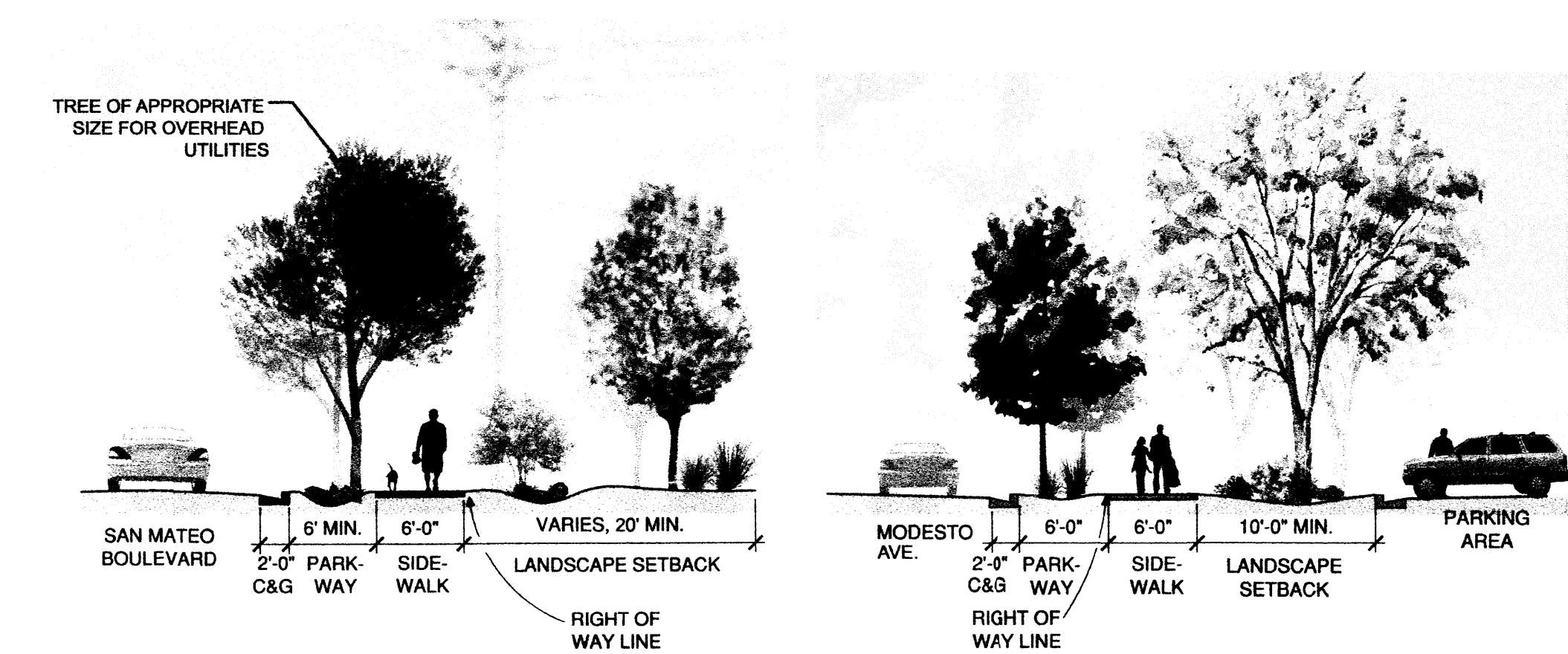
C. LANDSCAPE SETBACK AREAS

A minimum ten-foot wide landscape setback area shall be provided and maintained adjacent to the right-of-way line on San Mateo Blvd., San Diego Ave., Modesto Ave., and the Pan American Freeway (North I-25 Sector Development Plan 1998).

- The landscape setback area shall be incorporated into the overall landscaping plan for the site and contribute to the visual continuity of the street.
- The setback area shall be landscaped with a combination of trees, shrubs, and vegetative groundcover.
- The landscape setback area shall include street trees in compliance with the COA Street Tree Ordinance.



LANDSCAPE SETBACK AT SAN MATEO BLVD. - PLAN VIEW **LANDSCAPE SETBACK AT MODESTO AVE. - PLAN VIEW**



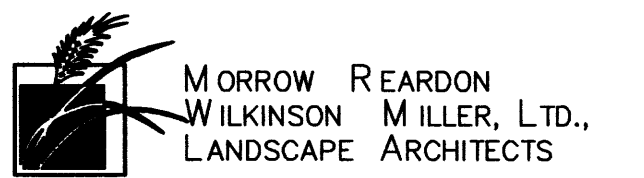
LANDSCAPE SETBACK AT SAN MATEO BLVD. - TYPICAL SECTION **LANDSCAPE SETBACK AT MODESTO AVE. - TYPICAL SECTION**



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MORROW R EARDON
WILKINSON MILLER, L.D.,
LANDSCAPE ARCHITECTS

NORTH I-25 CORPORATE CENTER

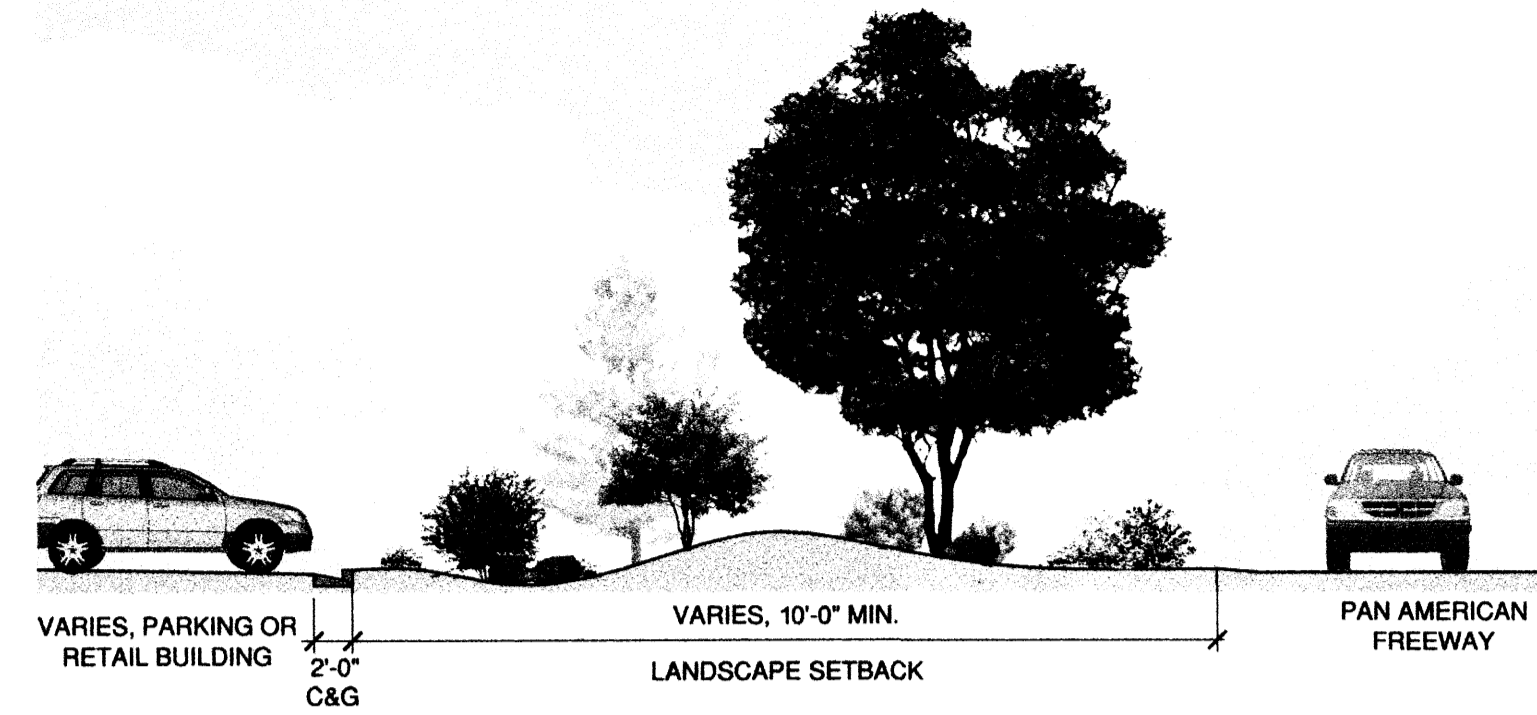
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	5/29/09	REVISED EPC SUBMITTAL
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	4/10/09	REVISED EPC SUBMITTAL
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CHECKED BY:	JLL

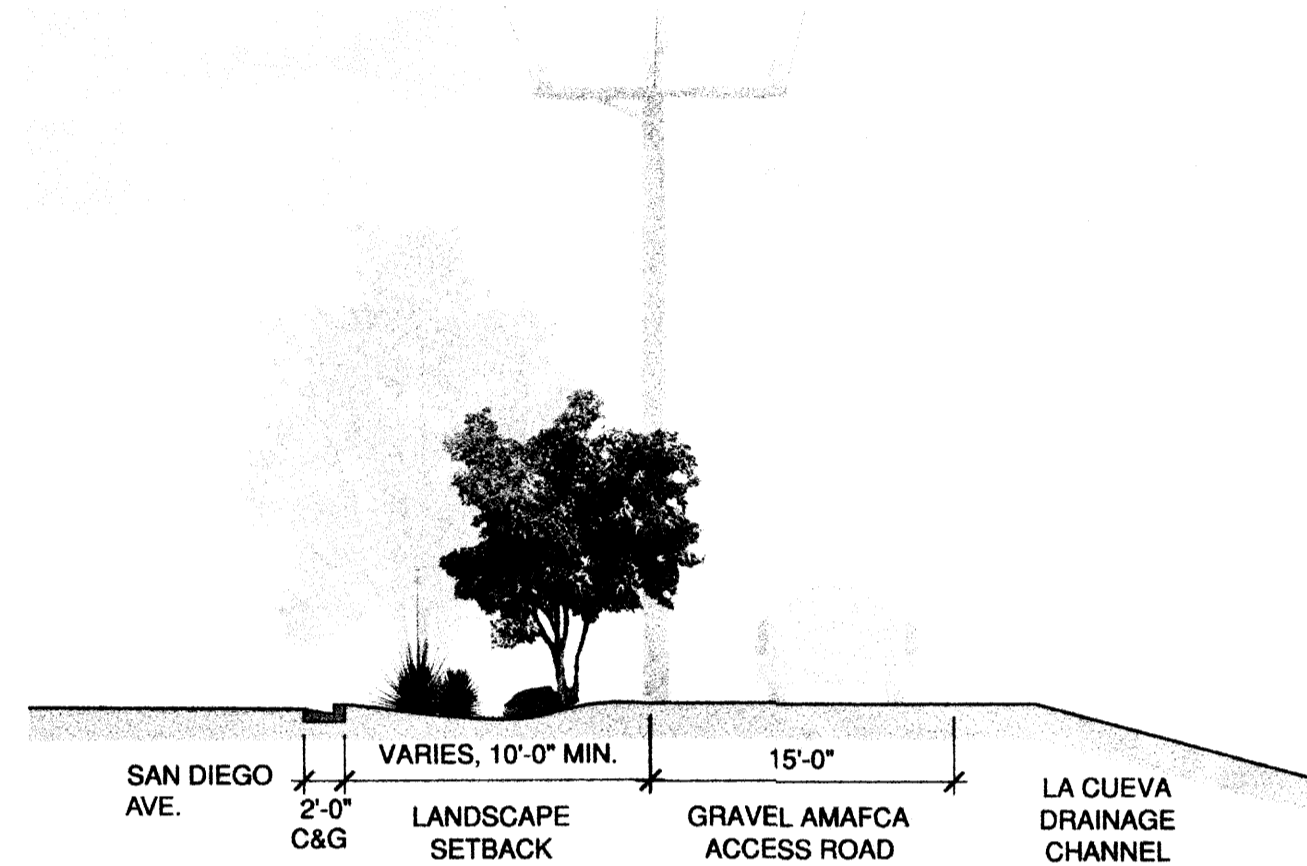
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DESIGN STANDARDS

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6- LANDSCAPING



LANDSCAPE SETBACK AT PAN AMERICAN FREEWAY - TYPICAL SECTION



LANDSCAPE SETBACK AT SAN DIEGO AVE. - TYPICAL SECTION

D. STANDARD LANDSCAPE BUFFERS

1. Entire Plan Area

Landscape buffer areas shall be provided to separate off-street parking and circulation areas from front, side, and rear boundaries of premises. Because this site is a planned development area controlled by site development plans, these requirements pertain to the edges of the entire area of the planning site (COA Landscape Ordinance). Specific required landscape buffer locations and minimum widths for the entire site shall be as follows:

- A. Front buffer width along San Mateo shall be a minimum of twenty feet from the property line.
- B. Side buffer width along San Diego Ave. and Modesto Ave. shall be a minimum of ten feet from the property line.
- C. Rear buffer width along the Pan American Freeway shall be a minimum of ten feet from the property line.

2. Individual Tracts

Landscape buffers for individual tracts shall comply with (and in some cases, exceed) the requirements of the COA Landscape Ordinance as follows:

- A. Front buffer width shall be a minimum of ten feet from the tract line for tracts of three acres or less, increasing at the rate of one foot in width per two-acre increase in tract size.
- B. Side buffer width shall be a minimum of six feet from the tract line.
- C. Rear buffer width shall be a minimum of ten feet from the tract line.

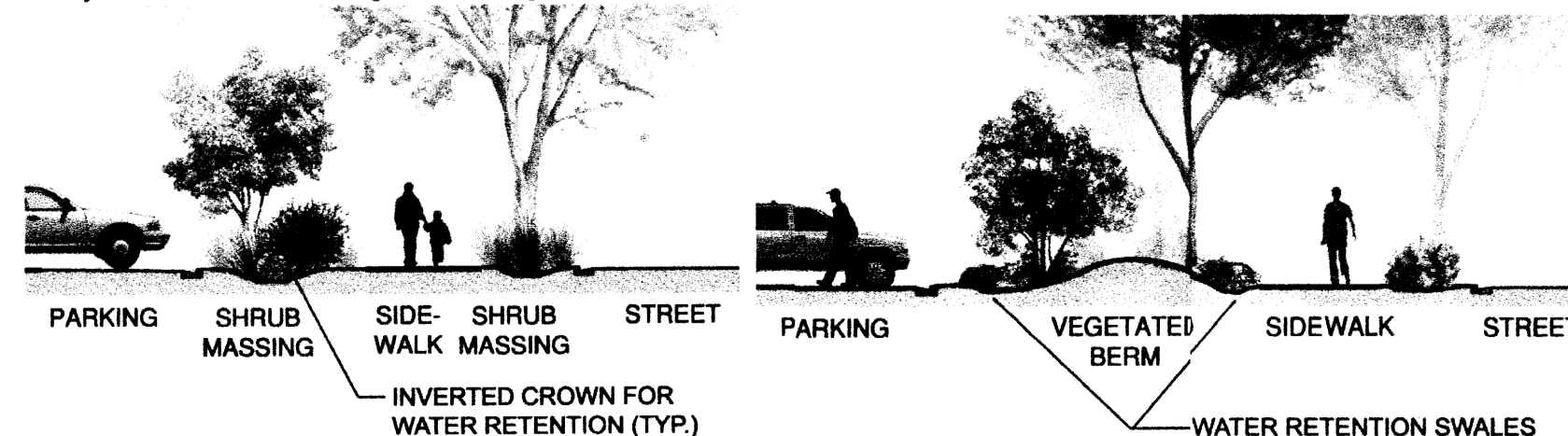
Landscape buffers may be crossed by driveways. No parking is permitted within a required landscape buffer area. Landscape approved within adjacent public right-of-way may be counted toward this requirement if there is no existing or planned public sidewalk between such landscaping and the premises (COA Landscape Ordinance).

E. STREET TREES

Street trees meeting the requirements of the COA Street Tree Ordinance shall be provided along San Mateo Blvd., San Diego Ave., Modesto Ave., and the Pan American Freeway. See this sheet for Plant Palette.

F. SCREENING

In addition to standard landscape buffers, parking and maintenance areas shall be screened from adjacent roadways with shrub massing or berming, or a combination thereof.



SCREENING WITH SHRUB MASSING

SCREENING WITH BERMING

6- LANDSCAPING

G. LANDSCAPING ADJACENT TO THE LA CUEVA ARROYO

Landscaping adjacent to the La Cueva Arroyo shall comply with the requirements of the Facility Plan for Arroyos. Plants proposed for the areas adjacent to the arroyo right-of-way shall be low-maintenance, native or naturalized shrubs and vegetative groundcovers, as well as clusters of drought resistant shade trees. Plants shall be selected from the plant palette on this sheet. Landscaping adjacent to the concrete lined La Cueva channel shall be designed and installed to serve as a screening element.

H. LANDSCAPING ALONG THE PAN AMERICAN FREEWAY

Landscaping along the Pan American Freeway shall comply with the requirements of the New Mexico Department of Transportation.

I. GENERAL LANDSCAPING REQUIREMENTS

1. The minimum size of tree planter wells shall be 64 sf.
2. If allowed by appropriate government authorities, the applicant shall landscape the I-25 right-of-way in front of the retail buildings.
3. There will be no pedestrian sidewalks along I-25.

PLANT PALETTE

Plants for common areas and for individual tracts shall be selected from the following list in order to provide a variety of seasonal color and texture, while creating a cohesive, campus-like setting. The plant palette consists of hardy plants that are native to the region or well adapted to local climate and soil conditions. No more than two highly allergenic trees (as defined by the COA Pollen Ordinance) shall be allowed on the plant palette for individual tracts.

Note: Alternative varieties to specified plant varieties are allowed pending approval by the City of Albuquerque.

COMMON NAME	BOTANICAL NAME	SIZE INSTALLED	MATURE SIZE	REMARKS	WATER USE
SHADE TREES					
Arizona Sycamore	<i>Platanus occidentalis</i>	2" cal., 12'-14' ht.	50' ht. & spd	30" Box	M
Autumn Purple Ash	<i>Fraxinus americana</i> 'Autumn Purple'	2" cal., 12'-14' ht.	40' ht. & 25' spd	30" Box	M
Chinese Pistache	<i>Pistacia chinensis</i>	2" cal., 12'-14' ht.	40' ht. & 25' spd.	30" Box	M
Honey Locust	<i>Gleditsia triacanthos inermis</i>	2" cal., 12'-14' ht.	50' ht. and 25' spd.	30" Box	M
Lacebark Elm	<i>Ulmus parvifolia</i>	2" cal., 12'-14' ht.	30' ht. & 25' spd.	30" Box	M
Raywood Ash	<i>Fraxinus oxycarpa</i> 'Raywood'	2" cal., 12'-14' ht.	30' ht. & 25' spd.	30" Box	M
Western Hackberry	<i>Celtis occidentalis</i>	2" cal., 12'-14' ht.	30' ht. & spd.	30" Box	M
Shumard Oak	<i>Quercus shumardii</i>	2" cal., 10'-12' ht.	30' ht. & 25' spd.	30" Box	L
ACCENT TREES					
Crabapple	<i>Malus ioensis</i> 'Plena'	2" cal., 10'-12' ht.	25' ht. & 20' spd.	30" Box	M
Desert Willow	<i>Chilopsis linearis</i>	8'-10' ht.	25' ht. & 20' spd.	30" Box, multi-trunk	M
Eastern Redbud	<i>Cercis canadensis</i>	2" cal., 10'-12' ht.	20' ht. & 15' spd.	30" Box	M
Ornamental Pear	<i>Pyrus calleryana</i> 'Aristocrat' & 'Chanticleer'	2" cal., 10'-12' ht.	25' ht. 20' spd.	30" Box	M
Mexican Elder	<i>Sambucus mexicana</i>	5'-7' ht.	15' ht. & spd.	24" Box, multi-trunk	L
Purpleleaf Plum	<i>Prunus cerasifera</i>	2" cal., 10'-12" ht.	20' ht. & spd.	30" Box	M
Vitex	<i>Vitex agnus-castus</i>	5'-7' ht.	15' ht. & spd.	15-Gal.	L
Washington Hawthorn	<i>Crataegus phaenopyrum</i>	2" cal., 10'-12' ht.	20' ht. & spd.	30" Box	M
EVERGREEN TREES					
Austrian Pine	<i>Pinus nigra</i>	10'-12' ht.	30' ht. & 20' spd.	B & B	M
Deodar Cedar	<i>Cedrus deodara</i>	10'-12' ht.	40' ht. & 30' spd.	B & B	M
Limber Pine	<i>Pinus flexilis</i>	10'-12' ht.	20' ht. & spd.	B & B	M
Scotch Pine	<i>Pinus sylvestris</i>	10'-12' ht.	30' ht. & 20' spd.	B & B	M
VERTICAL SHRUBS					
Bird of Paradise	<i>Caesalpinia gilliesii</i>	Min. 18" ht.	6' ht. & spd.	5-Gal.	L
Butterfly Bush	<i>Buddleia sp.</i>	Min. 18" ht.	6' ht. & spd.	5-Gal.	M
Golden Currant	<i>Ribes aureum</i>	Min. 18" ht.	5' ht. & spd.	5-Gal.	L
Mountain Mahogany	<i>Cercocarpus montanus</i>	Min. 18" ht.	10' ht. & spd.	5-Gal.	M
New Mexico Olive	<i>Foresteria neomexicana</i>	5' - 7' ht.	15' ht. & 10' spd.	15-Gal.	M
Smoke Bush	<i>Cotinus coggygria</i>	2" cal., 8'-10" ht.	20' ht. & spd.	30" Box	M
LARGE SHRUBS/ORNAMENTAL GRASSES					
Apache Plume	<i>Fallugia paradoxa</i>	Min. 18" ht.	5' ht. & spd.	5-Gal.	L
Arp Rosemary	<i>Rosmarinus officinalis</i> 'Arp'	Min. 18" ht.	4' ht. & 3' spd.	5-Gal.	M
Blue Mist Spirea	<i>Caryopteris clandonensis</i>	Min. 18" ht.	4' ht. & spd.	5-Gal.	M
Maiden Grass	<i>Miscanthus sinensis</i> 'Gracillimus'	Min. 18" spd.	4' ht. & spd.	5-Gal.	M
Red Yucca	<i>Hesperaloe parviflora</i>	Min. 18" ht.	4' ht. & spd.	5-Gal.	L
Rubber Rabbitbrush	<i>Chrysothamnus nauseosus</i>	Min. 18" ht.	6' ht. & spd.	5-Gal.	L
Sotol	<i>Dasylirion wheeleri</i>	Min. 12" ht.	4' ht. & spd.	5-Gal.	L
Switchgrass	<i>Panicum virgatum sp.</i>	Min. 18" ht.	4' ht. & 2' spd.	5-Gal.	M
Threeleaf Sumac	<i>Rhus trilobata</i>	Min. 18" ht.	5' ht. & spd.	5-Gal.	L
Yellow Yucca	<i>Hesperaloe parviflora</i> 'Yellow'	Min. 18" ht.	4' ht. & spd.	5-Gal.	L
SMALL SHRUBS /ORNAMENTAL GRASSES					
Blue Avena	<i>Helictotrichon sempervirens</i>	Min. 8" ht.	2' ht. & spd.	5-Gal.	M
Cherry Sage	<i>Salvia greggii</i>	Min. 12" ht.	3' ht. & spd.	5-Gal.	M
Compact Mahonia	<i>Mahonia aquifolium</i> 'Compacta'	Min. 12" spd.	2' ht. & 4' spd.	5-Gal.	M
Dwarf Rabbitbrush	<i>Chrysothamnus nauseosus</i>	Min. 8" ht.	2' ht. & spd.	5-Gal.	L
Feather Grass	<i>Stipa capillata</i>	Min. 8" ht.	18" ht. & spd.	5-Gal.	M
Foerster's Grass	<i>Calamagrostis acutiflora</i> 'Karl Foerster'	Min. 8" spd.	2' ht. & spd.	5-Gal.	M
Gro-Low Sumac	<i>Rhus aromatica</i> 'Gro-low'	Min. 12" spd.	18" ht. & 3' spd.	5-Gal.	M
Lavender	<i>Lavandula angustifolia sp.</i>	Min. 6" ht.	2'-4' ht. & spd.	1-Gal.	L
'Miss Huff' Lantana	<i>Lantana x 'Miss Huff'</i>	Min. 18" spd.	12" ht. & 4' spd.	5-Gal.	L
Nandina	<i>Nandina domestica sp.</i>	Min. 12" ht.	2'-4' ht. & spd.	5-Gal.	M
Pale Leaf Yucca	<i>Yucca pallida</i>	Min. 8" ht.	2' ht. & 3' spd.	5-Gal.	L
Regal Mist	<i>Muhlenbergia capillaris</i> 'Regal Mist'	Min. 10" spd.	3' ht. & spd.	5-Gal.	M
Shrubby Cinquefoil	<i>Potentilla fruticosa</i>	Min. 12" ht.	3' ht. & spd.	5-Gal.	L
Softblade Yucca	<i>Yucca recurvifolia</i>	Min. 18" ht.	3' ht. & spd.	5-Gal.	M
GROUNDCOVERS					
Bearberry Cotoneaster	<i>Cotoneaster dammeri</i>	Min. 12" spd.	2' ht. & 10' spd.	5-Gal.	M
Blue Fescue	<i>Festuca glauca</i> 'Boulder Blue'	Min. 6" ht.	12" ht. & spd.	1-Gal.	L
Blue Leadwort	<i>Cerastostigma plumbaginoides</i>	Min. 12" spd.	6" ht. & 2' spd.	1-Gal.	M
Buffalo Juniper (Female)	<i>Juniperus sabina</i> 'Buffalo'	Min. 12" spd.	12" ht. & 6' spd.	5-Gal.	M
CACTI					
Claret Cup Cactus	<i>Echinocerus triglochidiatus</i>	Min. 3" ht.	6" ht. & 8" spd.	1-Gal.	L
Royal George Spiny Hedgehog	<i>Echinocerus coccineus</i>	Min. 10" ht.	12" ht., 18" spd.	1-Gal.	L
Spineless Prickly Pear	<i>Opuntia stricta</i>	Min. 18" ht.	4' ht. & spd.	5-Gal.	L
PERENNIALS					
Autumn Joy Sedum	<i>Sedum telephium</i> 'Autumn Joy'	Min 6" ht.	2' ht. & spd.	1-Gal.	L
Bush Penstemon	<i>Penstemon ambiguus</i>	Min. 6" ht.	2' ht. & spd.	1-Gal.	L
Catmint	<i>Nepeta faassenii</i>	Min. 6" spd.	2' ht. & spd.	1-Gal.	M
Dragon's Blood Sedum	<i>Sedum spurium</i> 'Dragon's Blood'	Min. 6" spd.	10" ht. & 18" spd.	1-Gal.	L
Globe Mallow	<i>Sphaeralcea munroana</i>	Min. 6" ht.	3' ht., 2' spd.	1-Gal.	L
Mexican Primrose	<i>Oenothera speciosus</i> 'Rosea'	Min. 4" ht.	10" ht. & 18" spd.	1-Gal.	M
Paperflower	<i>Psilostrophe tagetina</i>	Min. 6" ht.	12" ht. & 2' spd.	1-Gal.	M
Pineleaf Penstemon	<i>Penstemon pinifolius</i>	Min. 6" ht.	15" ht. & 18" spd.	1-Gal.	L
Red Hot Poker	<i>Kniphofia uvaria</i>	Min. 6" ht.	3' ht. & spd.	1-Gal.	L
Sundrops	<i>Calylophus hartwegii</i>	Min. 6" spd.	15" ht. & 18" spd.	1-Gal.	L
Yarrow	<i>Achillea filipendulina</i>	Min. 6" ht.	3' ht. & 2' spd.	1-Gal.	L
TURF *					
Park Blend	<i>Poa pratensis, Festuca eliator & Lolium perenne</i>		Sod		H
Buffalo Grass Blue	<i>Buchloe dactyloides & Bouteloua gracilis</i>		Sod		L
Grama Mix					

* High water use turf shall be limited to no more than 40% of the turf blend used.

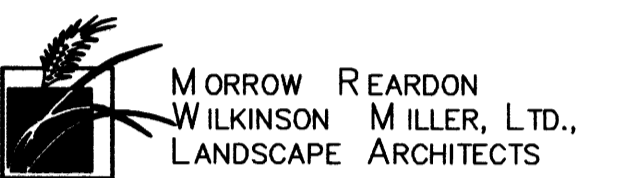


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MORROW REARDON
WILKINSON MILLER, LTD.,
LANDSCAPE ARCHITECTS

NORTH I-25
CORPORATE CENTER

MARK	DATE	DESCRIPTION
	3/2/10	DRB SUBMITTAL
	12/16/09	REVISED EPC SUBMITTAL
	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:	N-25-CC
CAD DWG FILE:	A-703 DESIGN GUIDELINES.DWG
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CHECKED BY:	JLL

SHEET TITLE
DESIGN STANDARDS

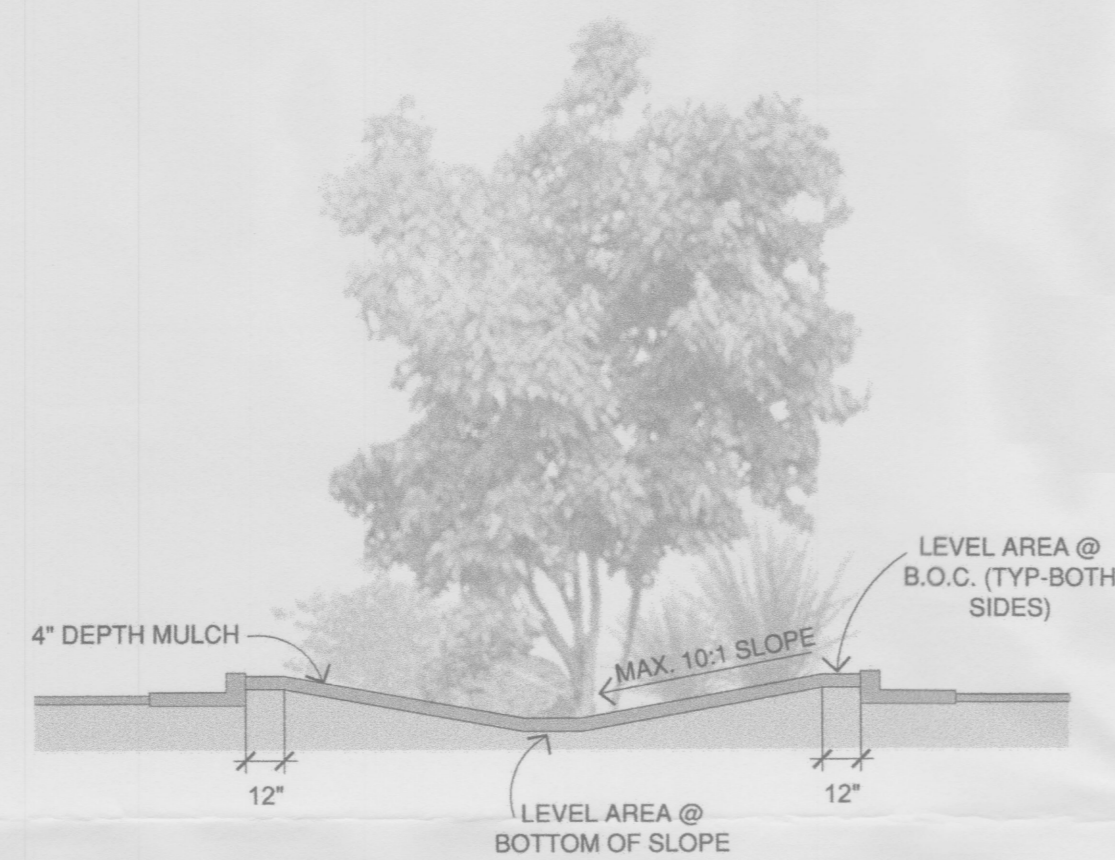
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7- SUSTAINABILITY:

SUSTAINABLE CHARACTERISTICS OF THE SITE SHALL INCLUDE:

- Site selection - this site was previously developed and is not in an ecologically sensitive area such as a prime farm land, wetland or habitat.
- Brownfield redevelopment - because the site has been complicated by real and perceived health and human safety issues due to the existing landfill, the project has entered the New Mexico Voluntary Remediation program following the remediation of the site.
- An existing city bus stop is located on the site on San Mateo.
- New construction shall minimize light trespass from the buildings and site, reduce sky-glow to increase night sky access, improve nighttime visibility through glare reduction, and reduce development impact on nocturnal environments. To accomplish this, all site and building mounted luminaires shall produce a maximum initial illuminance value no greater than 0.20 horizontal and vertical footcandles at the site boundary and no greater than 0.01 horizontal footcandles 15 feet beyond the site. No more than 5% of the total initial designed fixture lumens are emitted at an angle of 90 degrees or higher from nadir (straight down).
- In the Southwest, water is our most precious and finite resource. Several water conservation techniques shall be employed at the North I-25 Corporate Center, including irrigation efficiency, inverted crowns, erosion control, soil amendments (to decrease runoff), and a drought-tolerant plant palette.

- Landscape areas shall be irrigated with a highly efficient automatic irrigation system.
- Inverted crowns and dry streambed features such as bioswales shall be used to capture rainwater. These areas will not be used to retain or detain stormwater runoff from other areas on the site, but will capture rainfall to supplement the automatic irrigation system and decrease the total amount of stormwater runoff entering the city stormwater drainage system.



INVERTED CROWN AT PARKING MEDIAN

- Stable slopes decrease erosion and stormwater runoff and increase rainwater infiltration on site. Grade changes shall be addressed with stabilized landscaped slopes that compliment the overall landscape theme.
- Soil in landscape areas shall be amended with organic materials and soil conditioner which will increase moisture retention and reduce runoff.
- Plants shall be native to the region or well adapted to local climate and soil conditions. Plants shall be low to medium water use.
- Exterior site management shall reduce environmental impacts of landscape installation and maintenance by developing exterior site management plans that include:
 - Limited and target-specific herbicide application following ipm procedures.
 - Application of slow-release fertilizer based on soil analysis
 - Installation of soil amendments including organic compost, horticultural volcanic rock, and granulated sulfur soil conditioner.
 - Application of mycor-rhizal fungi and beneficial bacteria to help root growth
 - Organic fertilization.
 - Mulching and composting of landscape debris.
 - Application of pre-emergent herbicides.

- Low emission & fuel efficient vehicle parking - each tract shall provide preferred parking spaces for 5% of total vehicle parking.
- Each tract shall provide bicycle storage for 3% of occupants. In addition, each tract must also provide additional indoor bicycle storage. Indoor bicycle storage for each tract shall be provided at the rate of 1 space for every 15,000 sq ft of GLSF. Each tract shall also provide shower and changing rooms for .5% of building occupants as determined by applicable building codes. Shower and changing room requirement does not apply to tracts having retail and restaurant uses.
- All roofing materials shall be highly reflective having a solar reflectance index value of 75 or greater for low slope roofs and 29 for steep slope roofs. Highly reflective roofing materials shall be hidden behind parapets and not be visible to passers by or pedestrians.
- All buildings shall use low flow and high efficiency fixtures.
- Smoking in buildings shall be prohibited and smoking areas shall be located at least 25 feet from entries, air intakes, and operable windows.
- Each tract shall provide a recycle collection space that shall be accessible to all building tenants.
- 75% of construction waste shall be diverted from landfills.
- Storm water harvesting - all sites shall have water harvesting features
- Storm water quality - tracts programmed for building development shall treat and/or remove surface contaminants dislodged and conveyed by the first 0.25 inches ("first flush") of the storm event.
- Demolished asphalt parking areas and concrete curb, gutter and sidewalks currently located on the site will be recycled & kept on site for use in future development as base course for drive aisles and parking areas.
- Demolished building concrete and CMU block from the existing building located on the site will be crushed and reused on site as base course for drive aisles and parking areas.
- All new buildings 200,000 square feet and under will be at a minimum LEED certifiable.
- As outlined in these Design Standards, innovative and sustainable design techniques along with the use of "green" building materials is required.

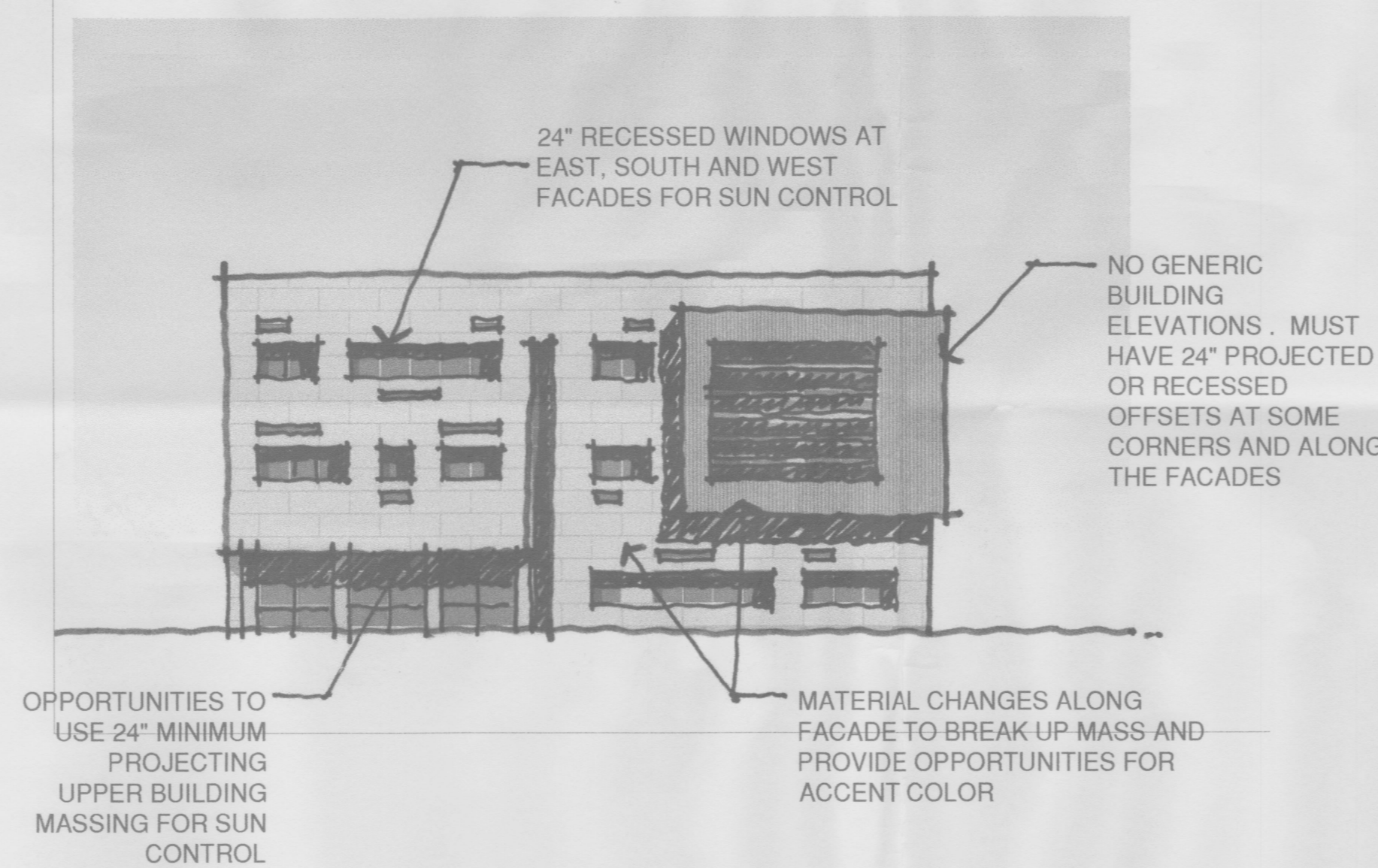
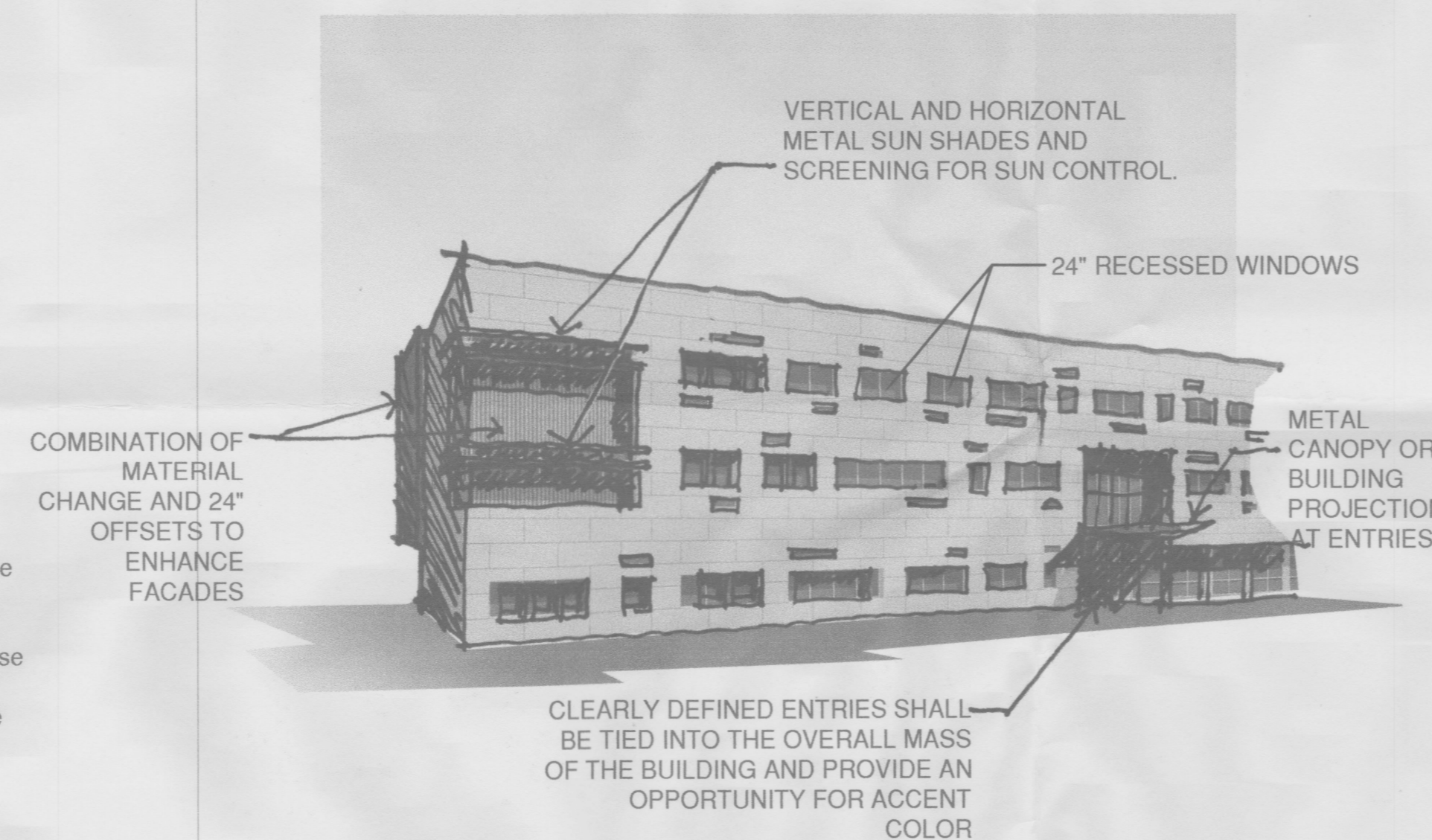
8- ARCHITECTURAL EXPRESSION

A. ARCHITECTURAL STYLES:

The goal of the architectural standards is not to limit design creativity, but to provide the framework for high quality design. The architectural theme is contemporary. No pueblo revival, territorial or northern NM or other "historical styles" of architecture are allowed. Generic franchise design is prohibited. Building design shall be contextual to land forms, adjacent buildings and these design standards.

B. BUILDING DESIGN, MATERIALS, AND COLORS

- Flat roofs concealed by parapets shall be predominant.
- Metal may be used as a roofing material. Metal roofs shall be corrugated or standing seam. At accent areas, metal roof colors shall be in shades of gray, red, green, brown, or silver. Metal roofs shall be non reflective where seen by pedestrians.
- Exterior building materials shall be consistent throughout the subdivision. Acceptable materials include earth tone stuccos (shades of brown, gray, green, tan and red), stone, non-reflective metal panels, poured concrete panels, non-mirrored curtain wall and other matt finish earth tone materials. Accent colors may be used to highlight key architectural features that encompass no more than 20% of the area of the elevation to which they are applied.
- The basic color concept is to provide an overall appearance of predominantly earth tone buildings with small areas of accent color to provide interest and variety between individual buildings.
- Shading devices above windows or recessed windows of at least 24" are required at East, South and West facades.
- Windows shall be commercial grade. Glass colors are limited to clear, light green, grey, bronze, and light blue.
- Long, uninterrupted exterior walls is prohibited on all structures.
- All canopies and ancillary structures shall be architecturally integrated with main buildings.
- ATM's shall be architecturally integrated with building design, materials and colors.
- No generic building elevations shall be permitted.
- Buildings adjacent to the arroyo are required to have windows on the north side that overlook the arroyo.
- Quality treatment shall be provided on all elevations of a building.
- All loading areas used for loading and unloading of commercial vehicles shall be setback from the public right of way line and from all property lines to reduce the visual impact of large commercial vehicles and loading areas.



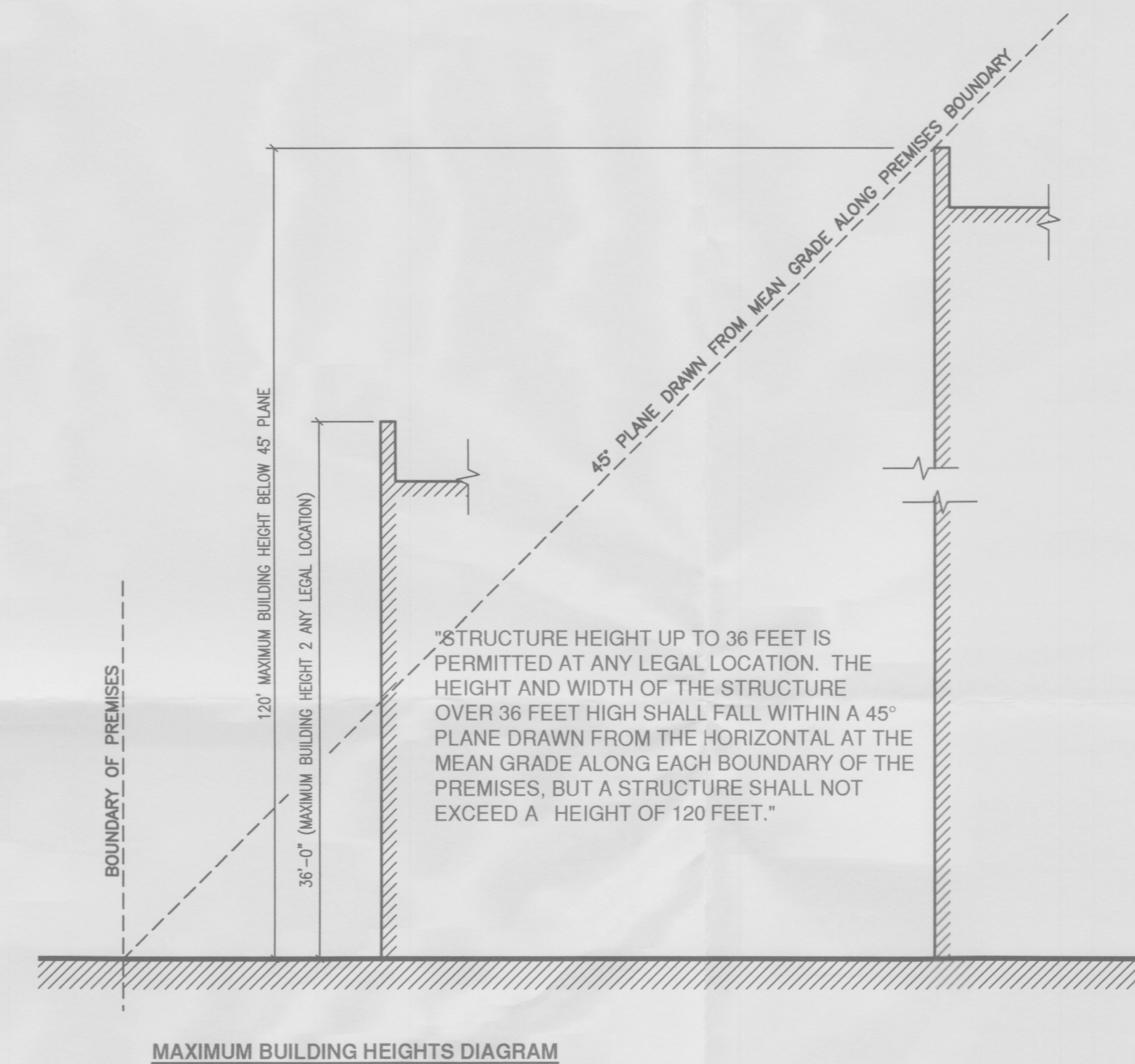
8- ARCHITECTURAL EXPRESSION

C. PROHIBITED DESIGN ELEMENTS:

- Large, blank, unarticulated wall surfaces
- Large, block like structures having long, uninterrupted exterior walls 100 feet or greater.
- Highly reflective windows
- Roofs that are illuminated
- Prefab metal buildings.
- Gutters or down spouts located on the front elevations of buildings.
- Pitched roofs @ office buildings visible to pedestrians other than @ accent areas.
- Mansard roofs
- Slump block or plain gray CMU block.
- Painted masonry

D. BUILDING HEIGHTS

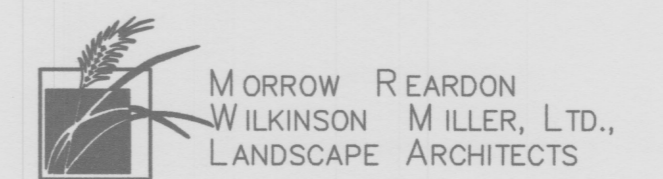
- Maximum building heights are as described in the M-1 zone and as illustrated in the following diagram:



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NORTH I-25
CORPORATE CENTER

MARK	DATE	DESCRIPTION
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	3/31/09	INITIAL EPC SUBMITTAL

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DRAWN BY:	JLL
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SHEET TITLE
DESIGN STANDARDS

A-704

9- SIGNAGE

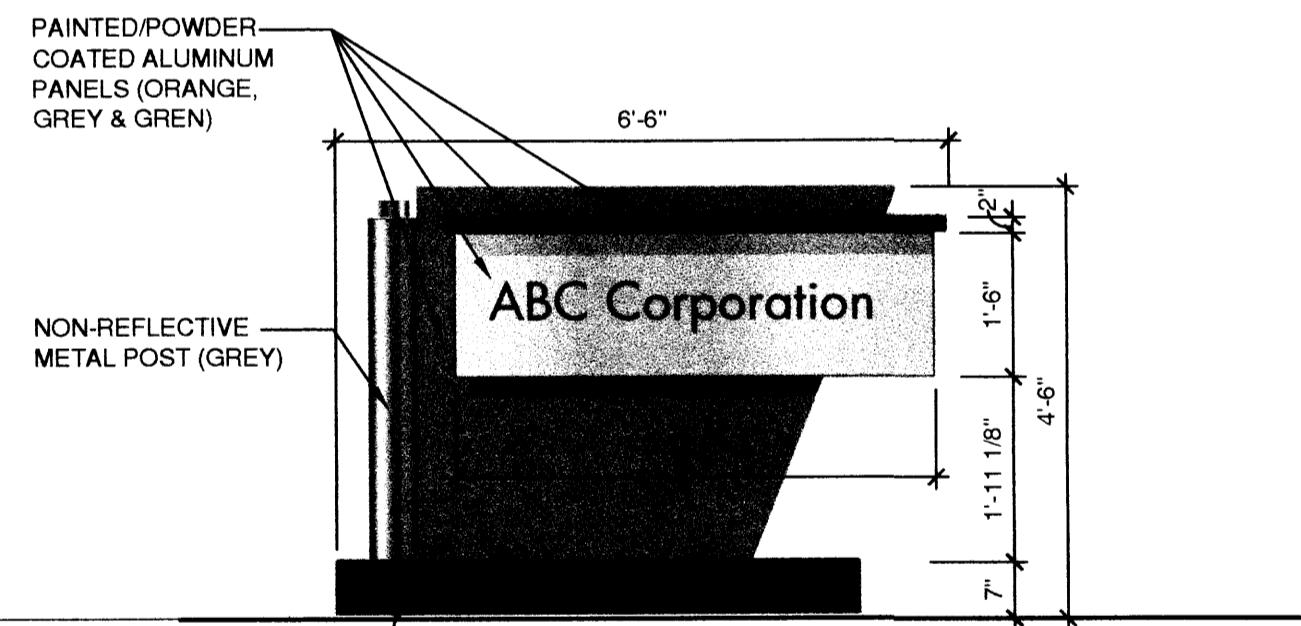
Signage shall enhance the overall attractive character of the complex as well as provide information and direction to pedestrian & vehicular traffic. A common design theme for signage in the plan area will enhance the overall image and emphasize the project. Providing adequate signage is critical to achieve successful wayfinding elements throughout the site. These design standards will be deemed amended if and to the extent of any application and approval duly made to the Zoning Hearing Examiner. All signs must be approved by the North I-25 Corporate Center Architectural Review Committee in order to ensure consistency and reduce visual clutter. Standards and covenants shall include detailed specifications for sign size, type and materials, fabrication details, mounting and installation.

A. GENERAL STANDARDS:

- Signs shall be limited to a maximum of 5 different colors and 3 different types of materials.
- All signage shall be designed with a minimum 70% contrast between graphic and background in all respects.
- Overly ornate or intricate typefaces are prohibited. Overly ornate shall be understood to mean typefaces that are "busy" or not clearly legible and easily interpreted and read.
- Minimize the amount of words on a sign to consist of theme, tenant info and directional information.
- All elements of a sign shall be maintained in a visually appealing manner.
- No sign shall overhang into the public right of way.
- No off premise signs are allowed.
- Signs that flash, blink, move, or have audible sound are not permitted. Portable or roof top signs are not permitted.
- Any sign component and or elements not depicted in the signage standards are not permitted.
- No electronic display panels of any kind shall be allowed.
- Signage shall comply with applicable Federal signage regulations.
- All signage is to be as restricted in the C-2 zone and the general sign regulations of the zoning code.
- All signage shall comply with Zoning Code 14-16-3-2, the Shopping Center regulations.
- Sign lighting will be in accordance with all night sky ordinances and shall comply with Dark Sky's.
- Signs shall be located according to the Signage Location Map, see sheet A-706.
- All signage shall be designed to be consistent and complement the materials, colors and architecture of the building and /or site location.
- Pedestrian oriented signs shall be smaller than vehicle oriented signs. A pedestrian oriented sign is usually read from a distance of 15 to 20 feet.
- Pedestrian crossing signage for the crossing at San Mateo Blvd. And Modesto shall be provided.

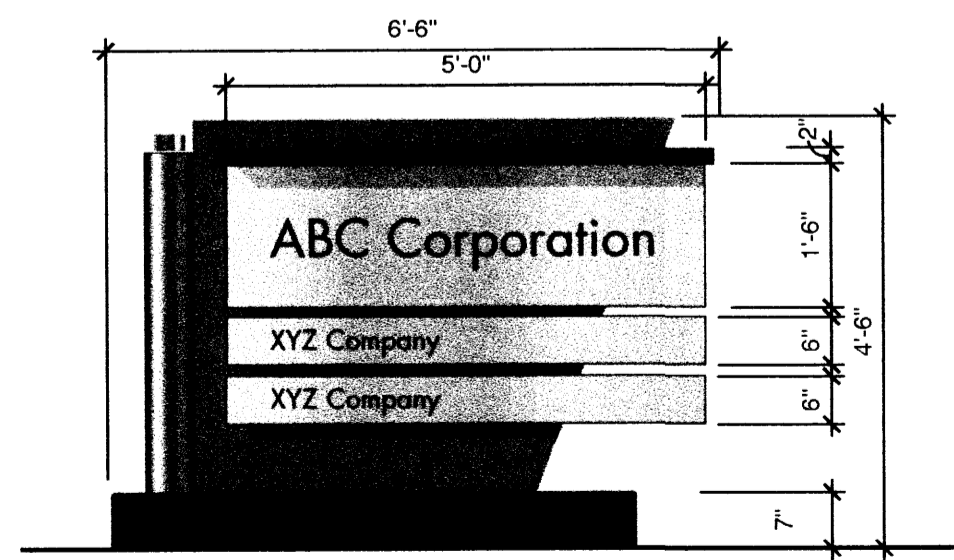
B. FREESTANDING SIGNS

- Freestanding sign quantities and sizes are as allowed by the shopping center regulations (14-16-3-2). Per the shopping center regulations 10 freestanding signs @ a maximum of 150sf each are permitted (does not include private traffic directional signage).
- Free standing signs shall not require any external bracing, angle iron supports, guy wires or similar devices.
- Free-standing signs shall not encroach upon clear-sight triangles.
- Free-standing signage shall be oriented perpendicular to the roadway.
- Free-standing signs shall not be constructed with a single pole base.
- Freestanding signs shall not exceed 26' in height along I-25 and 15' along San Mateo Blvd.
- All freestanding sign lighting shall not be upward facing and shall not create glare on street or adjacent property.
- All freestanding signage shall be of a consistent design, color and materials palette throughout the development.
- Minimum type size shall be 4 inches.



• 5" TYPE
• 7.5 SQ FT SIGN AREA
• 23 SQ FT TOTAL SIGN AREA

**FREESTANDING MONUMENT SIGN
- SINGLE TENANT IDENTIFICATION**



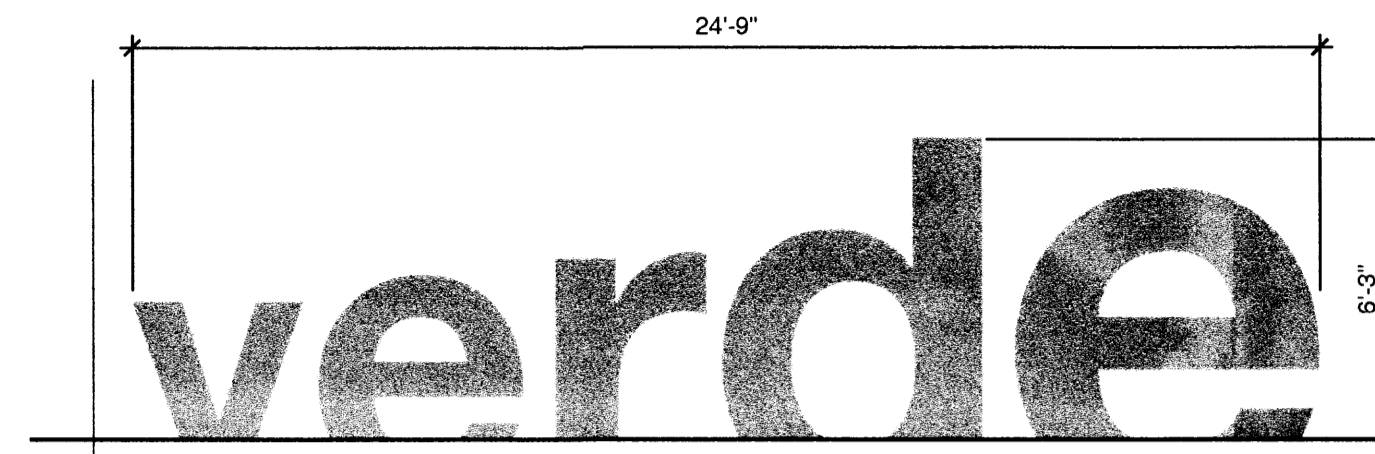
• 5" TYPE (4" MINIMUM)
• 12.5 SQ FT SIGN AREA
• 25 SQ FT TOTAL SIGN AREA

**FREESTANDING MONUMENT SIGN
- MULTIPLE TENANT IDENTIFICATION**

9- SIGNAGE

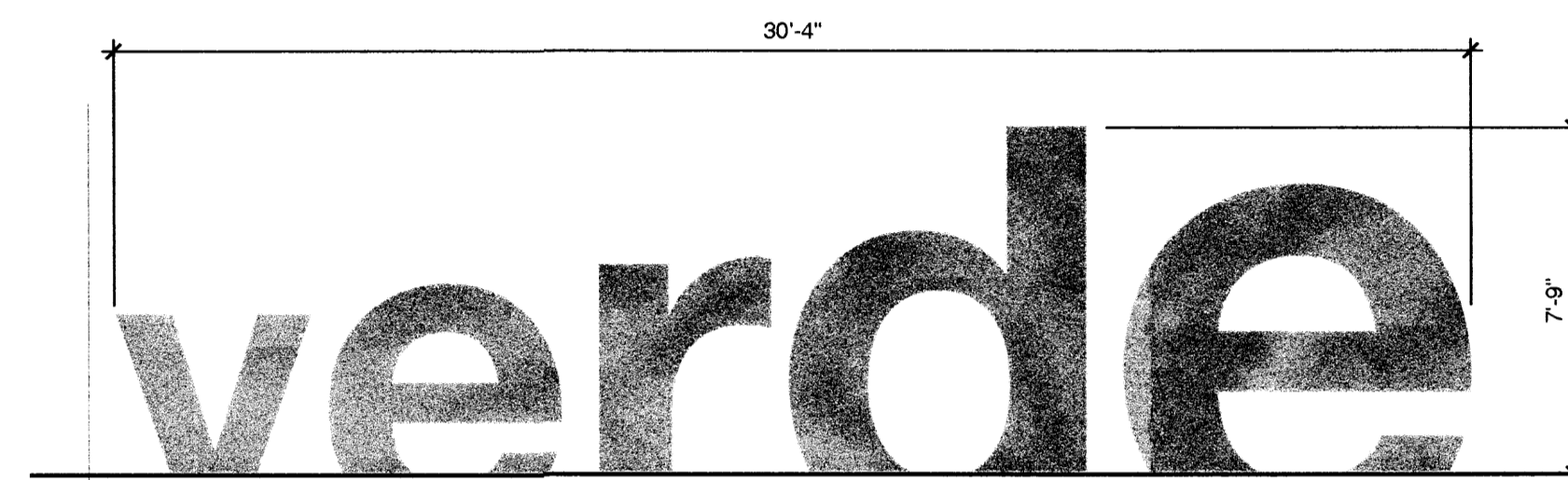
C. PROJECT IDENTIFICATION SIGNAGE:

At select locations as indicated on the site plan, free standing monument signs will serve to identify the entire project. The purpose of these signs is to help communicate to the public the intended name of the project. These signs are to be located along the Pan American Frontage Road in close proximity to Interstate 25 and at the intersection of San Mateo and Modesto. Sign lighting will be ground mounted lights in accordance with all night sky ordinances and shall comply with dark skies.



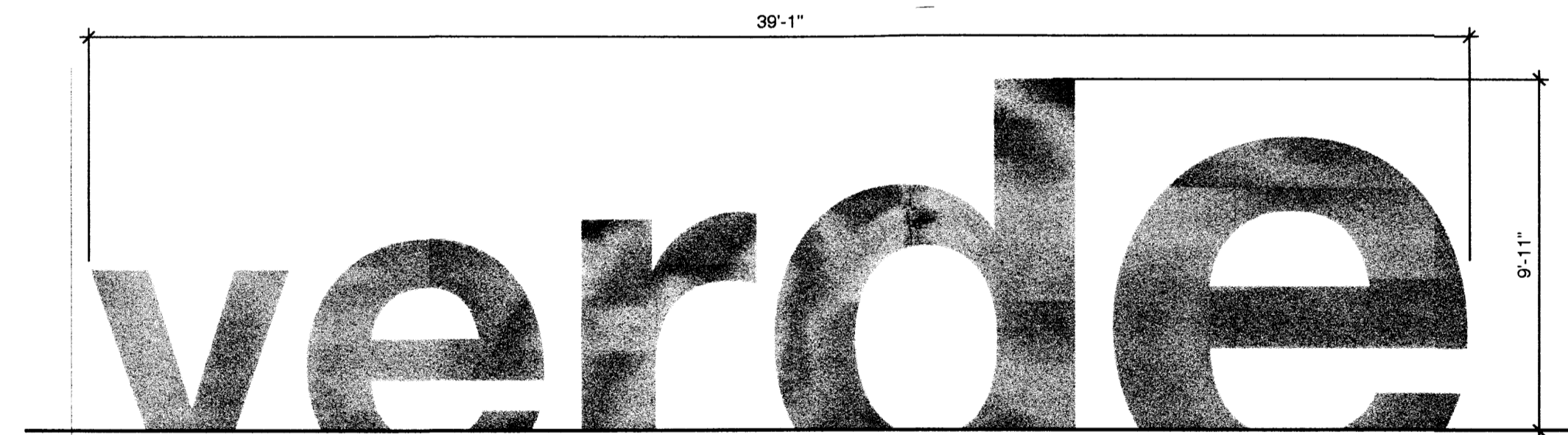
1. FREESTANDING PROJECT IDENTIFICATION SIGNAGE - SIGN AREA = 60 SQ. FT.
LOCATED ON TRACT 19 (MIDDLE OF EASTERN BOUNDARY OF SITE)

- NOTES:
- NON REFLECTIVE METAL (GREEN)
 - PAINTED ALUMINUM / ANODIZED ALUMINUM / POWDER-COATED ALUMINUM
 - LIGHTING WILL BE IN ACCORDANCE WITH NIGHT SKY ORDINANCES



2. FREESTANDING PROJECT IDENTIFICATION SIGNAGE - SIGN AREA = 90 SQ. FT.
LOCATED ON TRACT 14 (SOUTHWEST CORNER OF SITE)

- NOTES:
- NON REFLECTIVE METAL (GREEN)
 - PAINTED ALUMINUM / ANODIZED ALUMINUM / POWDER-COATED ALUMINUM
 - LIGHTING WILL BE IN ACCORDANCE WITH NIGHT SKY ORDINANCES



3. FREESTANDING PROJECT IDENTIFICATION SIGNAGE - SIGN AREA = 150 SQ. FT.
LOCATED ON TRACT 20 (NORTHEAST CORNER OF SITE)

- NOTES:
- NON REFLECTIVE METAL (GREEN)
 - PAINTED ALUMINUM / ANODIZED ALUMINUM / POWDER-COATED ALUMINUM
 - LIGHTING WILL BE IN ACCORDANCE WITH NIGHT SKY ORDINANCES



3-D VIEW OF FREESTANDING PROJECT IDENTIFICATION SIGNAGE

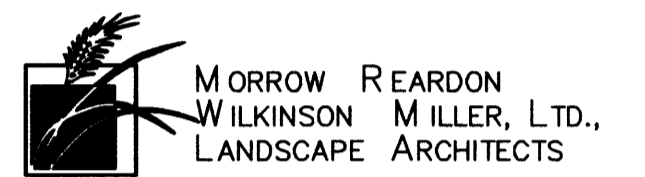


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MORROW REARDON
WILKINSON MILLER, LTD.,
LANDSCAPE ARCHITECTS

NORTH I-25
CORPORATE CENTER

MARK	DATE	DESCRIPTION
	3/2/10	DRB SUBMITTAL
	12/16/09	REVISED EPC SUBMITTAL
	8/20/09	REVISED EPC SUBMITTAL
	6/30/09	REVISED EPC SUBMITTAL
	5/29/09	REVISED EPC SUBMITTAL
	5/20/09	REVISED EPC SUBMITTAL
	4/10/09	REVISED EPC SUBMITTAL
	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:	N-25-CC
CAD DWG FILE:	A-701 DESIGN GUIDELINES.DWG
DRAWN BY:	JL
CHECKED BY:	JL

SHEET TITLE
DESIGN STANDARDS

A-705

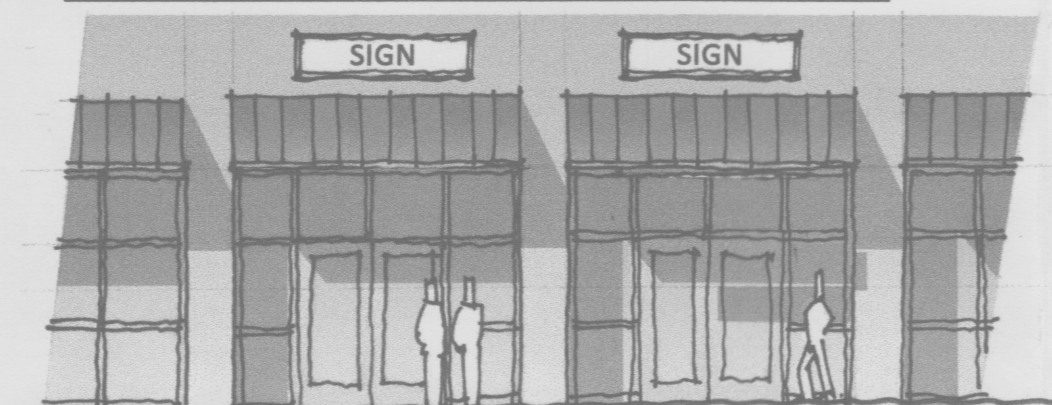
9- SIGNAGE

D. BUILDING MOUNTED SIGNAGE

1. A building-mounted sign on premises or joint sign premises where there is no free-standing sign shall not exceed 10% of the area of the facade to which it is applied.
2. At locations where a building mounted sign is installed in conjunction with a free-standing sign on the same premises, the area of the building mounted sign shall not exceed 5% of the facade area to which it is applied.
3. Building mounted sign lettering shall be individual channel letters. Illuminated plastic panel signs are not allowed with the exception of logos.
4. Building wall signs shall be compatible with the predominant visual features of the building. Where there is more than one (1) sign, all signs shall be complementary to each other in the following ways:
 - a. Type of construction materials
 - b. Type size and style
 - c. Shape of sign
 - d. Method used to support sign
 - e. Configuration of sign area
5. No wall mounted signs shall be illuminated adjacent to residential development.
6. Blade signage and signage mounted to building canopies and awnings is not allowed.
7. Each retail tenant is permitted to have building mounted signage on 2 sides of the building they occupy.

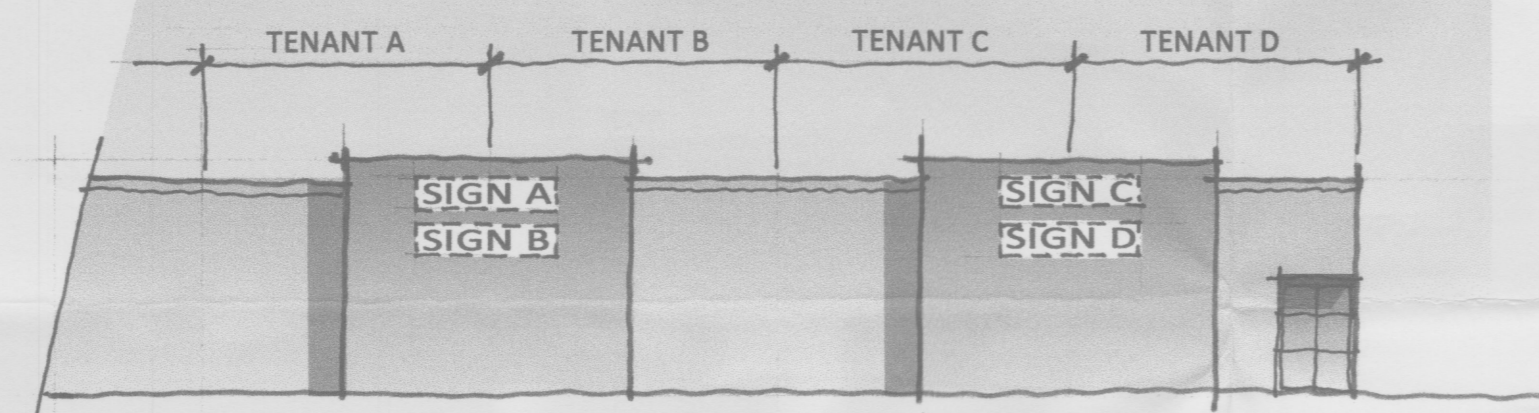
BUILDING MOUNTED RETAIL SIGNAGE - INTERIOR TO SITE

Note: Drawing indicates potential signage locations only.



BUILDING MOUNTED RETAIL SIGNAGE - ALONG FREEWAY

Note: Drawing indicates potential signage locations only.



- a. No signs shall project over parapets or beyond edge of building
- b. Sign area shall be as described in these design standards. See items C-1 & C-2 this sheet.

BUILDING MOUNTED OFFICE SIGNAGE

Note: - Drawing indicates potential signage locations only. Not quantities.

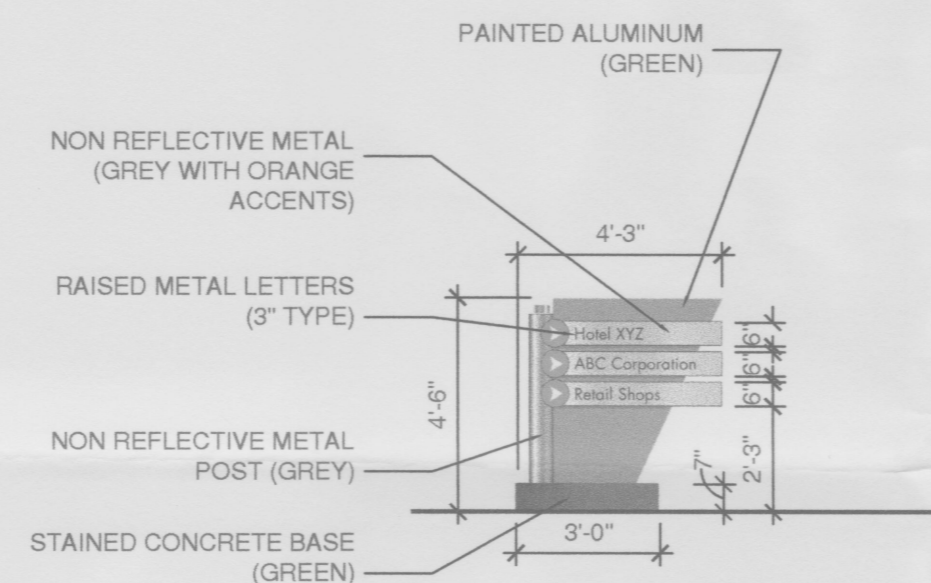


- a. No more than one sign per facade. 2 signs maximum per building.
- b. Sign area shall be as described in these design standards. See items C-1 & C-2 this sheet.
- c. No signage on projecting corner elements of building.

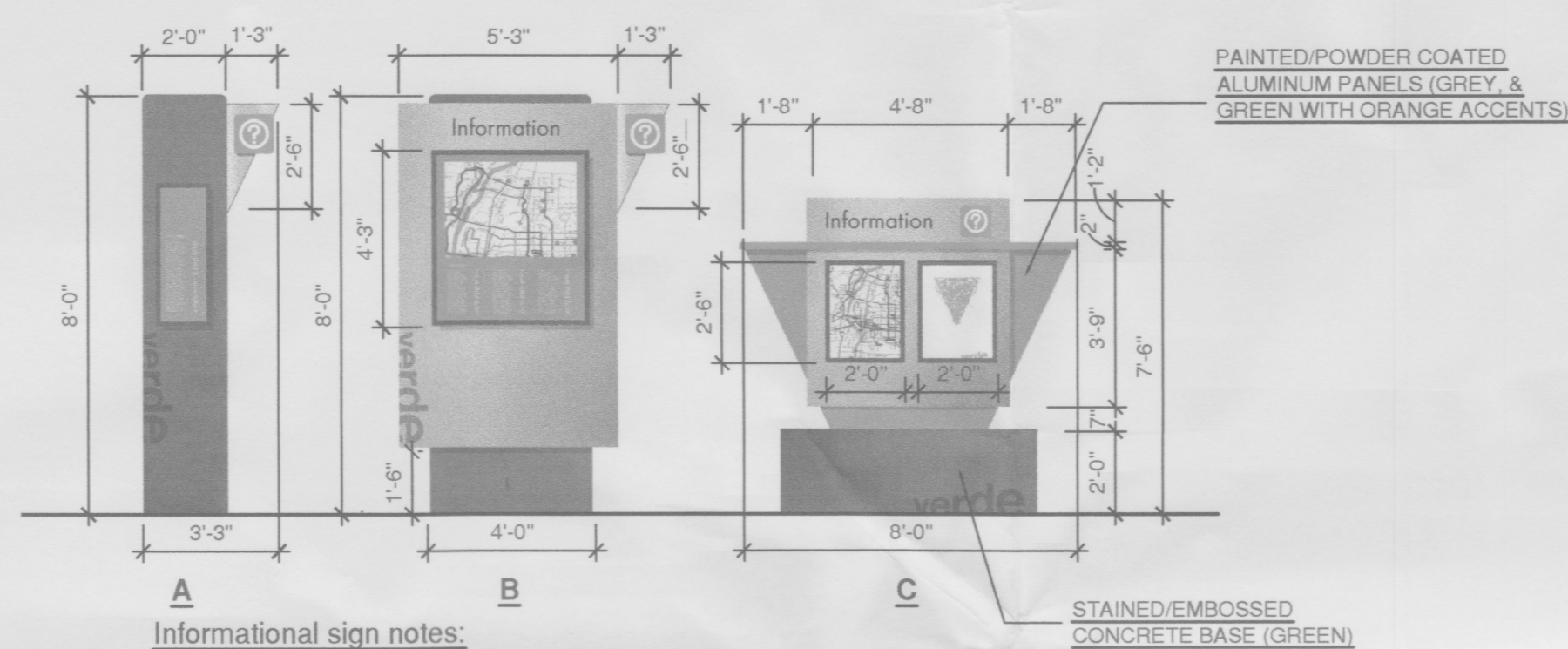
9- SIGNAGE

E. VEHICULAR AND PEDESTRIAN DIRECTIONAL SIGNAGE

1. The sign face area of traffic directional signs shall not exceed 6 square feet.
2. Private directional signage shall not contain commercial advertising such as logos.
3. Private directional signage shall not count towards the number of signs limited in other provisions of the zoning code.
4. "Signage area" is understood to be "sign face area".
5. Sign face area = 6 sf max.

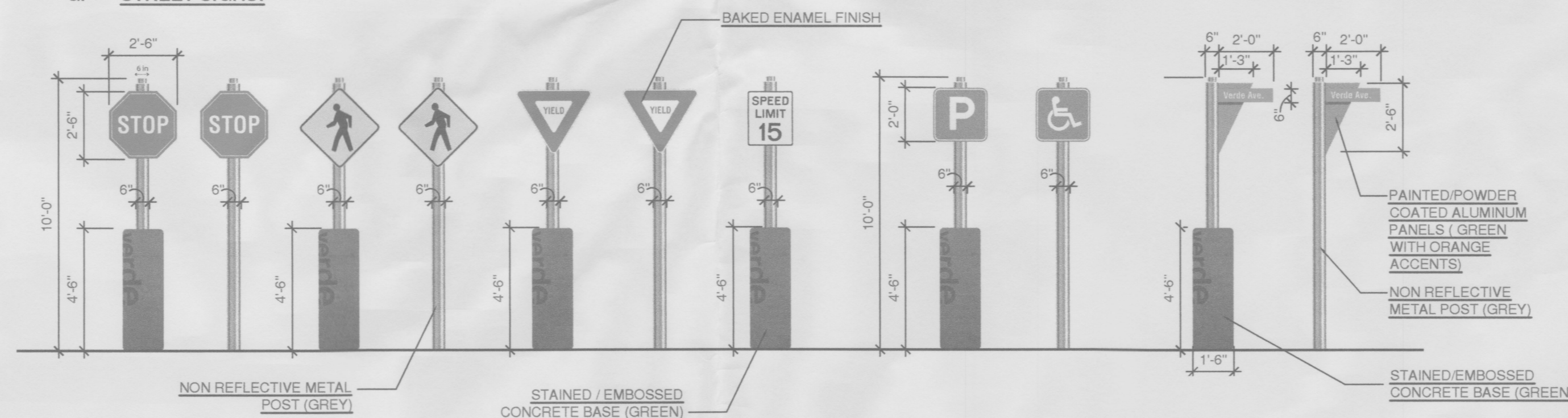


F. INFORMATIONAL SIGNS:



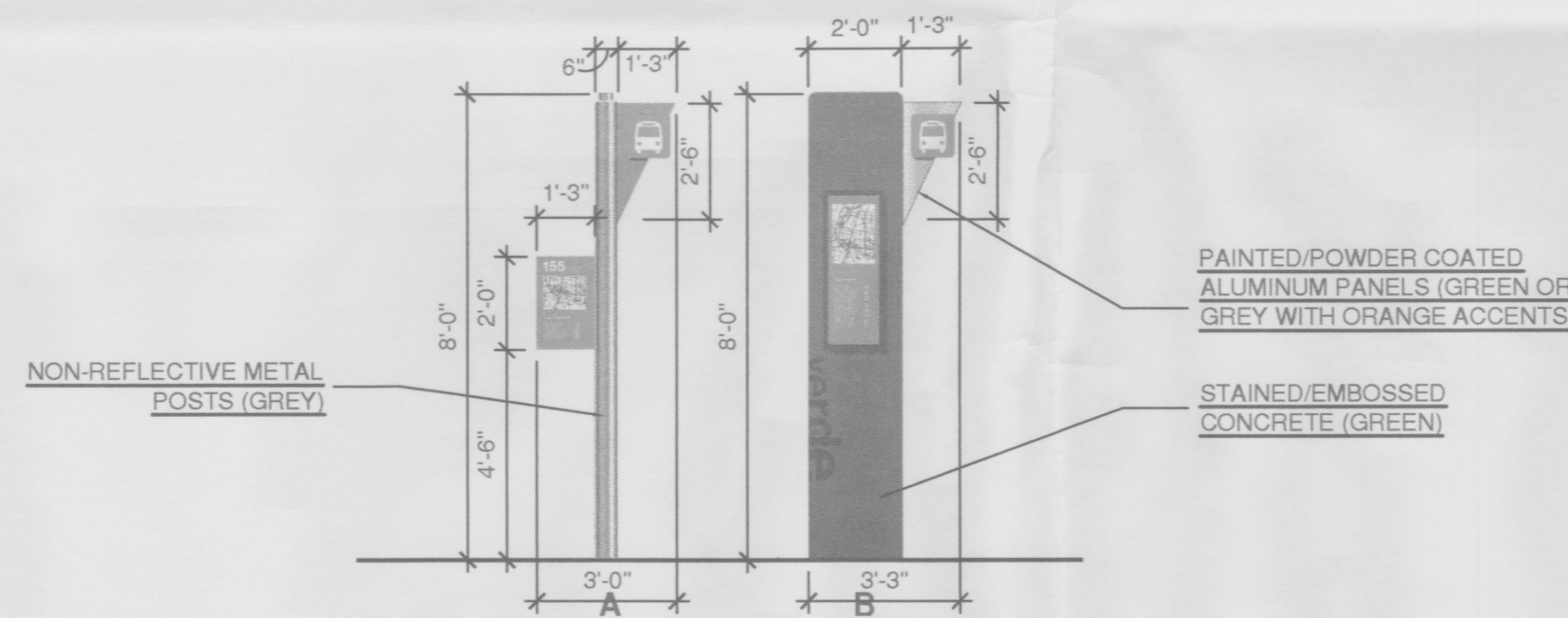
- Informational sign notes:
1. Non-glare down lighting
 2. Signage area = 20 sf max
 3. Sign face height shall not exceed 8 feet.

G. STREET SIGNS:



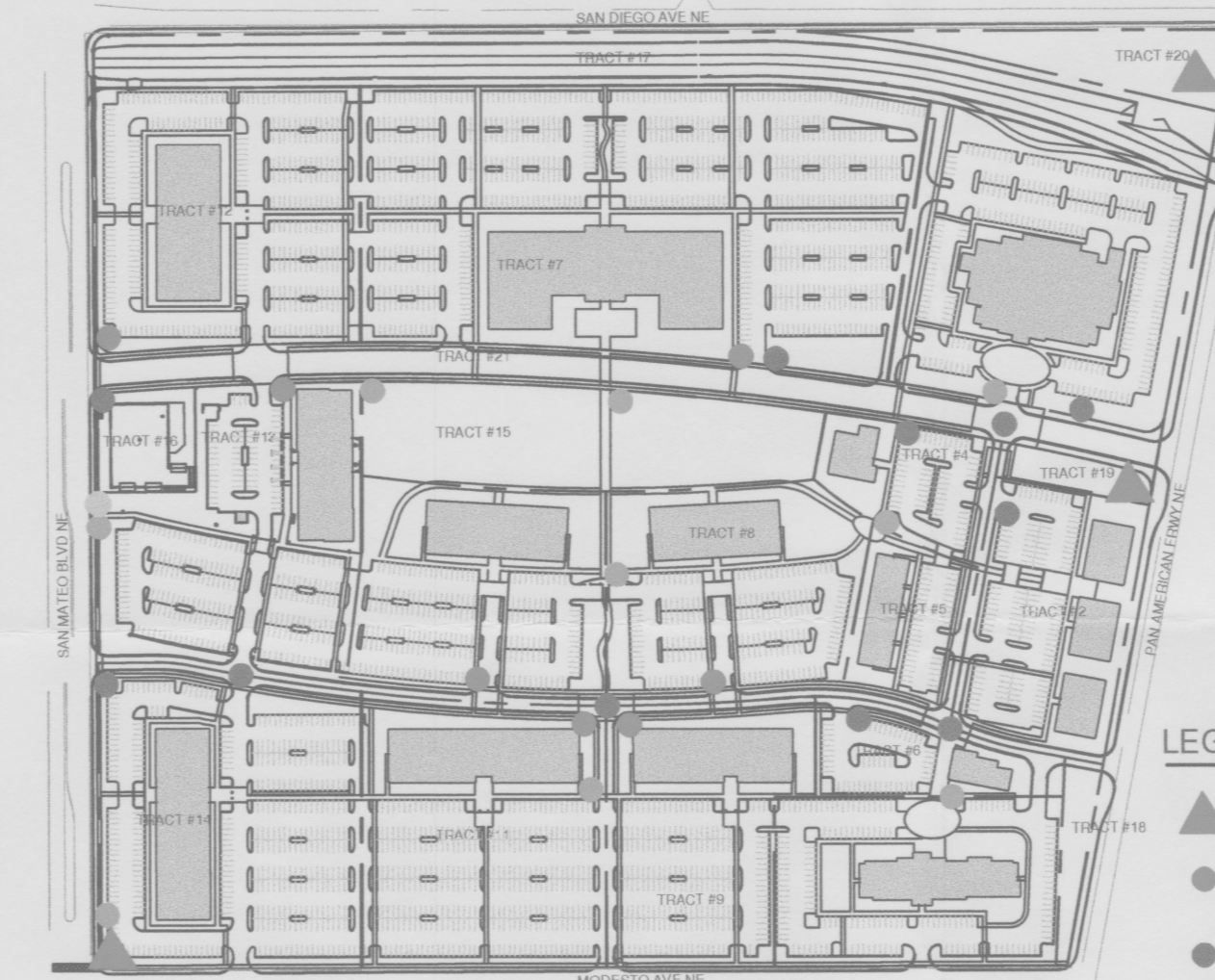
- Street sign notes:
1. Street signs to be located as required

H. TRANSIT SIGNS:



- Transit sign notes:
1. Each transit shelter shall only have one transit shelter sign.
 2. Transit shelter sign face shall not exceed 24 square feet.
 3. Transit shelter signs shall not be above 7 feet in height.
 4. Transit shelter signs shall not be placed without the approval of the City of Albuquerque transit department.

9- SIGNAGE



- LEGEND
- ▲ FREESTANDING SIGN - PROJECT ID, QTY 3
 - FREESTANDING SIGN - MONUMENT, QTY 7
 - DIRECTIONAL SIGN
 - INFORMATIONAL SIGN
 - TRANSIT SIGN

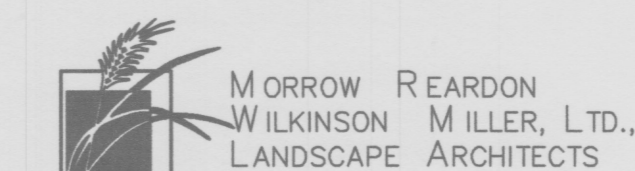
SIGNAGE LOCATION MAP



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PROJECT NO: N-025-CC
CAD DWG FILE: A-701 DESIGN GUIDELINES.DWG
DRAWN BY: JLL
CHECKED BY: JLL

SHEET TITLE DESIGN STANDARDS

A-706

10- LIGHTING

A primary objective of these lighting guidelines is to preserve the "dark sky" while providing lighting that enhances the safety, security, and visual aesthetics of the area. Light fixtures and standards shall conform to state and local safety illumination standards. Lighting shall be positioned to not conflict with proposed landscaping including trees.

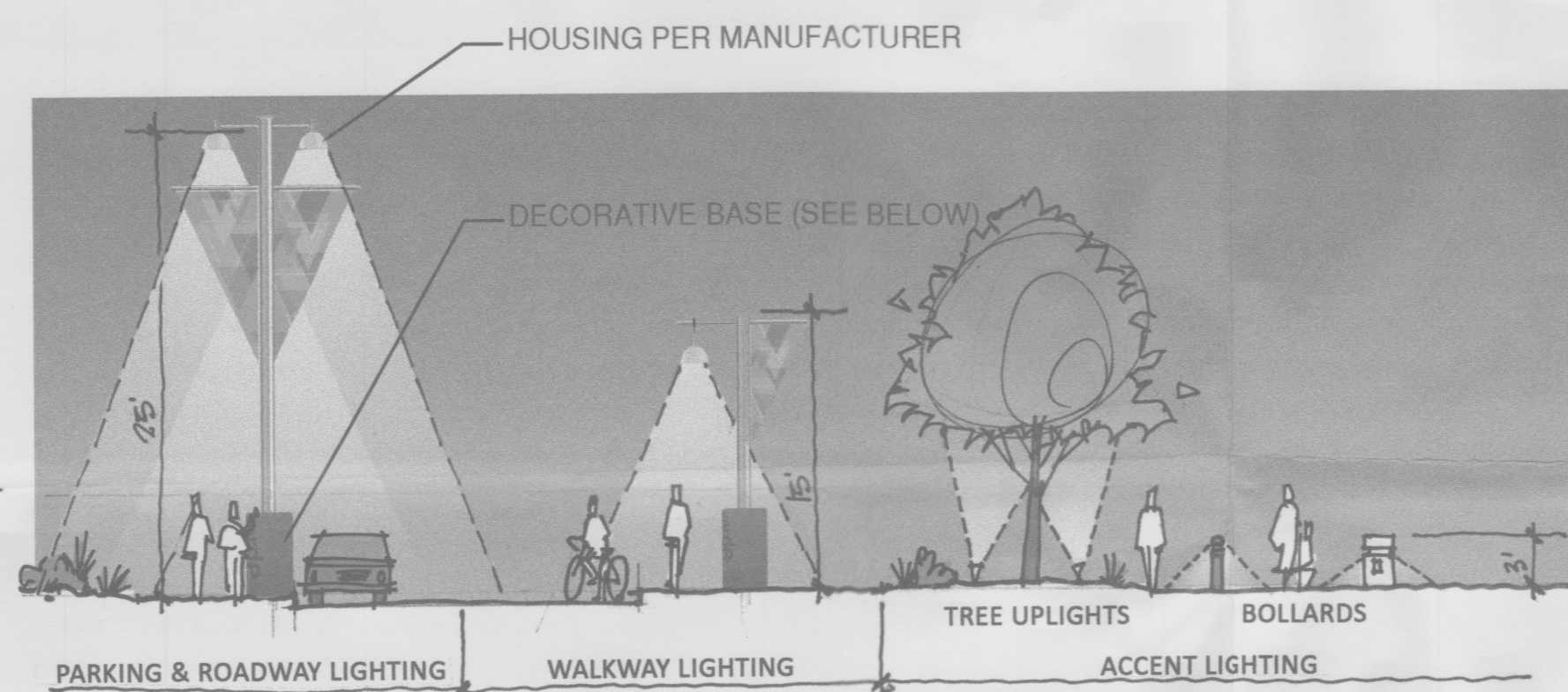
A. STREET LIGHTING

- Lighting shall be located to enhance the safety of pedestrian and vehicular flows at key points along roadways. Light shall be concentrated at intersections and pedestrian crosswalks. The maximum height of street light poles shall be twenty five (25) feet measured from top to grade.
- Excessive light spillage on adjacent properties shall not be allowed. Light fixtures shall be recessed or shielded.
- Cobra head fixtures shall not be used for street lighting. Lighting treatment within the development site shall be consistent to a standard full-cutoff shoebox fixture or of higher architectural quality conforming to all applicable night sky ordinances.
- High efficiency/high brightness led lighting is required.
- High pressure sodium lighting shall be prohibited.

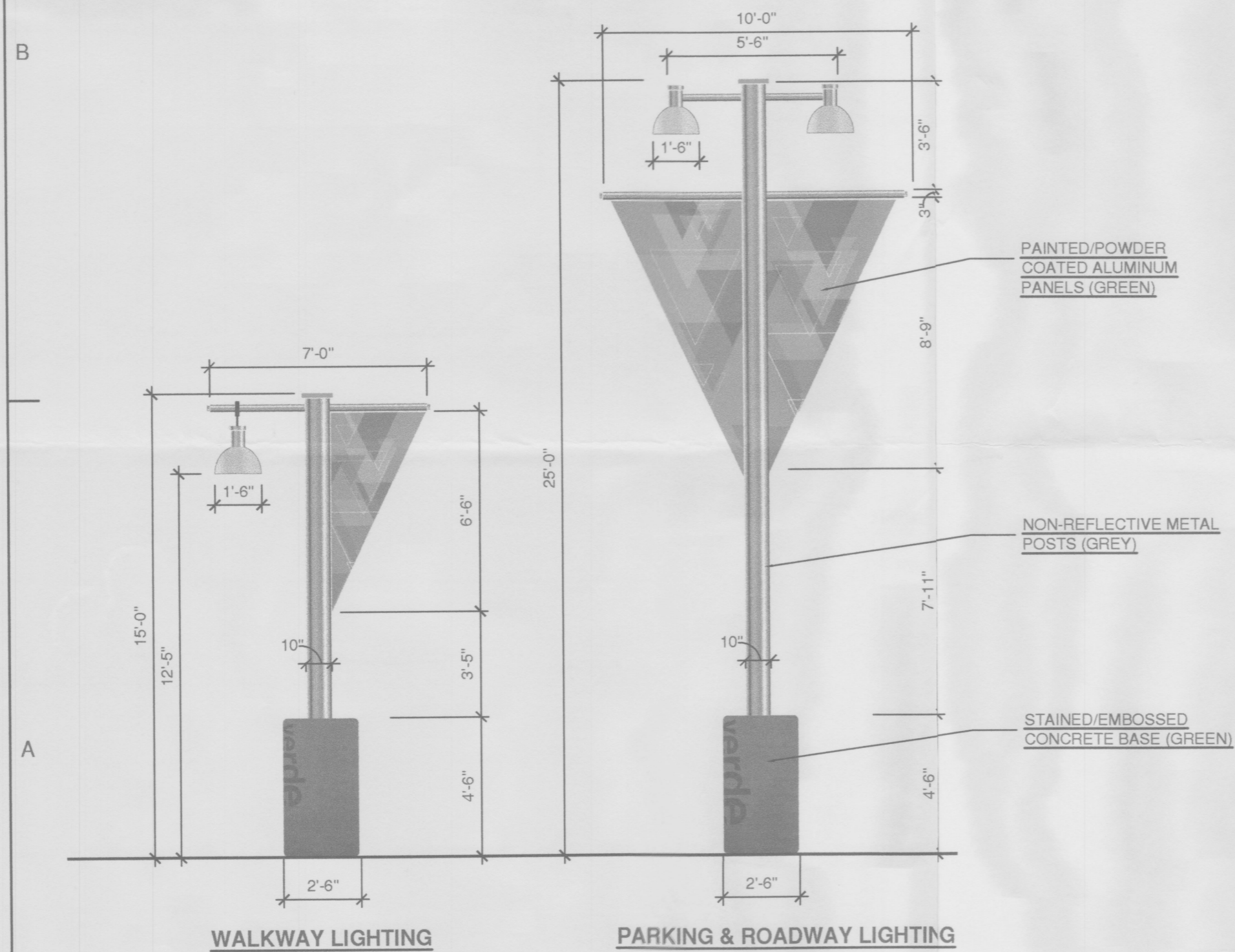
B. PARKING LOT AND BUILDING EXTERIOR LIGHTING

Lighting shall be used to provide illumination for the security and safety of on-site areas such as parking, loading, service, common areas, and walkways. Such lighting shall not conflict with proposed landscaping. Providing attractive lighting for building exteriors is an effective, yet subtle way to enhance the design of the structure. Parking lot and street lighting shall allow for the implementation of solar panels and dynamic lighting systems.

- The design of the lighting fixtures shall be compatible with the architectural features of the main structures on-site.
- Lighting fixtures shall be recessed or shielded to prevent light spread outside of the site boundary. The height of parking lot light poles shall be twenty five (25) feet maximum.
- Building entrances shall be well lit.
- Fixtures shall use high efficiency/high brightness LEDs.



SITE LIGHTING

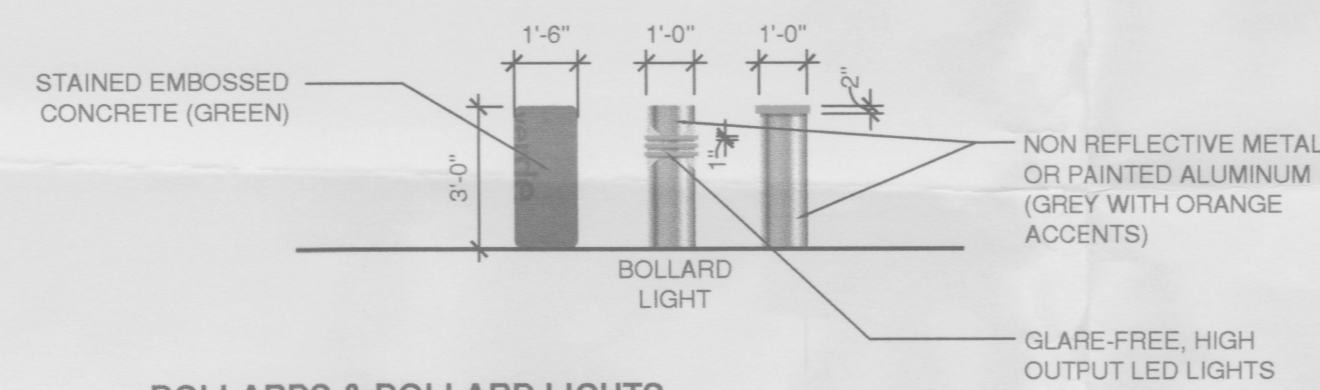


10- LIGHTING

C. PEDESTRIAN LIGHTING

Lighting shall be pedestrian oriented in districts with high pedestrian movement. Bollard or wall pocket lighting using high efficiency LEDs is required along sidewalks and other public areas.

- Pedestrian lighting shall not exceed fifteen (15) feet in height measured from top to grade.
- Bollard material and design shall be as described in the Bollard & Bollard Light detail below. Bollards shall be no greater than three (3) feet in height. Shatter-proof coverings shall be provided for bollards and other types of low-level lighting.
- Lighting may be used to accent certain landscape features. This type of lighting shall be of a low-level intensity and only illuminate the intended landscape feature.



BOLLARDS & BOLLARD LIGHTS

11- WALLS/FENCES

Perimeter walls are not permitted. Generally speaking, walls are to be used only as screening elements and as described in the utilities section of these design standards.

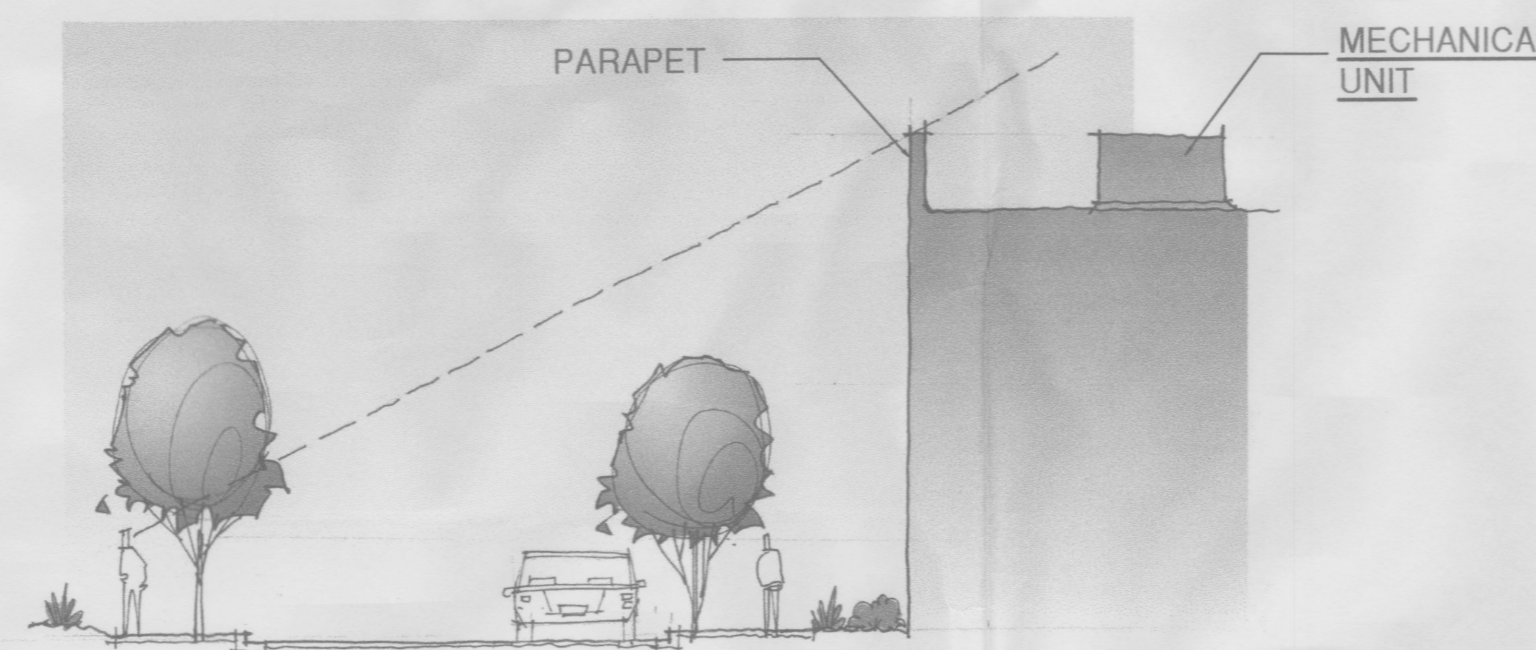
- Chain link fencing shall be prohibited.
- Concertina or barbed wire fencing shall not be permitted.
- Exposed, untreated block walls shall be prohibited.

12- UTILITIES

A. ANTENNA AND TOWERS

- Freestanding cellular antenna and cell towers are prohibited. Antennas shall be integrated with buildings, existing light poles, existing utility structures and other public facilities.

B. MECHANICAL EQUIPMENT:



MECHANICAL UNIT SCREENING

- Mechanical equipment, including but not limited to cooling and heating systems, ventilation, antenna and other reception devices, shall be screened from street view through the use of parapets or other architectural elements of the same nature as the building's basic design, material, and color. The height of a screening element such as a parapet shall be uniform around the entire structure and shall be at least equal in height to the mechanical units being screened from view.
- Mechanical equipment mounted on the ground shall be screened from street view with landscaping or fencing materials.
- On-site utilities, including electrical, telephone, and communication wires and equipment shall be installed and maintained underground. Transformers, utility pads, cable tv, and telephone boxes shall be located out of view from public rights-of-ways or visually screened with vegetation, fences, or walls.
- All screening and vegetation surrounding ground-mounted transformers and utility pads shall allow 10 feet clearance for access to ensure the safety of the work crews and the public during maintenance and repair.

C. TRASH ENCLOSURES

- Trash enclosures shall utilize similar colors and materials to buildings on the same tract.
- No refuse storage/collection areas will be allowed to be sited in close proximity to the front of a building. Refuse containers are to be sited so as to have at least 1 double loaded row of parking and associated drive aisle between it and the primary building entrance/elevation. Refuse collection areas shall be enclosed within a six (6) to 8 (eight) foot tall masonry enclosure which is large enough to contain all refuse generated between collections. The design of the enclosure shall be compatible with the architectural theme of the tract.

12- UTILITIES

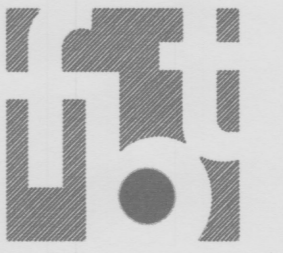
D. PNM CONDITIONS OF APPROVAL

- It is the applicant's obligation to determine if existing utility easements cross the property and to abide by any conditions or terms of those easements.
- PNM has existing facilities at the project site. Due to the addition of new streets, PNM will require adequate access to the existing PNM substation. Adequate access will be necessary to accommodate large equipment for maintenance or repair. It is required for the applicant to ensure adequate access to PNM's existing facilities by contacting PNM's System Engineering.
- Adequate clearance for electric utilities must be provided for safe operation and maintenance purposes. In addition, any relocation, changes or realignment regarding existing electric utilities will be the developer's expense. In some cases, relocation or changes to existing facilities may not be feasible due to physical, use or safety clearance constraints. PNM will review all technical needs, issues and safety clearances for its electric power systems.
- The applicant shall coordinate with PNM regarding proposed tree species, the height at maturity and tree placement to avoid interference with the existing electric transmission and/or distribution lines along the project site. PNM's standard is for trees to be planted outside the PNM easement.

13- PROCESS

A. GENERAL-

- Variation of building area in excess of areas shown by 10% or more, despite any future delegation to the DRB, shall return to the EPC for reconsideration.
- The site development plans for building permit shall be reviewed by the EPC. Delegation of approval authority to the DRB shall be discussed and decided upon at that time.

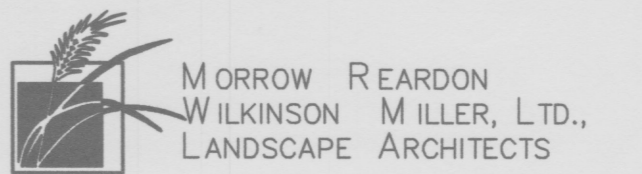


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NORTH I-25
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	3/31/09	INITIAL EPC SUBMITTAL

PROJECT NO:	N-125-CC
CAD DWG FILE:	A-707 DESIGN GUIDELINES.DWG
DRAWN BY:	JLL
CHECKED BY:	JLL

SHEET TITLE
DESIGN STANDARDS

A-707