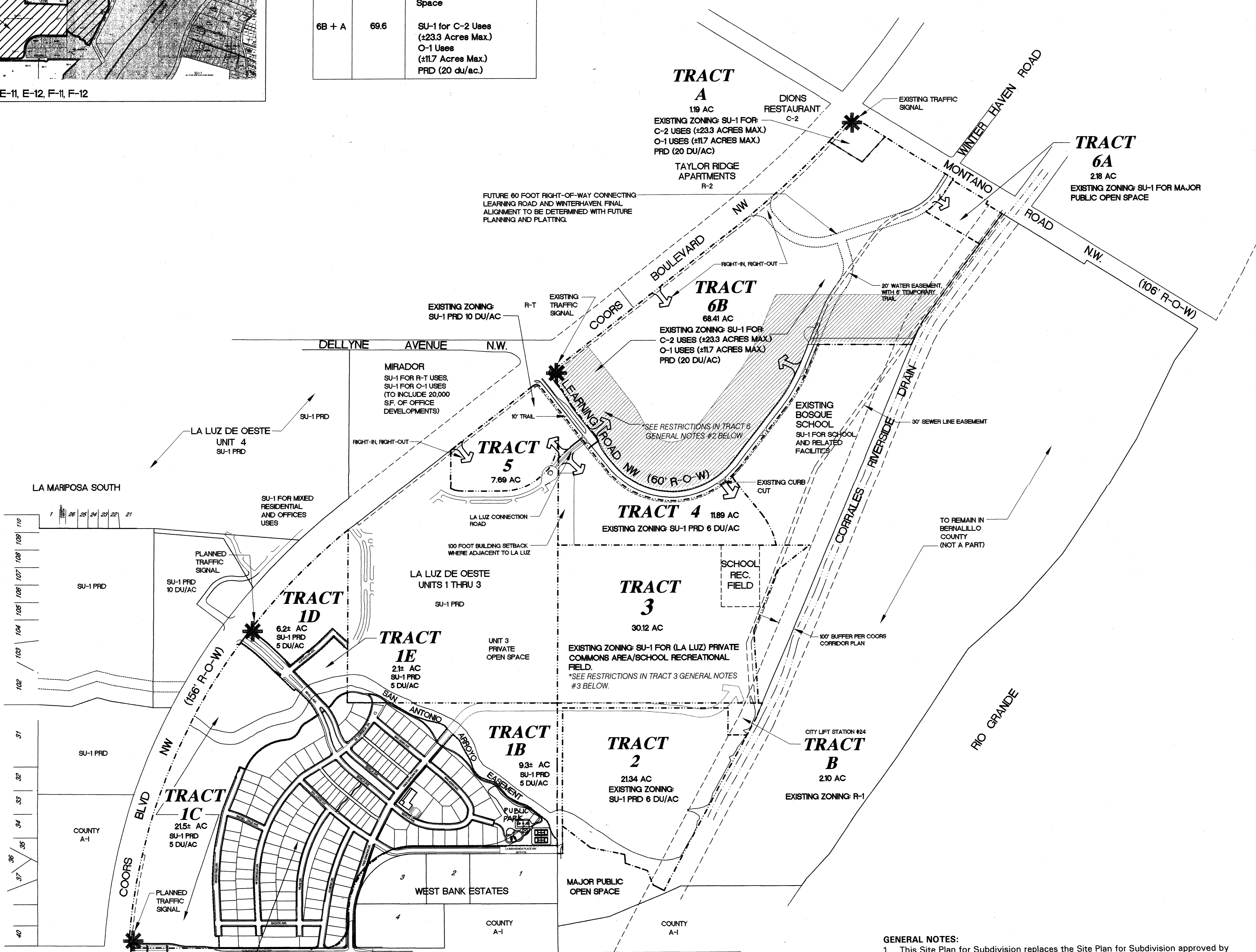


ZONE ATLAS: E-11, E-12, F-11, F-12

ZONE CATEGORIES

TRACT	ACREAGE	EXISTING ZONING
1	84.73	SU-1 PRD 5 du/ac
2	2134	SU-1 PRD 6 du/ac
3	30.12	SU-1 for (La Luz) Private Commons Area/School Recreational Field
4	11.89	SU-1 PRD 6 du/ac
5	7.69	SU-1 PRD 10 du/ac
6A	2.18	SU-1 for Major Public Open Space
6B + A	69.6	SU-1 for C-2 Uses (±23.3 Acres Max.) O-1 Uses (±11.7 Acres Max.) PRD (20 du/ac.)

SITE



SITE PLAN FOR SUBDIVISION - REQUIRED INFORMATION

THE SITE:
The site consists of approximately 228 acres.

PROPOSED USE:
The parcels to remain the same relative to land use-residential (varying densities), commercial, office, private commons area/school recreational field, and public park.

PEDESTRIAN AND VEHICULAR INGRESS AND EGRESS:
VEHICULAR ACCESS: The existing Learning Road and Namaste Road off Coors Boulevard provide the major access into Andalusia at La Luz. Local roads (at varying widths) are proposed to serve the project and provide vehicular ingress and egress to these parcels, to increase safety to existing development, and to be consistent with City policies contained in the Coors Corridor Plan (see Street Sections, on sheet 3). Two right-in/right-out access points onto Coors Boulevard are between Learning Road and Montano Road, and one right-in/right-out access to Tract 5 is proposed. Learning Road is a signalized intersection and the main entry road off Coors Boulevard to the project (Seville Avenue) is planned as a signalized intersection.

BICYCLE AND TRAIL ACCESS: Bicycle access is provided by 6 foot on-street bike lanes in Learning Road, Learning Road trail (a 10 foot trail within a 20 foot landscape easement), and a 6 foot temporary trail to connect the northern end of Learning Road to Montano Road. New public trails are planned along the San Antonio Arroyo and along La Bienvenida Place. Sidewalks and private trails will provide pedestrian connectivity throughout Andalusia at La Luz.

TRANSIT ACCESS: Coordination with the City Transit Department shall be initiated at the Site Plan for Building Permit to provide access and service to this property. Coors Boulevard is a major transit route on the West Side.

INTERNAL CIRCULATION REQUIREMENTS:
Conceptual access points (30 feet in width) have been identified on this Site Plan for each of the parcels; final locations, width, and configuration shall be determined with future Site Plans for Building Permit with approval by the City Engineer. Internal sidewalks and/or trails shall be provided within each parcel with future Site Plans for Building Permit.

BUILDING HEIGHTS AND SETBACKS:
See Sheet 2 of 3, Design Standards. Building height shall be consistent with the Coors Corridor Plan (see Sheet 2) and should be kept to a minimum.

MAXIMUM FAR:
A maximum floor area ratio (FAR) shall be .35 for the SU-1 for C-2 and SU-1 for O-1 portions of Tract 6B and Tract A.

LANDSCAPE PLAN:
The Design Standards (see Sheet 2 of 3) provide for preservation of significant cottonwoods, emphasis on native and naturalized plant species, landscape criteria, and landscape buffers. Subsequent landscape plans shall be consistent with City standards and policies regarding water conservation.

APPROVALS

PROJECT #000965	DRB# 03DRB-01715
EPC #03EPC-0105	
This Site Plan for Subdivision is consistent with the Site Plan approved by the Environmental Planning Commission on March 22, 2001, as Amended by on January 16, 2003, and as approved on August 21, 2003, and Conditions of Approval have been met.	
<i>Sharon Malton</i> Planning Director	10/29/03 Date
<i>John S. ...</i> Transportation Development	10/29/03 Date
<i>Bradley L. ...</i> City Engineer/AMAFCA	10/29/03 Date
<i>Ryan ...</i> Utility Development	10/29/03 Date
<i>Christine ...</i> Parks and Recreation Department	10/29/03 Date

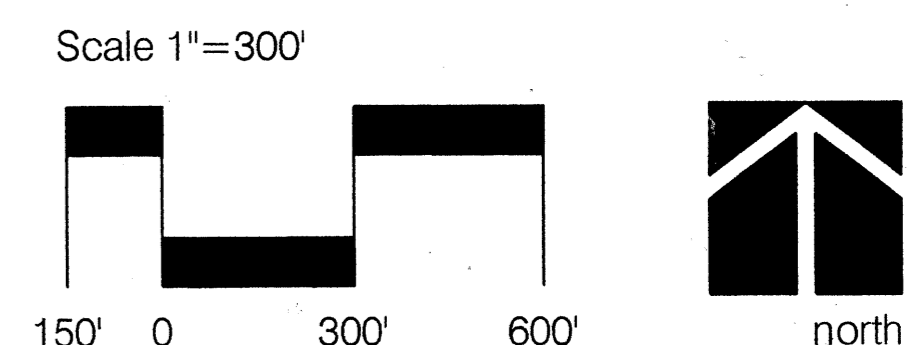
- GENERAL NOTES:**
- This Site Plan for Subdivision replaces the Site Plan for Subdivision approved by the Environmental Planning Commission (EPC) on January 16, 2003 (Project #000965/02EPC-01771).
 - The area of Tract 6B adjacent to Learning Road and the northern boundary of Bosque School (a minimum of 300 feet) shall be restricted to office and residential uses (PRD & O-1).
 - School Recreational Field shall be limited to an area of 240' x 390' in the northeasterly portion of Tract 3. No parking or field lighting is permitted in the vicinity of the Recreational Field.
 - Use of Tract 3 is limited to residents of La Luz only. Residents of Andalusia are not allowed access to Tract 3.
 - Tract 1 is divided into 4 major tracts- 1A, 1B, 1C, and 1D. Tract 1A is 161 Single Family Residential Lots; Tracts 1B, 1C, and 1D are bulk Tracts for future development.
 - Due to the existing Montano Pueblo Archeological Site, any future approvals for Site Development Plans for Building Permit on Tracts 6B and A shall require clearance and guidance from the State Historic Preservation Office. (Letter sent to Office of Cultural Affairs, State Historic Preservation Office, on January 11, 2002 requesting Pre-Consultation, Clearance, and Guidance).

Site Plan for Subdivision

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Design Standards

The purpose of these Design Standards is to provide a framework to assist the architects, landscape architects, and designers in understanding the vision and development goals for the property. The primary goal for this property is to achieve a vibrant, mixed-use community that fosters pedestrian accessibility and maintains a village-type character.

The Design Standards should be used to facilitate the design of buildings which respect the natural conditions of the site, maintain and highlight the spectacular views of the Sandia and Manzano Mountains, and to leave significant areas dedicated to open space. Innovative techniques such as cluster housing, water harvesting, and use of "green" building materials are strongly encouraged.

These standards address the issues of landscape, setbacks, pedestrian amenities, screening, lighting, signage, and architecture that will create the visual image desired for Andalucía at La Luz. They are intended to be complementary to La Luz, Albuquerque's first cluster housing project, and the Bosque School. These standards primarily address commercial, office, and multi-family projects. Where specifically applicable to single-family development, the standards are called out as such.

Subsequent Site Plans for Building Permits shall be consistent with the Design Standards established by this Site Plan for Subdivision. Future Site Plans for Building Permits for the Private Recreation Area on Tract 1A shall be delegated for approval to the Development Review Board. All other Site Plans for Building Permit shall be approved by the Environmental Planning Commission. Minor amendments to this Site Plan for Subdivision shall be approved administratively by the Planning Director in accordance with the Comprehensive City Zoning Code, Section 14-16-2-22 (A)(6) Special Use Zone.

COORS CORRIDOR PLAN - VIEW and HEIGHT RESTRICTIONS

A primary design requirement of the Coors Corridor Plan is view preservation. Andalucía at La Luz addresses the Coors Corridor Plan as follows:

- Coors Corridor Viewshed rules come into effect when a northbound vehicle passes Namaste Road.
- Measurement of building heights shall be consistent with the Coors Corridor Plan.
- A combination of walls and berms shall be built along Coors Boulevard. The high point of the wall or berm will occur within the first 60 feet of the property, but not closer than 20 feet from the right-of-way. From any point along the east edge of Coors, the high point of the wall or berm will obscure no more than 50% of the height of Sandia Crest, measured directly below Sandia Crest.
- All multi-story structures shall be built with the finish floor elevation at least 10 feet below the roadway, measured along a 45-degree angle from the northbound direction of travel along Coors.
- No structure shall project more than one-third of its height above the view plane, measured along the same 45-degree angle. The view plane is 4 feet above the roadway. For a 24 foot 2-story structure, no more than 8 feet of height shall project above the view plane.
- As viewed from Coors Boulevard, no structure shall obscure more than 50 percent of the height of Sandia Crest, measured directly below Sandia Crest.

PEDESTRIAN and SITE AMENITIES

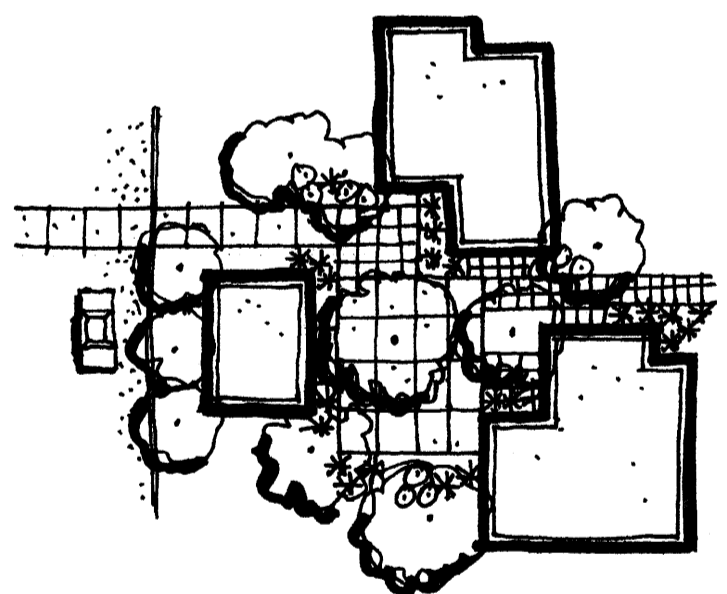
The creation of a pedestrian-friendly environment will depend on creative site design and will be a primary design objective for Andalucía at La Luz. Objectives to achieve this goal include maintaining a high quality and consistency in style for site amenities including benches, plazas, walkways, lighting, etc.; providing shaded walkways; and creating separate vehicular and pedestrian circulation systems in order to support the creation of a village-type character. Access to the Bosque will be a key feature for the property.

The use of alternative paving materials (brick, colored concrete, decomposed granite, etc.) for pedestrian pathways are encouraged. Public art is another site amenity that is strongly encouraged, and if proposed, should be part of the subsequent building plans.

TRAILS and SIDEWALKS

Public and private trails and sidewalk systems are a defining element to Andalucía at La Luz. The City's Trails and Bikeway's Facility Plan proposes a Primary Trail to cross Coors Boulevard and follow the San Antonio Arroyo. This trail will connect to the existing trail that currently ends at Namaste and then will follow north along La Bienvenida Place until it connects to the existing Learning Road Trail. Private trails for use by residents are designed to lessen the need for vehicular use and will provide pedestrian connectivity throughout the project (see Landscape Section for more detail on trail landscaping).

- City Trails - All public multi-use trails through Andalucía at La Luz shall be built to City standards, per the City's Trails and Bikeways Facility Plan.
- All private trails and paths shall be soft surface, with a minimum width of 6 feet.
- All pedestrian paths shall be designed to be handicapped accessible (see Americans with Disabilities Act Criteria for Barrier-Free Design, except where topography makes this unfeasible (several streets require grades over 6%).
- The use of asphalt paving for pedestrian paths trails is discouraged. Concrete or compacted decomposed granite with stabilizer are acceptable materials.
- Pedestrian connections to buildings should be provided in parking lots with greater than 50 spaces and should connect to adjacent roadways, sidewalks, and pathways.
- Pedestrian crossings shall be clearly demarcated with special paving treatment where they cross vehicular entrances and drive aisles and where City trails cross streets.
- Restaurants, if proposed by subsequent Site Plans for Building Permits, shall provide outdoor patios and shall be shaded by trees and/or a shade structure that is architecturally integrated with building architecture.
- Non-residential and multi-family development, if proposed by subsequent Site Plans for Building Purposes, shall provide secure bicycle storage racks that are conveniently located near building entrances. The minimum number of bicycle racks shall be determined by the number of parking spaces provided, consistent with the City Zoning Code.

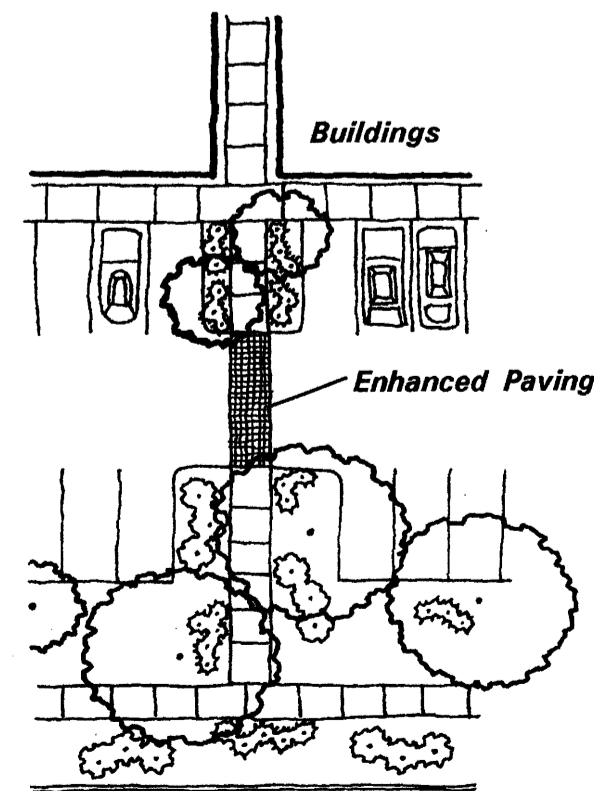


OUTDOOR AREAS - defined by building edges.

PARKING

In order to support the goals for the property regarding pedestrian accessibility, careful attention should be paid to the parking design. An effort should be made by site designers to lessen the impact of parking facilities on the land and to preserve views to the Bosque and the Sandia and Manzano Mountains. In order to lessen the visual impact of parking areas, parking facilities should be broken up into a series of smaller areas.

- Handicapped parking spaces shall be provided adjacent to building entries.
- The total maximum amount of parking provided shall meet parking requirements in the City Zoning Code, plus 10 percent.
- Structures and on-site circulation systems should be located to minimize pedestrian/vehicle conflicts. Pedestrian access shall be provided to link structures to the public sidewalk.



PARKING - pedestrian links should be provided between parking areas and buildings

- Parking areas shall be designed to include a pedestrian link to buildings.
- In cases where parking is adjacent to roadways, landscaping for screening purposes shall be required. The wall shall be architecturally compatible with the surrounding buildings.
- Parking is not allowed in the vicinity of the recreational fields in Tract 3.

SINGLE-FAMILY PARKING STANDARDS

The following parking standards are provided for single-family residential development. They have been developed in conjunction with the Unique Street Standards (see Sheet 3 of 3).

- Off-street parking shall be provided at the following rate:
 - 3 spaces per 2 bedroom dwelling unit
 - 4 spaces per 3-4 bedroom dwelling unit
 - 5 parking spaces per 5 bedrooms or greater dwelling unit

SETBACKS

The use of building and parking area setbacks is required to provide space for the creation of visually attractive streetscapes. Required within these setbacks will be pedestrian walkways and screening materials, including landscaping, earthen berms, and walls. Parking areas are discouraged from being adjacent to roadways. Front yard setbacks for single family residential developed have been reduced from the City's standard in order to provide as intimate a streetscape experience as possible.

- All development at Andalucía shall comply with the setback requirements of the Coors Corridor Plan.
- Commercial and office development shall comply with the building setback requirements of the O-1 Zone in the Comprehensive City Zoning Code and the Coors Corridor Plan.
- Accessory structures shall comply with Section 14-16-3-3 of the Comprehensive City Zoning Code.

SINGLE FAMILY LOTS

- Setbacks on westerly edge of Tract 4 shall be 100 feet where adjacent to La Luz only.
- Front Yard Setbacks Dwelling Unit - 8 feet
 - Garages - 8 feet, except 20 feet for garages facing street
- Rear Yard Setbacks Dwelling Unit - 15 feet
 - Garages (attached and detached) - 0 feet
- Side Yard Setbacks Dwelling Unit - 3 feet, except 8 feet for lots adjacent to roadways
 - Garages (attached and detached) - 0 feet
- Garage Setbacks
 - Where garages face the street, the garage shall have a minimum setback of 20 feet from the property line.
 - No more than 3 houses in a row with garages facing the street can have a 20 foot setback. Minimum offset for adjacent houses is 3 feet.
 - Garages provided on alleys shall have a minimum rear yard set back of 3 feet to allow for proper turning clearances.

TOWNHOUSE LOTS

- Front Yard Setbacks Dwelling Unit - 8 feet
 - Garages - 8 feet, except 20 feet for garages facing street
- Rear Yard Setbacks Dwelling Unit - 15 feet, or for dwellings with offset rear lot lines, not less than 5 feet provided the average rear yard setback is not less than 15 feet
 - Garages (attached and detached) - 0 feet
- Side Yard Setbacks
 - No required side yard setback, except there shall be 8 feet on the street side of corner lots and 3 feet from a side lot line that separates townhouse zone from another zone

PARKING AREA SETBACKS

To allow for an appropriately sized landscape buffer adjacent to roadways, parking areas shall be setback as follows:

- 15 feet

LANDSCAPE

The development of an overall landscape concept will establish a framework that unifies the property and is complementary to the Rio Grande Bosque. The landscape design should emphasize native and naturalized plant species. All landscaped areas need to be coordinated and responsive to existing environmental conditions and local building policies. These standards are to be used as a supplement to the City requirements in the Water Conservation Landscaping and Water Waste Ordinance, the Street Tree Ordinance, and landscape regulations included in the City of Albuquerque Comprehensive City Zoning Code.

The following are minimum standards for the development of specific landscape plans:

- Significant cottonwood specimens shall be identified and preserved, where feasible.
- For single family residential projects, street trees shall be provided along roadways at a rate of one tree per 40 linear feet. For multi-family and non-residential projects, they shall be provided along roadways at a rate of one tree per 25 linear feet. They may be randomly or regularly placed.



PEDESTRIAN AREAS - should include shade trees

- Individual lot owners will be responsible for the installation and maintenance of the landscape on their property. All plant material, including trees, shrubs, groundcovers, turf, wildflowers, etc. shall be maintained by the lot owner in a living, attractive condition.

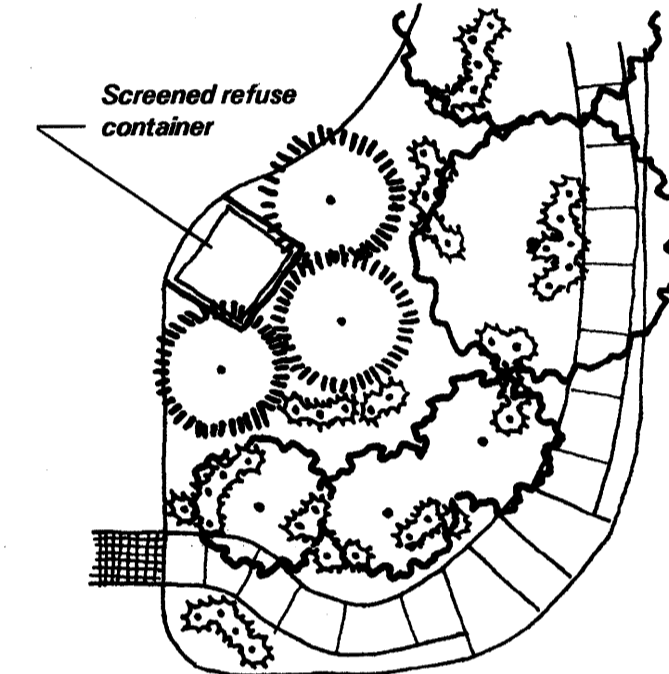
- All Private Commons Areas and parkway areas between curb and sidewalk shall be maintained by the Homeowners' Association.

NON-RESIDENTIAL and MULTIFAMILY RESIDENTIAL

- A minimum of 15 percent of the site area for commercial, office, and multi-family projects (minus the building square footage) shall be devoted to landscape materials.
- Landscaped areas shall be a minimum of 36 square feet and a minimum width of 6 feet. Living, vegetative materials shall cover a minimum of 75 percent of the landscaped areas. The area and percentage is calculated based on the mature canopy size of all plant materials.
- All planting areas not covered with turf shall have a ground topping of river rock, shredded bark, gravel mulch, or similar material which extends completely under the plant material.
- Appropriate landscape headers shall be used to separate any turf and groundcover areas.
- To shade and mitigate the negative visual impact of large expanses of pavement, interior parking areas shall have one tree for each 10 parking spaces with no space being more than 100 feet from a tree.
- 75 percent of the required parking area trees shall be deciduous and have a mature height and canopy of at least 25 feet.
- An automatic underground irrigation system shall be provided to support all required landscaping. Irrigation components should be checked periodically to ensure maximum efficiency.
- All plant materials shall be chosen from the City of Albuquerque's Water Conservation Ordinance Plant List.
- Minimum plant sizes at time of installation shall be as follows:
 - Trees 1 1/2 inch caliper, or 10 to 12 feet in height
 - Shrubs & Groundcovers 1 gallon
 - Turf grasses shall provide complete ground coverage within 1 growing season after installation.

SCREENING / WALLS and FENCES

The effective use of screening devices for parking lots, loading areas, refuse collection, and delivery/storage areas is essential to limit their adverse visual impact on the property. The site orientation of these elements shall be away from any street or pedestrian area. The standards established in the landscape and setback sections will provide the main objectives to screening unattractive elements and activities.



REFUSE CONTAINERS - shall be screened by a 6 foot tall enclosure and plant materials

Screening

- Parking areas shall be located away from adjacent streets and properties and shall be screened with plant materials, walls, or earthen berming. Such screening shall have a minimum height of 3 feet. Since the viewing public is from above site topographically, the focus of the screening materials should be on trees.
- All outdoor refuse containers shall be screened within a minimum 6 foot tall enclosure which is large enough to contain all trash generated between collections. Trash enclosures shall be screened with plant materials.
- The design and materials for refuse collection enclosures shall be compatible with the architectural theme and materials of the site and adjacent buildings.
- No refuse collection areas shall be allowed between streets and building fronts.
- All roof-mounted equipment shall be screened from public view by materials of the same nature as the basic materials of the building.

MECHANICAL SCREENING - should be screened from public view

Walls/Fences

Perimeter fencing is allowed on the property; however, an effort should be made by the site designer to lessen its visual impact through landscaping, meandering within a landscape area, and providing openings. Tensile wire fencing is encouraged for development adjacent to the Bosque to ensure visual access.

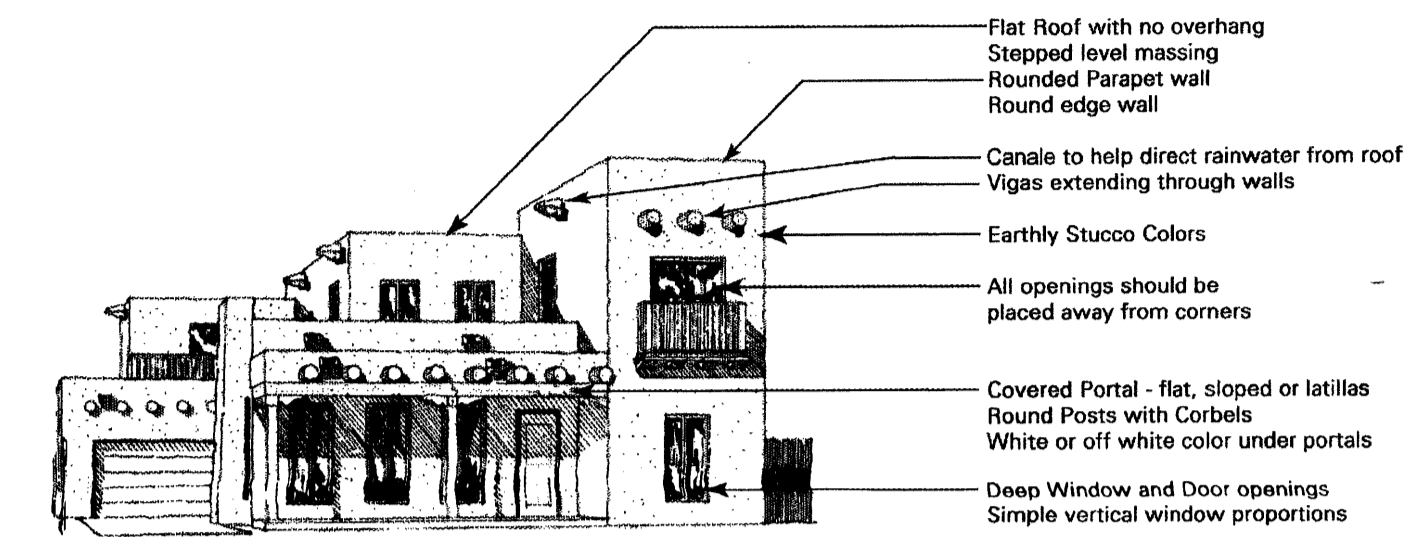
- Unfinished block walls and barbed wire, chain link, concertina wire, and plastic/vinyl fencing are prohibited.
- Development adjacent to the San Antonio Arroyo shall comply with Section F, Design Guidelines for Development Adjacent to Major Open Space Arroyos and Major Open Space Links of the Facility Plan for Arroyos.
- Perimeter walls, if used, shall include pedestrian openings at key locations within the development to ensure convenient access to other areas within the property, as well as to the Bosque.
- Visual openings shall be provided in perimeter walls every 100 feet.
- Front yard walls within the view lot areas shall have a maximum height of 6 feet for any single wall. Clear site distances will be maintained at all driveway locations.
- The maximum height of retaining walls is 8 feet. Areas requiring greater retainage are required to be terraced. Where a retaining wall and yard wall are combined, the maximum allowable height is 10 feet unless approved by the Architectural Control Committee.
- Materials acceptable for retaining walls are railroad ties, masonry block (no unfinished), split face block, and stone.
- Yard walls shall be constructed of adobe, cast pumice, rammed earth, concrete masonry, rastra or stuccoed masonry block and shall be finished to match adjoining exterior house walls. Walls shall not exceed 6 feet in height except at the gate opening unless approved by the Architectural Control Committee.
- The maximum height of the wall adjoining the open space, parks, private commons areas, or pathways is 6 feet, or a visual opening may be provided in the wall as an alternative.
- Coyote fences are allowed with or without stuccoed pilasters.

ARCHITECTURE

The architectural design should demonstrate a high quality aesthetic character throughout the property and should respond to climate, views, solar access, and aesthetic considerations. Each Site Plan for Building Permit will have a detailed description of architectural character intended for each parcel. These regulations are strictly aesthetic in their intent.

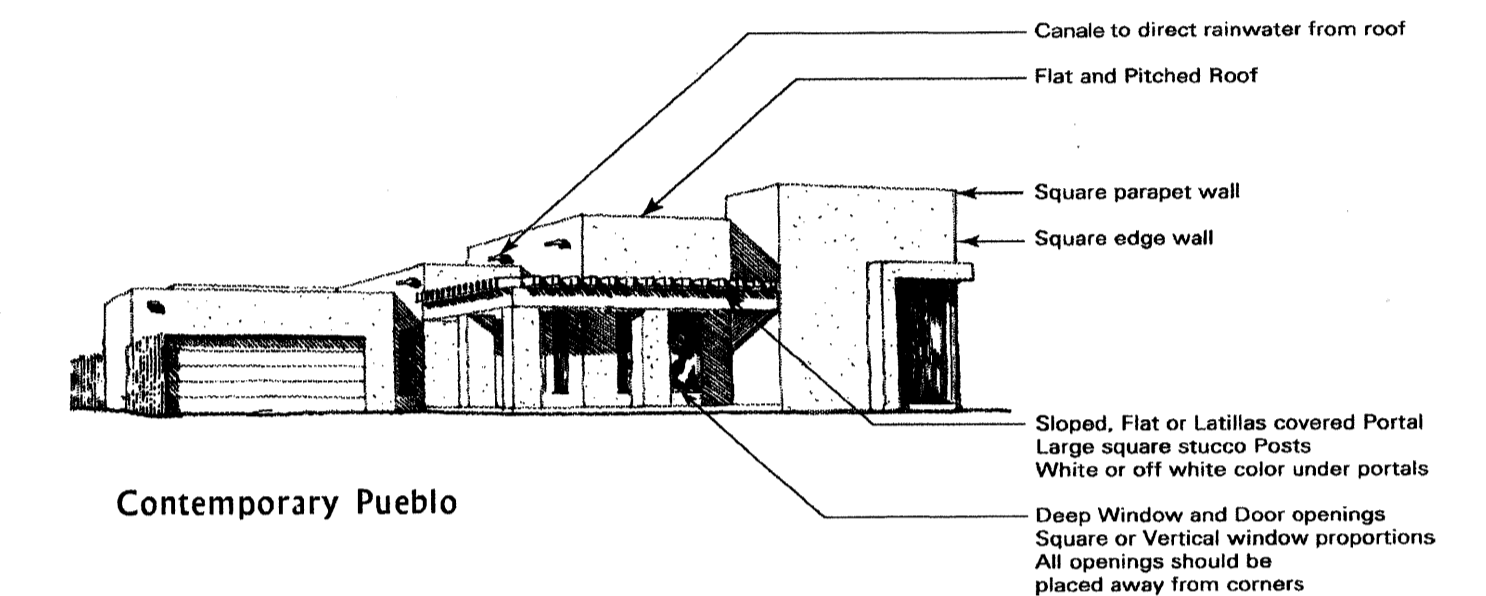
- Development adjacent to the San Antonio Arroyo shall comply with Section F, Design Guidelines for Development Adjacent to Major Open Space Arroyos and Major Open Space Links of the Facility Plan for Arroyos.

ARCHITECTURAL STYLES



Pueblo Revival

Pueblo Revival Style is characterized by either massive adobe brick walls, or wood framing with stucco to give the effect of adobe, projecting vigas, rounded parapets, interspersed with canales, and rounded wood columns with corbels. The flat roof and exterior mud/stucco colors of earth tones are also strong features of this style.



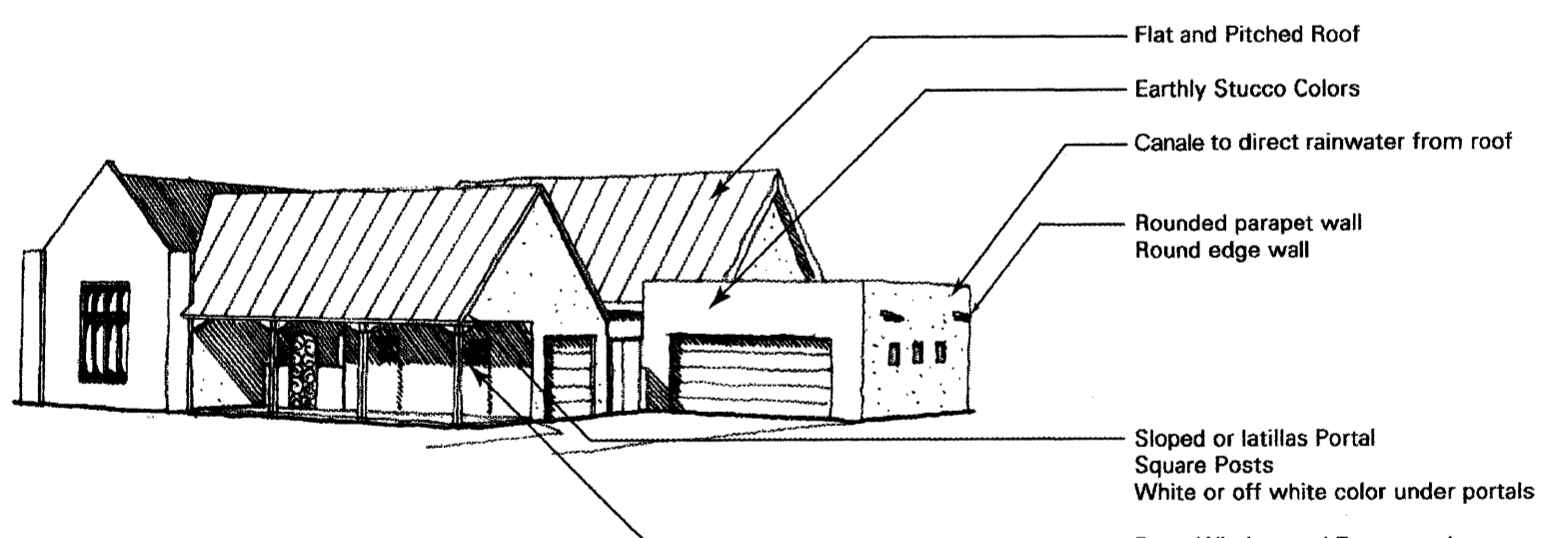
Contemporary Pueblo

Contemporary Pueblo Style is a more modern design which reflects much of the traditional Pueblo style architecture, combined with elements of modern architecture. The severe exterior walls are broken by large window openings that extend from floor to ceiling. Square large stuccoed columns replace the round columns of the Pueblo designs. Exterior colors range from earth tones to much lighter colors.



Territorial

Territorial Style was a departure from the more indigenous designs in this part of the country, and reflected emerging of some of the earlier features with some aspects of the Greek Revival Style. Windows and doors were set near the outside face of the adobe type wall with a wood casing and simple unadorned, pedimented lintel. Portal posts were solid square posts with chamfered corners. The carved corbel was replaced by molding at the top. Ceiling vigas were cut into rectangular beams and brick capped topped the older style mud plaster and adobe parapets. Another important characteristic of the Territorial Style was the introduction to New Mexico of the template roof, an alloy of lead and tin.



Northern New Mexico

Northern New Mexico Style is a more modern design which reflects much of the rural architecture combined with elements of other designs. A steep pitched roof extending over a veranda which extends along the length of the building is a major feature of this style. Square wood columns replace the round columns of the Pueblo designs. Exterior colors usually range from off-white to a wide range of earth tones. In contrast to the smaller windows found in the Spanish Pueblo style, this design has larger windows, some with wood shutters. Northern New Mexico style floor plans include "T", "L", "H", and "U" shapes.

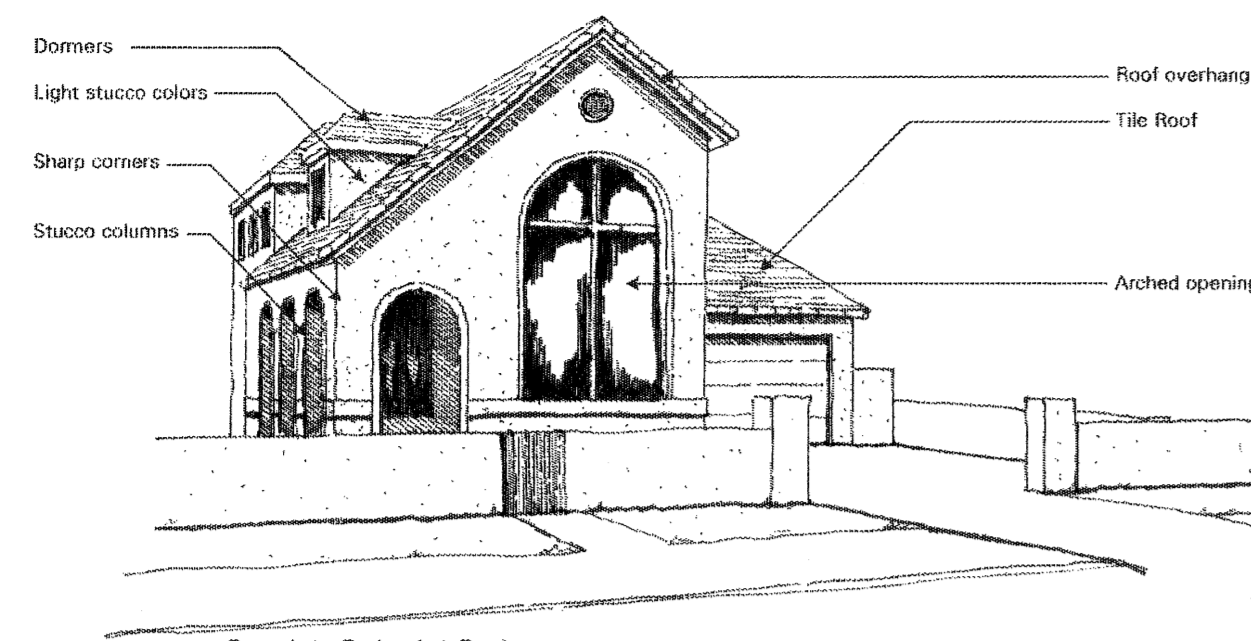
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Andalucia

at La Luz

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Spanish Colonial

Spanish Colonial Style exhibits authentic Colonial architectural elements brought from Spain and features a central courtyard or walled patio. The severe exterior masonry walls are broken only by an ornamental stone entrance. Most rooms open to a columned galeria which extends the entire length of the enclosed garden patio. The flat roof with red tile roof accents over the galeria and wall copings are common features of this design. Exterior colors range from earth tones to much lighter colors.

NON-RESIDENTIAL and MULTI-FAMILY STANDARDS

- Buildings and structures shall comply with all applicable City of Albuquerque zoning, building, and fire codes, as well as other local codes.
- Finished building materials shall be applied to all exterior sides of buildings and structures and shall be consistent on all sides. Any accessory buildings and enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.
- Generic franchise building elevations or canopies are prohibited.
- No plastic or vinyl building panels, awnings, or canopies are allowed. Awnings and canopies, if used, shall be integrated with building architecture.
- Building heights should be kept to a minimum. Maximum height shall be limited to 45 feet for the ridge of the building to correspond with the Coors Corridor Plan.
- Entry ways to non-residential and multi-family buildings shall be clearly defined.
- No freestanding cell towers or antennas are allowed; rather antenna shall be integrated with the building architecture.

SINGLE-FAMILY RESIDENTIAL STANDARDS

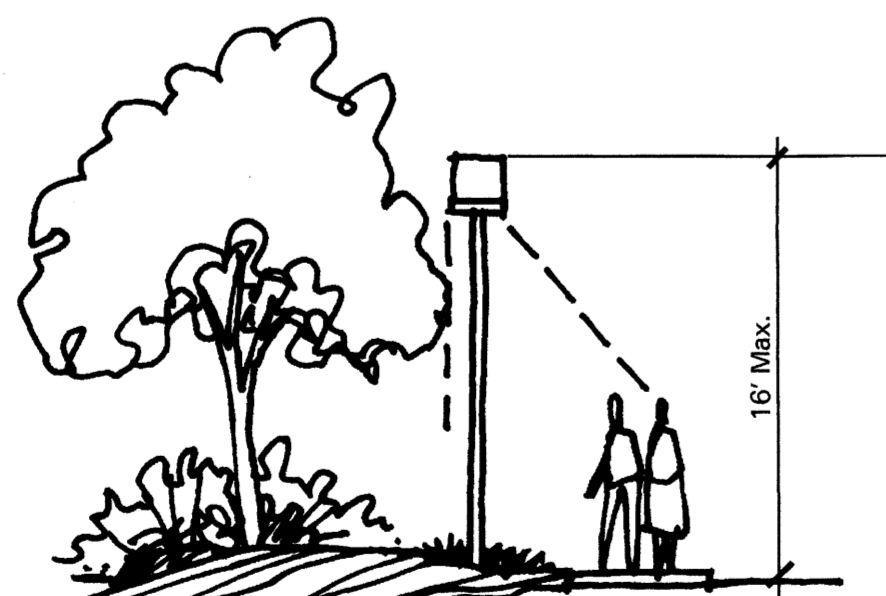
- Lot Size**
 - Minimum residential lot size shall be 3,500 s.f.
- Front Porches**
 - Front porches are required for each dwelling unit.
 - Front porches and/or portals shall have a minimum 6 foot depth.
- Building Colors and Materials**
 - Highly reflective surfaces shall be screened from public view.
 - Stucco shall be the primary building material. Stucco colors shall be limited to shades of brown, tan, sage green, and sand.
 - Accent colors in entry portals, front door, window frames and trim, and other incidental elements shall be shades of white, blue, brown, and green.
 - Windows may be wood, metal or solid vinyl. All windows visible from nearby street and path shall be glazed with clear, non reflective glass. Reflective film is prohibited.
 - Exterior doors, including garage doors, shall be stained or painted wood, or painted metal.
- Roof Colors and Materials**
 - Roofs may be flat, pitched (corrugated metal), or combination of both and shall be made of non-reflective materials.
 - Metal roof colors shall be limited to red, blue, green, brown, and silver.
- Accessory Buildings and Remodels**
 - All accessory buildings and/or building remodeling under 200 square feet are permitted without special approval from the City. Projects that are larger than 200 square feet are required to apply for administrative approval from the City Planning Director.

LIGHTING

In order to enhance the safety, security, and visual aesthetics, careful consideration must be given to both the daytime and night-time appearance of the lighting design and fixtures. The primary design objective of the site lighting system shall be to maximize public safety while not affecting adjacent properties, buildings, or roadways with unnecessary glare or reflection.

In order to accomplish the lighting goals, the following guidelines shall be required for the design of the lighting system:

- Placement of fixtures and standards shall conform to state and local safety and illumination standards.
- All lights shall be shielded source to prevent spillage onto adjoining properties or light pollution of the existing "dark sky". Cobra lights are prohibited.
- The height of street lights and parking area lights shall be kept to a minimum necessary to meet safety requirements.
- Individual site lighting standards shall blend with the architectural character of the buildings and other site fixtures.
- The location of light fixtures shall be identified on subsequent Site Plans for Building Permits. They shall be a maximum of 16 feet in height, consistent with the Coors Corridor Plan.
- No parking or field lighting is allowed in the vicinity of the recreational fields in Tract 3.
- Neon lights are prohibited in the area of Tract 6, adjacent to Tracts 3 and 4, and lying 300 feet north of Learning Road.
- Building-mounted signs shall not exceed 66 percent of the facade area and the lettering shall not exceed 42 foot in height.
- No illuminated plastic panel signs are allowed except business logos within the non-residential areas of the property.



SIGNAGE

The following signage standards were developed to regulate the size, location, type, and quality of sign elements within Andaluca at La Luz. The goal is to provide a signage program that is of high quality, maintains a consistent style, creates a sense of arrival, and complements the visual character of the property.

- All signs shall be in compliance with Section 14-16-3-5 General Sign Regulations of the Comprehensive City Zoning Code.
- Entry signs shall be monument type and shall complement the materials, color, and architectural character of the buildings.
- Free-standing signs shall be designed that do not require any external bracing, angle-iron supports, guy wires or similar devices.
- No signage is allowed that uses moving parts, makes audible sounds, or has blinking or flashing lights.
- Signs shall not overhang into the public right-of-way or extend above the building roof line.
- Off-premise signs are prohibited.
- Building-mounted signs shall not exceed 6 percent of the facade area and the lettering shall not exceed 2 feet in height.
- No illuminated plastic panel signs are allowed except business logos within the non-residential areas of the property.

UTILITIES

To ensure the overall aesthetic quality of the property and the natural environment, the visual impact of utilities and equipment should be minimized by the following:

- All new electric distribution lines shall be placed underground.
- Transformers, utility pads, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way.
- When an above-ground backflow prevention device is required by the City of Albuquerque, the heated enclosure shall be constructed of materials compatible with the architectural materials used as the main elements of the building. If pre-fabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.
- Alleys may be used for dry utilities and sewer, wherever possible.
- Utility easements shall be a minimum of 7.5 feet in width and may be spot easements instead of continuous easements for above-ground structures.

PRIVATE COMMONS AREAS

A defining feature of Andaluca at La Luz is the private commons areas that are spread throughout the community. The City's on-site useable open space requirements are met through a combination of the private commons areas and on-site useable open space.

- Minimum on-lot useable open space shall be as required in the SU-1 Zone: 250 square feet per two bedrooms and 300 square feet for three or more bedrooms.
- Covered outdoor space and balconies shall be considered on-lot useable open space.
- Private commons areas shall be considered as part of meeting the SU-1 useable open space requirements of 2,400 square feet provided the minimum on-lot requirements (as stated above) are met. The private commons areas shall be utilized to meet any deficiency in the 2,400 square foot requirement.

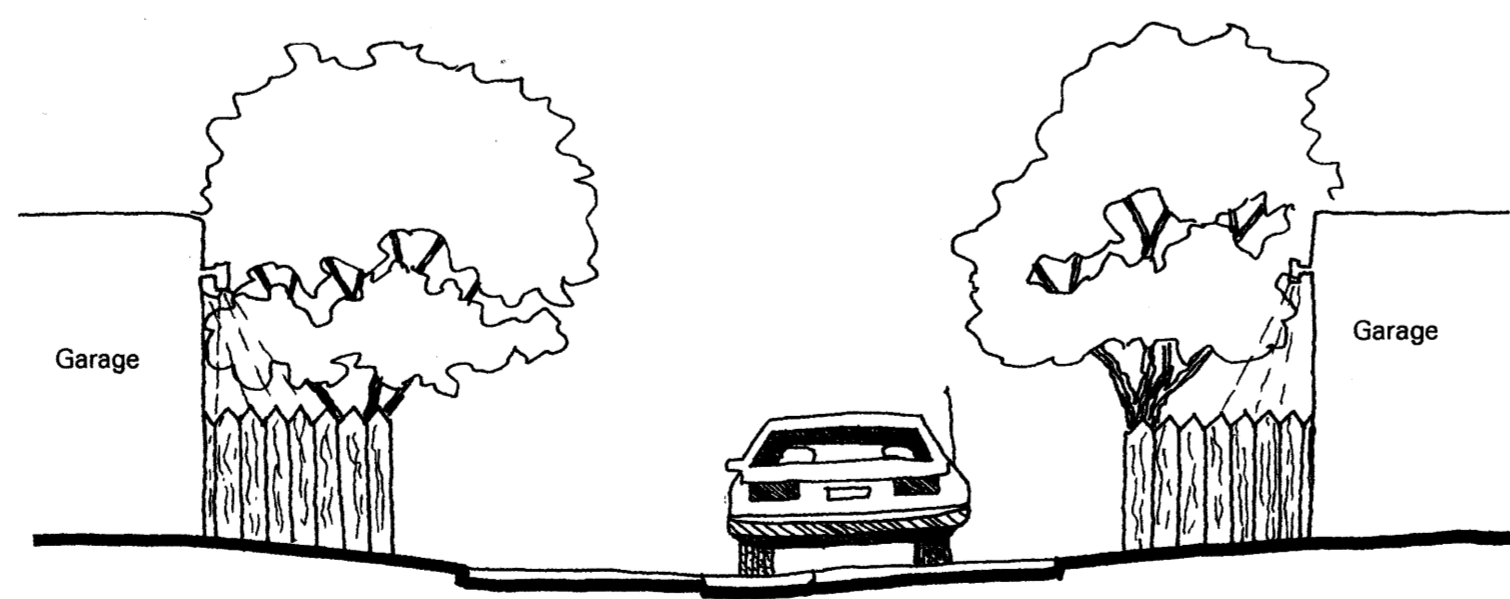
UNIQUE STREET and TRAFFIC CALMING STANDARDS

Much attention has been given to the design of the streetscapes at Andaluca at La Luz. These unique standards, along with the treatment given to garages and front yard setbacks, are critical to the success of creating an active, pedestrian-oriented urban community. The intent is to provide relatively short street blocks with a smaller number of lots in order to make the streets as intimate as possible and still meet the City's DPM standards.

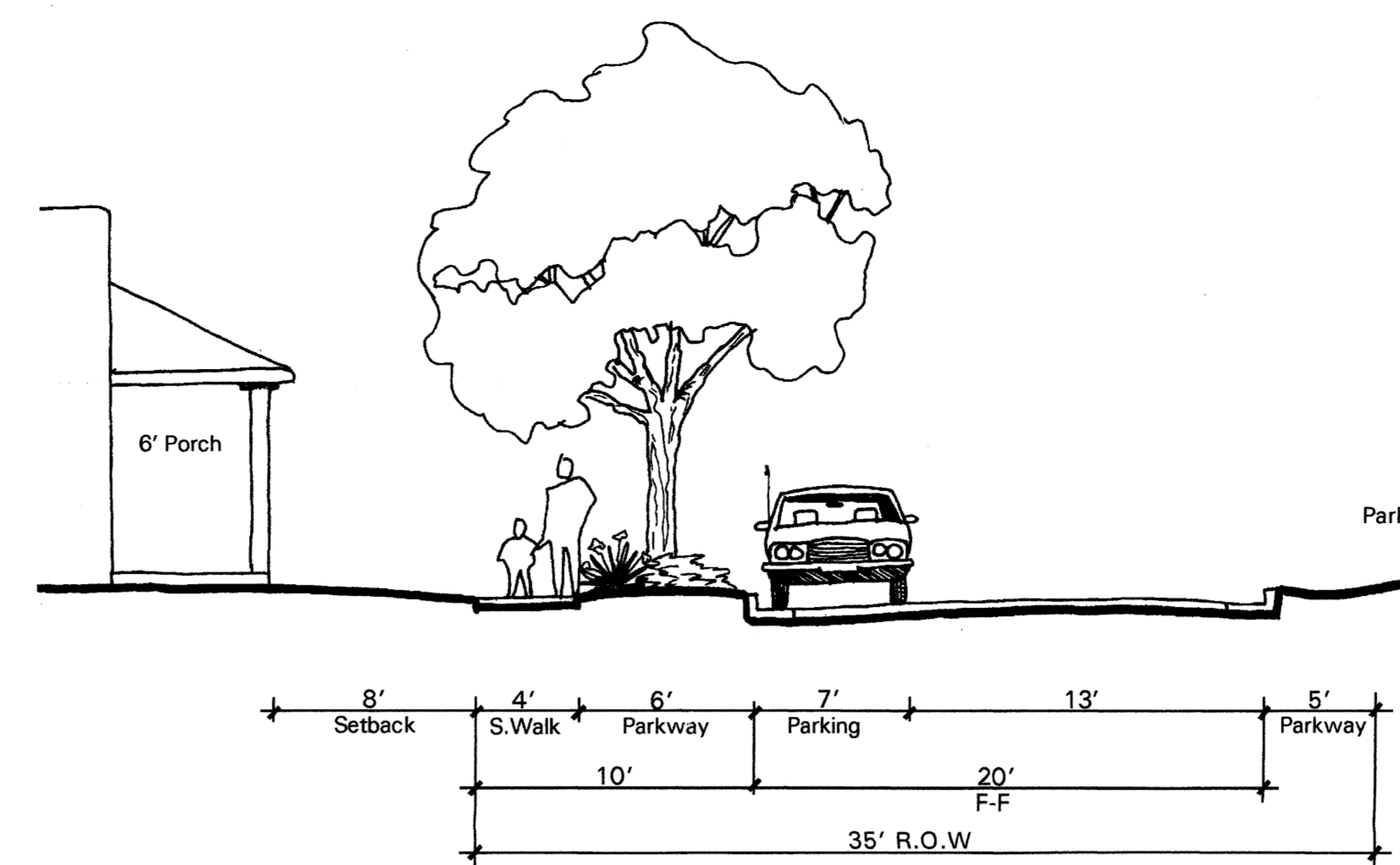
- All street types shall include a 5-6 foot landscaped parkway (actual width depending on street section) and 4 foot sidewalks on both sides of the street.
- Where alley lots are provided, driveway cuts along the roadway are prohibited, and garage and solid waste removal access shall be provided via the alley.
- Shared driveway access between two lots is allowed, with a minimum pavement width of 16 feet, and a 2 foot setback to adjacent structure. Where shared driveway access is provided, garages shall be in the rear of the lot and shall be attached to each other.
- Handicapped ramps shall be provided at each intersection, and shall be designed to direct the wheelchair user to the other side of the roadway.
- Bump-outs are allowed in order to slow traffic and to discourage parking close to certain intersections. Where provided, bump-outs shall protrude 4 feet maximum into the right-of-way.
- Curbs shall have a minimum curve radii of 10 feet in order to shorten the distance at pedestrian crossings and provide greater pedestrian safety.

Street Sections

The following street sections will be constructed at Andaluca at La Luz with each Site Plan for Building Permit. They have been designed in conjunction with the parking standards (see Sheet 2 of 3), and are intended to reduce traffic speeds.



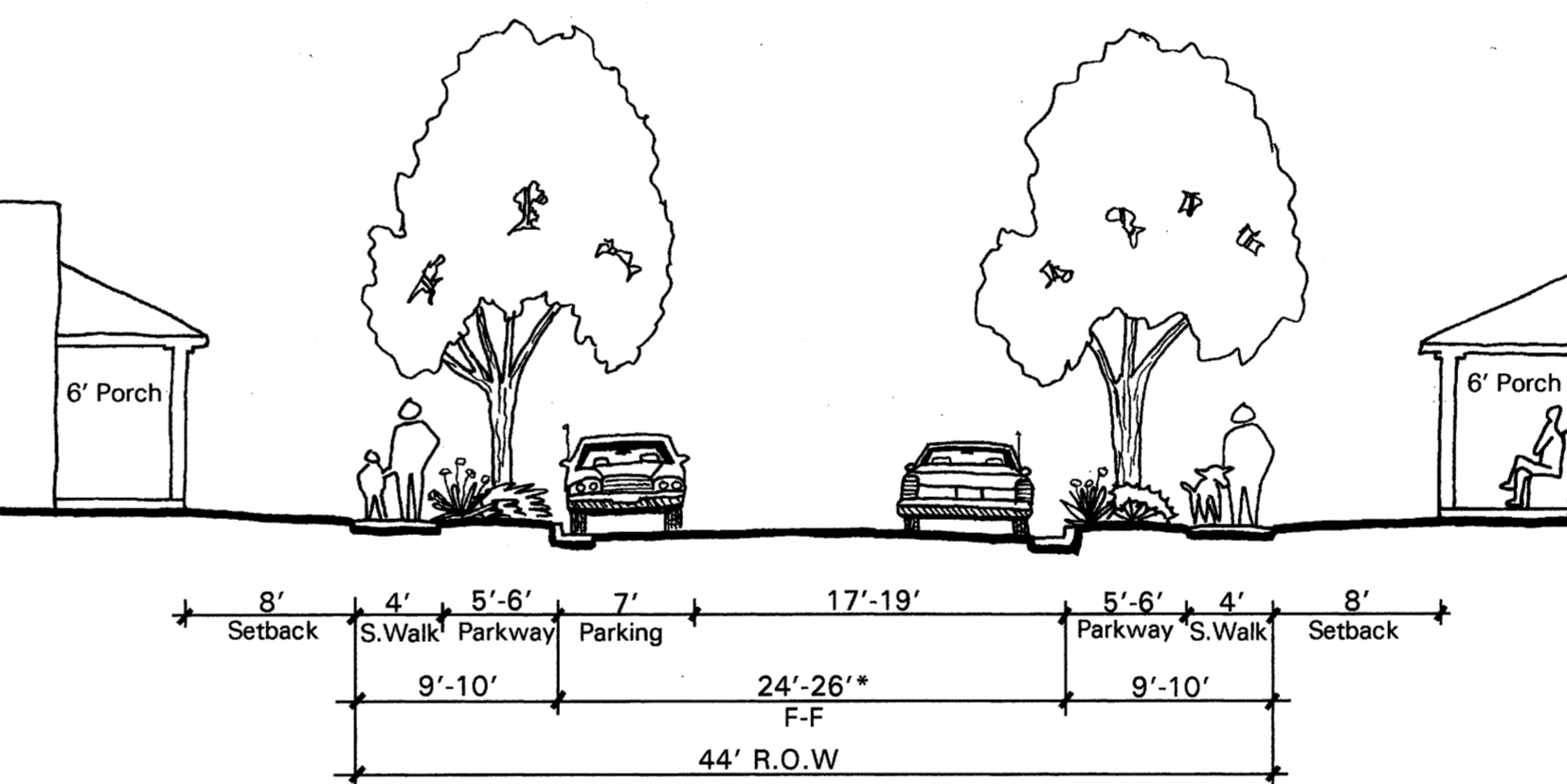
Alley Section 1-Way



35' Street Section 1-Way

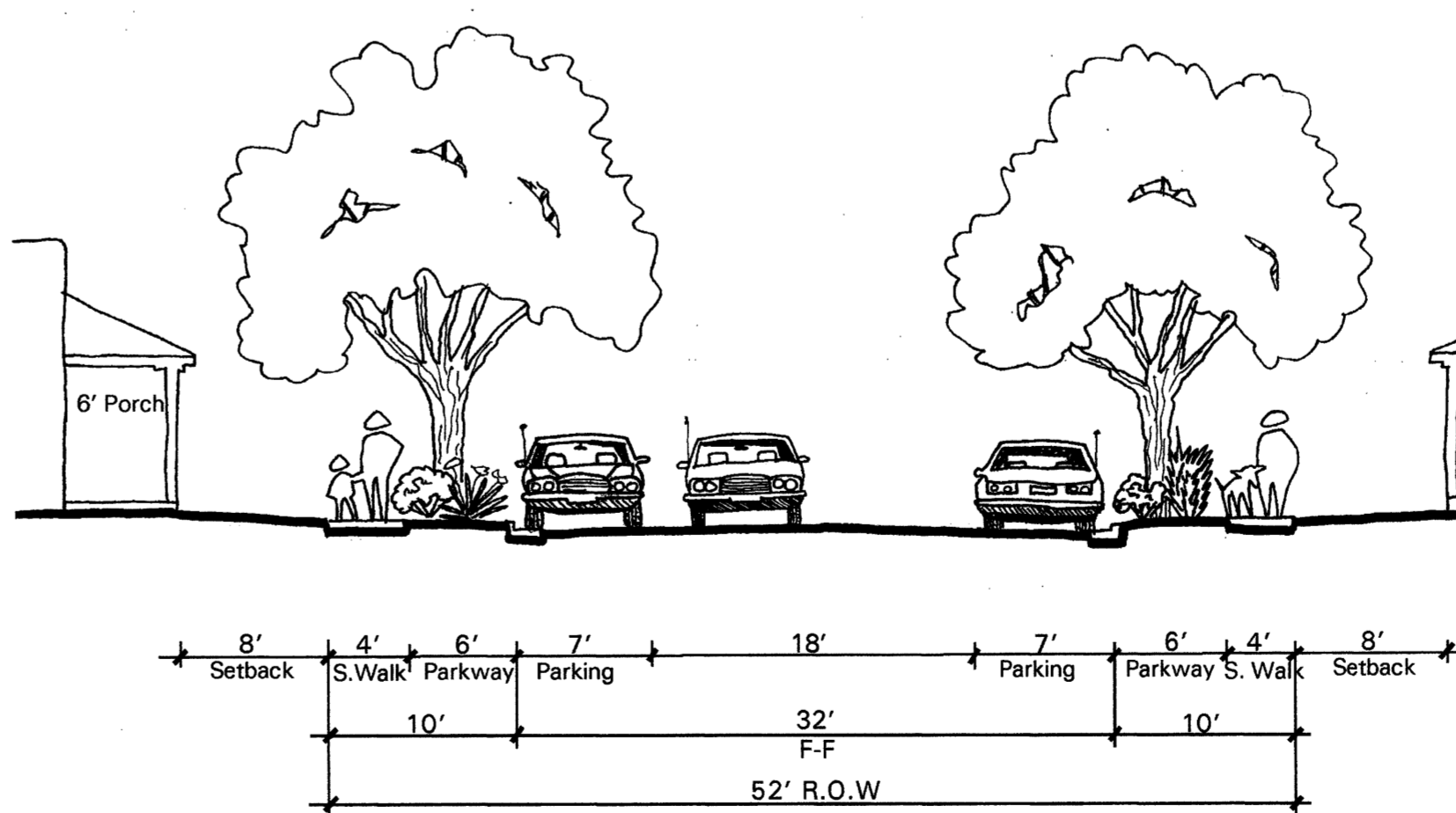


42' Private Drive Section 2-Way



44' Street Section 2-Way

*Note: For future phases, actual pavement width for the 44 foot street section is dependent on the average daily traffic count. For Phase 1 (Tract 1A) - see Site Plan for Building Permit for actual pavement face-to-face dimensions.



52' Street Section 2-Way

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