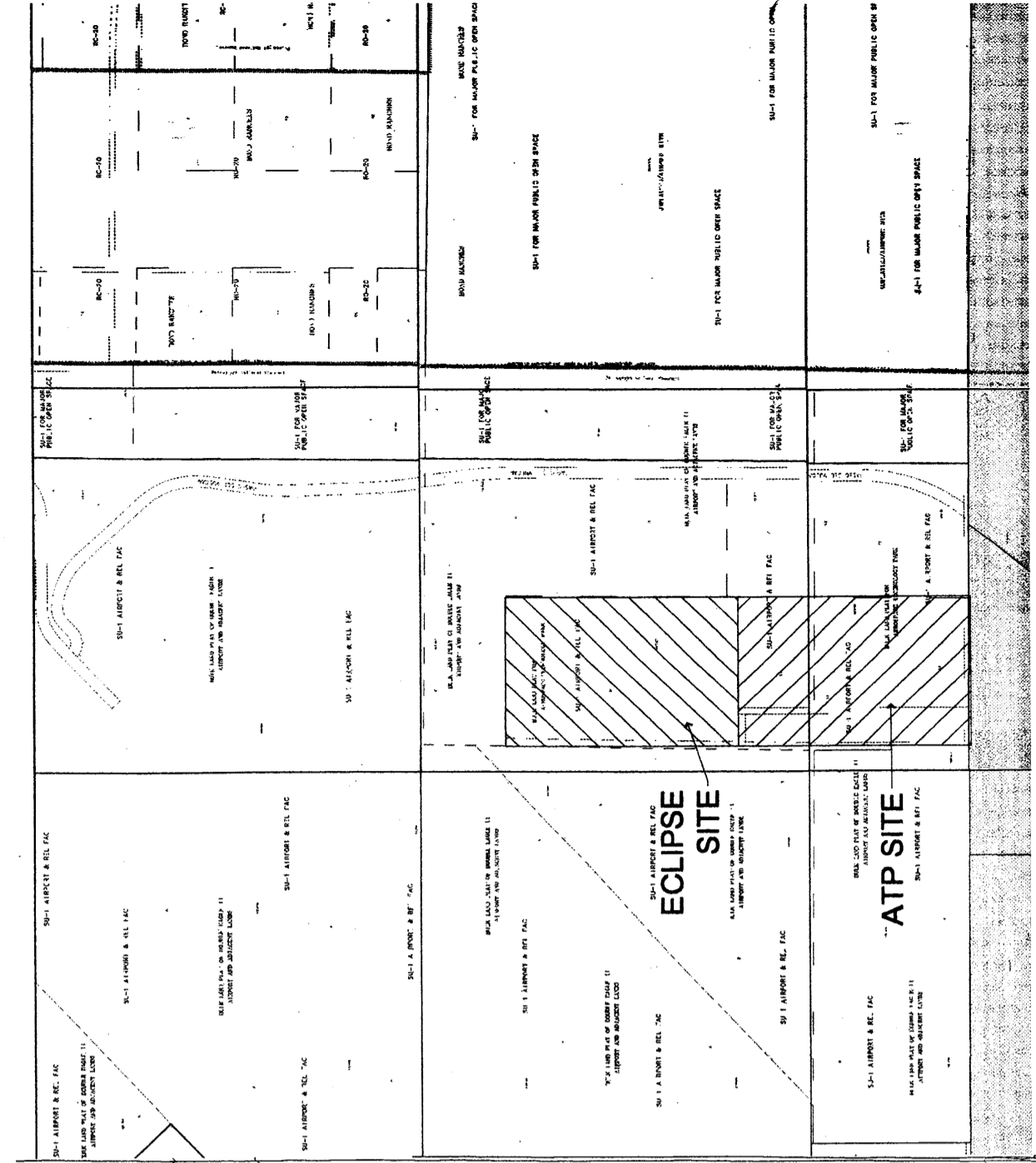


VICINITY MAP
Albuquerque Zone GIS Atlas E-5 TO E-7, F-5 TO F-7, G-5 TO G-7



SITE PLAN FOR SUBDIVISION BACKGROUND: This Site Plan for Subdivision is for the Eclipse Aviation Campus only. (Tract 02054) which includes the Eclipse Aviation Campus property.

SITE PLAN FOR SUBDIVISION REQUIRED INFORMATION

- Site:** Approximately 150 acres, zoned SU-1 for Airport and Related Facilities.
- Proposed Uses:** Airport Related Industrial, Office, Retail, Warehousing, and Manufacturing (See Permissive Uses) with the approval process as follows: In accordance with the City Council Designation of Double Eagle II as a "Certified Site" and with the provision of the Design Standards included in this Site Plan for Subdivision, future Site Development Plans for Building Permit shall be delegated to Building and Safety division and shall not require a public hearing provided they are consistent with this Site Plan, for Subdivision and Design Standards.
- Pedestrian and Vehicular Ingress and Egress:** Access: The primary access to the Eclipse Aviation Campus (EAC) is from Aerospace Parkway located in the Aerospace Technology Park (ATP) property. Access to Aerospace Parkway, via the Shooting Range Access Road (the southern boundary of the ATP), is from Paseo del Volcan (old), which is identified as a Minor Arterial on MRCOG's roadway Functional Classification Map. Paseo del Volcan provides access south to Interstate 40 and north to Paseo del Norte.
- Roadways:** Aerospace Parkway is an 86-foot right-of-way with a 42-foot public street. Access into the Eclipse Aviation Campus development areas shall be from Aerospace Parkway. This entry functions as the primary utility corridor and will accommodate water, sewer, and storm drains. The northern terminus of Aerospace Parkway shall serve as the entrance to the internal private roadways of Eclipse Aviation's Campus. Where gated access is provided, provisions for security and public turn-around space will be provided.
- Pedestrian Access:** Direct pedestrian access to the Eclipse Aviation Campus will be provided from the northern terminus of Aerospace Parkway, where pedestrian facilities are located on both sides of the road.
- Internal Circulation:** In addition to the primary vehicular and pedestrian circulation system, the Eclipse Aviation Campus is designed to accommodate access to a secondary circulation network of private roadways for service and deliveries from sites within the ATP. A private taxiway network connects to the Double Eagle II Airport Taxiway system. This circulation system design is without conflict with any roadway.
- Maximum Building Height:** Maximum building height shall be pursuant to the City of Albuquerque Industrial Park (IP) Zone and Federal Aviation Administration regulations.
- Minimum Building Setback:** 50-foot from Aerospace Parkway, 25-foot from the property line of adjoining properties, and 36-foot from the taxiway or service/delivery pavement.
- Maximum Floor Area Ratio:** .30.
- Landscape Plan:** The landscape design will reinforce the identity of the Eclipse Aviation Campus. The landscape plan will place special emphasis on the main entry to the property, public building entrances and circulation areas, and private recreation and gathering spaces (see Design Standards for more specific information related to site and landscape design).

REG. NUMBER: **1003125**
Application Number: **06 DRG-00701**

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC) dated [Date] and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? () Yes () No If yes, then a set of approved DRG plans with a work order is required for any construction within Public Right-of-Way or for construction of public DRG SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:

Engineering/Transportation Division	Date	10-24-06
<i>[Signature]</i>	Date	4-5-06
Water Utility Department	Date	4/5/06
<i>[Signature]</i>	Date	4/5/06
Parks and Recreation Department	Date	4/5/06
<i>[Signature]</i>	Date	4/5/06
City Engineer	Date	
<i>[Signature]</i>	Date	
NA	Date	
Environmental Health Department (conditional)	Date	
<i>[Signature]</i>	Date	
Solid Waste Management	Date	10/26/06
<i>[Signature]</i>	Date	10/26/06
DRB Chairperson, Planning Department	Date	
<i>[Signature]</i>	Date	

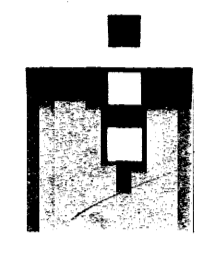
Site Plan for Subdivision

Eclipse Aviation Campus

Prepared for:
Eclipse Aviation Campus

Prepared by:
Dekker/Perich/Sabatini
Molzen-Corbin & Associates

Sheet 1 of 4
March 28, 2006



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MOLZEN-CORBIN & ASSOCIATES
ENGINEERS/ARCHITECTS/PLANNERS

5218001#1003125 PROJECT

1. FRAMEWORK

In order to implement the City's Economic Development Goals, it is crucial that this project include an expedited approval process for specific development projects. In accordance with the City Council designation of Double Eagle II as a "Certified Site" and with the provision of the Design Standards included in this Site Plan for Subdivision, future Site Plans for Building Permit shall be delegated to the Development Review Board and shall not require a public hearing provided they are consistent with this Site Plan for Subdivision and Design Standards.

2. PERMISSIVE USES

A. SU-1 FOR AIRPORT AND RELATED USES

This zone provides suitable sites for a wide range of industrial and commercial uses, provided such uses are conducted in a compatible and harmonious manner within the Aerospace Technology Park at Double Eagle II Airport achieved through a Site Development Plan for Subdivision

PERMISSIVE USES.

- 1) Air separation plant for nitrogen, oxygen and argon only.
- 2) Aircraft sales and service, aircraft storage, and aircraft manufacturing**.
- 3) Bottling plant.
- 4) Cold storage plant.
- 5) Golf driving range.
- 6) Laboratory experimentation, testing, or medical, provided all activities are conducted within a completely enclosed building. Noxious fumes, odor, or dust shall not be emitted from the premises.
- 7) Machine Shop.
- 8) Manufacturing, assembling, treating, repairing, or rebuilding articles.
- 9) Metal processing and painting provided it is conducted within a completely enclosed building.
- 10) Office.
- 11) Office machine equipment sales and repairs.
- 12) One dwelling per premises for a watchman or caretaker on the same premises with commercial or industrial use.
- 13) Printing, publishing, lithographing, blueprinting, or photostating.
- 14) Public utility use or structure and the stations.
- 15) Restaurant.
- 16) Retail sales of the following goods, plus incidental retailing of related goods and incidental service or repair:
 - a) Books, magazines, notions, gifts.
 - b) Cosmetics, notions, gifts.
 - c) Drugs, medical supplies.
 - d) Flowers and plants.
 - e) Gasoline, oil, liquefied petroleum gas**.
 - f) Food and drink, for consumption on premises.
 - g) Fuel and display rooms or buildings for wholesalers, distributors, warehouses, or manufacturers agents, including accessory stock, storage & warehouse space.
 - 17) Services:
 - a) Automobile repairing, but no bodywork; repairing shall be done within a completely enclosed building at least 20 feet from any residential zone.
 - b) Banking, loaning money, including pawn. Drive-in facilities permitted on the condition the vehicle movement plan is approved by the City Traffic Engineer.
 - c) Barber, beauty.
 - d) Car wash.
 - e) Clinic.
 - f) Day care center.
 - g) Dry cleaning, laundry, clothes pressing.
 - h) Florist.
 - i) Health gymnasium.
 - j) Hotel or motel, including incidental uses.
 - k) Photography, except adult photo studio.
 - 18) School, commercial/trade/training facility
 - 19) Signs on-premises as provided in § 14-16-3-5 of the City Comprehensive Zoning Code, and as regulated by the Design Standards included as part of the Site Plan for Subdivision.
 - 20) Sheet metalworking.
 - 21) Storage Structure or yard for equipment, material or activity incidental to a specific construction project, provided it is temporary.
 - 22) Swimming pool.
 - 23) Warehouse, self-storage.
 - 24) Wind Farm energy-generating facility.
 - 30) Wholesaleing.

**Uses that compete with the activities of the existing Fixed Base Operators (FBO's) at Double Eagle II Airport, shall not be permitted to be accessible to the general public, these uses may include: dispensing of aviation fuels and lubricants; ground services and support; tie-down, hangar and parking; aircraft maintenance; aircraft rental; flight training; and aircraft charter and air taxi. These uses will be permitted on the Eclipse Aviation campus as services ancillary to their primary business (i.e. Eclipse Aviation will most likely have fueling capabilities, aircraft maintenance, hangar space, and flight training which will be provided to Eclipse customers, in conjunction with their primary business of manufacturing and selling airplanes).

3. DESIGN STANDARDS

A. INTRODUCTION

These Design Standards are established to provide planners and designers with a design framework to accomplish the following:

- Create an attractive built environment that promotes the unique corporate identity of Eclipse Aviation and supports opportunities for Industrial Airport activities.
- Define a common image for architectural and landscape design, while still providing a broad palette for unique innovation.
- Enable efficient development of quality temporary** and permanent campus facilities necessary for the successful operation of Eclipse Aviation.

** Temporary facilities are buildings and site facilities (including parking lots) that are meant to be used for a period not-to-exceed five (5) years, and that are not subject to Eclipse Aviation Design Guidelines. After five (5) years, temporary buildings and facilities must be removed or improved to meet Eclipse Aviation Campus Design Guidelines.

B. PARKING

The intent of the standards for the development of parking areas is to: mitigate headlights through the provision of landscaping; minimize the visual impact of parking areas; and provide accessible, safe circulation within and adjacent to the parking areas.

- Parking space dimensions shall be per the City of Albuquerque's Comprehensive Zoning Code and Development Process Manual.
- Parking space quantity standards will be appropriate to proposed use.
- Large parking areas shall be subdivided with landscaping and pedestrian circulation corridors.
- Parking areas may be enlarged and/or reduced in order to accommodate shift schedules.
- Clear pedestrian connections shall be provided through parking areas.
- ADA-compliant parking shall be located adjacent to main building entries.
- Alternate paving materials are permitted for both vehicular and pedestrian zones.
- Landscape areas shall be distributed throughout parking areas and shall be designed to reinforce the overall landscape design concept for the EAC.
- City area requirements for parking lot landscaping shall apply.
- Parking shall be screened from contiguous public streets. Screening may include earth berms, perimeter or retaining walls, landscaping, or buildings.
- Car/vanpool preferred parking shall be provided near building entrances and shall be clearly demarcated.
- Temporary parking areas are not subject to EAC design guidelines and shall be allowed for a period of 5 years. After year 5, temporary facilities must be removed or improved to meet EAC guidelines.
- All temporary parking areas must be designated to avoid parking in natural or native areas. These areas will require clearing of vegetation, perimeter demarcation and surface dust control.

C. BICYCLE FACILITIES

The intent of these Design Guidelines is to support alternative modes of transportation on and around the Eclipse Aviation Campus.

- Bicycle parking for employees shall be provided to promote alternative vehicle use. Due to the EAC's remote location, one bicycle rack space per 50 parking spaces is required, and shall be conveniently located near buildings.
- Covered and secured bicycle storage for 10 percent of the required bicycle parking shall be provided.
- A convenient shower facility available to bicyclists and other employees shall be provided.

D. SITE AND LANDSCAPE

The landscape environment serves to enhance the visual dynamic of the development and aid in reinforcing the identity of Eclipse Aviation. The primary focus for site landscape shall be the main entry to the property, public entrances to buildings and public circulation areas, and private recreation and gathering spaces. Vegetation is discouraged along the aviation side of development, including apron areas and land adjacent to the taxi-lane.

The plant palette for the Eclipse Aviation Campus will provide for year-round color and interest, and reflect the natural beauty of New Mexico. The landscape design and complementary site elements, which may include paving, street furniture, lighting, bollards, and environmental graphics, shall be used to reinforce the EAC identity. A clear theme and image for the EAC will be established through the use of these materials.

- All landscape plans shall comply with the City's Water Conservation Landscaping and Water Waste Ordinance and Pollen Ordinance.
- Water harvesting techniques are encouraged.
- A minimum of 15 percent of the developed site area (minus building square footage) shall be devoted to landscape materials.
- Future development areas within a site shall be maintained in their natural condition. If these areas are disturbed, they shall be re-vegetated with native seed mix appropriate for this region.
- Tree planting and/or the use of high albedo paving shall be utilized to reduce heat absorption in developed areas of the site.
- Live plant materials shall cover a minimum of 75 percent of the total required landscaped area.
- Gravel mulch, cobble, and similar materials are acceptable as a top dressing for landscape areas, however, they are not to be considered a focal landscape element.
- An automatic underground irrigation system is required to support all site landscaping. The system shall be designed to avoid over-spraying onto walks, buildings, fences, etc.
- All plant material shall be maintained free of weeds.
- Plant material sizes at the time of installation shall be determined based on availability and species. Large material sizes shall be encouraged in highly visible locations, when available, and where species are known to have good success rates in this region when transplanted in larger sizes.

E. SITE PLANNING

The intent of the following guidelines is to create a pedestrian-friendly, functional environment for employees and visitors.

All temporary buildings in public areas and visible from public areas shall be similar to permanent buildings in relation to color, material, site planning and design.

- Organization of the Eclipse Aviation Campus will be based on appropriate adjacencies and principles of campus design.
- EAC facilities shall be connected by a hierarchical network of vehicular and pedestrian circulation corridors.
- EAC facilities shall be oriented on circulation corridors, open spaces or the runway.
- Perimeter walls, if provided, shall comply with the intent of the City's Design Manual for Subdivision Access and Perimeter Walls. Chain-link is allowed for airport security fencing purposes as well as where out of public view. The use of razor or barbed wire, wood fencing, or plastic vinyl fencing is not permitted unless required by airport regulations.
- Alternate paving materials are permitted for both vehicular and pedestrian zones.
- Accessible routes shall be provided between buildings, and between buildings and accessible parking.

F. SETBACKS

The use of building and parking area setbacks is required to provide spaces for the creation of visually attractive streetscapes and provide a safe and compatible environment for aircraft, service vehicles, and pedestrians throughout the EAC. Required within these setbacks will be pedestrian walkways, screening devices, and landscape improvements.

- Buildings shall be located according to the following minimum setback dimensions:

- 50 feet from the right-of-way line at terminus of Aerospace Parkway
- 25 feet from the property line of other adjacent properties
- 38 feet from existing taxi lane pavement
- Not closer to the airfield than the Building Restriction Line as shown on the Airport Layout Plan, 2003
- Maintain clear line of site from air traffic control tower controller positions to airfield pavements on the airfield

- Parking areas shall be setback as follows:

- 25 feet from the right-of-way line of Aerospace Parkway
- 10 feet from the property line of other adjacent properties

G. ARCHITECTURE

In general, all buildings shall comply with the current City of Albuquerque Zoning Code. However, in cases where the EAC requirements exceed the Zoning Code, the EAC standards will govern. The focus of these architectural standards and guidelines is for the permanent campus facilities.

Development Densities

ATP Park is currently master-planned for an overall Floor Area Ratio (F.A.R.) of .30. EAC shall not exceed an overall (F.A.R.) of .30.

Building Heights

- Building height limits will be enforced in accordance with the City of Albuquerque Zoning Code for the Industrial Park (I-P) Zone and Federal Aviation Administration regulations.
- The Federal Aviation Administration (FAA) limits building and signage heights to below Federal Aviation Regulation (FAR) Part 77, Objects Affecting Navigable Airspace, surfaces including a 7:1 (horizontal to vertical from a distance 250 ft. from runway centerline) Transitional Surface and the Horizontal Surface located 150 ft. above airport elevation. Additionally, FAA FAR Part 77 states that any structure penetrating the plane created by a 100:1 sloped surface beginning at the nearest point or the nearest runway must be screened by FAA in accordance with Part 77. Notification is required by completing and submitting SF 7460, Notice of Proposed Construction of Alteration, to the FAA for review and airspacing.

Building Entrances

- The major entry to each building should face the primary circulation corridors that defined by a campus environment.
- Entries shall be clearly defined and connect to pedestrian linkages.

Service/Loading Areas

It is assumed that most, if not all, buildings in the EAC will have a service and/or a loading area. All service functions, with the exception of airplanes viewed from taxi-lanes, shall be screened from public view. This includes but is not limited to: exterior refuse facilities, mechanical equipment, storage yards, and loading docks. Screens should be compatible with the overall building design of Eclipse Aviation Campus.

Aviation Storage and Parking Areas

- The EAC facilities located adjacent to the airport taxi-lane system or service corridors are assumed to support aviation related activities, which may require the outside storage of aircraft on-site.

Context

- New permanent buildings should be designed to harmonize with other Eclipse Aviation Campus buildings.

Building Design

- Buildings should convey an image appropriate to a state-of-the-art corporate industrial aviation-manufacturing environment, and celebrate the industrial scale and creative nature of aviation related activities.

- Building design integrated with site planning will create pedestrian friendly areas on campus and encourage a unique relationship for the pedestrian to the industrial scale.

- Special attention shall be given to the articulation of aircraft hanger facilities through use of color, materials, or building offsets. Metal structures are acceptable, however, they must visually tie to other building elements, such as the front office, storage areas, etc.

Equipment

- All roof top equipment and appurtenances shall be integrated into architectural design of the facility.
- All ground-mounted equipment shall be architecturally screened and enclosed with the top of the equipment below the top of the screen. This includes, but is not limited to: water backflow preventers, gas meters, electric meters, exposed conduit, cabling, and wiring.

Portable/Temporary Buildings

- Generally temporary buildings will have restricted access from public pedestrian traffic circulation and will be designed for employee egress/circulation.
- All portable buildings, both temporary and permanent must be screened from the public street entry (Aerospace Parkway) to EAC property.

- Permanent portable and temporary buildings must be compatible with Architecture of campus buildings and with materials and colors of EAC guidelines.
- Temporary buildings are not subject to EAC guidelines and shall be allowed for a period of 5 year. After year 5, temporary facilities must be removed or improved to meet EAC guidelines.

Materials and Colors

Building materials and colors shall reflect the location, character and identity of EAC. Acceptable materials include stucco, concrete (sand blasted or finished surface) stone, masonry, glass curtain wall, glass block, brick, exterior ceramic, metal and EIFS.

- The use of accent colors and materials is encouraged to bring out detailing to better articulate or give scale to a building.

Sustainability

- Green architectural design is encouraged. The EAC promotes the use of sustainable design principles, environmentally-responsible building concepts, and earth-friendly products. EAC supports the use of solar photovoltaic, wind turbines, and other alternative energy production facilities on site.

Plan Arrangement Opportunities

- Open courtyard designs may be employed to form transitions between parking areas and buildings.

H. LIGHTING

A consistent lighting theme for common areas of the EAC will contribute significantly to the EAC's overall aesthetic character. Safety and security are primary design considerations.

- All light fixtures including streetlights, parking area, walkways and entry shall not exceed a maximum height of 30 feet.
- Lighting shall be fully shielded so that no fugitive light may escape beyond the property line.
- Lighting shall not impinge upon airfield sight planes. Lighting shall not be directed at Double Eagle II aircraft movement areas, runway approaches, or aviation towers.
- Controlled, directional lighting should be used to highlight public spaces and walkways.
- The pattern of light pooling from each fixture shall avoid glare or reflection on adjacent properties, buildings, or roadways.
- Exterior lighting fixtures should relate stylistically to the architecture theme of the EAC.
- All fixtures and design shall comply with the New Mexico Night Sky Protection Act and City of Albuquerque Zoning Code, §14-16-3-9 Area Lighting Regulations.

I. SCREENING/BUFFERING

- Refuse enclosures shall be fully screened from pedestrians or motorists. Screening shall be compatible with materials and design of the campus.
- Walls or landscaped berms 2½ to 3 feet in height may be provided to screen parking areas adjacent to the EAC property entry. Walls, if used, shall be designed to integrate with materials and design of campus.

J. STREET DESIGN

Aerospace Parkway serves as the entry to EAC, the public street terminates at the entrance to EAC property.

K. SIGNAGE

- EAC Developer shall provide entry monument signs for the project to create a sense of arrival and to contribute to the unique identity of the project. These signs shall be located on the EAC property.
- Entry monument signs shall be a maximum of 20 feet in height and 300 square feet in sign face.
- Directional signs may be provided, at the discretion of the EAC Developer throughout the EAC to direct visitors and delivery services.
- The Eclipse Aviation Campus, this site shall be permitted to have two entry signs not to exceed 20 feet in height and 300 SF of sign face.

L. TAXILANE/AVIATION AREAS

Aircraft circulation throughout the EAC shall be developed to minimize interaction with vehicular traffic. Taxi-lane guidelines are based on an average tailing speed of at least 20 mph and wingspan and wingtip clearance. Taxi-lane access must have a minimum right-of-way width of 150 feet with a minimum paved surface area 50 feet in width.

- Taxi-lane Shoulders must be either stabilized or paved to reduce possibility of blast erosion and engine ingestion problems.

- Minimum separation between centerline of taxi-lane to structure (parked aircraft, structure, etc.) is 0.70 times the wingspan of the most demanding airplane, plus 10 feet. Seaback standards have been designed to accommodate all Type I and II aircraft.

Taxilane Design Standards

Airplane Design Group Taxilane Width/Taxilane Object Free Area Width/Taxilane Shoulder Width/Wing Span (w)
Airplane Design Group 25 ft. 79 ft. 10 ft. w < 49 ft. 135 ft. 115 ft. 10 ft. 49 ft. < 4= w 79 ft. 162 ft. 20 ft. 79 ft. < 4= w 118 ft. 175 ft. 225 ft. 25 ft. 118 ft. < 4= w 171 ft.

M. WIRELESS TELECOMMUNICATIONS FACILITIES

Wireless telecommunications facilities are allowed within the EAC, with approval by the FAA and the City Aviation Department.

Eclipse Aviation Campus

Prepared for:

Eclipse Aviation Campus

Prepared by:

Dekker/Perich/Sabatini

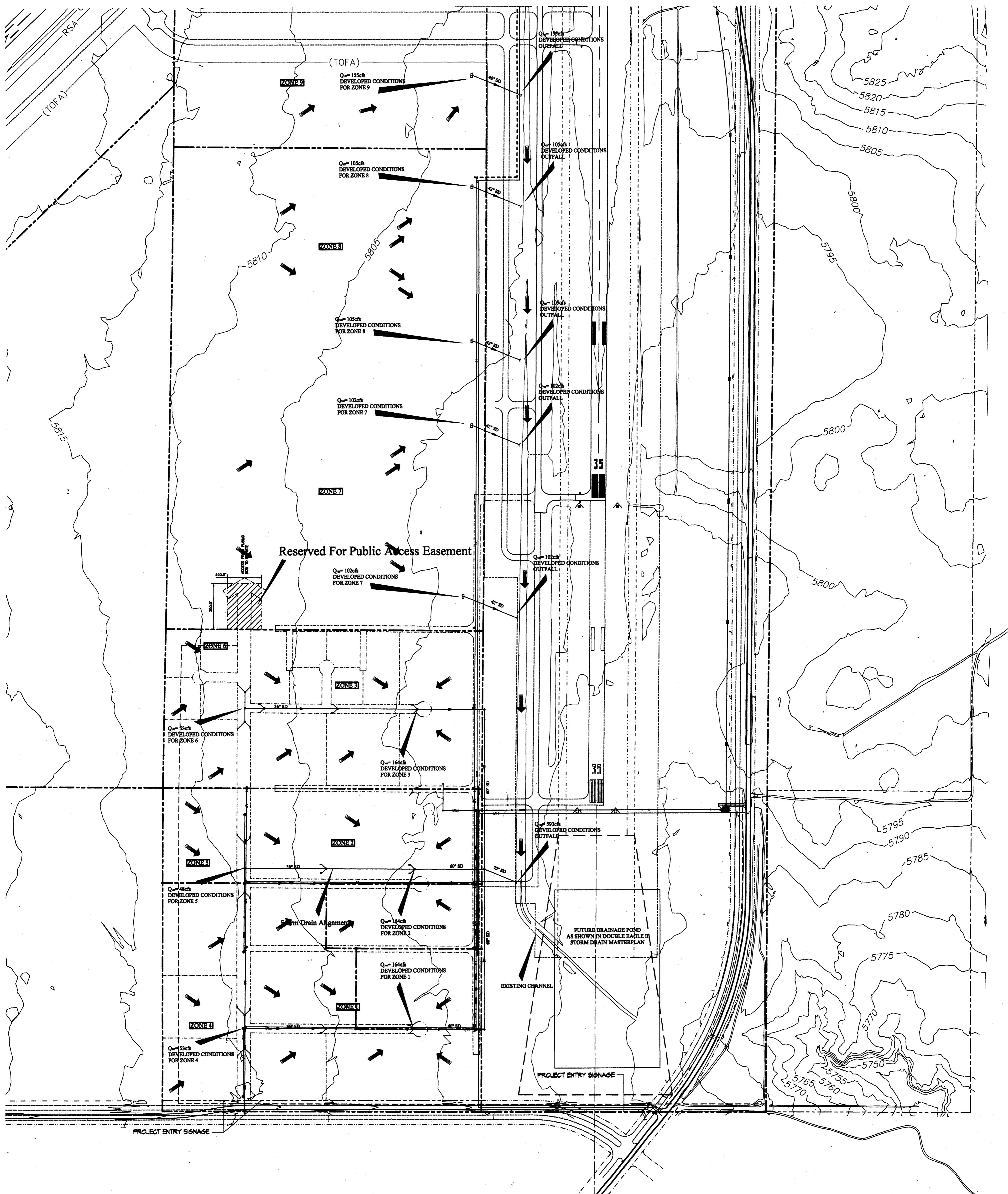
Molzen-Corbin & Associates



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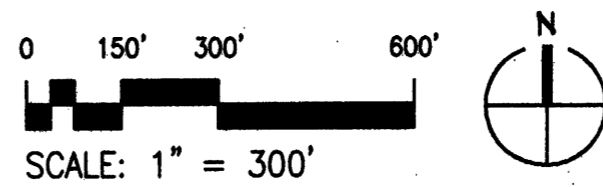
Sheet 2 of 4

March 28, 2006



LEGEND

- PROPOSED STORM DRAIN
- ZONE BASIN BOUNDARIES
- FLOW DIRECTION
- EXISTING CONTOURS

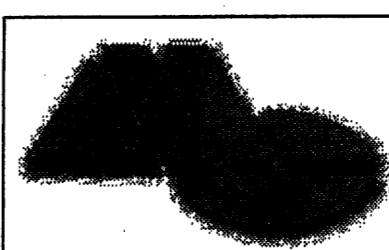


Grading and Drainage Eclipse Aviation Campus

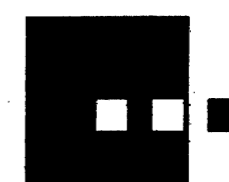
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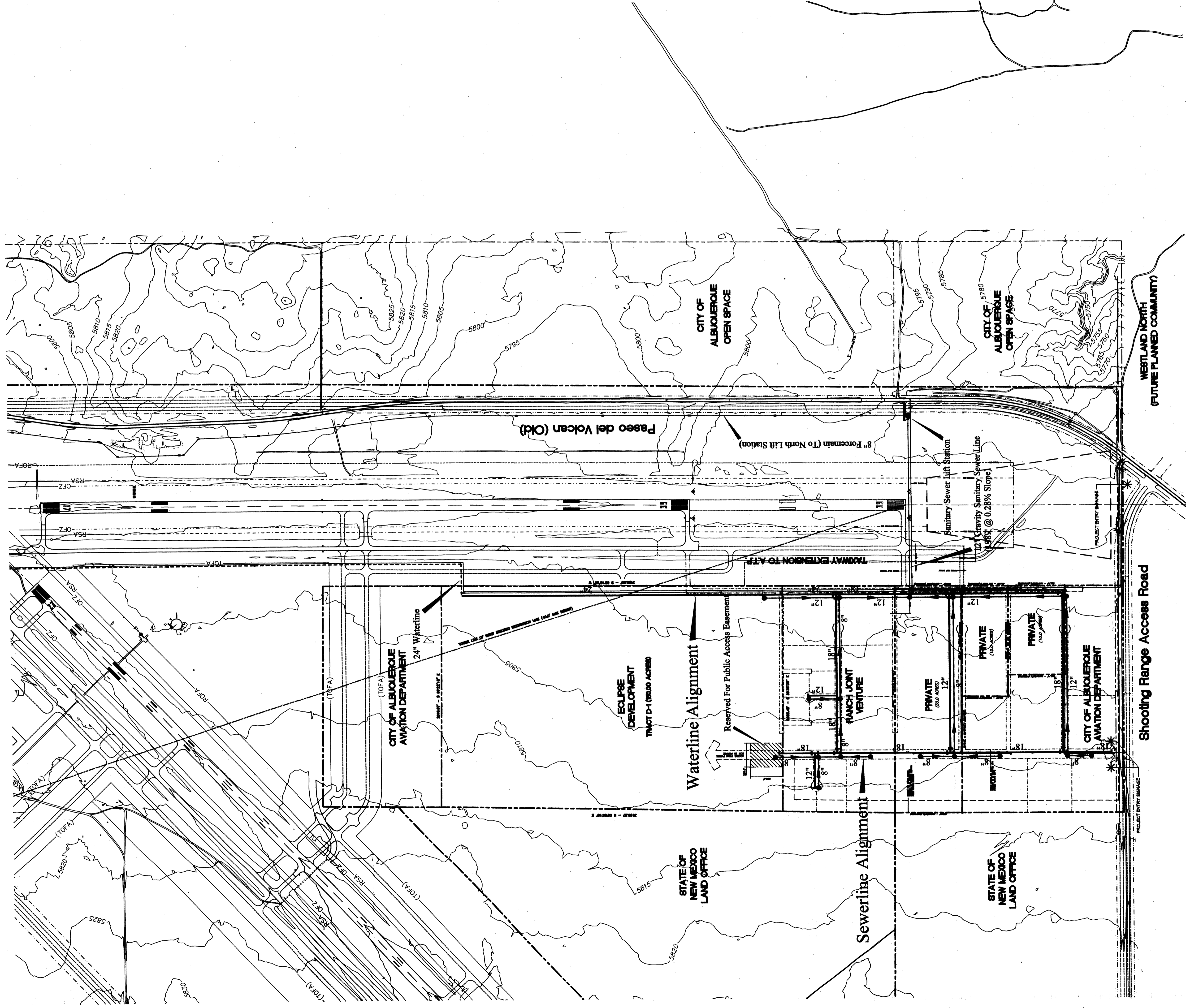


MOLZEN-CORBIN & Associates
ENGINEERS/ARCHITECTS/PLANNERS

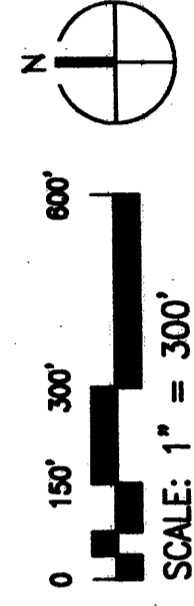


Dekker/Perich/Sabatini
architecture • interiors • planning • engineering

Sheet 3 of 4
March 28, 2006



Legend
 --- Proposed Sanitary Sewer Line
 --- Proposed Waterline



Conceptual Utility Plan
 Alignment

Eclipse Aviation
 Campus

Prepared for:
 Eclipse Aviation Campus

Prepared by:
 Dekker/Perich/Sabatini
 Molzen-Corbin & Associates



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