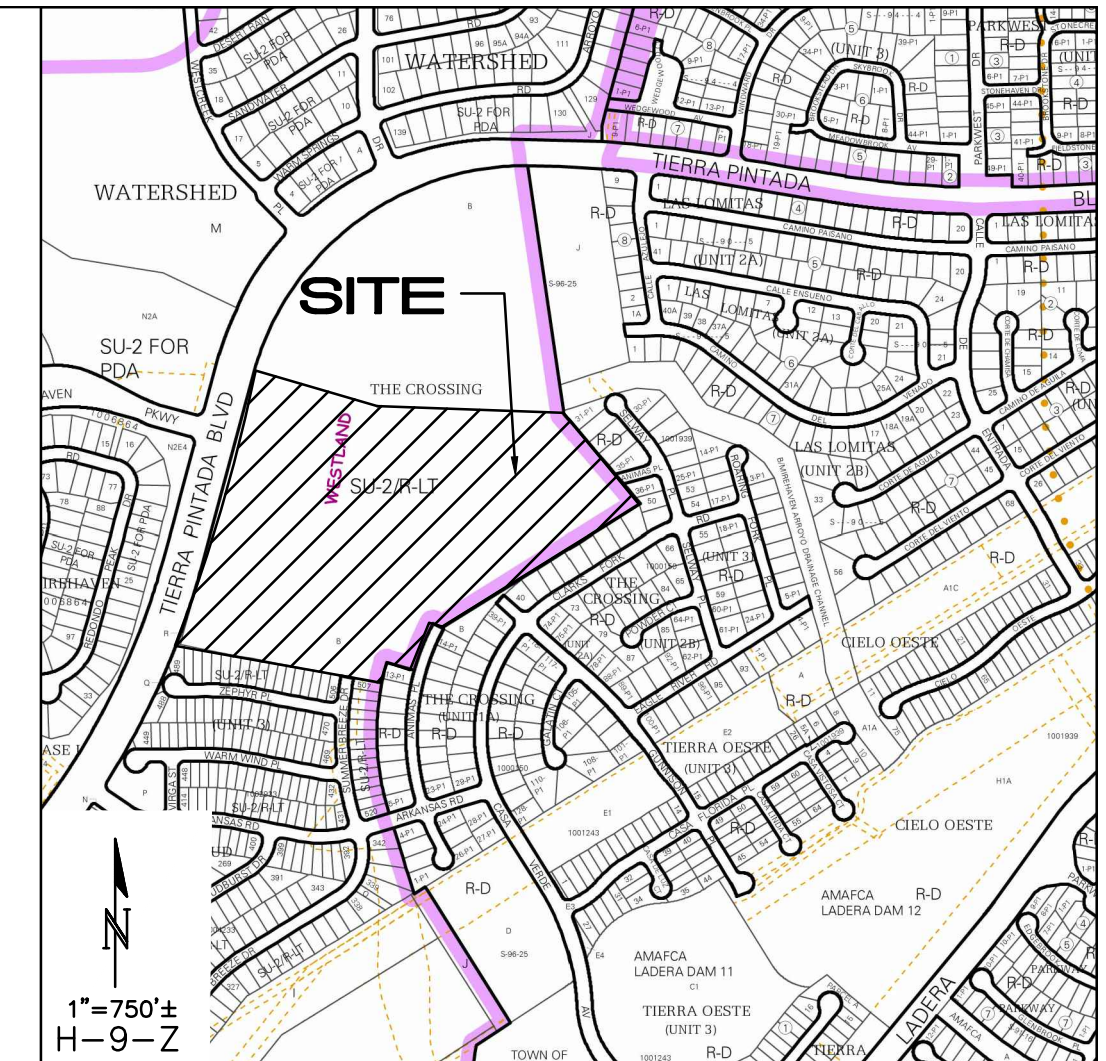


TIERRA PINTADA PHASE I  
CITY PROJECT #650272

CHANNEL, CULVERT & STREET  
IMPROVEMENTS BY OTHERS  
(CITY PROJECT #650272)

BOUNDARY CURVE TABLE						
CURVE	LENGTH	RADIUS	TANGENT	CHORD	CHORD BEARING	DELTA
C1	305.79'	3000.00'	153.03'	305.66'	N18°05'56"E	5°50'25"
C2	1444.55'	1100.00'	847.76'	1342.96'	N52°48'00"E	75°14'32"
C3	284.65'	4546.13'	142.37'	284.60'	S87°45'27"E	3°35'15"
C4	190.45'	762.00'	95.72'	189.95'	S50°44'51"W	14°19'12"
C5	208.45'	762.00'	104.88'	207.80'	S35°45'41"W	15°40'25"
C6	197.52'	920.00'	99.14'	197.14'	S21°46'26"W	12°18'05"
C7	84.44'	1032.00'	42.24'	84.41'	S13°16'45"W	4°41'16"



PROJECT NOTES:

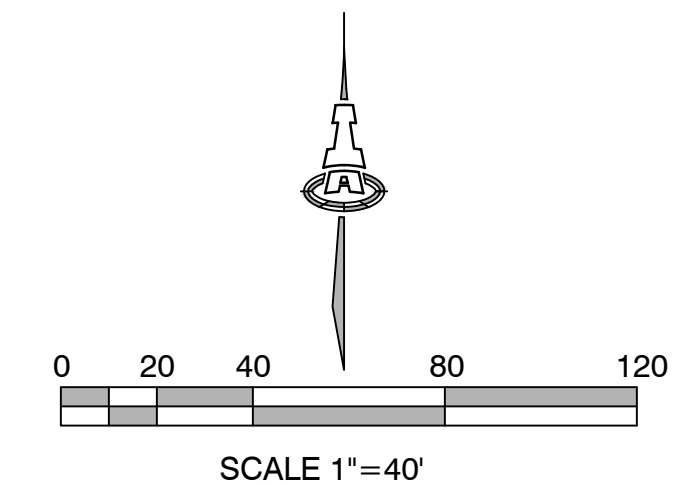
- EXST LEGAL: TRACTS B & J, THE CROSSINGS
- BENCHMARK: ACS MONUMENT 4-H9  
ELEVATION: 5209.315 (NAVD 1988)
- AREA: UNIT 4 AREA=31.03 AC.
- ENGINEER: ASA NILSSON-WEBER  
ISAACSON & ARFMAN, P.A.
- SURVEYOR: RUSTY HUGG  
SURVTEK, INC.
- FLOOD PLAIN: THE SITE LIES WITHIN ZONE "X",  
ZONE "AO" AND ZONE "AE"  
PER FIRM MAP 35001C03266.

NOTES:

- STREET GRADES ARE AT FLOWLINE.
- SEE SHEETS 5-8 FOR RETAINING WALL GRADES AND SECTIONS.
- SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN

LEGEND

- 5270 --- EXISTING CONTOUR
- 52 --- PROPOSED CONTOUR
- ◆ 78.3 PROPOSED ELEVATION
- 78.9 PROPOSED ELEV. AT TIERRA PINTADA R/W
- PG=5272.1 PAD GRADE ELEVATION
- ==== STANDARD (STD) CURB & GUTTER
- ==== MOUNTABLE (MTBL) CURB & GUTTER
- S --- STORM DRAIN W/ MANHOLE
- S --- STORM DRAIN INLET
- DW --- PROPOSED RETAINING WALL (DESIGN BY OTHERS)
- DW --- RECOMMENDED DRIVEWAY LOCATION



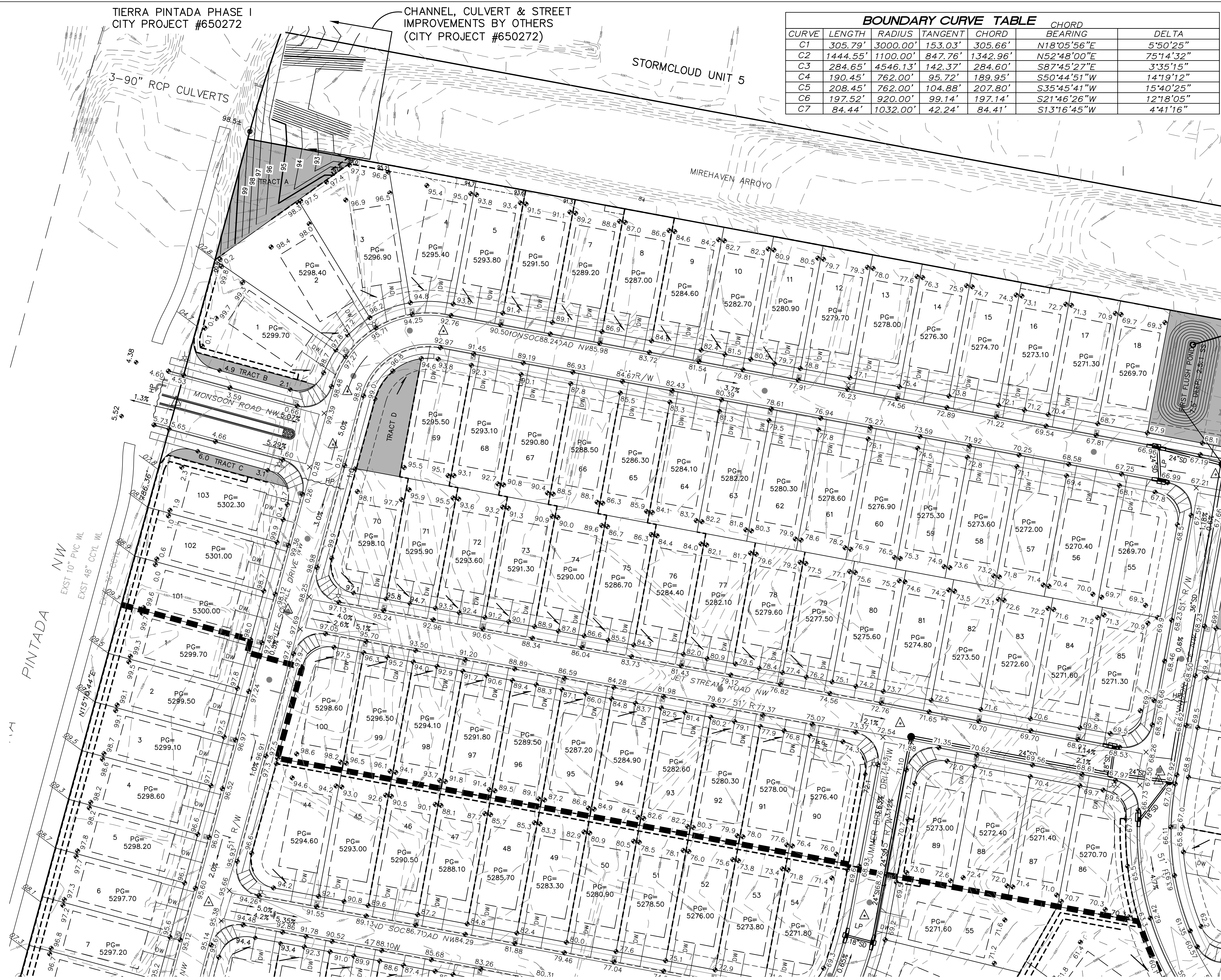
**ISAACSON & ARFMAN, P.A.**  
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Ph. 505-268-8828 www.isaacson.com

8-11-16

1821 CG-101.dwg Aug 11, 2016

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<b>STORMCLOUD SUBDIVISION UNITS 4A &amp; 4B PULTE HOMES</b>	
<b>GRADING AND DRAINAGE PLAN NW</b>	
Date: 08-2016	Job No. 2174
Drawn By: ANW/THOR	<b>SHEET 1</b>
Ckd By: ANW	<b>OF 9</b>



SEE SHEET 2

SEE SHEET 1



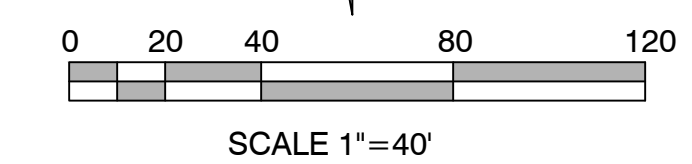
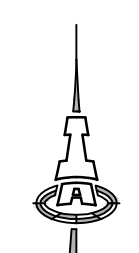
SEE SHEET 3

NOTES:

1. STREET GRADES ARE AT FLOWLINE.
2. SEE SHEETS 5-8 FOR RETAINING WALL GRADES AND SECTIONS.
3. SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN

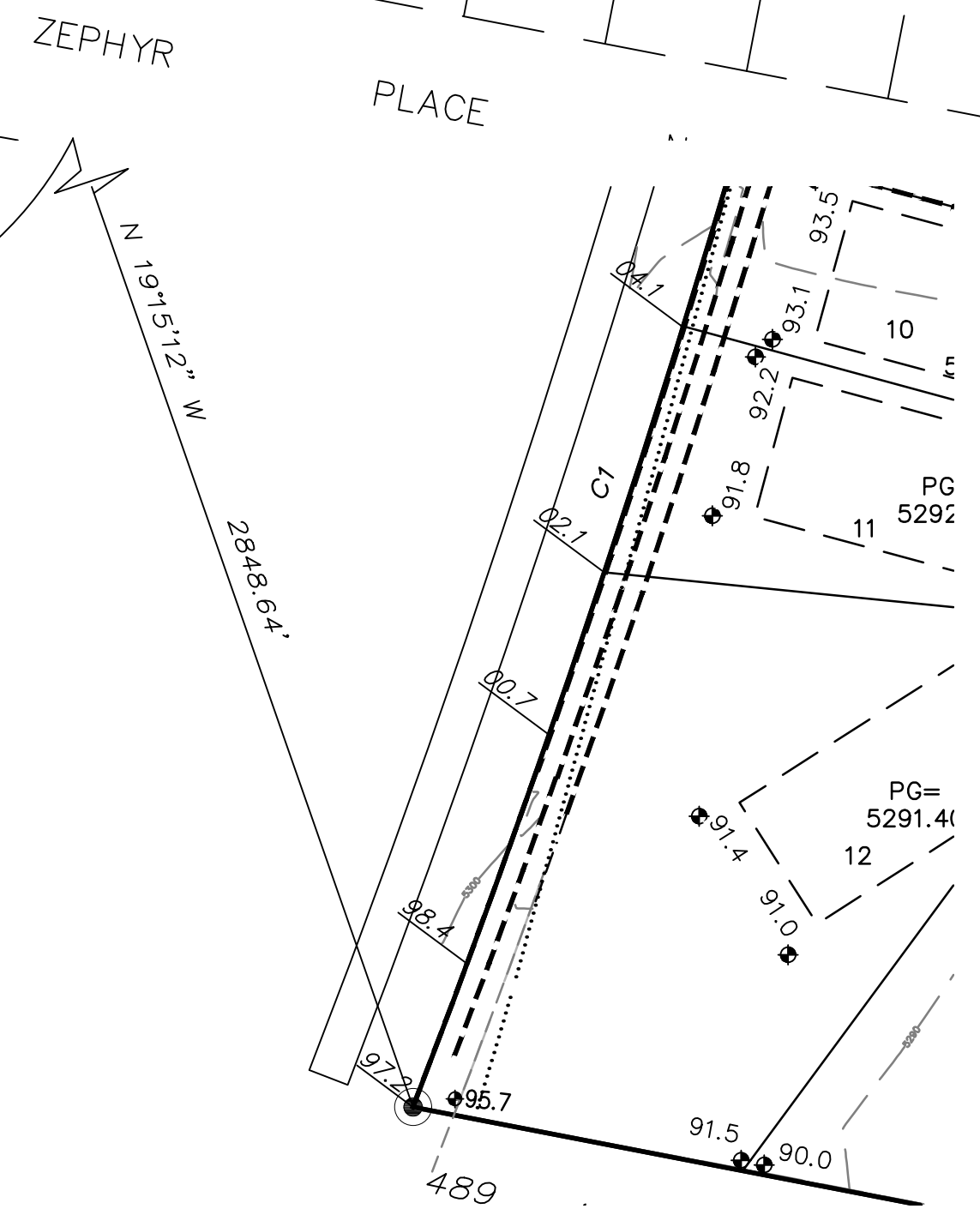
LEGEND

- 5270 --- EXISTING CONTOUR
- 52 --- PROPOSED CONTOUR
- + 78.3 PROPOSED ELEVATION
- 78.9 PROPOSED ELEV. AT TIERRA PINTADA R/W
- PG=5272.1 PAD GRADE ELEVATION
- ==== STANDARD (STD) CURB & GUTTER
- ==== MOUNTABLE (MTBL) CURB & GUTTER
- STORM DRAIN W/ MANHOLE
- STORM DRAIN INLET
- - - - - PROPOSED RETAINING WALL (DESIGN BY OTHERS)
- DW --- RECOMMENDED DRIVEWAY LOCATION



SEE BELOW FOR ACS MONUMENT TIE

Albuquerque Control Survey Monument "BH-41"  
 New Mexico State Plane Coordinates  
 (Central Zone - NAD 83) as published  
 North= 1,496,608.828 feet  
 East= 1,491,701.376 feet  
 Delta Alpha= -00°17'09.70"  
 Ground to Grid Factor= 0.999670930

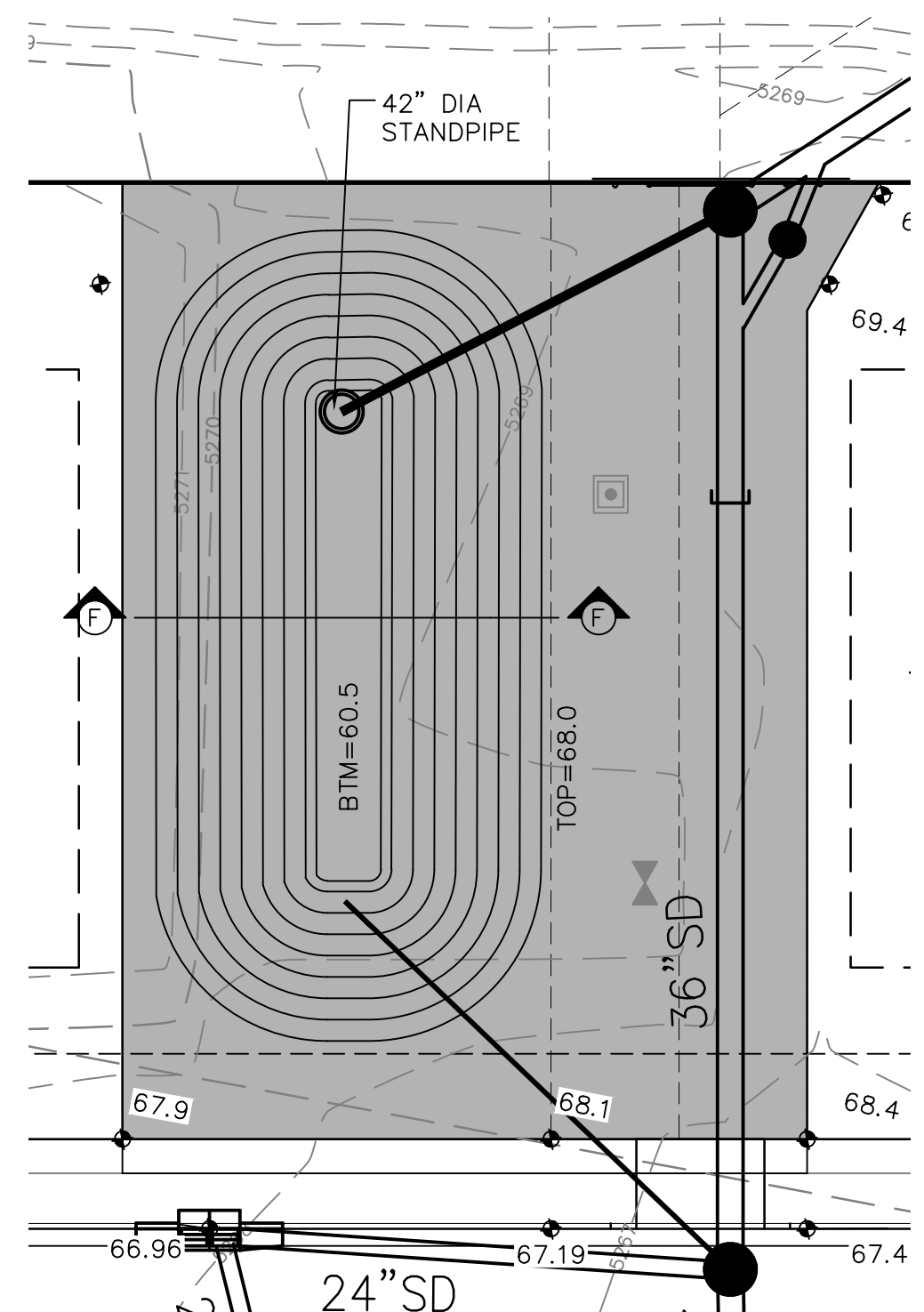
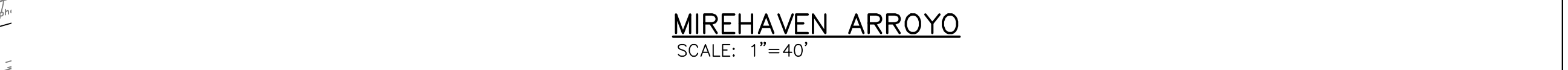


NEW MEXICO  
17631  
Professional Engineer  
8-11-16

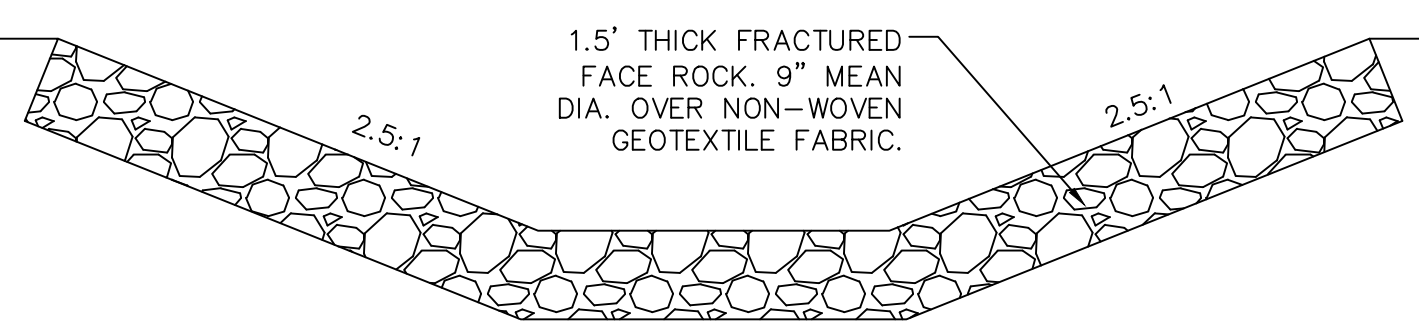
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<b>STORMCLOUD SUBDIVISION UNITS 4A &amp; 4B PULTE HOMES</b>			
<b>GRADING AND DRAINAGE PLAN SE AND SW</b>			
Date:	No. / Revision:	Date:	Job No.
08-2016			2174
Drawn By:			<b>SHEET</b>
ANW/THOR			<b>2</b>
Chd By:			<b>OF 9</b>
ANW			



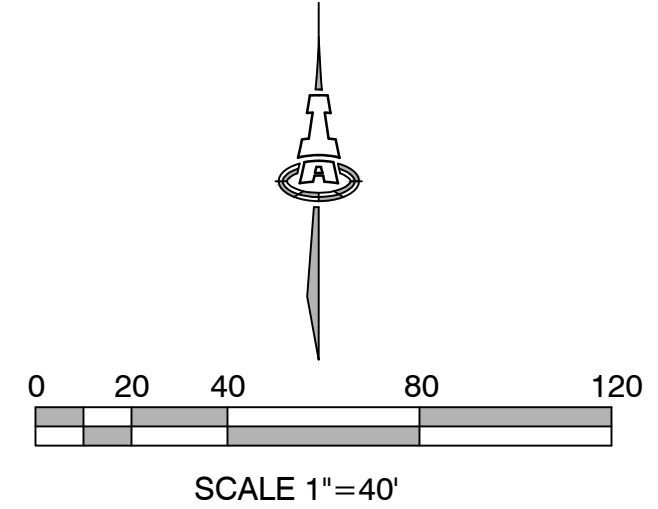
TO BE CONSTRUCTED WITH WORK ORDER PLANS  
**FIRST FLUSH POND**  
 SCALE: 1"=20'



**SECTION F-F**  
 NTS

- NOTES:**
1. STREET GRADES ARE AT FLOWLINE.
  2. SEE SHEETS 5-8 FOR RETAINING WALL GRADES AND SECTIONS.
  3. SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN.

- LEGEND**
- 5270 --- EXISTING CONTOUR
  - 52 --- PROPOSED CONTOUR
  - ◆ 78.3 PROPOSED ELEVATION
  - 78.9 PROPOSED ELEV. AT TIERRA PINTADA R/W
  - PG=5272.1 PAD GRADE ELEVATION
  - ==== STANDARD (STD) CURB & GUTTER
  - ==== MOUNTABLE (MTBL) CURB & GUTTER
  - 36\"/> SD --- STORM DRAIN W/ MANHOLE
  - 24\"/> SD --- STORM DRAIN INLET
  - --- PROPOSED RETAINING WALL (DESIGN BY OTHERS)
  - DW --- RECOMMENDED DRIVEWAY LOCATION



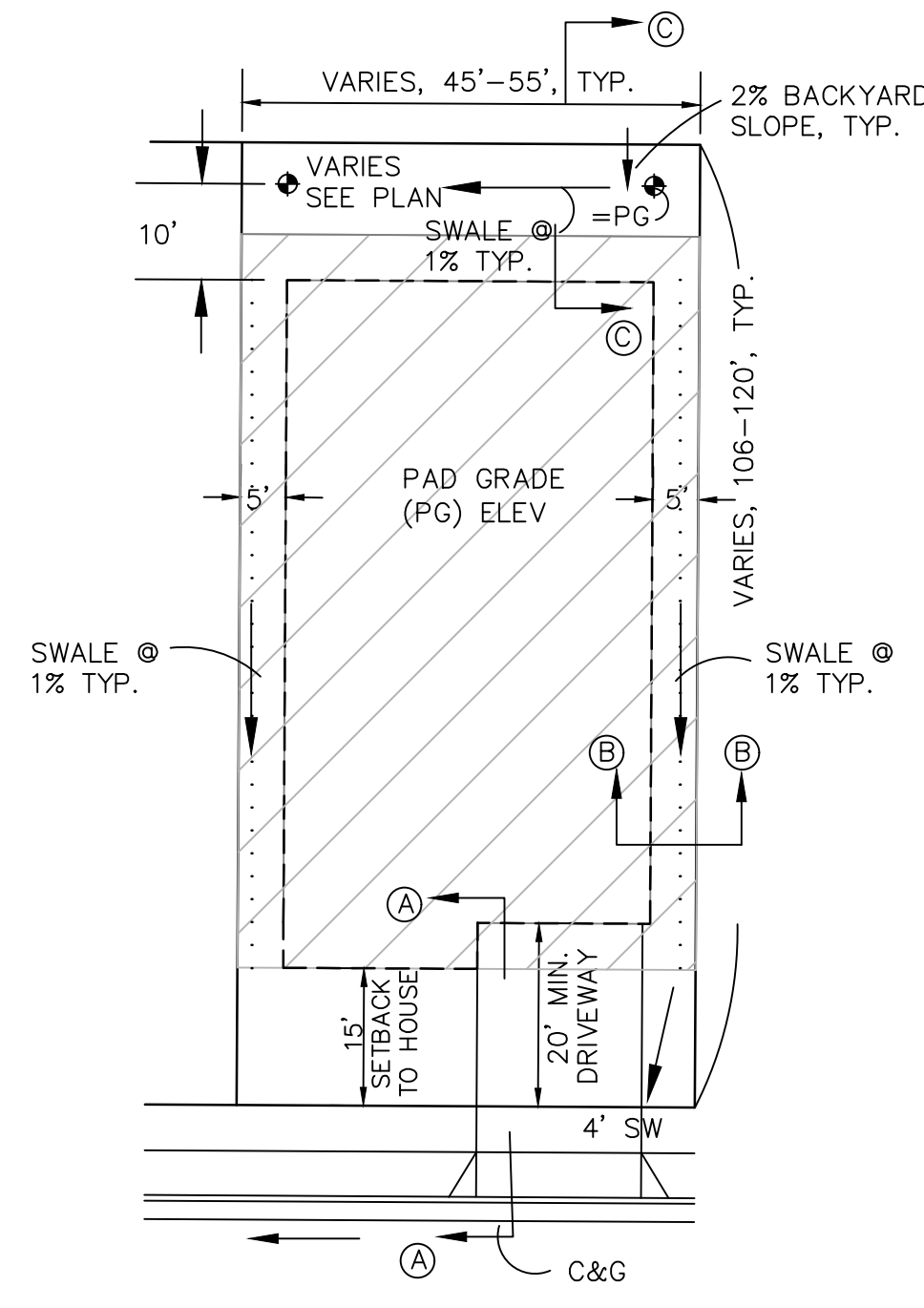
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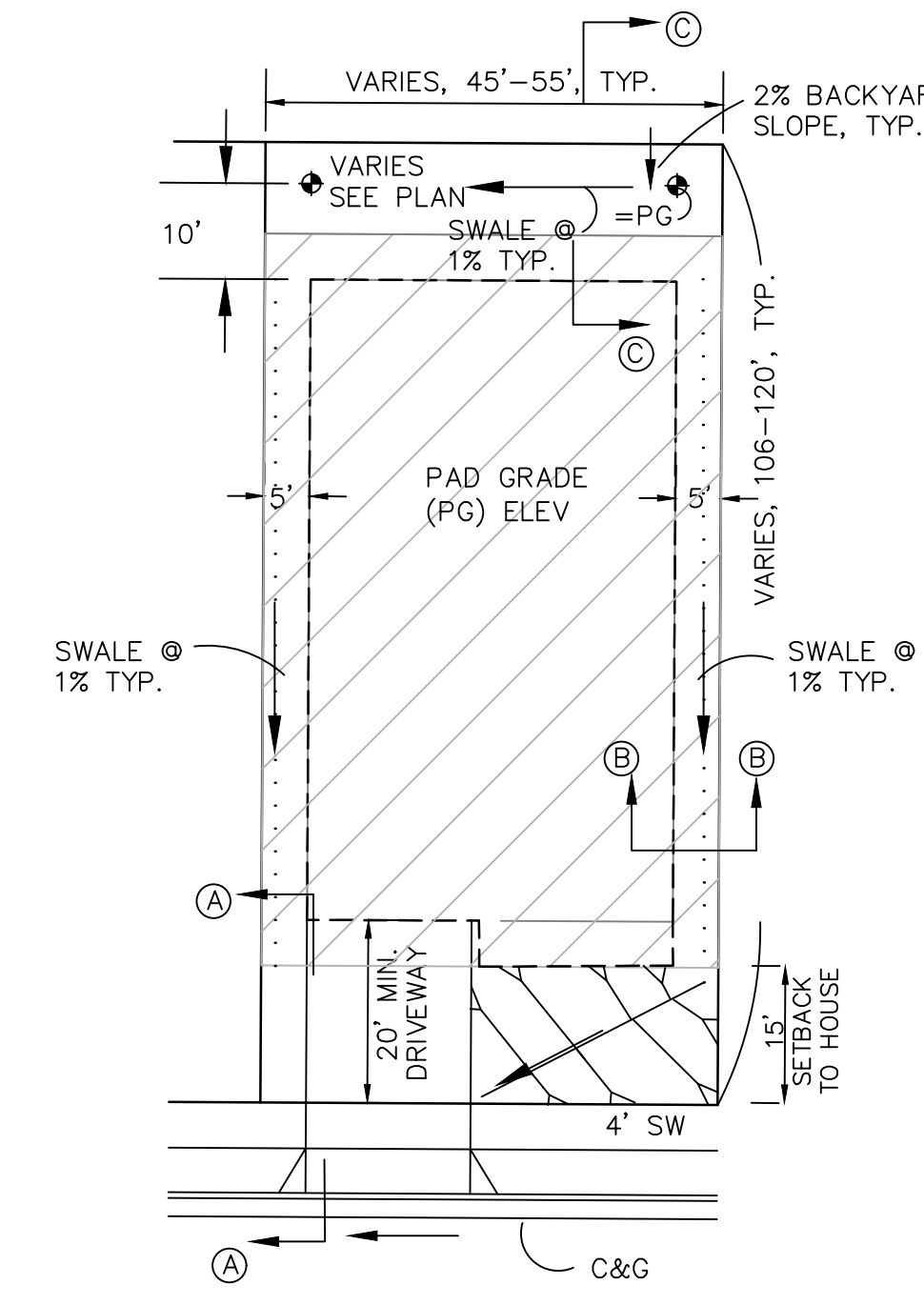
<b>STORMCLOUD SUBDIVISION</b>		<b>UNITS 4A &amp; 4B</b>	
<b>PULTE HOMES</b>			
<b>GRADING AND DRAINAGE PLAN NE</b>			
Date:	No. Revision:	Date:	Job No.
08-2016			2174
Drawn By:			<b>SHEET</b>
ANW/THOR			<b>3</b>
Chd By:			<b>OF 9</b>
ANW			

## GENERAL NOTES

1. ALL WORK DETAILED ON THESE PLANS AND PERFORMED UNDER THIS CONTRACT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT GEOTECHNICAL REPORT. WHERE APPLICABLE AND CITY OF ALBUQUERQUE SPECIFICATIONS APPLY.
2. THE CONTRACTOR SHALL ABIDE BY ALL STATE, LOCAL, AND FEDERAL LAWS, CODES, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS, INCLUDING EPA AND ADA REQUIREMENTS.
3. ALL SUBGRADE, OVEREXCAVATION, BACKFILL, AND FILL SHALL BE PLACED AND / OR COMPACTED PER THE GEOTECHNICAL REPORT AND CITY OF ALBUQUERQUE SPECIFICATIONS.
4. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION, OR PRIOR TO OCCUPANCY, AS APPROPRIATE. IF PERMITS ARE DELAYED OR ISSUED WITH CONDITIONS, THE CONTRACTOR SHALL NOTIFY THE OWNER AND ARCHITECT IMMEDIATELY.
5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING OBSTRUCTIONS, AND CONDITION OF ALL EXISTING INFRASTRUCTURE PRIOR TO CONSTRUCTION. REPORT ALL DISCREPANCIES TO THE ARCHITECT / ENGINEER AND VERIFY THE ARCHITECT / ENGINEER'S INTENT BEFORE PROCEEDING.
6. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE SAFETY.
7. THE CONTRACTOR SHALL MAINTAIN RECORD DRAWINGS ON SITE AT ALL TIMES. THE CONTRACTOR SHALL NOT SCALE DRAWINGS. ONLY WRITTEN DIMENSIONS OR KEYED NOTES SHALL BE USED.
8. CONTRACTOR SHALL OBTAIN ALL REQUIRED INSPECTIONS OF THE WORK. CONTRACTOR SHALL REGULARLY UPDATE OWNER AND ARCHITECT REGARDING THE STATUS OF THE INSPECTIONS.
9. CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT STRUCTURES RESULTING FROM THE CONSTRUCTION PROCESS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL BE RESPONSIBLE FOR DOCUMENTING EXISTING CONDITIONS PRIOR TO CONSTRUCTION.
10. CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS. EQUIPMENT SHALL ONLY OBSTRUCT DESIGNATED TRAFFIC LANES IF APPROPRIATE BARRICADING PERMITS HAVE BEEN OBTAINED. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL IN THE RIGHT-OF-WAY.
11. THE CONTRACTOR SHALL PROVIDE A CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN THAT CONFORMS TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND LOCAL REQUIREMENTS. THE CONTRACTOR SHALL OBTAIN BARRICADING PERMITS FROM THE APPROPRIATE AUTHORITIES PRIOR TO ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.
12. THE CONTRACTOR SHALL MAINTAIN ALL BARRICADING AND CONSTRUCTION SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
13. ALL TRASH, DEBRIS, & SURFACE VEGETATION SHALL BE CLEARED AND LEGALLY DISPOSED OF OFFSITE.
14. PROPOSED SPOT AND CONTOUR ELEVATIONS SHOWN REPRESENT TOP OF FINISH MATERIAL (I.E. TOP OF CONCRETE, TOP OF CONCRETE BUILDING PAD, TOP OF PAVEMENT MATERIAL, TOP OF LANDSCAPING MATERIAL, ETC.). CONTRACTOR SHALL GRADE, COMPACT SUBGRADE AND DETERMINE EARTHWORK ESTIMATES BASED ON ELEVATIONS SHOWN MINUS FINISH MATERIAL THICKNESSES.
15. IF FIELD GRADE ADJUSTMENTS ARE REQUIRED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER.
16. EXISTING UTILITY LINES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND MAY BE INCOMPLETE OR OBSOLETE. SUCH LINES MAY OR MAY NOT EXIST WHERE SHOWN OR NOT SHOWN. CONTRACTOR SHALL CONTACT NM-811 FOR UTILITY LINE SPOTS TWO WORKING DAYS PRIOR TO CONDUCTING SITE FIELD WORK. CONTRACTOR SHALL FIELD VERIFY AND LOCATE ALL UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF NECESSARY DRY UTILITY ADJUSTMENTS.
17. SOIL TESTING AND INSPECTION SERVICES DURING EARTHWORK OPERATIONS ARE REQUIRED. CONTRACTOR SHALL ALLOW TESTING LABS TO INSPECT AND APPROVE COMPACTED SUBGRADES, BACKFILL, AND FILL LAYERS BEFORE FURTHER CONSTRUCTION WORK IS DONE. SHOULD COMPACTION TESTS INDICATE INADEQUATE DENSITY, CONTRACTOR SHALL PROVIDE ADDITIONAL COMPACTION AND TESTING AT THE CONTRACTOR'S SOLE EXPENSE.
18. CONTRACTOR SHALL PROVIDE ALL OTHER CONSTRUCTION STAKING. CONTRACTOR SHALL LOCATE AND PRESERVE ALL BOUNDARY CORNERS AND REPLACE ANY LOST OR DISTURBED CORNERS AT CONTRACTOR'S SOLE EXPENSE. PROPERTY CORNERS SHALL ONLY BE RESET BY A REGISTERED LAND SURVEYOR.
19. THE ENVIRONMENTAL PROTECTION AGENCY (EPA) AND THE CITY OF ALBUQUERQUE REQUIRE A STORM WATER POLLUTION PREVENTION PLAN (SWPPP), AN NPDES PERMIT, AND AN EROSION AND SEDIMENT CONTROL (ESC) PERMIT FOR PROJECTS WHERE CONSTRUCTION ACTIVITIES MEET THE EPA THRESHOLD. (SWPPP, NPDES PERMIT, AND ESC PLAN BY OTHERS.) A CITY-APPROVED ESC PERMIT MUST BE INCLUDED WITH THE CONTRACTOR'S SUBMITTAL FOR A ROUGH GRADING, GRADING, PAVING, BUILDING, OR WORK ORDER PERMIT.
20. A CURRENT STORMWATER CONTROL PERMIT, INCLUDING AN EROSION SEDIMENT CONTROL PLAN (E.S.C.) FOR EROSION AND SEDIMENT CONTROL IS REQUIRED FOR ALL CONSTRUCTION, DEMOLITION CLEARING, AND GRADING OPERATIONS THAT DISTURB THE SOIL ON ONE ACRE OR MORE OF LAND. OWNER WILL COORDINATE.
21. POST-CONSTRUCTION MAINTENANCE FOR PRIVATE STORMWATER FACILITIES WILL BE THE RESPONSIBILITY OF THE FACILITIES OWNER. PERIODIC INSPECTION AND CERTIFICATIONS OF THE FACILITIES MAY BE REQUIRED BY THE CITY ENGINEER.
22. STORMWATER CONTROL MEASURES SHOWN ON THIS PLAN ARE REQUIRED TO PROVIDE MANAGEMENT OF "FIRST FLUSH" (DEFINED AS THE 90TH PERCENTILE STORM EVENT. PER THE CITY DRAINAGE ORDINANCE, THE 90TH PERCENTILE STORM EVENT, WHICH IS 44 INCHES, IS TO BE MANAGED. REDUCE 0.44 INCH BY THE 0.1 INCH FOR THE INITIAL IMPERVIOUS ABSTRACTION IN TABLE A-6 OF SECTION 22 OF THE DPM. MULTIPLY THE REMAINING 0.34 INCH BY YOUR IMPERVIOUS AREA. THIS IS THE PORTION TO RETAIN).
23. ADJUST ANY RIMS OF EXISTING UTILITY FEATURES AS NECESSARY TO MATCH NEW GRADES.
24. BUILDING PAD ELEVATION SHALL BE ±0.05' FROM PLAN ELEVATION.
25. WHERE GRADES BETWEEN NEW AND EXISTING ARE SHOWN AS 'MATCH' OR '±', TRANSITIONS SHALL BE SMOOTH.
26. SIDESLOPES STEEPER THAN 3:1 BUT LESS THAN 2:1 MUST HAVE PERMANENT EROSION CONTROL (F.F. ROCK) INSTALLED, TYPICAL. NO SLOPE SHALL BE STEEPER THAN 2:1.
27. CONTRACTOR SHALL COMPLY WITH LOCAL REGULATIONS FOR RESEEDING OF DISTURBED AREAS.
28. POND DESIGN PARAMETERS (TOP OF POND, BOTTOM OF POND, SIZE OF ORIFICE, AREA OF POND, ETC.) TO BE STRICTLY ADHERED TO FOR CERTIFICATION PURPOSES. SEE DETAIL SHEET FOR ADDITIONAL INFORMATION.
29. ENGINEER RECOMMENDS THAT OWNER MAINTAIN EROSION PROTECTION ELEMENTS. ENGINEER RECOMMENDS THAT OWNER INSPECT SITE YEARLY AND AFTER EACH RAINFALL TO IDENTIFY NEW AREAS OF EROSION AND INSTALL ADDITIONAL EROSION PROTECTION AS NEEDED BASED ON ACTUAL OCCURRENCES.
30. GRADING SHALL BE PERFORMED AT THE ELEVATIONS AND IN ACCORDANCE WITH THE DETAILS SHOWN ON THIS PLAN.
31. MEASURES REQUIRED FOR EROSION AND SEDIMENT CONTROL SHALL BE INCIDENTAL TO THE PROJECT COST.
32. ALL SITE PREPARATION, GRADING OPERATIONS, FOUNDATION CONSTRUCTION, AND PAVEMENT INSTALLATION WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT, WHICH WILL BE PROVIDED BY THE OWNER OR ARCHITECT. ALL OTHER WORK SHALL, UNLESS OTHERWISE NOTED IN THE PLANS, BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT SPECIFICATION (FIRST PRIORITY), AND/OR NMDOT STANDARD SPECIFICATIONS FOR PUBLIC WORK (SECOND PRIORITY.)
33. FIVE WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (811) FOR LOCATION OF EXISTING UTILITIES.
34. ALL SUBGRADE, OVEREXCAVATION, AND FILL SHALL BE PLACED AND / OR COMPACTED PER THE GEOTECHNICAL REPORT.



**TYPICAL LOT GRADING DETAIL**  
 SCALE: 1"=20'  
 NOTE: 10' SIDERYARD SETBACK ADJACENT TO STREETS.

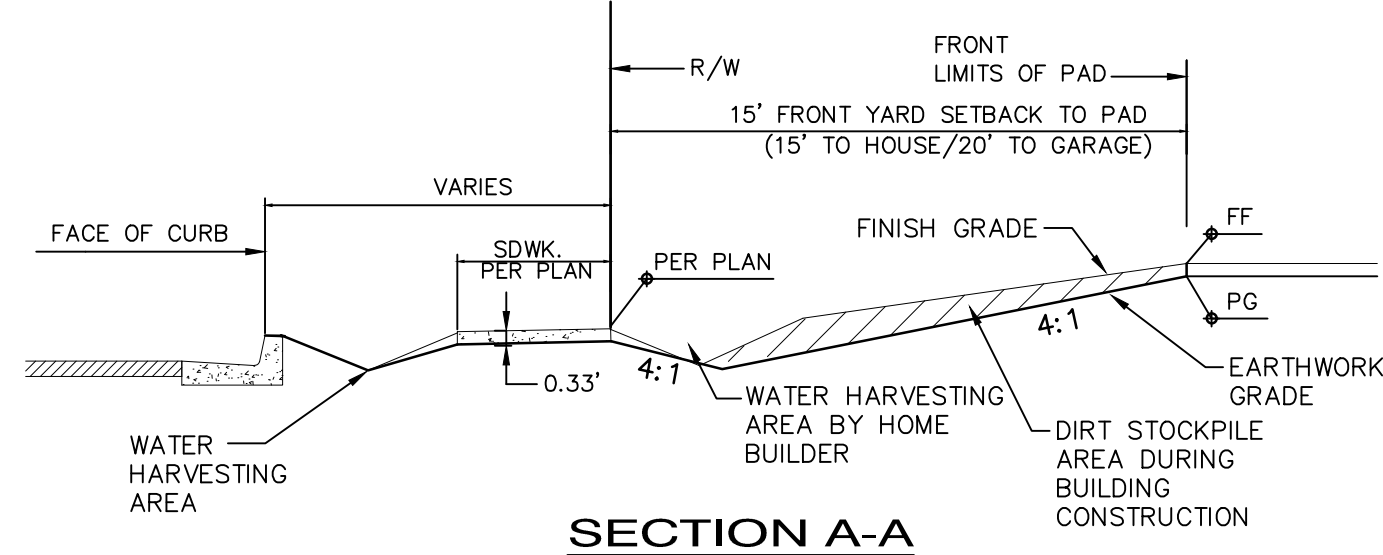


**TYPICAL LOT GRADING DETAIL**  
 DRIVEWAY ON LOW SIDE  
 SCALE: 1"=20'  
 NOTE: 10' SIDERYARD SETBACK ADJACENT TO STREETS.

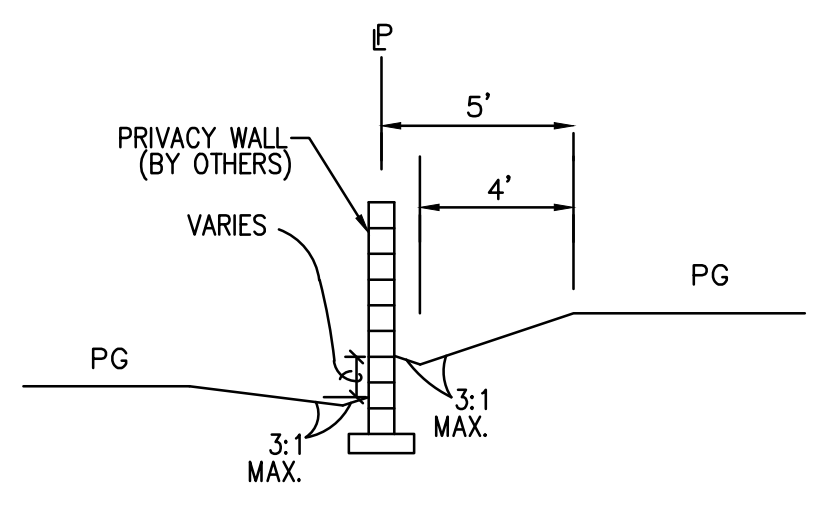
**DRIVEWAY NOTE:**

RECOMMENDED DRIVEWAY LOCATIONS ARE SHOWN ON PLANS WHERE STEEP STREET SLOPES OCCUR TO MINIMIZE DRIVEWAY SLOPES AND TO MINIMIZE RETAINING WALL HEIGHTS. WHERE DRIVEWAYS ARE SHOWN ON LOW SIDE, DRIVEWAYS SHALL BE CONSTRUCTED IN THIS LOCATION TO ENSURE A MIN. 1% SIDERYARD SWALE SLOPE.

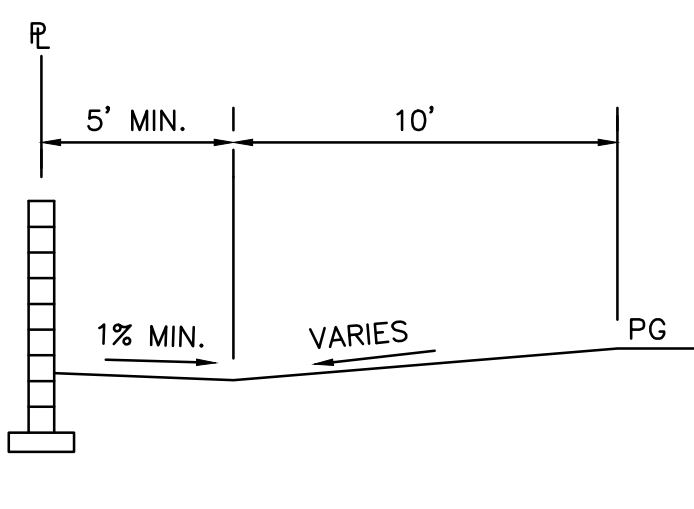
DRIVEWAYS SHALL NOT INTERFERE WITH IMPROVEMENTS SHOWN ON PUBLIC PLANS, SUCH AS STORM DRAIN INLETS AND ADA RAMPS.



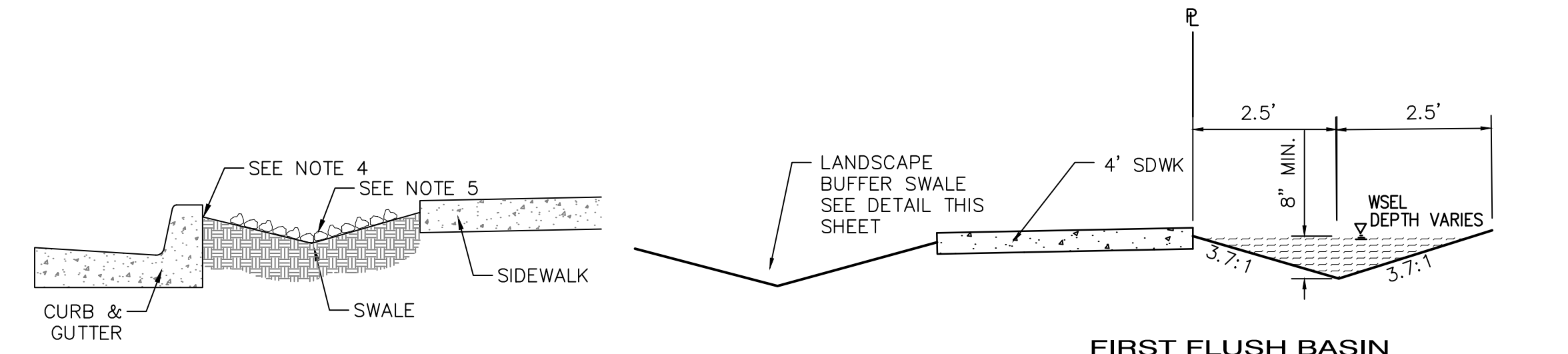
**SECTION A-A**  
**FRONT YARD GRADING**  
 SCALE: 1"=5'-0"



**SECTION B-B**  
**TYPICAL SIDERYARD GRADING**  
 SCALE: 1"=5'



**SECTION C-C**  
**TYPICAL BACKYARD GRADING**  
 SCALE: 1"=5'

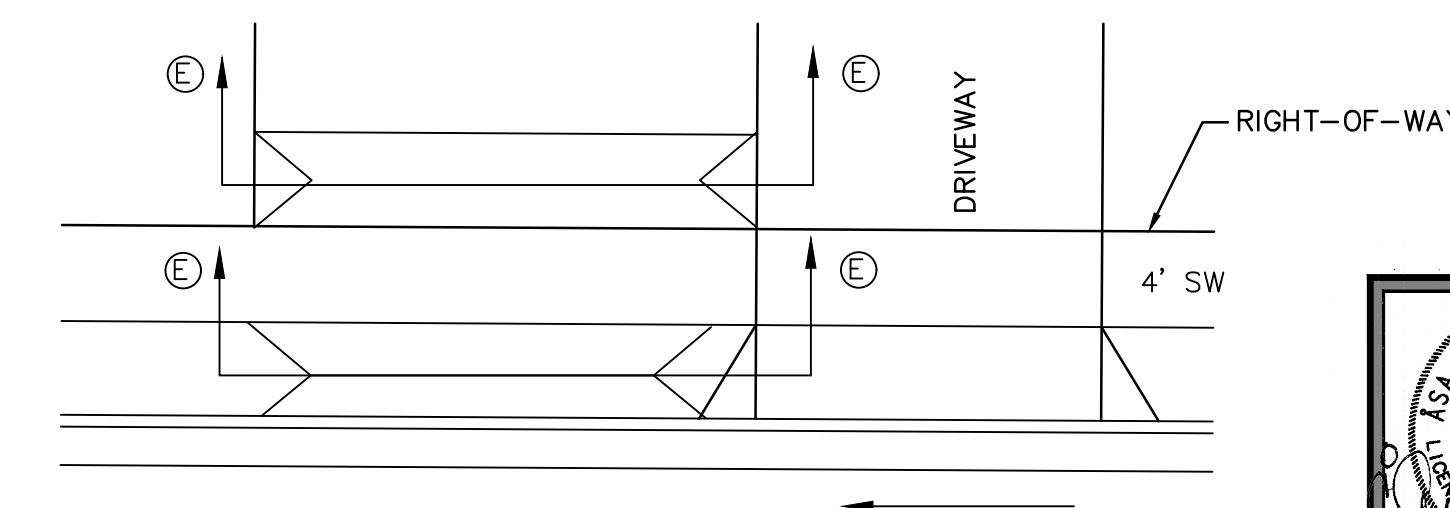


**FIRST FLUSH BASIN**  
**SECTION D-D**  
 1"=2'

1. SWALE TO BE 6" DEEP WHEN THE DISTANCE BETWEEN BACK OF CURB AND THE SIDE WALK IS 5 FEET.
2. SWALE TO BE 1" DEEPER THAN THE DISTANCE IN FEET BETWEEN THE BACK OF CURB AND THE SIDEWALK FOR LANDSCAPE BUFFERS DIFFERENT THAN 5 FEET WIDE.
3. FOR WIDE LANDSCAPE BUFFERS, GREATER THAN 10 FEET, THE MAXIMUM DEPTH IS 10".
4. FINAL GRADE OF DIRT TO BE 1" TO 2" BELOW TOP OF CURB AND TOP OF SIDEWALK GRADE.
5. SURFACE BETWEEN BACK OF CURB AND SIDEWALK TO BE COVERED WITH GRAVEL MULCH (MINIMUM 3/4"), COBBLES OR RIP-RAP. DO NOT FILL ENTIRE SWALE.
6. A CHECK DAM WILL BE REQUIRED FOR SWALES ON STEEPER LONGITUDINAL SLOPES AND LONGER SECTIONS. THE ENGINEER WILL DETERMINE LOCATION.
7. LANDSCAPE FABRIC IS RECOMMENDED, BUT NOT REQUIRED, BETWEEN THE DIRT AND THE STONE. IF LANDSCAPE FABRIC IS TO BE USED, IT IS TO BE PERMEABLE.
8. DETAIL IS TO BE BUILT FOR ALL NEW CONSTRUCTION. IN THE CASE WHERE SIDEWALK IS EXISTING AND THE LANDSCAPE BUFFER IS IMPROVED WITH LANDSCAPING AND/OR SOME FORM OF EROSION PROTECTION, THIS REQUIREMENT DOES NOT APPLY.

**LANDSCAPE BUFFER SWALE**

REVISION TO CITY STANDARD DRAWINGS  
 2405A & 2405B



**PLAN**  
 1"=10'  
**FIRST FLUSH BASIN DETAILS**

FIRST FLUSH BASIN AREAS SHALL BE GRADED TO CAPTURE "FIRST FLUSH" WATER. GRADING SHALL TRANSITION TO ENSURE WATER METERS, LIGHT POLE BASES AND INLET BOXES ARE NOT LOCATED IN THE WATER HARVESTING AREAS. FIRST FLUSH BASINS ADJACENT TO LOTS WITH DEFERRED SIDEWALKS SHALL BE CONSTRUCTED BY HOME BUILDER AND ARE NOT PART OF GRADING CERTIFICATION. THE FRONT YARD GRADING DETAIL ON THIS SHEET SHOWS INTERIM GRADING THAT WILL CREATE TEMPORARY FIRST FLUSH BASINS UNTIL HOMES ARE CONSTRUCTED.

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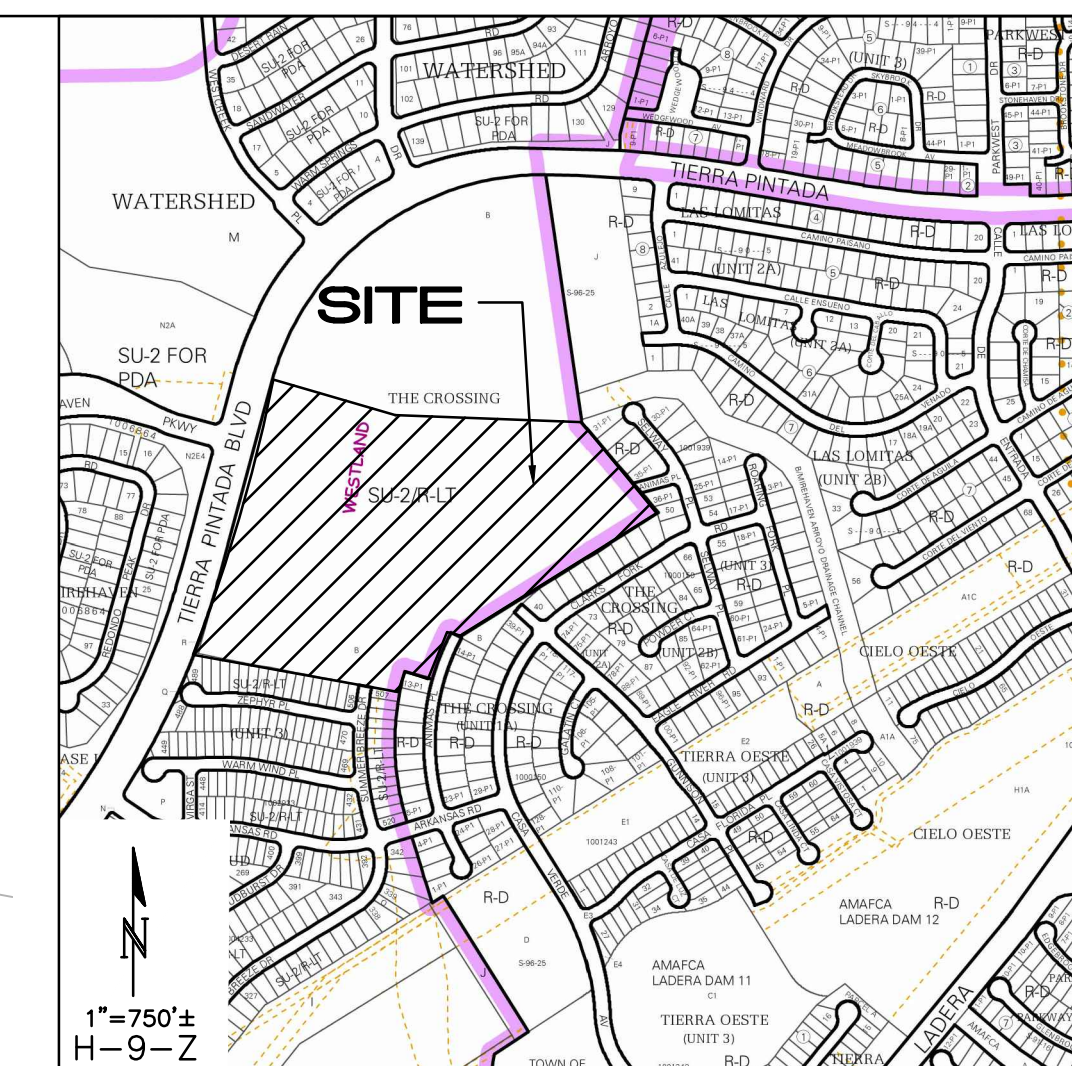
1821 CG-101.dwg Aug 11, 2016

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**STORMCLOUD SUBDIVISION**  
**UNITS 4A & 4B**  
**PULTE HOMES**

**GRADING DETAILS**

Date:	No.:	Revision:	Date:	Job No.
08-2016				2174
Drawn By:				<b>SHEET</b>
ANW/THOR				<b>4</b>
Ckd By:				<b>OF 9</b>
ANW				

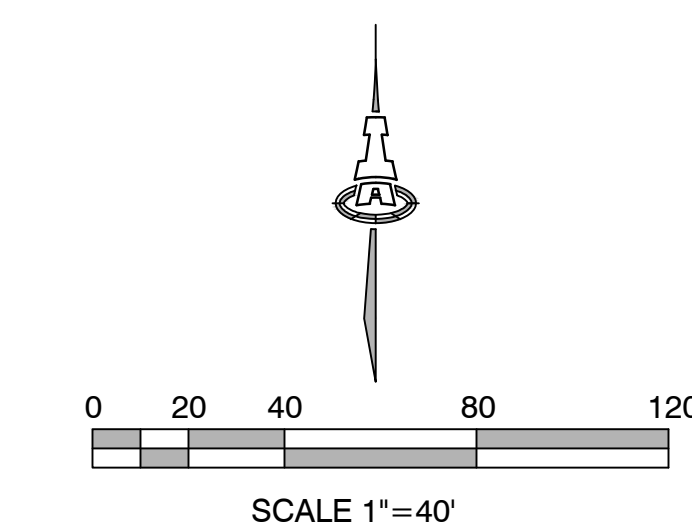


**GENERAL WALL NOTES:**

1. RETAINING WALL DESIGN BY OTHERS.
2. ALL EXISTING RETAINING WALLS AND PERIMETER WALLS SHALL BE REMOVED, UNLESS OTHERWISE NOTED.
3. SEE SHEET 8 FOR RETAINING WALL SECTIONS.
4. SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN.

**LEGEND**

- EXISTING RETAINING WALL TO REMAIN
- - - PROPOSED RETAINING WALL
- - - PROPOSED GARDEN RETAINING WALL (MAX. 1.5' RETAINING)
- TW= PROPOSED TOP/BOTTOM OF RETAINING WALL
- BW= PROPOSED TOP/BOTTOM OF GARDEN RETAINING WALL
- Δ=2.7' PROPOSED RETAINING WALL HEIGHT



SEE SHEET 7

SEE SHEET 6

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 Ph. 505-268-8828 www.isaifirm.com

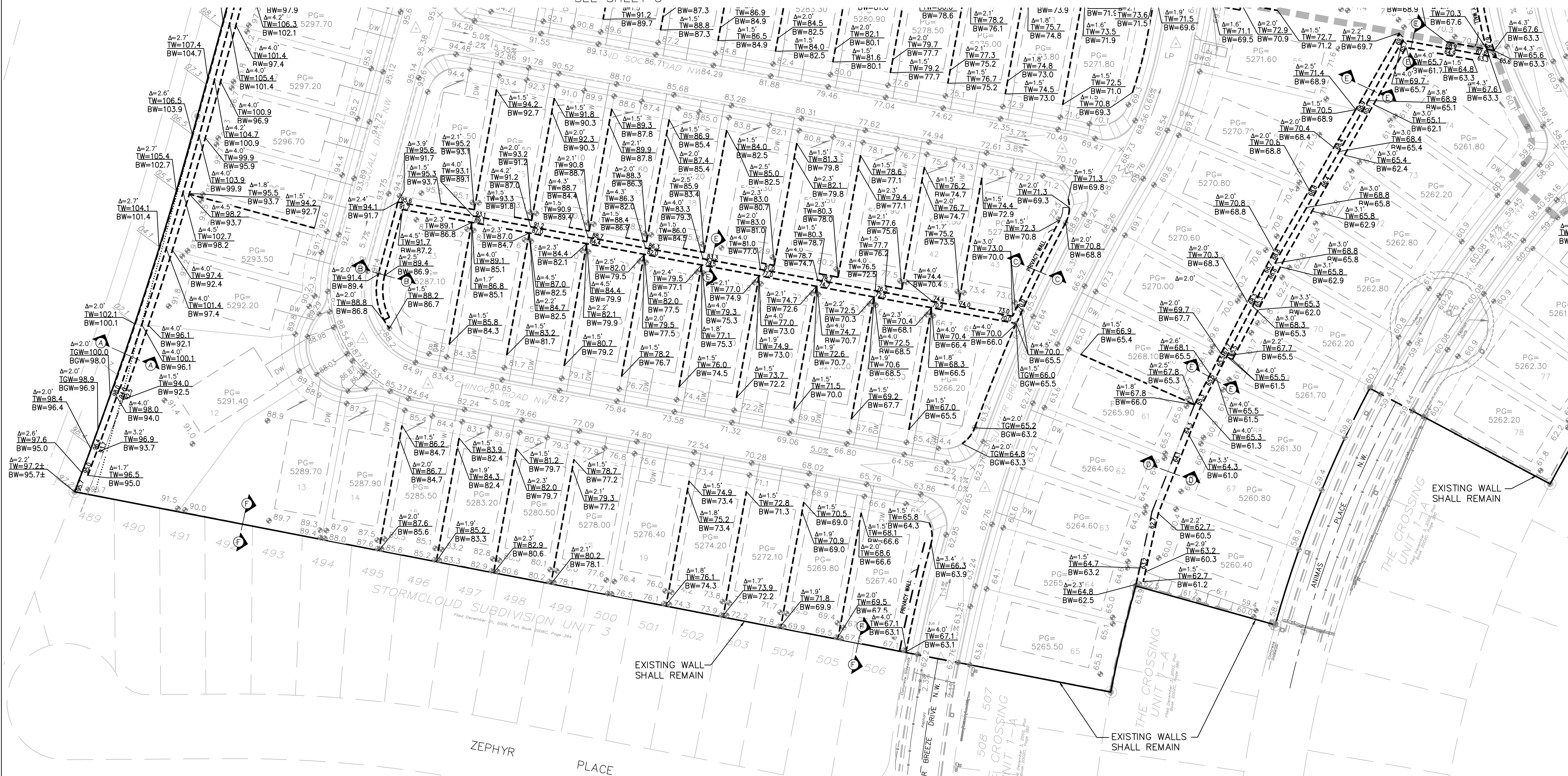
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<b>STORMCLOUD SUBDIVISION UNITS 4A &amp; 4B</b>	
PULTE HOMES	
<b>RETAINING WALL PLAN NW</b>	
Date: 08-2016	Job No. 2174
Drawn By: ANW/THOR	<b>SHEET 5</b>
Ckd By: ANW	<b>OF 9</b>

SEE SHEET 5

SEE SHEET 7

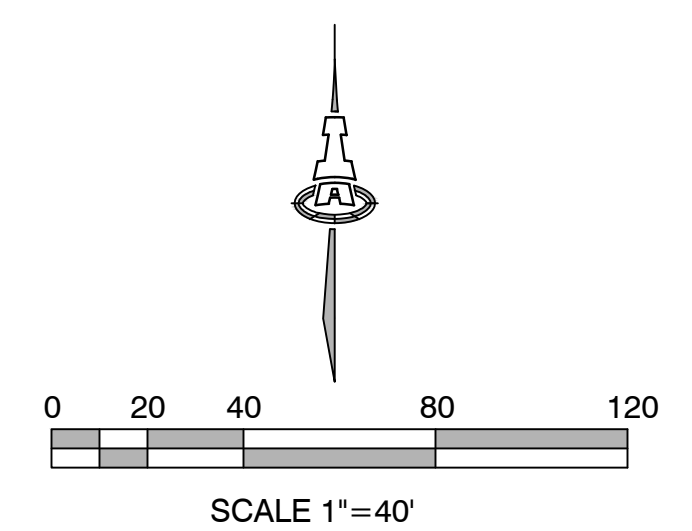


GENERAL WALL NOTES:

1. RETAINING WALL DESIGN BY OTHERS.
2. ALL EXISTING RETAINING WALLS AND PERIMETER WALLS SHALL BE REMOVED, UNLESS OTHERWISE NOTED.
3. SEE SHEET 8 FOR RETAINING WALL SECTIONS.
4. SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN.

LEGEND

- EXISTING RETAINING WALL TO REMAIN
- - - - PROPOSED RETAINING WALL
- - - - PROPOSED GARDEN RETAINING WALL (MAX. 1.5' RETAINING)
- TW= PROPOSED TOP/BOTTOM OF RETAINING WALL
- IGW= PROPOSED TOP/BOTTOM OF GARDEN RETAINING WALL
- Δ=2.7' PROPOSED RETAINING WALL HEIGHT



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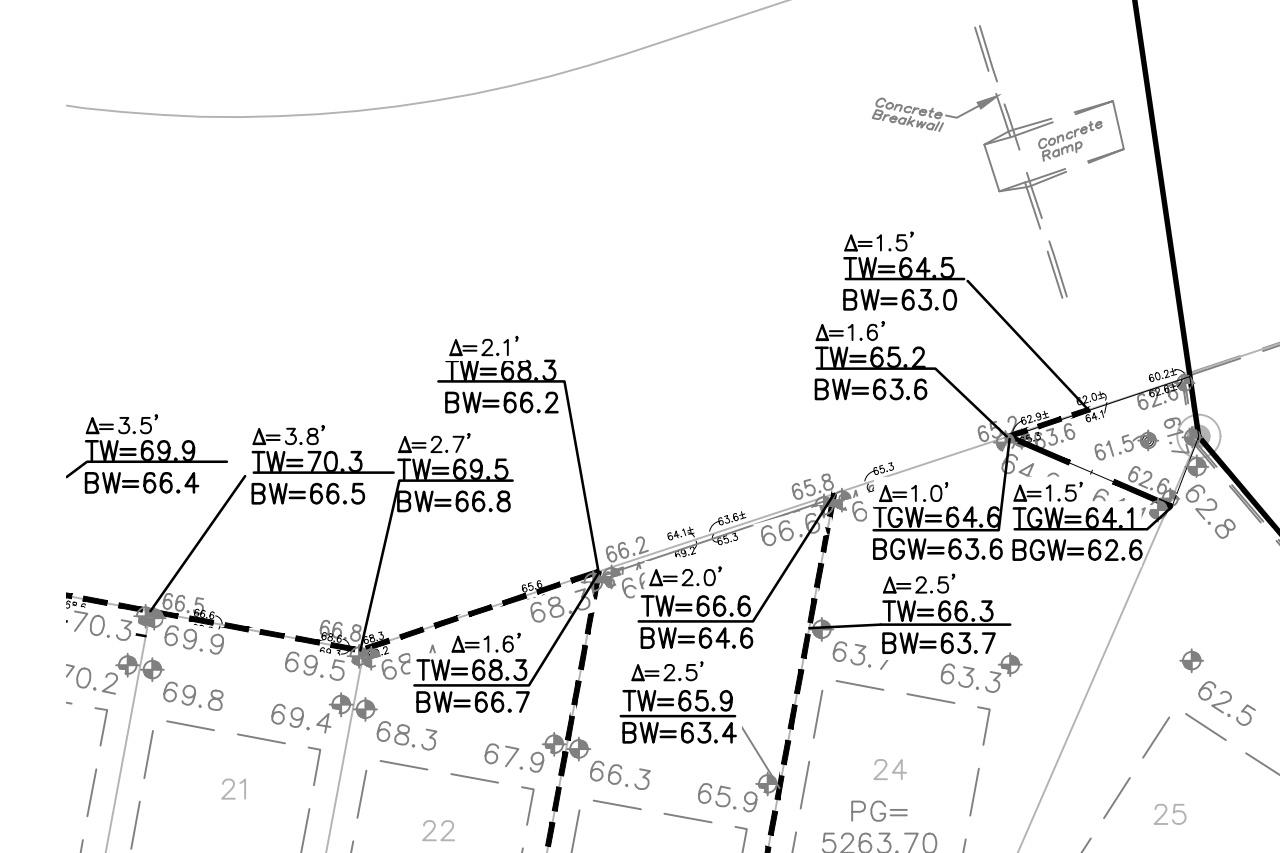
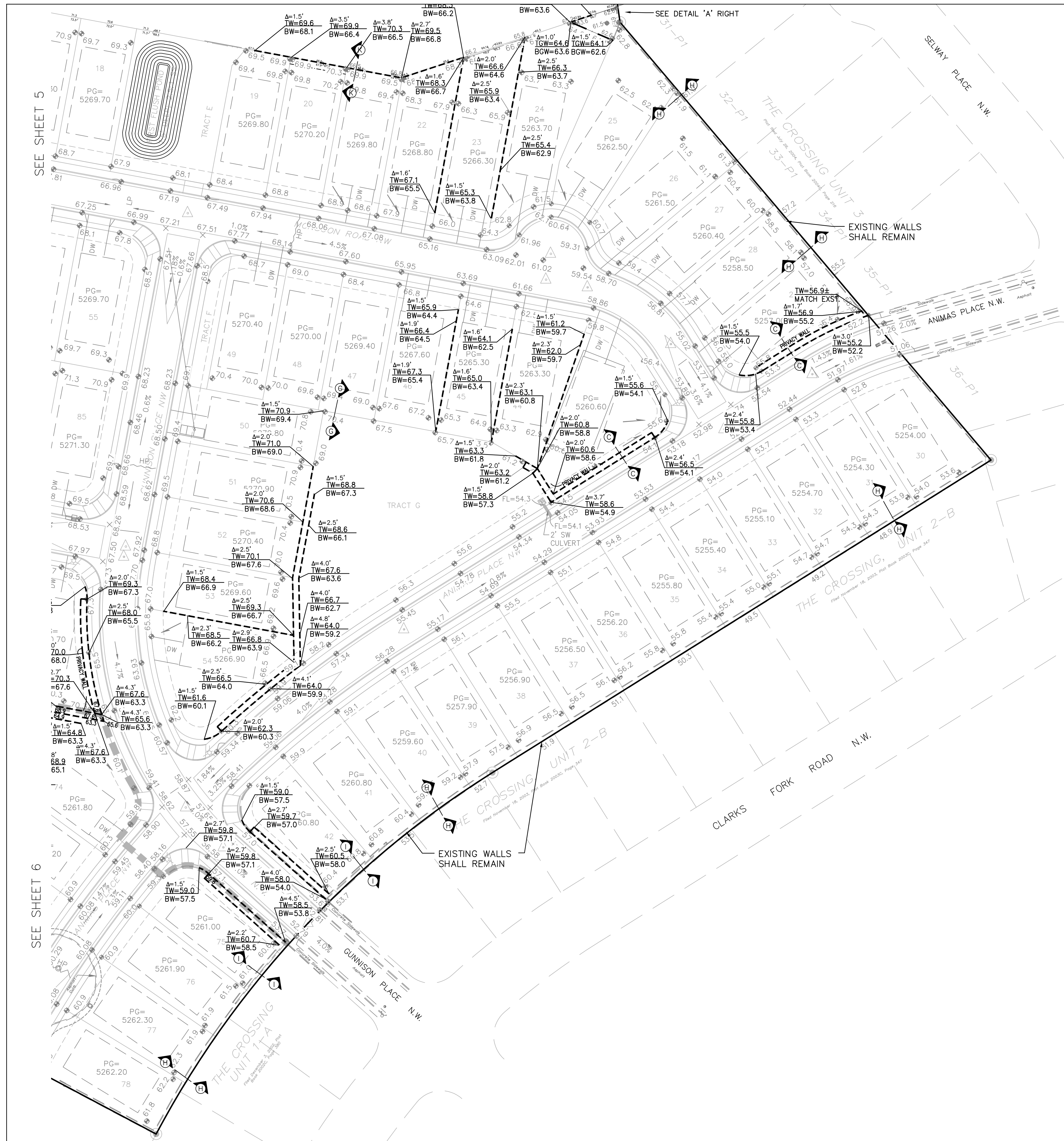
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**STORMCLOUD SUBDIVISION  
UNITS 4A & 4B  
PULTE HOMES**

**RETAINING WALL PLAN SE AND SW**

Date:	Revision:	Date:	Job No.
08-2016			2174
Drawn By:			<b>SHEET</b>
ANW/THOR			<b>6</b>
Ckd By:			<b>OF 9</b>
ANW			



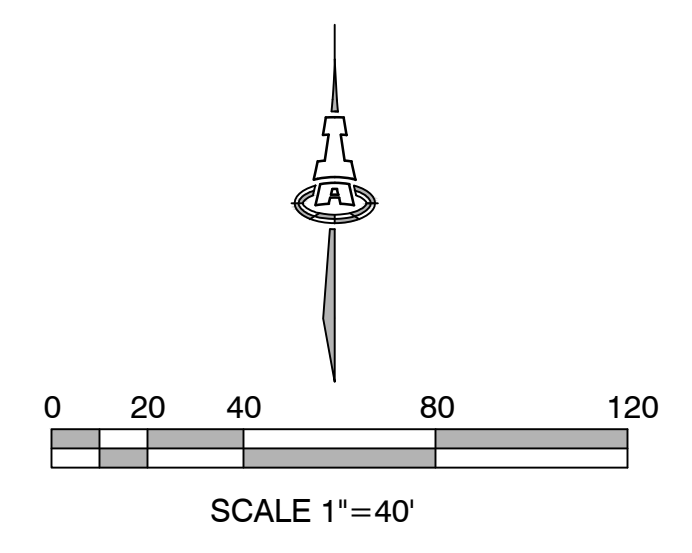
**DETAIL 'A'**  
SCALE: 1"=40'

**GENERAL WALL NOTES:**

1. RETAINING WALL DESIGN BY OTHERS.
2. ALL EXISTING RETAINING WALLS AND PERIMETER WALLS SHALL BE REMOVED, UNLESS OTHERWISE NOTED.
3. SEE SHEET 8 FOR RETAINING WALL SECTIONS.
4. SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN.

**LEGEND**

- EXISTING RETAINING WALL TO REMAIN
- - - PROPOSED RETAINING WALL
- · - · - PROPOSED GARDEN RETAINING WALL (MAX. 1.5' RETAINING)
- TW= PROPOSED TOP/BOTTOM OF RETAINING WALL
- TGW= PROPOSED TOP/BOTTOM OF GARDEN RETAINING WALL
- BW= PROPOSED TOP/BOTTOM OF RETAINING WALL
- BGW= PROPOSED TOP/BOTTOM OF GARDEN RETAINING WALL
- Δ=2.7' PROPOSED RETAINING WALL HEIGHT



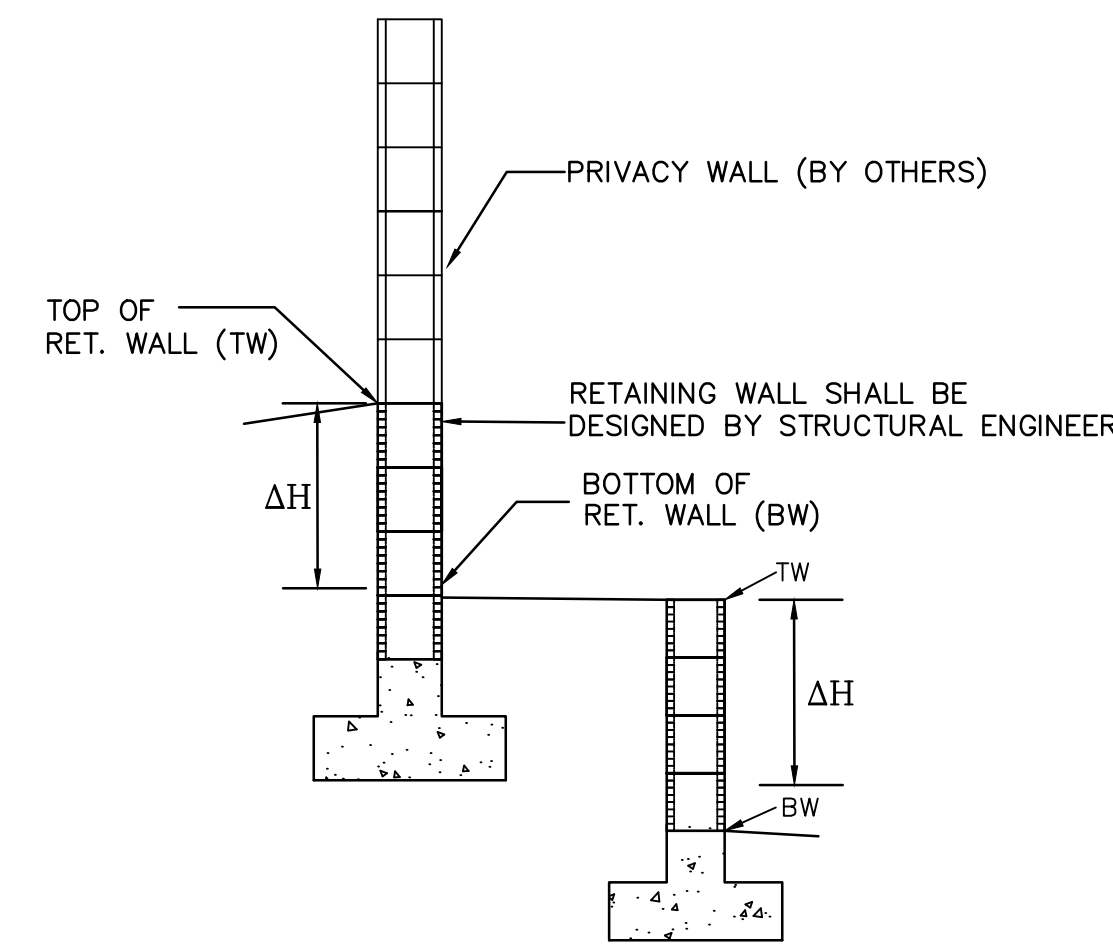
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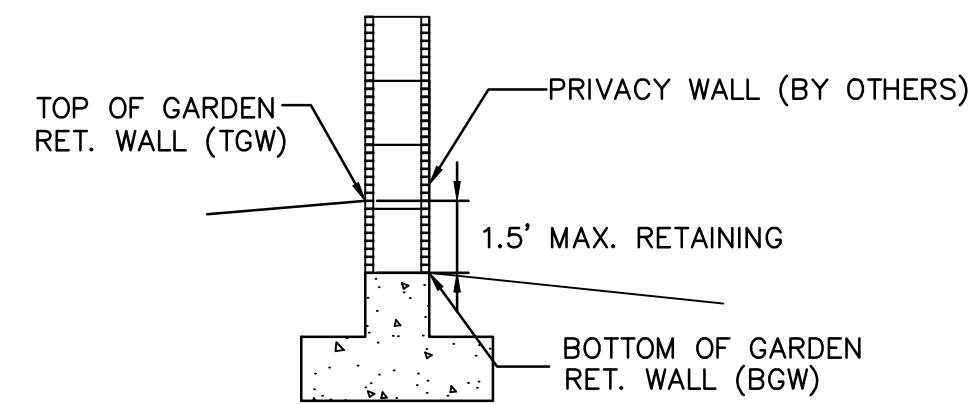
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<b>UNITS 4A &amp; 4B</b>			
<b>PULTE HOMES</b>			
<b>RETAINING WALL PLAN NE</b>			
Date:	No. Revision:	Date:	Job No.
08-2016			2174
Drawn By:			<b>SHEET</b>
ANW/THOR			<b>7</b>
Clk By:			<b>OF 9</b>
ANW			

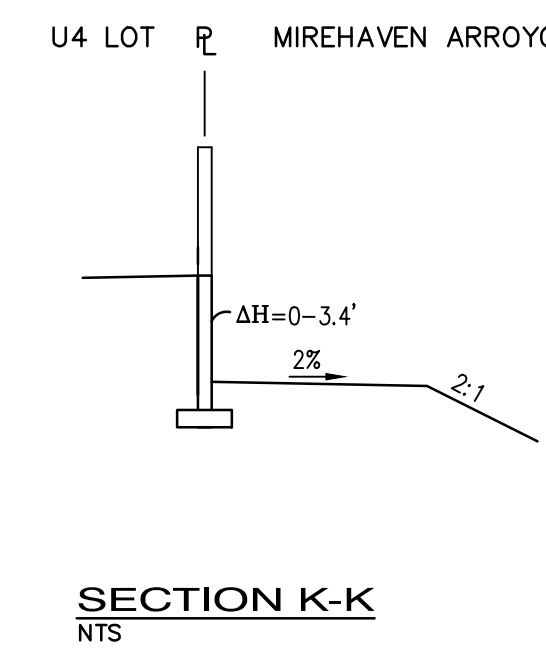
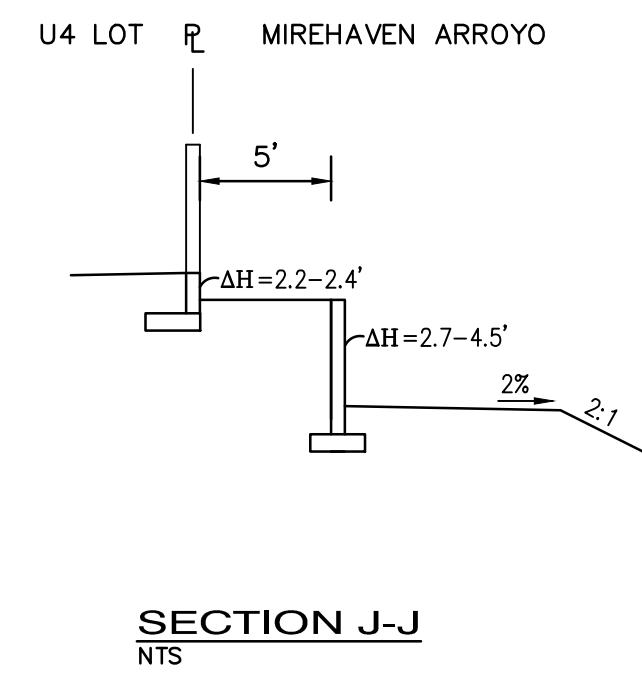
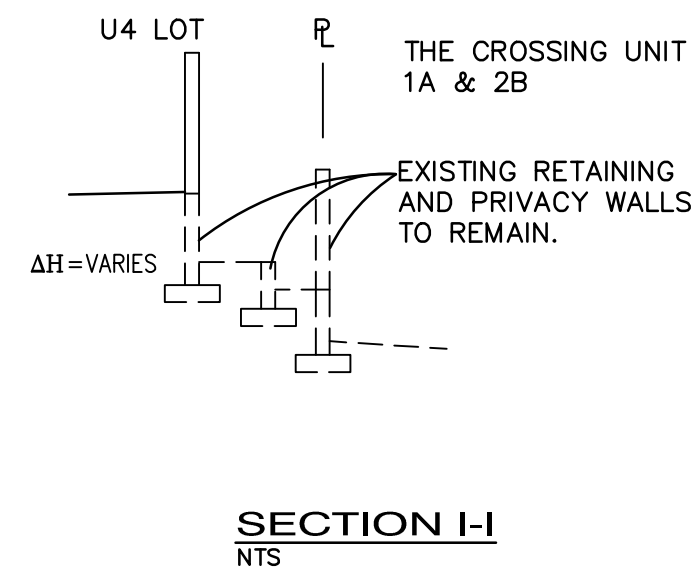
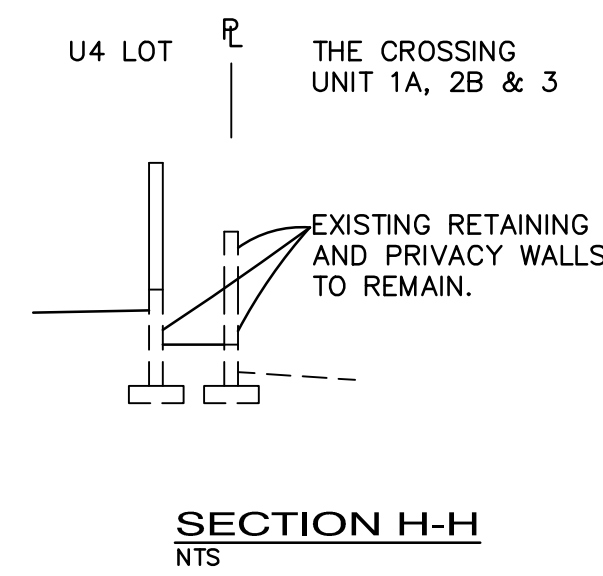
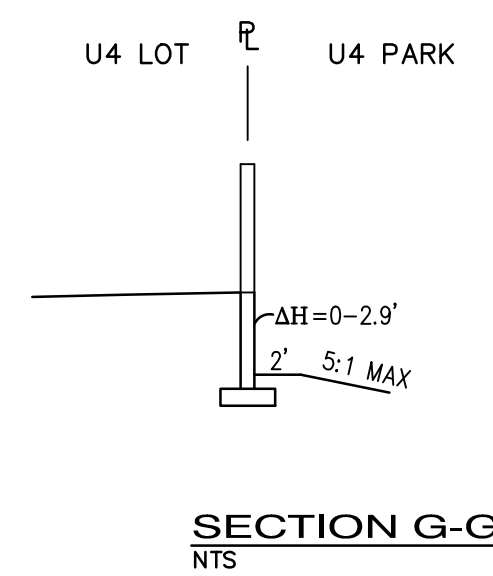
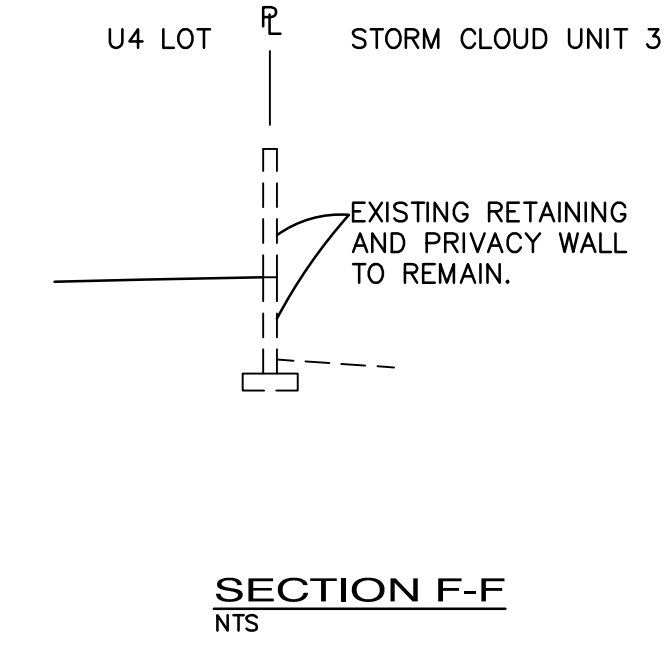
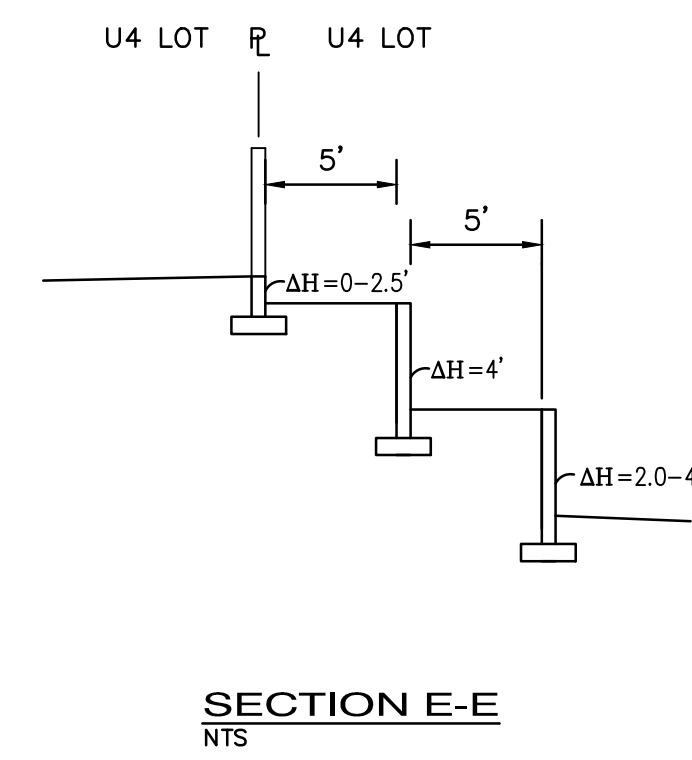
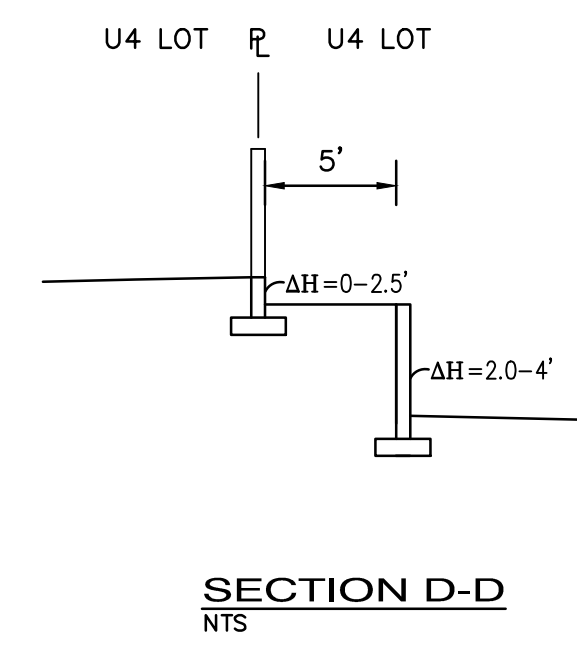
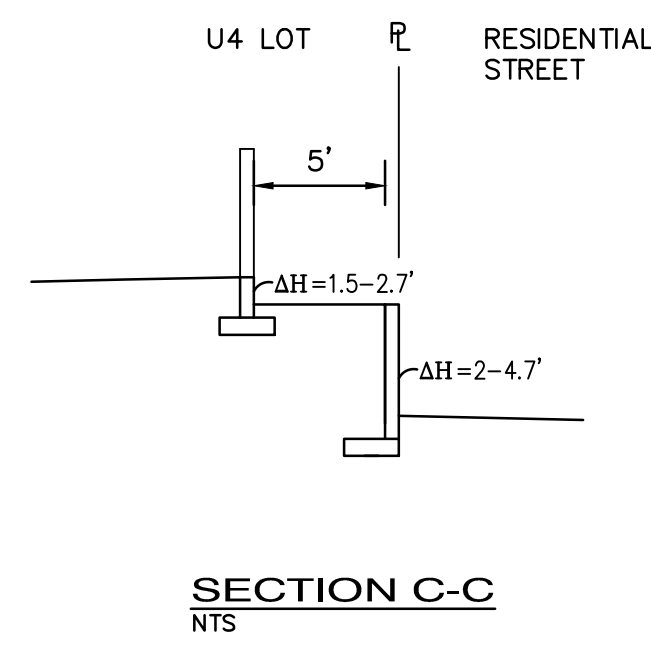
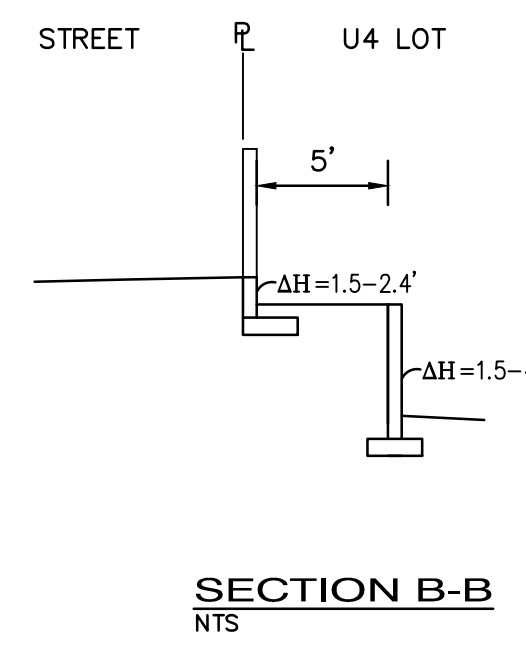
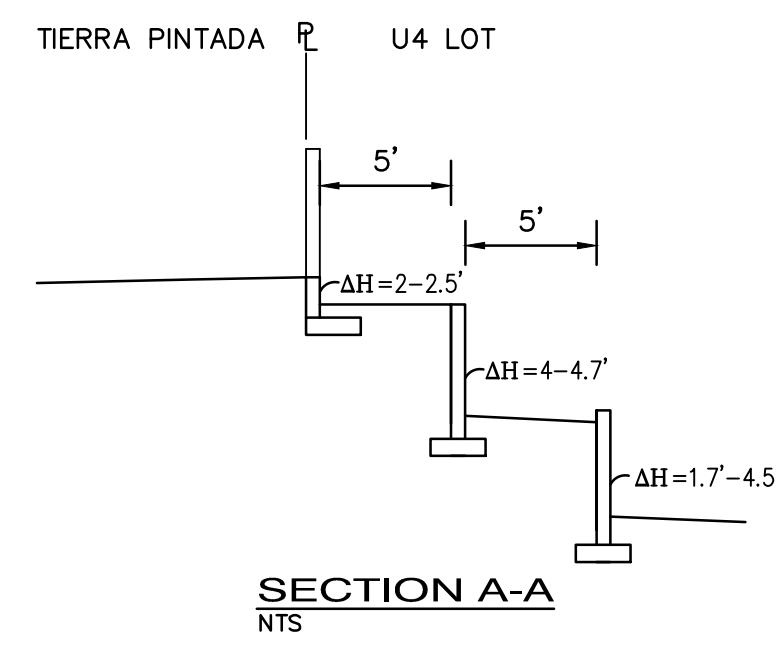


**RETAINING WALL**  
SCALE: 1"=5'-0"

ΔH=RETAINING HEIGHT (SEE SECTIONS THIS SHEET)



**GARDEN WALL RETAINING WALL**  
SCALE: 1"=5'-0"



NOTE:

SEE SHEET 9 FOR EXISTING WALLS TO BE DEMOLISHED AND EXISTING WALLS TO REMAIN.

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Professional Engineer  
8-11-16

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**STORMCLOUD SUBDIVISION  
UNITS 4A & 4B  
PULTE HOMES**

**RETAINING WALL SECTIONS**

Date: 08-2016	No. / Revision:	Date:	Job No. 2174
Drawn By: ANW/THOR			<b>SHEET 8</b>
Chk By: ANW			<b>OF 9</b>



# WALL DEMOLITION NOTES

ALL WALLS SHALL BE DEMOLISHED EXCEPT AS NOTED ON PLAN.

DASHED LINES REPRESENT WALLS TO BE REMOVED. PATCH & REPAIR EXISTING ADJOINING WALLS TO REMAIN.

THE CONTRACTOR SHALL VERIFY ALL CONDITIONS WITHIN THE CONTRACT LIMITS AND NOTIFY THE ENGINEER IN WRITING OF ANY DEVIATION FROM CONTRACT DOCUMENTS NECESSITATED BY FIELD CONDITIONS OR ITEMS NOT COVERED.

ALL DEMOLITION SHALL BE PERFORMED IN A SAFE AND ACCEPTABLE MANNER TO ALL AUTHORITIES HAVING JURISDICTION AND THE OWNER.

THOROUGHLY CLEAN ADJACENT AREAS OF DEBRIS CAUSED BY DEMOLITION WORK.

PRIOR TO ANY DEMOLITION, THE CONTRACTOR SHALL COORDINATE BRACING AND MAINTAIN THE STRUCTURAL INTEGRITY OF THE WALLS TO REMAIN.

THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY BRACING, SHORING, SCAFFOLDING, ETC., WHICH ARE NECESSARY TO PREVENT COLLAPSE, SUBSIDENCE, DEFLECTION OR ANY OTHER TYPE OF DAMAGE.

CONTRACTOR SHALL REVIEW ALL WALLS TO BE DEMOLISHED WITH OWNER TO IDENTIFY ANY ITEMS TO BE SALVAGED AND DETERMINE SALVAGE LOCATION. UNSALVAGEABLE WALL BLOCKS AND FOOTINGS SHALL BE REMOVED FROM SITE AND LEGALLY DISPOSED.



## LEGEND

- EXISTING WALL TO REMAIN
- EXISTING WALL TO BE DEMOLISHED

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## STORMCLOUD SUBDIVISION UNITS 4A & 4B PULTE HOMES

### WALL DEMOLITION PLAN

Date: 08-2016	No. Revision:	Date:	Job No. 2174
Drawn By: ANW/THOR			<b>SHEET 9</b>
Chd By: ANW			<b>OF 9</b>