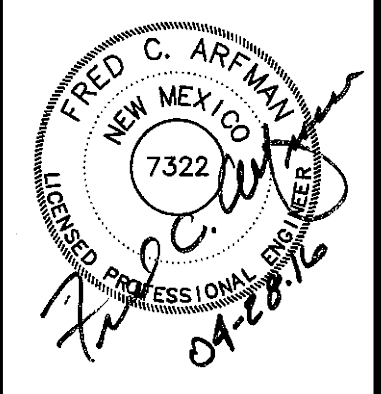
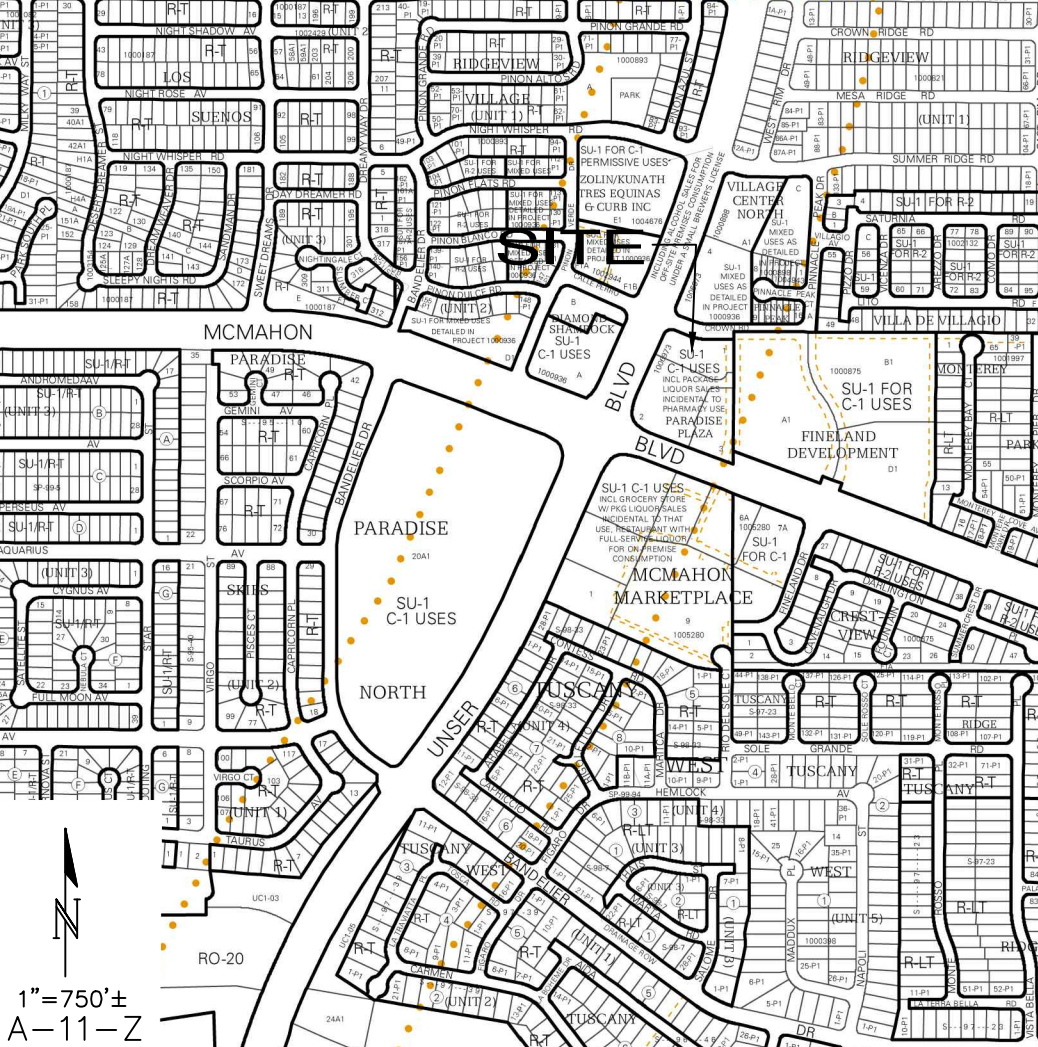


**KEYED NOTES**

1. CONSTRUCT NEW ASPHALT PAVEMENT AND CURB AND GUTTER AT ELEVATIONS SHOWN. SEE PAVING PLAN FOR PAVEMENT MATERIAL, SECTIONS, PARKING LAYOUT, DIMENSIONS, STRIPING, ETC.
2. CONSTRUCT HC PARKING AREA TO ADA STANDARDS. MAX. 2% SLOPE IN ANY DIRECTION.
3. CONSTRUCT 18" WIDE (BOTTOM WIDTH) COVERED SIDEWALK CULVERT PER C.O.A. STD. DWG. 2236 MODIFIED PER DETAIL ON SHEET CG-501 (2 LOCATIONS).
4. CONSTRUCT 24" WIDE CONCRETE ALLEY GUTTER PER C.O.A. STD. DWG. 2415B.
5. NOT USED
6. INSTALL ROCK EROSION PROTECTION AT CURB OPENING, ON ALL SIDESLOPES > 4:1 AND WITHIN FLOWLINES CARRYING CONCENTRATED FLOW (3' WIDE) LIMITS HATCHED PER LEGEND. SEE DETAIL SHEET CG-501.
7. EXISTING DETENTION POND CONSTRUCTED AS PART OF THE DRAINAGE MASTER PLAN.
8. CONSTRUCT NEW CONCRETE DUMPSTER PAD AND ENCLOSURE AT ELEVATIONS SHOWN.
9. 0.5' CONTOURS (DASHED) ARE PROVIDED TO CLARIFY GRADING DRAINAGE INTENT. SEE LEGEND.
- 10.
- 10.1. EXTEND ROOF DOWNSPOUT BELOW GRADE AND INSTALL 6" DIA. DRAINLINE (HDPE OR PVC. SEE GENERAL NOTES ON SHEET CG-501). INVERT AT BUILDING = 93.0. EXTEND DRAINLINE TO DISCHARGE INTO EXISTING POND AT INVERT = 91.5. INSTALL 3' WIDE EROSION PROTECTION FROM END OF DRAINLINE TO POND BOTTOM.
- 10.2. OWNER'S OPTION: EXTEND ROOF DOWNSPOUT BELOW GRADE (SEE 10.1 ABOVE) OR RELEASE TO SURFACE 3' DIA. F.F. ROCK SPLASHPAD. CONSTRUCT 3' WIDE SWALE TO POND BOTTOM. SEE CG-501 FOR DETAIL.
11. CONSTRUCT 2' WIDE (BOTTOM WIDTH) CONCRETE RUNDOWN TO PASS CONCENTRATED FLOW TO POND. INSTALL EROSION PROTECTION FROM END OF RUNDOWN TO POND BOTTOM.
12. SEE ARCHITECTURAL PLANS FOR INFORMATION REGARDING BUILDING EXTENDED AND RETAINING STEMWALLS REQUIRED TO ACHIEVE SITE GRADES.
13. CONSTRUCT ADA COMPLIANT ACCESS RAMP. SEE ARCHITECTURAL FOR DIMENSIONS.

**VICINITY MAP**



**PROJECT DATA**

PROPERTY: THE SITE IS A PARTIALLY DEVELOPED COMMERCIAL PROPERTY WITHIN C.O.A. VICINITY MAP A-11. THE SITE IS BOUND TO THE EAST BY A PERMANENT DETENTION POND SIZED FOR THE FULLY DEVELOPED CONDITION FOR TRACTS 1, 2 AND 3, PARADISE PLAZA, TO THE WEST BY UNSER BLVD. NW, TO THE NORTH BY CROWN ROAD NW AND TO THE SOUTH BY FULLY DEVELOPED COMMERCIAL PROPERTY.

**SITE AREA:** 0.8990 ACRES

**PROPOSED IMPROVEMENTS:** TO INCLUDE A 6,889 SF COMMERCIAL BUILDING WITH ASSOCIATED PAVED PARKING, PEDESTRIAN WALKS, DRAINAGE IMPROVEMENTS, AND LANDSCAPING.

**LEGAL:** TRACT 1, PARADISE PLAZA, CITY OF ALBUQUERQUE, NEW MEXICO.

**BENCHMARK:** ACS MONUMENT "5-A11" ELEV. 5333.08 NAVD 1988

**OFF-SITE:** CROSS-LOT DRAINAGE EASEMENT BETWEEN TRACTS 1, 2 AND 3 IS PROVIDED BY PLAT. DRAINAGE COVENANTS FOR MAINTENANCE OF THE PRIVATE STORM DRAIN SYSTEM AND PONDS HAS BEEN ADDRESSED BY SEPARATE DOCUMENTS. EXISTING DETENTION POND WAS CONSTRUCTED AS PART OF THE TRACT 2 CONSTRUCTION.

**FLOOD HAZARD:** PROPERTY IS LOCATED WITHIN ZONE X, DESIGNATING AREAS DETERMINED TO BE OUTSIDE THE 100-YEAR FLOOD PLAN ACCORDING TO THE FLOOD INSURANCE RATE MAP, BERNALILLO COUNTY, NEW MEXICO AND INCORPORATED AREAS PER MAP NO. 35001C0104H, EFFECTIVE DATE AUGUST 16, 2012.

**DRAINAGE PLAN CONCEPT:** THIS SITE WILL DISCHARGE ALL DEVELOPED FLOW TO THE EXISTING DETENTION POND CONSTRUCTED AS PART OF TRACT 2 DEVELOPMENT. THE DETENTION POND IS SIZED TO RELEASE THE FULLY DEVELOPED PARADISE PLAZA TRACTS (1, 2 AND 3) AT THE APPROVED RATE.

**FIRST FLUSH REQUIREMENT:** STORMWATER CONTROL MEASURES ARE REQUIRED TO PROVIDE MANAGEMENT OF "FIRST FLUSH" DEFINED AS THE 90TH PERCENTILE STORM EVENT OR 0.34" [0.44" LESS 0.1" FOR INITIAL ABSTRACTION] OF STORMWATER WHICH DISCHARGES DIRECTLY TO A PUBLIC STORM DRAINAGE SYSTEM. BASED ON 0.44 ACRE IMPERVIOUS, THE REQUIRED FIRST FLUSH VOLUME FOR TRACT 1 WILL BE 544 CF.

THE EXISTING DETENTION POND BOTTOM WILL PROVIDE FIRST FLUSH RETENTION FOR TRACT 1 WITH ADDITIONAL VOLUME AVAILABLE FOR FUTURE DEVELOPMENT (SEE SUPPLEMENTAL INFORMATION PACKET). ALL STORM WATER FROM THE IMPERVIOUS AREAS OF TRACT 1 SHALL BE DIRECTED TO THIS POND.

**ENGINEER:** FRED C. ARFMAN, NMPE 7322  
 ISAACSON & ARFMAN, P.A.  
 128 MONROE NE, 87111  
 TELEPHONE: (505) 268-8828

**LEGEND**

- EXISTING SPOT ELEVATION
- EXISTING CONTOUR
- 95 PROPOSED CONTOUR (1' INCREMENT)
- 95.5 PROPOSED CONTOUR (0.5' INCREMENT)
- 96.3 PROPOSED SPOT ELEVATION
- FLOW ARROW
- FF = 5296.00 FINISH FLOOR ELEVATION
- PROPOSED GRADE BREAK
- PROPOSED FIRST FLUSH RETENTION PONDING AREA WITH PERCOLATION PIT
- PROPOSED EROSION CONTROL

REV	DATE	DESCRIPTION

DATE: 4-26-16  
 PROJECT # IA2152  
 DRAWN BY: BUB  
 CHECK BY: FCA  
 VERSION 04 - 83x83  
 ALL REPORTS, PLANS, SPECIFICATIONS, FIELD DATA, NOTES AND OTHER DOCUMENTS, INCLUDING ALL DOCUMENTS ON ELECTRONIC MEDIA, PREPARED BY THE DESIGN PROFESSIONAL AS INSTRUMENTS OF SERVICE SHALL REMAIN THE PROPERTY OF THE DESIGN PROFESSIONAL. DISSEMINATION MAY NOT BE MADE WITHOUT PRIOR CONSENT OF THE DESIGN PROFESSIONAL. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE, ARE HEREBY SPECIFICALLY RESERVED.

**GRADING & DRAINAGE PLAN**

**ISAACSON & ARFMAN, P.A.**  
 Consulting Engineering Associates  
 128 Monroe Street N.E.  
 Albuquerque, New Mexico 87108  
 Ph. 505-268-8828 www.iacivil.com  
 2152 CG-101.dwg Apr 27, 2016

66ARCHITECT, LLC  
 Clint Wisley, Architect  
 clint.wisley@gmail.com  
 505 280-0043



M:\PROJECTS\2016\1521\1521-01\1521-01.dwg

**CIVIL GENERAL NOTES**

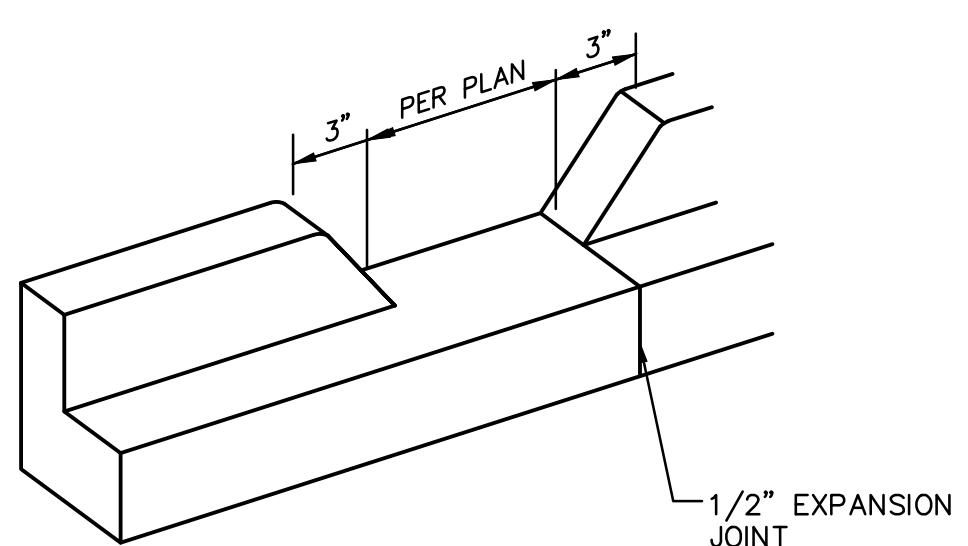
- A. THE CONTRACTOR SHALL ABIDE BY ALL STATE, LOCAL, AND FEDERAL LAWS, CODES, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS, INCLUDING EPA AND ADA REQUIREMENTS.
- B. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED ON OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE CURRENT EDITION OF THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS (COA SPEC.).
- C. NO WORK SHALL BE PERFORMED WITHOUT THE APPROPRIATE PERMITS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION, OR PRIOR TO OCCUPANCY, AS APPROPRIATE. IF PERMITS ARE DELAYED OR ISSUED WITH CONDITIONS, THE CONTRACTOR SHALL NOTIFY THE OWNER AND ARCHITECT IMMEDIATELY.
- D. COORDINATE WORK WITH SITE PLAN, UTILITY PLAN, DEMOLITION PLAN, AND LANDSCAPE PLAN.
- E. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING OBSTRUCTIONS, AND CONDITION OF ALL EXISTING INFRASTRUCTURE PRIOR TO CONSTRUCTION. REPORT ALL DISCREPANCIES TO THE ENGINEER AND VERIFY THE ENGINEER'S INTENT BEFORE PROCEEDING.
- F. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE SAFETY.
- G. THE CONTRACTOR SHALL MAINTAIN RECORD DRAWINGS ON SITE AT ALL TIMES.
- H. CONTRACTOR SHALL OBTAIN ALL REQUIRED INSPECTIONS OF THE WORK. CONTRACTOR SHALL REGULARLY UPDATE OWNER AND ARCHITECT REGARDING THE STATUS OF THE INSPECTIONS.
- I. CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT STRUCTURES RESULTING FROM THE CONSTRUCTION PROCESS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL BE RESPONSIBLE FOR DOCUMENTING EXISTING CONDITIONS PRIOR TO CONSTRUCTION.
- J. CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS. EQUIPMENT SHALL ONLY OBSTRUCT DESIGNATED TRAFFIC LANES IF APPROPRIATE BARRICADING PERMITS HAVE BEEN OBTAINED. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL IN THE RIGHT-OF-WAY.
- K. THE CONTRACTOR SHALL PROVIDE A CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN THAT CONFORMS TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND LOCAL REQUIREMENTS. THE CONTRACTOR SHALL OBTAIN BARRICADING PERMITS FROM THE APPROPRIATE AUTHORITIES PRIOR TO ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.
- L. THE CONTRACTOR SHALL MAINTAIN ALL BARRICADING AND CONSTRUCTION SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
- M. EXISTING UTILITY LINES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND MAY BE INCOMPLETE OR OBSOLETE. SUCH LINES MAY OR MAY NOT EXIST WHERE SHOWN OR NOT SHOWN. CONTRACTOR SHALL CONTACT NM-811 FOR UTILITY LINE SPOTS FIVE WORKING DAYS PRIOR TO CONDUCTING SITE FIELD WORK. CONTRACTOR SHALL FIELD VERIFY AND LOCATE ALL UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF NECESSARY DRY UTILITY ADJUSTMENTS.
- N. FIVE WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT NM811 (811) FOR LOCATION OF EXISTING UTILITIES.
- V. ALL SITE PREPARATION, GRADING OPERATIONS, FOUNDATION CONSTRUCTION, AND PAVEMENT INSTALLATION WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT, WHICH WILL BE PROVIDED BY THE OWNER OR ARCHITECT. ALL OTHER WORK SHALL, UNLESS OTHERWISE NOTED IN THE PLANS, BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT SPECIFICATION (FIRST PRIORITY), AND/OR NMDOT STANDARD SPECIFICATIONS FOR PUBLIC WORK (SECOND PRIORITY.)
- W. ALL TRASH, DEBRIS, & SURFACE VEGETATION SHALL BE CLEARED AND LEGALLY DISPOSED OF OFFSITE.
- X. VIBRATORY COMPACTION SHALL NOT BE USED OVER IN-PLACE UTILITIES.
- Y. SOIL TESTING AND INSPECTION SERVICES DURING SITE OPERATIONS ARE REQUIRED. CONTRACTOR SHALL PROVIDE SOIL TESTING AND INSPECTION SERVICES. ALLOW TESTING LABS TO INSPECT AND APPROVE COMPACTED SUBGRADES, BACKFILL, AND FILL LAYERS BEFORE FURTHER CONSTRUCTION WORK IS DONE. SHOULD COMPACTION TESTS INDICATE INADEQUATE DENSITY, CONTRACTOR SHALL PROVIDE ADDITIONAL COMPACTION AND TESTING AT THE CONTRACTOR'S SOLE EXPENSE.
- Z. CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING. CONTRACTOR SHALL LOCATE AND PRESERVE ALL BOUNDARY CORNERS AND REPLACE ANY LOST OR DISTURBED CORNERS AT CONTRACTOR'S SOLE EXPENSE. PROPERTY CORNERS SHALL ONLY BE RESET BY A REGISTERED LAND SURVEYOR.
- AA. ADJUST ANY RIMS OF EXISTING UTILITY FEATURES AS NECESSARY TO MATCH NEW GRADES. UTILITIES IN PAVED AREAS SHALL BE HS-25 TRAFFIC RATED.
- AB. CONTRACTOR SHALL COMPLY WITH LOCAL REGULATIONS FOR RESEEDING OF DISTURBED AREAS.

**GRADING GENERAL NOTES**

- A. GRADING SHALL BE PERFORMED AT THE ELEVATIONS AND IN ACCORDANCE WITH THE DETAILS SHOWN ON THIS PLAN.
- B. PROPOSED SPOT AND CONTOUR ELEVATIONS SHOWN REPRESENT TOP OF FINISH MATERIAL (I.E. TOP OF CONCRETE, TOP OF CONCRETE BUILDING PAD, TOP OF PAVEMENT MATERIAL, TOP OF LANDSCAPING MATERIAL, ETC.). CONTRACTOR SHALL GRADE, COMPACT SUBGRADE AND DETERMINE EARTHWORK ESTIMATES BASED ON ELEVATIONS SHOWN MINUS FINISH MATERIAL THICKNESSES.
- C. IF FIELD GRADE ADJUSTMENTS ARE REQUIRED, THE CONTRACTOR SHALL NOTIFY THE ARCHITECT FOR FURTHER INSTRUCTIONS.
- D. THE ENVIRONMENTAL PROTECTION AGENCY (EPA) AND THE CITY OF ALBUQUERQUE REQUIRE A STORM WATER POLLUTION PREVENTION PLAN (SWPPP), AN NPDES PERMIT, AND AN EROSION AND SEDIMENT CONTROL (ESC) PERMIT FOR PROJECTS WHERE CONSTRUCTION ACTIVITIES MEET THE EPA THRESHOLD. (SWPPP, NPDES PERMIT, AND ESC PLAN BY OTHERS.) A CURRENT CITY-APPROVED ESC PERMIT MUST BE INCLUDED WITH THE CONTRACTOR'S SUBMITTAL FOR A ROUGH GRADING, GRADING, PAVING, BUILDING, OR WORK ORDER PERMIT. CONTRACTOR SHALL COORDINATE WITH OWNER TO DETERMINE WHO WILL PREPARE SWPPP AND INSPECT REQUIRED ELEMENTS.
- E. IF THE SITE IS SMALL ENOUGH NOT TO REQUIRE A SWPPP/NPDES PERMIT (LESS THAN ONE ACRE), THE CONTRACTOR SHALL STILL BE RESPONSIBLE FOR USING EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMP'S) TO ENSURE THAT NO SOIL ERODES FROM THE SITE ONTO ADJACENT PUBLIC RIGHT-OF-WAY.
- F. MEASURES REQUIRED FOR EROSION AND SEDIMENT CONTROL SHALL BE INCIDENTAL TO THE PROJECT COST.
- G. ALL NEW PAVEMENT SURFACES SHALL BE CONSTRUCTED WITH POSITIVE SLOPE AWAY FROM BUILDINGS AND POSITIVE SLOPE TOWARD EXISTING AND/OR PROPOSED DRAINAGE PATHS. PAVING AND ROADWAY GRADES SHALL BE  $\pm 0.1'$  FROM PLAN ELEVATIONS. BUILDING PAD ELEVATION SHALL BE  $\pm 0.05'$  FROM PLAN ELEVATION.
- H. WHERE GRADES BETWEEN NEW AND EXISTING ARE SHOWN AS "MATCH" OR "±", TRANSITIONS TO EXISTING SHALL BE SMOOTH.
- I. PAVEMENT GRADES IN MARKED HANDICAPPED PARKING AREAS SHALL NOT EXCEED 2.0% IN ANY DIRECTION. FOR ALL ACCESSIBLE ROUTES, MAXIMUM ALLOWABLE CROSS SLOPE IS 2.0% AND MAXIMUM LONGITUDINAL SLOPE WITHOUT RAMP IS 5.0%. FOLLOW ALL ADA ACCESSIBILITY GUIDELINES OR CITY CODES, WHICHEVER IS MORE STRINGENT.
- J. ALL EROSION PROTECTION TO BE INSTALLED AS 6" AVG. DIA. ANGULAR FACED ROCK (F.F. ROCK) PLACED OVER GEOTEX 501 NON-WOVEN GEOTEXTILE (O.E.).
- K. SIDESLOPES STEEPER THAN 5:1 MUST HAVE PERMANENT EROSION PROTECTION INSTALLED. TYPICAL. NO SLOPE SHALL BE STEEPER THAN 1.5:1.
- L. STORMWATER CONTROL MEASURES SHOWN ON THIS PLAN ARE TO BE STRICTLY ADHERED TO FOR CERTIFICATION PURPOSES.
- M. POST-CONSTRUCTION MAINTENANCE FOR PRIVATE STORMWATER FACILITIES WILL BE THE RESPONSIBILITY OF THE FACILITIES OWNER. PERIODIC INSPECTION AND CERTIFICATIONS OF THE FACILITIES MAY BE REQUIRED BY THE CITY ENGINEER. ENGINEER RECOMMENDS THAT OWNER INSPECT SITE YEARLY AND AFTER EACH RAINFALL TO IDENTIFY NEW AREAS OF EROSION AND INSTALL ADDITIONAL EROSION PROTECTION AS NEEDED BASED ON ACTUAL OCCURRENCES.
- N. FOR ENGINEER'S CERTIFICATION OF SUBSTANTIAL COMPLIANCE (FOR CERTIFICATE OF OCCUPANCY) CONTRACTOR SHALL PROVIDE AN AUTOCAD FORMAT AS-BUILT SURVEY PREPARED BY A LICENSED SURVEYOR WHICH INCLUDES:
  - AS-BUILT SPOT ELEVATIONS AT EACH DESIGN SPOT ELEVATION SHOWN ON THE APPROVED PLAN;
  - ALL CONSTRUCTION, INCLUDING DRAIN INLETS, PIPES AND PONDS SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN SUBSTANTIAL COMPLIANCE WITH THE APPROVED PLAN IN ORDER TO RECEIVE ENGINEER'S CERTIFICATION.
- P. GRADING OF FIRST FLUSH BASINS WILL BE INSPECTED AS PART OF ENGINEER'S CERTIFICATION FOR CERTIFICATE OF OCCUPANCY. DURING LANDSCAPING, FIRST FLUSH BASINS WILL BE SMOOTHLY INTEGRATED INTO LANDSCAPING WHILE MAINTAINING REQUIRED TOP AND BOTTOM ELEVATION, VOLUME AND INLET / OVERFLOW ELEVATIONS.

**STORM DRAIN GENERAL NOTES**

- A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COMPLETE INSTALLATION OF ALL WORK RELATED TO PROPOSED STORM DRAINS SHOWN ON THIS PLAN INCLUDING: TRENCHING, BACKFILL, SUPPORTS, INLET AND MANHOLE COLLARS, MANHOLES, WATER QUALITY FEATURES, EROSION CONTROL FEATURES, TESTING, CLEANING, AND STERILIZING. ANY WORK NOT ACCEPTED BY THE ARCHITECT OR ENGINEER DUE TO IMPROPER WORKMANSHIP OR LACK OF PROPER COORDINATION SHALL BE REMOVED AND CORRECTLY INSTALLED AT THE CONTRACTOR'S EXPENSE, AS DIRECTED.
- B. MINIMUM COVER FOR STORM DRAIN PIPES SHALL BE 12", UNLESS OTHERWISE NOTED.
- C. STORM DRAINS SHALL BE INSTALLED AFTER COMPLETION OF THE SITE ROUGH GRADING.
- D. STORM DRAINS SHALL BE INSTALLED PRIOR TO SURFACE IMPROVEMENTS SUCH AS PAVEMENT, SIDEWALKS, AND LANDSCAPING.
- E. CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTIONS TO ROOF DOWNSPOUTS AND ALL NECESSARY FITTINGS. FITTING COSTS SHALL BE INCIDENTAL.
- F. TRENCHING, BORING, AND JACKING SHALL BE CONSTRUCTED IN ACCORDANCE WITH COA SPEC. SECT. 700. ALL BACKFILL SHALL BE COMPACTED TO A MINIMUM 95% DENSITY PER ASTM D-1557.
- G. ALL STORM DRAIN CROSSINGS OF WATER AND SEWER LINES SHALL HAVE 18" MIN CLEARANCE. IF 18" CLEARANCE IS NOT POSSIBLE, CONTACT THE ENGINEER IMMEDIATELY.
- H. HDPE PIPE SHALL BE ADS N-12 (WATERTIGHT) OR ENGINEER APPROVED EQUIVALENT. HDPE PIPE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
- I. PVC PIPES SHALL BE PVC SDR-35, INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
- J. STORM DRAINS SHALL BE INSTALLED AT INVERTS AND SLOPES SPECIFIED ON THE PLANS.



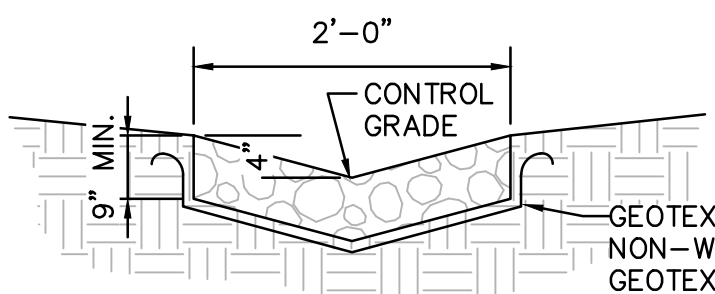
**GENERAL NOTES**

- 1. EDGES NOT SPECIFICALLY DIMENSIONED SHALL BE SHAPED WITH A 3/8" EDGING TOOL.

**CURB OPENING**

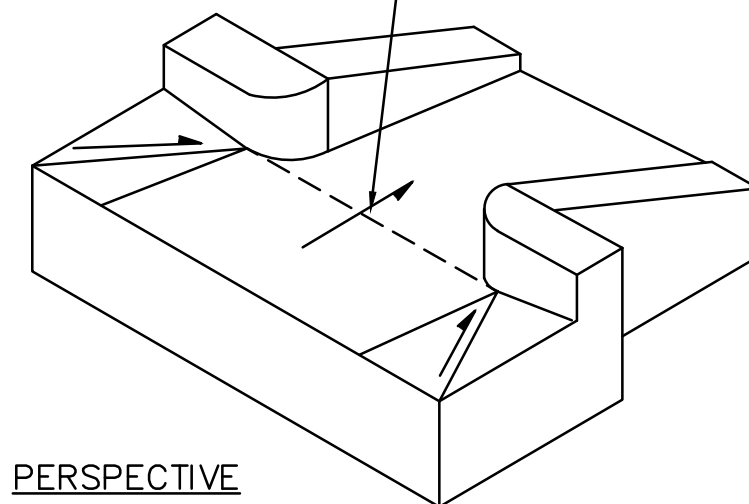
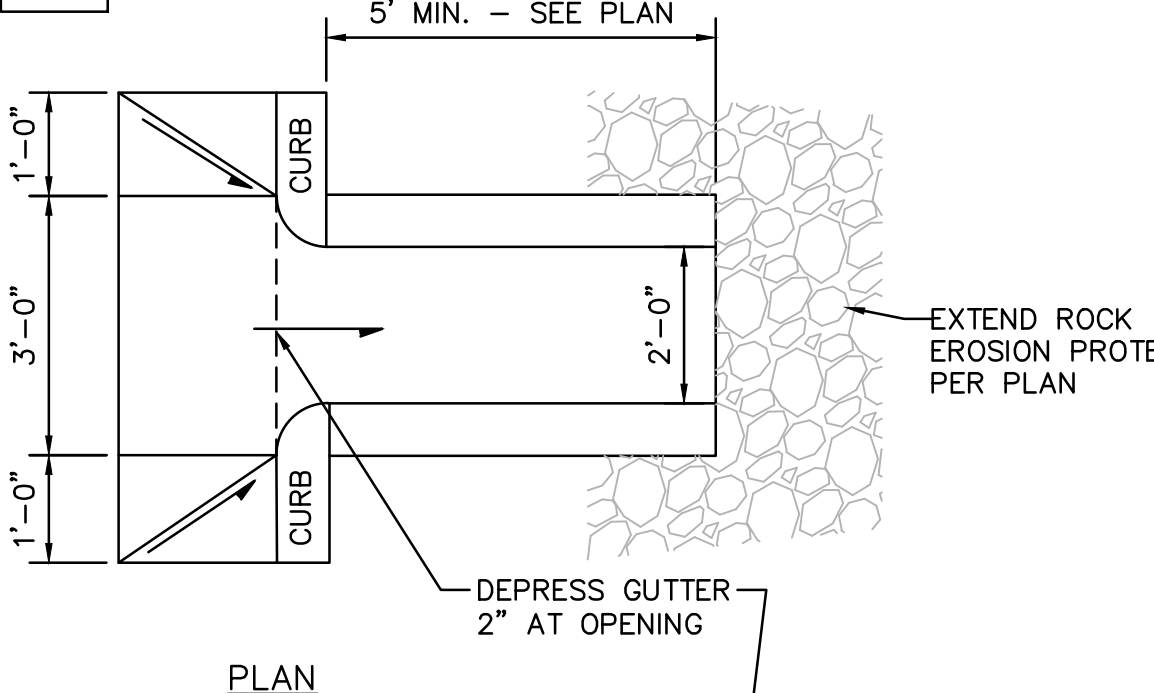
SCALE: N.T.S.

- VARY ANGULAR FACE ROCK SIZE BETWEEN 4" AND 8" DIA. (AVG.=6")
- PLACE GEOTEX 501 NON-WOVEN GEOTEXTILE (O.E.) BENEATH ALL EROSION PROTECTION
- CONSTRUCT ALL EROSION PROTECTION INSET INTO (NOT ON TOP OF) GRADE TO ENSURE RUNOFF CAN BE CAPTURED AND CONVEYED PROPERLY



**ROCK EROSION PROTECTION**

SCALE: N.T.S.

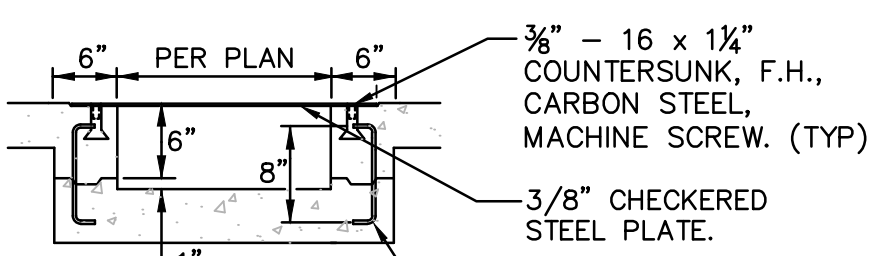


**CURB OPENING**

SCALE: N.T.S.

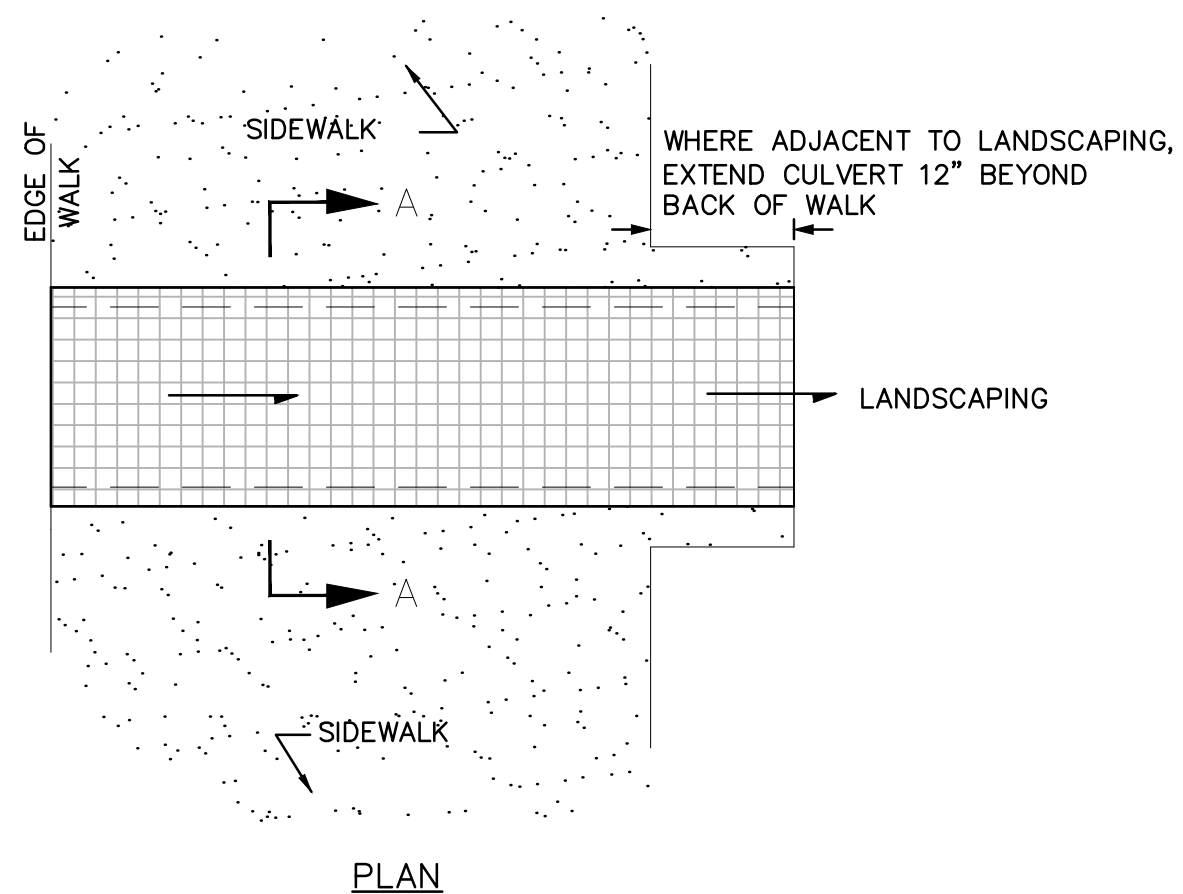
WELD 3/8" THICK, 3/8" MIN. DIAMETER OVER ALL SCREWS. COMPLETELY COVER SCREW HEADS. GRIND EDGES SMOOTH.

FOR SECURING PLATE USE 1"x5" S.S. ROD ANCHOR, "RED HEAD MULTI-SET II SRM-38 ANCHOR" OR APPROVED EQUAL. INSTALL PER MANUFACTURER'S INSTRUCTIONS AT MAX. 24" O.C., A MINIMUM OF 2 PER SIDE AND ONE WITHIN 6" OF EACH END.



USE NO. 3 DEFORMED BAR DOWELS. SPACE DOWELS AT 18" O.C. MAXIMUM. 1-1/2" MINIMUM FROM FACE OF CONCRETE

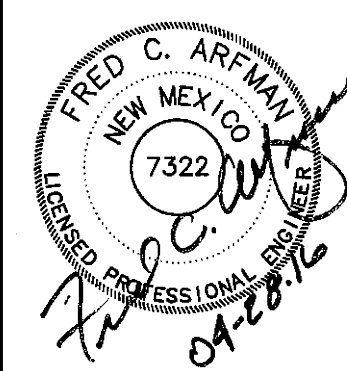
**SECTION A-A**



CONSTRUCT PER C.O.A. STD. DWG. 2236 WITH MODIFICATIONS AS SHOWN ON THIS DETAIL

**COVERED SIDEWALK CULVERT**

SCALE: N.T.S.



**Advance Auto Parts**  
 TRACT 1, PARADISE PLAZA  
 10800 UNISER BLVD. NW  
 ALBUQUERQUE, NM 87117  
 STORE # ---

REV	DATE	DESCRIPTION

DATE: 4-26-16  
 PROJECT # 1A2152  
 DRAWN BY: BJB  
 CHECK BY: FCA  
 VERSION 04-83x83  
ALL REPORTS, PLANS, SPECIFICATIONS, FIELD DATA, NOTES AND OTHER DOCUMENTS, INCLUDING ALL DOCUMENTS ON ELECTRONIC MEDIA, PREPARED BY THE DESIGN PROFESSIONAL AS INSTRUMENTS OF SERVICE SHALL REMAIN THE PROPERTY OF THE DESIGN PROFESSIONAL. DISSEMINATION MAY NOT BE MADE WITHOUT PRIOR CONSENT OF THE DESIGN PROFESSIONAL. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

**GRADING & DRAINAGE DETAILS**

**ISAACSON & ARFMAN, P.A.**  
 Consulting Engineering Associates  
 128 Monroe Street N.E.  
 Albuquerque, New Mexico 87108  
 Ph. 505-268-8828 www.iacivil.com

66ARCHITECT, LLC  
 Clint Wilsey, Architect  
 clint.wilsey@gmail.com  
 505 280-0043

CG-102

