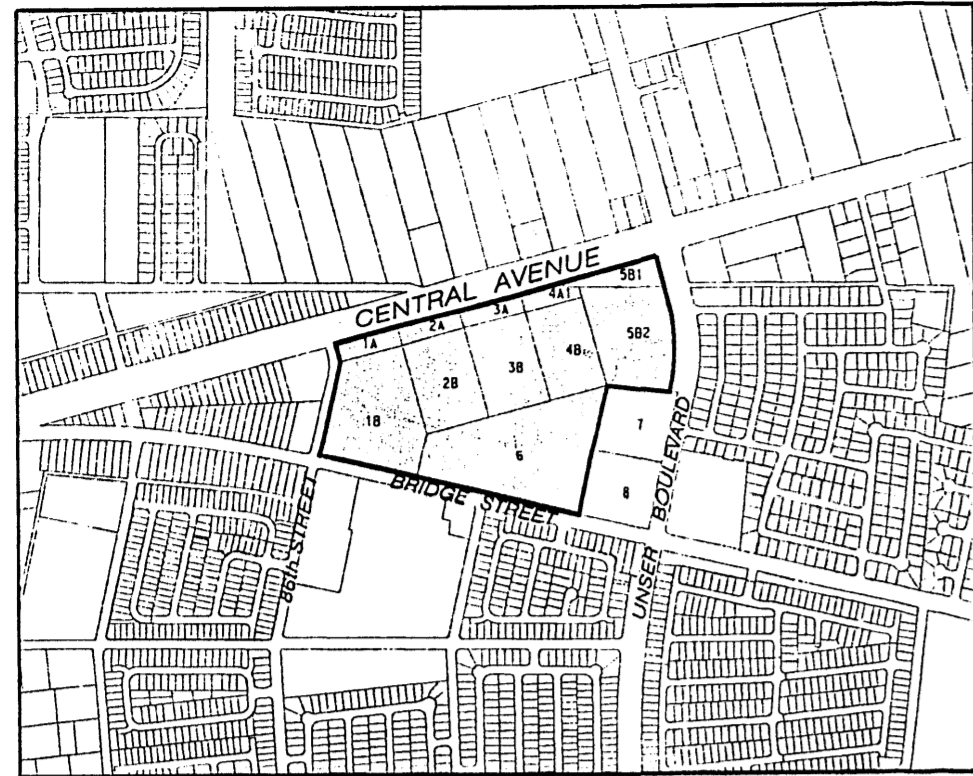


SP-2008242970



LOCATION MAP

ZONE ATLAS INDEX MAP Nos. 1:9 & K10  
NOT TO SCALE

SUBDIVISION DATA

- 1. DRB No.
2. Zone Atlas Index Nos. K-9 and K-10
3. Zoning: C-2 & SU-1
4. Gross Subdivision Acreage: 50.6833 Acres.
5. Total number of tracts Created: Fourteen (14) Tracts.
6. Total area of dedicated public street right-of-way: 0.1432 acre.
7. Date of Survey: June, 2008.
8. Plat is located within the Town of Atrisco Grant in projected Sections 21 & 22, Township 10 North, Range 2 East, NMPM, City of Albuquerque, Bernalillo County, New Mexico.

DISCLOSURE STATEMENT

The purpose of this Plat is to subdivide Tracts One-A (1-A), One-B (1-B), Two-A (2-A), Two-B (2-B), Three-A (3-A), Three-B (3-B), Four-B (4-B) and Six (6) of V.E. Barrett Subdivision, as the same is shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico, on February 13, 1967, in Book C6, Page 161 together with all of Tracts Four-A-1 (4-A-1), Five-B-1 (5-B-1), and Five-B-2 (5-B-2), Lands of Weeco, Partners, as the same are shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico on April 6, 1987, in Book C-33 Page 81, together with all of Tracts Four-A-1 (4-A-1), Five-B-1 (5-B-1), and Five-B-2 (5-B-2), Lands of Weeco, Partners, as the same are shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico on April 6, 1987, in Book C-33, Page 81, together with all of Tracts Four-A-1 (4-A-1), Five-B-1 (5-B-1), and Five-B-2 (5-B-2), Lands of Weeco, Partners, as the same are shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico on April 6, 1987, in Book C-33, Page 81.

PUBLIC UTILITY EASEMENTS

PUBLIC UTILITY EASEMENTS shown on this plat are for the common joint use of:

- A. PNM Electric Services for the installation, maintenance and service of underground electrical lines, transformers, and other equipment, fixtures, structures, and related facilities reasonably necessary to provide electrical service.
B. PNM Gas services for installation, maintenance, and service of underground natural gas lines, valves and other equipment and facilities reasonably necessary to provide natural gas.
C. QWEST for the installation, maintenance, and service of all buried communication lines and other related equipment and facilities reasonably necessary to provide communication services, including but not limited to above ground pedestals and closures.
D. Comcast Cable for the installation, maintenance, and service of such underground lines, cable, and other related equipment and facilities reasonably necessary to provide Cable TV service.

Included is the right to build, rebuild, construct, reconstruct, locate, relocate, change, remove, modify, renew, operate, and maintain facilities for the purposes described above, together with free access to, from, and over said easements, including sufficient working area space for electric transformers, with the right and privilege to trim and remove trees, shrubs or bushes which interfere with the purposes set forth herein. No building, sign, pool, (above ground or subsurface), hot tub, concrete or wood pool decking, or other structure shall be erected or constructed on said easements, nor shall any well be drilled or operated thereon. Property owners shall be solely responsible for correcting any violations of National Electric Safety Code caused by construction of pools, decking, or any structures adjacent to or near easements shown on this plat.

Easements for electric transformers/switchgears, as installed shall extend ten feet (10) in front of transformer/switchgear doors and five feet (5) on each side.

DESCRIPTION

A certain tract of land situate within the Town of Atrisco Grant in projected Sections 21 and 22, Township 10 North, Range 2 East, New Mexico Principal Meridian, City of Albuquerque, Bernalillo County, New Mexico being and comprising all of Tracts One-A (1-A), One-B (1-B), Two-A (2-A), Two-B (2-B), Three-A (3-A), Three-B (3-B), Four-B (4-B) and Six (6) of V.E. Barrett Subdivision, as the same is shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico, on February 13, 1967, in Book C6, Page 161 together with all of Tracts Four-A-1 (4-A-1), Five-B-1 (5-B-1), and Five-B-2 (5-B-2), Lands of Weeco, Partners, as the same are shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico on April 6, 1987, in Book C-33, Page 81.

Tract contains 50.6833 acres of land, more or less.

NOTES

- 1. Bearings are New Mexico State Plane Grid Bearings (Central Zone) NAD 1983 and the Basis of Bearings is the Inverse between Albuquerque Control Monuments "7-K10" and "14-K9" Bearing = S58°28'10"W.
2. Record Bearings and distances are shown in parenthesis.
3. Distances are ground distances.
4. All easements of record are as shown on the Plat of record or made known to me by the owner, utility companies, or other interested parties.
5. All interior tract corners to be monumented by a #5 rebar and yellow plastic survey cap stamped "GROMATZKY PS 16469".
6. All streets are public, to be dedicated to the City of Albuquerque with the filing of this plat.
7. Pursuant to section 14-14-4.7 of the City of Albuquerque Code of Ordinances, "No property within the area of this Plat shall at anytime be subject to a deed restriction, covenant, or binding agreement prohibiting solar collectors from being installed on buildings or erected on the lots or parcels within the area of the proposed plat. The foregoing requirements shall be a condition to approval of this plat."
8. Reciprocal cross access easements for the benefit of Tracts 1 through 14, are granted by separate Easements, Convents, Conditions and Restrictions ("ECC&Rs") recorded concurrently herewith to be maintained by the owners of those Tracts as provided in the ECC&Rs, RECORDED 2008102534.
9. Reciprocal storm water drainage easements for the benefit of Tracts 1 through 14 are granted by the ECC&Rs to be maintained by the owners of those Tracts as provided in the ECC&Rs, RECORDED 2008102534.
10. Tracts 1 thru 14 are subject to covenants and restrictions in the ECC&Rs.
11. This grant of easement is for the benefit of the owners, tenants, employees and invitees of Tract 7 and 8, V.E. Barrett Subdivision for the sole purpose of vehicular access. Grantees' exercise of this easement shall not unreasonably interfere with business operations on the grantor's property. Grantees shall indemnify the grantor against claims arising from use of this easement. Grantees shall reimburse an equitable share of the reasonable costs of maintaining such vehicular access upon invoice from the party performing such maintenance and insuring such vehicular access. Grantees shall obtain general commercial liability insurance covering use of this easement with limits reasonably acceptable to grantors naming grantors as additional insureds.

SUBORDINATION CLAUSE:

The Undersigned Subordinates its interest in the herein - described Real Property to the Right and Interests of The City of Albuquerque in The Real Property and all interest herein dedicated to The City of Albuquerque.

LIENHOLDER

In witness thereof, The Huntington National Bank, as the lending institution, has caused these presents to be

Executed this 25th day of July, 2008

BY: JOHN CLINGAN, VICE PRESIDENT - COMMERCIAL LENDING, Commonwealth of Pennsylvania, COUNTY OF Allegheny

The foregoing instrument was acknowledged before me this 25th day of July, 2008.

THE HUNTINGTON NATIONAL BANK, BY: JOHN CLINGAN, VICE PRESIDENT - COMMERCIAL LENDING

Witness My Hand and Official Seal, Judith L. White, Notary Public, City of Pittsburgh, Allegheny County, My Commission Expires 4-15-2010

SURVEYOR'S CERTIFICATION

I, Robert Gromatzky, a registered Professional New Mexico Surveyor, certify that I am responsible for this survey and that this plat was prepared by me or under my supervision, shows all easements as shown on the plat of record, or made known to me by the owner, utility companies, or other interested parties and conforms to the Minimum Requirements of the Board of Registration for Professional Engineers and Professional Surveyors and meets the minimum requirements for monumentation and surveys contained in the City of Albuquerque Subdivision Ordinance, and is true and accurate to the best of my knowledge and belief.

Robert Gromatzky, New Mexico Professional Surveyor 16469

Date: 7-24-08



FREE CONSENT AND DEDICATION

This plat of that certain tract of land situate within the Town of Atrisco Grant in projected Sections 21 & 22, Township 10 North, Range 2 East, New Mexico Principal Meridian, City of Albuquerque, Bernalillo County, New Mexico being and comprising all of Tracts One-A (1-A), One-B (1-B), Two-A (2-A), Two-B (2-B), Three-A (3-A), Three-B (3-B), Four-B (4-B) and Six (6) of V.E. Barrett Subdivision, as the same is shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico, on February 13, 1967, in Book C6, Page 161 together with all of Tracts Four-A-1 (4-A-1), Five-B-1 (5-B-1), and Five-B-2 (5-B-2), Lands of Weeco, Partners, as the same are shown and designated on the plat thereof, filed in the office of the County Clerk of Bernalillo County, New Mexico on April 6, 1987, in Book C-33, Page 81, records of Bernalillo County, New Mexico, now comprising Tracts 1 thru 14, Unser Crossings is with the free consent and in accordance with the desires of the undersigned owner(s) and/or proprietor(s) thereof. Said owner(s) and/or proprietor(s) do hereby dedicate additional public right-of-way for 86th Street SW as shown hereon to the City of Albuquerque in fee simple with warranty covenants and do hereby grant to the City All Access, Utility and Drainage Easements shown hereon as granted to the City including the right to construct, operate, inspect, and maintain facilities therein; and to the providers of public utilities all Public Utility Easements shown hereon for the common and joint use of Gas, Electrical Power, and Communication Services for buried distribution lines, conduits, and pipes for underground utilities where shown or indicated, including the right of ingress and egress for construction and maintenance, and the right to trim interfering trees and shrubs. The City has the right to enter upon the easement property at any time and perform whatever inspection, installation, maintenance, repair, modification or removal ("Work") it deems appropriate without liability to the City. If the Work effects any improvements or encroachments by the grantor, their assigns or heirs, the City will not be financially or otherwise responsible for rebuilding or repairing of improvements or encroachments. If in the opinion of the City, the Work to be performed by the City could endanger the structural integrity or otherwise damage the improvements or encroachments, the grantor, their assigns or heirs, shall, at its own expense, take whatever protective measures are required to safeguard the improvements or encroachments. This plat shows all easements, lots, and tracts which were created by any previously recorded plat or subdivision map which comprised or affected the real property subdivided by this plat shall be deemed vacated and abandoned unless otherwise noted on its plat. Said owner warrants that he holds complete and indefeasible title in fee simple to the land subdivided. Said owner(s) and/or proprietor(s) do hereby consent to all of the foregoing and do hereby certify that this subdivision is their free act and deed.

ARMSTRONG CENTRAL UNSER BLVD, LLC, a New Mexico Limited Liability Company
By: Armstrong Development Investments, LLC, a Delaware Limited Liability Company
By: Armstrong Development Holdings, Inc., a Delaware corporation, its sole member
Robert W. FRISCH, VICE PRESIDENT

Commonwealth of Pennsylvania, COUNTY OF Allegheny

This instrument was acknowledged before me on 25th day of July, 2008, by ROBERT W. FRISCH, VICE PRESIDENT, managing member of Armstrong Central Unser, LLC, a New Mexico limited liability company.

My Commission Expires: 4-15-2010, Judith L. White, Notary Public

SANDIA PLAZA PARTNERS, LLC, a New Mexico Limited Liability Company

By: TRACI J. WOLF, MANAGING MEMBER

State of New Mexico, County of Bernalillo

This instrument was acknowledged before me on 20th day of July, 2008, by TRACI J. WOLF of Sandia Plaza Partners, LLC a New Mexico limited liability company.

My Commission Expires: 11-8-2010, Rachael Ruidar, Notary Public

JURISDICTIONAL AFFIDAVIT

I, Robert Gromatzky, a registered Professional New Mexico Surveyor, hereby affirm that the property described does lie within the platting and subdivision jurisdiction of the City of Albuquerque.

Robert Gromatzky, New Mexico Professional Surveyor 16469

Date: 7-24-08

DOCH 2008103544, 09/19/2009 03:41 PM Page: 1 of 7, 74-31-2008 - mcole

PLAT OF UNSER CROSSINGS (A REPLAT OF TRACTS 1A, 1B, 2A, 2B, 3A, 3B, 4B & TRACT 6 V.E. BARRETT SUBDIVISION AND TRACTS 4-A-1, 5-B-1 & 5-B-2 LANDS OF WEFCO, PARTNERS) ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO JULY, 2008

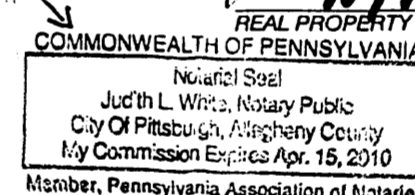
PROJECT NUMBER 1007204

APPLICATION NUMBER 08DRB-70296

PLAT APPROVAL

Table with columns for Utility Approvals (WEST TELECOMMUNICATIONS, COMCAST CABLE, PNM ELECTRIC SERVICES, PNM GAS SERVICES) and Dates (8/1/08, 8-4-08, 8-7-08, 8-7-08).

Table with columns for City Approvals (CITY SURVEYOR, TRAFFIC ENGINEERING, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY, PARKS & RECREATION DEPARTMENT, A.M.A.F.C.A., CITY ENGINEER, DRB CHAIRPERSON, REAL PROPERTY DIVISION) and Dates (7-29-08, 8/6/08, 9-12-08, 8/6/08, 8/6/08, 9-18-08).



TAX CERTIFICATION

THIS IS TO CERTIFY THAT TAXES ARE CURRENT AND PAID ON UPC #

10100570280374314

PROPERTY OWNER OF RECORD NED ROBERT, ALBUQUERQUE BERNALILLO COUNTY TREASURER'S OFFICE, DATE 9-18-08



In approving this plat, PNM Electric Services and Gas Services (PNM) did not conduct a Title Search of the properties shown hereon. Consequently, PNM does not waive or release any easement or easement rights to which it may be entitled.

Bohannon & Huston logo and address: Courtyard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335. ENGINEERING & SPATIAL DATA & ADVANCED TECHNOLOGIES

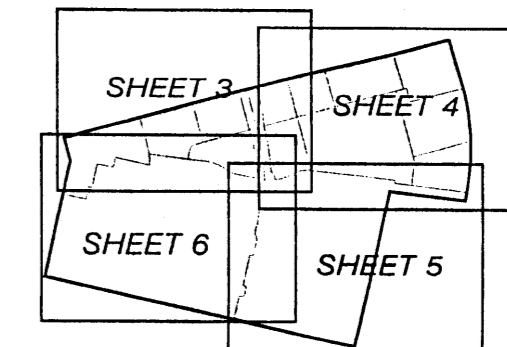
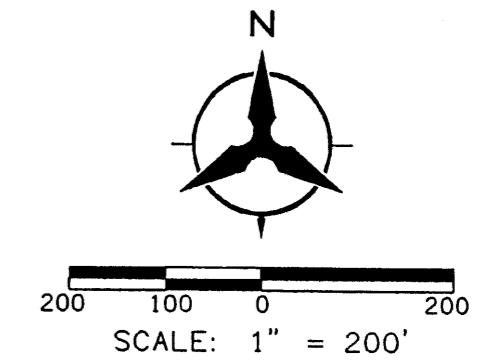
NOTE: SEE SHEETS 3 THRU 7 FOR SUBDIVISION DETAILS

PLAT OF  
**UNSER CROSSINGS**  
 (A REPLAT OF TRACTS 1A, 1B, 2A, 2B, 3A, 3B, 4B  
 & TRACT 6 V.E. BARRETT SUBDIVISION  
 AND TRACTS 4-A-1, 5-B-1 & 5-B-2  
 LANDS OF WEFCO, PARTNERS)  
 ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
 JULY, 2008

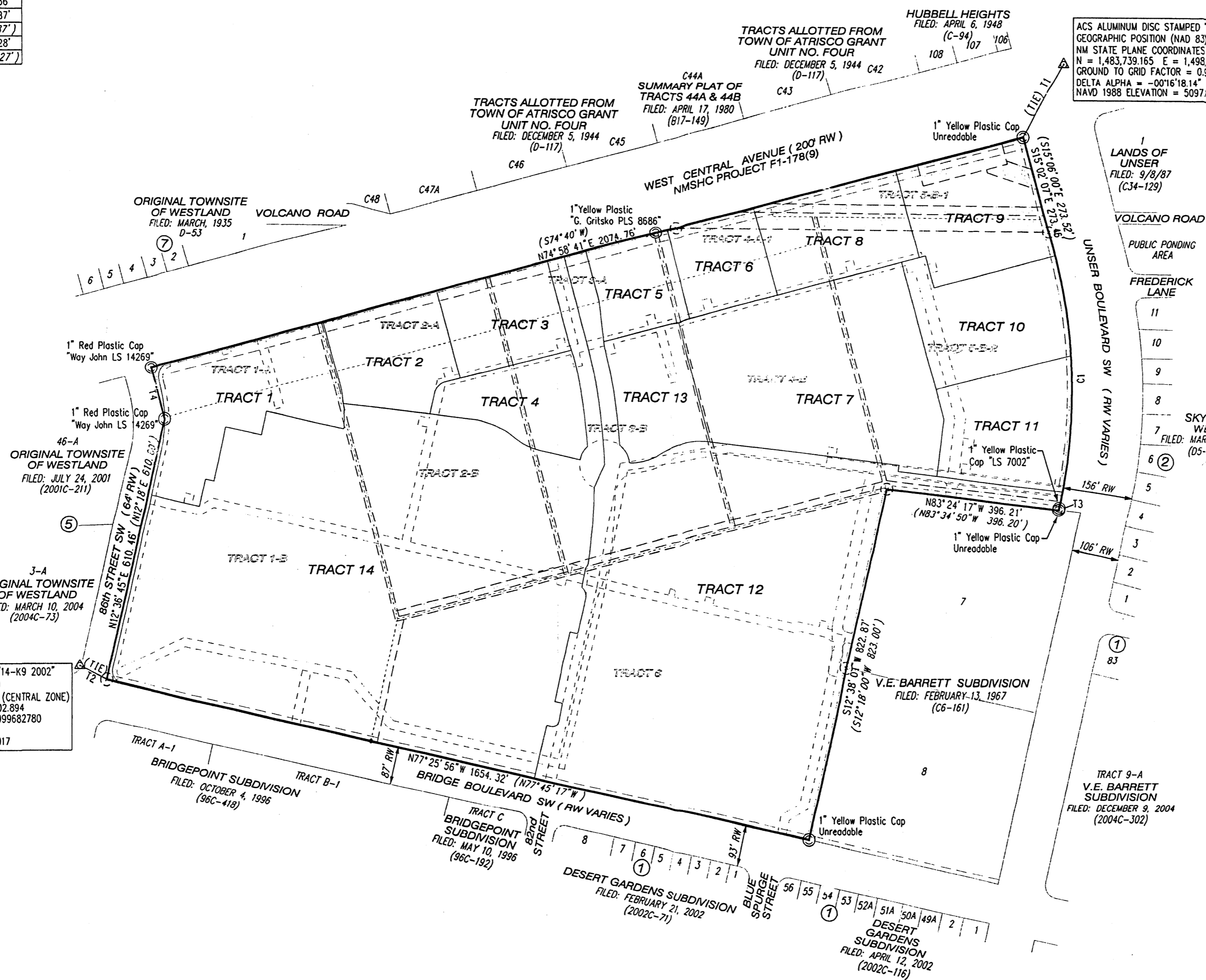
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	(27° 34' 45")	(301.72')	(591.75')	(1229.36')	(586.05')	

TABLES ARE FOR THIS SHEET ONLY

Tangent Data		
ID	BEARING	DISTANCE
T1	N29° 25' 55" E	190.37'
T2	N63° 31' 04" W	68.56'
T3	S12° 48' 38" W	4.37'
	(S12° 48' 43" W)	(3.87')
T4	N14° 59' 22" W	122.28'
	(N13° 15' 20" W)	(122.27')



ACS ALUMINUM DISC STAMPED "14-K9 2002"  
 GEOGRAPHIC POSITION (NAD 83)  
 NM STATE PLANE COORDINATES (CENTRAL ZONE)  
 N = 1482352.731 E = 1,496702.894  
 GROUND TO GRID FACTOR = 0.999682780  
 DELTA ALPHA = -00°16'33.63"  
 NAVD 1988 ELEVATION = 5118.017



LEGEND	
	SUBDIVISION BOUNDARY LINE
	EXISTING (OLD) LOT LINE TO BE ELIMINATED WITH THE FILING OF THIS PLAT
	NEW LOT LINE
	CENTERLINE
	PROPOSED EASEMENT LINE
	EXISTING EASEMENT LINE
	ADJOINING PROPERTY LINE
	RIGHT OF WAY
	PUE PUBLIC UTILITY EASEMENT GRANTED WITH THE FILING OF THIS PLAT
	CITY OF ALBUQUERQUE CONTROL MONUMENT
	#5 REBAR WITH YELLOW PLASTIC SURVEY CAP STAMPED "GROMATZKY PS 16469"
	FOUND #5 REBAR/CAP AS NOTED
	FOUND #5 REBAR NO CAP
	EXISTING EASEMENTS VACATED BY VACATION ACTION 08DRB-70232

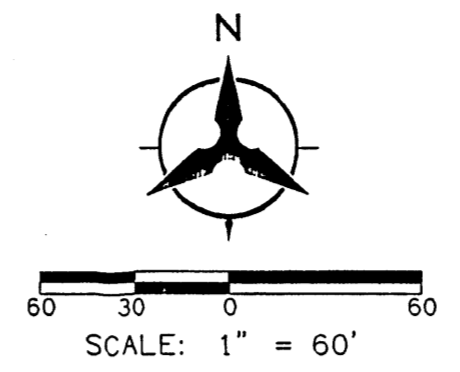
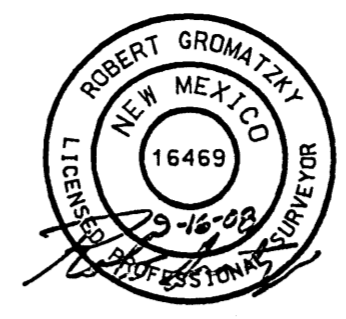


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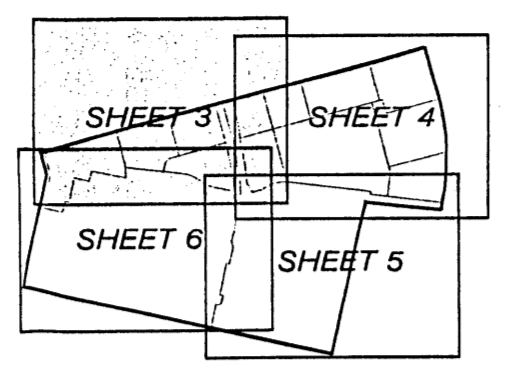
**Bohannon & Huston**  
 Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
 ENGINEERING & SPATIAL DATA & ADVANCED TECHNOLOGIES

PLAT OF  
**UNSER CROSSINGS**

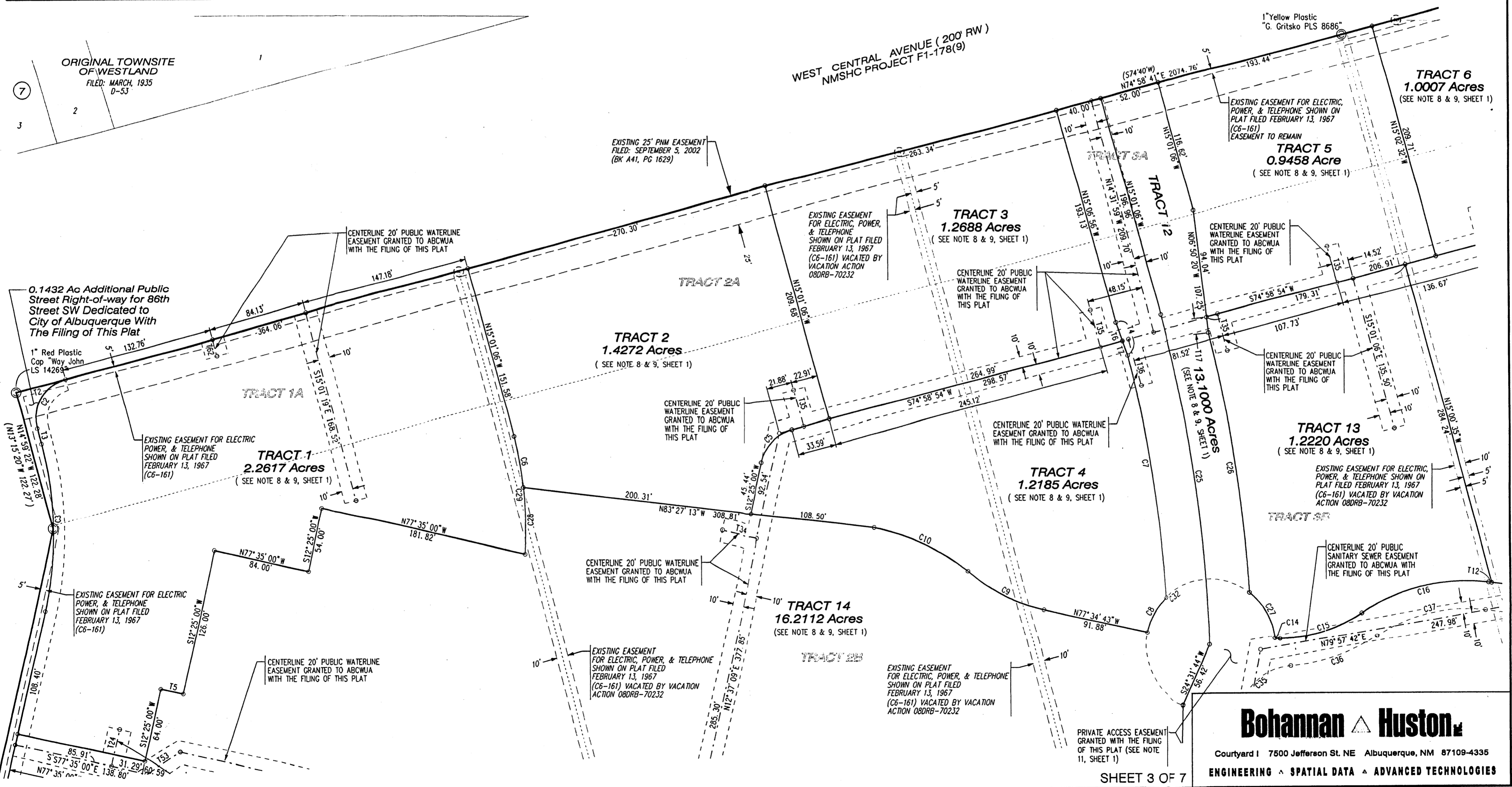
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& TRACT 6 V.E. BARRETT SUBDIVISION  
AND TRACTS 4-A-1, 5-B-1 & 5-B-2  
LANDS OF WEFCO, PARTNERS)  
ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
JULY, 2008



NOTE: SEE SHEET 7 FOR CURVE & TANGENT DATA



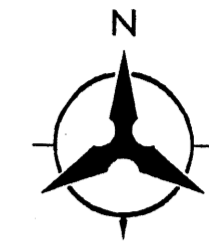
LEGEND	
	SUBDIVISION BOUNDARY LINE
	EXISTING (OLD) LOT LINE TO BE ELIMINATED WITH THE FILING OF THIS PLAT
	NEW LOT LINE
	CENTERLINE
	PROPOSED EASEMENT LINE
	EXISTING EASEMENT LINE
	ADJOINING PROPERTY LINE
	RIGHT OF WAY
	PUBLIC UTILITY EASEMENT GRANTED WITH THE FILING OF THIS PLAT
	CITY OF ALBUQUERQUE CONTROL MONUMENT
	#5 REBAR WITH YELLOW PLASTIC SURVEY CAP STAMPED "GROMATZKY PS 16469"
	FOUND #5 REBAR/CAP AS NOTED
	FOUND #5 REBAR NO CAP



**Bohannon & Huston**  
 Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
 ENGINEERING & SPATIAL DATA & ADVANCED TECHNOLOGIES

SHEET 3 OF 7

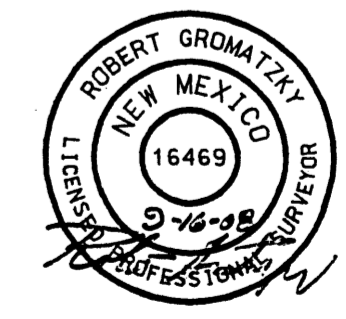
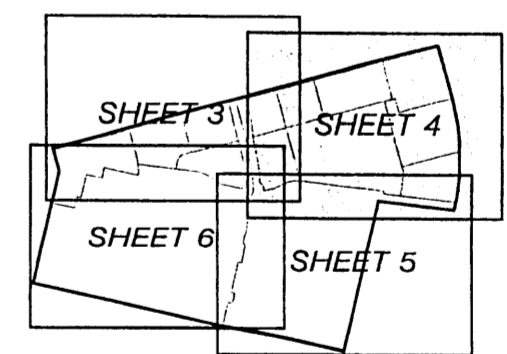
**PLAT OF  
UNSER CROSSINGS**  
(A REPLAT OF TRACTS 1A, 1B, 2A, 2B, 3A, 3B, 4B  
& TRACT 6 V.E. BARRETT SUBDIVISION  
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LANDS OF WEFICO, PARTNERS)  
ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
JULY, 2008



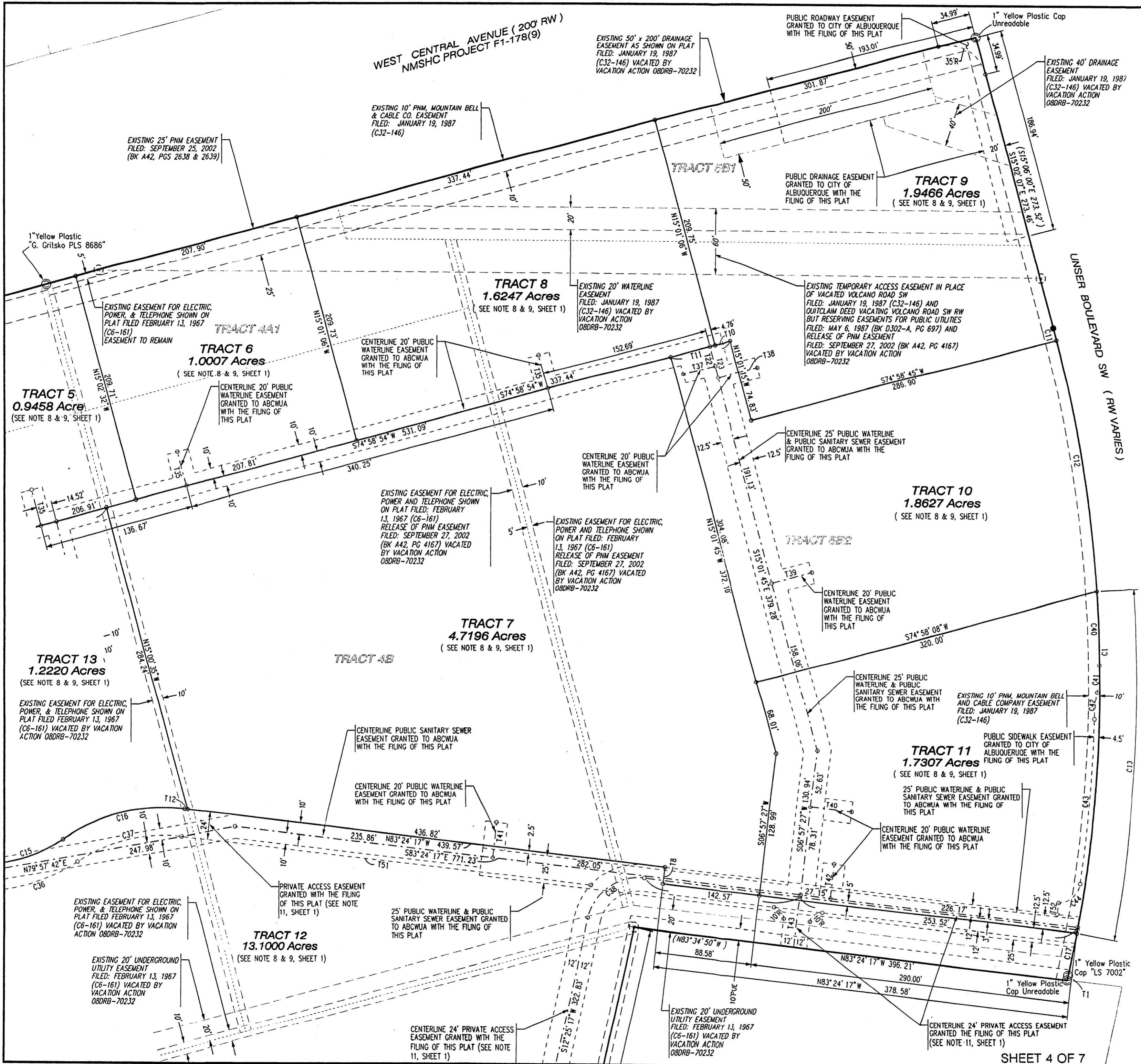
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LEGEND	
—	SUBDIVISION BOUNDARY LINE
- - - -	EXISTING (OLD) LOT LINE TO BE ELIMINATED WITH THE FILING OF THIS PLAT
—	NEW LOT LINE
- - - -	CENTERLINE
- - - -	PROPOSED EASEMENT LINE
- - - -	EXISTING EASEMENT LINE
- - - -	ADJOINING PROPERTY LINE
- - - -	RIGHT OF WAY
PUE	PUBLIC UTILITY EASEMENT GRANTED WITH THE FILING OF THIS PLAT
⊠	CITY OF ALBUQUERQUE CONTROL MONUMENT
●	#5 REBAR WITH YELLOW PLASTIC SURVEY CAP STAMPED "GROMATZKY PS 16469"
⊙	FOUND #5 REBAR/CAP AS NOTED
⊗	FOUND #5 REBAR NO CAP

NOTE: SEE SHEET 7 FOR CURVE & TANGENT DATA



**Bohannon & Huston**  
Court yard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES



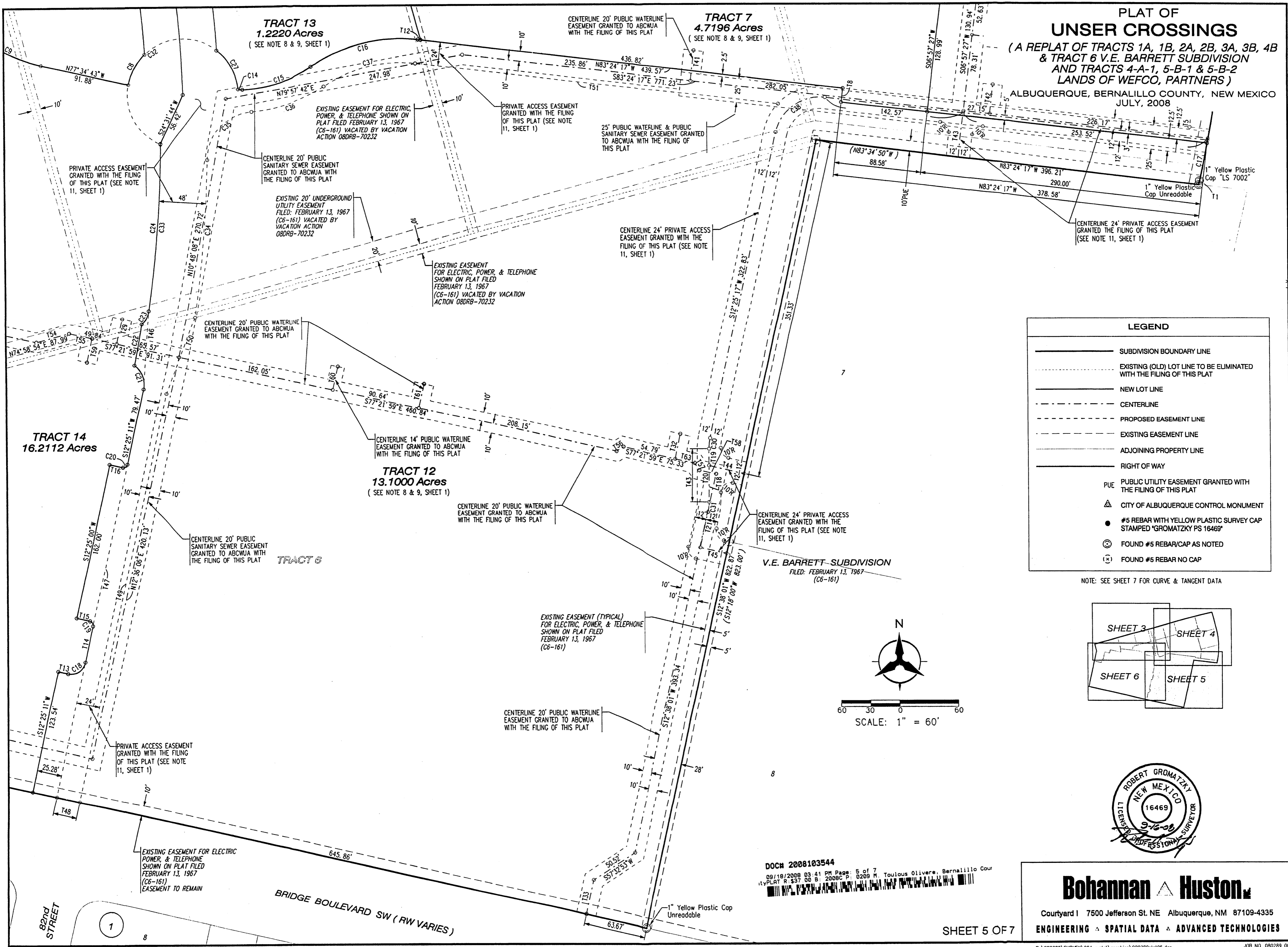
SHEET 4 OF 7

DOCH 2008103544

# PLAT OF UNSER CROSSINGS

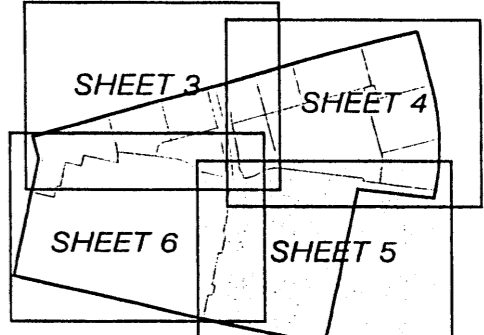
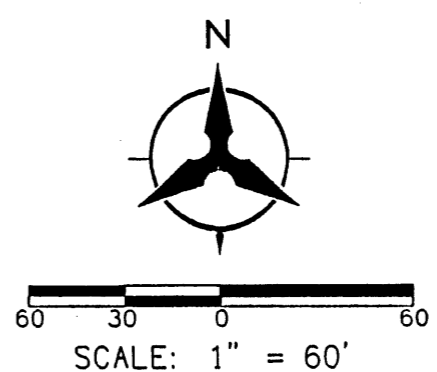
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& TRACT 6 V.E. BARRETT SUBDIVISION  
AND TRACTS 4-A-1, 5-B-1 & 5-B-2  
LANDS OF WEFCO, PARTNERS)

ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
JULY, 2008



LEGEND	
	SUBDIVISION BOUNDARY LINE
	EXISTING (OLD) LOT LINE TO BE ELIMINATED WITH THE FILING OF THIS PLAT
	NEW LOT LINE
	CENTERLINE
	PROPOSED EASEMENT LINE
	EXISTING EASEMENT LINE
	ADJOINING PROPERTY LINE
	RIGHT OF WAY
	PUE PUBLIC UTILITY EASEMENT GRANTED WITH THE FILING OF THIS PLAT
	CITY OF ALBUQUERQUE CONTROL MONUMENT
	#5 REBAR WITH YELLOW PLASTIC SURVEY CAP STAMPED 'GROMATZKY PS 16469'
	FOUND #5 REBAR/CAP AS NOTED
	FOUND #5 REBAR NO CAP

NOTE: SEE SHEET 7 FOR CURVE & TANGENT DATA



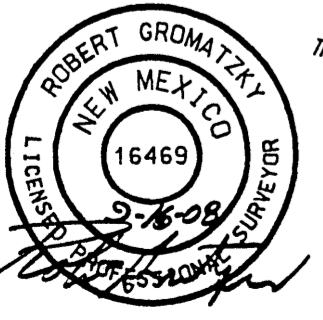
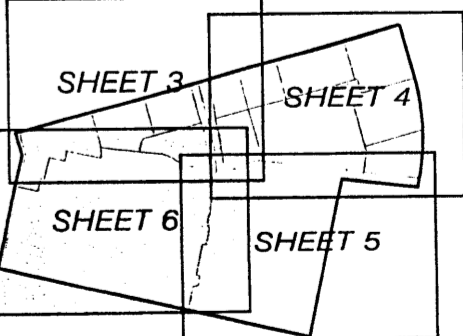
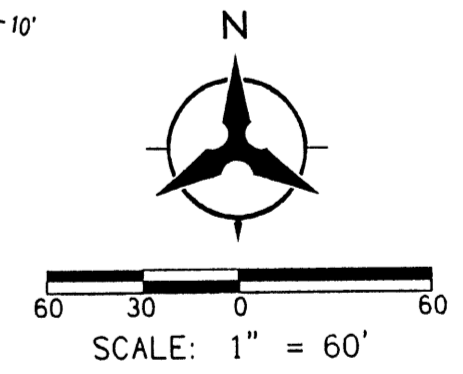
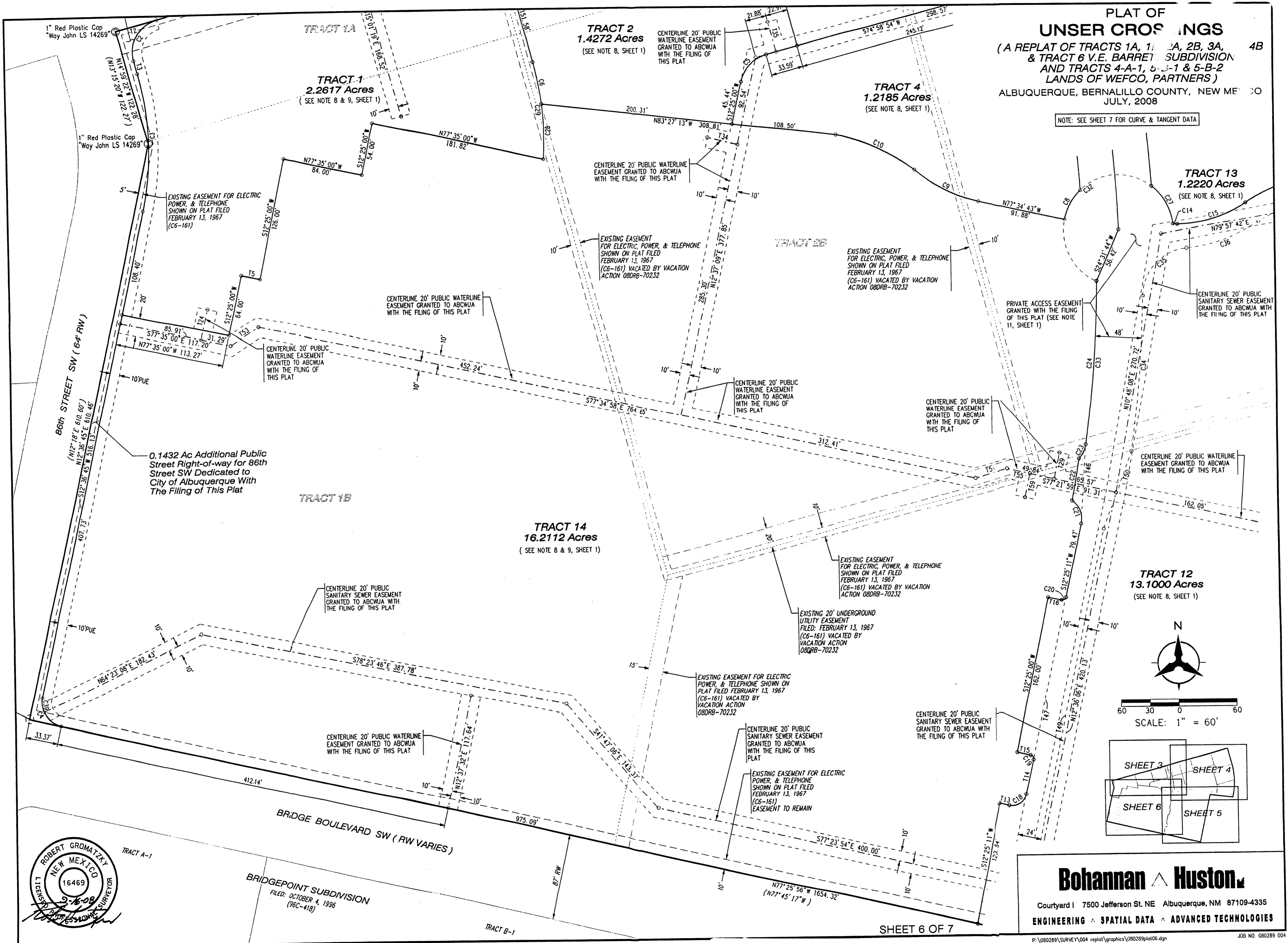
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09/18/2008 03:41 PM Page: 5 of 7  
t:\p\PLAT R-337-00-3-2008c.p: 0209 M. Toulouse Olivero, Bernalillo Cour  
14:44  
mcole

SHEET 5 OF 7

**Bohannon & Huston**  
 Courtyard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**PLAT OF  
UNSER CROSSINGS**  
(A REPLAT OF TRACTS 1A, 1B, 2A, 2B, 3A, 4B  
& TRACT 6 V.E. BARRETT SUBDIVISION  
AND TRACTS 4-A-1, 4-B-1 & 5-B-2  
LANDS OF WEFCCO, PARTNERS)  
ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
JULY, 2008

NOTE: SEE SHEET 7 FOR CURVE & TANGENT DATA



**Bohannon & Huston**  
Court yard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
ENGINEERING & SPATIAL DATA & ADVANCED TECHNOLOGIES

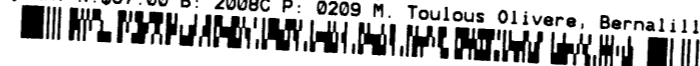
**PLAT OF  
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LANDS OF WEFCO, PARTNERS )  
ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO  
JULY, 2008

Curve Data						
ID	DELTA	TANGENT	ARC	RADIUS	CHORD	CHORD BRG
C1	27°34'44"	301.66'	591.62'	1229.11'	585.92'	S01°14'45"E
	(27°34'45")	(301.72')	(591.75')	(1229.36')	(586.05')	
C2	89°58'03"	34.98'	54.96'	35.00'	49.48'	S29°59'40"W
C3	27°36'07"	73.69'	144.52'	300.00'	143.13'	S01°11'19"E
C4	90°02'41"	25.02'	39.29'	25.00'	35.37'	S32°24'36"E
C5	62°33'54"	22.48'	40.40'	37.00'	38.43'	S43°41'57"W
C6	08°34'35"	22.49'	44.91'	300.00'	44.86'	N10°43'48"W
C7	09°56'21"	105.47'	210.42'	1213.00'	210.16'	S08°48'37"E
C8	35°08'42"	18.05'	34.96'	57.00'	34.42'	S29°07'40"W
C9	28°29'54"	38.09'	74.61'	150.00'	73.84'	N63°19'46"W
C10	25°49'36"	45.47'	89.40'	198.33'	88.65'	N66°16'01"W
C11	00°31'07"	5.56'	11.12'	1229.11'	11.12'	S14°46'33"E
C12	10°45'24"	115.71'	230.75'	1229.11'	230.41'	S09°08'18"E
C13	14°31'04"	156.56'	311.44'	1229.11'	310.60'	S03°29'55"W
C14	17°10'55"	2.27'	4.50'	15.00'	4.48'	S82°55'32"E
C15	32°05'30"	38.83'	75.61'	135.00'	74.63'	N72°26'16"E
C16	40°12'12"	60.39'	115.78'	165.00'	113.42'	N76°29'37"E
C17	01°47'09"	19.16'	38.31'	1229.11'	38.31'	S11°33'02"W
C18	90°00'00"	15.00'	23.56'	15.00'	21.21'	N57°25'11"E
C19	90°00'11"	4.00'	6.28'	4.00'	5.66'	N32°34'54"W
C20	89°59'49"	4.00'	6.28'	4.00'	5.66'	N57°25'06"E
C21	64°30'23"	15.78'	28.15'	25.00'	26.68'	N19°50'00"W
C22	02°03'08"	21.87'	43.73'	1221.00'	43.73'	N10°15'24"E
C23	37°13'34"	8.42'	16.24'	25.00'	15.96'	N27°08'02"E
C24	07°58'50"	85.52'	170.77'	1226.00'	170.63'	N04°31'50"E
C25	13°11'43"	144.58'	287.88'	1250.00'	287.24'	N08°25'14"W
C26	10°05'13"	113.58'	226.58'	1287.00'	226.28'	S08°49'31"E
C27	46°40'10"	24.59'	46.43'	57.00'	45.16'	S28°59'08"E
C28	10°45'54"	28.27'	56.37'	300.00'	56.28'	N01°03'34"W
C29	19°20'29"	51.12'	101.27'	300.00'	100.79'	S05°20'51"E
C30	12°11'31"	5.34'	10.64'	50.00'	10.62'	S06°19'32"W
C31	12°24'15"	5.43'	10.82'	50.00'	10.80'	S06°25'54"W
C32	240°04'00"	-----	238.83'	57.00'	98.69'	S54°18'07"W
C33	07°57'03"	85.20'	170.13'	1226.00'	169.99'	S04°32'43"W
C34	07°17'09"	81.11'	162.00'	1274.00'	161.89'	N04°52'40"E
C35	84°10'26"	45.16'	73.46'	50.00'	67.03'	N43°19'19"E
C36	29°01'01"	41.15'	80.52'	159.00'	79.67'	N70°54'02"E
C37	40°12'12"	51.60'	98.94'	141.00'	96.92'	N76°29'37"E
C38	84°10'26"	42.45'	69.05'	47.00'	63.00'	S54°30'30"W
C39	36°54'18"	8.34'	16.10'	25.00'	15.83'	N05°50'24"W
C40	03°05'50"	33.23'	66.44'	1229.11'	66.43'	N02°12'42"W
C41	11°19'05"	12.88'	25.68'	130.00'	25.64'	S04°59'46"W
C42	08°56'47"	12.71'	25.37'	162.50'	25.35'	S06°10'55"W
C43	06°57'21"	74.43'	148.67'	1224.61'	148.58'	S05°11'12"W
C44	80°41'45"	16.99'	28.17'	20.00'	25.90'	S49°00'45"W

Tangent Data		
ID	BEARING	DISTANCE
T1	S12°48'38"W	4.37'
	(S12°48'43"W)	(3.87')
T2	N74°58'41"E	44.41'
T3	S14°59'22"E	13.60'
T4	N19°33'49"W	30.14'
T5	N77°35'00"W	20.00'
T6	N19°33'49"W	16.62'
T7	N19°33'49"W	13.52'
T8	S06°35'43"W	15.00'
T9	S74°58'54"W	41.76'
T10	S74°58'54"W	15.00'
T11	S74°58'54"W	56.76'
T12	N83°24'17"W	2.76'
T13	N77°34'49"W	10.28'
T14	S12°25'11"W	37.09'
T15	N77°35'00"W	14.00'
T16	N77°35'00"W	14.01'
T17	N06°50'20"W	13.22'
T18	S00°13'46"W	37.25'
T19	S00°13'46"W	17.00'
T20	S00°13'46"W	54.25'
T21	S12°38'01"W	36.53'
T22	S15°01'45"E	18.65'
T23	S15°01'45"E	11.44'
T24	N12°25'00"E	40.43'
T29	N12°38'01"E	27.37'
T32	N12°38'01"E	31.32'
T33	S12°38'01"W	35.91'
T34	N77°22'51"W	31.85'
T35	N15°01'06"W	32.50'
T36	S15°01'06"E	42.63'
T37	S74°58'15"W	29.92'
T38	N74°58'15"E	37.51'
T39	N74°58'15"E	37.50'
T40	S83°02'33"E	37.77'
T41	N06°35'43"E	30.83'
T42	N06°35'43"E	31.10'
T43	N06°35'43"E	45.58'
T44	S77°11'06"E	36.67'
T45	S77°25'56"E	27.50'
T46	S04°10'34"W	82.55'
T47	S12°25'11"W	425.17'
T48	S77°25'56"E	24.00'
T49	N12°25'11"E	425.23'
T50	N20°29'35"E	85.82'
T51	S83°24'17"E	374.78'
T52	S06°35'43"W	10.88'
T53	N57°25'02"E	34.65'
T54	N74°58'54"E	34.83'
T55	S77°21'59"E	24.11'
T56	N57°38'01"E	8.49'
T57	S32°21'59"E	8.49'
T58	S77°21'59"E	17.43'
T59	S12°38'01"W	25.00'
T60	S12°38'01"W	25.05'
T61	S12°38'01"W	26.03'
T62	S15°01'19"E	15.00'
T63	S77°21'59"E	20.54'

DOCH 2008103544

09/18/2008 03:41 PM Page: 7 of 7  
 City: PLAT R: \$37.00 B: 2008C P: 0209 M: Toulous Olivere, Bernalillo Cour



SHEET 7 OF 7

**Bohannon & Huston**

Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
 ENGINEERING & SPATIAL DATA & ADVANCED TECHNOLOGIES

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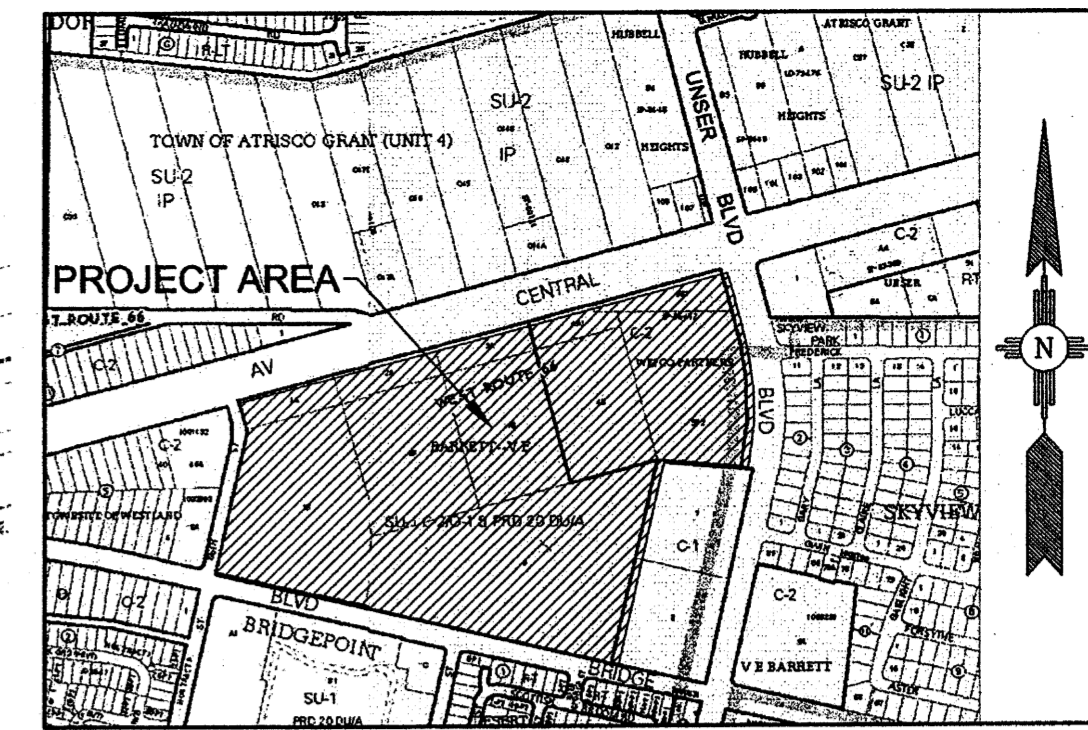
Building #	Use	Building SF	Parking Calculations										Plaza Requirements			
			Required	Transit	Required	Provided	Disabled	Disabled	Motorcycle	Motorcycle	Bike Pkg	Bike Pkg	Plaza SF	Plaza SF		
			Parking	Discount	Parking	Parking	Parking	Required	Provided	Required	Provided	Required	Provided	Required	Provided	Shown
1	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	2,249	
2	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	5,422	
3	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	1,489	
4	Retail	5,200	26	4	22	28	52	2	2	2	2	2	5	234	2,001	
5	Retail	6,500	33	5	28	56	70	2	4	3	4	2	5	234	2,001	
6	Health Club	40,000	267	41	226	228	347	8	8	5	6	6	14	15	800	3,773
7A	Retail	30,000	135	21	114											
7B	Retail	20,000	95	15	80	214	214	1	13	5	6	2	5	3,511	7,904	
7C1	Retail	4,800	24	4	20											
7C2	Retail	4,800	24	4	20											
7D	Retail	18,000	67	14	73											
7E	Retail	97,942	382	58	324	417	455	1	18	6	6	2	5	25		
8	Home Improvement	125,601	474	73	402	402	438	12	12	6	6	24	25	2,513	Share with Bldg 9	
Total		372,343	1,646	253	1,393	1,393	1,646	59	81	29	32	86	95	7,448	22,838	

- Parking Calculation Notes:**
- Parking provided includes disabled parking spaces.
  - Required parking for Building 6 (Health Club/Gym) calculated using 1space per 150 SF. All other buildings follow section 14-16-3.1-A-27 of the City of Albuquerque zoning code.
  - Transit discount of 15% taken due to the fact that the site is adjacent to a regular Albuquerque Transit system route and two transit rider shelters will be built with this project.
  - First column of required parking is without transit discount.
  - Provided parking is divided into logical grouping of buildings to allow users to park once and visit multiple retailers.

MONUMENT SIGN <sup>1</sup>	TYPE
◇	A
◇	B
◇	C
◇	D
◇	E
◇	F
◇	G

<sup>1</sup>ALL ON-SITE SIGNAGE HAS BEEN SUBMITTED TO EPC (08PC-40063) AND WILL RETURN SEPARATELY TO DRB.

- TRANSIT DEPARTMENT NOTE:**
- THE SW MESA PARK & RIDE IS LOCATED NEAR THE NW CORNER OF CENTRAL & UNSER.
  - AS OF 3/15/08, BUS ROUTES 766, 162, 66, AND 54 HAVE A STOP NEAR OR ADJACENT TO THE SITE.
  - PARTICIPATION IN TRANSIT DEMAND MANAGEMENT PROGRAM IS ENCOURAGED FOR USERS OF THE SITE.



VICINITY MAP  
ZONE ATLAS PAGES K-09-Z & K-10-Z

**SHEET INDEX**

- |  |                                 |
|--|---------------------------------|
| C-001 SITE PLAN FOR BUILDING PERMIT      | A-107B ELEVATIONS BUILDING 7    |
| C-002 SITE PLAN DETAILS                  | A-107C ELEVATIONS BUILDING 7    |
| LS-101 LANDSCAPE PLAN                    | A-107D ELEVATIONS BUILDING 7    |
| LS-102 LANDSCAPE DETAILS                 | A-107E-1 ELEVATIONS BUILDING 7  |
| C-101 CONCEPTUAL GRADING & DRAINAGE PLAN | A-107E-2 ELEVATIONS BUILDING 7  |
| C-201 CONCEPTUAL UTILITY PLAN            | A-108A ELEVATIONS BUILDING 8    |
| A-101 ELEVATIONS BUILDING 1              | A-108B ELEVATIONS BUILDING 8    |
| A-102 ELEVATIONS BUILDING 2              | A-108C ELEVATIONS BUILDING 8    |
| A-103 ELEVATIONS BUILDING 3              |                                 |
| A-104 ELEVATIONS BUILDING 4              | C-003 SITE PLAN FOR SUBDIVISION |
| A-105 ELEVATIONS BUILDING 5              | G-101 DESIGN STANDARDS          |
| A-106A ELEVATIONS BUILDING 6             | G-102 DESIGN STANDARDS          |
| A-106B ELEVATIONS BUILDING 6             | G-103 DESIGN STANDARDS          |
| A-107A ELEVATIONS BUILDING 7             |                                 |

**KEYED NOTES**

- 6" CONCRETE CURB.
- COA STANDARD CURB & GUTTER PER COA STD DWG 2454. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- 6" TEMPORARY ASPHALT CURB.
- 14' WIDE CONCRETE SIDEWALK.
- 6" WIDE CONCRETE SIDEWALK PER COA STD DWG 2430. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- 8" WIDE CONCRETE SIDEWALK.
- 10' WIDE CONCRETE SIDEWALK. SLOPE TO LANDSCAPE AREA IF POSSIBLE.
- 6"x8" LANDSCAPE PLANTER.
- BIKE RACK PER 3/C-002.
- LIGHT POLE PER 5/C-002.
- REFUSE ENCLOSURE PER 2/C-002 & 9/C-002.
- 6" HIGH SCREEN WALL PER 9/C-002.
- HANDICAP PARKING SPACE PER 4/C-002.
- TRANSIT STOP WITH SHELTER PER COA STD DWG 2555. PEDESTRIAN AMENITIES SHALL BE PER SECTION M OF THE SITE PLAN FOR SUBDIVISION.
- COLOR, TEXTURED, CONCRETE CROSSWALK.
- LOADING DOCK.
- END ISLAND PLANTER PER 1/C-002.
- 10' WIDE CONTRACTOR PARKING SPACES. TOTAL OF 24.
- TRASH COMPACTOR.
- NEW DRIVEWAY, FULL ACCESS, NEW SIGNAL RECOMMENDED BY TIA. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW RIGHT IN, RIGHT OUT DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW FULL ACCESS DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW RIGHT IN, RIGHT OUT, LEFT IN DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- CART CORRAL, WELDED METAL PIPE, PAINTED TO MATCH BUILDING.
- MOTORCYCLE PARKING WITH SIGN PER 8/C-002.
- WALL MOUNTED LIGHT.
- PROPOSED DECEL LANE/TURN LANE TO BE COMPLETED WITH PUBLIC INFRASTRUCTURE PLANS.
- APPLIANCE AND PALLET RECYCLING STORAGE UNIT.
- CROSS ACCESS EASEMENT TO BE CREATED BY PLATTING ACTION WITH THIS PROJECT.
- COLUMN, SEE ARCHITECTURAL ELEVATION.
- 8' HIGH SCREEN WALL BEHIND LOADING DOCKS PER 9/C-002.
- PROPOSED FIRE HYDRANT LOCATION.
- EXISTING BUS STOP LOCATION.
- HANDICAP PEDESTRIAN CONNECTION TO ADJACENT PAD SITES.
- ADA ACCESSIBLE RAMP AND STEPS FOR PEDESTRIAN CONNECTION.
- NEW HANDICAP RAMP PER COA STD DWG 2441. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW WHEELCHAIR RAMP PER COA STD DWG 2426. MAX SLOPE IS 12:1.

PROJECT NUMBER: 1007204  
Application Number: 08 DRB-70276

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated May 15, 2008 and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? (X) Yes ( ) No. If yes, then a set of approved DRB plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:

<i>[Signature]</i>	Date
<i>[Signature]</i>	7/9/08
<i>[Signature]</i>	7/9/08
<i>[Signature]</i>	7/16/08
<i>[Signature]</i>	9/18/08
<i>[Signature]</i>	7/15/08
<i>[Signature]</i>	7/18/08

**Unser Crossing  
DRB Submittal  
Site Plan for Building Permit  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121**

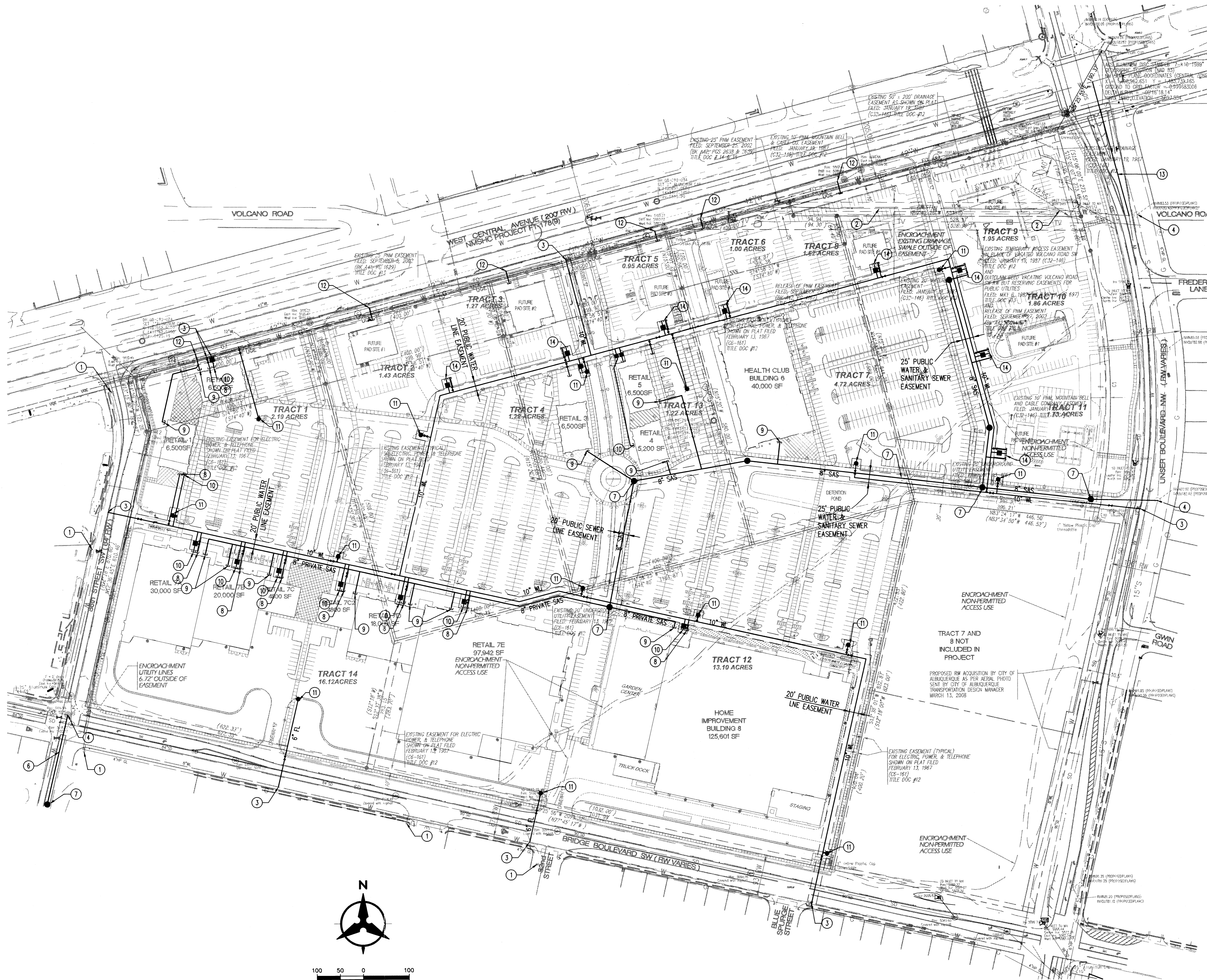
Rev. 1 June 24, 2008  
Rev. 2 July 8, 2008

Job Number  
Drawn By BO  
Checked RM/BJB  
Issue Date June 17, 2008

**SITE PLAN FOR BUILDING PERMIT**

Scale  
**C-001**





KEYED NOTES

1. EXISTING FIRE HYDRANT
2. DEMO EXISTING WATERLINE
3. CONNECT WATER SERVICE TO EXISTING WATERLINE.
4. CONNECT SEWER LINE TO EXISTING MANHOLE AT INVERT ELEVATION SHOWN.
5. CONNECT SEWER SERVICE TO NEW MANHOLE AT INVERT ELEVATION SHOWN.
6. NEW PUBLIC SEWER LINE.
7. NEW SAS MANHOLE.
8. CONNECT FIRE LINE TO WITHIN 5' OF BUILDING.
9. CONNECT SAS LINE TO WITHIN 5' OF BUILDING.
10. CONNECT WATER LINE TO WITHIN 5' OF BUILDING.
11. NEW FIRE HYDRANT.
12. CONNECT SEWER LINE TO EXISTING LINE.
13. CONNECT EXISTING WATERLINE IN CENTRAL TO EXISTING WATERLINE IN UNSER
14. STUB OUT DOMESTIC WATER SERVICE AND FIRE SERVICE LINES TO FUTURE PAD SITES.

LEGEND

---	PROPERTY LINE
---	EXISTING EASEMENT
SAS	EXISTING SANITARY SEWER
W	EXISTING WATER LINE
□	EXISTING WATER METER
+	EXISTING CAP
⊕	EXISTING VALVE
⊙	EXISTING FIRE HYDRANT
⊙	EXISTING SANITARY SEWER MANHOLE
⊙	EXISTING STORM DRAIN MANHOLE
□	EXISTING INLET
---	PROPOSED EASEMENT
SAS	PROPOSED SANITARY SEWER LINE
●	PROPOSED SANITARY SEWER MANHOLE
●	PROPOSED CLEANOUT
W	PROPOSED WATER LINE
+	PROPOSED VALVE
FL	PROPOSED FIRE LINE
⊕	PROPOSED HYDRANT
+	PROPOSED CAP
■	PROPOSED WATER METER

**DISIA**  
 Darren Sowell  
 ARCHITECTS  
 4700 Lincoln N.E., Suite 111  
 Albuquerque, N.M. 87109  
 Phone: (505) 342-6200  
 Fax: (505) 342-6201

**Bohannon & Huston**  
 Engineers & Planners  
 1700 Jefferson St. NE, Albuquerque, NM 87108-4300  
 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**  
 Landscape Architects  
 1000 1st St. NE, Suite 112  
 Albuquerque, NM 87110  
 Phone: (505) 839-8955  
 am@mtchellassociates.com

**ARMSTRONG DEVELOPMENT**  
 PROPERTIES, INC.

**Unser Crossing**  
**DRB Submittal**  
**Site Plan for Building Permit**  
**Central Avenue and Unser Boulevard**  
**Albuquerque, New Mexico 87121**

Rev. 1 June 20, 2008

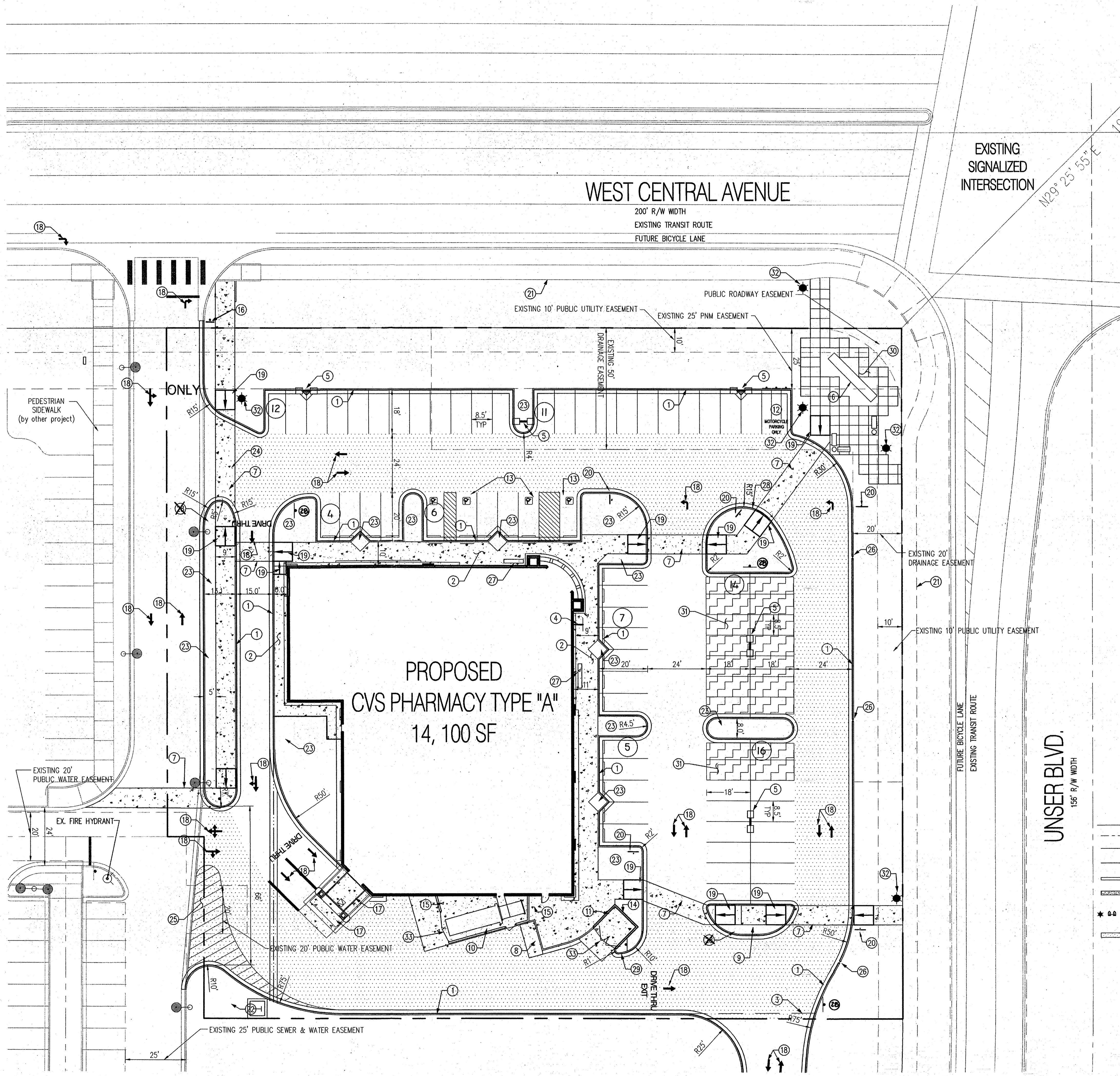
Job Number  
 Drawn By BO  
 Checked RM/BJS  
 Issue Date June 17, 2008

**CONCEPTUAL**  
**UTILITY PLAN**

Scale

**C-201**

∞ OF ∞



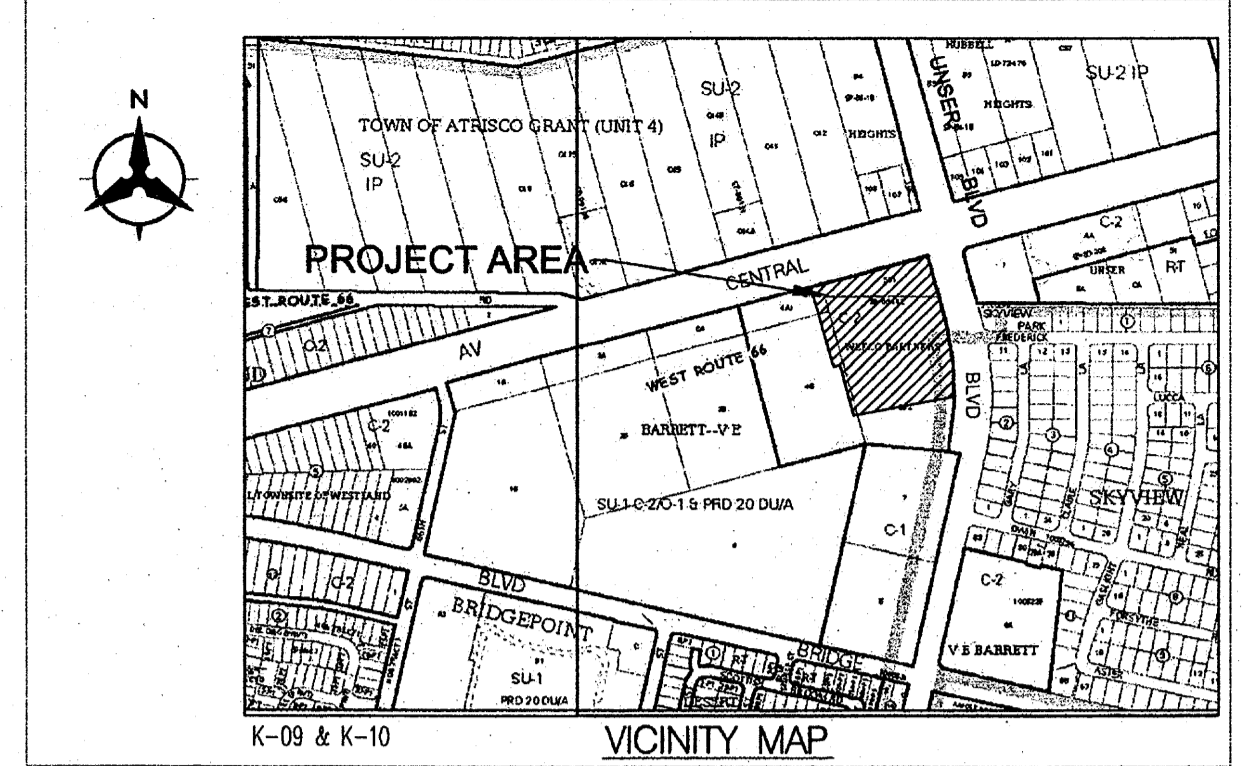
**WEST CENTRAL AVENUE**

200' R/W WIDTH  
EXISTING TRANSIT ROUTE  
FUTURE BICYCLE LANE

EXISTING SIGNALIZED INTERSECTION

UNSER BLVD.  
150' R/W WIDTH

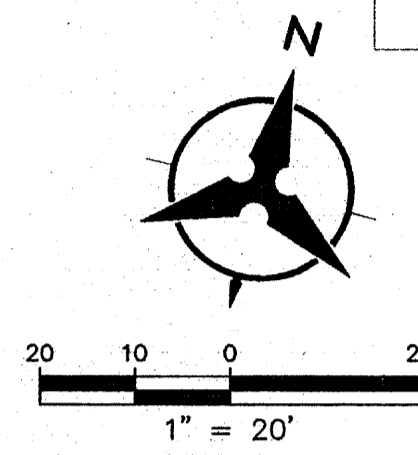
PROPOSED  
CVS PHARMACY TYPE "A"  
14,100 SF



**SHEET INDEX**

C-001	SITE PLAN FOR BUILDING PERMIT	G-101	DESIGN STANDARDS
C-002	SITE PLAN DETAILS	G-102	DESIGN STANDARDS
LS-101	LANDSCAPE PLAN	G-103	DESIGN STANDARDS
C-101	CONCEPTUAL GRADING & DRAINAGE PLAN	S-101	SIGN STANDARDS
C-201	CONCEPTUAL UTILITY PLAN	S-102	SIGN STANDARDS
A-201	BUILDING ELEVATIONS	S-103	SIGN STANDARDS
A-202	BUILDING SIGNAGE LAYOUT		
A-203	TENANT MONUMENTS		
A-501	CHANNEL LETTER DETAILS		

**UNSER CROSSING REFERENCES**  
C-001 SITE PLAN FOR BUILDING PERMIT  
C-003 SITE PLAN FOR SUBDIVISION



**SITE DATA**

LEGAL DESCRIPTION: TRACT 9, UNSER CROSSINGS.  
SITE AREA: 1.9466 ACRES  
ZONING: C-2  
PROPOSED F.A.R. = 0.16

**PARKING CALCS**

REQUIRED = 1 SPACE/200 SF  
14,100 SF  
BASE REQUIRED PARKING = 70.5  
10% TRANSIT REDUCTION, ADJ REQUIRED  
PARKING = 64  
PARKING PROVIDED = 75  
HANDICAP PARKING PROVIDED = 4  
MOTORCYCLE PARKING PROVIDED = 3  
BICYCLE PARKING PROVIDED = 4

**PLAZA AREA**

CORNER PLAZA 1,650 SF

**PERVIOUS PAVEMENT CALCS**

ASPHALT AREA = 37,450 SF  
ASPHALT PERVIOUS AREA = 2,600 SF  
% ASPHALT PERVIOUS = 7.0%  
PLAZA PERVIOUS AREA = 1,220 SF  
TOTAL PERVIOUS AREA = 3,820 SF

**GENERAL NOTES**

- The Site Development Plan for Building Permit and associated drawings were designed in accordance with the approved Design Standards and Sign Standards for Unser Crossing found on Sheets G-101 thru G-103 and S-101 thru S-103. The Unser Crossing Site Plan for Building Permit project number is: 1007204-080RB-70276 EPC. The Unser Crossing Site Plan for Subdivision Project number is 1007204-080RB-70277. The Unser Crossing Amended Site Plan for Subdivision Project number is 1007204-080RB-70407.
- All roof top and ground mounted equipment shall be screened in accordance with Section 3, Item H of the approved Design Standards for Unser Crossing as noted above.
- Site lighting locations are approximate. Exact locations to be defined by photometric study.
- Southwest Mesa Park and Ride and Transfer Facility is located at the Northwest corner of Central Avenue and Unser Blvd.
- There is an existing bike, pedestrian trail located on the East side of Unser Blvd.
- The roofing material will be Firestone UltraPly TPO White (0608-0008) with a minimum solar reflectance of 0.79, thermal emittance rating of 0.85 and 15% post industrial content or the equivalent.

**KEYED NOTES**

- 6" CONCRETE CURB.
- CONCRETE SIDEWALK - WIDTH AS NOTED.
- TEMPORARY ASPHALT CURB.
- BIKE RACK PER 3/C-002.
- LIGHT POLE PER 5/C-002.
- A1 SIGN PER S102.
- COLORLED, TEXTURED, CONCRETE CROSSWALK.
- LOADING PAD.
- END ISLAND PLANTER PER 1/C-002.
- TRASH COMPACTOR.
- REFUSE ENCLOSURE PER 2/C-002 & 9/C-002.
- MOTORCYCLE PARKING WITH SIGN PER 8/C-002.
- HANDICAP PARKING SPACE PER 4/C-002.
- 6" HIGH SCREEN WALL PER 9/C-002.
- 8" HIGH SCREEN WALL BEHIND LOADING DOCKS PER 9/C-002.
- STOP SIGN.
- DRIVEUP WINDOW.
- NEW PAINTED DIRECTIONAL ARROW.
- WHEEL CHAIR RAMP.
- PEDESTRIAN CROSSING SIGN PER 10/C-002.
- 6" CONCRETE SIDEWALK TO BE CONSTRUCTED UNDER COA PROJECT NO. 735684.
- TRANSFORMER PAD FINAL LOCATION TO BE DETERMINED. TRANSFORMER TO BE SCREENED PER DESIGN GUIDELINES.
- LANDSCAPE ISLAND.
- CONCRETE WALK FLUSH WITH ASPHALT THIS AREA.
- PAINTED, STRIPED AREA.
- CURB CUT PER 1/C-101.
- BENCH 8' LONG NORTH SIDE OF BUILDING 10' LONG EAST SIDE OF BUILDING.
- C3 SIGN PER A-202.
- C4 SIGN PER A-202.
- PLAZA AREA w/PERVIOUS PAVEMENT.
- PERVIOUS PAVEMENT.
- PEDESTRIAN SCALE LIGHTING PER 11/C-002.
- ENCLOSURE GATE PER 6/C-002.

**LEGEND**

- BOUNDARY
- EXISTING EDGE OF ROAD
- CONCRETE CURB
- SCREEN WALL
- SIDEWALK
- TYPICAL POLE MOUNTED LIGHT PER 5/C002
- HEAVY DUTY PAVEMENT

**PROJECT NUMBER:**

Application Number:

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated \_\_\_\_\_ and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? ( ) Yes ( ) No If yes, then a set of approved DRC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

**DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:**

Traffic Engineering, Transportation Division	Date _____
Water Utility Department	Date _____
Parks and Recreation Department	Date _____
City Engineer <i>Michael Holten</i> <i>adjustment</i>	Date <i>12/4/08</i>
Solid Waste Management	Date _____
DRB Chairperson, Planning Department	Date _____

**DISIA**  
Darren Sowell  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

**Bohannon & Huston**  
Consulting 7800 Johnson Bl. NE Albuquerque, NM 87109-6200  
ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**  
Landscape Architects  
Catala Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 838-6006  
mitchell@bohannonhuston.com

**ARMSTRONG DEVELOPMENT PROPERTIES, INC.**

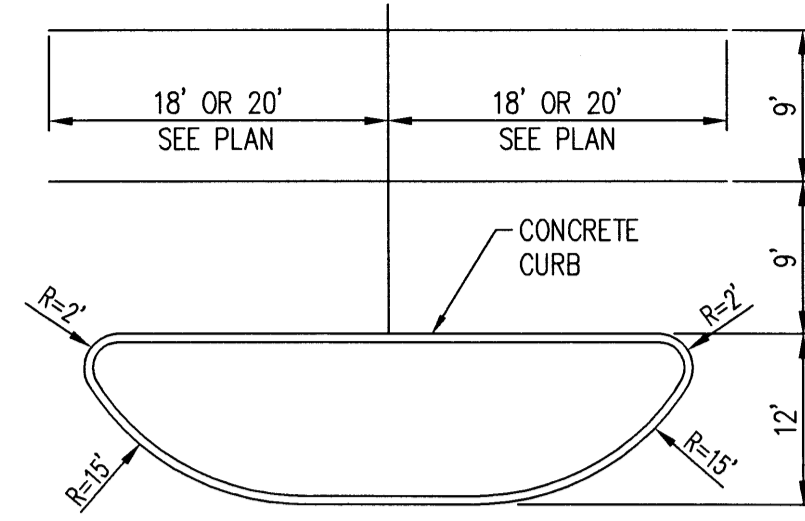
**CVS PHARMACY**  
Site Plan  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121



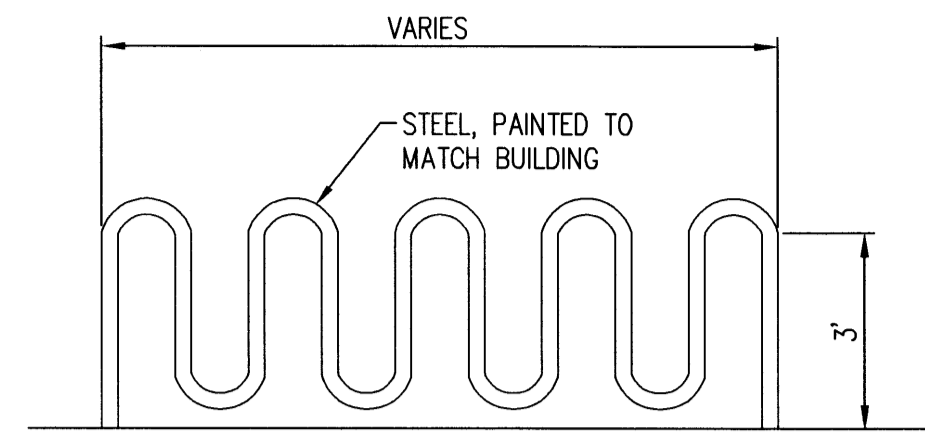
Job Number \_\_\_\_\_  
Drawn By \_\_\_\_\_ MRP  
Checked \_\_\_\_\_ SJS  
Issue Date OCT 31, 2008

**SITE PLAN FOR BUILDING PERMIT**

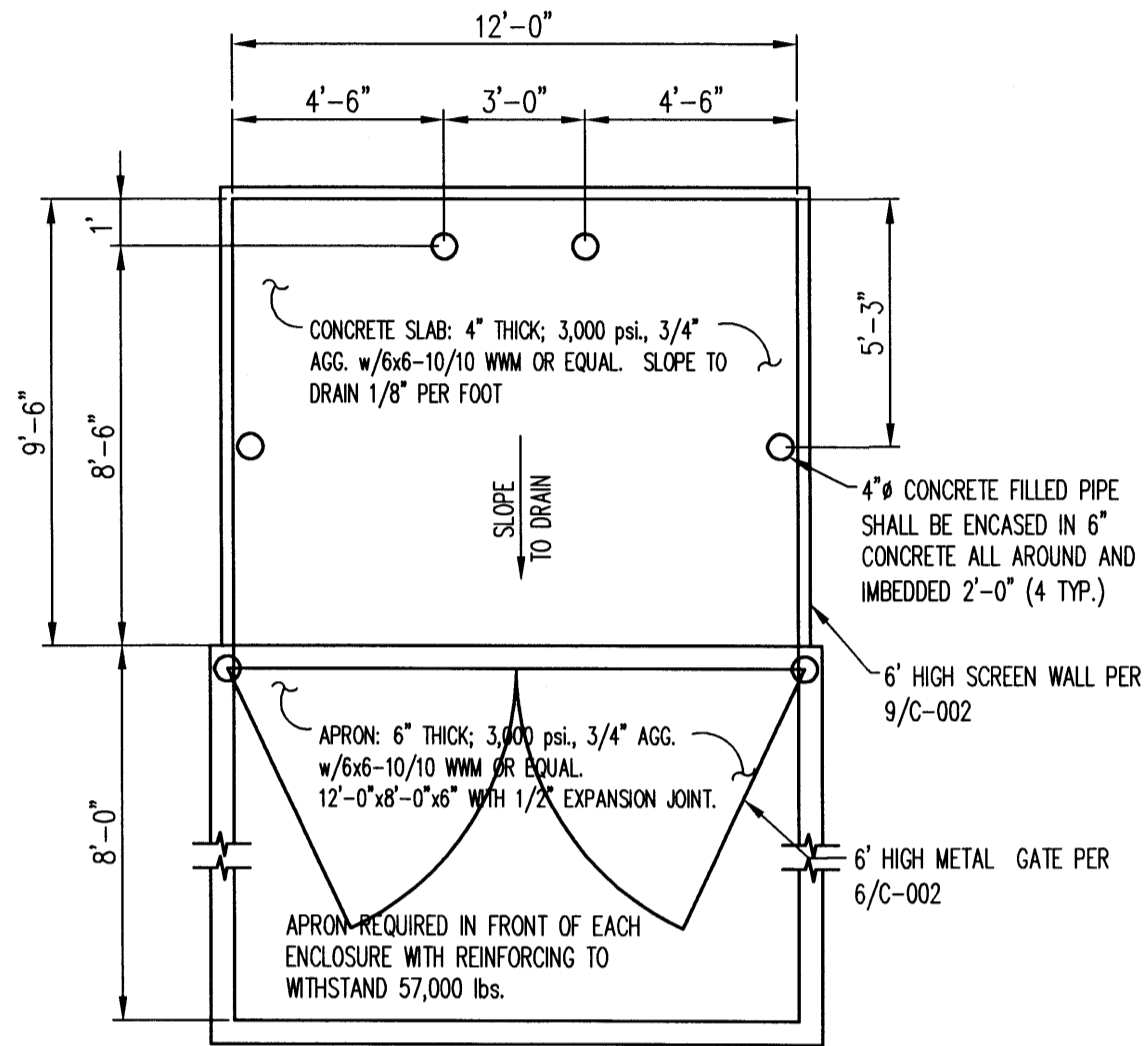
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**C-001**  
00 OF 00



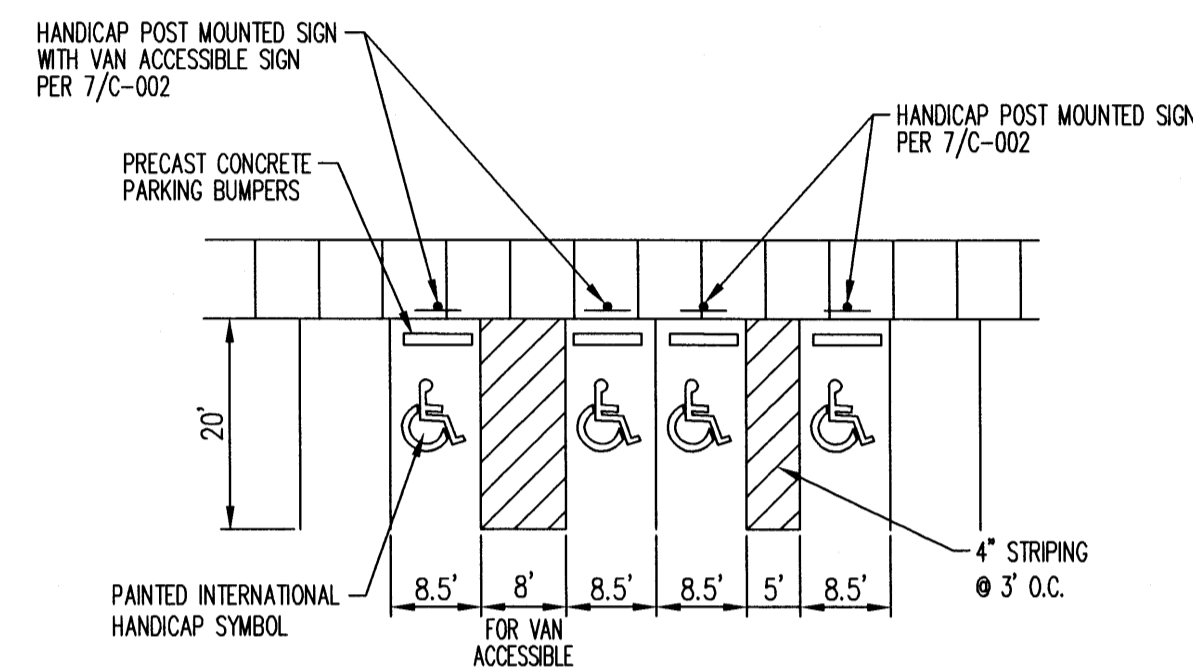
1 TYPICAL END PLANTER ISLAND & PARKING LOT STRIPING  
NTS



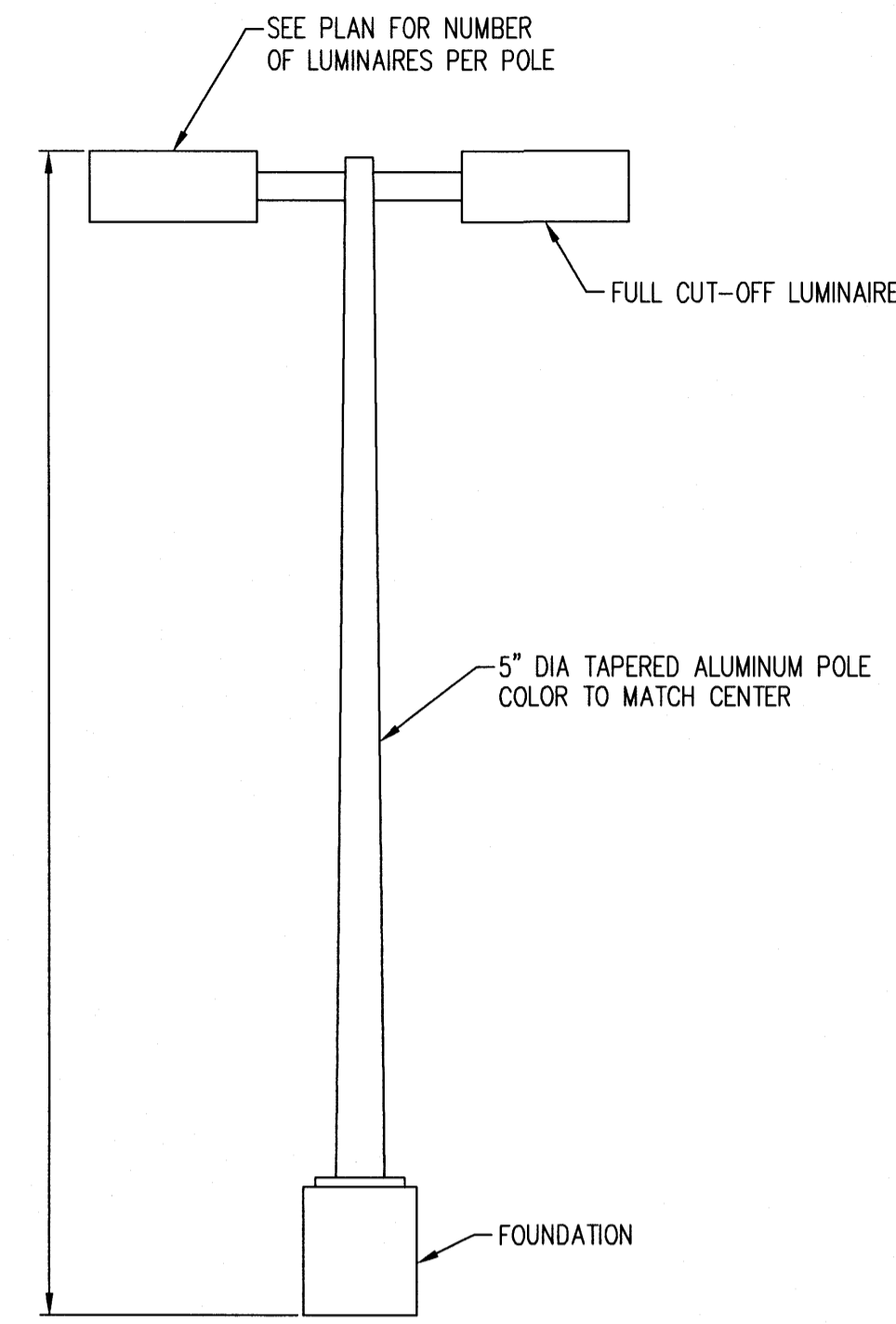
3 BIKE RACK  
NTS



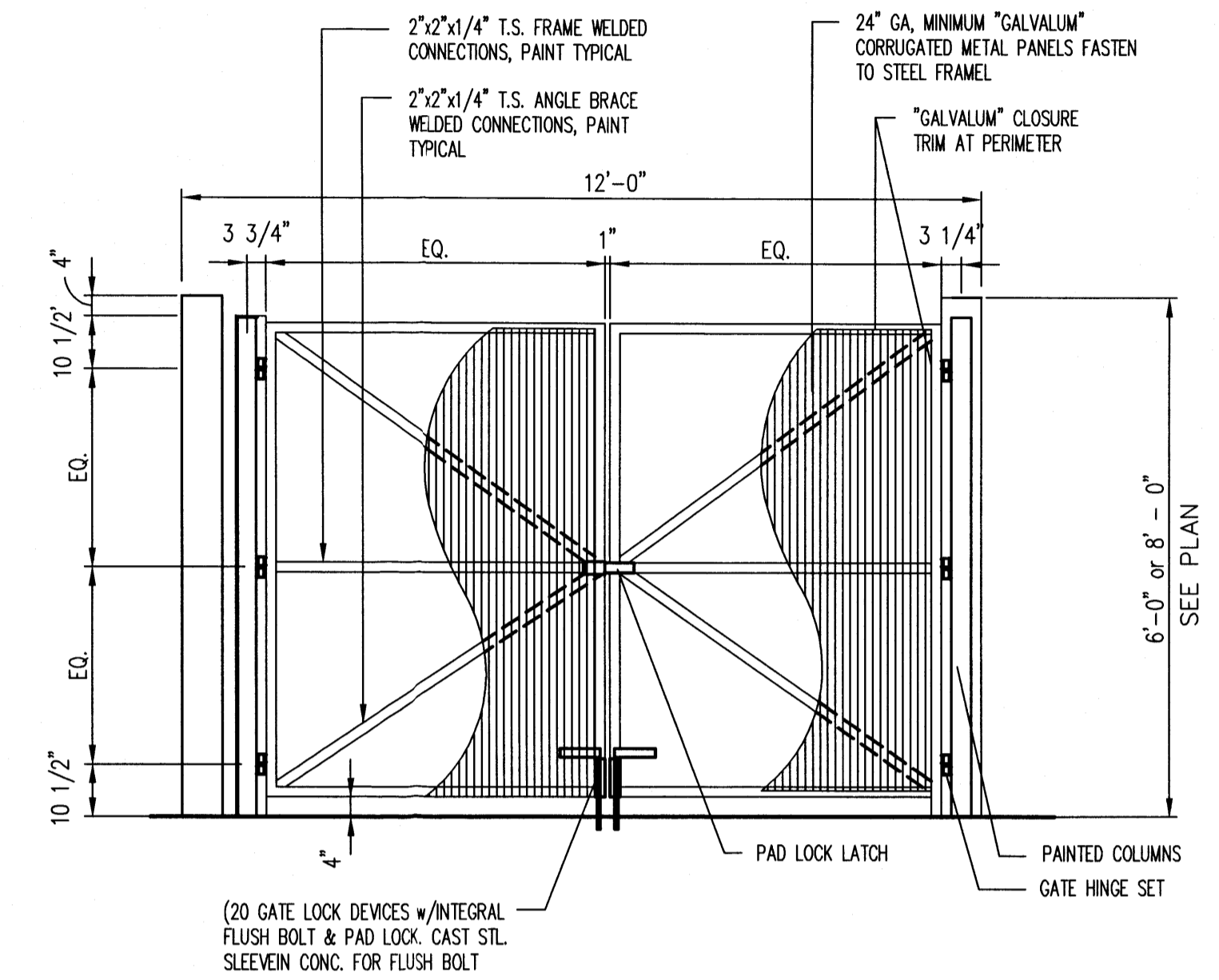
2 REFUSE ENCLOSURE  
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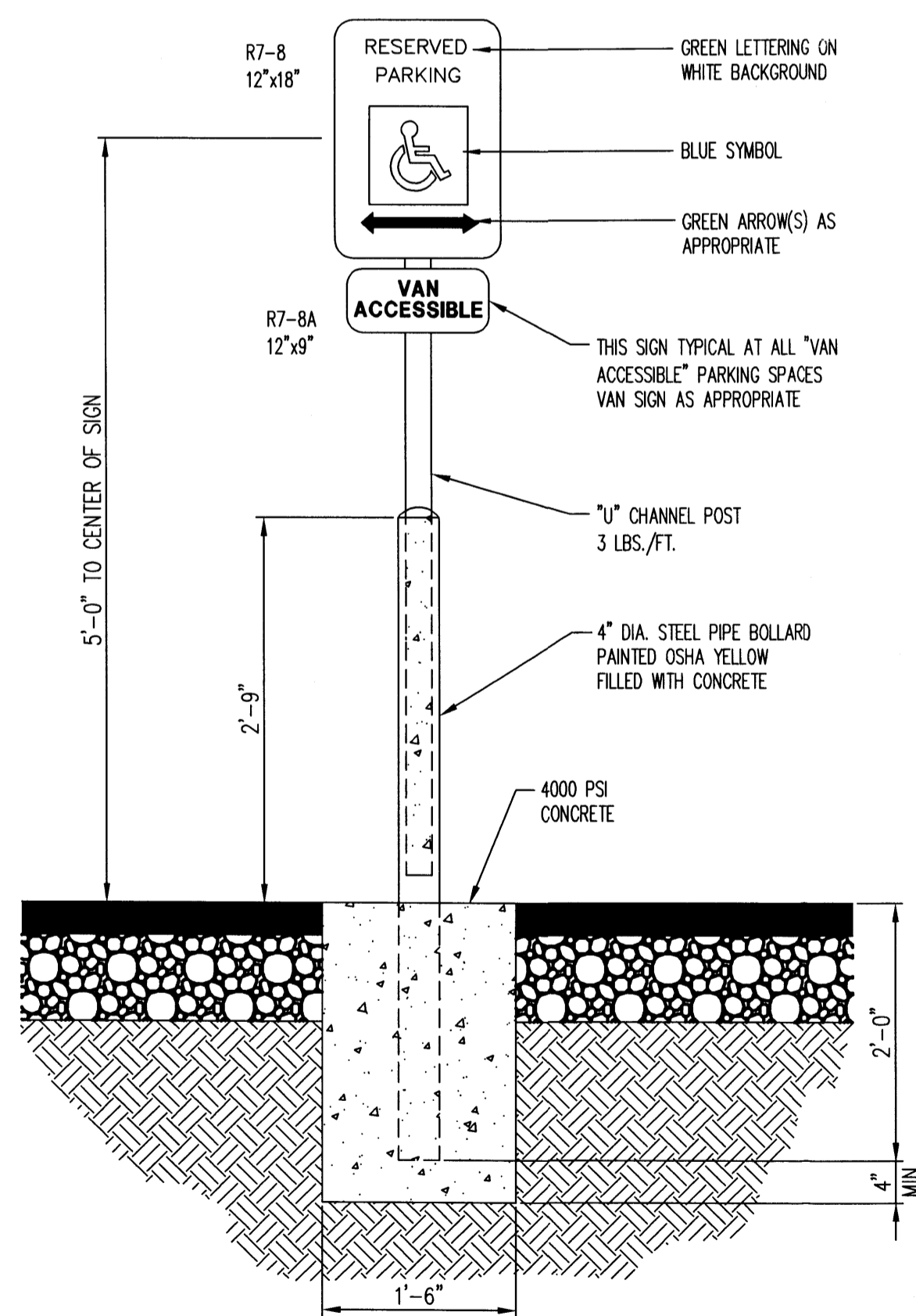
4 TYPICAL HANDICAP PARKING DETAIL  
NTS



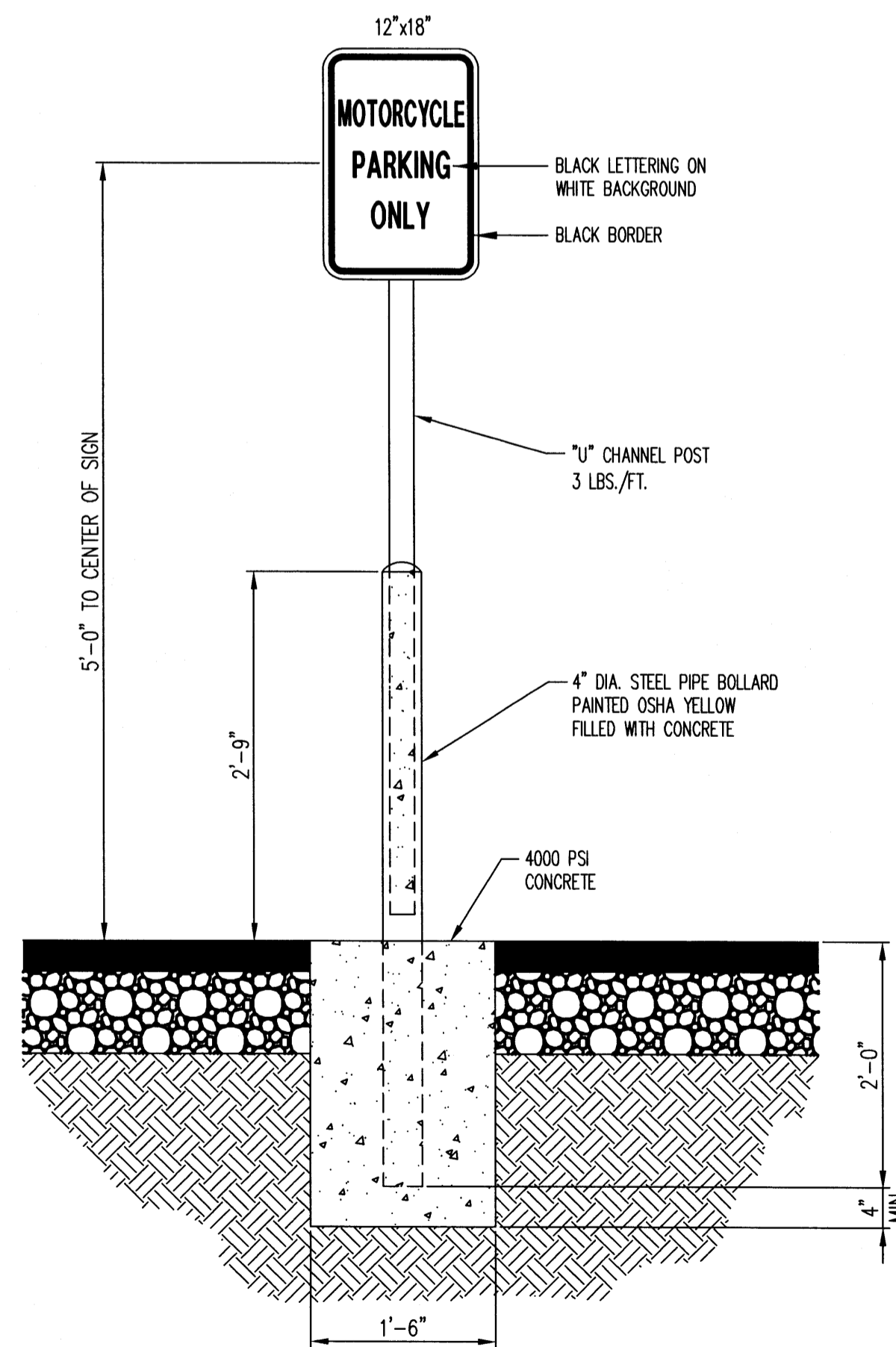
5 TYPICAL POLE MOUNTED LIGHT  
NTS



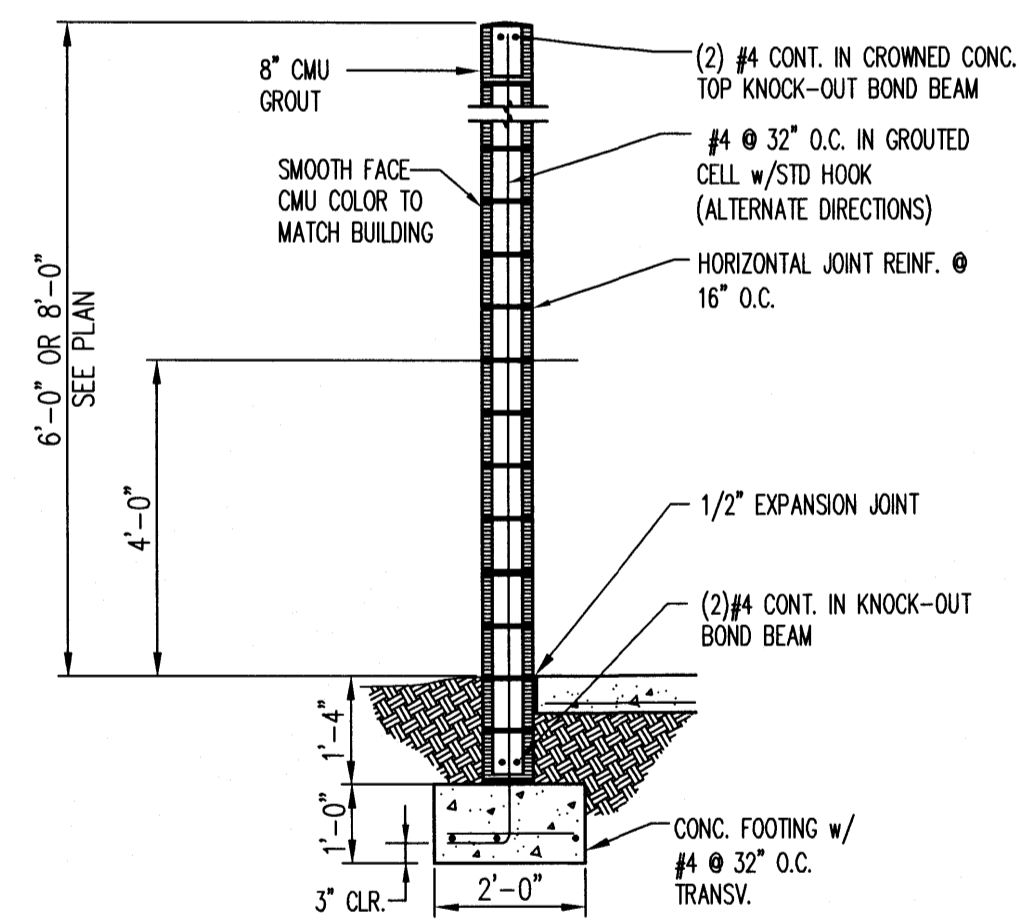
6 GATE DETAIL  
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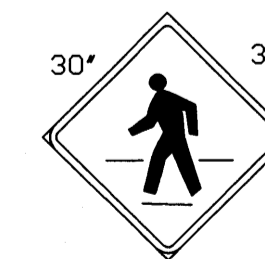
7 HANDICAP PARKING SIGN MOUNTING DETAIL  
NTS



8 MOTORCYCLE PARKING SIGN  
NTS

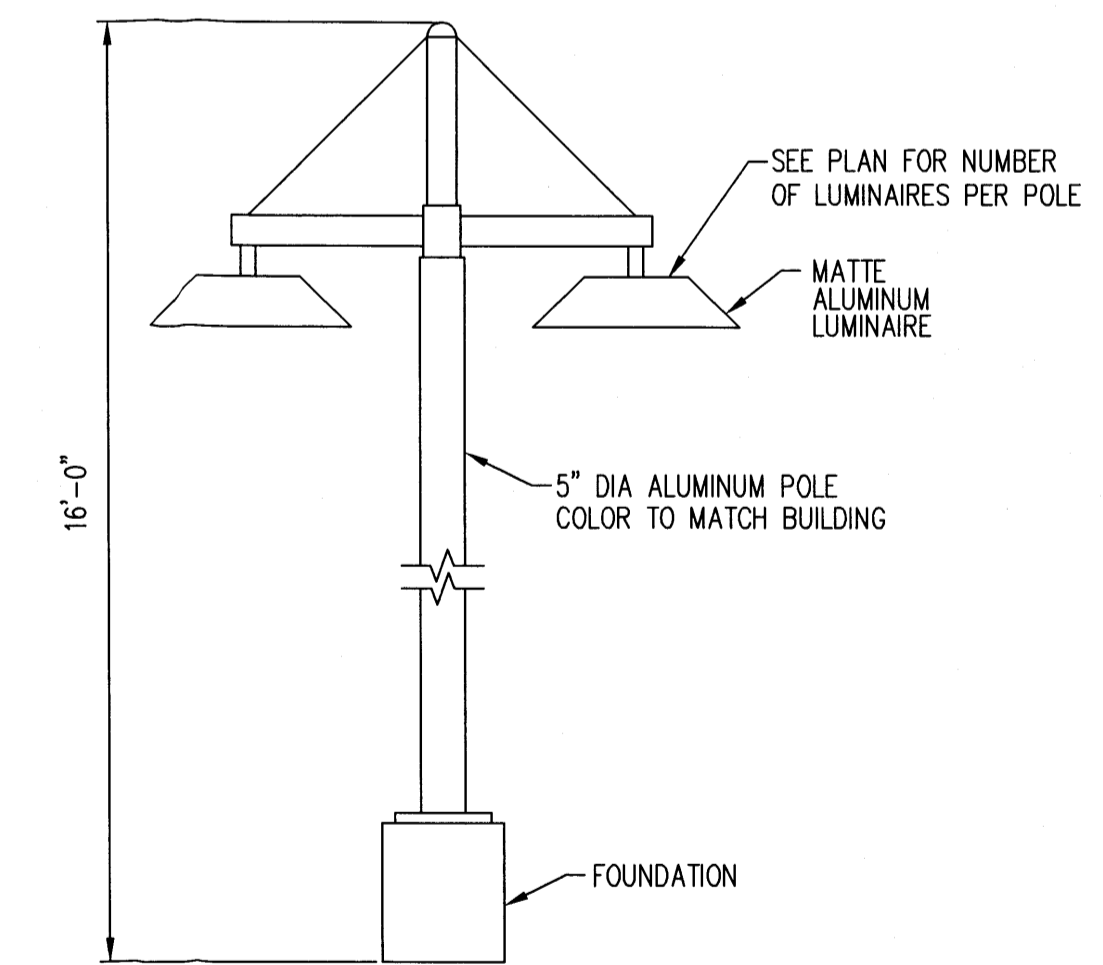


9 CMU SCREEN WALL  
NTS



W11A-2-30  
B/Y, RB

10 PEDESTRIAN CROSSING SIGN  
NTS



NOTE: THIS FIXTURE TO BE USED IN ALL PEDESTRIAN/PLAZA AREAS

11 DECORATIVE POLE MOUNTED LIGHT  
NTS



**Darren Sowell**  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201



**Mitchell Associates, LLC**  
Landscape Architects  
Caribe Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 636-6066  
dmy@mlsassoc.com



**CVS PHARMACY**  
Site Plan  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121



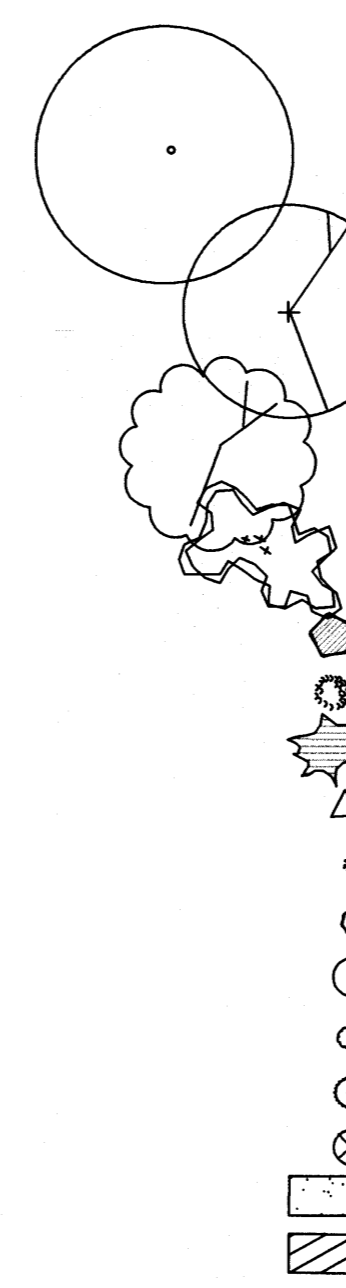
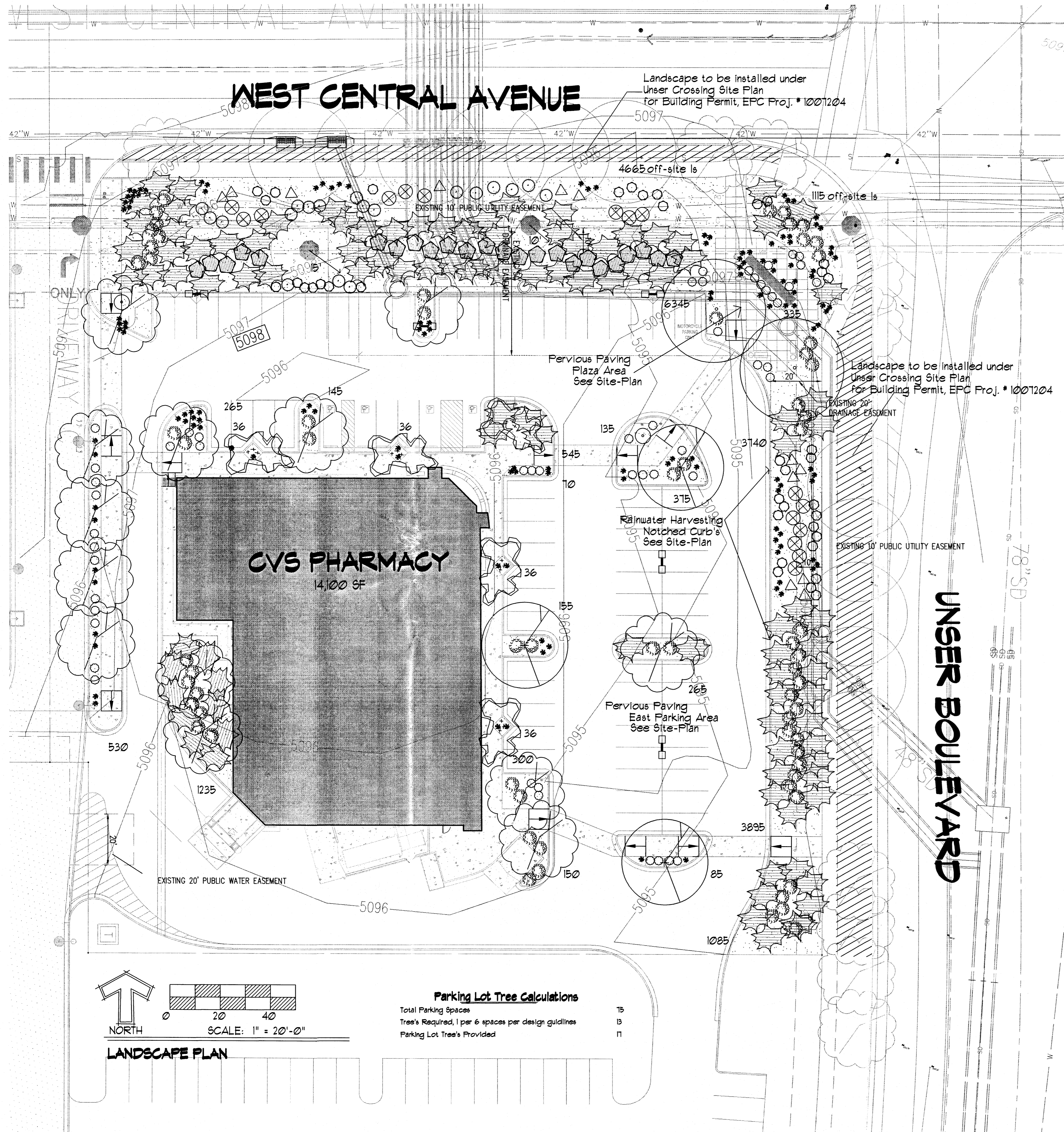
Job Number \_\_\_\_\_  
Drawn By \_\_\_\_\_ MRP  
Checked \_\_\_\_\_ SJS  
Issue Date DEC 4, 2008

SITE PLAN  
DETAIL SHEET

Scale \_\_\_\_\_

C-002

00 OF 00



### LANDSCAPE LEGEND

QTY	SIZE	*D/E	COMMON / BOTANICAL	AV. SP.	AV. HT.	H2O USE
2	2" cal	D	Honey Locust <i>Gleditsia triacanthos</i>	40-45'	40-50'	M
3	2" cal	D	Ash <i>Fraxinus pennsylvanica</i> 'Patmore', 'Summit', or 'Marshall'	35-40'	60'	M*
13	2" Cal.	D	Chitalpa <i>Chitalpa x tashkenensis</i>	30'	30'	M
5	15 Gal.	D	Washington Hawthorne <i>Crataegus phaenopyrum</i>	25'	M	
24	5 Gal.	E	Wintergreen Barbery <i>Barberis juliflora</i>	5'	5'	M
68	5 Gal.	E	India Hawthorne <i>Raphiolepis indica</i>	4'	4'	M
104	5 Gal.	E	Honeysuckle <i>Lonicera japonica</i>	12'	2'	M
12	5 Gal.	D	Dwarf Butterfly Bush <i>Buddleia davidii nanhoensis</i>	5'	5'	M
138	5 Gal.	D	Dwarf Feathertop <i>Pennisetum villosum</i>	2'	2'	M
36	5 Gal.	E	Desert Sage <i>Salvia dorrii</i>	3'	3'	M
2	5 Gal.	E	Rosemary <i>Rosemary officinalis</i>	6'	6'	L*
52	5 Gal.	E	English Lavender <i>Lavandula angustifolia</i>	3'	3'	M
19	5 Gal.	E	Apache Plume <i>Fallugia paradoxa</i>	5'	5'	L
16	5 Gal.	D	Fernbush <i>Chamaebatiaria millefolium</i>	5'	5'	L

3/4" Santa Ana Tan Gravel over Commercial Grade Weaved Fabric, or Organic Mulch  
Landscape by other's, Unser Crossing Common Area

High water use turf is not used on this project.  
Plant List contains only approved species for Unser Crossing, Site Development Plan  
\* Indicates Deciduous or Evergreen

### LANDSCAPE CALCULATIONS

Total Lot Area	84,600
TOTAL BUILDING AREA (sf)	14,100
TOTAL OFF-SITE LANDSCAPE PROVIDED (sf)	5,100
NET LOT AREA (sf)	64,800
LANDSCAPE REQUIREMENT (%)	.15
TOTAL LANDSCAPE REQUIRED (sf)	9,720
TOTAL ON-SITE LANDSCAPE PROVIDED (sf)	19,860
TOTAL OFF-SITE LANDSCAPE PROVIDED (sf)	5,100
TOTAL LANDSCAPE PROVIDED (sf)	25,560
TOTAL VEGETATIVE COVER REQUIRED, 80%	20,448
TOTAL VEGETATIVE COVER PROVIDED, 810%	20,730

### LANDSCAPE NOTES

Owner is responsible for maintenance of landscape and irrigation system.

Landscape shall be watered by a complete underground irrigation system operated by automatic timer. Irrigation system shall provide for water requirements of plant material at maturity. Trees shall have bubblers and shrubs shall have a minimum of 2 emitters per shrub. Trees and shrubs shall be zoned separately. Point of Connection for irrigation system shall be coordinated between the Landscape Contractor and the General Contractor of the project prior to construction.

All landscape areas shall be covered in 3" of landscape gravel, fines, bark mulch or other landscape mulch and commercial grade weaved barrier fabric approved by the Owner's representative.

It is the intent of this plan to comply with the City of Albuquerque Landscape Regulations Applicable to Apartments and Nonresidential Development.

Landscape shall be installed according to the approved plan.

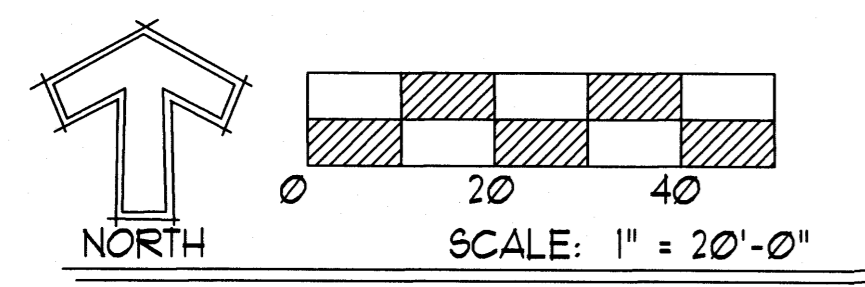
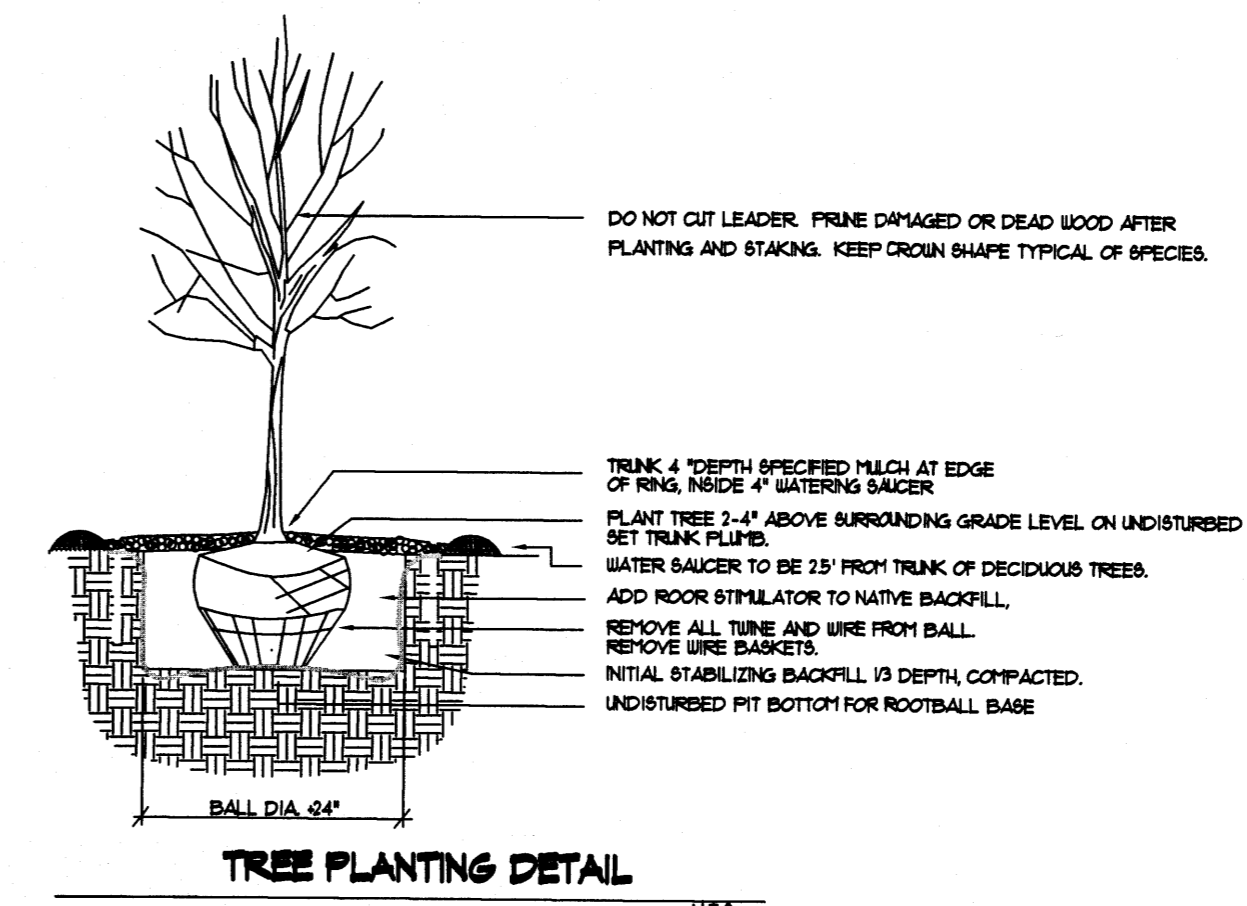
Landscape areas in excess of 36 square feet shall be planted so as to achieve 80% Live Ground Cover at maturity.

Approval of this plan does not constitute or imply exemption from water waste provisions of the water conservation landscaping and water waste ordinance. Water management is the sole responsibility of the property owner.

This plan shall comply with the Street Tree requirement's as defined in the Street Tree Ordinance 6-2-2-1.

The landscape is designed in accordance with Section 3, Item E of the approved Design Standards for Unser Crossing found on sheet G-101. The Unser Crossing project number is 10071204. 06DRB-102711 EPC.

The hardscape materials for this project will be consistent with those used for the Unser Crossing Development.



### Parking Lot Tree Calculations

Total Parking Spaces	15
Trees Required, 1 per 6 spaces per design guidelines	3
Parking Lot Trees Provided	11

**DISA**  
Darren Sowell  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

**Bohannon & Huston**  
Landscape Architects  
Caribe Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 834-9095  
carie@bohannonhuston.com

**ARMSTRONG DEVELOPMENT PROPERTIES, INC.**

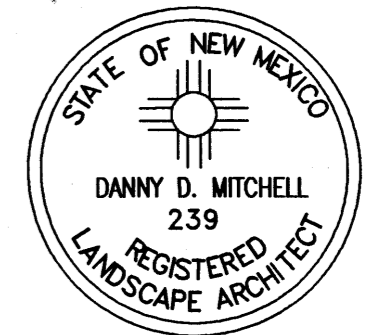
**CVS PHARMACY**  
Site Plan for Building Permit  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121

**CVS/**  
pharmacy

Revisions  
EPC Comment's, 9/30/2008  
Plaza Area, 10/31/2008

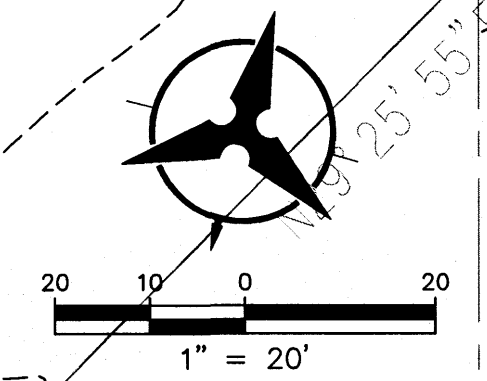
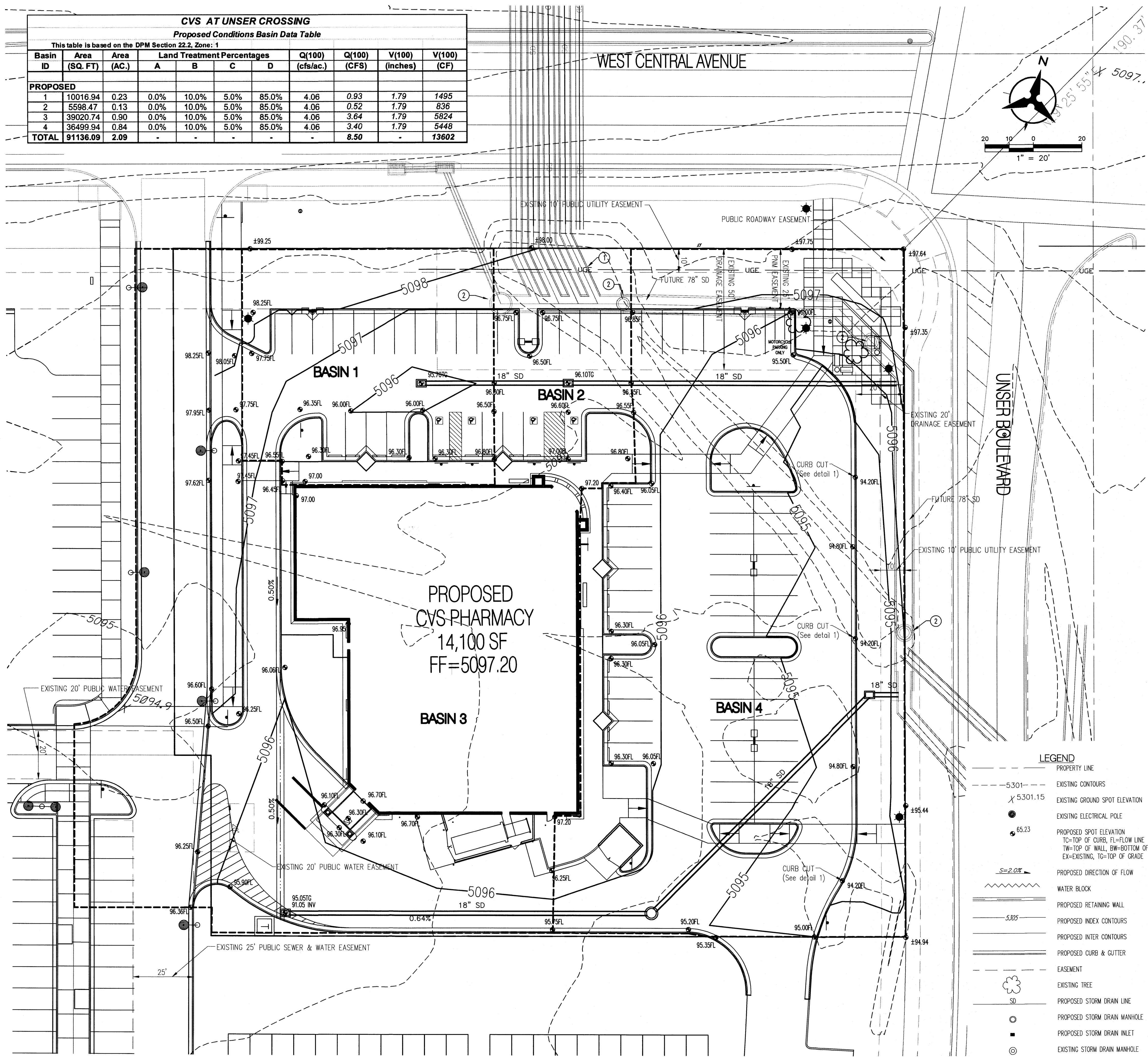
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Checked: SJS  
Issue Date: SEPT 2, 2008

DRB SUBMITTAL  
12/02/2008



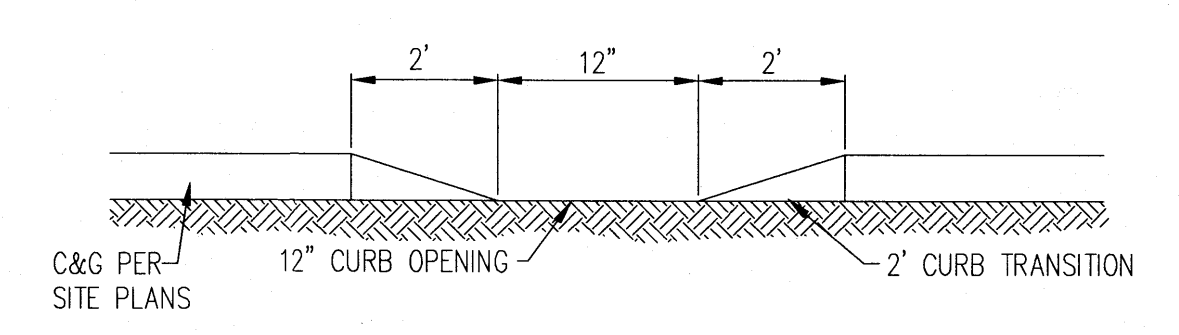
**L-100**  
OF

CVS AT UNSER CROSSING										
Proposed Conditions Basin Data Table										
This table is based on the DPM Section 22.2, Zone: 1										
Basin ID	Area (SQ. FT)	Area (AC.)	Land Treatment Percentages				Q(100) (cfs/ac.)	Q(100) (CFS)	V(100) (inches)	V(100) (CF)
			A	B	C	D				
<b>PROPOSED</b>										
1	10016.94	0.23	0.0%	10.0%	5.0%	85.0%	4.06	0.93	1.79	1495
2	5598.47	0.13	0.0%	10.0%	5.0%	85.0%	4.06	0.52	1.79	836
3	39020.74	0.90	0.0%	10.0%	5.0%	85.0%	4.06	3.64	1.79	5824
4	36499.94	0.84	0.0%	10.0%	5.0%	85.0%	4.06	3.40	1.79	5448
<b>TOTAL</b>	<b>91136.09</b>	<b>2.09</b>	-	-	-	-	-	<b>8.50</b>	-	<b>13602</b>

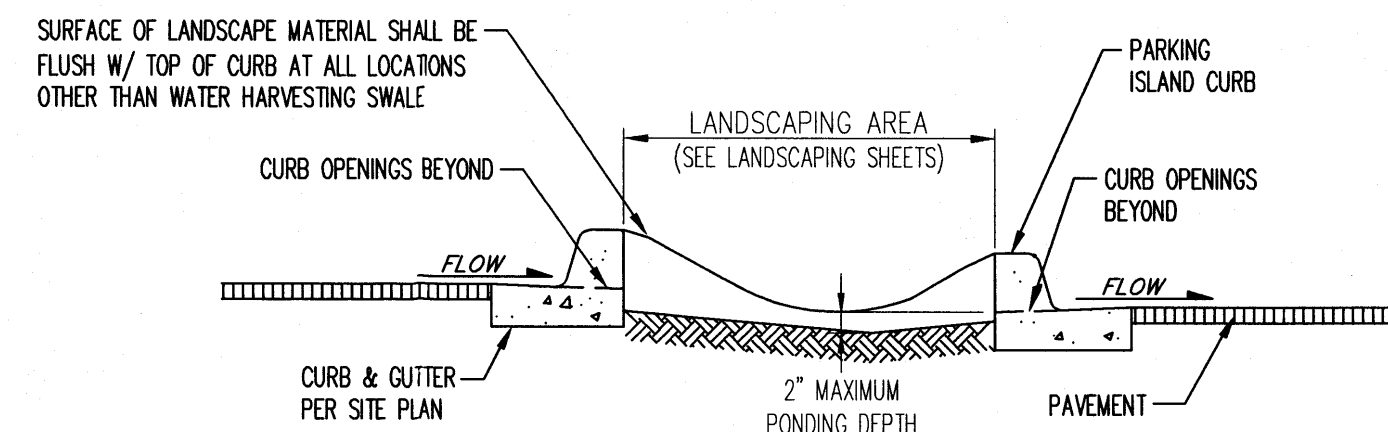


**KEYED NOTES**

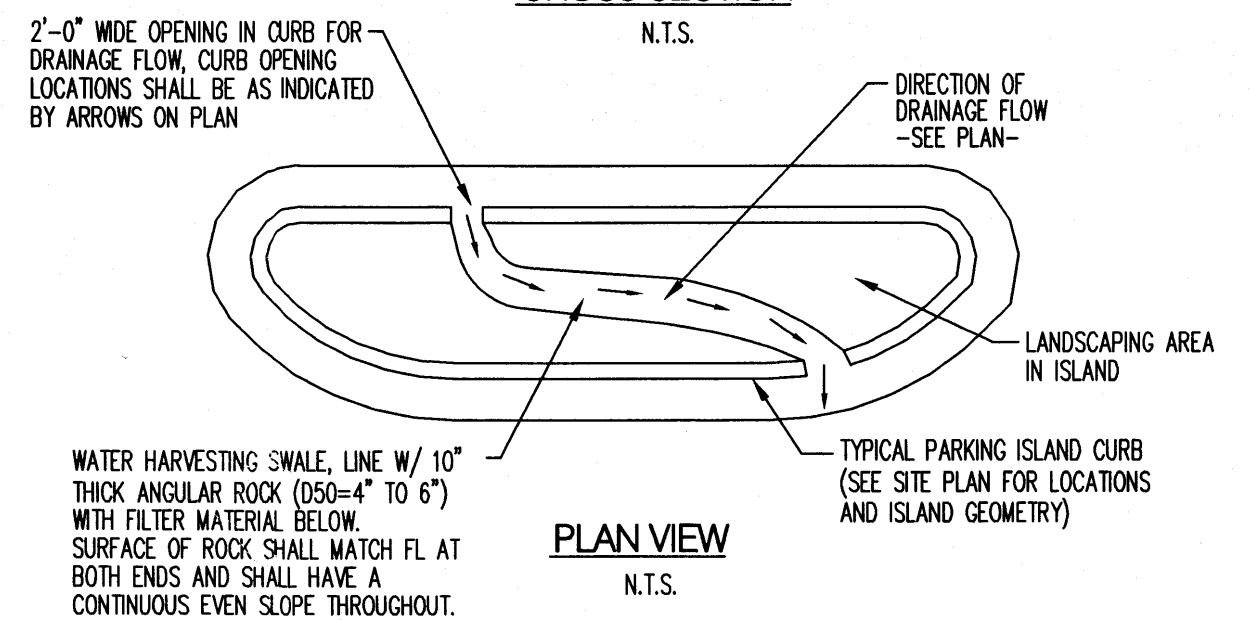
1. STORM DRAIN MANHOLE.
2. STORM DRAIN MANHOLE.



**1 TYPICAL CURB CUT DETAIL**  
N.T.S.



**CROSS SECTION**  
N.T.S.



**PLAN VIEW**  
N.T.S.

**8 TYPICAL WATER HARVESTING DETAIL**  
N.T.S.

**GRADING AND DRAINAGE NARRATIVE**

**I. INTRODUCTION**  
The purpose of this submittal is to present a grading and drainage plan for the proposed CVS Pharmacy development in SW Albuquerque. The site is located at the northeast corner of the Unser Crossing project at the intersection of Central Avenue and Unser Boulevard. The project will include a single retail building with pedestrian access, parking, and landscaping on approximately 1.9 acres. This Grading and Drainage Plan is submitted for DRB approval. Detailed hydrologic and hydraulic calculations will be submitted at the time of Building Permit submittal.

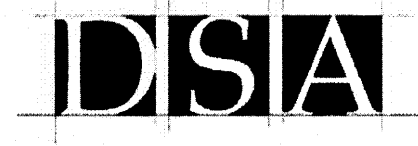
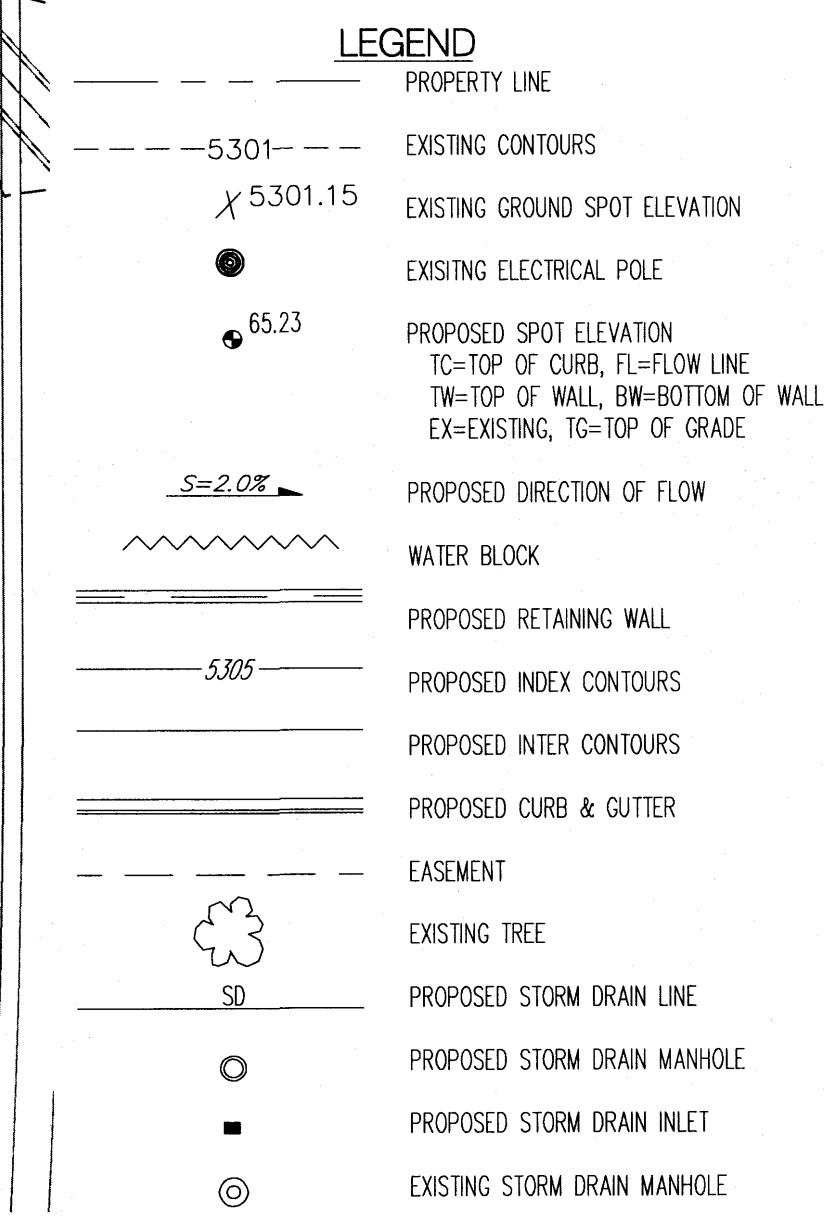
**II. SITE LOCATION**  
The site is located within zone atlas map K-10-Z. The project is located on Tract 9 of Unser Crossing and is bounded along the north by Central Ave., along the east by Unser Blvd., and along the south and west by the Unser Crossing development.

**III. EXISTING HYDROLOGIC CONDITIONS**  
The site encompasses approximately 1.9 acres and is undeveloped. Slopes for the site range between 1 to 3% to the east and there is sparse vegetation cover. Currently, the majority of the site drains via surface flow into inlets located in Unser Blvd. These flows discharge through storm drain to a concrete lined channel near the southeast corner of Bridge and Unser.

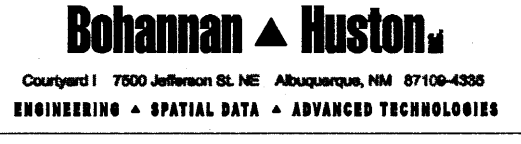
**IV. OFFSITE DRAINAGE**  
There are currently five culverts that divert flow across Central Ave. to the northeast corner of the site. In existing conditions, the culvert flow travels across the site via a natural channel into an end section of the existing storm drain system along Unser Blvd. In developed conditions, the five culverts will be directly connected to the storm drain system in Unser with a junction box and storm drain pipe. There is no additional offsite flow that reaches the site.

**V. PROPOSED HYDROLOGIC CONDITIONS**  
The proposed site drains over surface and through on-site storm drains to the Unser Boulevard storm drain per the Unser Crossing Drainage Management Plan. Basins 1 and 2 drain to storm drain inlets in the drive aisle north of the building. The flow in this storm drain is 1.5 cfs. Basin 3 (0=3.6 cfs) drains to a storm drain inlet in the drive aisle to the south of the building. Basin 4 (0=3.4 cfs) drains to the 20 foot landscape area adjacent to Unser Boulevard through curb cuts on the east side of the drive aisle. The intent of this is to water harvest a portion of the site runoff. A storm drain basin in the landscape area will serve as the emergency outfall for runoff in Basin 4.

**V. CONCLUSION**  
The grading and drainage plan for the CVS site is consistent with the approved Unser Crossing Drainage Management Plan. The plan is capable of safely passing the 100 year storm and meets city requirements. All analysis was completed in accordance with section 22.2 of the Development Process Manual.



**Darren Sowell**  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201



**Mitchell Associates, LLC**  
Landscape Architects  
Cofe Rd. W. Suite 112  
Albuquerque, NM 87110  
Phone: (505) 838-6956  
darm@mittellassociates.com



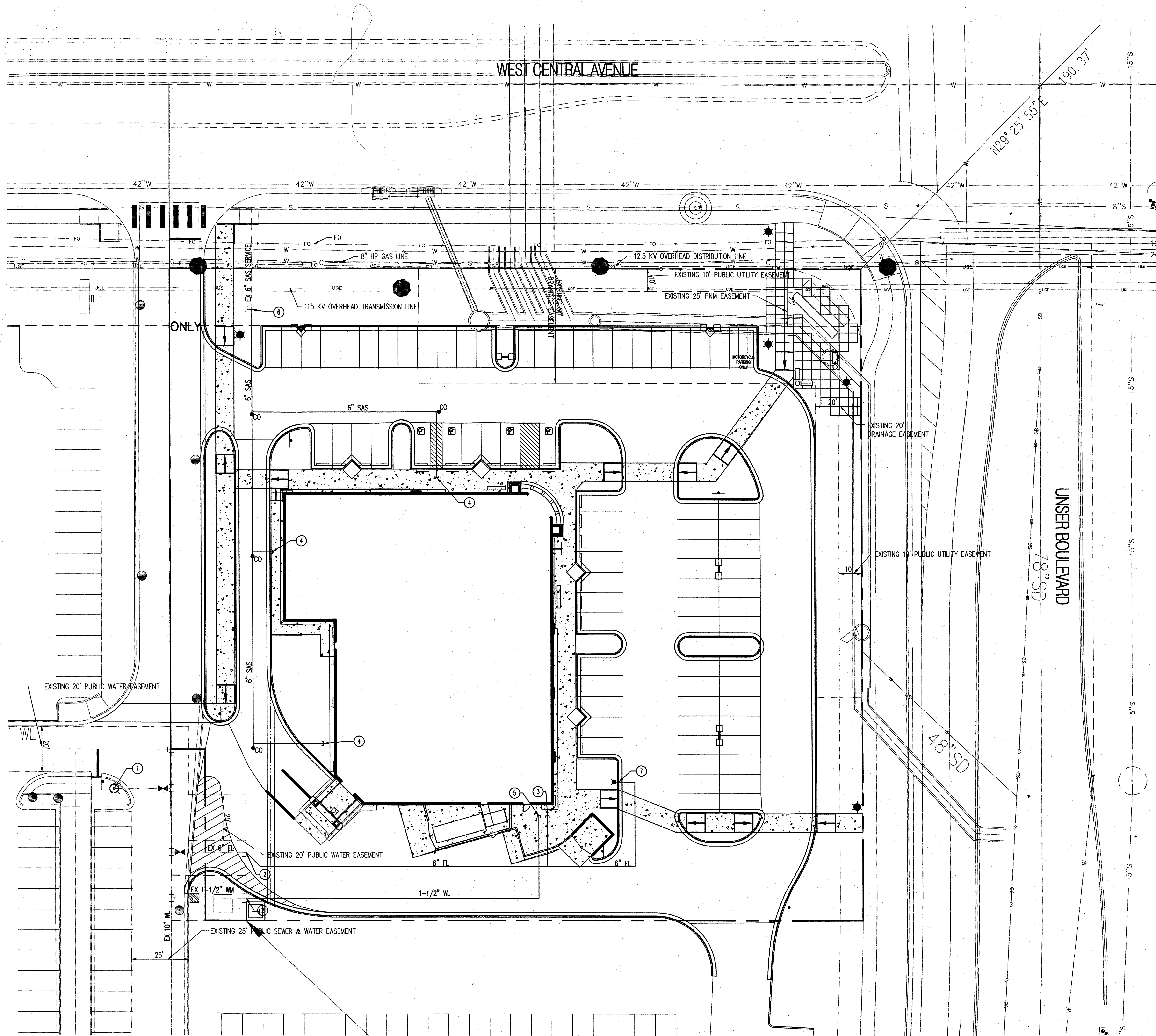
**CVS PHARMACY**  
Site Plan  
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Job Number	
Drawn By	MRP
Checked	SJS
Issue Date	DEC 4, 2008

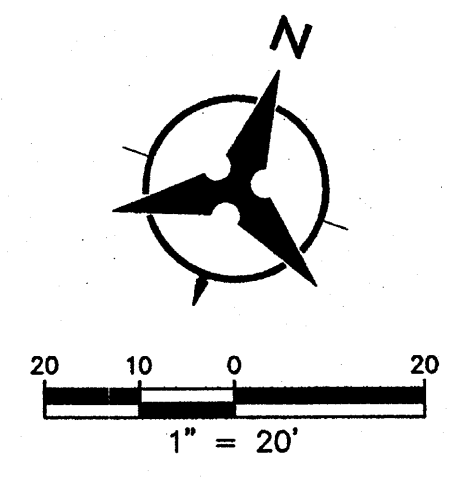
**GRADING AND DRAINAGE PLAN**  
Scale

**C-101**



**KEYED NOTES**

1. EXISTING FIRE HYDRANT
2. CONNECT WATER/FIRE SERVICE TO EXISTING WATERLINE.
3. CONNECT FIRE LINE TO WITHIN 5' OF BUILDING.
4. CONNECT SAS LINE TO WITHIN 5' OF BUILDING.
5. CONNECT WATER LINE TO WITHIN 5' OF BUILDING.
6. CONNECT SEWER LINE TO EXISTING LINE
7. PROPOSED FIRE HYDRANT.



**LEGEND**

---	PROPERTY LINE
---	EXISTING EASEMENT
SAS	EXISTING SANITARY SEWER
W	EXISTING WATER LINE
□	EXISTING WATER METER
○	EXISTING CAP
+	EXISTING VALVE
⊗	EXISTING FIRE HYDRANT
○	EXISTING SANITARY SEWER MANHOLE
□	EXISTING INLET
---	EXISTING EASEMENT
SAS	PROPOSED SANITARY SEWER LINE
●	PROPOSED SANITARY SEWER MANHOLE
⊗	PROPOSED CLEANOUT
W	PROPOSED WATER LINE
+	PROPOSED VALVE
FL	PROPOSED FIRE LINE
○	PROPOSED CAP
■	PROPOSED WATER METER

**DSA**  
**Darren Sowell**  
 ARCHITECTS  
 4700 Lincoln N.E., Suite 111  
 Albuquerque, N.M. 87109  
 Phone: (505) 342-6200  
 Fax: (505) 342-6201

**Bohannon & Huston**  
 Consulting Engineers & Architects  
 1900 Johnson St. NE  
 Albuquerque, NM 87109-0202  
 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**  
 Landscape Architects  
 3000 Blvd. NE, Suite 112  
 Albuquerque, NM 87110  
 Phone: (505) 838-6996  
 dmj@mitassoc.com

**ARMSTRONG DEVELOPMENT**  
 PROPERTIES, INC.

**CVS PHARMACY**  
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 Albuquerque, New Mexico 87121



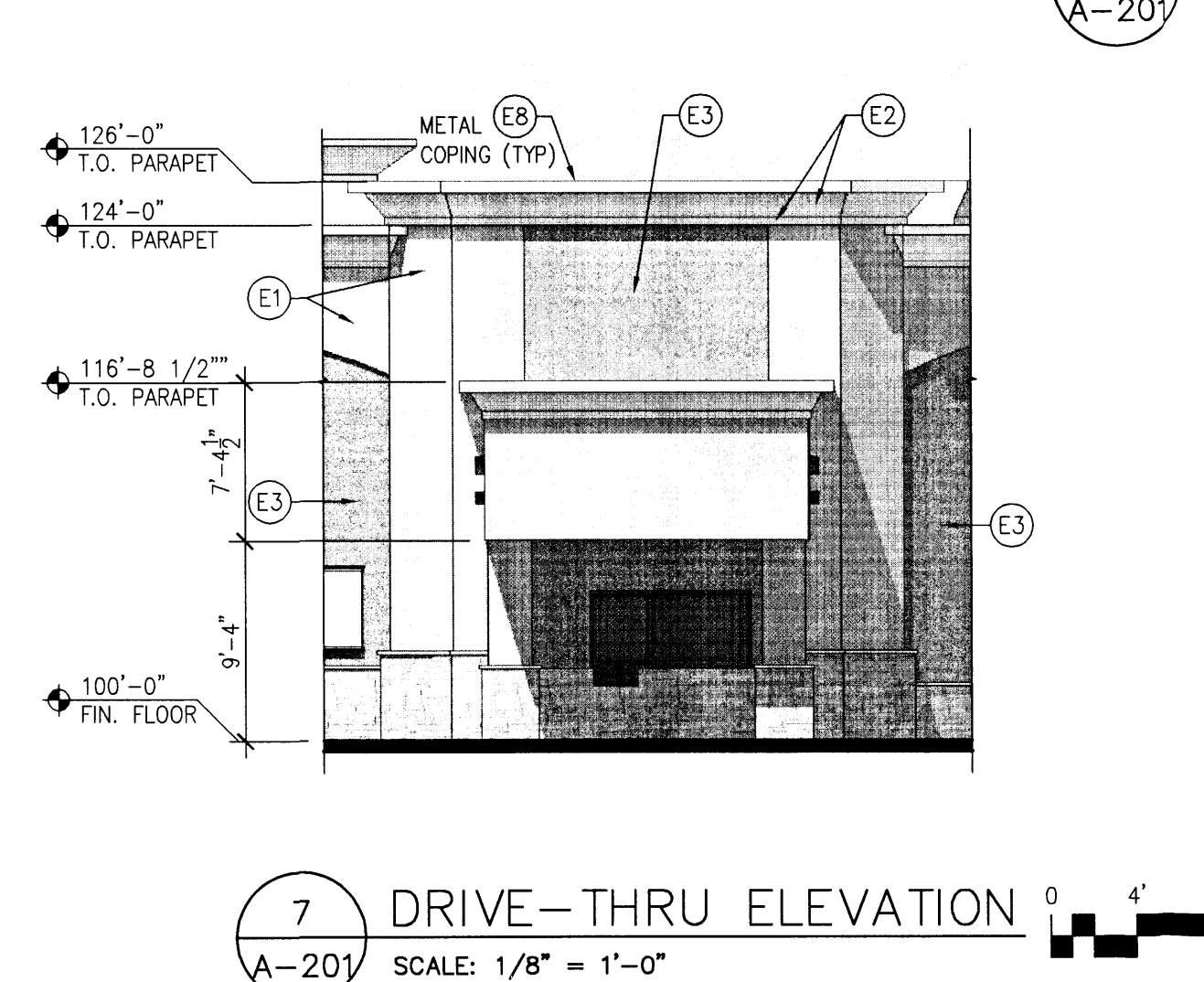
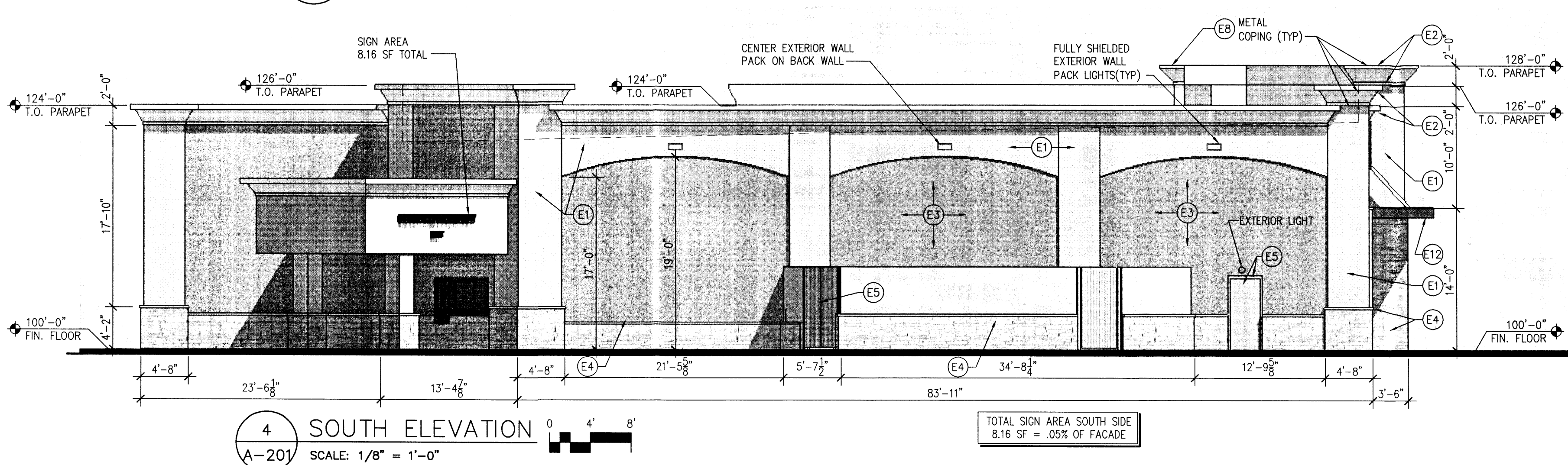
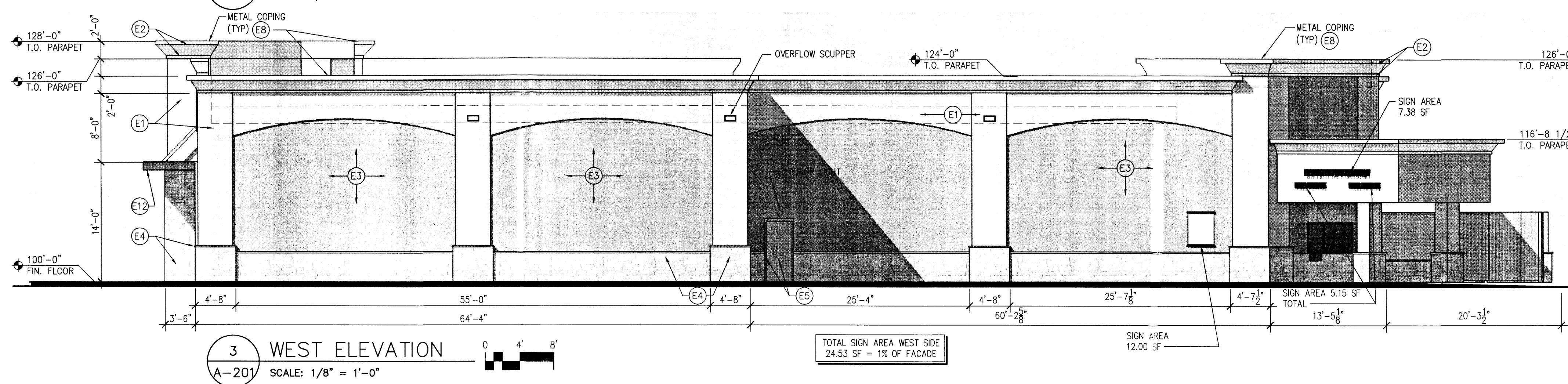
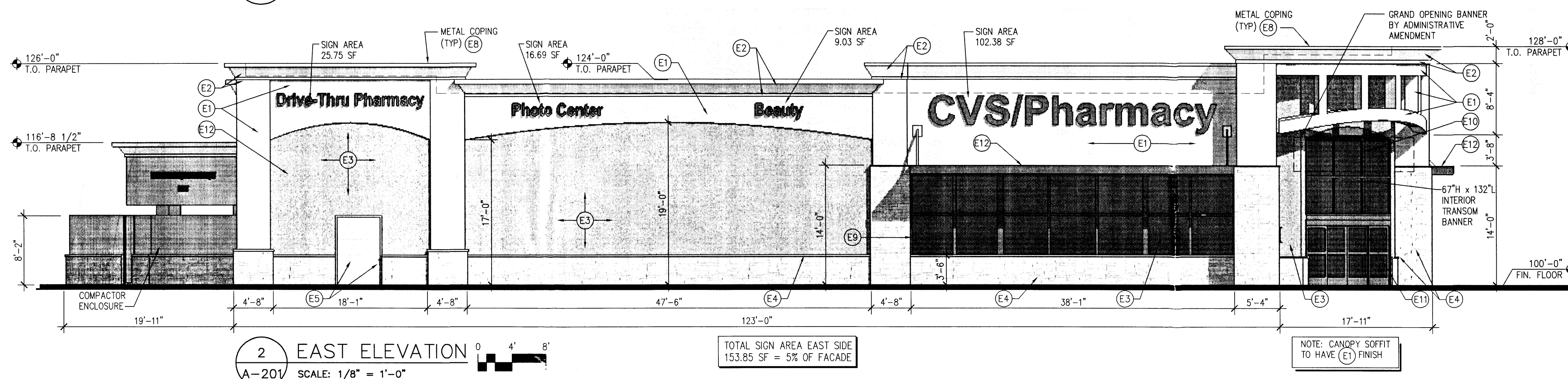
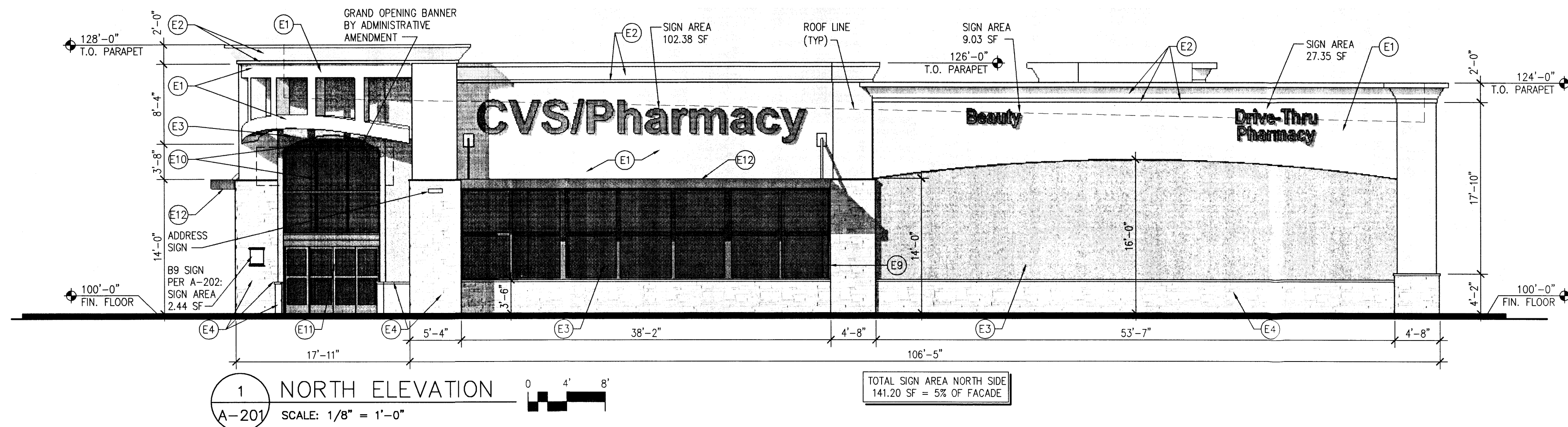
Job Number \_\_\_\_\_  
 Drawn By \_\_\_\_\_ MRP  
 Checked \_\_\_\_\_ SJS  
 Issue Date OCT 31, 2008

**CONCEPTUAL UTILITY PLAN**

Scale \_\_\_\_\_

**C-201**

AFD PLANS CHECKING OFFICE  
 924-3611  
 APPROVED/DISAPPROVED  
 HYDRANT(S) ONLY  
 R.C. Arch 12-4-08  
 SIGNATURE & DATE

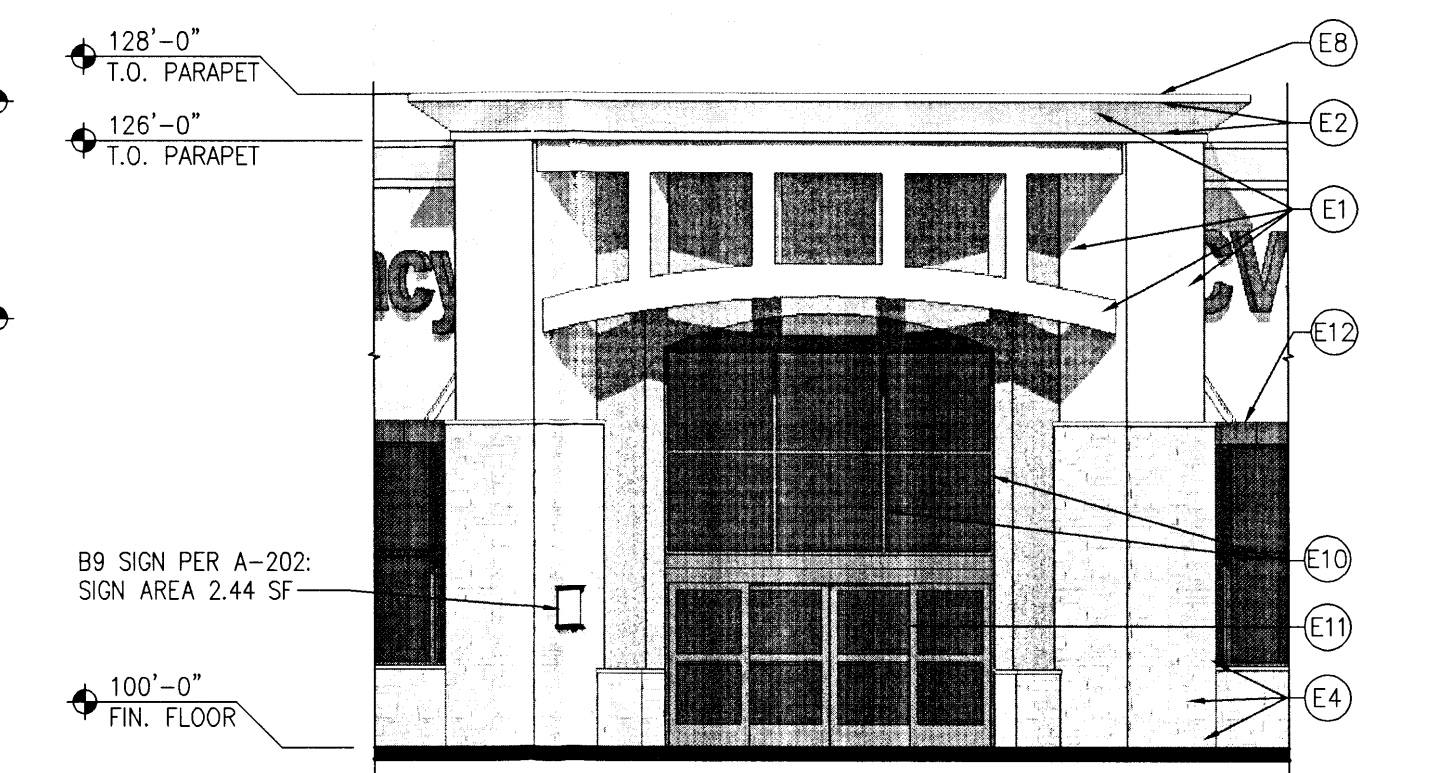
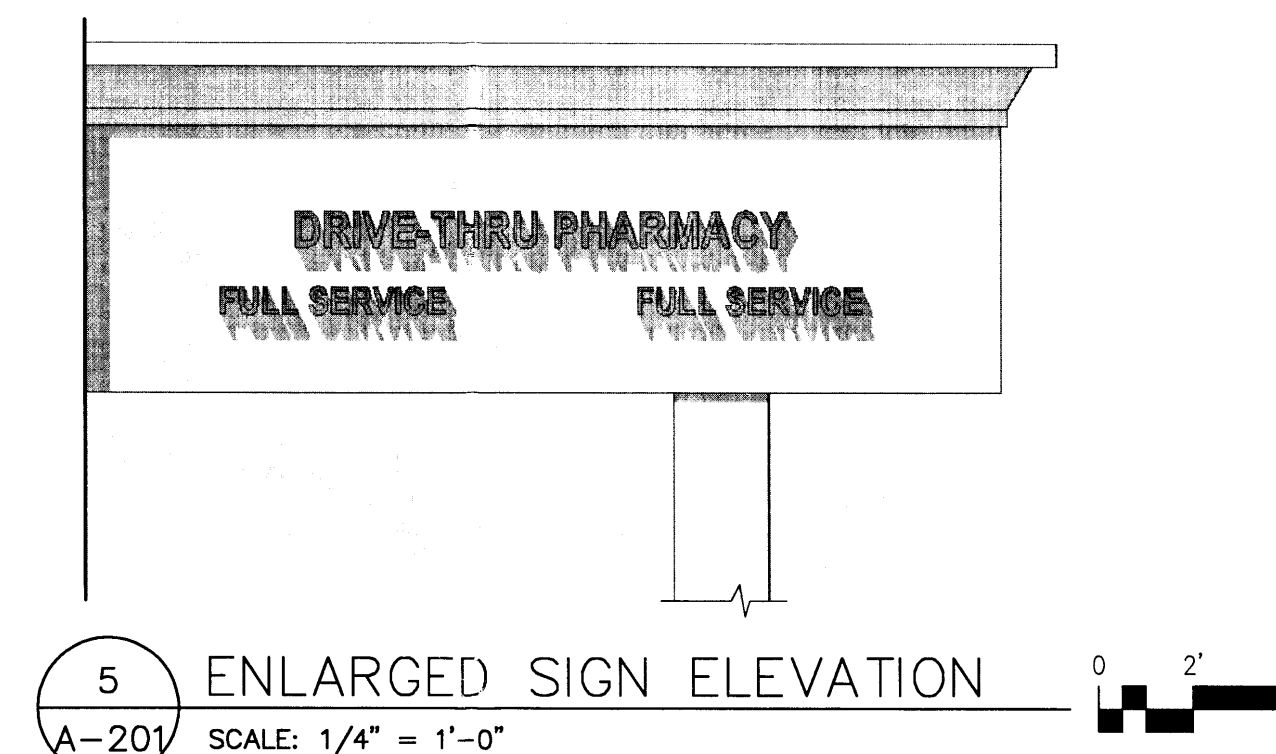


EXTERIOR FINISH SCHEDULE					
TAG	MATERIAL / DESCRIPTION	MFR.	COMMON COLOR NAME	MFR. COLOR	NOTES
E1	EIFS/STUCCO	EL REY ACRYLIC	LIGHT TAN	#327 CASA	
E2	EIFS	EL REY ACRYLIC	OFF-WHITE	#1641 MIST	
E3	EIFS/STUCCO	EL REY ACRYLIC	LIGHT BROWN	#1565 RIVER ROCK	
E4	STONE VENEER & CAP	CanyonRock PREMIUM DECORATIVE STONE	TAN	PREMIUM LEDGESTONE	
E5	PAINT	BENJAMIN MOORE	-	PAINT TO MATCH "E4"	
E6	NOT USED	-	-	-	
E7	NOT USED	-	-	-	
E8	PAINT	BENJAMIN MOORE	-	PAINT TO MATCH EIFS "E2"	
E9	ALUM STOREFRONT	-	RED	CARNIVAL RED VALSPAR SL44139	
E10	ALUM CURTAIN WALL	-	RED	CARNIVAL RED VALSPAR SL44139	
E11	ALUM AUTOMATIC DOOR	STANLEY	RED	CARNIVAL RED VALSPAR SL44139	
E12	PAINTED STEEL AWNING	-	RED	CARNIVAL RED VALSPAR SL44139	

**LEGEND**

(X) - RE: EXTERIOR FINISH SCHEDULE

NOTE: SIGNAGE UNDER SEPARATE PERMIT



**DSA**  
Darren Sowell  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

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**Mitchell Associates, LLC**  
Landscape Architects  
Carlie Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 833-6006  
camy@mitcheassociates.com

**ARMSTRONG**  
DEVELOPMENT  
PROPERTIES, INC.

**CVS PHARMACY**  
Site Plan for Building Permit  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121

**CVS/**  
pharmacy

1 9/30/08 EPC COMMENTS  
2 10/31/08 REVISIONS  
3 12/8/08 REVISIONS

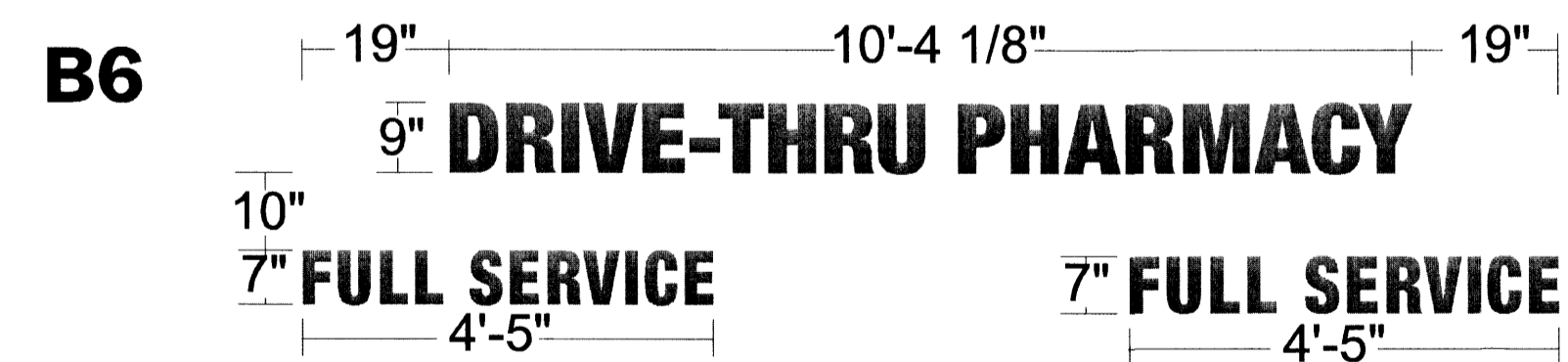
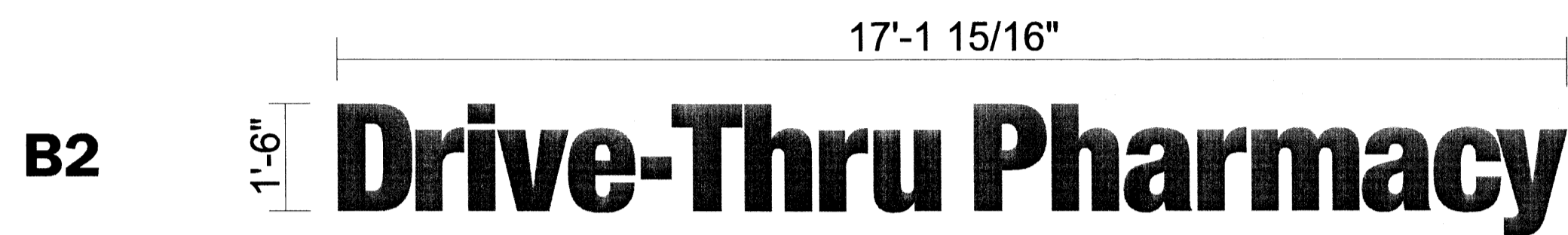
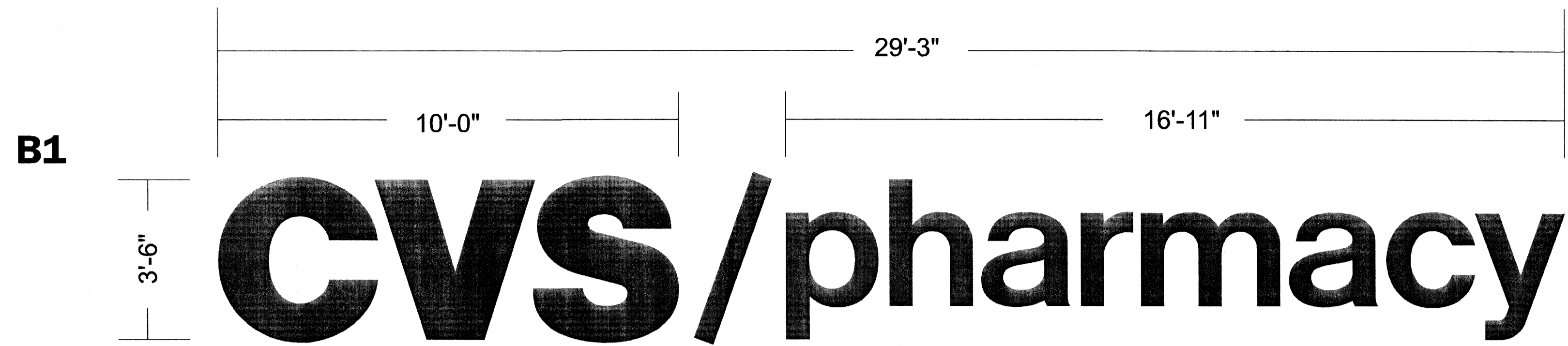
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Drawn By CDC  
Checked DLS  
Issue Date SEPT 2, 2008

**BUILDING**  
ELEVATIONS

Scale

**A-201**  
OF

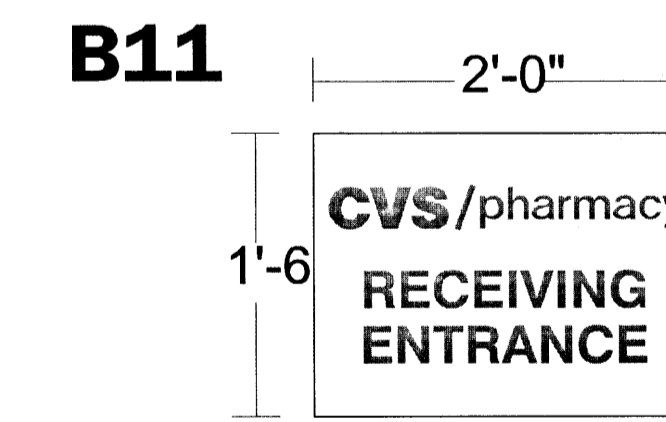
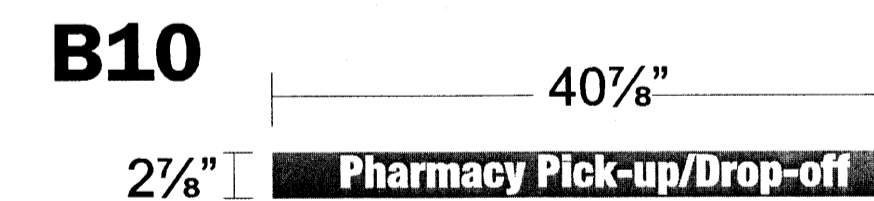
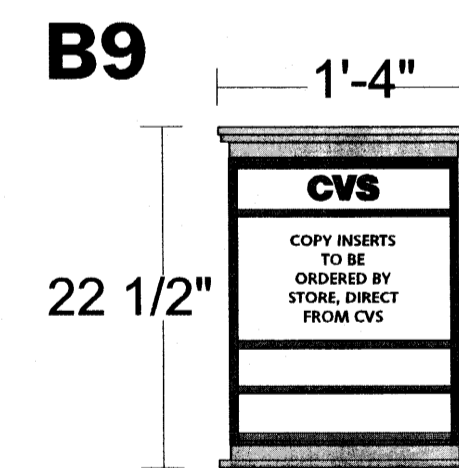
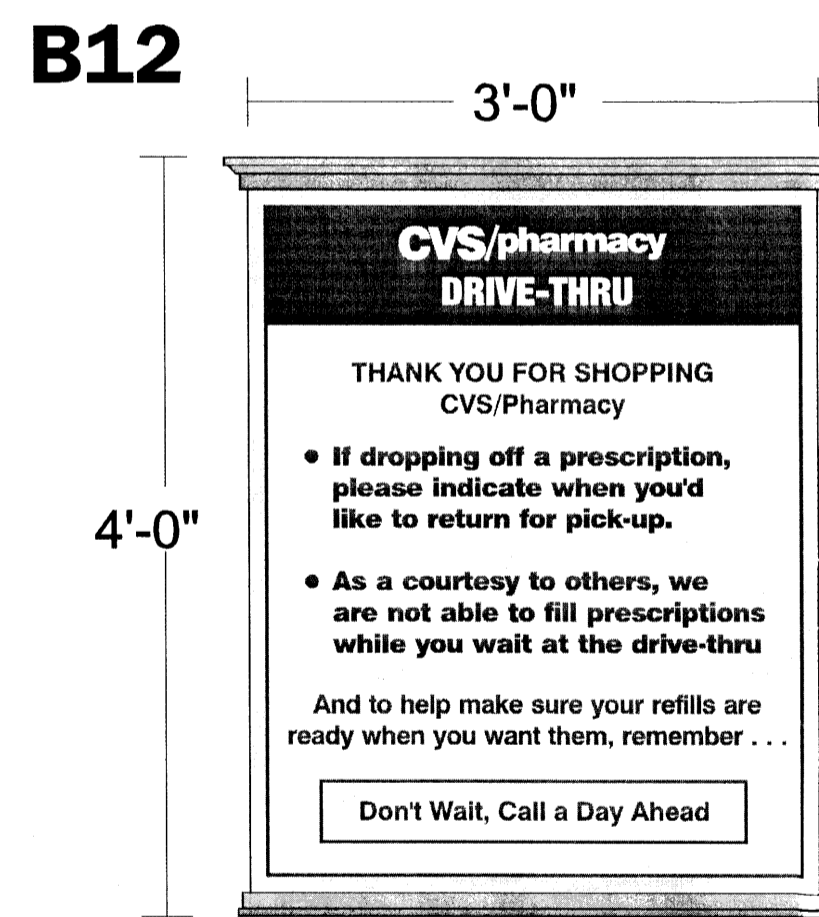
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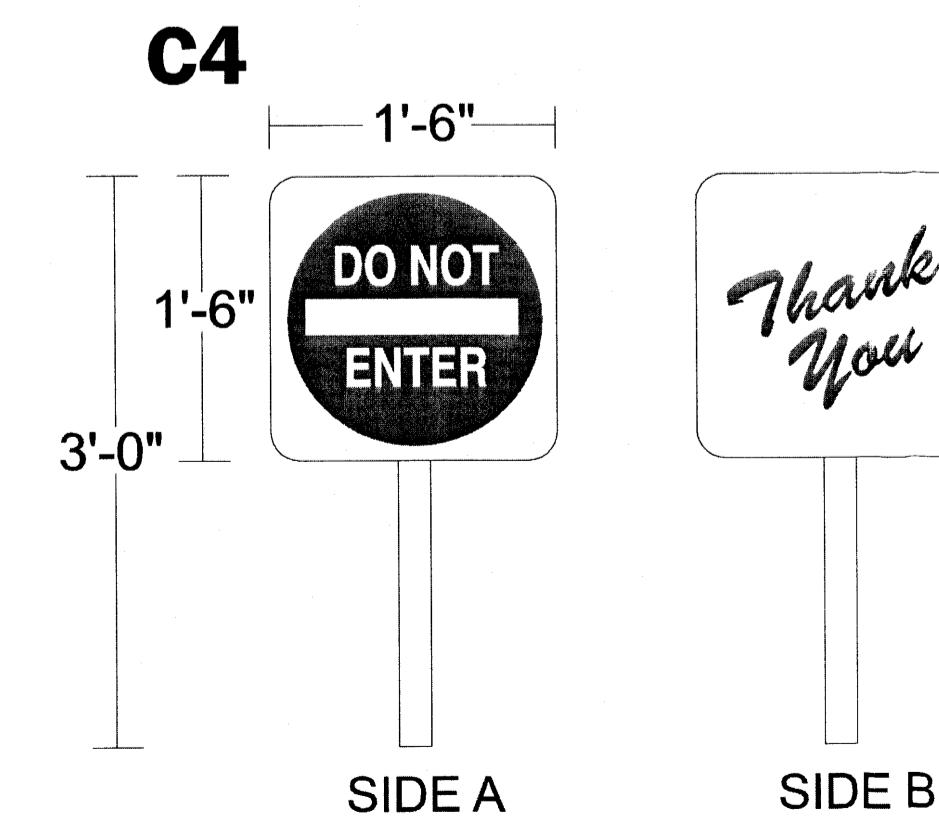
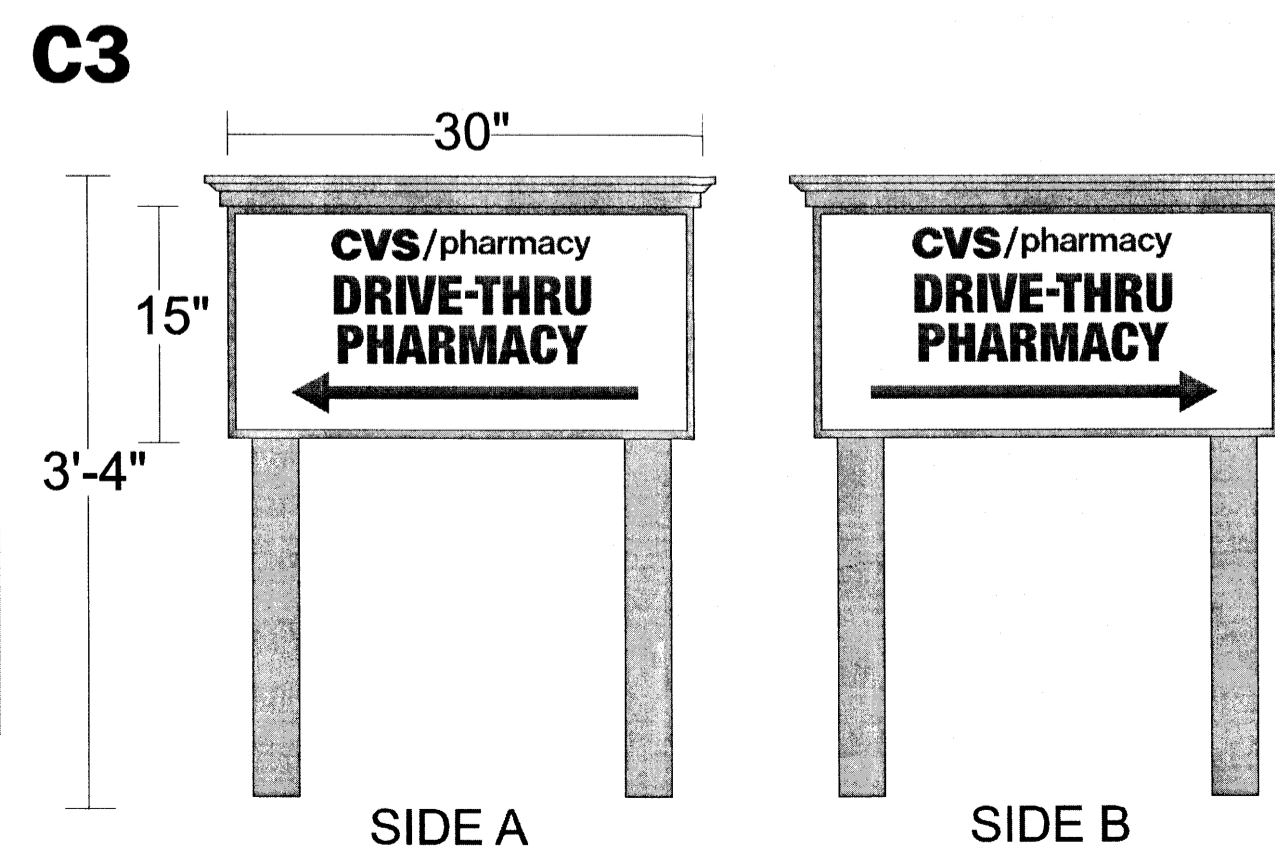
NOTE: THESE SIGNS ARE FOR ILLUSTRATIVE PURPOSES ONLY, EXACT WORDING WILL BE DETERMINED BY CVS AT A LATER DATE

NOTE: CVS/PHARMACY, ACILLARY & DRIVE-THRU PHARMACY (FOR DT CANOPY) CHANNEL LETTERS WILL BE INTERNALLY ILLUMINATED

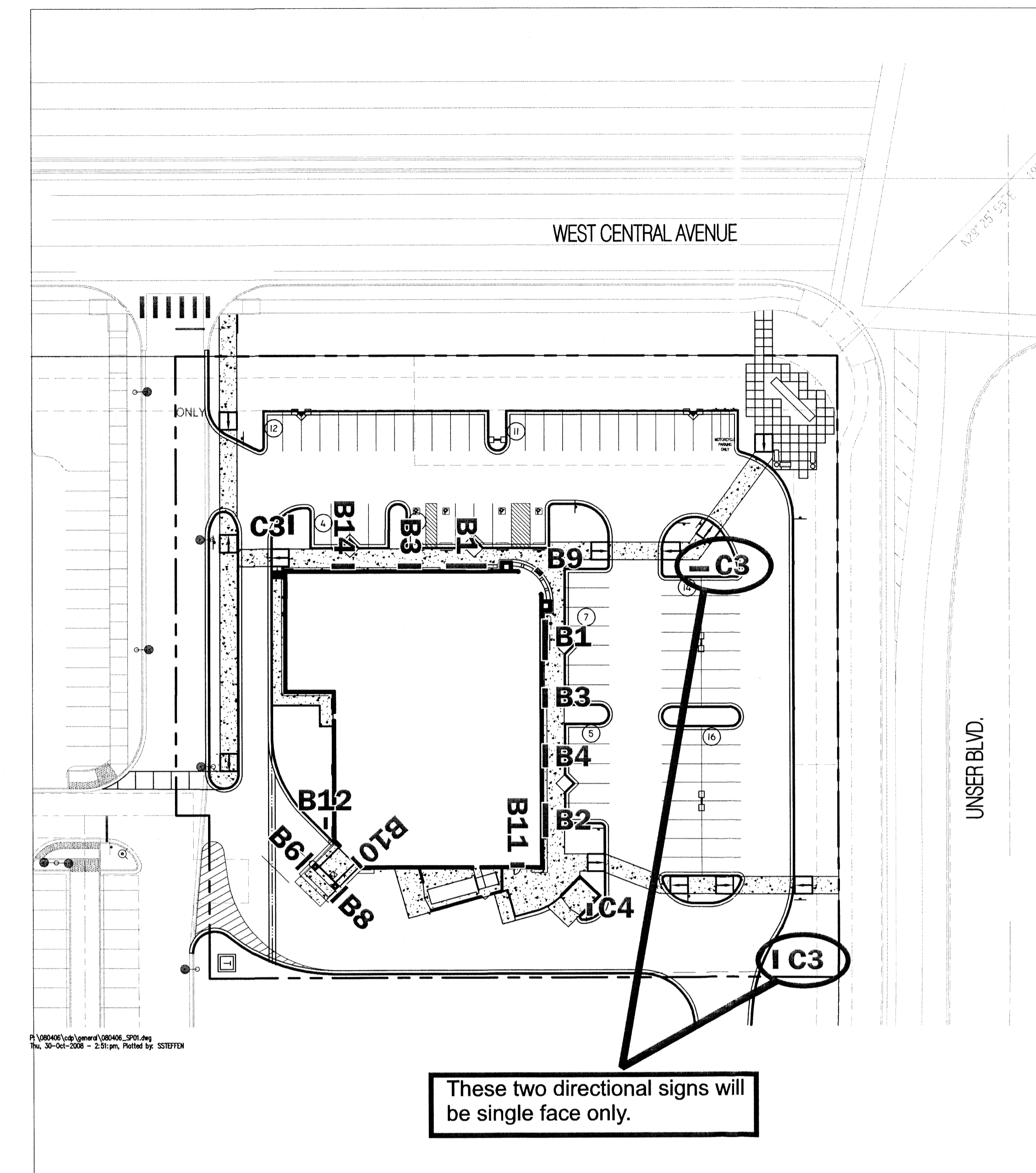
WALL SIGNS SCALE: 1" = 1'



DIRECTIONAL SIGNS SCALE: 1" = 1'



NOTE: DOUBLE FACE DIRECTIONAL.



Darren Sowell  
ARCHITECTS  
4700 Linclon N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201



Mitchell Associates, LLC  
Landscape Architects  
Cottage Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 839-6896  
dmv@mitchellassociates.com



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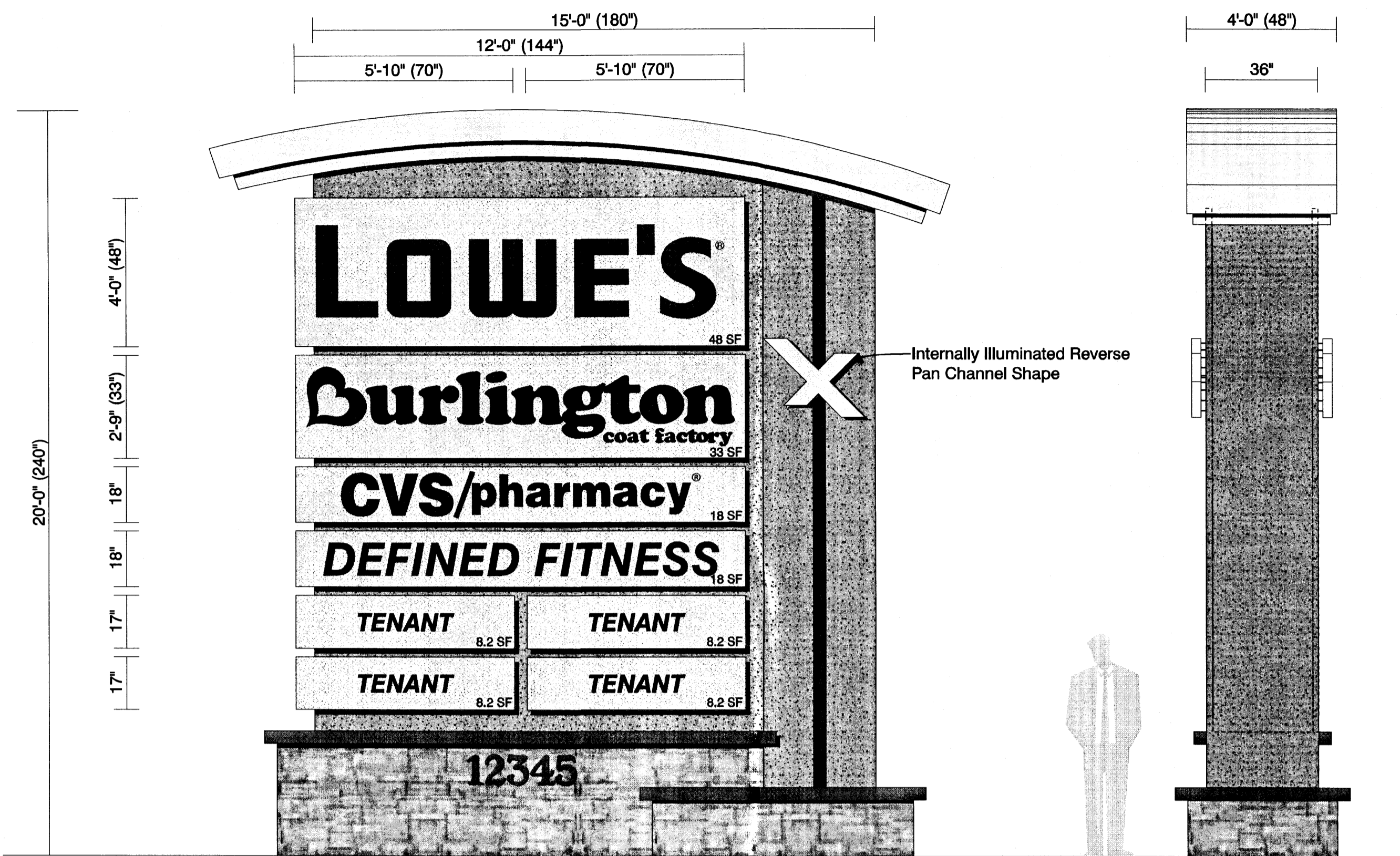
CVS PHARMACY  
Site Plan for Building Permit  
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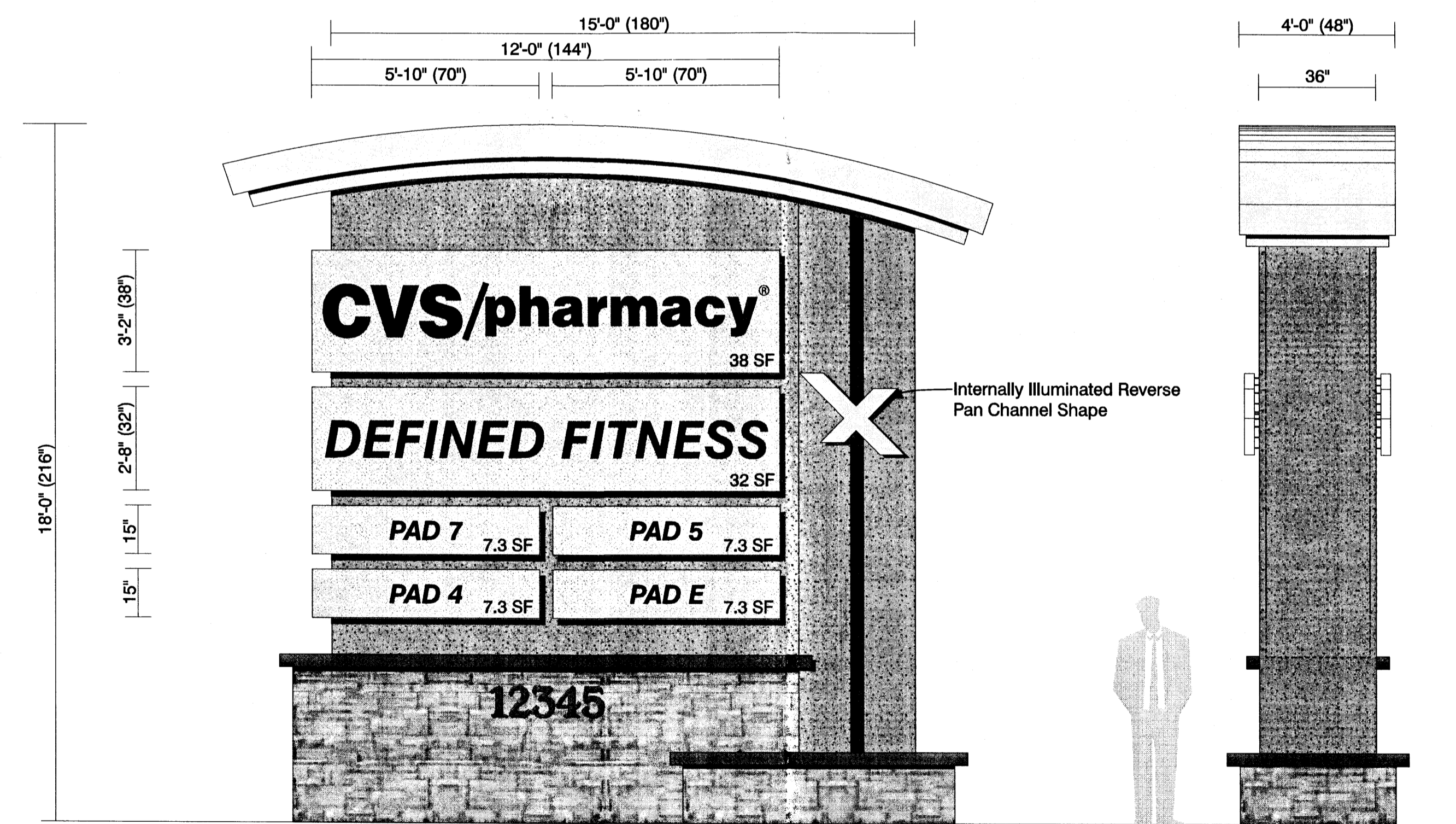
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Drawn By	AS
Checked	
Date	04/18/08
Revision	R10 -12/04/08

CHANNEL LTRS.,  
DIR, WALL SIGNS  
Scale: NOTED

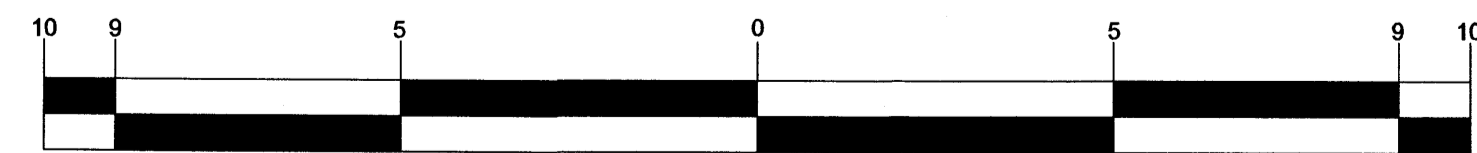




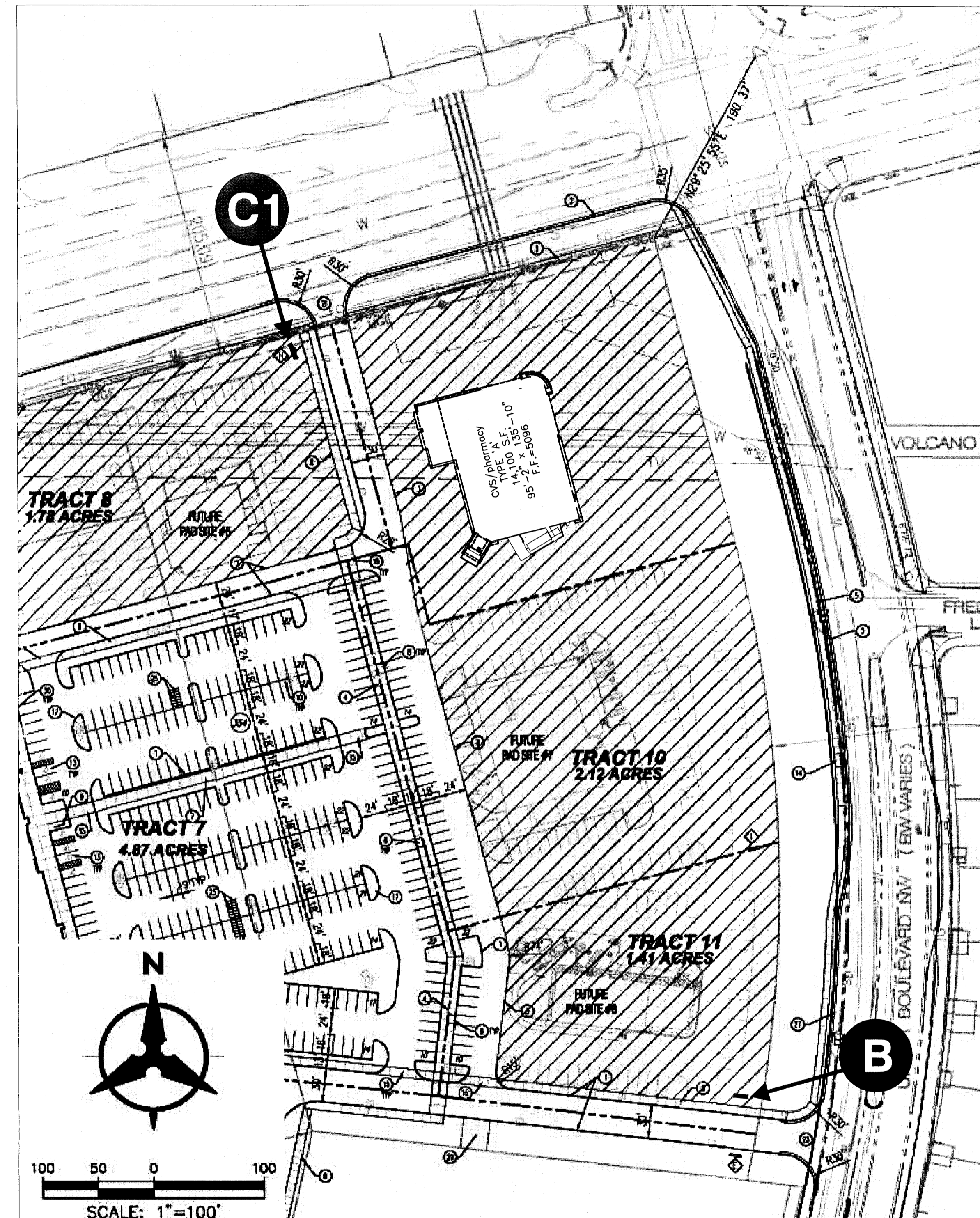
**B** One(1) 20' Tall Unser Blvd Sign @ 150 sq ft w/ 8 panels



**C** One(1) 18' Tall Central Ave Signs @ 100 sq ft w/ 6 panels



SCALE: 3/16" = 1'



Darren Sowell  
ARCHITECTS  
4700 Linclon N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201



Courtesy: 1700 Jefferson St. NE, Albuquerque, NM 87108-4336  
ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

Mitchell Associates, LLC

Landscape Architects  
Curtis Blvd, NE, Suite 112  
Albuquerque, NM 87113  
Phone: (505) 432-6095  
denny@mhassociates.com



www.southwestsigns.com  
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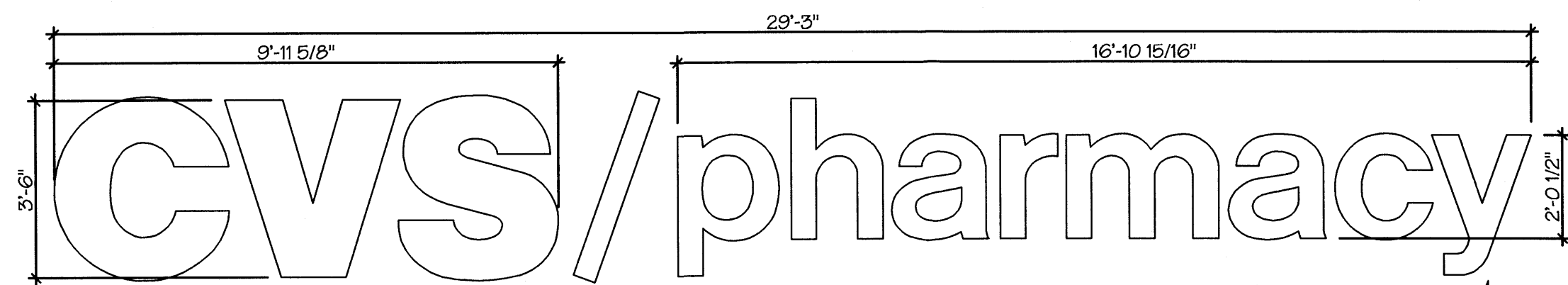
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Job	17892-Layout
Drawn By	AS
Checked	
Date	04/18/08
Revision	R9 - 10/31/08

TENANT MONUMENTS

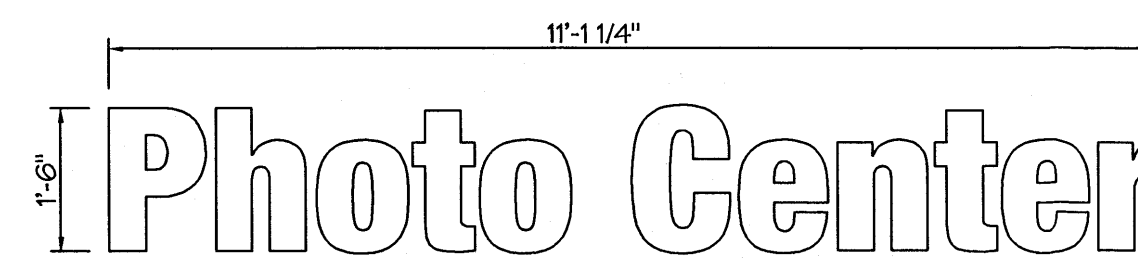
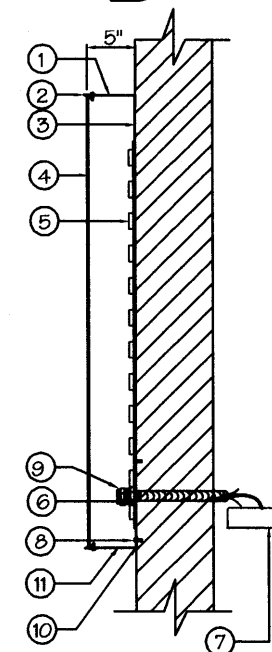
Scale: 3/16" = 1'-0"



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS

- .040 ALUM RETURNS PRE FINISH DK. BRONZE
- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
- POWER SUPPLY: GELcore
- 3/8" ANCHOR BOLT & RIV-NUT
- 1/2" SEALTITE CONDUIT FOR CONNECTION TO PRIMARY ELECTRICAL SERVICE
- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

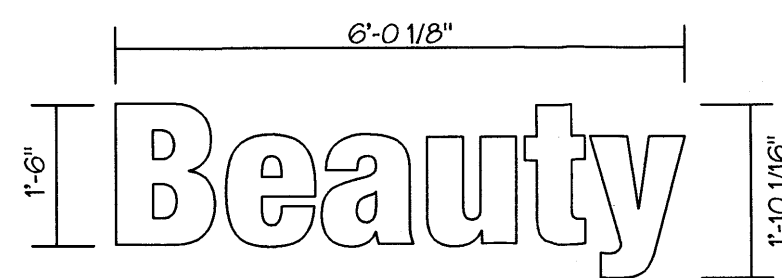
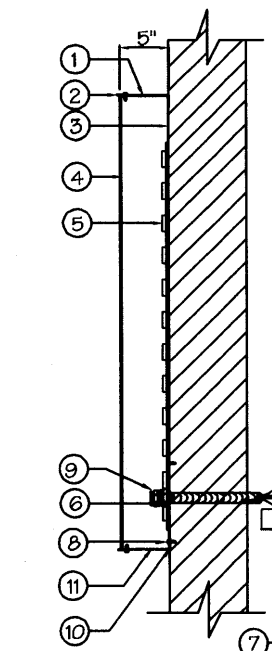
**B1** REMOTE WIRED CHNL LTRS  
3/8" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS

- .040 ALUM RETURNS PRE FINISH DK. BRONZE
- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
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- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

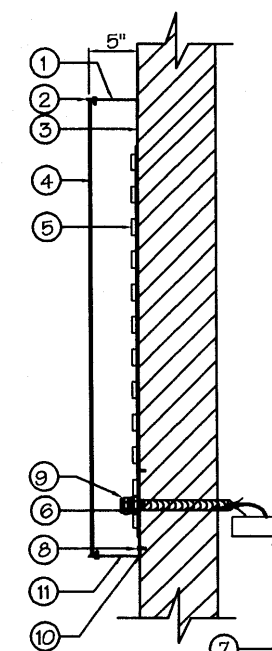
**B4** REMOTE WIRED CHNL LTRS  
1/2" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS

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- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
- POWER SUPPLY: GELcore
- 3/8" ANCHOR BOLT & RIV-NUT
- 1/2" SEALTITE CONDUIT FOR CONNECTION TO PRIMARY ELECTRICAL SERVICE
- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

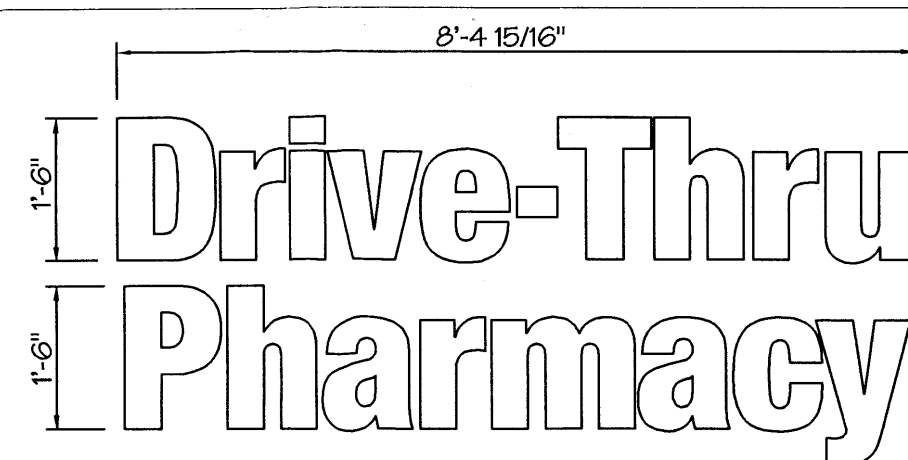
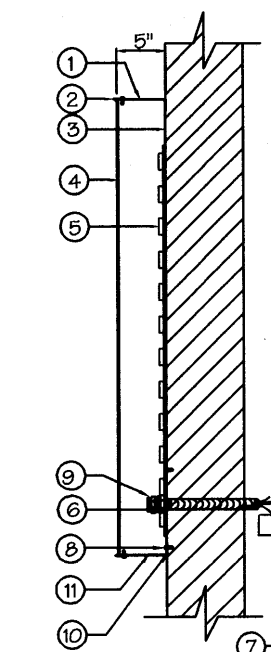
**B3** REMOTE WIRED CHNL LTRS  
1/2" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS

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- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
- POWER SUPPLY: GELcore
- 3/8" ANCHOR BOLT & RIV-NUT
- 1/2" SEALTITE CONDUIT FOR CONNECTION TO PRIMARY ELECTRICAL SERVICE
- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

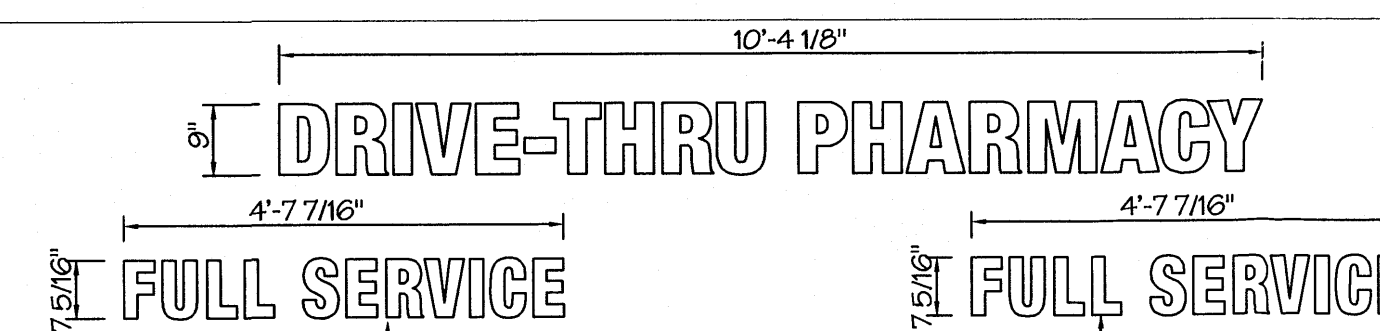
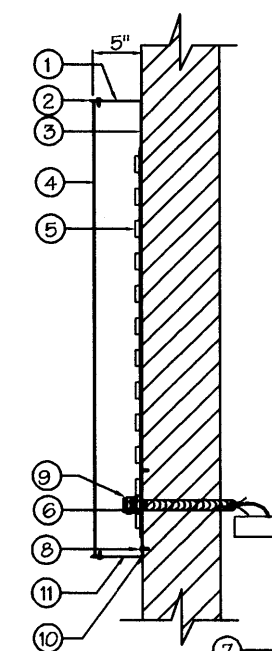
**B2** REMOTE WIRED CHNL LTRS  
1/2" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS

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- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
- POWER SUPPLY: GELcore
- 3/8" ANCHOR BOLT & RIV-NUT
- 1/2" SEALTITE CONDUIT FOR CONNECTION TO PRIMARY ELECTRICAL SERVICE
- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

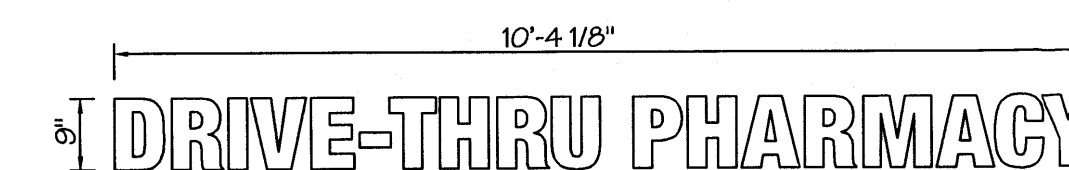
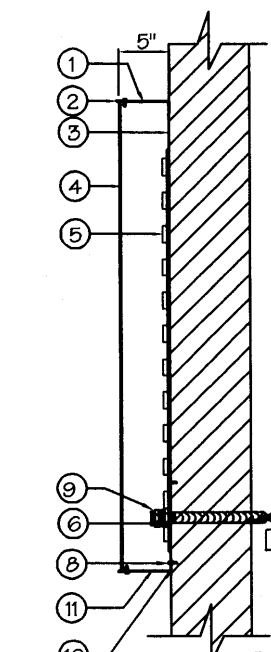
**B14** REMOTE WIRED CHNL LTRS  
1/2" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS WITH F.C.O.

- .040 ALUM RETURNS PRE FINISH DK. BRONZE
- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
- POWER SUPPLY: GELcore
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- 1/2" SEALTITE CONDUIT FOR CONNECTION TO PRIMARY ELECTRICAL SERVICE
- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

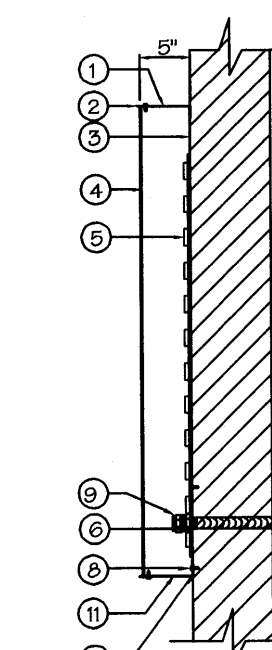
**B6** REMOTE WIRED CHNL LTRS  
1/2" = T



FABRICATION NOTES: (1) ONE SET OF REMOTE WIRED CHNL LTRS WITH F.C.O.

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- 1" BRONZE JEWELITE TRIMCAP
- .063 ALUM BACKS INSIDE TO BE MILL FINISH
- 3/16" #2793 RED PLEXI FACES
- GE TETRA-MAX LED SYSTEMS, RED
- 1/2" GC-50 STRAIGHT CONNECTOR FOR SUPPLY WIRE TO PASS THRU ALUM. BACKS
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- 1/4" WEEP HOLES (2) TWO PER LETTER WITH BAFFLES
- CAULK BEAD AROUND THE INSIDE OF RETURN AND BACK TO SEAL FROM WATER LEAKAGE

**B8** REMOTE WIRED CHNL LTRS  
1/2" = T



**Darren Sowell**  
ARCHITECTS  
4700 Linclon N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201



Courtesy: 1700 Jefferson St. NE, Albuquerque, NM 87104-4326  
ENGINEERS • SPATIAL DATA • ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**

Landscape Architects  
C/O: 844 N. 1st St. Suite 112  
Albuquerque, NM 87102  
Phone: (505) 839-9296  
dm@mtaassociates.com



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**CVS PHARMACY**  
Site Plan for Building Permit  
Central Avenue and Unser Boulevard  
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Job	17892-Layout
Drawn By	AS
Checked	
Date	04/18/08
Revision	R9 - 10/31/08

**CHANNEL LTRS.,  
DIR, WALL SIGNS**

Scale: NOTED

A-501

## INTRODUCTION

The following Design Standards for Unser Crossing are attached to and made part of the Site Plan for Subdivision. These Design Standards are intended to address areas of the site shown for illustrative purposes, and are provided to ensure a consistent level of development throughout Unser Crossing.

Future phases of the development may be delegated to the DRB after EPC review of the first pad site.

All development on the site must comply with the Zoning Code and West Route 66 Sector Development Plan Design overlay Zone regulations. If conflict exist, the most restrictive shall apply.

because of its proximity to the Enhanced Transit Corridor. The goal of this project is to combine the requirements of all of the City's Sector Plans and Overlay Zones affecting this property to create a place where residents from surrounding neighborhoods will frequent for their everyday purchasing needs in a convenient sustainable environment.

## 2. PERMITTED USES

### EXISTING USES: C-2

The Unser Crossing development will be a commercial center complying with all plans zones affecting this property. Any C-2 conditional uses proposed for the site will require a Conditional Use Permit.

## 3. DESIGN STANDARDS

### A. INTRODUCTION

These Design Standards are established to provide assurance that future design and construction will meet the cohesive standards of quality established by which these original structures are produced. These standards will assist in creating an attractive environment that promotes desirable opportunities for commercial activities and will define a unified image for architectural and landscape design that creates a distinctive visual identity.

### B. STREETScape

The development of a bold, dynamic entry and streetscape is essential in defining an image for Unser Crossing that places it at the forefront of Albuquerque's Community Activity Centers. The streetscape will provide a pedestrian friendly environment that will encourage users to park once and walk to more than one destination in Unser Crossing as well as encouraging nearby residents to walk rather than drive to Unser Crossing.

- Landscape buffers shall be provided to ensure flexibility for entry monumentation, landscaping, sidewalk development and other pedestrian amenities, as well as opportunities for visual screening and environmental amenities, such as storm water infiltration, and heat island effect reduction. Buffers shall be provided as follows:
  - 25 foot minimum from the right-of-way at Central Avenue.
  - 20 foot minimum from right-of-way at Unser Boulevard, Bridge Boulevard and 86<sup>th</sup> Street.
- Trees shall be provided at an average spacing of 25 feet on-center along the four (4) public right-of-ways surrounding Unser Crossing, and at an average spacing of 30 feet on-center along the building facades. Trees shall be provided in the landscaped strips of inward facing facades at an average spacing of 50 feet on-center.
- Living plant materials shall cover a minimum of 75 percent of the required landscape area.
- Low water use turf may be provided at a maximum of 40 percent of the required landscape area.
- High water use turf is not allowed.
- Materials for the separation of turf and planting beds may be concrete, brick, or similar quality material.
- Streetscape will encourage nearby residents to walk rather than drive to Unser Crossing.

### C. PARKING

The intent of the standards for the development of parking areas is to: mitigate heat/glare through the provision of landscaping; minimize the visual impact of parking areas; and provide accessible, safe circulation within and adjacent to the parking areas.

- Parking shall be placed on at least two sides of a building within a particular lot and, if possible, shall not dominate the building or street frontage.

- Parking space standards shall be in accordance with the City of Albuquerque's Comprehensive Zoning Code.
- Parking shall be distributed on the site to minimize visual impact from the adjoining street, shall not dominate the street frontage, and shall be adequately screened with landscape walls and plantings.

- Clear pedestrian connections shall be provided through parking areas at a minimum width of 6 feet and shall be clearly demarcated with slightly raised and textured paving where they cross vehicular entrances and drive aisles. Shade trees shall be provided along pedestrian walks at an average spacing of 25 feet on center.
- ADA-compliant parking shall be located adjacent to main building entries.
- Sidewalks and/or pedestrian paths that are perpendicular to and at the front of parking spaces shall be protected from overlapping cars by permanently anchored tire stops, bollards, or raising the walkway to provide for a 6-foot wide, clear pedestrian area.

- Landscaped islands shall be distributed throughout parking areas. Parking areas visible from the public street shall include at least one tree for every six parking spaces, and the maximum distance from any parking space to a tree shall be 50 feet. Trees located within 8 feet of the perimeter of the parking area may be counted toward this requirement, but may not be used to fulfill street tree requirements.

- Trees shall be provided in parking areas per the requirements of the West Route 66 Development Plan Design Overlay Zone

- A minimum of 75 percent of parking lot trees shall be large canopied deciduous trees to provide shade during the summer months.

- Parking screening shall be in compliance with requirements in the Zoning Code §14-16-3-1 (F) (4).
- Motorcycle, moped, and motor scooter parking shall be provided close to building entrances per City Regulation §14-16-3-1 and shall be visible from building entrances.

### D. BICYCLE FACILITIES

Providing convenient bicycle facilities to encourage non-vehicular travel to Unser Crossing is an integral part of the City of Albuquerque's goal of supporting alternative modes of transportation and sustainability. Safe and convenient connections to the surrounding city bicycle routes will be provided to all of the Unser Crossing's occupants.

- Bicycle parking for employees shall be provided to promote alternative vehicle use. One bicycle rack space per 20 parking spaces is required, and shall be conveniently located near building entrances, but not within pedestrian pathways or landscape areas.

### E. SITE LANDSCAPE

Landscaped areas serve to enhance the visual dynamic of the development and aid in reinforcing the street edge and pedestrian environment, as well as providing environmental benefits such as storm water infiltration opportunities and heat island effect reduction. The primary focus for site landscape shall be the common areas and major pedestrian pathways.

The plant palette for Unser Crossing shall provide year-round color and interest and shall consist of regionally appropriate, low and moderate water use plants. Elements such as street furniture, lighting, bollards, and graphic pylons shall be used to reinforce the street and pathway edge. These elements shall be consistent throughout the entire site to reinforce the unified aesthetic. A palette of hardscape materials will also be selected for the entire site to further contribute to the sense of visual entity. Please see the plant pallet in the Landscape drawings included in this package.

- All landscape plans shall comply with the City's Water Conservation Ordinance, Pollen Ordinance, Zoning Code, and West Route 66 Sector Development Plan Design Overlay Zone .

- Water harvesting techniques, including some combination of curb cuts for drainage to landscaped areas, bioswales to slow and treat storm water runoff, and pervious paving or other innovative technologies, are encouraged.

- The total landscaped area required for each site shall equal not less than 15% of the net lot area as defined in the Zoning Code §14-16-3-10 (E) (1).

- Future development areas within the site shall be revegetated with native seed mix appropriate for this region and maintained appropriately to discourage erosion and blowing dust.
- The hard-scape palette shall be selected by the Developer at the time of building permit drawings submittal.

- Trees shall be provided at an average density of 1 tree per 1500 square feet of landscaped area.

- Live plant materials shall cover a minimum of 75 percent of all required and provided landscaped areas.

- Landscape headers shall be used to separate turf and planting beds. Headers may be concrete, brick, stone or other similar quality material.

- Gravel mulch, cobble, and similar materials are acceptable as a top dressing for landscaped areas, but organic mulches shall be encouraged where the grade allows.

- An automatic underground irrigation system is required to support all site landscaping. The system shall be designed to maximize efficiency and avoid over-spraying onto walks, buildings, fences, walls, etc.

- All plant material shall be maintained by the Owner or Tenant in a living, attractive condition. This includes proper maintenance of the irrigation system and removal of weeds, trash, or debris, as well as repair of any areas exhibiting erosion damage.

- Minimum plant material sizes at the time of installation shall be as follows:

- Canopy trees - 2" caliper B&B or boxed
- Evergreen trees - 8 foot minimum height
- Accent trees - 1 1/2" caliper B&B or boxed
- Multitrunk trees – 15-gallon minimum
- Shrubs, ornamental grasses and groundcovers - 1 gallon minimum

### F. SITE PLANNING

The intent of the following standards is to create pedestrian-friendly environments for Unser Crossing customers.

- Pedestrian Plazas shall be provided in the amount of 400 square feet for every 20,000 square feet of building space. If the development of future phases produce a deficiency in this requirement additional plaza areas shall be provided.

- A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.

- Pedestrian Plazas shall be linked to the main entrance of the principal structure and the public sidewalk or internal driveway, shall include seating, 40% of the area of the plaza shall be landscaped including shade trees. Plazas shall be safe and visible from the public right-of-way as much as possible, and shall have pedestrian scale lighting and other amenities such as trash receptacles.

- A sidewalk with a minimum width of 8 feet shall be provided along the front of buildings that are less than or equal to 10,000 square feet. A sidewalk with a minimum of 10 feet in width shall be provided along the front of buildings that are 10,000 to 30,000 square feet. A sidewalk with a minimum width of 15 feet shall be provided along the front of buildings that are greater than 30,000 square feet. These sidewalks shall be shaded with architecturally integrated awnings, portals, or canopies, or by trees planted at intervals of 25 feet in planters with a minimum interior dimension of 6' x 6'.

- Pedestrian connections with a 8-foot minimum clear path shall be provided from each building to the internal circulation system and to adjacent roadways. Shade trees shall be provided along the pedestrian connection at an interval of 25 feet in planters that have a minimum interior dimension of 6' x 6'.

- A continuous sidewalk with a minimum width of 6 feet shall be located around the perimeter of buildings that are greater than 4,000 square feet.



**Bohannon ▴ Huston**

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**Mitchell Associates, LLC**

Landscape Architects  
Central Ave., Suite 112  
Albuquerque, NM 87110  
Phone: (505) 693-6200  
Fax: (505) 693-6201  
cm@mitcheassociates.com



Unser Crossing  
Site Plan for Building Permit  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121

Revised: June 17, 2008  
Revised: September 4, 2008

Job Number \_\_\_\_\_  
Drawn By \_\_\_\_\_  
Checked \_\_\_\_\_  
Issue Date April 3, 2008

DESIGN STANDARDS

Scale \_\_\_\_\_

**G-101**  
00 OF 00

- Paving of primary pedestrian crossings and primary outdoor activity areas shall use patterned, stained, or integrally colored concrete. Stone or brick paving systems may also be used.
- Asphaltic paving shall only be used in drive isles and parking/service areas. The design of all bicycle paths shall comply with the AASHTO 1999 Guide for the Development of Bicycle Facilities, and shall be coordinated with the City's designated Trails Planner.
- Perimeter walls, if provided, shall comply with the City's Design Manual for Subdivision Access and Perimeter Walls. Chain-link may be allowed for security fencing purposes when out of public view. The use of barbed wire, wood fencing, or plastic vinyl fencing is not permitted. All site walls shall comply with Zoning Code §14-16-3-19.

**G. SETBACKS**

The use of building area setbacks is required to provide space for the creation of visually attractive streetscapes and provide a safe and compatible environment for vehicles and pedestrians throughout the Unser Crossing. Required within these setbacks will be pedestrian walkways, screening devices, and landscape improvements. The landscaped set back areas will receive vegetation on 80% of the area.

- Buildings shall be located according to the following minimum setback dimensions:
  - 25 foot minimum from the right-of-way at Unser Boulevard, and Central Avenue.
  - 20 foot minimum from the right-of-way at Bridge Boulevard and 86<sup>th</sup> Street.

**H. ARCHITECTURE**

In general, all buildings shall comply with the current City of Albuquerque Zoning Code and all applicable sector plan, policies, and ordinances. However, in cases where the Unser Crossing requirements exceed the Zoning Code, the Unser Crossing standards will govern. The focus of these architectural standards and guidelines is to promote consistency and continuity in relation to building aesthetics and designs. The use of the Albuquerque Energy Conservation Code and LEED guidelines are encouraged.

**Development Densities**

Infrastructure for the Unser Crossing is currently master planned for a Maximum Floor Area Ratio (F.A.R.) of 1.0.

**Building Heights**

Building height limits will be enforced in accordance with the City of Albuquerque Zoning Code for the C-2 zone. However, buildings located on interior parcels not fronting any right-of-way or affecting any adjacent neighborhoods do not comply with the Zoning Code by being located closer to the property lines than the required setbacks.

**Building Entrances**

Primary entrances along major facades shall be clearly defined with façade variations, porticos, roof variations, recesses or projections, or other integral building forms.

- Facades that contain a primary customer entrance and facades adjacent to a public street or plaza or an internal driveway shall contain display windows, or a recessed patio at a minimum depth of 20 feet, or a combination of all three, along 50% of the length of the façade. Where patios are provided, at least one of these recessed walls shall contain a window for ease of surveillance and the patio shall contain shading and seating. Where Retail Suite Liners are provided, they shall be accessible to the public from the outside.

- Break up the Mass. Major facades greater than 100 feet in length shall break up building mass by including at least two of the following architectural features:
  - wall plane projections or recesses of at least 2 feet in depth, occurring at least every 100 feet and extending at least 25% of the length of the façade.
  - a vertical change in color, texture, or material occurring every 50 linear feet and extending at least 20% of the length of the façade.
  - an offset, reveal, pilaster, or projecting element, no less than two feet in width and projecting from the façade by at least six inches and repeating at minimum intervals of 30 feet.
- Every 30,000 gross square feet of structure shall be designed to appear as a minimum of one distinct building mass with different expressions. The varied building masses shall have a change in visible roof plane or parapet height. Massing and articulation are required to be developed so that no more than 100' of a wall may occur without an offset vertically of at least 24".
- Facades adjacent to a public right-of-way internal driveway and facades that contain primary customer entrance shall contain features that provide shade along at least 40% of the length of the façade for the benefit of pedestrians.

**Service/Loading Areas**

Loading areas which face a public street or residentially-zoned property and which are not separated from the public street or a residentially-zoned property by intervening buildings, landscaping or by a distance of at least 100 feet, shall be screened with solid walls which are a minimum of six feet in height when measured from the finished grade exterior to the loading area and eight feet at loading dock areas.

**Context**

New buildings shall be designed to harmonize with adjacent buildings.

**Building Articulation/Design**

Buildings shall convey an image appropriate to a neighborhood retail/activity center.

- Buildings shall be sensitive to the local character of the region and the vernacular elements of buildings.
- Edge treatments shall "step down" to relate to the scale of pedestrians: reduce the scale of the building edge by stepping down to the street. It is encouraged that building masses be arranged in order to cast shadows on each other to emphasize the contrast of light and shaded surfaces at corners and edges.
- The front building facade shall employ variety in structural forms to create visual character and interest. Front facades shall have varied front setbacks, with wall planes not running in one continuous direction for more than 100 feet without a change in architectural treatment (i.e., 3 foot minimum offset, fenestration, material change, etc.)
- Columns, arcades, corner articulation, overhangs, awnings, marquees, gutters and scuppers, breezeways, and soffits shall be carefully dimensioned and detailed to provide a human scale and visual interest.
- Extended architectural detailing on the ground floor in an elevational band from 0 to 10 feet, such as a change in color or texture, the architectural expression expansion joints as reveals, door and window articulation, and architectural accents, is required.
- The rears of building shall be articulated meeting the same standards as the sides of buildings.

**Equipment**

All roof top equipment and appurtenances shall be below the top of the parapet, or architecturally screened with a visually-solid surround that is compatible with the predominant building materials and color. Roof penetrations that are not architecturally screened with a visually-solid surround shall be painted to match roof color or general building color.

All equipment shall be screened in accordance with §14-16-3-18 of the Comprehensive Zoning Code.

Transformers utility pads and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way but in such a way that is acceptable to the affected service provider to ensure safety and access for maintenance, repair, and replacement of equipment.

**Portable Buildings**

Permanent or temporary portable buildings are not allowed in Unser Crossing.

**Materials and Colors**

Buildings shall convey solidity and durability and employ high quality materials in their construction. Materials and colors shall pay homage to the vernacular architecture of the area. Acceptable materials include: stucco, concrete (sand blasted or finished surface), stone, split face block, burnished block, glass curtain wall, glass block, brick, exterior ceramic tile, painted metal, and EIFS. Architects shall use those materials traditionally used in the Albuquerque or New Mexico area.

- Acceptable colors include those taken from the native New Mexican landscape palette, neutral, and earth tones, accent colors in deeper neutral or earth tones may be used. Colors shall be the inherent color, rather than from application of color (painted), except when used as an accent color.
- Prohibited building materials include the following:
  - plastic or vinyl building panels, awnings, or canopies
  - exposed, untreated precision block or wood
  - highly reflective surfaces
- Special consideration shall be given to roof structures. The use of contrasting colors between roofs and walls is encouraged to help differentiate the planes of building masses.
- The use of accent colors and materials is encouraged to bring out detailing to better articulate or give scale to a building, including glazed tile, wood trim, tile roofing, paint, metal, etc.

**Sustainability**

Green architectural design and construction has been implemented whenever possible. Unser Crossing uses sustainable design principles, environmentally responsible building concepts and practices, and earth-friendly products.

- Low water use turf may be provided at a maximum of 40 percent of the landscaped area.
- High water use turf will not be allowed.
- All landscape plans comply with the City's Water Conservation Ordinance and Pollen Ordinance.
- Water harvesting techniques, including some combination of curb cuts for drainage to landscaped areas, bioswales to slow and treat storm water runoff, cisterns for the collection and reuse of storm water and gray water, and pervious paving or other innovative technologies, are encouraged.
- An automatic underground irrigation system will be installed to support all site landscaping. The system shall be designed to maximize efficiency and avoid over-spraying onto walks, buildings, fences, walls, etc.
- Providing convenient bicycle facilities to encourage non-vehicular travel to the Unser Crossing is an integral part of the City of Albuquerque's goal of supporting alternative modes of transportation and sustainability. Safe and convenient connections to surrounding City bicycle routes will be provided to all Unser Crossing employees and customers.
- The use of the Albuquerque Energy Conservation Code is encouraged.
- Convenient recyclables collection facilities shall be provided by all tenants of Unser Crossing.
- Impervious surfaces shall be limited by installing permeable paving surfaces, such as bricks and concrete lattice or such devices that are approved by the City Hydrologist, where possible.
- Access to transit and safe and convenient pedestrian connections are provided to facilitate multi-modal transportation.



**Bohannon & Huston**

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**Mitchell Associates, LLC**

Landscape Architects  
 6700 9th NE, Suite 112  
 Albuquerque, NM 87118  
 Phone: (505) 838-4095  
 dmy@mtcheesestack.com



**Unser Crossing Permit  
 Site Plan for Building  
 Central Avenue and Unser Boulevard  
 Albuquerque, New Mexico 87121**

Revised: June 17, 2008  
 Revised: September 4, 2008

Job Number \_\_\_\_\_  
 Drawn By \_\_\_\_\_  
 Checked \_\_\_\_\_  
 Issue Date April 3, 2008

DESIGN STANDARDS

Scale \_\_\_\_\_

**Plan Arrangement Opportunities**

Open courtyard designs shall be employed in order to form transitions between parking areas and building facades. Building access and entries shall be visible from the street through open passages (such as garden courts).

**I. LIGHTING**

A consistent theme for the lighting fixtures within the streetscape and common areas of Unser Crossing will contribute significantly to Unser Crossing's overall aesthetic character. Safety and security shall be the primary design consideration, as well as the daytime appearance of the light fixtures.

- Lighting shall be fully shielded horizontal lamps so that no fugitive light may escape beyond the property line and no light source shall be visible from the site perimeter.
- Height standards for light fixtures are as follows:
  - Small parking area light fixtures shall not exceed 20 feet
  - Main parking area light fixtures shall not exceed 20 feet unless the Zoning Hearing Examiner approves variance.
  - Lighting fixtures for walkways, entry plazas, and within 100 feet of a residential zone shall not exceed 16 feet.
- Controlled, directional lighting shall be used to highlight public spaces and walkways. The use of walkway level lighting, such as wall pockets or bollard lights, is encouraged to accent pedestrian areas.
- Fixture style and design shall be compatible and consistent with the lighting design of other projects within Unser Crossing.
- The pattern of light pooling from each fixture shall avoid glare or reflection on adjacent properties, buildings, or roadways.
- Exterior lighting fixtures shall relate stylistically to the architecture of the adjacent buildings.
- Uplighting fixtures to highlight trees, walls and architectural features shall be limited to 100 maximum wattage per fixture, and shall be designed to comply with the New Mexico Night Sky Protection Act.
- The use of energy efficient lighting is required.

**J. SCREENING/BUFFERING**

Each development site design shall incorporate certain criteria in order to provide proper site screening from public roadways.

- Mechanical equipment whether on roof areas or at street level, shall be fully screened from pedestrians or motorists. Screening shall be compatible with materials and design of the building.
- Loading areas shall be screened from public view by walls, trellises, or landscaping. Passenger loading areas do not require screening.
- The use of barbed wire, wood fencing, or plastic vinyl fencing is not permitted.
- Refuse enclosures shall be adjacent to the building and screened with walls of the same material as the building itself.
- Walls or landscaped berms 2 1/2 to 3 feet in height shall be provided to screen parking areas adjacent to public streets. Walls, if used, shall be designed to integrate with building materials and colors.

**K. SIGNAGE**

Refer to the Sign Package on sheets S-101, S-102, and S103.

**L. TRANSIT FACILITIES**

Bus Route 66 Central currently serves the Unser Crossing site. Credit for the bus route will be used in parking calculations. Existing bus stops will be incorporated into the site design by providing seating, trash cans, and shade structures using the same materials and design as those provided in the Unser Crossing buildings. Future development (phase 2) shall meet the guidelines of the Comprehensive Plan for buildings adjacent to Enhanced Transit Corridors including siting buildings close to the street with parking on the side or rear providing entrances facing the street.

Currently, the following bus routes service this location as of March 15, 2008: Route 54, Route 162, Route 766, and Route 66.

**M. DRIVE-UP SERVICE WINDOWS**

Drive-up windows will be located on or adjacent to the side or rear walls of service or retail structures and the window shall not face a public right-of-way or pedestrian areas, residentially zoned areas, and public streets where possible. In cases where drive-up windows face these areas, screening shall be provided. Screening may be in the form of walls, earth berms, or evergreen landscaping, or a combination thereof and shall be a minimum of three feet in height.

Drive-up service windows shall be limited to 4, with the number of adjacent drive-up service window uses limited to two. Drive-up window uses may include bank, pharmacy, and a maximum of two "quick-serve-restaurants."

**N. WIRELESS TELECOMMUNICATIONS FACILITIES**

Any allowance for wireless telecommunications facilities shall require architectural integration.



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Mitchell Associates, LLC

Landscape Architects  
Carlsbad, NM, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 836-6106  
dmy@mtaheboassociates.com



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Checked \_\_\_\_\_  
Issue Date April 3, 2008

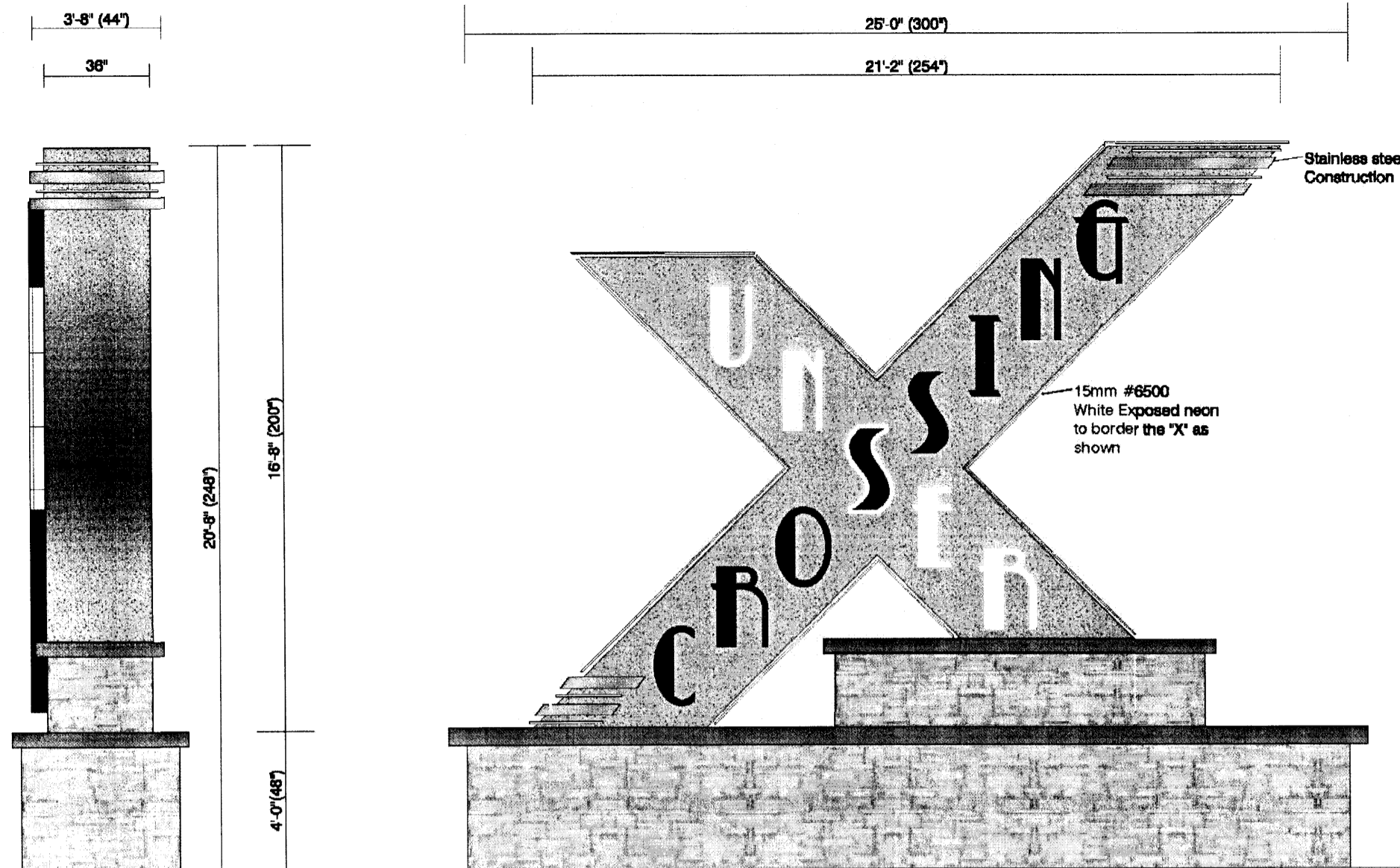
DESIGN STANDARDS

Scale \_\_\_\_\_



- Manufacture & install a single faced .125 aluminum construction monument sign.
- Center ID Pan Channel Letters with internal and halo illumination
- Install on standard steel pipes in custom engineered approved concrete footings. Electrical service to installation site should be existing, if not, will be provided by others

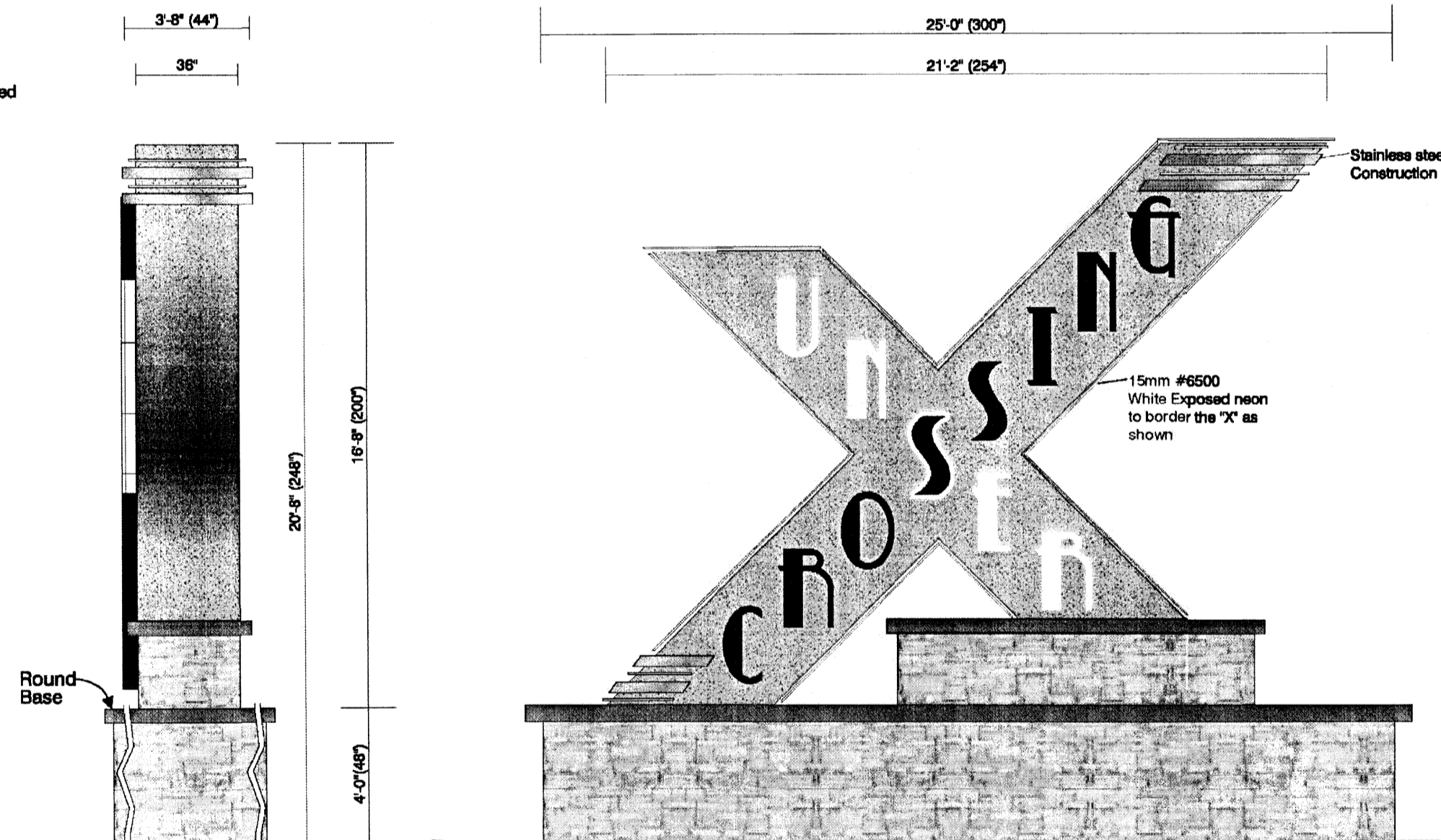
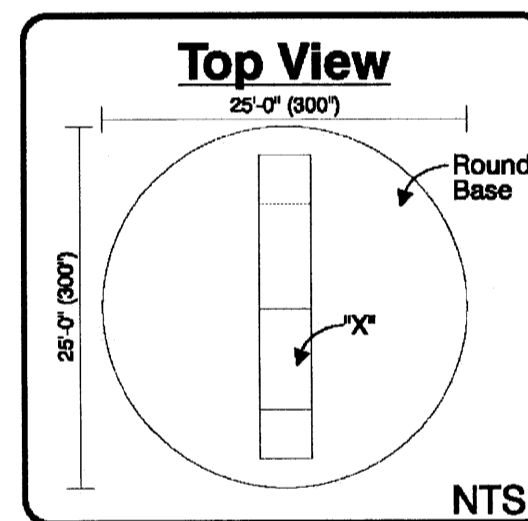
**Pan Channel Letters:**  
 3/16" Clear Lexan Backs  
 5" deep .040 Aluminum CLC returns to match face  
 1" Trim cap to match face color  
 Faces to be 3/16" White SG Acrylic with 3M HP Trans Burgundy vinyl applied  
 Illuminated with 13mm #6500 White Neon.  
 Stud mount 1" from sign face allow for halo-illumination



**A1 One(1) Single Faced Center ID Sign**  
 Scale: 1/4"=1'

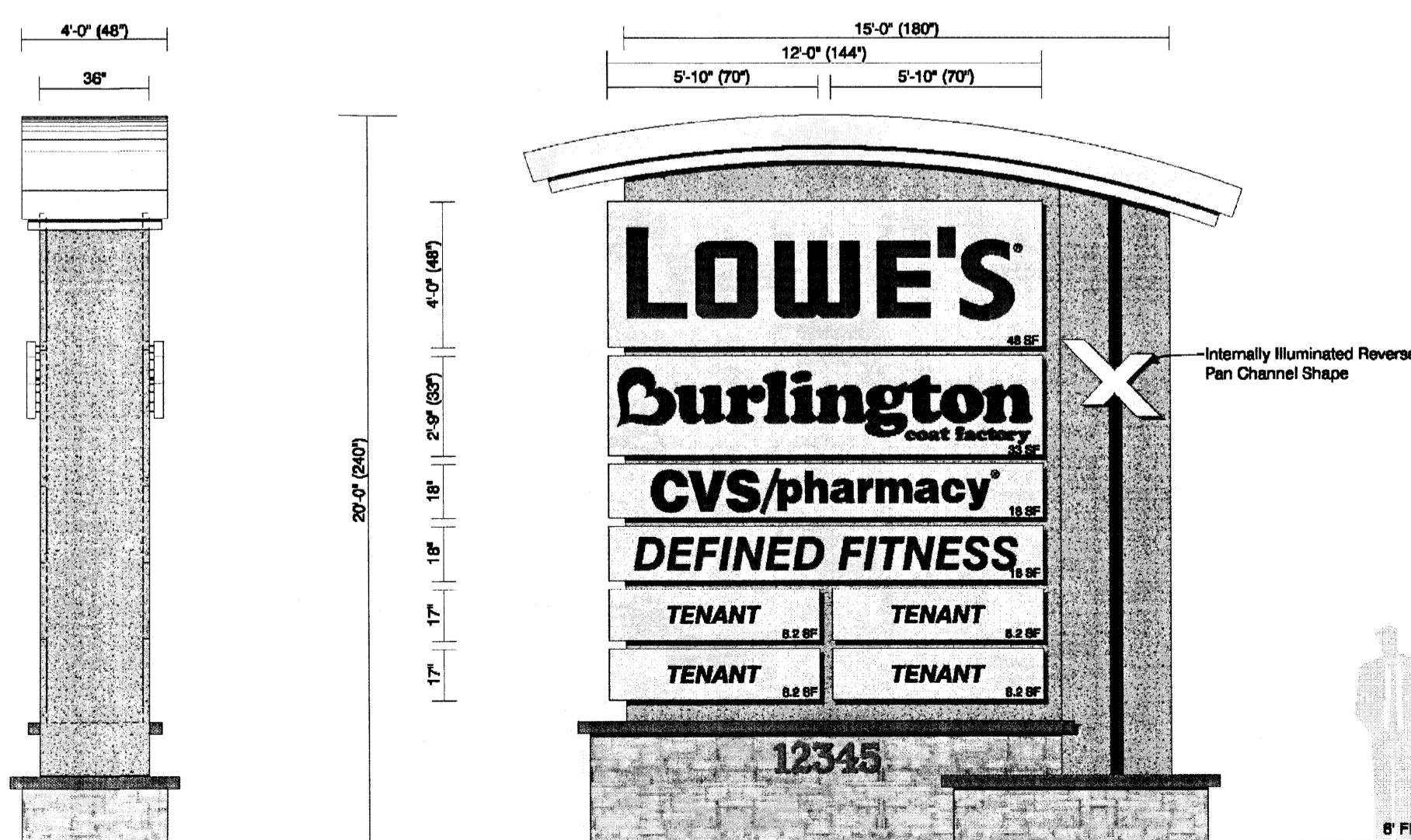
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 Stud mount 1" from sign face allow for halo-illumination



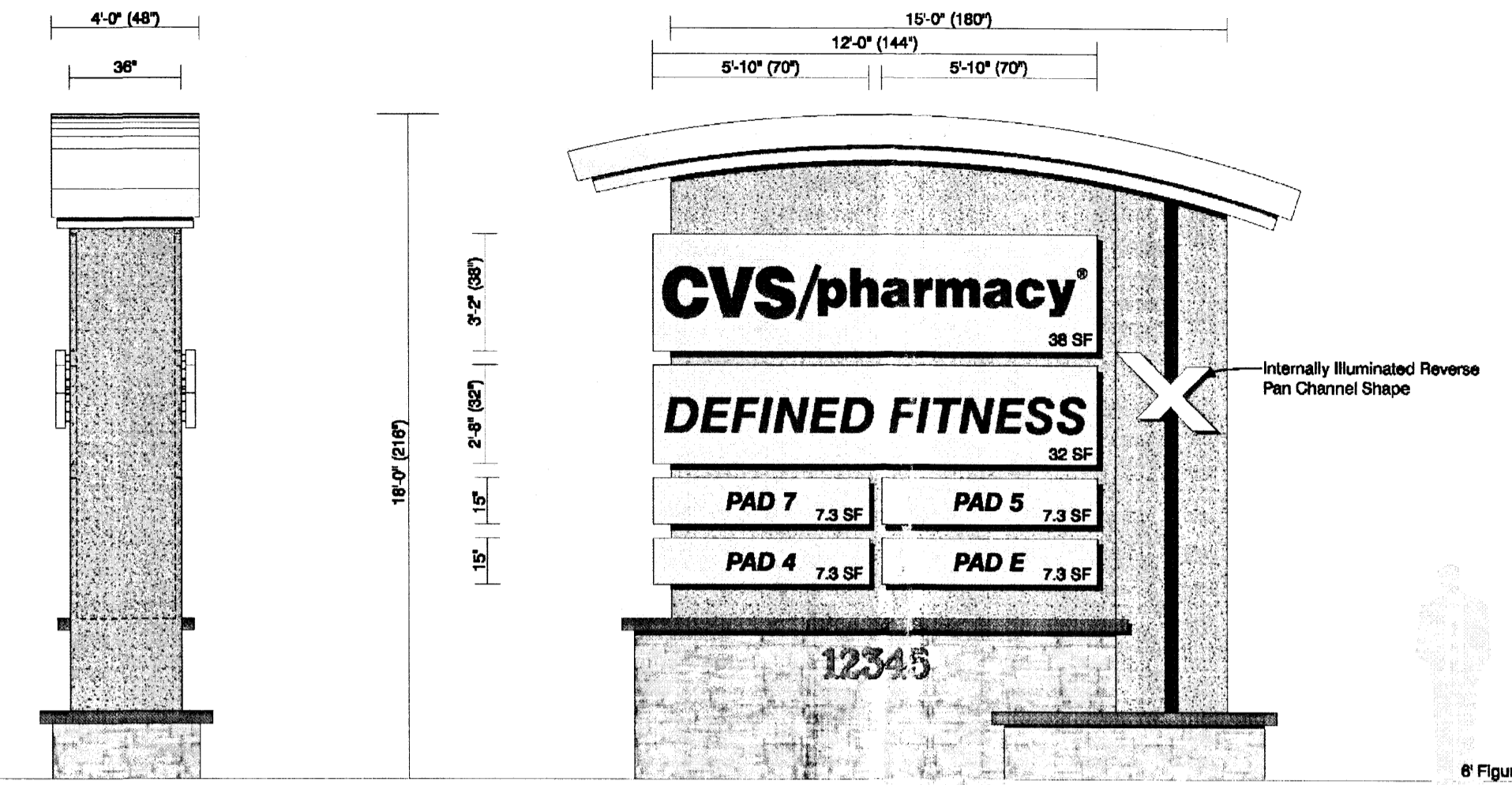
**A2 One(1) Single Faced Center ID Sign**  
 Scale: 1/4"=1'

- Manufacture & install a double faced internally illuminated aluminum monument sign.
- Sign will be illuminated with France 400 HID lamps.
- Tenant panels will be raised 2" from sign
- Install on standard steel pipes in approved concrete footings. Electrical service to installation site should be existing, if not, will be provided by others
- 25" Aluminum painted with a smooth satin finish for address numbers



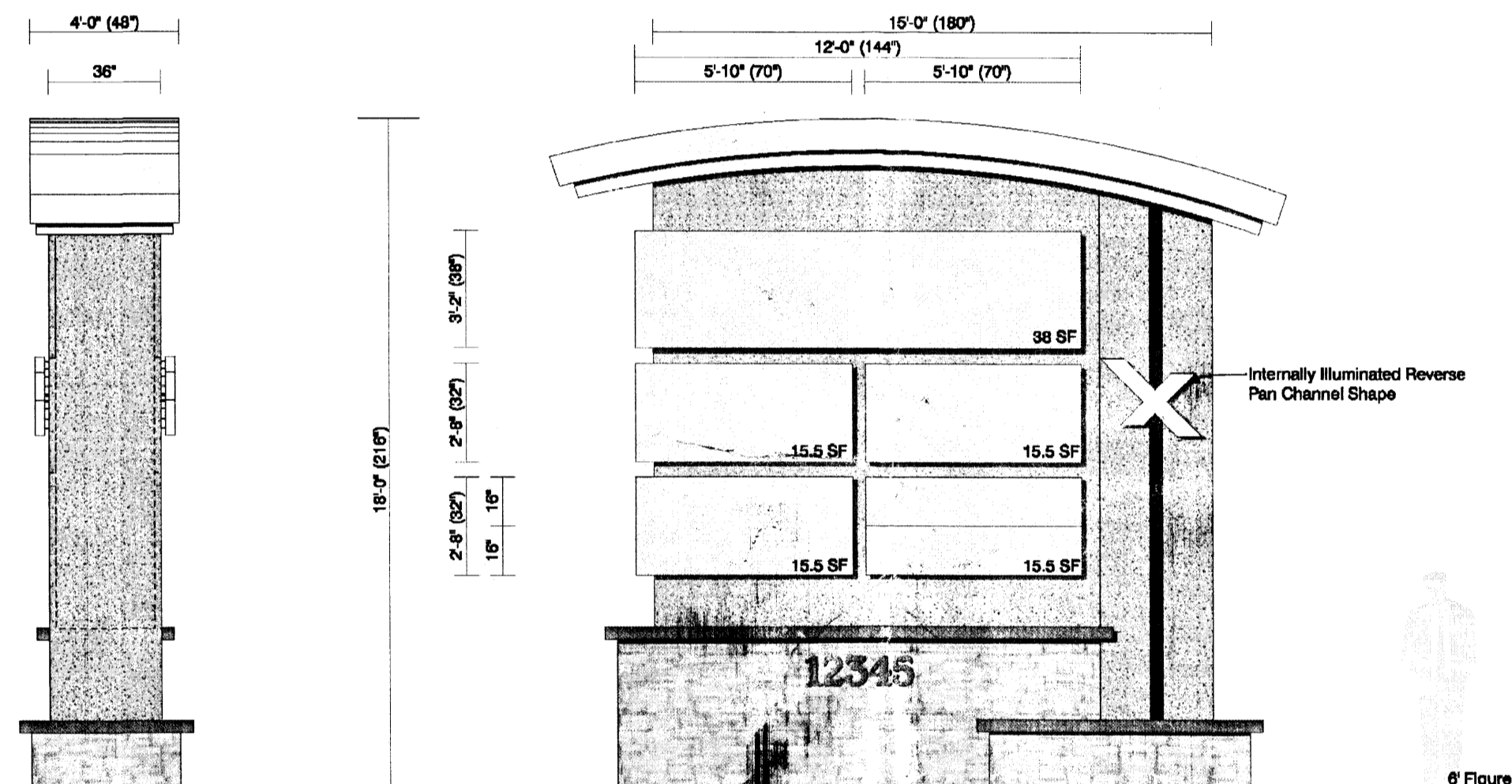
**B One(1) 20' Tall Unser Blvd Sign @ 150 sq ft w/ 8 panels**  
 Scale: 1/4"=1'

- Manufacture & install a double faced internally illuminated aluminum monument sign.
- Sign will be illuminated with France 400 HID lamps.
- Tenant panels will be raised 2" from sign
- Install on standard steel pipes in approved concrete footings. Electrical service to installation site should be existing, if not, will be provided by others
- 25" Aluminum painted with a smooth satin finish for address numbers



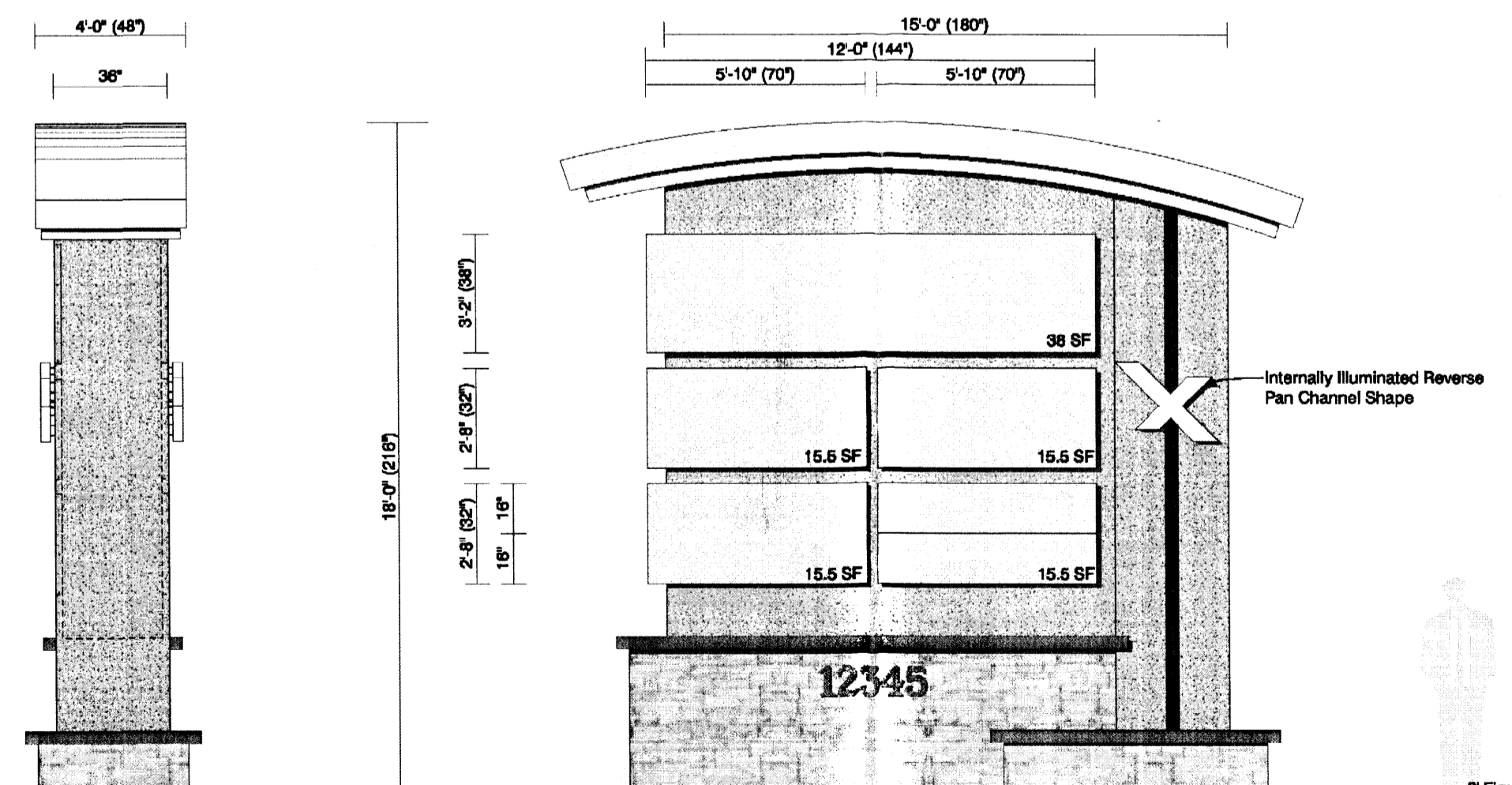
**C One(1) 18' Tall Central Ave Signs @ 100 sq ft w/ 6 panels**  
 Scale: 1/4"=1'

- Manufacture & install a double faced internally illuminated aluminum monument sign.
- Sign will be illuminated with France 400 HID lamps.
- Tenant panels will be raised 2" from sign
- Install on standard steel pipes in approved concrete footings. Electrical service to installation site should be existing, if not, will be provided by others
- 25" Aluminum painted with a smooth satin finish for address numbers



**C2 One(1) 18' Tall Central Ave Signs @ 100 sq ft w/ 7 panels**  
 Scale: 1/4"=1'

- Manufacture & install a double faced internally illuminated aluminum monument sign.
- Sign will be illuminated with France 400 HID lamps.
- Tenant panels will be raised 2" from sign
- Install on standard steel pipes in approved concrete footings. Electrical service to installation site should be existing, if not, will be provided by others
- 25" Aluminum painted with a smooth satin finish for address numbers



**C3 One(1) 18' Tall Central Ave Signs @ 100 sq ft w/ 7 panels**  
 Scale: 1/4"=1'

EL REY 1614 "RUST" SMOOTH SATIN FINISH	EL REY 827 "SCAR" MEDIUM MONTEZ FINISH	S.W. 8272 "PLUM BROOK" SMOOTH SATIN FINISH	S.W. 7177 "ROOT BEER FLOAT" SMOOTH SATIN FINISH	EL REY 1988 "WIKER FLOOR" MEDIUM MONTEZ FINISH	STONE VENEER CANYON ROCK LEDGESTONE (T.M.)
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**MATERIALS AND COLOR LEGEND**

**DISA**  
 Darren Sowell  
 ARCHITECTS  
 4700 Lincoln N.E., Suite 111  
 Albuquerque, N.M. 87109  
 Phone: (505) 342-6200  
 Fax: (505) 342-6201

**Bohannon & Huston**  
 ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**  
 Landscape Architects  
 10000 Central Ave. N.E., Suite 112  
 Albuquerque, NM 87110  
 Phone: (505) 838-6996  
 dmy@mtlbaassociates.com

**ARMSTRONG DEVELOPMENT**  
 PROPERTIES, INC.

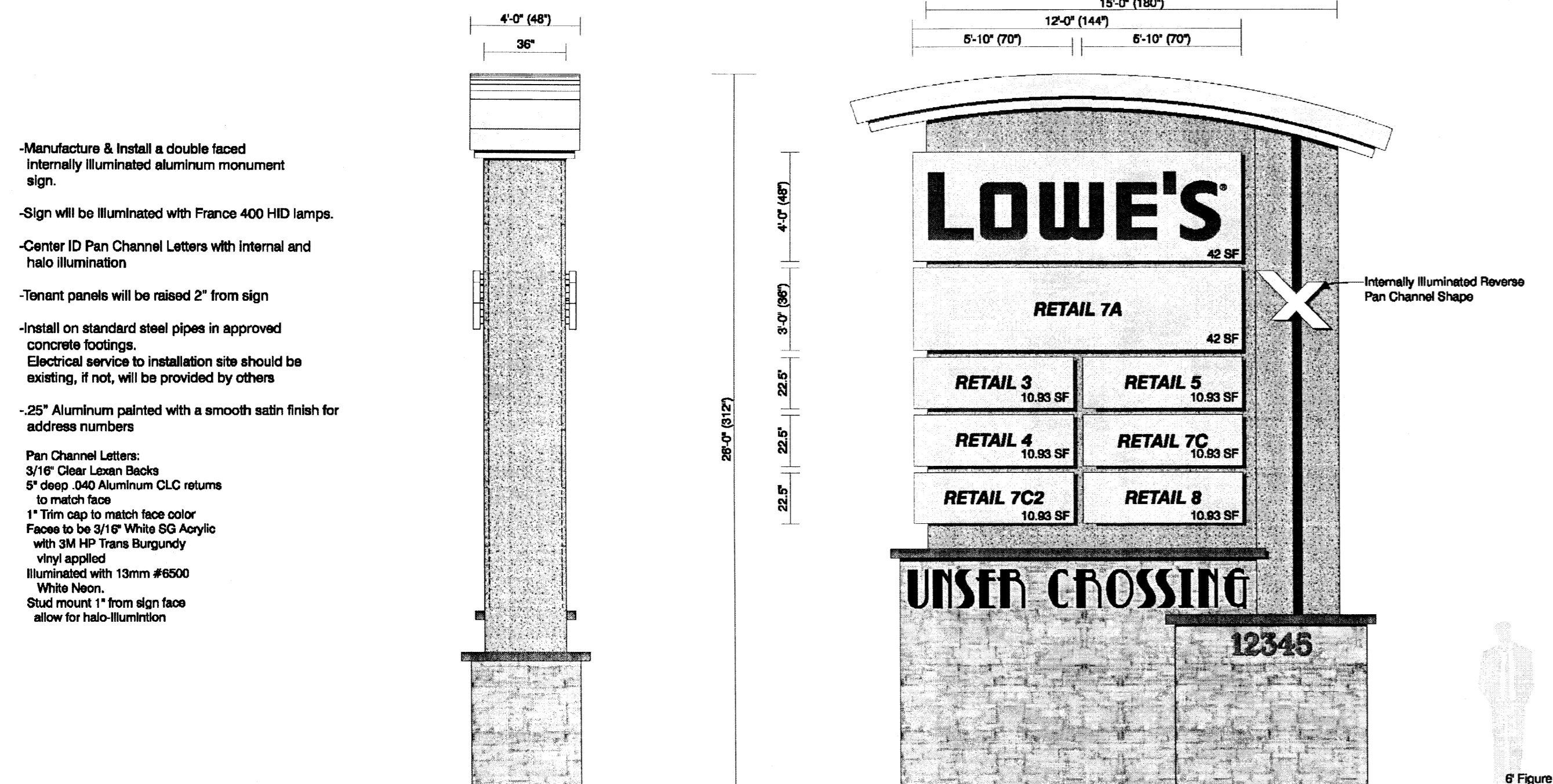
**Unser Crossing Permit Site Plan for Building**  
 Central Avenue and Unser Boulevard  
 Albuquerque, New Mexico 87121

Revised: August 29, 2008

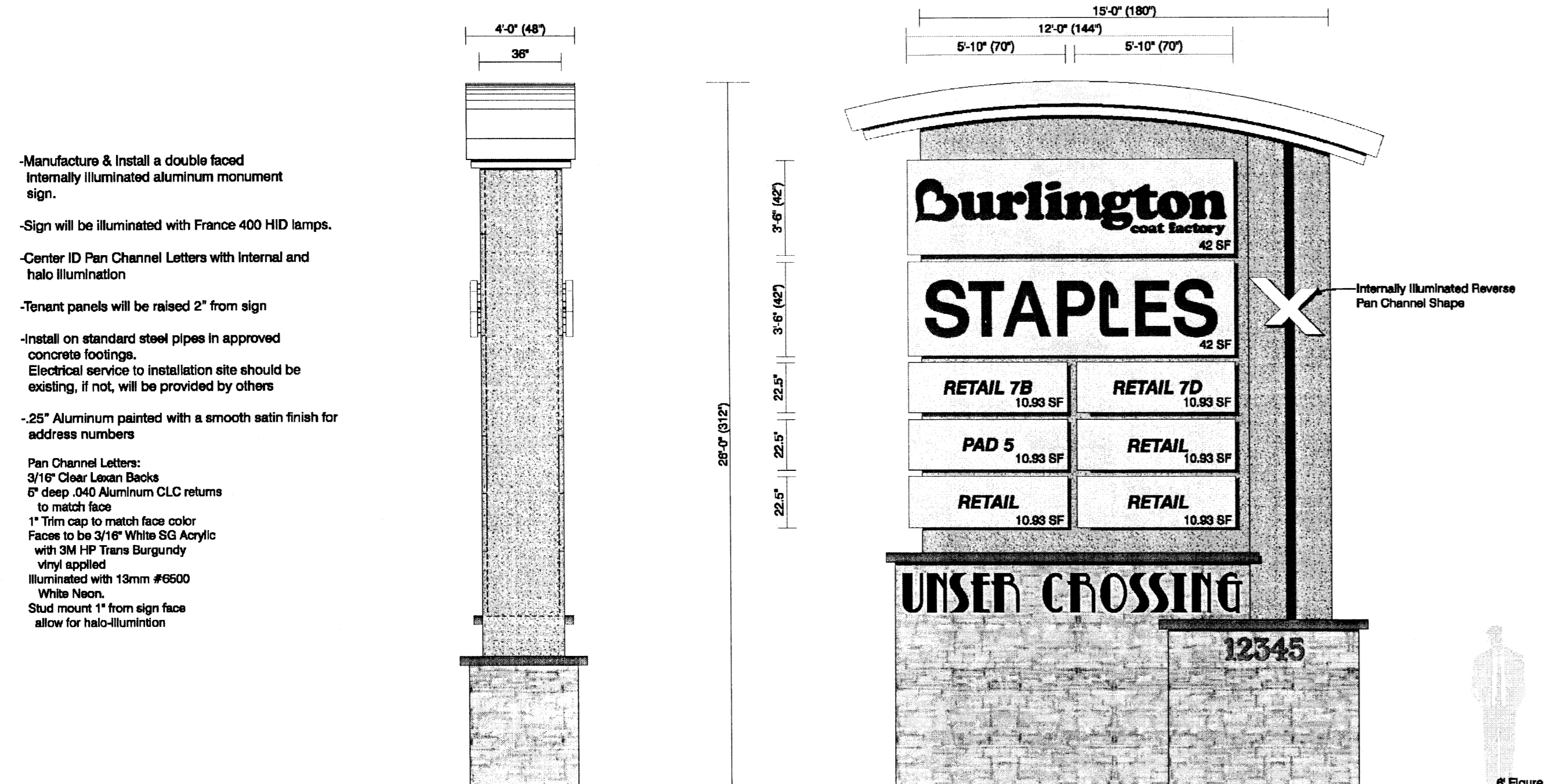
Job Number \_\_\_\_\_  
 Drawn By \_\_\_\_\_  
 Checked \_\_\_\_\_  
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SIGN STANDARDS

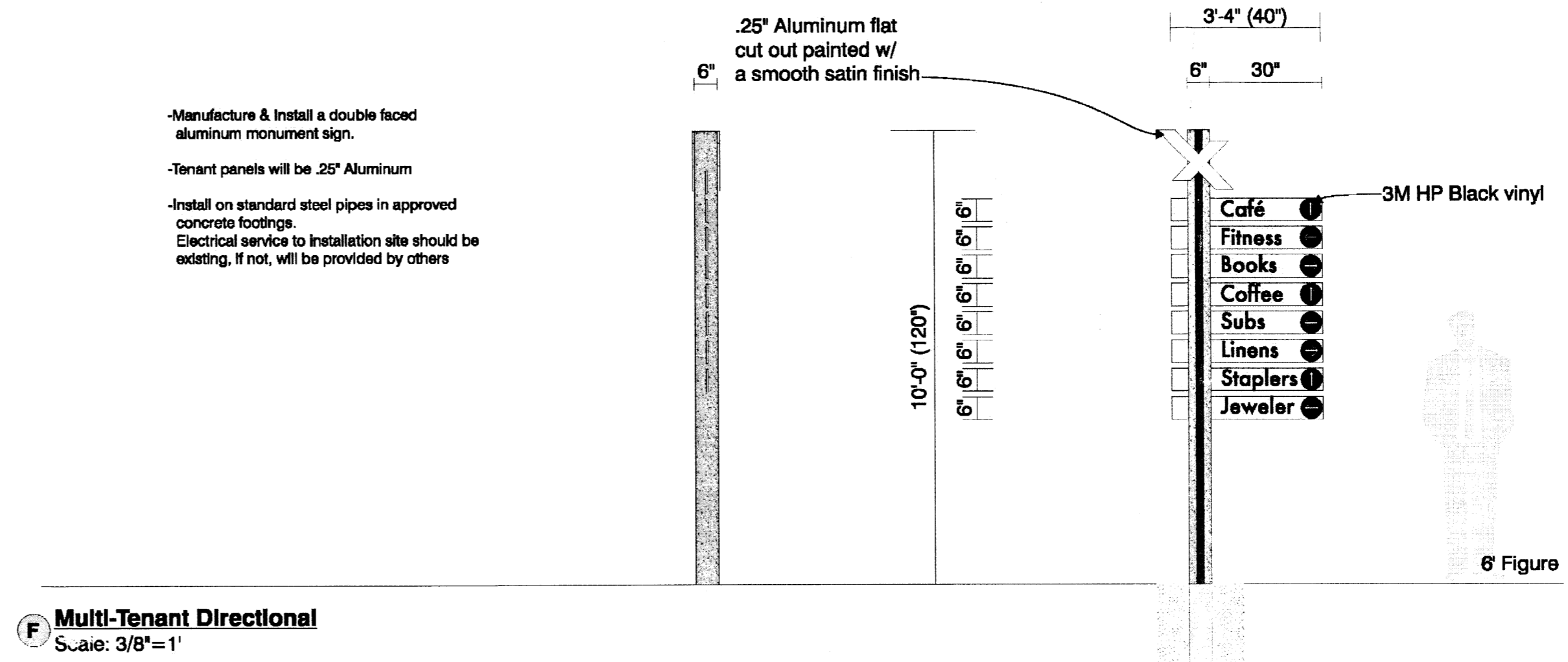
Scale \_\_\_\_\_



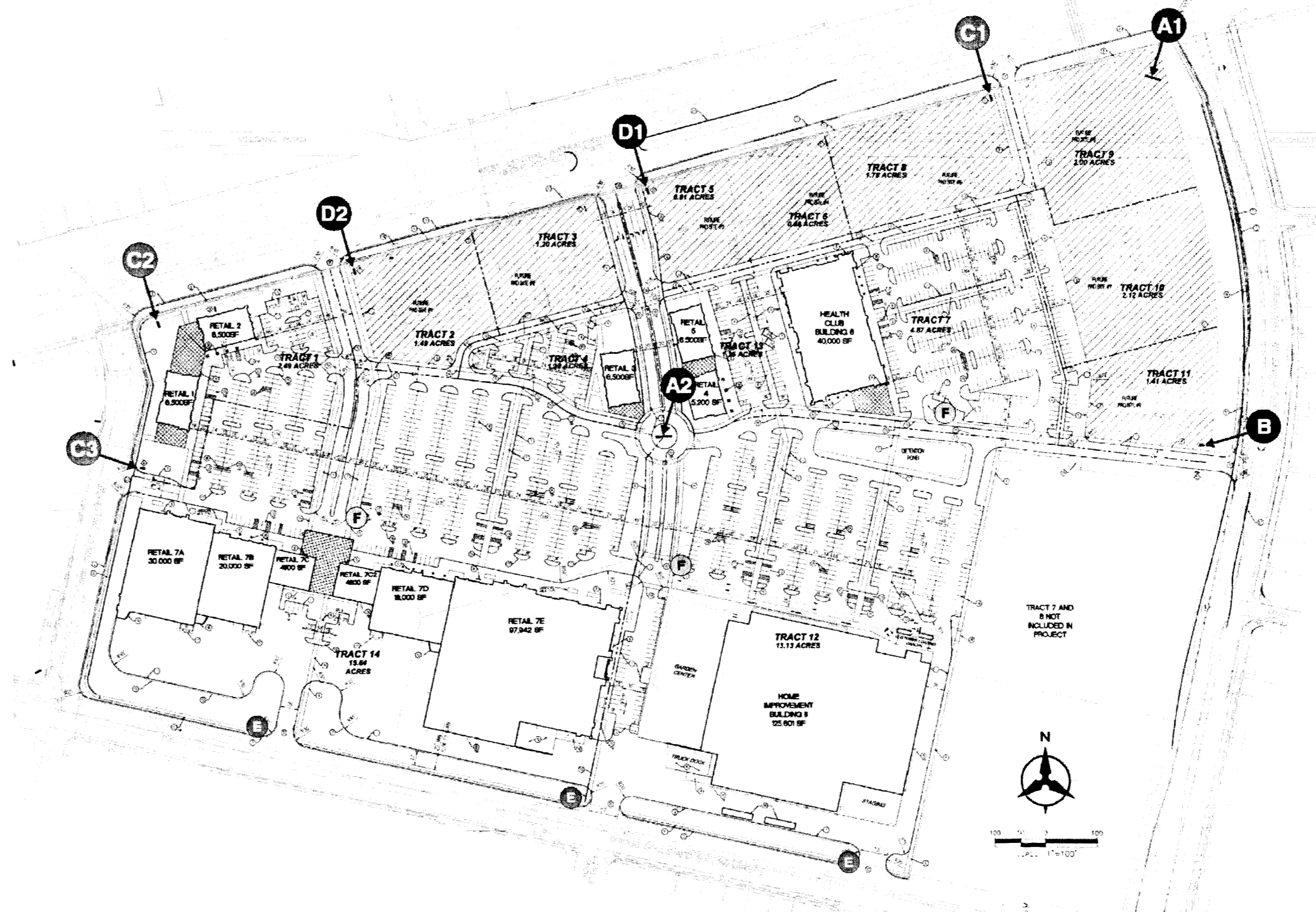
**D1 One(1) 26' Tall Central Ave Signs @ 150 sq ft w/ 8 panels**  
Scale: 1/4"=1'



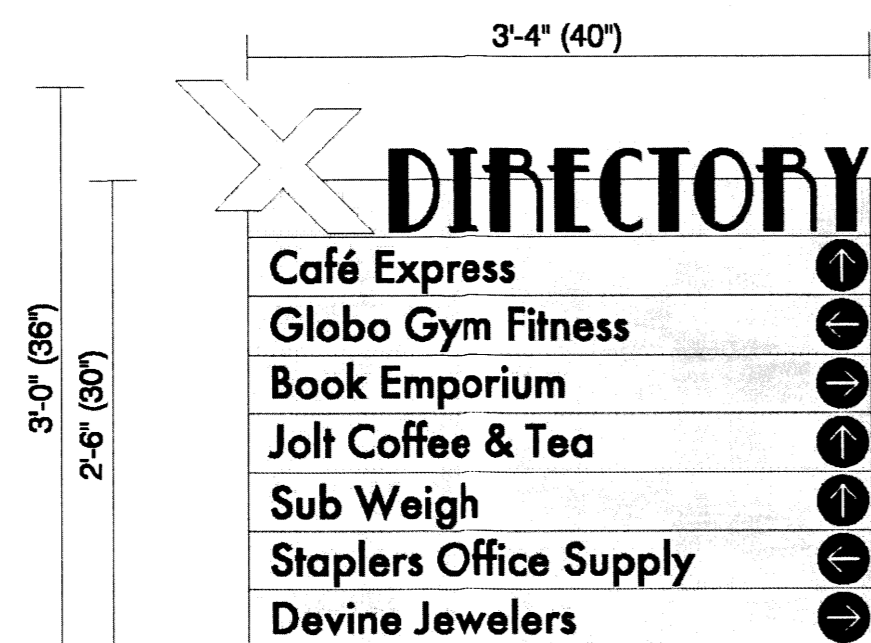
**D2 One(1) 26' Tall Central Ave Signs @ 150 sq ft w/ 8 panels**  
Scale: 1/4"=1'



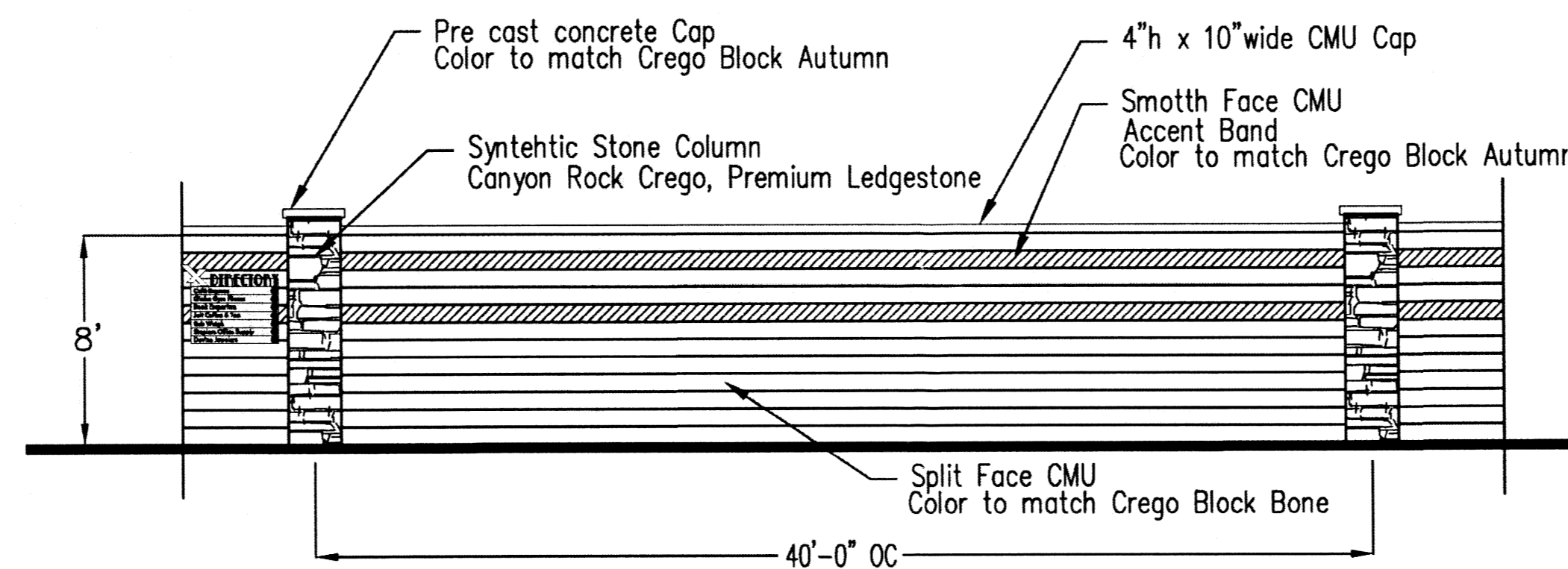
**F Multi-Tenant Directional**  
Scale: 3/8"=1'



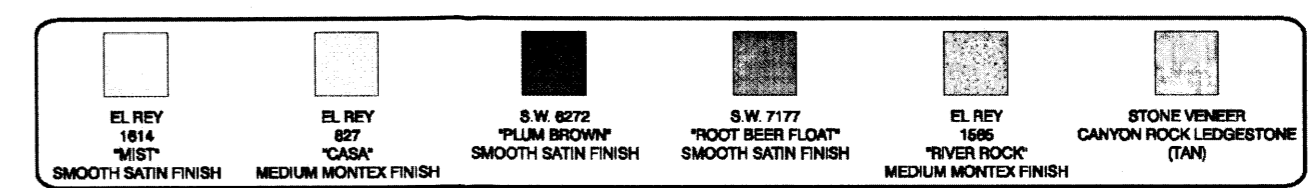
**SITE SIGNAGE LOCATION PLAN**



**E Three(3) Directory Signs @ 10 sq ft w/ 7 panels**  
Scale: 1"=1'



**TYPICAL DIRECTORY ELEVATION**



**MATERIALS AND COLOR LEGEND**

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SIGN STANDARDS

Scale



Building #	Use	Building SF	Parking Calculations										Plaza Requirements		
			Required	Transit	Required	Required	Provided	Disabled	Disabled	Motorcycle	Motorcycle	Bike Pkg	Bike Pkg	Plaza SF	Plaza SF
			Parking	Discount	Parking	Parking	Parking	Required	Provided	Required	Provided	Required	Provided	Required	Shown
1	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	2,249
2	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	5,422
3	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	130	1,489
4	Retail	6,500	33	5	28	56	72	2	4	3	4	2	5	234	2,001
5	Health Club	40,000	287	41	226	226	347	8	8	5	6	14	15	800	3,773
7A	Retail	30,000	135	21	114	114	171	4	4	3	4	5	5	15	
7B	Retail	20,000	95	15	80	80	120	4	4	3	4	5	5	15	
7C1	Retail	4,800	24	4	20	20	30	1	1	1	1	2	2	3,511	7,904
7C2	Retail	4,800	24	4	20	20	30	1	1	1	1	2	2		
7D	Retail	18,000	81	14	73	73	109	4	4	3	4	5	5	25	
7E	Retail	97,942	382	59	324	324	486	12	12	9	10	20	25	2,513	Share with site 8
8	Home Improvement	125,601	474	72	402	402	603	12	12	9	10	20	25	2,513	Share with site 8
<b>Total</b>		<b>372,343</b>	<b>1,846</b>	<b>293</b>	<b>1,393</b>	<b>1,393</b>	<b>1,848</b>	<b>89</b>	<b>81</b>	<b>29</b>	<b>32</b>	<b>86</b>	<b>95</b>	<b>7,448</b>	<b>22,838</b>

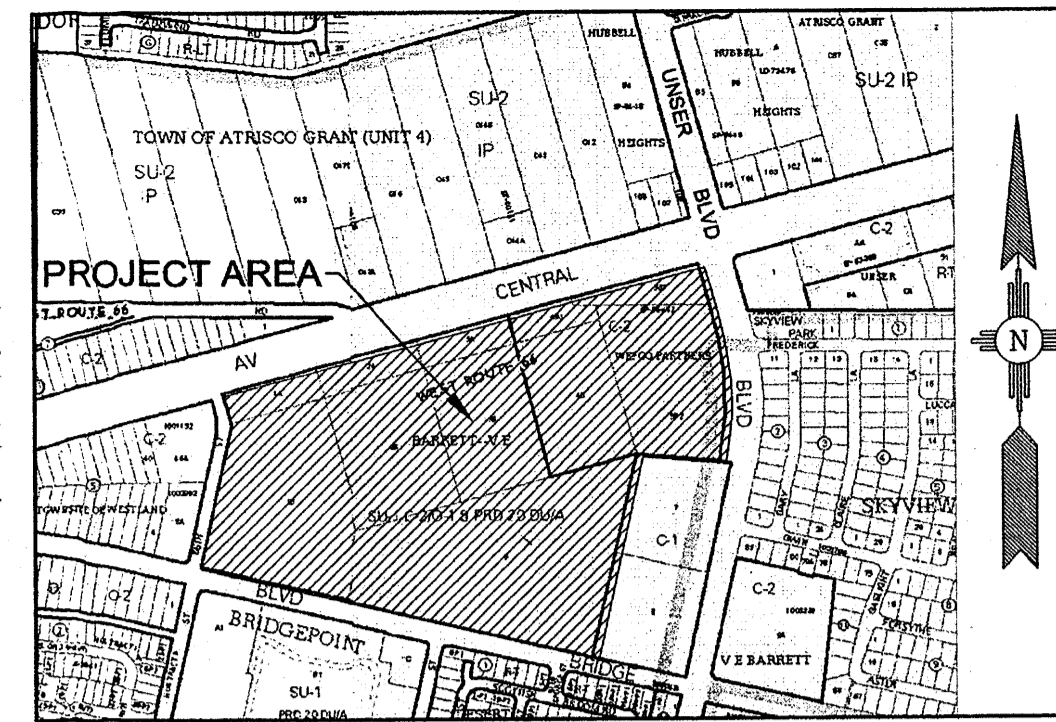
- Parking Calculation Notes:**
- Parking provided includes disabled parking spaces.
  - Required parking for Building 6 (Health Club/Gym) calculated using 1space per 150 SF. All other buildings follow section 14-16-3-1-A-27 of the City of Albuquerque zoning code.
  - Transit discount of 15% taken due to the fact that the site is adjacent to a regular Albuquerque Transit system route and two transit rider shelters will be built with this project.
  - First column of required parking is without transit discount.
  - Provided parking is divided into logical grouping of buildings to allow users to park once and visit multiple retailers.

**MONUMENT SIGNS\***

TYPE
A
B
C
D
E
F
G

\*ALL ON-SITE SIGNAGE HAS BEEN SUBMITTED TO EPC (OBEP-40063) AND WILL RETURN SEPARATELY TO DRB.

- TRANSIT DEPARTMENT NOTE:**
- THE SW MESA PARK & RIDE IS LOCATED NEAR THE NW CORNER OF CENTRAL & UNSER.
  - AS OF 3/15/08, BUS ROUTES 766, 162, 66, AND 54 HAVE A STOP NEAR OR ADJACENT TO THE SITE.
  - PARTICIPATION IN TRANSIT DEMAND MANAGEMENT PROGRAM IS ENCOURAGED FOR USERS OF THE SITE.



**VICINITY MAP**  
ZONE ATLAS PAGES K-09-Z & K-10-Z

**SHEET INDEX**

C-001	SITE PLAN FOR BUILDING PERMIT	A-107B	ELEVATIONS BUILDING 7
C-002	SITE PLAN DETAILS	A-107C	ELEVATIONS BUILDING 7
LS-101	LANDSCAPE PLAN	A-107D	ELEVATIONS BUILDING 7
LS-102	LANDSCAPE DETAILS	A-107E-1	ELEVATIONS BUILDING 7
C-101	CONCEPTUAL GRADING & DRAINAGE PLAN	A-107E-2	ELEVATIONS BUILDING 7
C-201	CONCEPTUAL UTILITY PLAN	A-108A	ELEVATIONS BUILDING 8
A-101	ELEVATIONS BUILDING 1	A-108B	ELEVATIONS BUILDING 8
A-102	ELEVATIONS BUILDING 2	A-108C	ELEVATIONS BUILDING 8
A-103	ELEVATIONS BUILDING 3		
A-104	ELEVATIONS BUILDING 4	C-003	SITE PLAN FOR SUBDIVISION
A-105	ELEVATIONS BUILDING 5	G-101	DESIGN STANDARDS
A-105A	ELEVATIONS BUILDING 5	G-102	DESIGN STANDARDS
A-105B	ELEVATIONS BUILDING 5	G-103	DESIGN STANDARDS
A-107A	ELEVATIONS BUILDING 7		

**KEYED NOTES**

- 6" CONCRETE CURB.
- COA STANDARD CURB & GUTTER PER COA STD DWG 2415A. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- 6" TEMPORARY ASPHALT CURB.
- 14" WIDE CONCRETE SIDEWALK.
- 6" WIDE CONCRETE SIDEWALK PER COA STD DWG 2450. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- 8" WIDE CONCRETE SIDEWALK.
- 10" WIDE CONCRETE SIDEWALK. SLOPE TO LANDSCAPE AREA IF POSSIBLE.
- 6"x8" LANDSCAPE PLANTER.
- BIKE RACK PER 3/C-002.
- LIGHT POLE PER 5/C-002.
- REFUSE ENCLOSURE PER 2/C-002 & 9/C-002.
- 6" HIGH SCREEN WALL PER 9/C-002.
- HANDICAP PARKING SPACE PER 4/C-002.
- TRANSIT STOP WITH SHELTER PER COA STD DWG 2535. PEDESTRIAN AMENITIES SHALL BE PER SECTION 11 OF THE SITE PLAN FOR SUBDIVISION.
- COLORLED, TEXTURED, CONCRETE CROSSWALK.
- LOADING DOCK.
- END ISLAND PLANTER PER 1/C-002.
- 10' WIDE CONTRACTOR PARKING SPACES. TOTAL OF 24.
- TRASH COMPACTOR.
- NEW DRIVEWAY, FULL ACCESS. NEW SIGNAL RECOMMENDED BY TIA. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW RIGHT IN/RIGHT OUT DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW FULL ACCESS DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW RIGHT IN, RIGHT OUT, LEFT IN DRIVEWAY. DRIVEWAY PER COA STD DWG 2426. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- CART CORRAL, WELDED METAL PIPE, PAINTED TO MATCH BUILDING.
- MOTORCYCLE PARKING WITH SIGN PER 8/C-002.
- WALL MOUNTED LIGHT.
- PROPOSED DECEL LANE/TURN LANE TO BE COMPLETED WITH PUBLIC INFRASTRUCTURE PLANS.
- APPLIANCE AND PALLET RECYCLING STORAGE UNIT.
- CROSS ACCESS EASEMENT TO BE CREATED BY PLATTING ACTION WITH THIS PROJECT.
- COLUMN. SEE ARCHITECTURAL ELEVATION.
- 8" HIGH SCREEN WALL BEHIND LOADING DOCKS PER 9/C-002.
- PROPOSED FIRE HYDRANT LOCATION.
- EXISTING BUS STOP LOCATION.
- FUTURE PEDESTRIAN CONNECTION TO ADJACENT PAD SITES.
- ADA ACCESSIBLE RAMP AND STEPS FOR PEDESTRIAN CONNECTION.
- NEW HANDICAP RAMP PER COA STD DWG 2441. TO BE COMPLETED AS A PUBLIC WORK ORDER.
- NEW WHEELCHAIR RAMP PER COA STD DWG 2426. MAX SLOPE IS 12:1.

PROJECT NUMBER: 1007204  
Application Number: 08 DRB-7-0276

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated May 15, 2008 and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? (X) Yes ( ) No. If yes, then a set of approved DRB plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

**DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVALS:**

Traffic Engineering, Transportation Division  
*Royce A. Green* 7/9/08  
Date: 7/9/08

Water Utility Department  
*Christie Sandoral* 7/6/08  
Date: 7/6/08

Parks and Recreation Department  
*Bradley A. Biggs* 9/18/08  
Date: 9/18/08

City Engineer  
*Michael Holton* 7/15/08  
Date: 7/15/08

Solid Waste Management  
*Joel Wood* 7/18/08  
Date: 7/18/08

DRB Chairperson, Planning Department

**DISIA**  
Darren Sowell  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

**Bohannon & Huston**  
Engineers  
1700 Jefferson St. NE Albuquerque, NM 87102-3335  
ENGINEERING SPATIAL DATA ADVANCED TECHNOLOGIES

**Mitchell Associates, LLC**

Landscape Architects  
Cable Blvd. NE, Suite 112  
Albuquerque, NM 87110  
Phone: (505) 839-6996  
dany@mitchellassociates.com

**ARMSTRONG DEVELOPMENT PROPERTIES, INC.**

**Unser Crossing DRB Submittal**  
**Site Plan for Building Permit**  
**Central Avenue and Unser Boulevard**  
**Albuquerque, New Mexico 87121**

Rev. 1 June 24, 2008  
Rev. 2 July 8, 2008

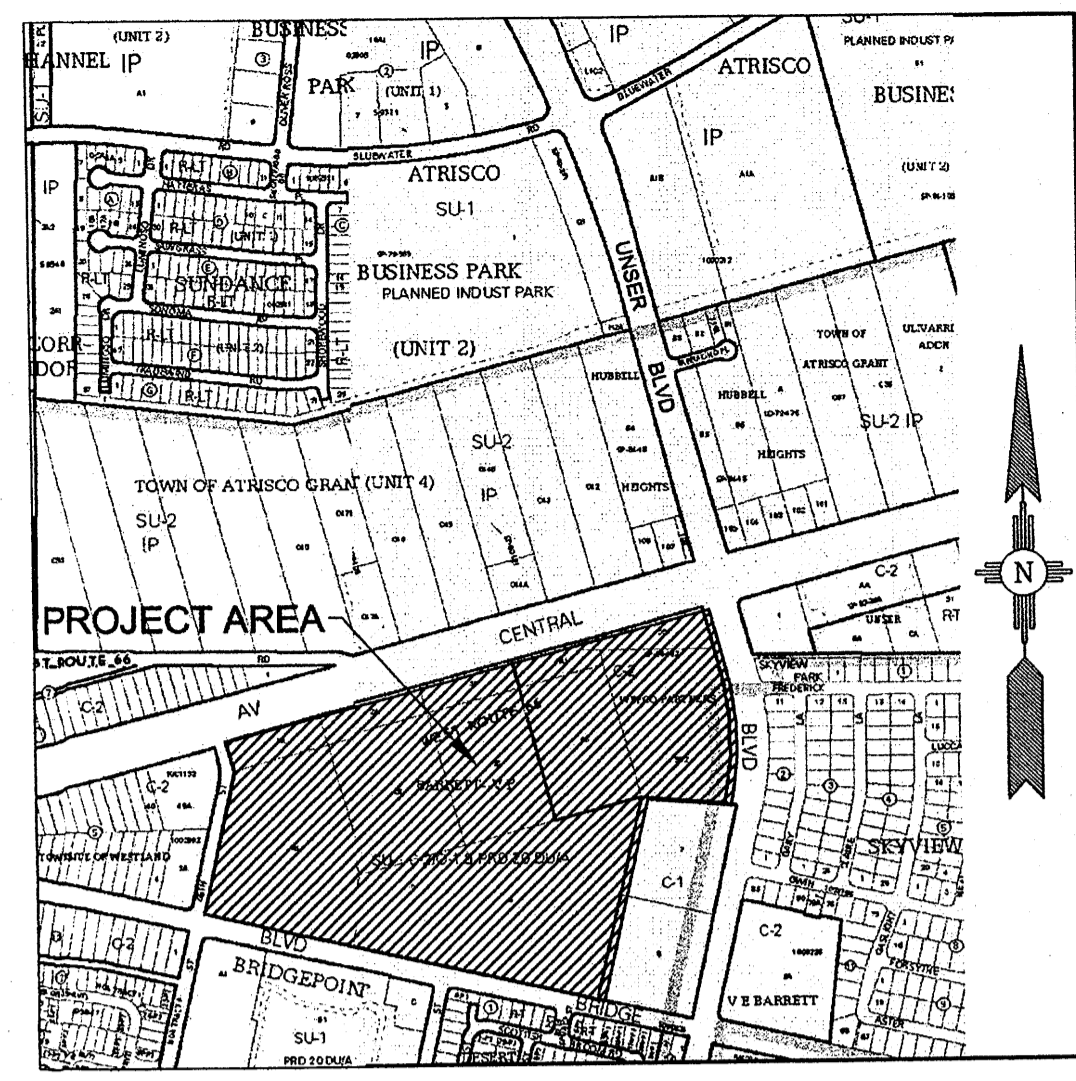
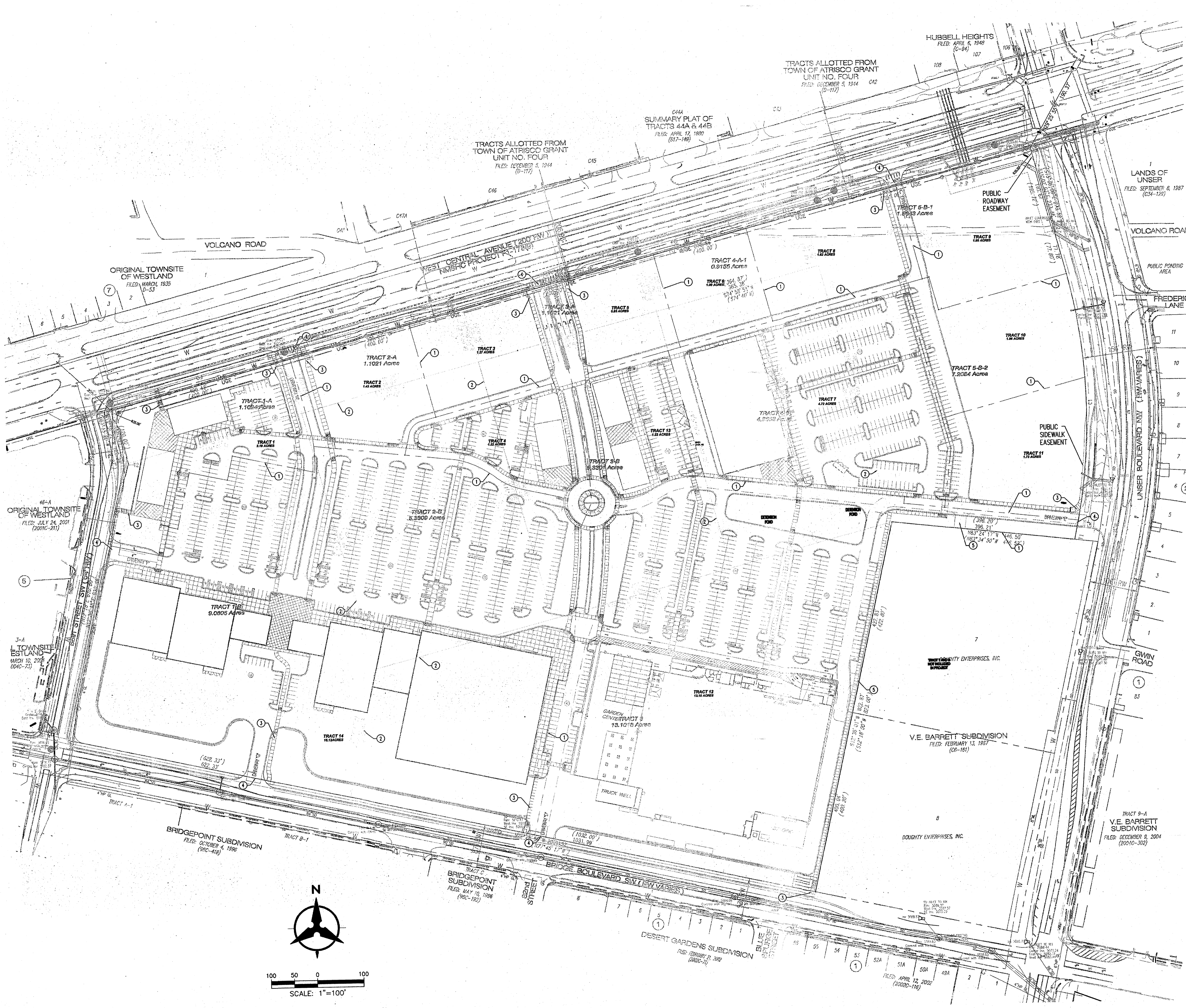
Job Number  
Drawn By BO  
Checked RM/BJS  
Issue Date June 17, 2008

**SITE PLAN FOR BUILDING PERMIT**

Scale

**C-001**

00 OF 00



VICINITY MAP  
ZONE ATLAS PAGES K-09-Z & K-10-Z

**SHEET INDEX**

- C-003 SITE PLAN FOR SUBDIVISION
- G-101 DESIGN STANDARDS
- G-102 DESIGN STANDARDS
- G-103 DESIGN STANDARDS

\* SEE SITE PLAN FOR BUILDING PERMIT PACKAGE FOR UTILITY PLAN

**KEYED NOTES**

1. NEW PROPERTY LINE.
2. EXISTING PROPERTY LINE TO BE ELIMINATED BY PLATTING ACTION WITH THIS PROJECT.
3. LOCATION OF PEDESTRIAN INGRESS OR EGRESS.
4. LOCATION OF VEHICULAR INGRESS OR EGRESS.
5. CROSS ACCESS EASEMENT TO BE CREATED BY PLATTING ACTION WITH THIS PROJECT.

**NOTE**

1. SEE DESIGN STANDARDS FOR BUILDING HEIGHT, SETBACK, AND FAR REQUIREMENTS.
2. FUTURE PHASES SHALL FOLLOW DESIGN GUIDELINES INCLUDED IN THE SITE PLAN FOR SUBDIVISION TO ENSURE A CONSISTENT LEVEL OF DEVELOPMENT THROUGHOUT UNSER CROSSING. SITE PLANS FOR BUILDING PERMIT WHICH ARE IN COMPLIANCE WITH THE DESIGN GUIDELINES MAY BE DELEGATED TO THE DEVELOPMENT REVIEW BOARD AFTER EPC APPROVES THE FIRST PAD SITE.
3. MAINTENANCE AGREEMENT BETWEEN THE CITY OF ALBUQUERQUE & OWNER SHALL BE SIGNED PER LARGE RETAIL FACILITY ORDINANCE.
4. A BLANKET CROSS ACCESS AND CROSS DRAINAGE EASEMENT WILL BE GRANTED BY PLATTING ACTION WITH THIS PROJECT.

PROJECT NUMBER: 1007204

Application Number: \_\_\_\_\_  
This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated \_\_\_\_\_ and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? ( ) Yes ( ) No If yes, then a set of approved DRG plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:	
<i>[Signature]</i>	10-07-08
Traffic Engineering, Transportation Division	Date
<i>[Signature]</i>	10-1-08
Water Utility Department	Date
<i>[Signature]</i>	10-1-08
Parks and Recreation Department	Date
<i>[Signature]</i>	10/1/08
City Engineer	Date
<i>[Signature]</i>	10/1/08
Solid Waste Management	Date
DRB Chairperson, Planning Department	Date

**DSA**  
Darren Sowell  
ARCHITECTS  
4700 Lincoln N.E., Suite 111  
Albuquerque, N.M. 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

**Bohannon & Huston**  
Civil Engineers  
4700 Lincoln N.E., Suite 111  
Albuquerque, NM 87109  
Phone: (505) 342-6200  
Fax: (505) 342-6201

**ARMSTRONG DEVELOPMENT**  
PROPERTIES, INC.

**Unser Crossing  
DRB Submittal  
Site Plan for Subdivision  
Central Avenue and Unser Boulevard  
Albuquerque, New Mexico 87121**

Rev. 1	June 24, 2008
Rev. 2	July 8, 2008
Rev. 3	Oct 1, 2008

Job Number	
Drawn By	BO
Checked	RM/BSJ
Issue Date	June 17, 2008

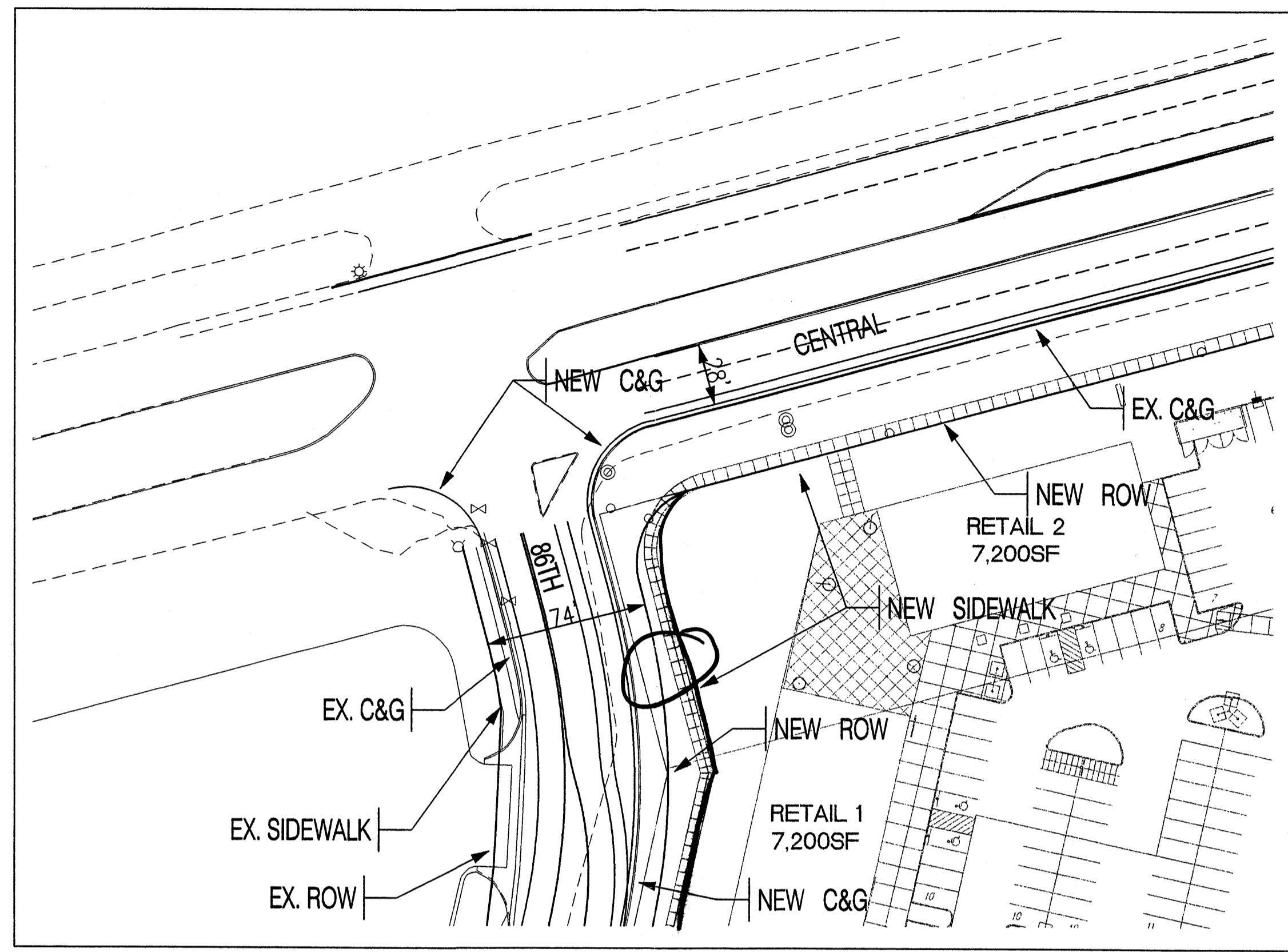
**SITE PLAN FOR SUBDIVISION**

Scale \_\_\_\_\_

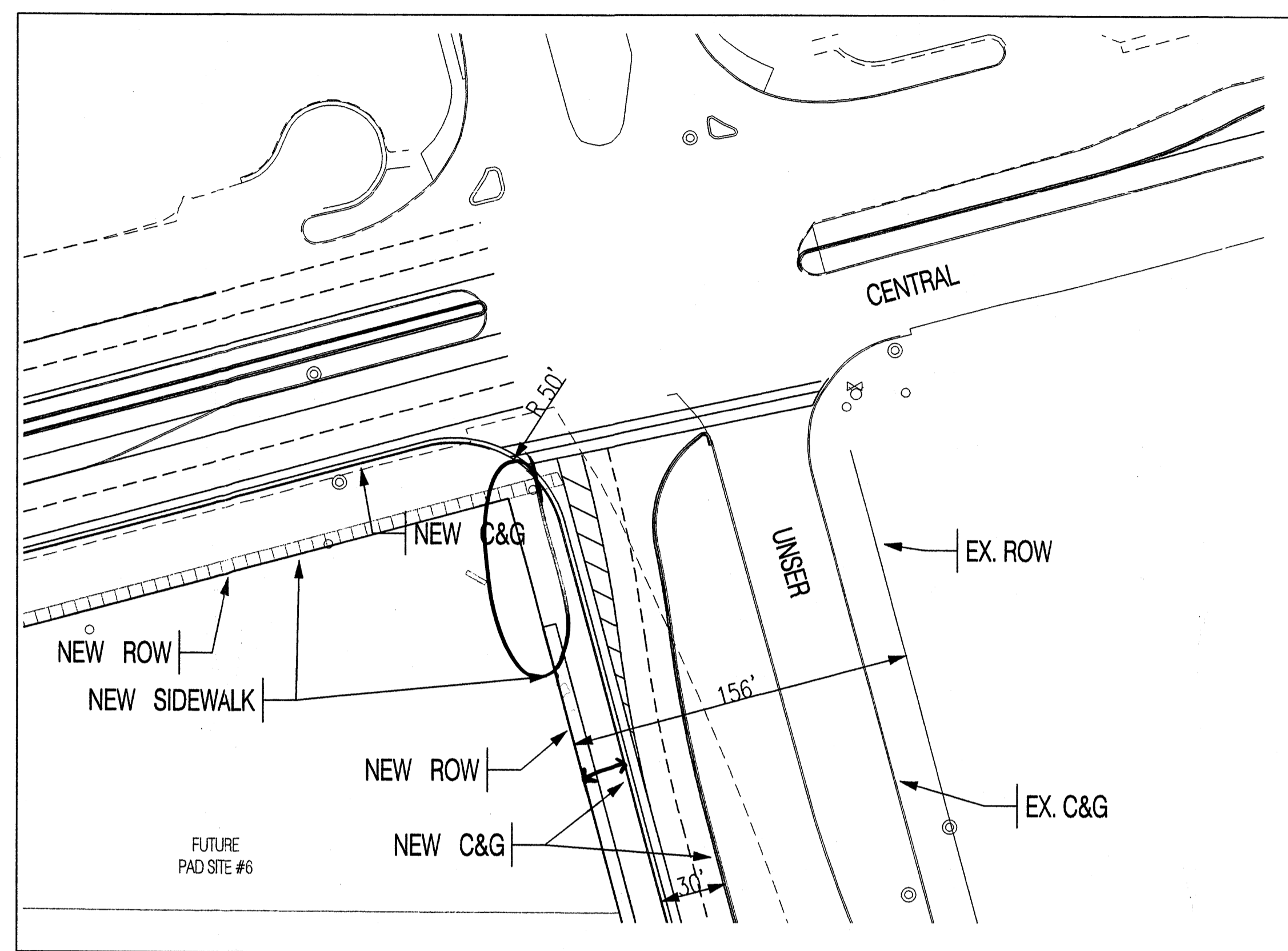
**C-003**

8/6/08

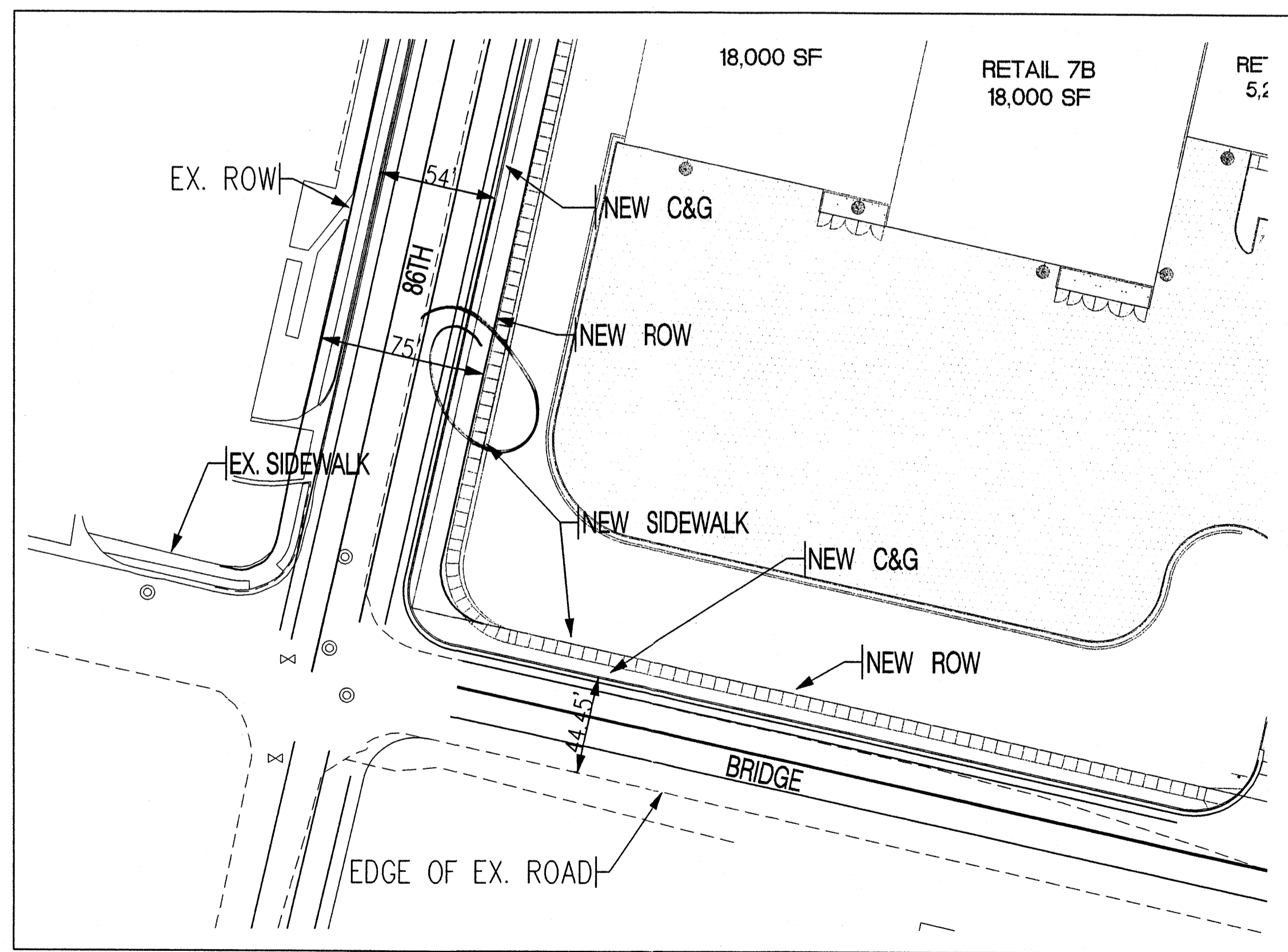
# 1007204 UNSER CROSSING RIGHT OF WAY EXHIBIT



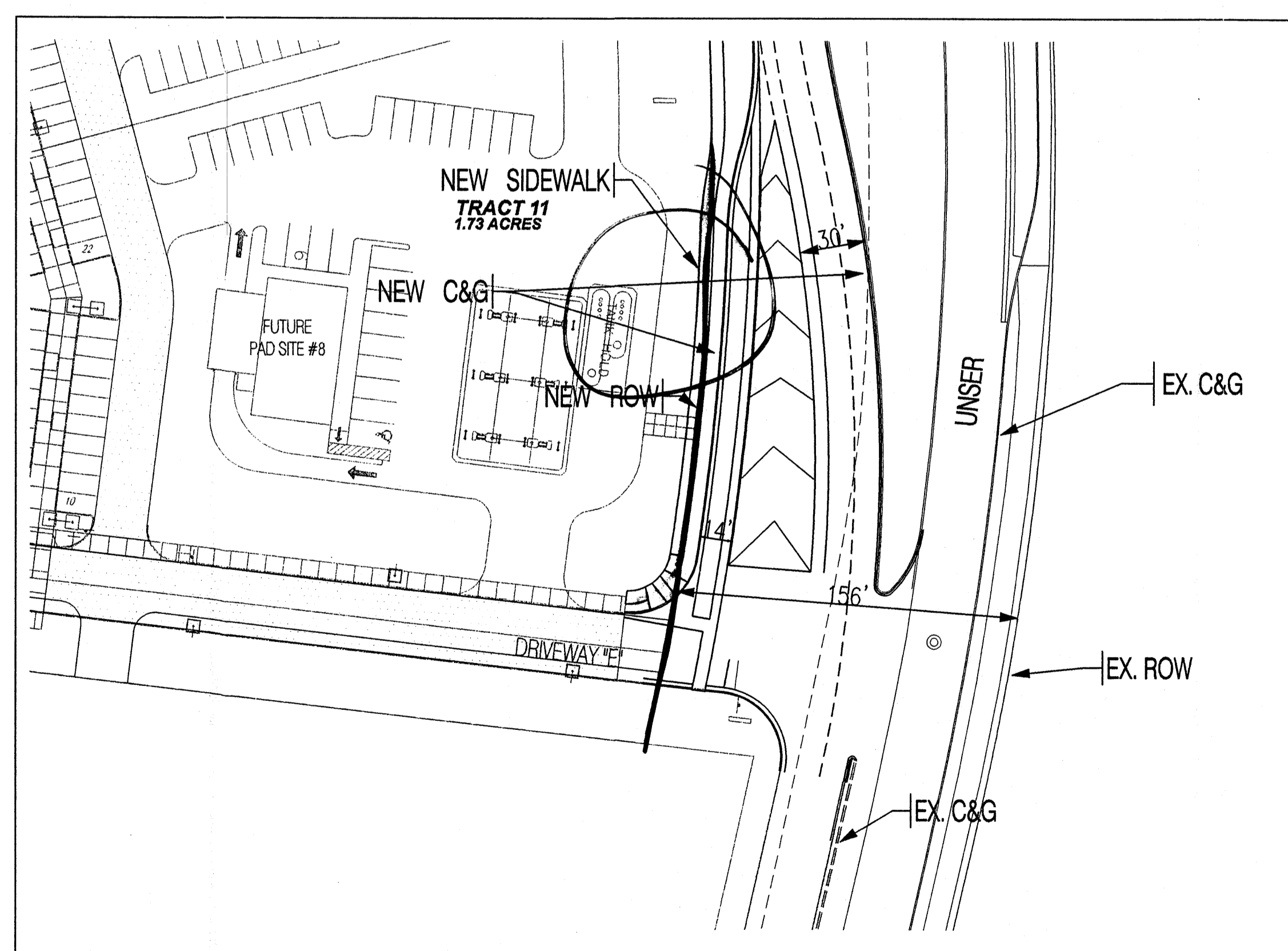
CENTRAL & 86TH



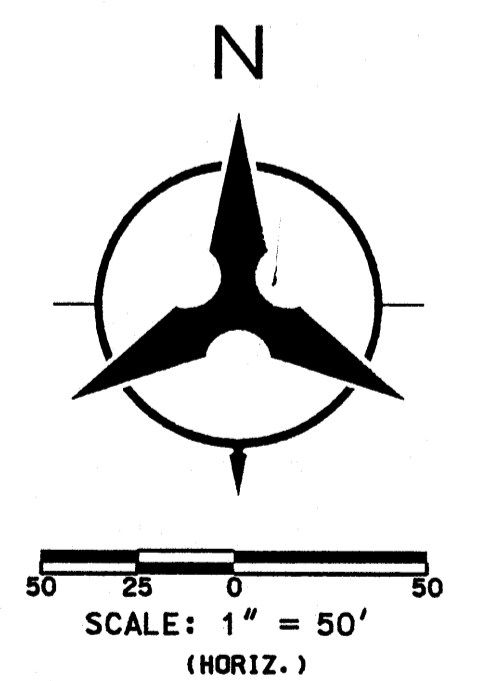
UNSER & CENTRAL



BRIDGE & 86TH



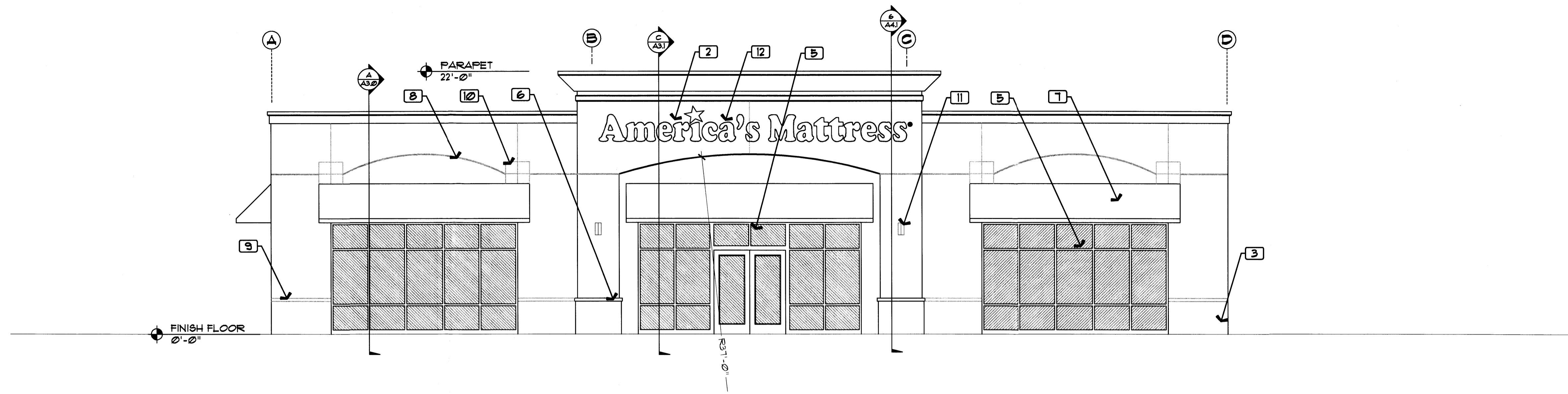
UNSER & DRIVEWAY "E"



ALBUQUERQUE, NEW MEXICO  
AUGUST, 2008

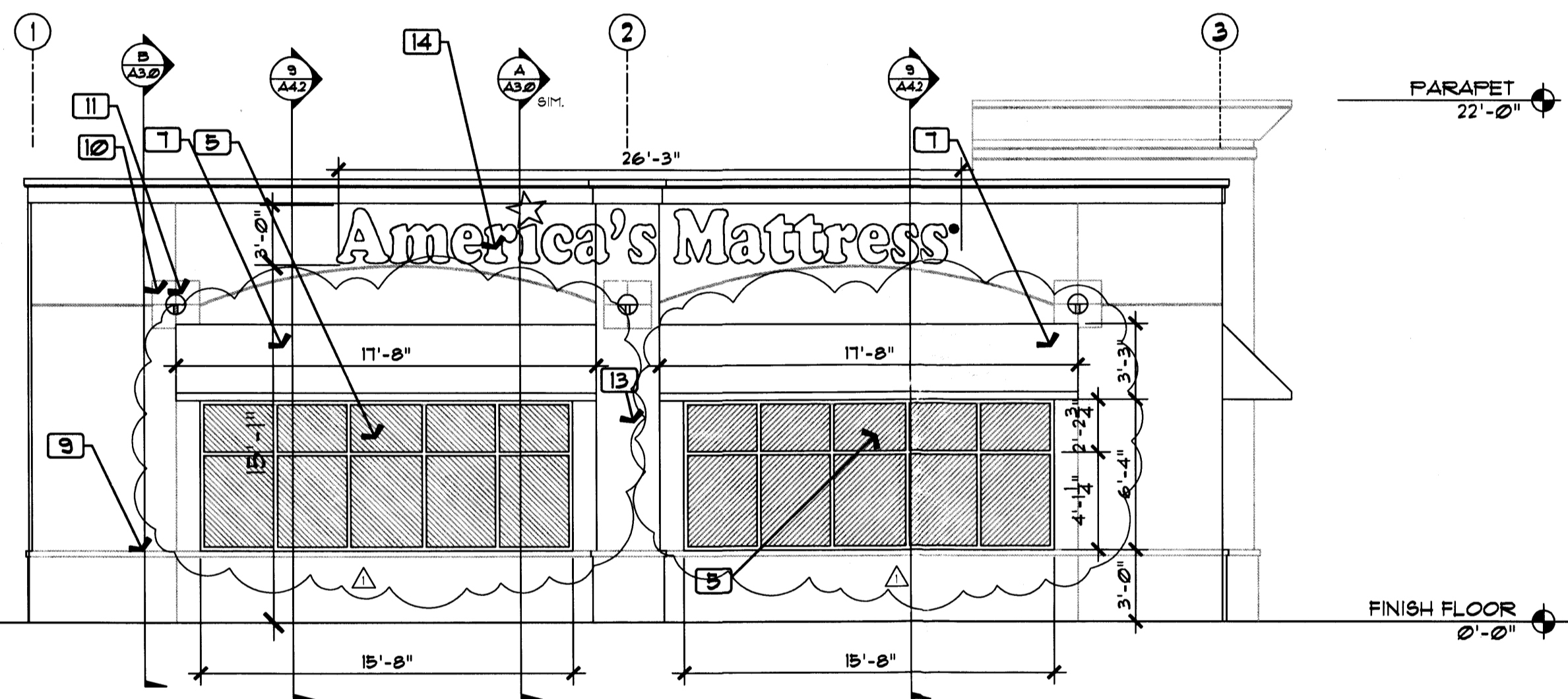
## Bohannon Huston

Courtyard | 7500 Jefferson St. NE Albuquerque, NM 87109-4335  
ENGINEERING • SPATIAL DATA • ADVANCED TECHNOLOGIES



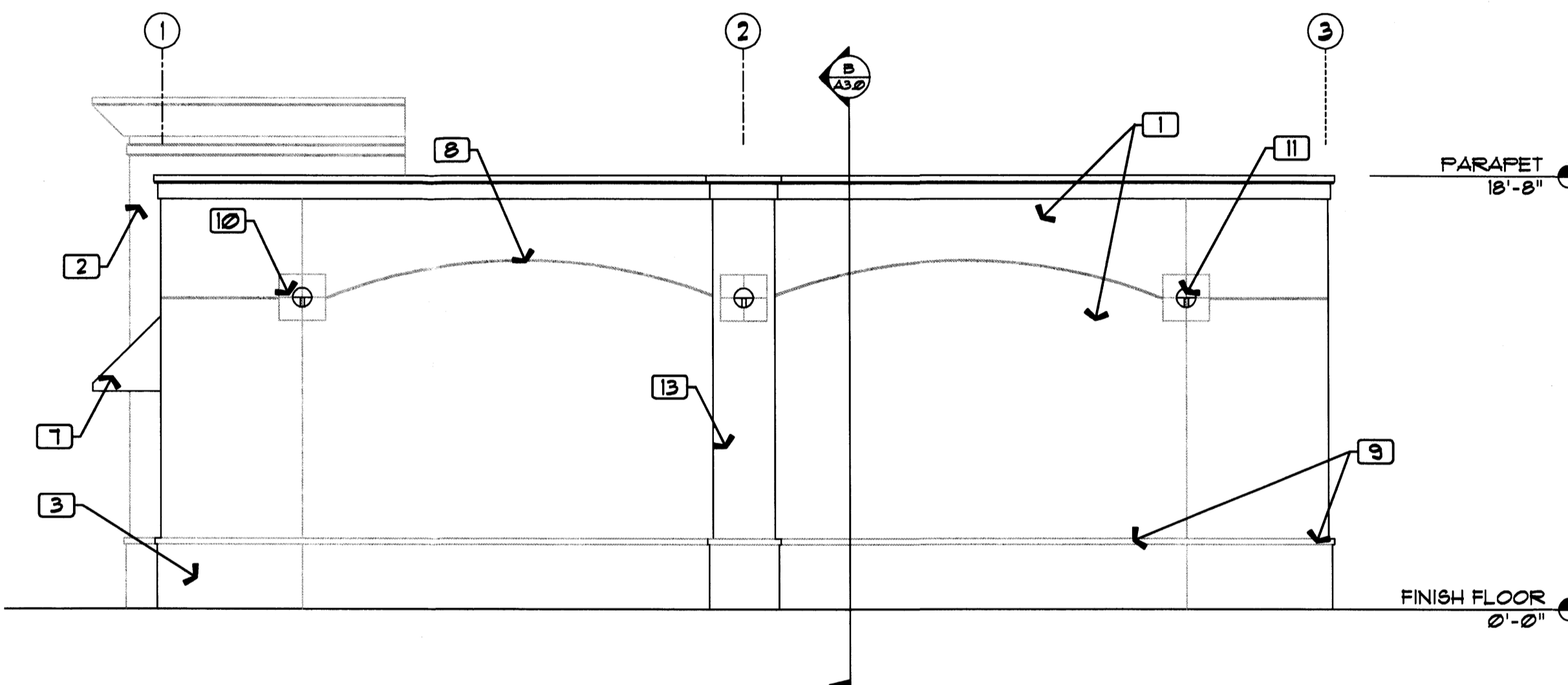
1 NORTH BUILDING ELEVATION

SCALE: 3/16" = 1'-0"



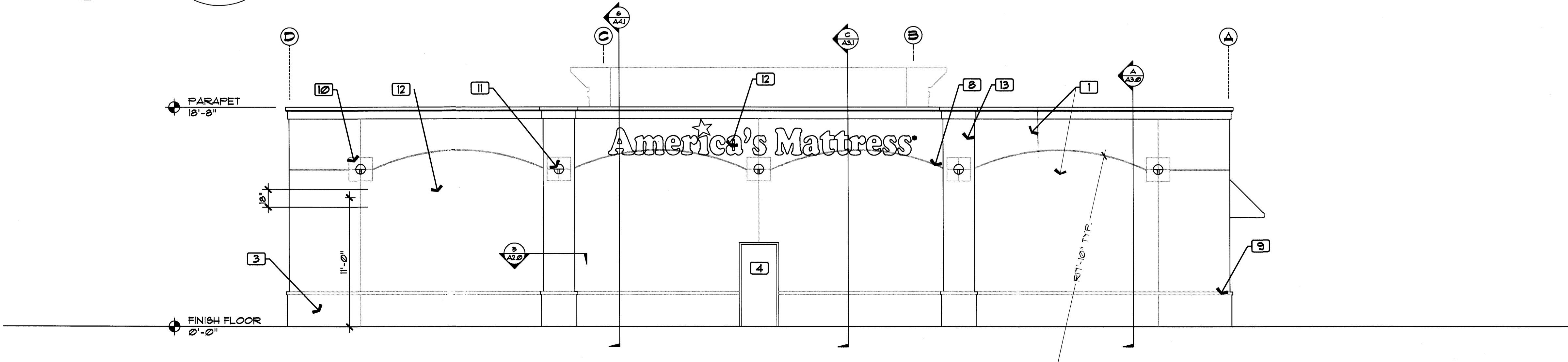
2 EAST BUILDING ELEVATION

SCALE: 3/16" = 1'-0"



3 WEST BUILDING ELEVATION

SCALE: 3/16" = 1'-0"



4 SOUTH BUILDING ELEVATION

SCALE: 3/16" = 1'-0"

**KEYNOTES:**

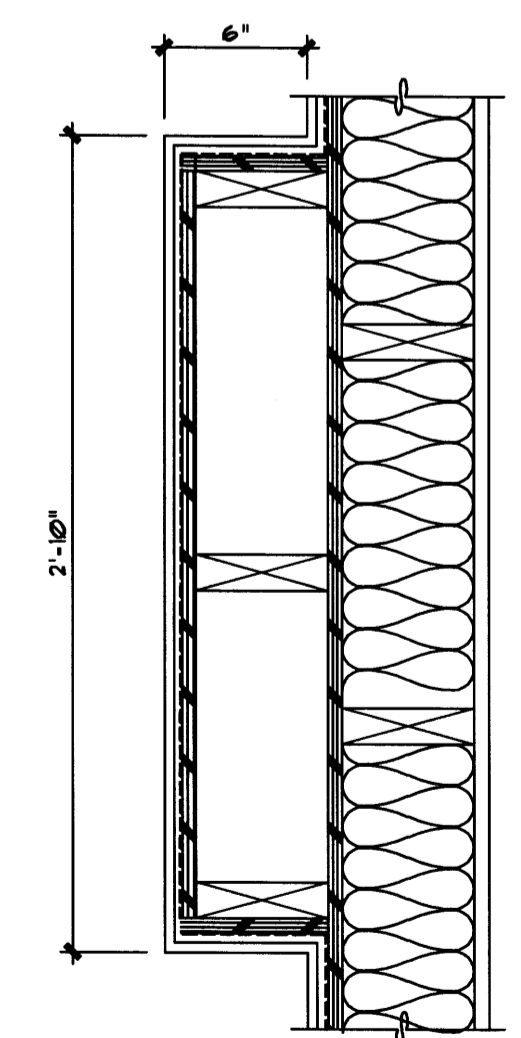
- 1 CEMENT STUCCO SYSTEM - COLOR #1
- 2 CEMENT STUCCO SYSTEM - COLOR #2
- 3 CEMENT STUCCO SYSTEM - COLOR #3
- 4 HM. DOOR & FRAME, PAINTED TO MATCH.
- 5 DARK BRONZE ALUMINUM STOREFRONT & TINTED GLAZING SYSTEM.
- 6 STUCCO CAP
- 7 FABRIC AWNING WITH PRE-FINISHED STEEL TUBE FRAME (DESIGN - BUILD BY SUPPLIER) COLOR OF AWNING TO BE SUNBRELLA CANVAS PAFRIKA #5451-0000 SOLID DECORATIVE.
- 8 STUCCO REVEAL TYPE #1
- 9 STUCCO REVEAL TYPE #2
- 10 1'x1' CERAMIC TILE INSERT
- 11 BUILDING LIGHTING
- 12 INTERNALLY ILLUMINATED FIBERGLASS FACED SIGNAGE BY OTHERS NOT TO EXCEED 10% OF TOTAL SURFACE AREA. ACTUAL SIGNAGE IS 3.03' X 26.21' = 79.42 SF WHICH IS LESS THAN 5% OF BUILDING ELEVATION SURFACE AREA.
- 13 PILASTER - SEE DETAIL.
- 14 INTERNALLY ILLUMINATED FIBERGLASS FACED SIGNAGE BY OTHERS NOT TO EXCEED 10% OF TOTAL SURFACE AREA. ACTUAL SIGNAGE IS 3.03' X 26.21' = 79.42 SF WHICH IS LESS THAN 5% OF BUILDING ELEVATION SURFACE AREA.

**GENERAL NOTES:**

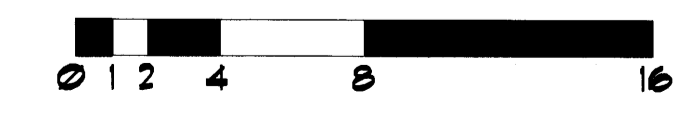
1. IF THERE ARE ANY SIGNIFICANT CHANGES TO THE APPROVED BUILDING ELEVATIONS REQUESTED BY FUTURE TENANTS, THESE REVISIONS WILL BE SUBMITTED TO THE ZONING DEPARTMENT FOR ADMINISTRATIVE APPROVAL ONLY.
2. ALL BUILDING SIGNAGE IS TO BE SUBMITTED FOR REVIEW UNDER THE CITY OF ALBUQUERQUE ORDINANCES GOVERNING THE APPLICATION OF SUCH SIGNAGE. SIGNAGE DIMENSIONS, COLORS, MATERIALS, AND LIGHTING WILL BE PROVIDED BY TENANTS. NO SIGNAGE WILL BE ALLOWED ON THE WEST FACADE. THERE WILL BE NO ELECTRONIC DISPLAY PANELS AND WALL SIGNS WILL BE ALLOWED ON THE NORTH, SOUTH AND EAST FACADES THAT WILL TAKE UP NO MORE THAN 10% OF THE FACADE TO WHICH IT IS APPLIED.
3. ALL ROOF TOP MOUNTED MECHANICAL EQUIPMENT WILL BE EFFECTIVELY SCREENED FROM PUBLIC SIGHT BY THE USE OF THE PROPOSED BUILDING PARAPETS.

**STUCCO COLORS:**

- COLOR #1 - FIELD STUCCO - SW 6070 'HERON PLUME' (OFF WHITE)
- COLOR #2 - ACCENT - SW 6142 'MACADAMIA' (LIGHT BROWN)
- COLOR #3 - ACCENT - SW 6082 'COBBLE BROWN' (DARK BROWN)
- COLOR #4 - PARAPET - SW 1006 'EXTRA WHITE' (WHITE)



5 PILASTER DETAIL PLAN VIEW



DATE:  
03 APR 09

FRANCIS H HOFHEINS  
NM LICENSED ARCHITECT  
NO. 4120  
MY LICENSE EXPIRES  
DEC 31, 2011

**RADIX CONSTRUCTION Inc.**

F. H. HOFHEINS, JR. - ARCHITECT

2402 17TH AVE. NE. RD. #153  
N.M.P.C. ID. 83886  
(208) 442-7106 • FAX (208) 442-7107

RETAIL SHOPS BUILDING  
CARLISLE & CLAREMONT  
ALBUQUERQUE, NM

PROPOSED  
EXTERIOR ELEVATIONS

NO.	REVISION	DATE
1	ADD WINDOWS TO EAST ELEV.	1/18/2009
2		
3		

THIS DRAWING IS THE PROPERTY OF THE ARCHITECT. IT HAS BEEN PREPARED SPECIFICALLY FOR THIS PROJECT AND IS NOT TO BE USED FOR ANY OTHER PURPOSE WITHOUT WRITTEN CONSENT OF THE ARCHITECT. COPYRIGHT 2007 RADIX CONSTRUCTION, INC.

PROJECT NUMBER:	A9006
DRAWN BY:	SSM
CHECKED BY:	FHH
DATE:	03 APR 09
SHEET #	A2.0

**SITE DATA**

SITE AREA 0.52 AC  
EXISTING ZONING C-2

**BUILDING DATA**

RETAIL SHOPS 4000 SF  
MAXIMUM BUILDING HEIGHT = 26'  
SETBACK = 11'  
CONSTRUCTION TYPE VB (NON-SPRINKLED)

NOTE: ADDITIONAL BUILDING INFORMATION IS CONTAINED ON THE BUILDING ELEVATION SHEETS.

**LEGAL DESC.**

TRACT "F-1-B", PLAT FOR AMERICAN SQUARE, ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO, AS THE SAME IS SHOWN AND DESIGNATED ON THE CORRECTION PLAT THEREOF, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO, ON JULY 15, 2005, IN PLAT BOOK 2005C, PAGE 251.

**LEGEND**

- EASEMENT
- - - - - PROPERTY BOUNDARY
- ▭ NEW CONCRETE
- ▭ NEW CURB
- ▭ PARKING STALL COUNT

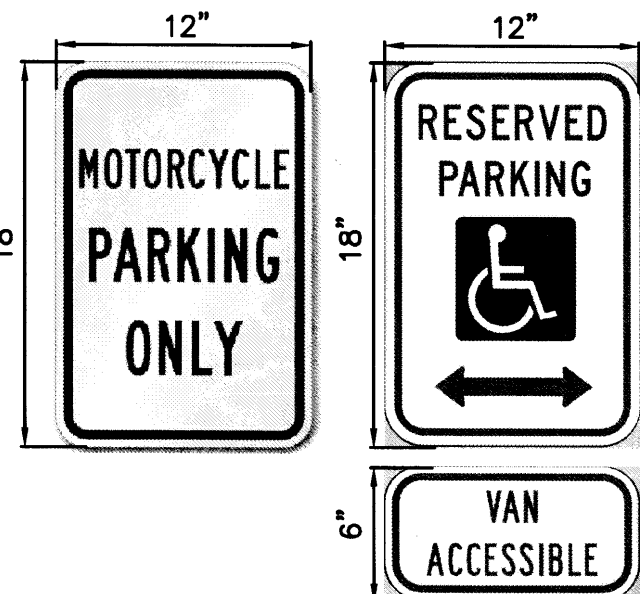
**BOUNDARY INFORMATION**

LINE TABLE

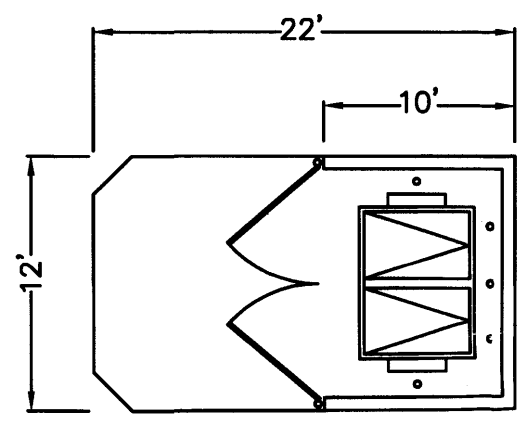
LINE	BEARING	LENGTH
L1	S30°25'03"W	45.00
L2	N89°54'57"E	7.00
L3	S00°05'03"W	15.00
L4	S89°54'57"E	7.00
L5	S00°05'03"W	5.18

CURVE TABLE

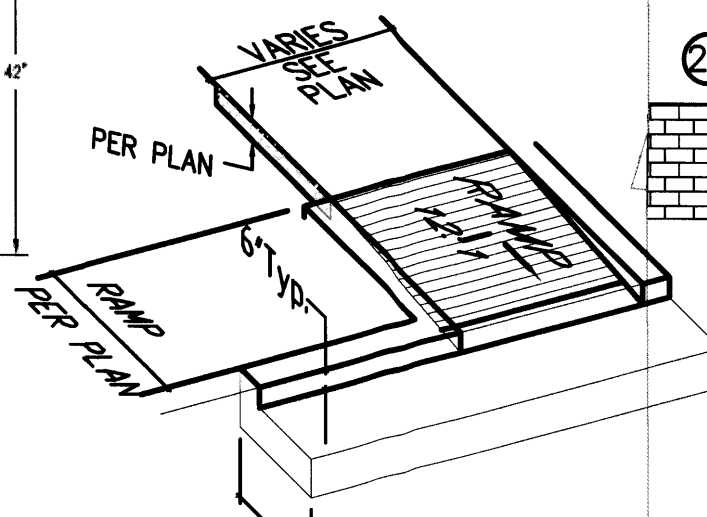
CURVE	DELTA	LENGTH	RADIUS	CHORD BEARING	CHORD
C1	90°27'29"	47.36'	30.00'	N44°53'08"W	42.60'
C2	19°56'48"	52.22'	150.00'	N15°04'49"E	51.96'
C3	19°57'01"	17.41'	50.00'	S15°04'55"W	17.32'
C4	19°57'01"	17.41'	50.00'	S14°44'38"E	17.32'
C5	19°54'36"	52.23'	156.88'	N14°57'27"W	51.99'
C6	97°29'13"	44.70'	26.30'	N48°15'53"E	39.51'



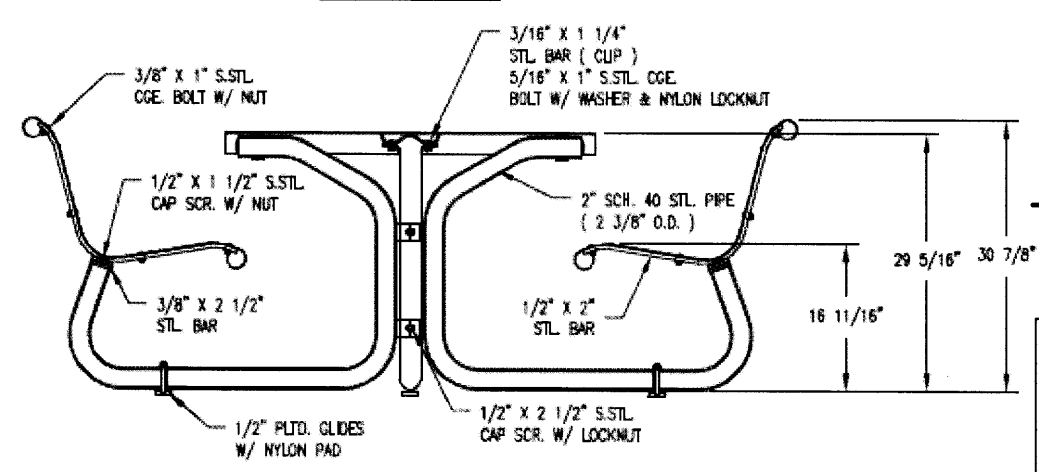
**SITE LIGHTING**



**TRASH ENCLOSURE DETAIL**



**SIDEWALK RAMP DETAIL**



**OUTDOOR SEATING DETAIL**

**PARKING DATA**

RETAIL SHOPS = 4000 SF  
PARKING RATIO = 5 / 1000 SF LESS 10% TRANSIT REDUCTION

PARKING REQUIRED (ADJUSTED)	18
PARKING PROVIDED	30
HANDICAP PARKING PROVIDED	1
HANDICAP PARKING REQUIRED	1
MOTORCYCLE PARKING PROVIDED	2
MOTORCYCLE PARKING REQUIRED	1
BICYCLE PARKING PROVIDED	4
BICYCLE PARKING REQUIRED	2

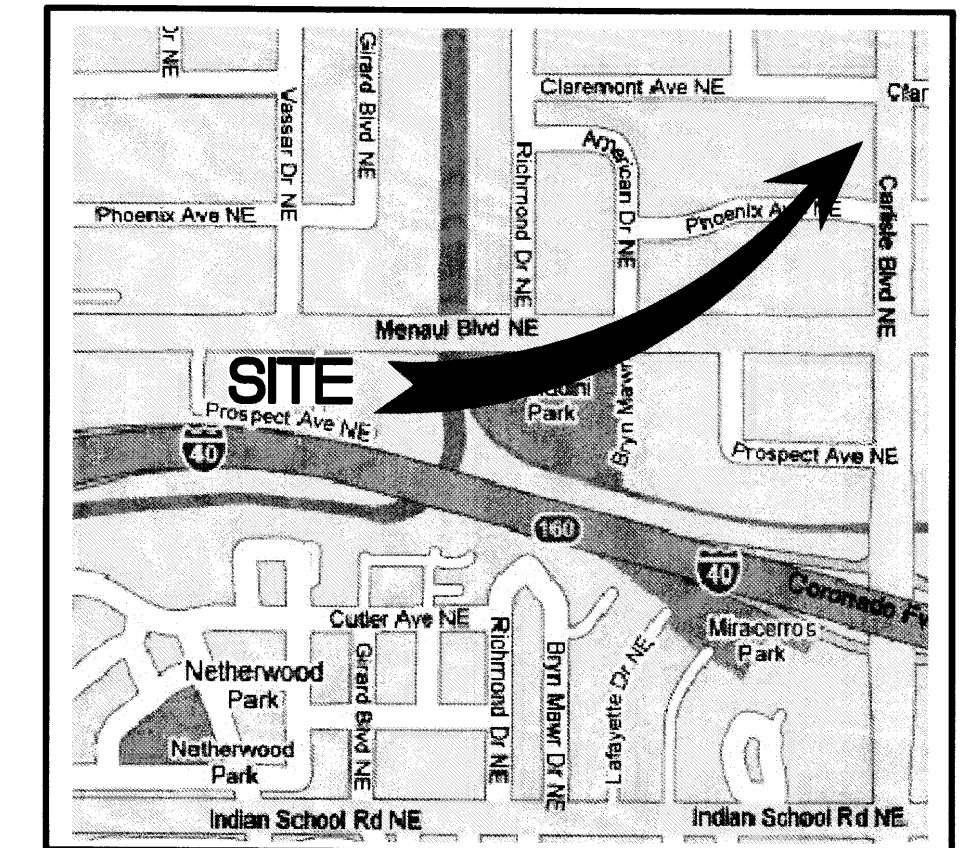
THIS SITE WILL HAVE A CROSS PARKING AGREEMENT WITH THE JACK-IN-THE-BOX SITE TO THE NORTH.  
THIS SITE WILL HAVE A CROSS ACCESS AGREEMENT WITH THE JACK-IN-THE-BOX SITE TO THE NORTH.

**NOTES:**

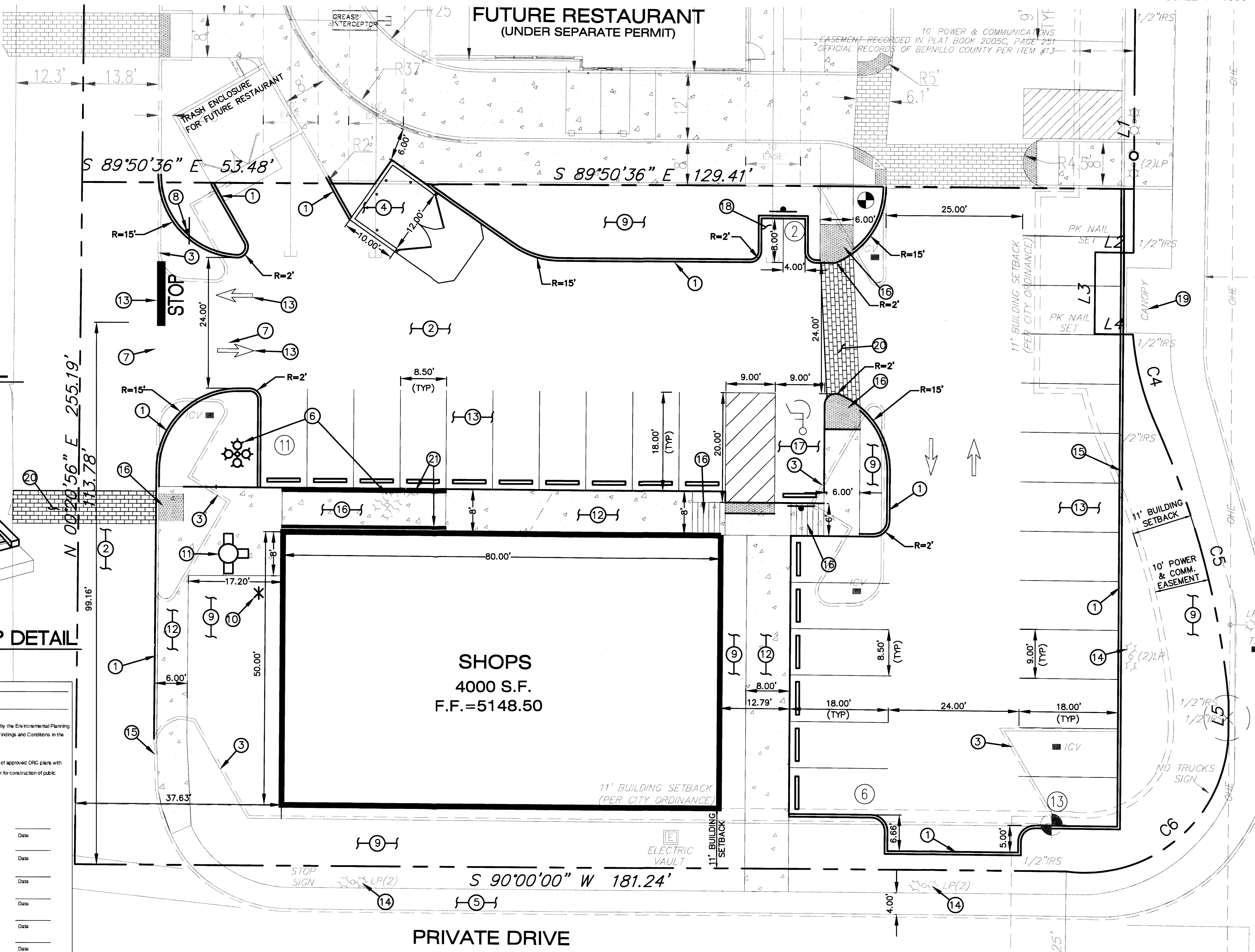
- LANDSCAPE REQUIRED
- ZONING C-2 COMMUNITY COMMERCIAL
- ALL CURVE RADI EQUALS 3' UNLESS NOTED OTHERWISE
- THIS PROJECT IS NOT PHASED.
- SHOULD REVISIONS TO THE ARCHITECTURAL ELEVATIONS BE REQUIRED DUE TO TENANT REQUIREMENTS, REVISED ELEVATIONS WILL BE SUBMITTED TO PLANNING STAFF FOR ADMINISTRATIVE APPROVAL.
- THE PROPOSED RESTAURANT NORTH OF THIS PROJECT IS NOT A PART OF THIS PROJECT AND IS BEING DESIGNED BY OTHERS AND PERMITTED SEPARATELY.
- THERE WILL BE A CROSS ACCESS AND PARKING AGREEMENT FOR THIS SITE AND THE RESTAURANT SITE TO THE NORTH.
- ADDITIONAL SITE INFORMATION, INCLUDING ADJACENT RIGHT OF WAY WIDTHS AND PEDESTRIAN ACCESS, IS SHOWN ON SHEET C1.1.
- PRELIMINARY GRADING IS SHOWN ON SHEET C3.0.
- UTILITY INFORMATION IS SHOWN ON SHEETS C4.0 AND C4.1.
- LANDSCAPE INFORMATION IS SHOWN ON THE ATTACHED LANDSCAPE SHEETS.
- BUILDING ELEVATIONS AND INFORMATION ARE SHOWN ON THE ATTACHED ELEVATION SHEET A2.0.
- A SEPARATE SIGN PACKAGE WILL BE SUBMITTED FOR THE PROJECT ONCE TENANT(S) HAVE BEEN DETERMINED.
- NEW SITE LIGHTING IS NOT PROPOSED. THE PROJECT WILL UTILIZE NEW BUILDING MOUNTED LIGHTING TO SUPPLEMENT THE EXISTING SITE LIGHTING.

**KEYNOTES**

- NEW CURB
- EXISTING VEHICULAR ACCESS
- EXISTING CURB TO BE REMOVED
- REFUGE ENCLOSURE - 6' HIGH CMU STRUCTURE TO MATCH BUILDING EXISTING SIDEWALK TO REMAIN.
- RELOCATE EXISTING LIGHT.
- EXISTING PAVEMENT MARKING TO BE REMOVED
- PROPOSED STOP SIGN
- PROPOSED LANDSCAPE
- PROPOSED BIKE RACK
- PROPOSED OUTDOOR SEATING / PUBLIC SPACE
- PROPOSED SIDEWALK
- PROPOSED PAVEMENT MARKING
- EXISTING SITE LIGHTING TO REMAIN
- EXISTING CURB TO REMAIN
- PROPOSED ADA RAMP
- PROPOSED HANDICAP PARKING AND SIGNAGE PER SECTION 14-16-3-1(F)(9)(b) OF THE ZONING CODE.
- PROPOSED MOTORCYCLE PARKING AND SIGNAGE PER SECTION 14-16-3-1(C)(2) OF THE ZONING CODE.
- EXISTING TRANSIT SHELTER
- 6 FOOT WIDE PEDESTRIAN CROSSING WITH PATTERNED COLORED CONCRETE TO CONNECT TO EXISTING PEDESTRIAN ROUTE.
- HANDRAILS PER ADAAG GUIDELINES.



SCALE 1"=1000'



PROJECT NUMBER: \_\_\_\_\_

Application Number: \_\_\_\_\_

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated \_\_\_\_\_ and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? ( ) Yes ( ) No If yes, then a set of approved DRG plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:

Traffic Engineering, Transportation Division	Date
Water Authority	Date
Parks and Recreation Department	Date
City Engineer	Date
Solid Waste Management	Date
DRB Chairperson, Planning Department	Date

Designed By: MAJ  
Drawn By: MAJ  
Checked By: MAJ

Issue Date: 5-09

60072006

**PACLAND**  
T (602) 273-7500  
F (602) 273-7505  
WWW.PACLAND.COM

432 N. 44TH ST  
SUITE 353  
PHOENIX, AZ 85008

RETAIL SHOPS  
2701 CARLISLE AVE NE  
ALBUQUERQUE, NM

SITE PLAN FOR BUILDING PERMIT

**C-1.1**

**SITE DATA**

SITE AREA 0.52 AC  
EXISTING ZONING C-2

**BUILDING DATA**

RETAIL SHOPS 4000 SF  
MAXIMUM BUILDING HEIGHT = 26'  
SETBACK = 11'  
CONSTRUCTION TYPE VB (NON-SPRINKLED)

NOTE: ADDITIONAL BUILDING INFORMATION IS CONTAINED ON THE BUILDING ELEVATION SHEETS.

**LEGAL DESC.**

TRACT "F-1-B", PLAT FOR AMERICAN SQUARE, ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO, AS THE SAME IS SHOWN AND DESIGNATED ON THE CORRECTION PLAT THEREOF, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO, ON JULY 15, 2005, IN PLAT BOOK 2005C, PAGE 251.

**LEGEND**

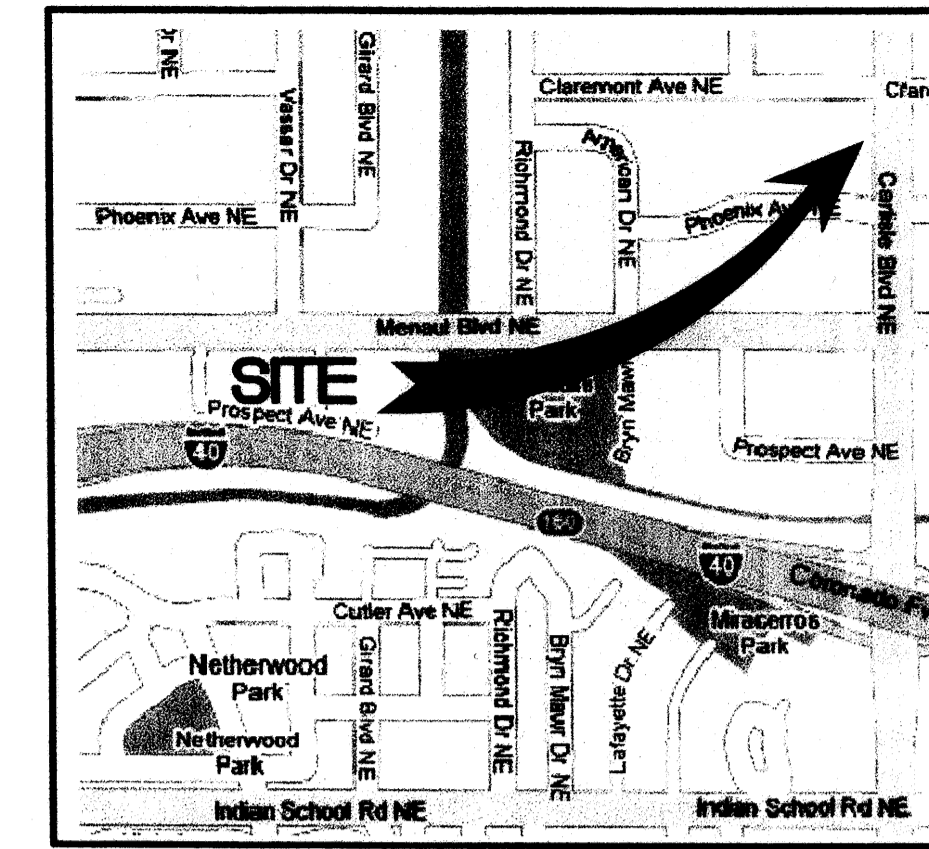
- EASEMENT
- PROPERTY BOUNDARY
- NEW CONCRETE
- NEW CURB
- PARKING STALL COUNT

**NOTES:**

1. LANDSCAPE REQUIRED
2. ZONING C-2 COMMUNITY COMMERCIAL
3. ALL CURVE RADII EQUALS 3' UNLESS NOTED OTHERWISE
4. THIS PROJECT IS NOT PHASED.
5. SHOULD REVISIONS TO THE ARCHITECTURAL ELEVATIONS BE REQUIRED DUE TO TENANT REQUIREMENTS, REVISED ELEVATIONS WILL BE SUBMITTED TO PLANNING STAFF FOR ADMINISTRATIVE APPROVAL.
6. THE PROPOSED RESTAURANT NORTH OF THIS PROJECT IS NOT A PART OF THIS PROJECT AND IS BEING DESIGNED BY OTHERS AND PERMITTED SEPARATELY.
7. THERE WILL BE A CROSS ACCESS AND PARKING AGREEMENT FOR THIS SITE AND THE RESTAURANT SITE TO THE NORTH.
8. ADDITIONAL SITE INFORMATION, INCLUDING ADJACENT RIGHT OF WAY WIDTHS AND PEDESTRIAN ACCESS, IS SHOWN ON SHEET C1.1.
9. PRELIMINARY GRADING IS SHOWN ON SHEET C3.0.
10. UTILITY INFORMATION IS SHOWN ON SHEETS C4.0 AND C4.1.
11. LANDSCAPE INFORMATION IS SHOWN ON THE ATTACHED LANDSCAPE SHEETS.
12. BUILDING ELEVATIONS AND INFORMATION ARE SHOWN ON THE ATTACHED ELEVATION SHEET A2.0.
13. A SEPARATE SIGN PACKAGE WILL BE SUBMITTED FOR THE PROJECT ONCE TENANT(S) HAVE BEEN DETERMINED.
14. NEW SITE LIGHTING IS NOT PROPOSED. THE PROJECT WILL UTILIZE NEW BUILDING MOUNTED LIGHTING TO SUPPLEMENT THE EXISTING SITE LIGHTING.

**KEYNOTES**

- 1 NEW CURB
- 2 EXISTING VEHICULAR ACCESS
- 3 EXISTING CURB TO BE REMOVED
- 4 REFUGE ENCLOSURE - 6' HIGH CMU STRUCTURE TO MATCH BUILDING
- 5 EXISTING SIDEWALK TO REMAIN
- 6 RELOCATE EXISTING LIGHT.
- 7 EXISTING PAVEMENT MARKING TO BE REMOVED
- 8 PROPOSED STOP SIGN
- 9 PROPOSED LANDSCAPE
- 10 PROPOSED BIKE RACK
- 11 PROPOSED OUTDOOR SEATING / PUBLIC SPACE
- 12 PROPOSED SIDEWALK
- 13 PROPOSED PAVEMENT MARKING
- 14 EXISTING SITE LIGHTING TO REMAIN
- 15 EXISTING CURB TO REMAIN
- 16 PROPOSED ADA RAMP
- 17 PROPOSED HANDICAP PARKING AND SIGNAGE PER SECTION 14-16-3-1(F)(9)(b) OF THE ZONING CODE.
- 18 PROPOSED MOTORCYCLE PARKING AND SIGNAGE PER SECTION 14-16-3-1(C)(2) OF THE ZONING CODE.
- 19 EXISTING TRANSIT SHELTER
- 20 6 FOOT WIDE PEDESTRIAN CROSSING WITH PATTERNED COLORED CONCRETE TO CONNECT TO EXISTING PEDESTRIAN ROUTE.
- 21 HANDRAILS PER ADAAG GUIDELINES.



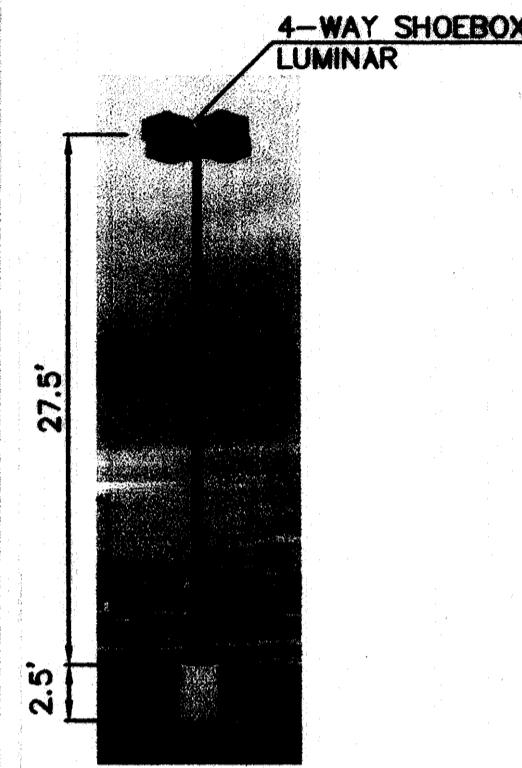
**VICINITY MAP**

SCALE 1"=1000'

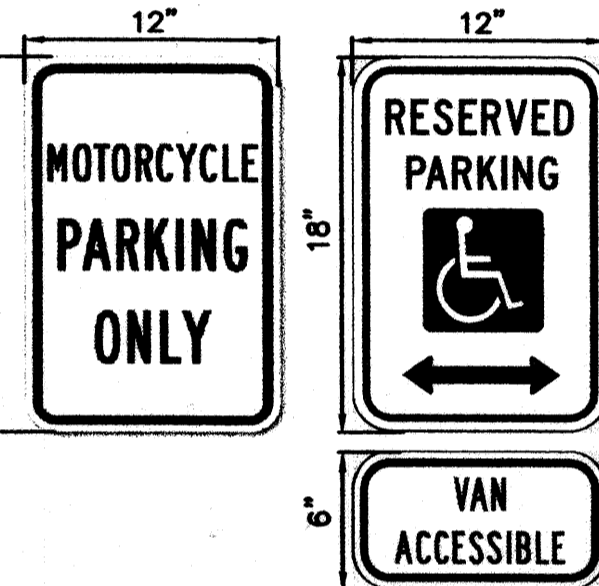
**BOUNDARY INFORMATION**

LINE	BEARING	LENGTH
L1	S00°05'03"W	45.00
L2	N89°54'57"W	7.00
L3	S00°05'03"W	15.00
L4	S89°54'57"E	7.00
L5	S00°05'03"W	5.18

CURVE	DELTA	LENGTH	RADIUS	CHORD BEARING	CHORD
C1	90°27'29"	47.36'	30.00'	N44°53'08"W	42.60'
C2	19°56'48"	52.22'	150.00'	N15°04'49"E	51.96'
C3	19°5'70"	17.41'	50.00'	S15°04'55"W	17.32'
C4	19°5'70"	17.41'	50.00'	S14°44'30"E	17.32'
C5	19°04'36"	52.23'	156.88'	N14°57'27"W	51.99'
C6	97°23'13"	44.70'	26.30'	N48°15'53"E	39.51'

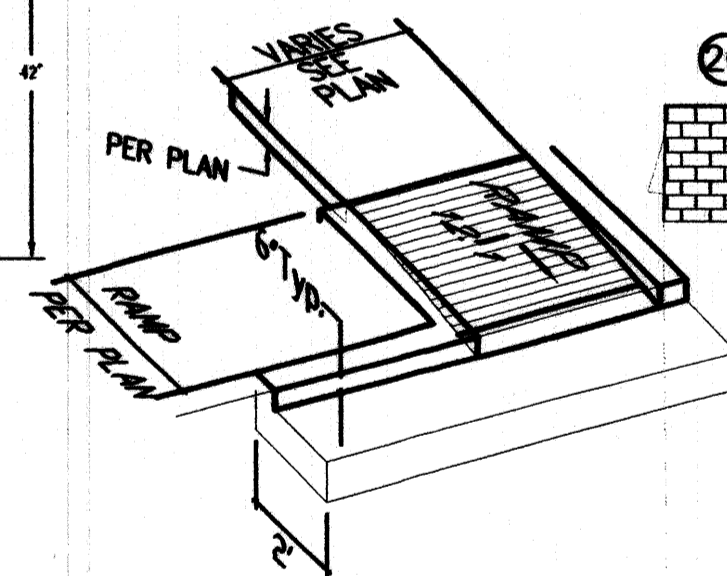


**SITE LIGHTING**

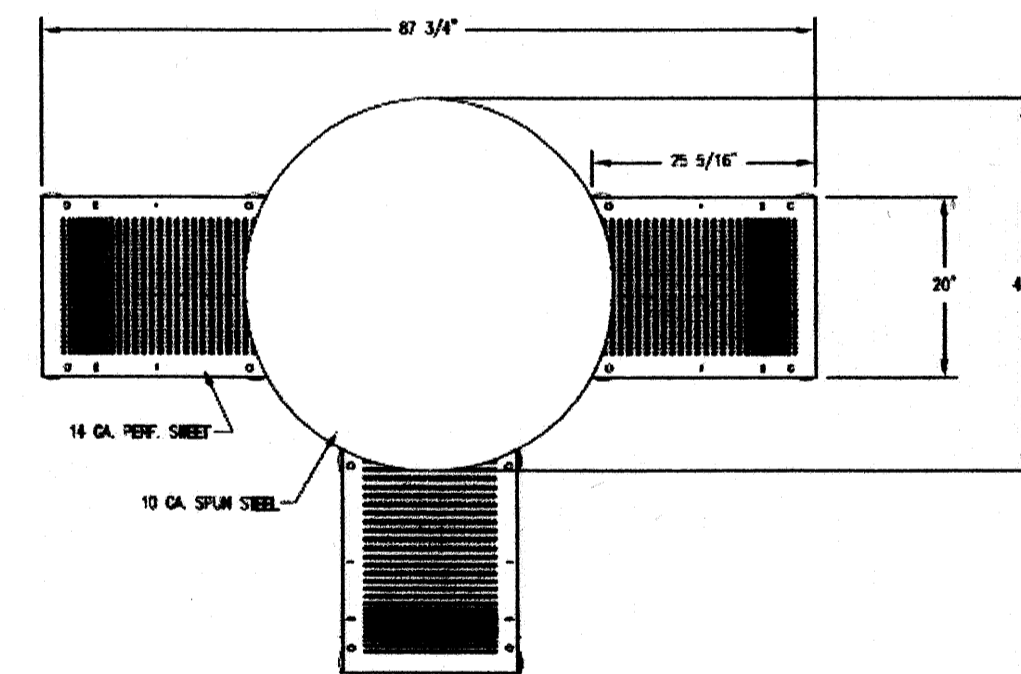


**SIGN DETAILS**

**TRASH ENCLOSURE DETAIL**



**SIDEWALK RAMP DETAIL**



**OUTDOOR SEATING DETAIL**

**PARKING DATA**

RETAIL SHOPS = 4000 SF  
PARKING RATIO = 5 / 1000 SF LESS 10% TRANSIT REDUCTION

PARKING REQUIRED (ADJUSTED)	18
PARKING PROVIDED	30
HANDICAP PARKING PROVIDED	1
HANDICAP PARKING REQUIRED	1
MOTORCYCLE PARKING PROVIDED	2
MOTORCYCLE PARKING REQUIRED	1
BICYCLE PARKING PROVIDED	4
BICYCLE PARKING REQUIRED	2

THIS SITE WILL HAVE A CROSS PARKING AGREEMENT WITH THE JACK-IN-THE-BOX SITE TO THE NORTH.  
THIS SITE WILL HAVE A CROSS ACCESS AGREEMENT WITH THE JACK-IN-THE-BOX SITE TO THE NORTH.

PROJECT NUMBER: 1002249  
Application Number: 4-1609

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated 4-16-09, and the Findings and Conditions in the Official Notification of Decision and conditions.

Is an Infrastructure List required? ( ) Yes (X) No. If yes, then a set of approved EPC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

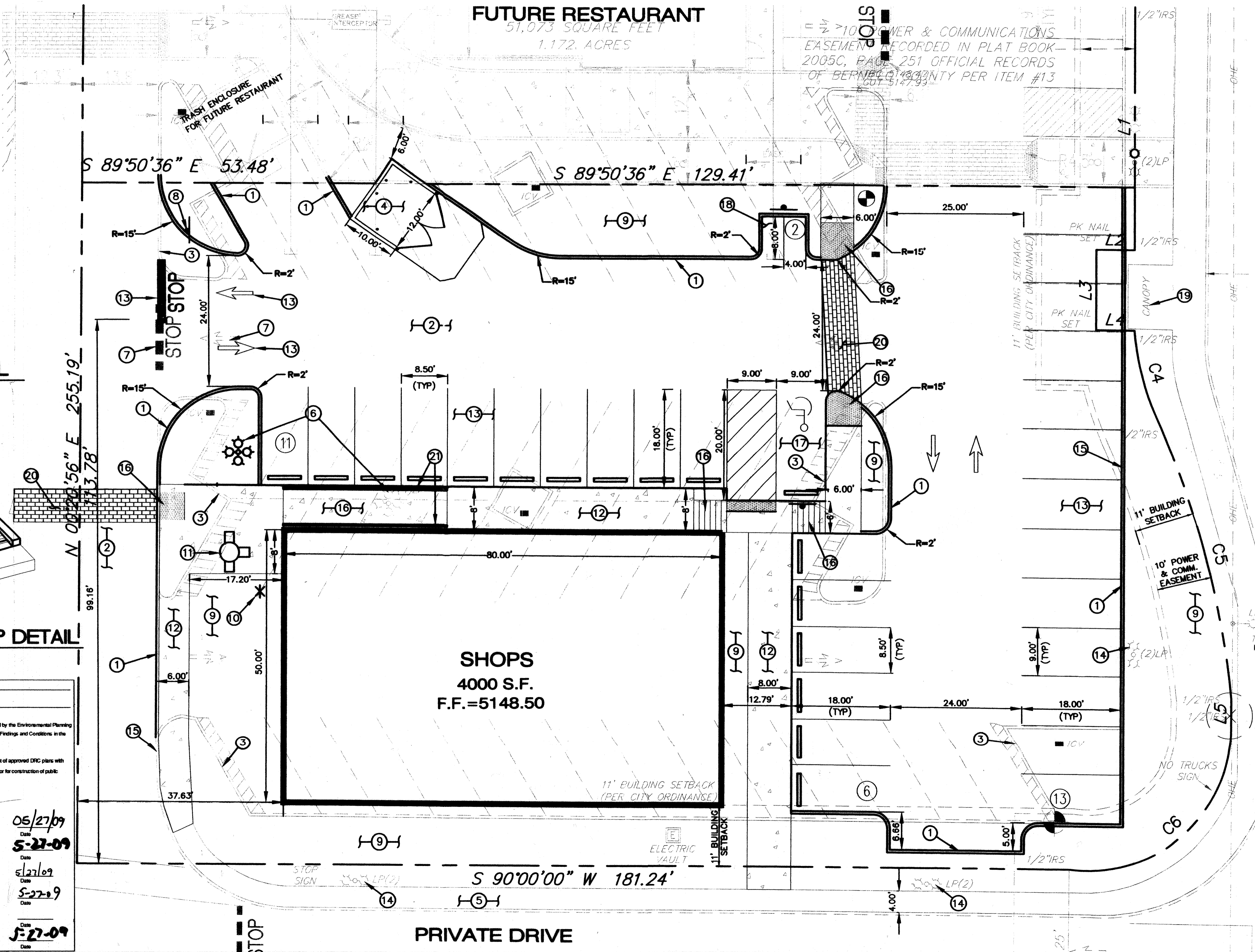
DOB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:

*[Signature]* 05/27/09  
Date: 5-27-09

*[Signature]* 5/27/09  
Date: 5-27-09

*[Signature]* 5-27-09  
Date: 5-27-09

*[Signature]* 5-27-09  
Date: 5-27-09



DESIGNED MAJ 5-09  
DRAWN MAJ  
CHECKED MAJ 60072006

**PACLAND**  
T (602) 272-7900  
F (602) 272-7905  
www.PacLand.com  
432 N. 44TH ST  
SUITE 303  
PHOENIX, AZ 85008

RETAIL SHOPS  
2701 CARLISLE AVE NE  
ALBUQUERQUE, NM  
SITE PLAN FOR BUILDING PERMIT  
C-1.1