



Please check the appropriate box and refer to supplemental forms for submittal requirements. All fees must be paid at the time of application.

| Administrative Decisions | Decisions Requiring a Public Meeting or Hearing | Policy Decisions |
|---|---|--|
| <input type="checkbox"/> Archaeological Certificate (Form P3) | <input type="checkbox"/> Site Plan – EPC including any Variances – EPC (Form P1) | <input type="checkbox"/> Adoption or Amendment of Comprehensive Plan or Facility Plan (Form Z) |
| <input type="checkbox"/> Historic Certificate of Appropriateness – Minor (Form L) | <input type="checkbox"/> Master Development Plan (Form P1) | <input type="checkbox"/> Adoption or Amendment of Historic Designation (Form L) |
| <input type="checkbox"/> Alternative Signage Plan (Form P3) | <input type="checkbox"/> Historic Certificate of Appropriateness – Major (Form L) | <input type="checkbox"/> Amendment of IDO Text (Form Z) |
| <input checked="" type="checkbox"/> Minor Amendment to Site Plan (Form P3) | <input type="checkbox"/> Demolition Outside of HPO (Form L) | <input type="checkbox"/> Annexation of Land (Form Z) |
| <input type="checkbox"/> WTF Approval (Form W1) | <input type="checkbox"/> Historic Design Standards and Guidelines (Form L) | <input type="checkbox"/> Amendment to Zoning Map – EPC (Form Z) |
| | <input type="checkbox"/> Wireless Telecommunications Facility Waiver (Form W2) | <input type="checkbox"/> Amendment to Zoning Map – Council (Form Z) |
| | | Appeals |
| | | <input type="checkbox"/> Decision by EPC, LC, ZHE, or City Staff (Form A) |

APPLICATION INFORMATION

| | | |
|--|-------------------------|----------------------------------|
| Applicant: McDonald's USA LLC | | Phone: 972-869-1888 |
| Address: 511 E John Carpenter Fwy | | Email: |
| City: Irving | State: TX | Zip: 75062 |
| Professional/Agent (if any): Cesar Segovia | | Phone: 817-529-6874 |
| Address: 513 Main Street #300 | | Email: cesar@roguearchitects.com |
| City: Fort Worth | State: TX | Zip: 76102 |
| Proprietary Interest in Site: | List <u>all</u> owners: | |

BRIEF DESCRIPTION OF REQUEST

Administrative Amendment for City of Albuquerque permit #BP-2020-27106

SITE INFORMATION (Accuracy of the existing legal description is crucial! Attach a separate sheet if necessary.)

| | | |
|---|---------------------|-----------------------------|
| Lot or Tract No.: LT A PLAT OF ONE PRESIDENTIAL PLAZA | Block: D | Unit: 2 |
| Subdivision/Addition: | MRGCD Map No.: | UPC Code: |
| Zone Atlas Page(s): | Existing Zoning: | Proposed Zoning: |
| # of Existing Lots: | # of Proposed Lots: | Total Area of Site (acres): |

LOCATION OF PROPERTY BY STREETS

Site Address/Street: 4300 Osuna Between: Jefferson St NE and: Presidential Drive NE

CASE HISTORY (List any current or prior project and case number(s) that may be relevant to your request.)

| | |
|-----------------------------|---|
| Signature: | Date: 7/29/2020 |
| Printed Name: Cesar Segovia | <input type="checkbox"/> Applicant or <input checked="" type="checkbox"/> Agent |

FOR OFFICIAL USE ONLY

| Case Numbers | Action | Fees | Case Numbers | Action | Fees |
|---------------|--------|------|--------------|--------|------|
| SI-2020-00688 | AA | | | | |
| | | | | | |
| | | | | | |

| | |
|-----------------------|--------------------------------|
| Meeting/Hearing Date: | Fee Total: |
| Staff Signature: | Date: Project # PR-2020-004189 |

FORM P3: ADMINISTRATIVE DECISIONS AND MINOR AMENDMENTS

A single PDF file of the complete application including all plans and documents being submitted must be emailed to PLNDRS@cabq.gov prior to making a submittal. Zipped files or those over 9 MB cannot be delivered via email, in which case the PDF must be provided on a CD.

INFORMATION REQUIRED FOR ALL ADMINISTRATIVE DECISIONS OR AMENDMENTS

- Letter of authorization from the property owner if application is submitted by an agent
- Zone Atlas map with the entire site clearly outlined and labeled

ARCHEOLOGICAL CERTIFICATE

- Archaeological Compliance Documentation Form with property information section completed
- Only the information above is required unless the City Archaeologist determines that the application does not qualify for a Certificate of No Effect, in which case a treatment plan prepared by a qualified archaeologist that adequately mitigates any archeological impacts of the proposed development must be submitted and reviewed for a Certificate of Approval per the criteria in IDO Section 14-16-6-5(A)(3)(b)

MINOR AMENDMENT TO SITE PLAN – ADMIN, EPC, or DRB

- Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-4(X)(2)
- Three (3) copies of all applicable sheets of the approved Site Plan being amended, folded
- Copy of the Official Notice of Decision associated with the prior approval
- Three (3) copies of the proposed Site Plan, with changes circled and noted
Refer to the Site Plan Checklist for information needed on the proposed Site Plan.

Minor Amendments must be within the thresholds established in IDO TABLE 6-4-5. Any amendment beyond these thresholds is considered a Major Amendment and must be processed through the original decision-making body for the request.



MINOR AMENDMENT TO SITE DEVELOPMENT PLAN APPROVED PRIOR TO THE EFFECTIVE DATE OF THE IDO

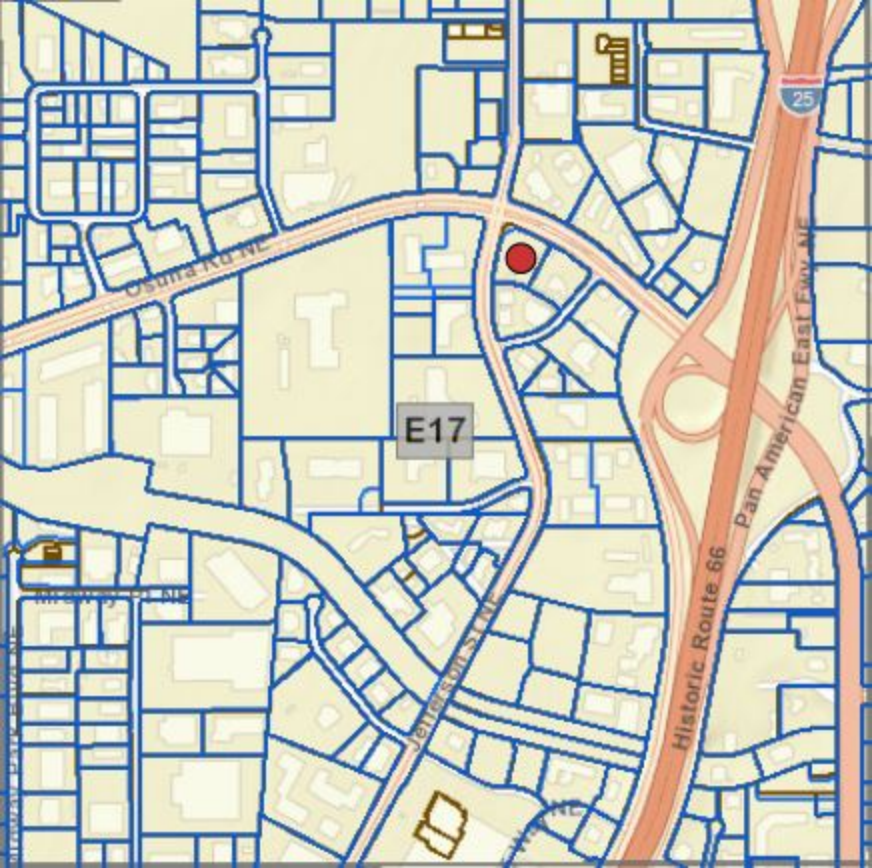
- Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-4(Y)(1)(a)
- Three (3) copies of all applicable sheets of the approved Site Development Plan being amended, folded
- Copy of the Official Notice of Decision associated with the prior approval
- Three (3) copies of the proposed Site Development Plan, with changes circled and noted
Refer to the Site Plan Checklist for information needed on the proposed Site Plan.

Minor Amendments must be within the thresholds established in IDO TABLE 6-4-5. Any amendment beyond these thresholds is considered a Major Amendment and must be processed through the original decision-making body for the request.

ALTERNATIVE SIGNAGE PLAN

- Proposed Alternative Signage Plan compliant with IDO Section 14-16-5-12(F)(5)
- Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-5(F)(4)(c)
- Required notices with content per IDO Section 14-16-6-4(K)(6)
- Office of Neighborhood Coordination notice inquiry response and proof of emailed notice to affected Neighborhood Association representatives
- Sign Posting Agreement

| | |
|---|---|
| I, the applicant or agent, acknowledge that if any required information is not submitted with this application, the application will not be scheduled for a public meeting or hearing, if required, or otherwise processed until it is complete. | |
| Signature:  | Date: 7/23/2020 |
| Printed Name: Cesar Segovia | <input type="checkbox"/> Applicant or <input checked="" type="checkbox"/> Agent |
| FOR OFFICIAL USE ONLY | |
| Project Number: | Case Numbers |
| PR-2020-004189 | SI-2020-00688 |
| | - |
| | - |
| Staff Signature: |  |
| Date: | |



Osuna Rd NE

E17

Johnson St NE

Historic Route 66

Pan American East Freeway

25



Wednesday, August 12, 2020

City of Albuquerque
Planning Department
600 2nd St. NW
Albuquerque, NM 87102
(505) 924-3991

Re: McDonald's 4300 Osuna Rd. NE (PR 2020-004189)

Ms. Jeanne Wolfenbarger:

Thank you for taking the time to review the Administrative Amendment application for the McDonald's at 4300 Osuna Rd. NE. We have addressed your comments as follows:

1. Dimension all of the drive aisles.
 - All drive aisles are dimensioned. See C3.0 Site & Paving Plan.
2. Add "Do Not Enter" signs at the end of all of the one-way drive aisles.
 - A "Do Not Enter" sign has been added to the end of the one-way drive aisle. See C3.0 Site & Paving Plan.
3. On sidewalk and ADA accessible pathways as well as parking, list 2% maximum cross-slope in the details and/or general notes.
 - It's listed under the "Standard Accessibility Requirements" notes. See C3.0 Site & Paving Plan and C6.0 Grading Plan.
4. There were not any van accessible aisles shown (minimum 8 feet wide) for the ADA parking. List van accessible parking requirements and include as part of the design.
 - An 11' wide van accessible space with a 5' aisle are shown in accordance with ADA Standards 502.2 and 502.3.1. See C3.0 Site & Paving Plan.
5. Include C7.1, C7.3 and C7.5 standard details as part of this set. All details for curb, sidewalk, curb ramps, and signage have to be included.
 - Detail sheets have been included.
6. The ADA accessible parking sign must have the required language per 66-7-352.4C NMSA 1978 **"Violators Are Subject to a Fine and/or Towing."** Provide detail of sign.
 - Detail with required language is shown on Sheet C7.1 Standard Details.
7. Provide any bike rack details.
 - Bike rack detail is on Sheet C7.6 City Details.
8. Is existing curb ramp ADA compliant at southeast corner of McDonald's
 - The existing ramp at the northeast corner of the McDonald's building is ADA compliant. Plan view is rotated.
9. For any new concrete work tying to existing concrete pedestrian ways into the building (such as on northeast corner of the building), provide a note.
 - A note has been added. See Sheet C3.0 Site & Paving Plan.

Please let us know if any further corrections are needed.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Matthew D. Korte'.

Matthew D. Korte, P.E.



July 28, 2020

Mr. Brennon Williams
City of Albuquerque Planning Department
600 2nd Street
Albuquerque, NM 87102

RE: Amendment to Site Development Plan
McDonald's Restaurant – 4300 Osuna Rd. NE

Dear Mr. Williams:

This letter is in reference to our attached application and site plans for a remodel of the existing McDonald's restaurant at 4300 Osuna Rd. NE, near the southeast corner of the intersection with Jefferson Blvd. NE. The site was developed under the Site Development Plan ZA99-330, and project number 1000138.

The scope of work with this plan includes the remodel of the existing McDonald's restaurant, replacement of existing menu board, order point and gateway (clearance bar) and installation of a new (additional) menu board and order point in a side by side Drive Thru configuration. McDonald's has found that the addition of a second order point, in a side-by-side configuration, decreases the response time experienced by customers and thereby improves their satisfaction.

The scope also includes ADA accessibility improvements throughout the site. Sidewalks and ramps are being replaced where needed to provide compliant slopes and accessible routes to parking and the right-of-way. ADA parking spaces are being restriped to provide compliant configurations.

Should you have any questions or need additional information, please do not hesitate to reach out to us or the McDonald's Area Construction Manager for this project, Eddie Nunez.

Sincerely,

ADAMS

A handwritten signature in blue ink, appearing to read 'Matthew D. Korte'.

Matthew D. Korte, P.E.
Program Director


Attachments

- [Profile](#)
- [Values](#)**
- [Map](#)
- [Satellite View](#)
- [Tax Calculator](#)

PARID: 101706232437110303
CELCO INVESTMENTS LLC, **4300 OSUNA RD**

1 of 1
 Return to Search Results
 Tax Year

| | |
|--------------------------|----------------------------|
| Class | |
| Class | Non Residential |
| Tax District | A1A |
| Current Owner | |
| Tax Year | 2020 |
| Owner | CELCO INVESTMENTS LLC |
| Owner Mailing Address | 2021 LOS POBLANOS PL NW |
| Unit | |
| City | LOS RANCHOS DE ALBUQUERQUE |
| State | NM |
| Zip Code | 87107 |
| Foreign Mailling Address | |

- Actions
-  Printable Summary
 -  Printable Version

Reports

Property Attributes

Ownership for Tax Year Selected

| | |
|--------------------------|----------------------------|
| Tax Year | |
| Tax Year | 2020 |
| Owner Name | CELCO INVESTMENTS LLC |
| Owner Mailing Address | 2021 LOS POBLANOS PL NW |
| Unit | |
| City | LOS RANCHOS DE ALBUQUERQUE |
| State | NM |
| Zip Code | 87107 |
| Foreign Mailling Address | |

Description

| | |
|-------------------------------------|---|
| Location Address | |
| Location Address | 4300 OSUNA RD NE |
| City | ALBUQUERQUE |
| State | NM |
| Zip Code | 87109 |
| Property Description | LT A PLAT OF ONE PRESIDENTIAL PLAZA (REPL OF LT 1 BLK D UNIT 2 INTERSTATE INDUSTRIAL TRACT) CONT 2.0031 AC +- |
| Public Improvement District | |
| Tax Increment Development Districts | |

Document #

| | |
|-------------|---|
| Document #: | 2010035346 042710 SW - ENTRY BY CBL 071310 CODED BY LV 042810 |
|-------------|---|

Real Property Attributes

| | |
|------------------------|--------------------------|
| Primary Building SQ FT | 6479 |
| Year Built | 2001 |
| Lot Size (Acres) | 2.0031 |
| Land Use Code | CONVIENIENCE FOOD MARKET |
| Style | |

Manufactured Home Attributes

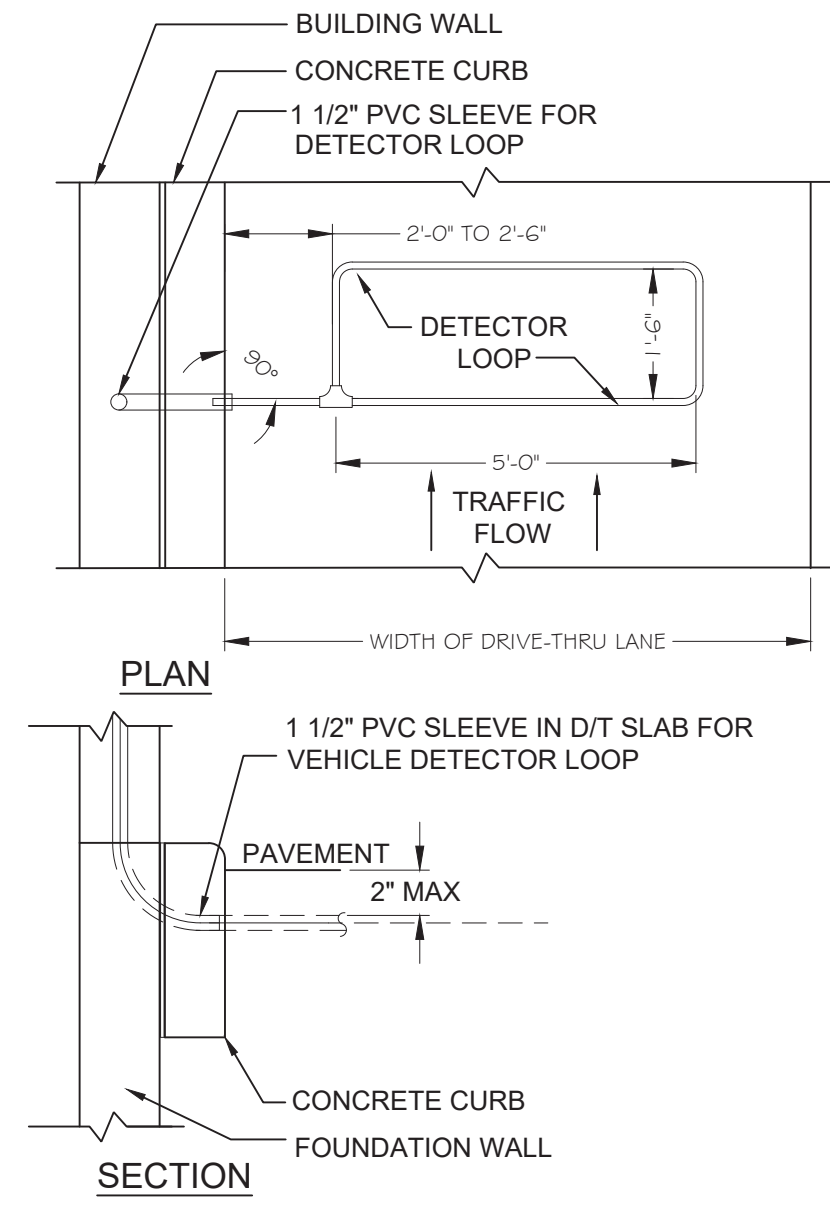
Make :
 License :
 VIN :
 Year :
 Size :

DISCLAIMER

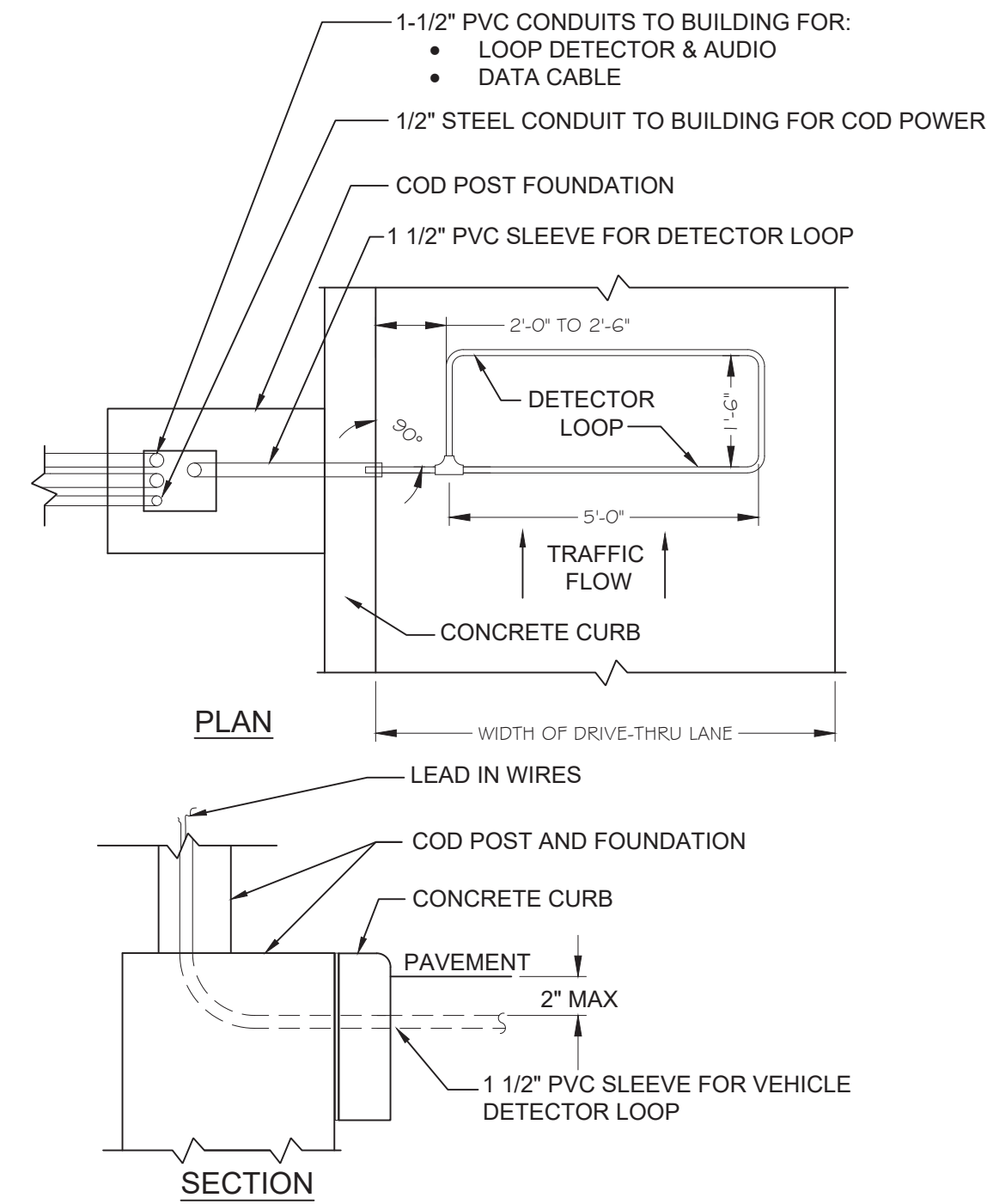
[Click here to view the Disclaimer](#)

NOTES:

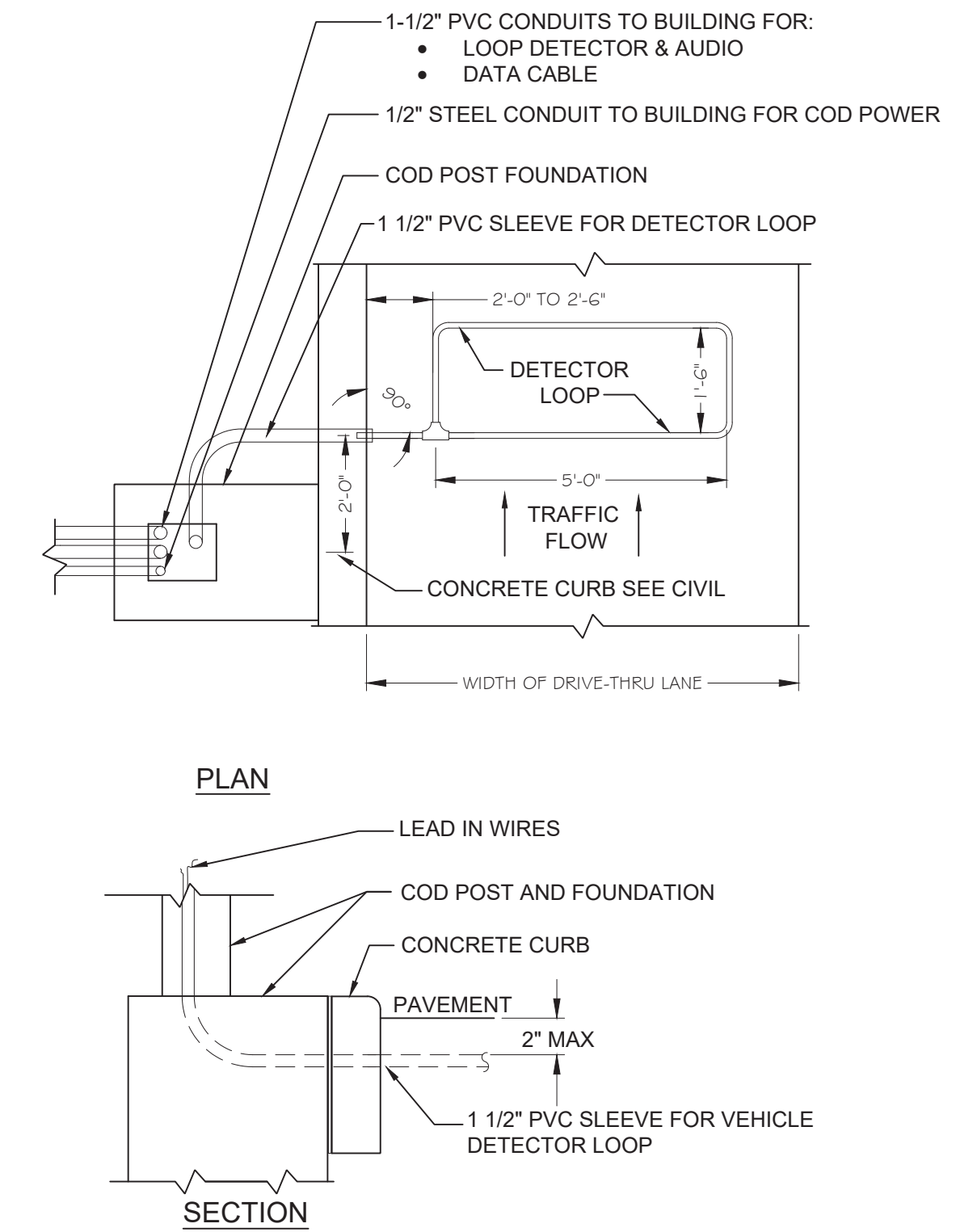
1. VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
2. CENTER VEHICLE DETECTOR LOOP IN DRIVE THRU LANE. INSTALL PER MANUFACTURERS RECOMMENDATIONS.
3. NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2" OF LOOP.
4. DETECTOR LOOP MANUFACTURERS: DETECTOR LOOPS MAY BE BY ONE OF THE FOLLOWINGS COMPANIES OR EQUAL.
3M: 1-800-328-0033
HME: 1-800-848-4468
5. DETECTOR LOOP MATERIAL:
PVC TUBING 1/2" I.D., 100 PSI LOOP MADE FROM ONE LENGTH OF THIN FOURTEEN GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.
6. DETECTOR LOOP CONSTRUCTION:
FORMED WITH ONE CONTINUOUS LENGTH OF PVC WITH NO SHARP CORNERS AS DETAILED. WIRE LOOPED, FORMED, & PIGTAILED AS DETAILED.



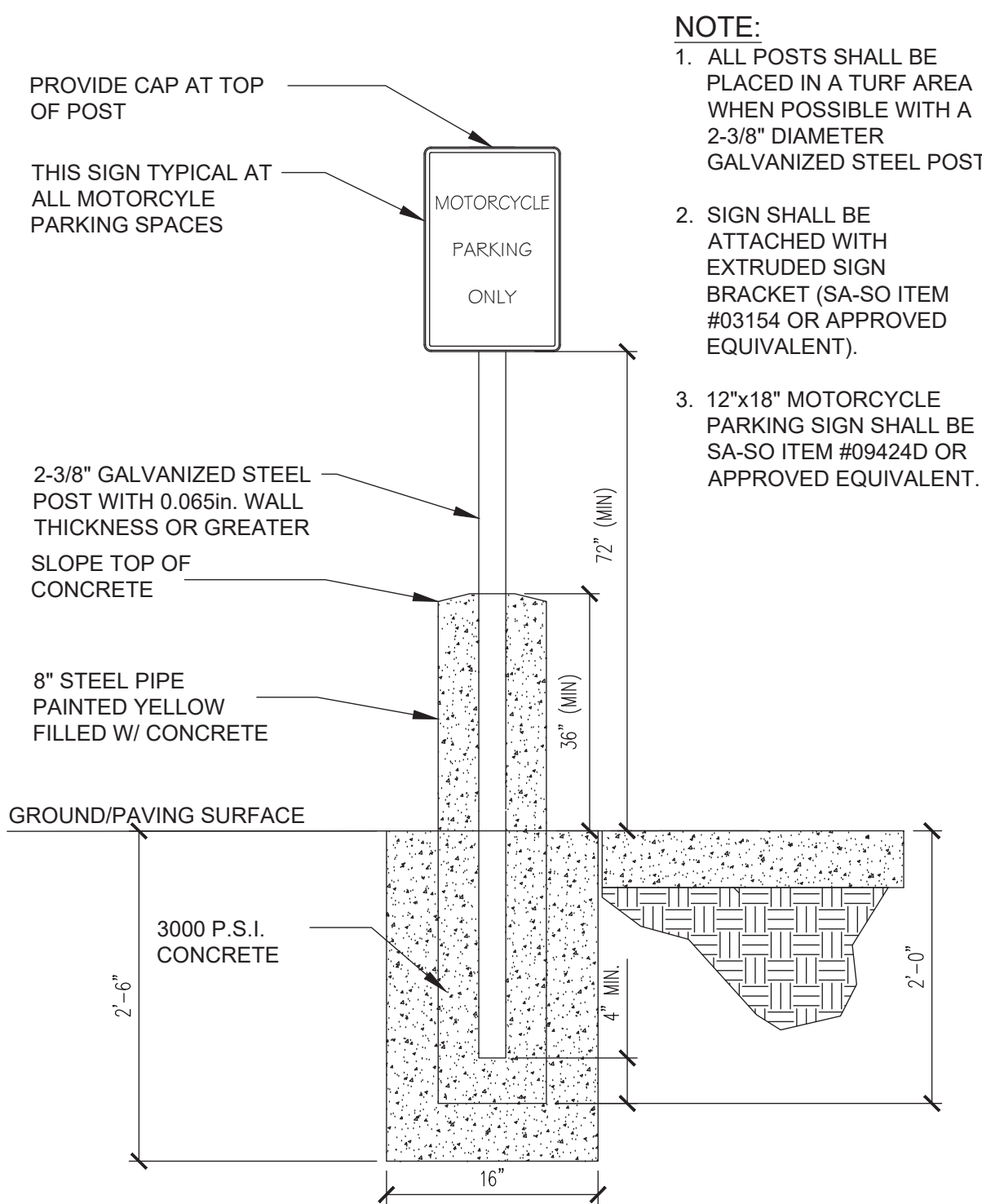
DETECTOR LOOP DETAIL AT DRIVE-THRU WINDOW
NOT TO SCALE



PRIMARY LANE DETECTOR LOOP DETAIL AT C.O.D.
NOT TO SCALE



SECONDARY LANE DETECTOR LOOP DETAIL AT C.O.D.
NOT TO SCALE



MOTORCYCLE PARKING SIGN DETAIL
NOT TO SCALE

NOTE:

1. ALL POSTS SHALL BE PLACED IN A TURF AREA WHEN POSSIBLE WITH A 2-3/8" DIAMETER GALVANIZED STEEL POST.
2. SIGN SHALL BE ATTACHED WITH EXTRUDED SIGN BRACKET (SA-SO ITEM #03154 OR APPROVED EQUIVALENT).
3. 12"x18" MOTORCYCLE PARKING SIGN SHALL BE SA-SO ITEM #09424D OR APPROVED EQUIVALENT.

| | | | |
|------------|-----|---------------------|----------------|
| BY | BMB | DESCRIPTION | ADAMS JOB NO.: |
| REV | HJM | ISSUE FOR PERMIT | 2017.095 |
| DATE | | RE-ISSUE FOR PERMIT | |
| 07/05/2018 | | | |
| 06/05/2020 | | | |



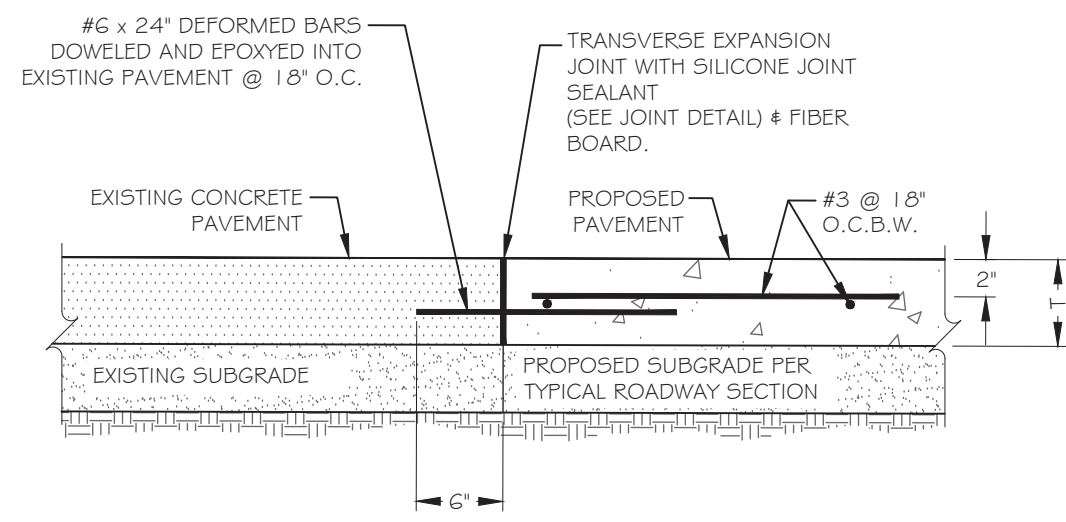
8951 Cypress Waters Blvd., Suite 150 ■ Dallas, Texas 75019 ■ (817) 334-3200



| | | | |
|--|---|--|----------|
| OFFICE | LONG BEACH FIELD OFFICE | MCDONALD'S LLC | 030-0127 |
| ADDRESS | 110 N. CARPENTER ST., CHICAGO, ILLINOIS 60607 | McDonald's USA, LLC <small>These drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be used, copied, or reproduced in any form without the written consent of McDonald's USA, LLC. Use of these drawings for reference or example on another project requires the services of properly licensed architects and engineers. Reproduction of the contract documents for reuse on another project is not authorized.</small> | |
| DATE | | 4300 OSUNA NE ALBUQUERQUE, NEW MEXICO | |
| PLAN APPROVALS SIGNATURE (2 REQUIRED) _____ CO-SIGN SIGNATURES _____ | | | |
| REGIONAL MGR. | | DATE | BY |
| CONST. MGR. | | | |
| OPERATIONS DEPT. | | | |
| REAL ESTATE DEPT. | | | |
| CONTRACTOR | | | |
| OWNER | | | |

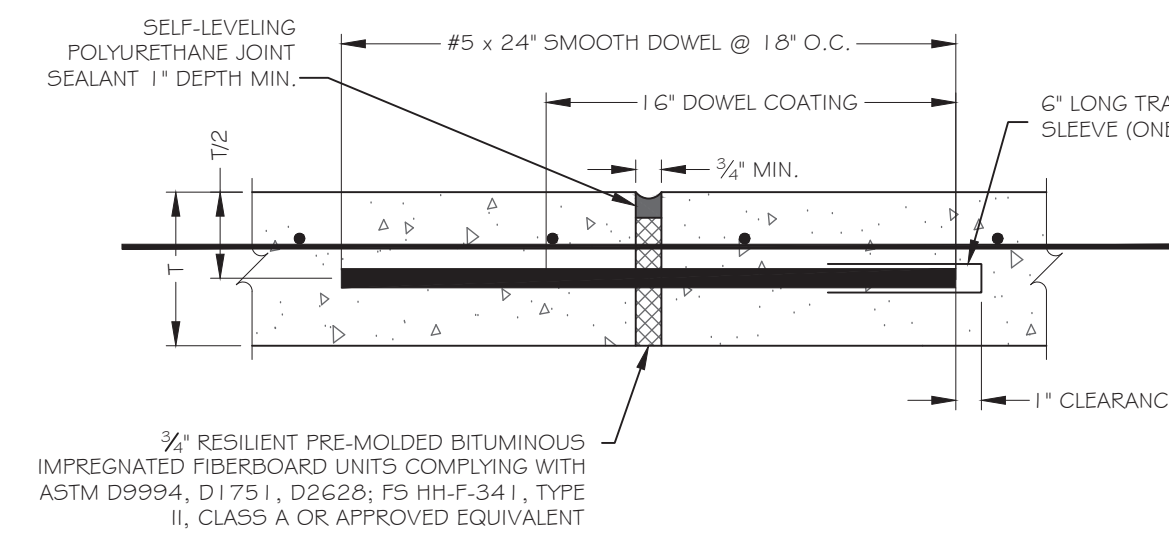
| | | |
|----------|----------|-----|
| DESIGNED | JUL 2018 | BMB |
| DRAWN | JUL 2018 | BMB |
| CHECKED | JUL 2018 | DL |
| AS-BUILT | | |

**STANDARD
DETAILS
C7.0**



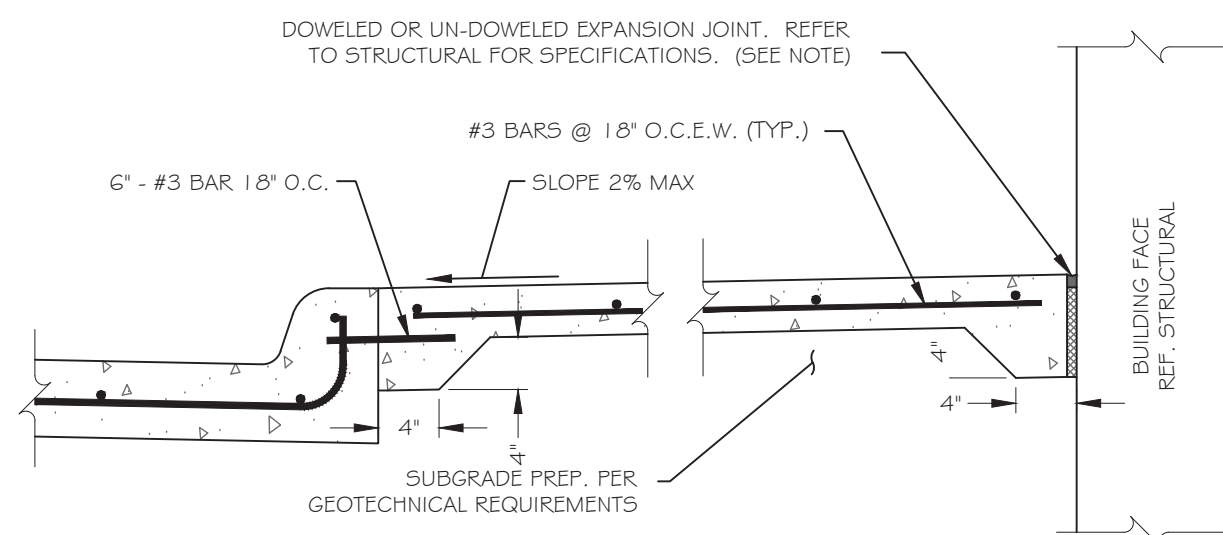
TRANSVERSE EXPANSION JOINT

NOT TO SCALE



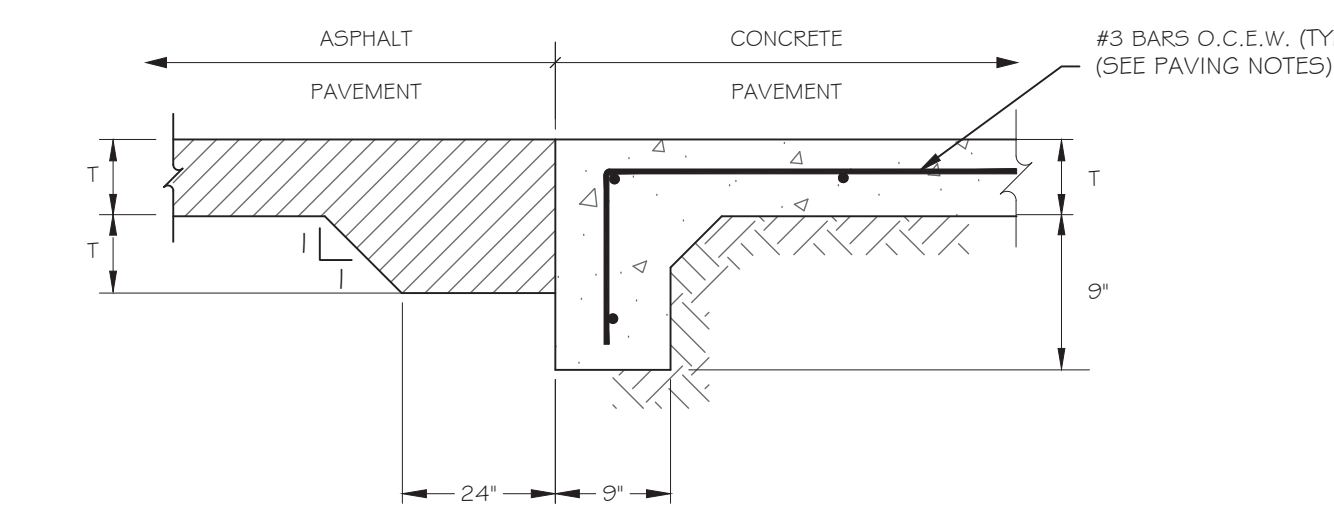
EXPANSION JOINT (ISOLATION)

NOT TO SCALE



SIDEWALK/BUILDING INTERFACE

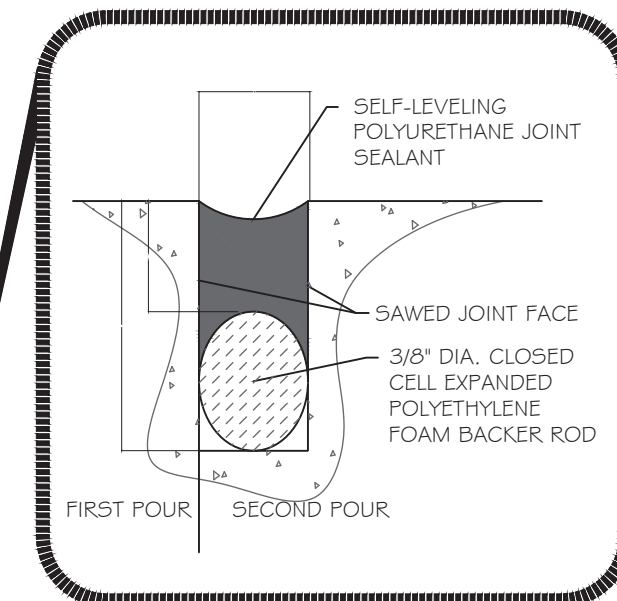
NOT TO SCALE



ASPHALT TO CONCRETE HEADER

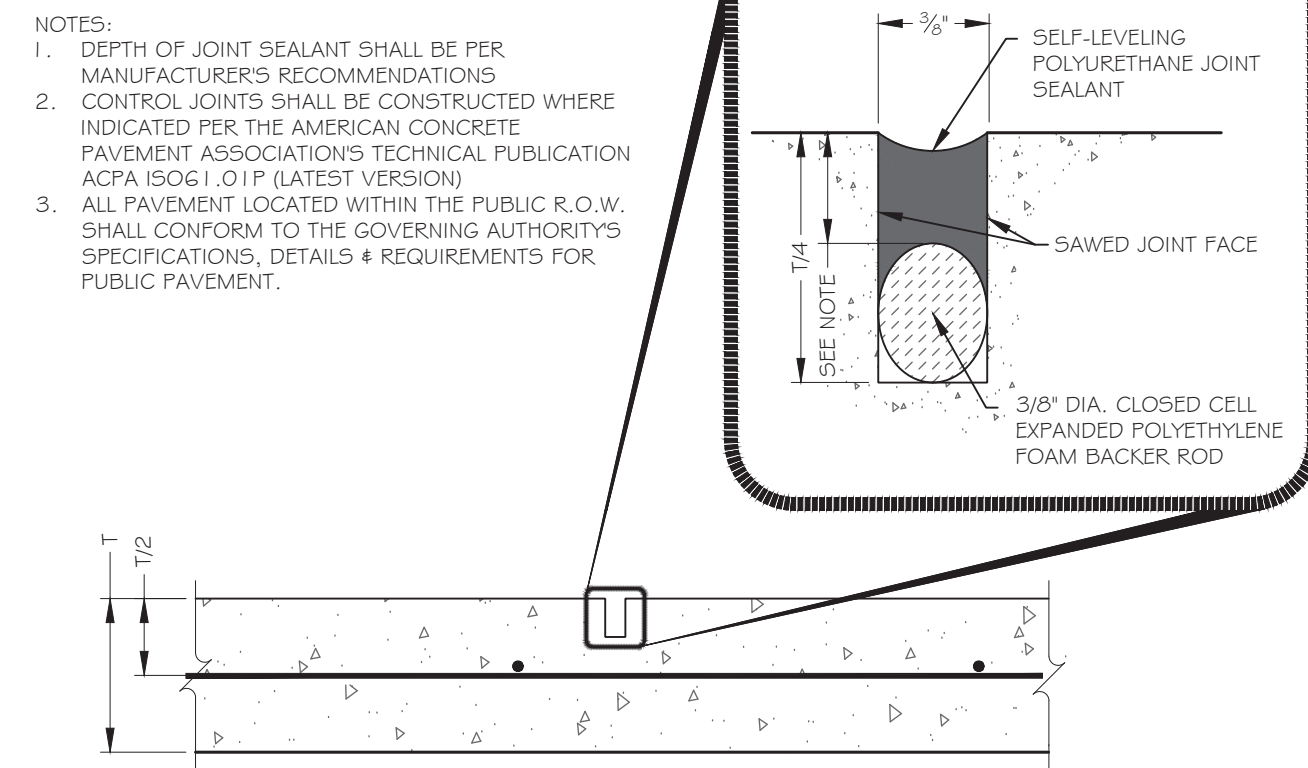
NOT TO SCALE

- NOTES:
1. DEPTH OF JOINT SEALANT SHALL BE PER MANUFACTURER'S RECOMMENDATIONS.
 2. ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR PUBLIC PAVEMENT.



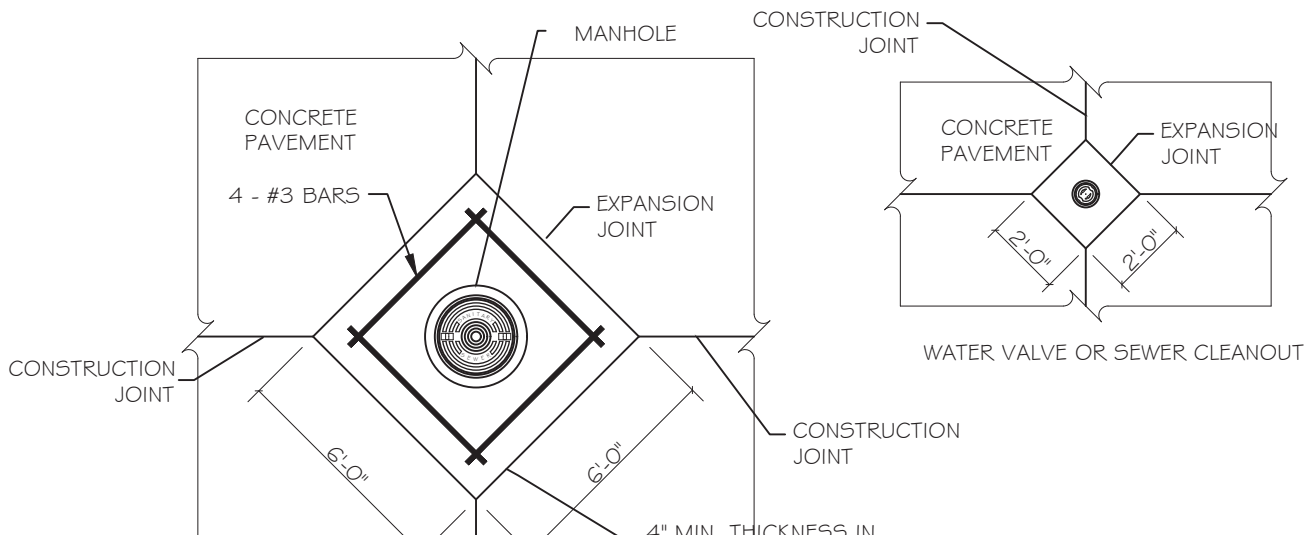
CONSTRUCTION BUTT JOINT

NOT TO SCALE



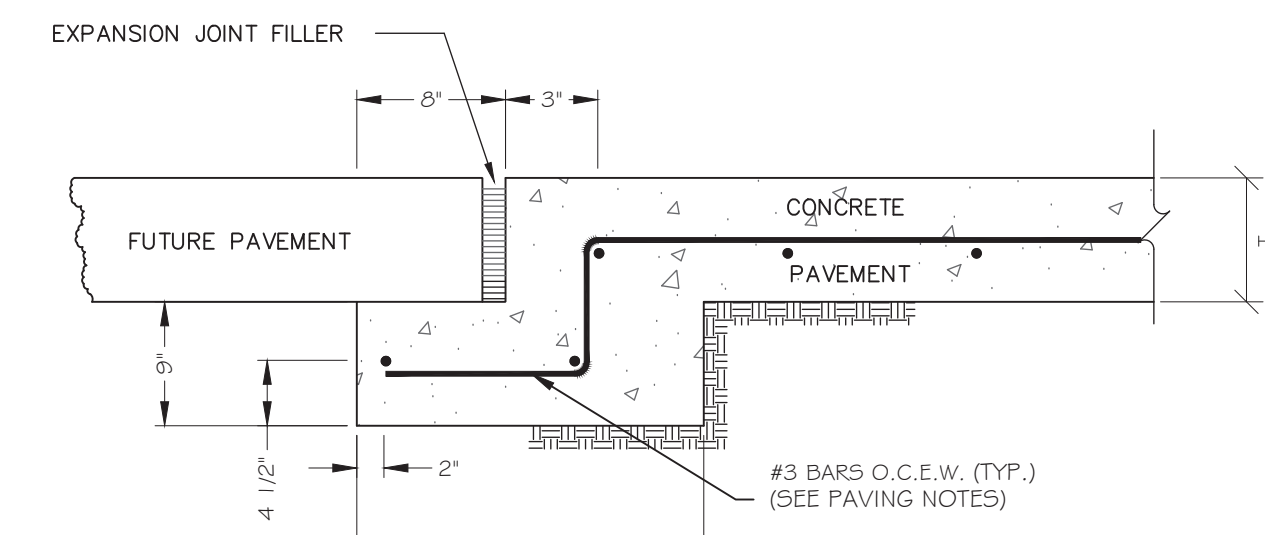
CONTROL JOINT (CONTRACTION)

NOT TO SCALE



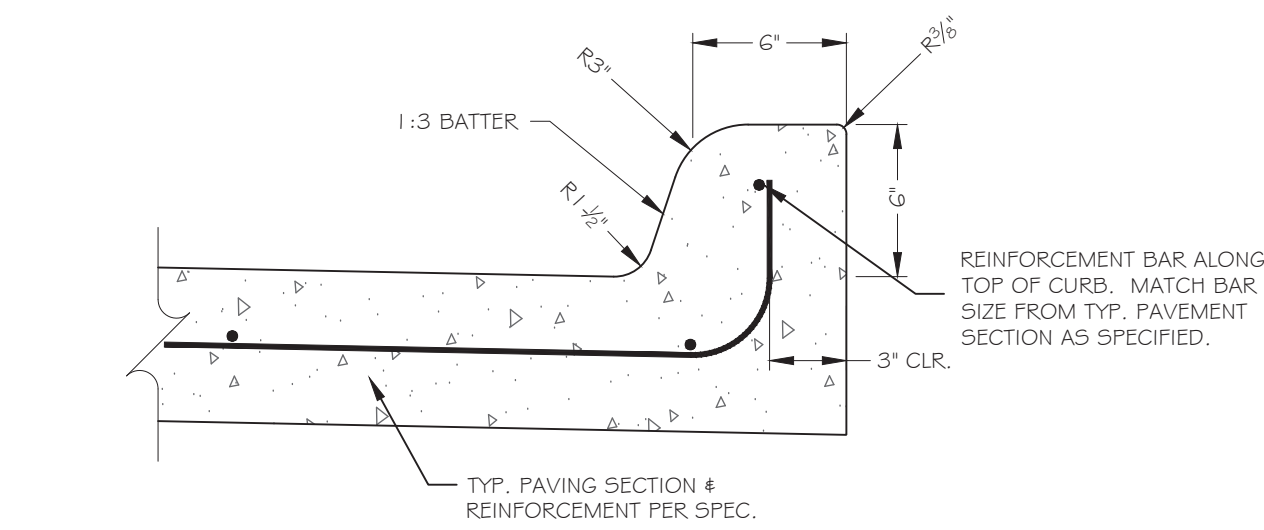
MANHOLE/VALVE/CLEANOUT JOINTING

NOT TO SCALE



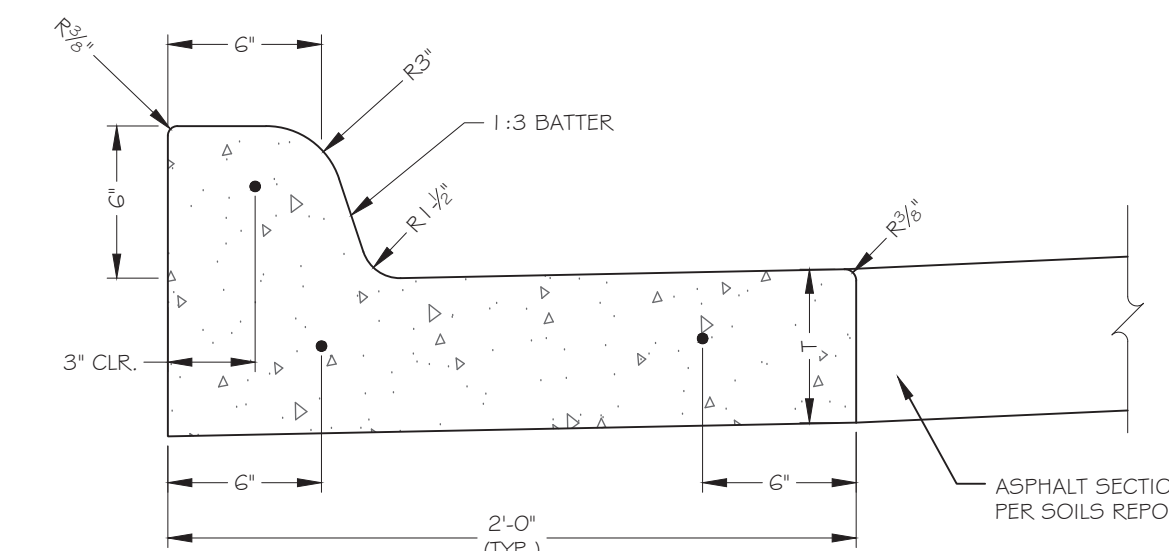
STREET HEADER FOR FUTURE PAVEMENT

NOT TO SCALE



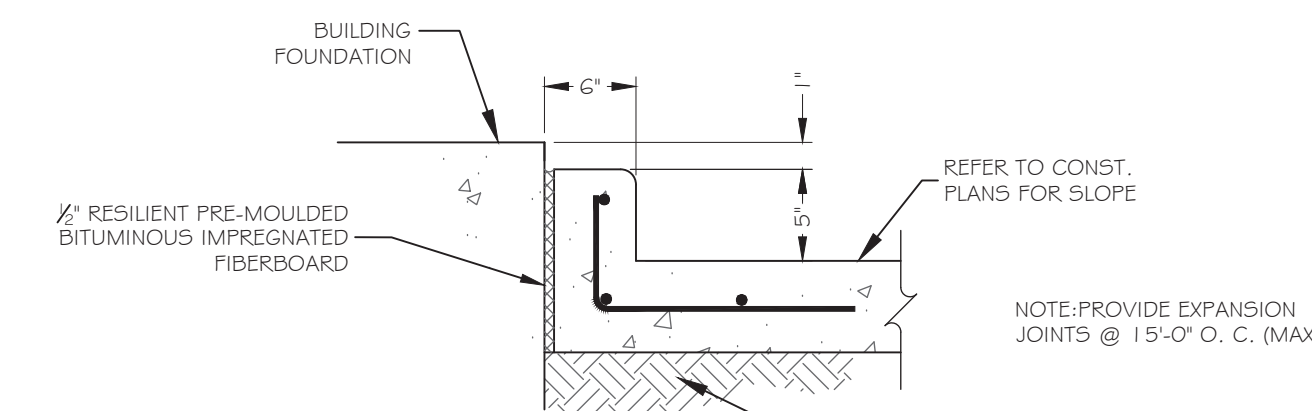
MONOLITHIC CURB

NOT TO SCALE



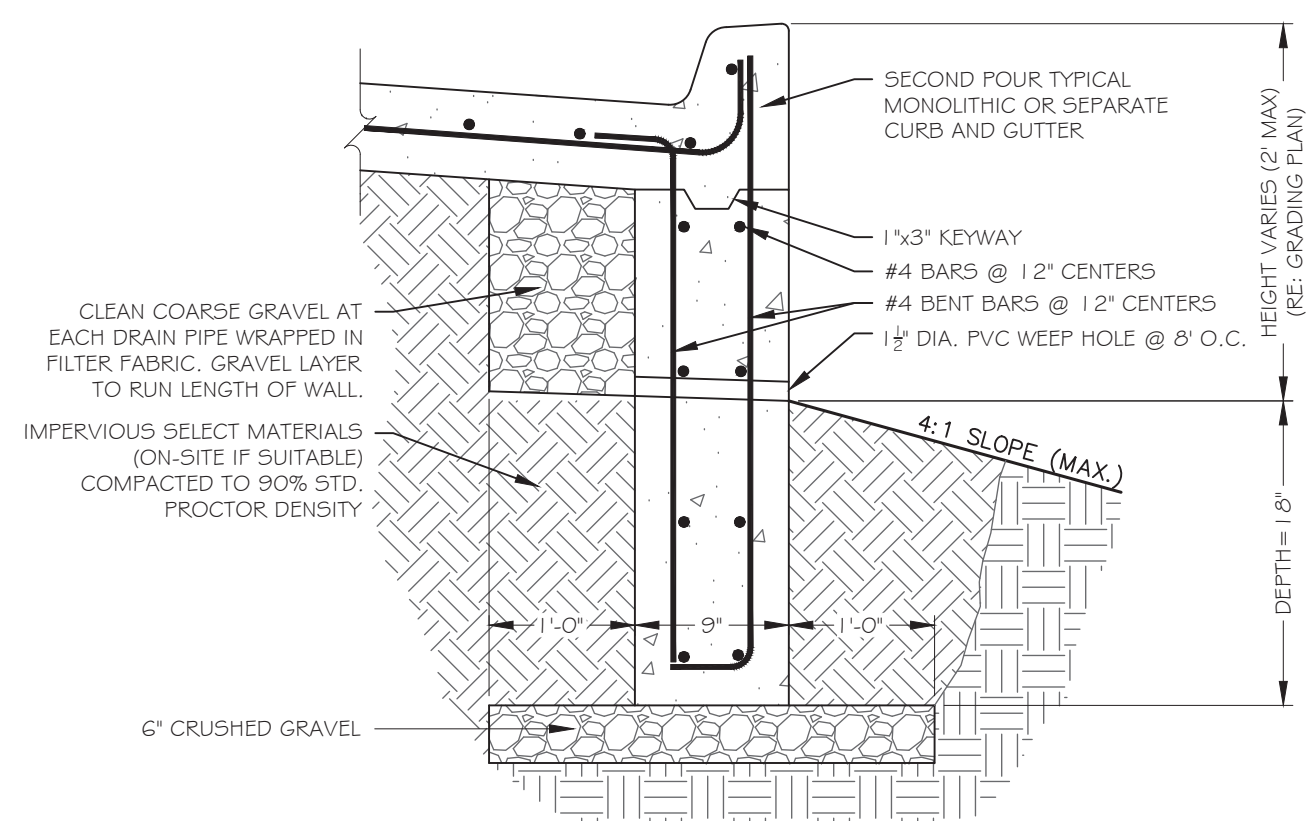
SEPARATE CURB & GUTTER

NOT TO SCALE



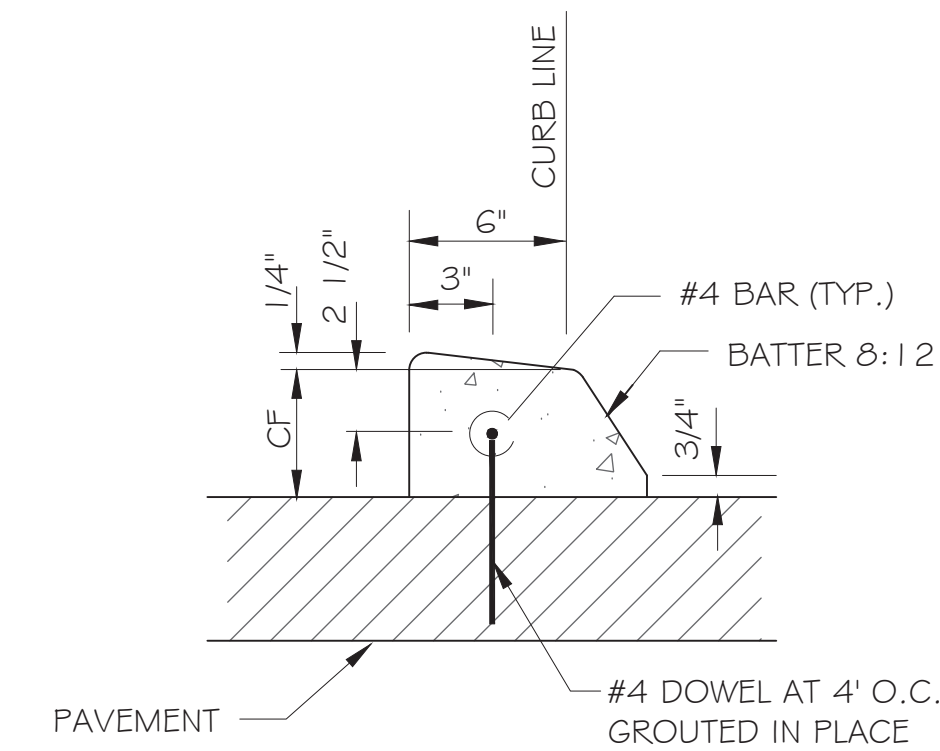
VERTICAL CURB @ DRIVE THRU

NOT TO SCALE



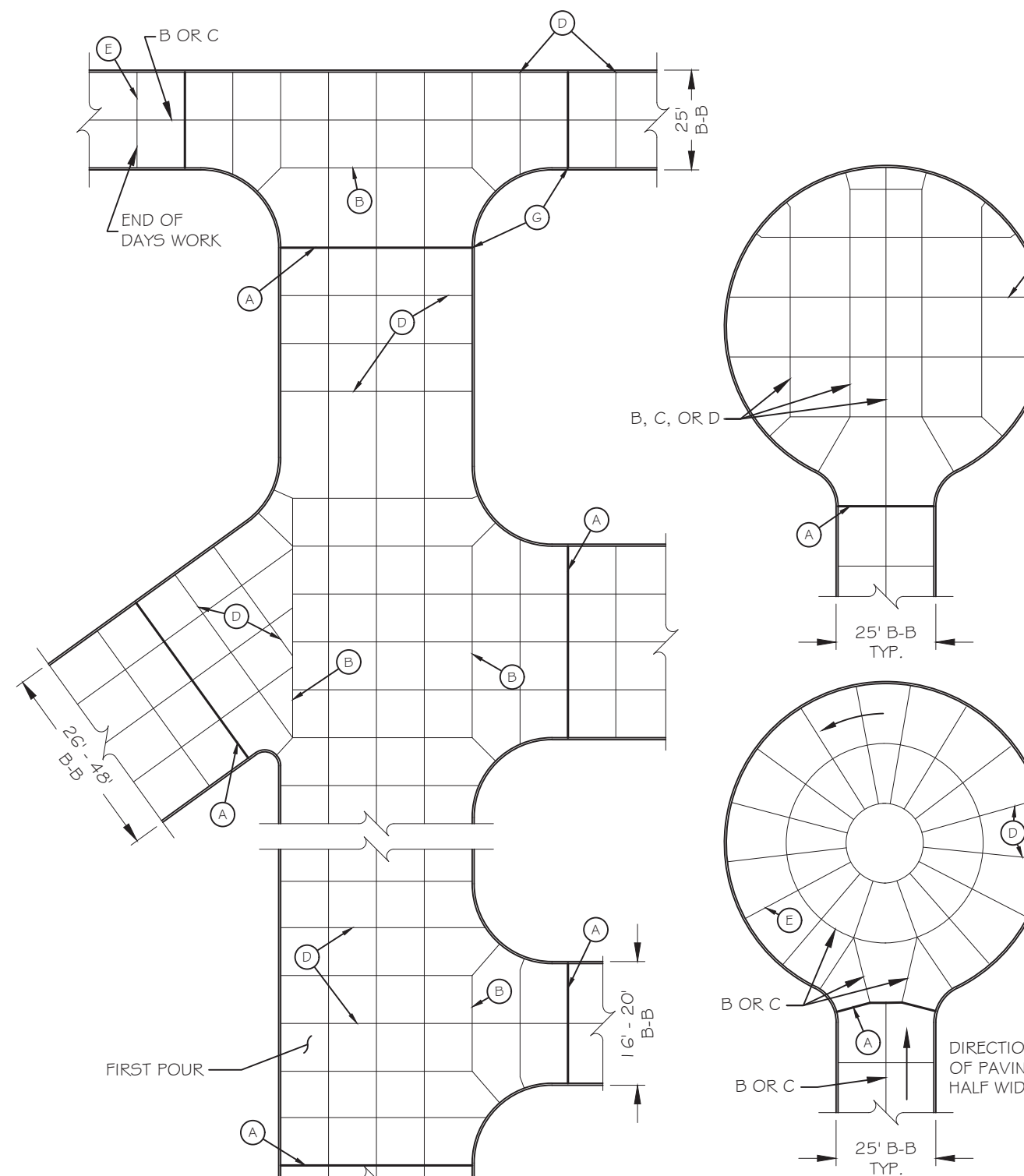
DEEPEMED CURB

NOT TO SCALE



DOWELED CURB

NOT TO SCALE



- KEY NOTES:
1. EXPANSION JOINT (ISOLATION)
 2. LONGITUDINAL CONSTRUCTION JOINT
 3. LONGITUDINAL CONTROL JOINT (CONTRACTION)
 4. TRANSVERSE CONTROL JOINT (CONTRACTION)
 5. PLANNED TRANSVERSE CONSTRUCTION JOINT
 6. EMERGENCY TRANSVERSE CONSTRUCTION JOINT
 7. PLACE 1/2" EXPANSION JOINT FILLER IN TOP OF CURB ONLY AT ALL RADIUS POINTS

- RULES:
1. AVOID ODD-SHAPED SLABS.
 2. MAXIMUM TRANSVERSE JOINT SPACING FOR PAVEMENT SHOULD EITHER BE 24 TO 30 TIMES THE SLAB THICKNESS OR 15ft.
 3. LONGITUDINAL JOINT SPACING SHOULD NOT EXCEED 12.5ft.
 4. KEEP SLABS AS SQUARE AS POSSIBLE. LONG NARROW SLABS TEND TO CRACK MORE THAN SQUARE ONES.
 5. ALL TRANSVERSE CONTRACTION JOINTS MUST BE CONTINUOUS THROUGH THE CURB AND HAVE A DEPTH EQUAL TO 1/4 THE PAVEMENT THICKNESS.
 6. IN ISOLATION JOINTS, THE FILLER MUST BE FULL DEPTH AND EXTEND THROUGH THE CURB.
 7. IF THERE IS NO CURB, LONGITUDINAL JOINTS SHOULD BE TIED WITH DEFORMED BARS.
 8. OFFSETS AT RADIUS POINTS SHOULD BE AT LEAST 1.5ft WIDE. JOINT INTERSECTION ANGLES LESS THAN 60° SHOULD BE AVOIDED.
 9. MINOR ADJUSTMENTS IN JOINT LOCATION MADE BY SHIFTING OF SKEWING TO MEET INLETS AND MANHOLES WILL IMPROVE PAVEMENT PERFORMANCE.
 10. WHEN THE PAVEMENT AREA HAS DRAINAGE STRUCTURES, PLACE JOINTS TO MEET THE STRUCTURES IF POSSIBLE.

TYPICAL JOINTING LAYOUT

NOT TO SCALE

| REV | DATE | DESCRIPTION |
|------------|------|---------------------|
| 07/06/2018 | | ISSUE FOR PERMIT |
| 06/05/2020 | | RE-ISSUE FOR PERMIT |



McDonald's USA, LLC

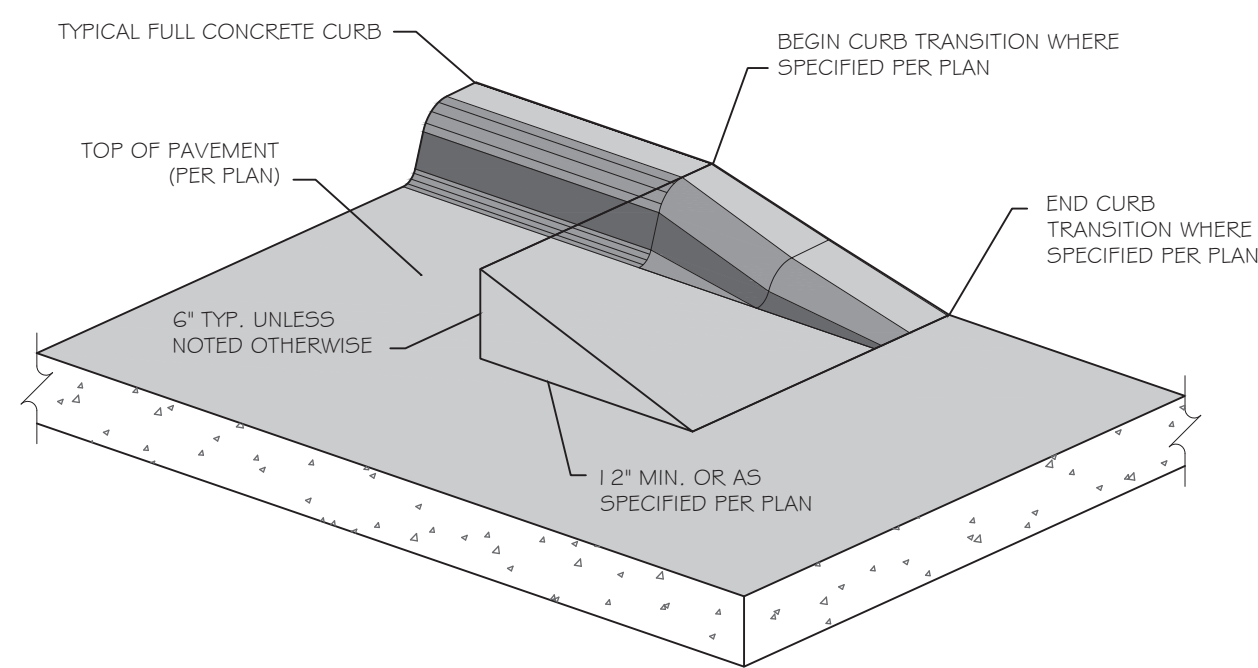
McDONALD'S U.S. CHICAGO, ILLINOIS 60607

110 N. CARPENTER ST., CHICAGO, ILLINOIS 60607

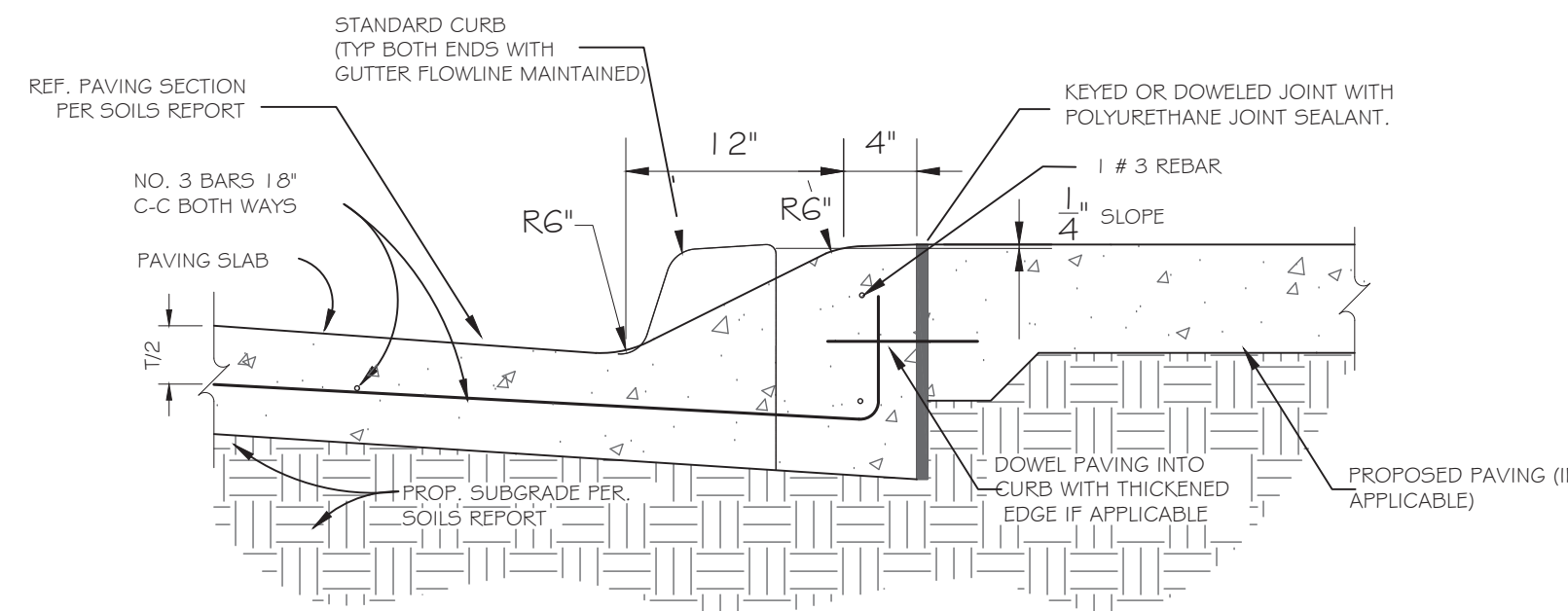
4300 OSUNA NE ALBUQUERQUE, NEW MEXICO

| PLAN APPROVALS | DATE | BY |
|---|------|----|
| SIGNATURE (2 REQUIRED) | | |
| REGIONAL MGR. | | |
| CONST. MGR. | | |
| OPERATIONS DEPT. | | |
| REAL ESTATE DEPT. | | |
| EMERGENCY TRANSVERSE CONSTRUCTION JOINT | | |
| CO-SIGN SIGNATURES | | |
| CONTRACTOR | | |
| OWNER | | |

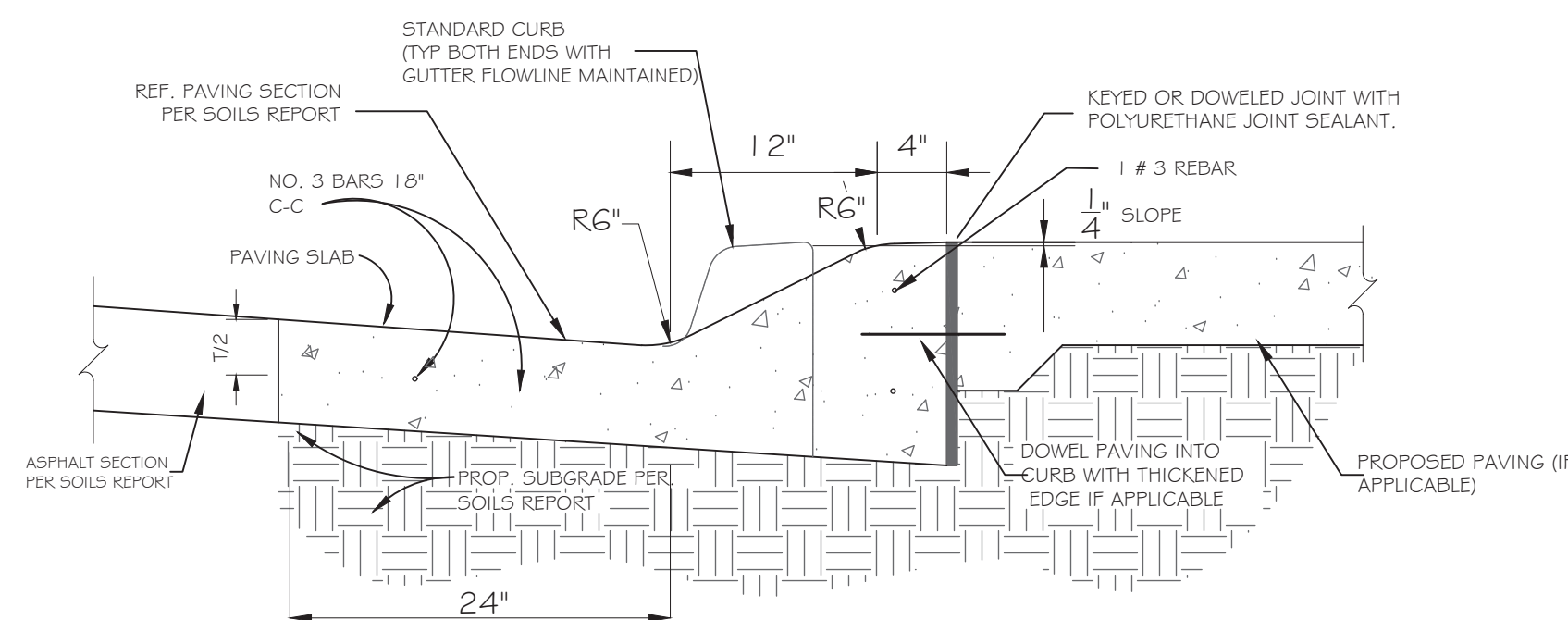
| DESIGNED | DATE | BY |
|-------------------------|----------|-----|
| | JUL 2018 | BMB |
| DRAWN | DATE | BY |
| | JUL 2018 | BMB |
| CHECKED | DATE | BY |
| | JUL 2018 | DL |
| AS-BUILT | DATE | BY |
| | | |
| STANDARD DETAILS | | |
| C7.2 | | |



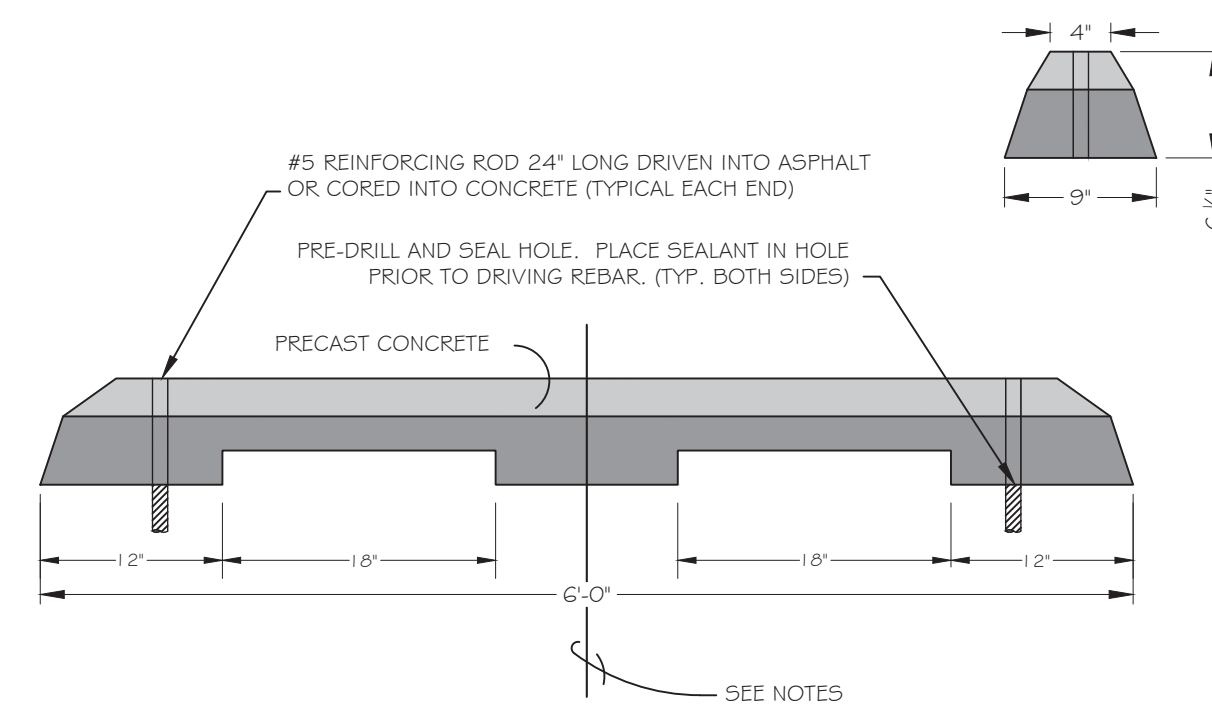
TRANSITION CURB
NOT TO SCALE



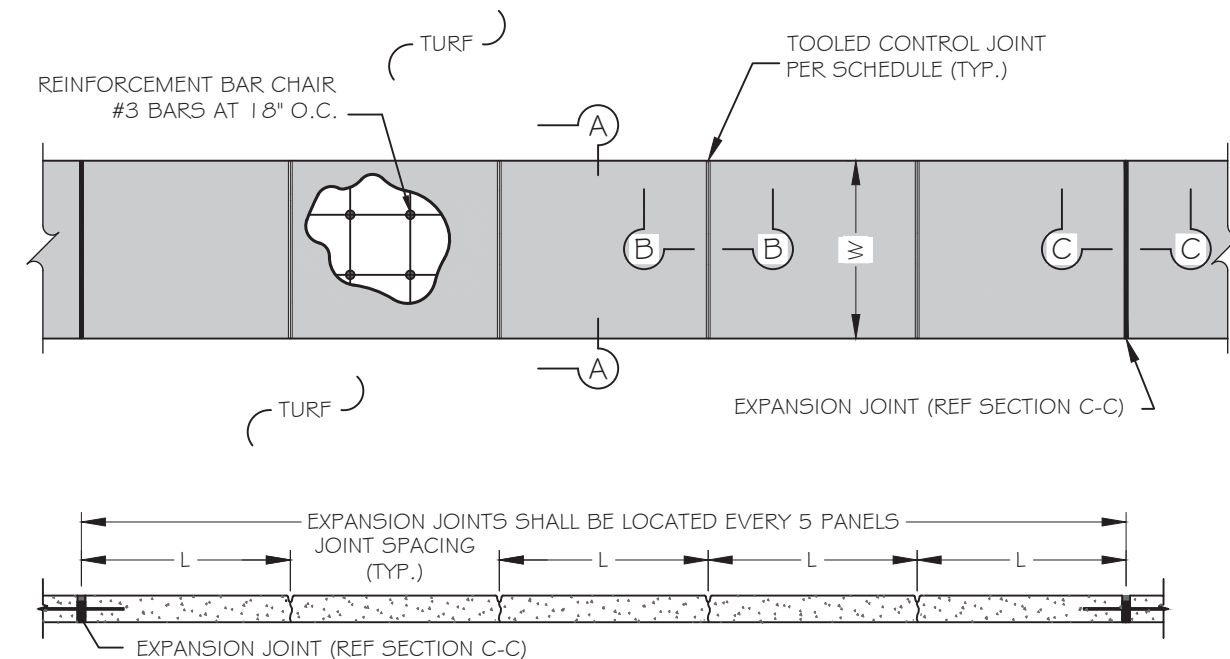
MONOLITHIC MOUNTABLE CURB
NOT TO SCALE



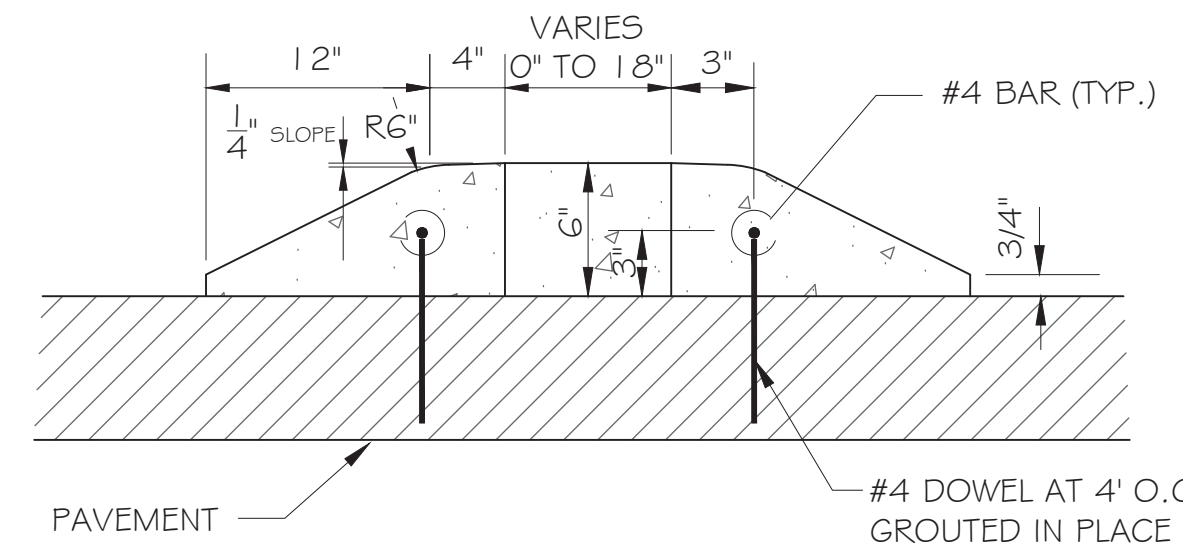
SEPARATE MOUNTABLE CURB & GUTTER
NOT TO SCALE



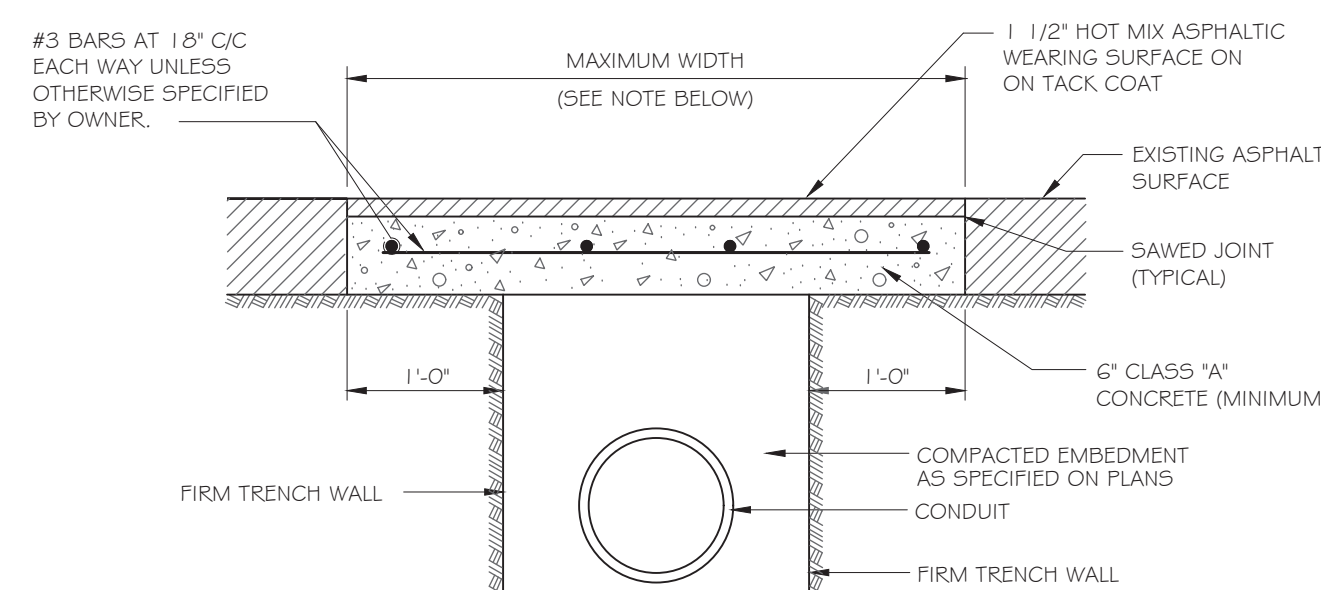
WHEEL STOP
NOT TO SCALE



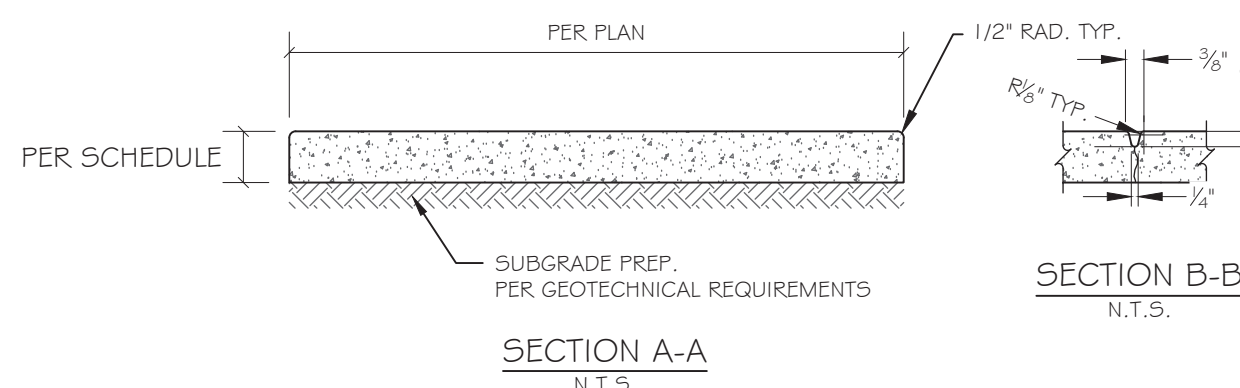
- SIDEWALK NOTES:**
- SAND CUSHION WILL NOT BE PERMITTED UNDER SIDEWALKS OR OTHER PAVEMENT.
 - DOVEL WITH #4 BARS AT 18" C-C WHEN CONNECTING TO EXISTING SIDEWALKS, DRIVEWAYS, CURBS AND GUTTER.
 - CONCRETE STRENGTH FOR SIDEWALKS SHALL BE A MINIMUM OF 3,600 PSI @ 28 DAYS OR AS SPECIFIED BY THE AUTHORITY HAVING JURISDICTION, WHICHEVER IS GREATER.
 - ALL JOINTS LOCATED WITHIN LARGE AREAS OF CONCRETE PLATWORK (PLAZA AREAS OR PAVEMENT BETWEEN BUILDINGS) SHALL BE SEALED WITH POLYURETHANE JOINT SEALANT PER JOINT DETAILS THIS SHEET.
 - CONTRACTOR SHALL SUBMIT FULL-SIZE SCALEABLE PLAZA AREAS JOINT LAYOUT FOR APPROVAL.
 - JOINT SEALANT IS NOT REQUIRED ON SIDEWALKS LOCATED IN TURF AREAS.
 - ALL SIDEWALKS LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR SIDEWALKS.



MOUNTABLE CURB RAISED ISLAND
NOT TO SCALE



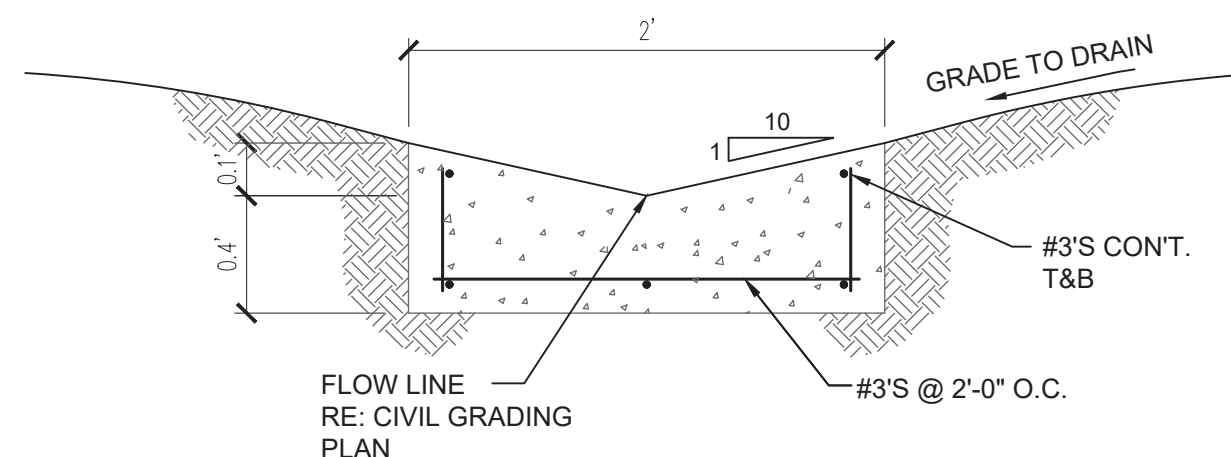
ASPHALT PAVEMENT
N.T.S.



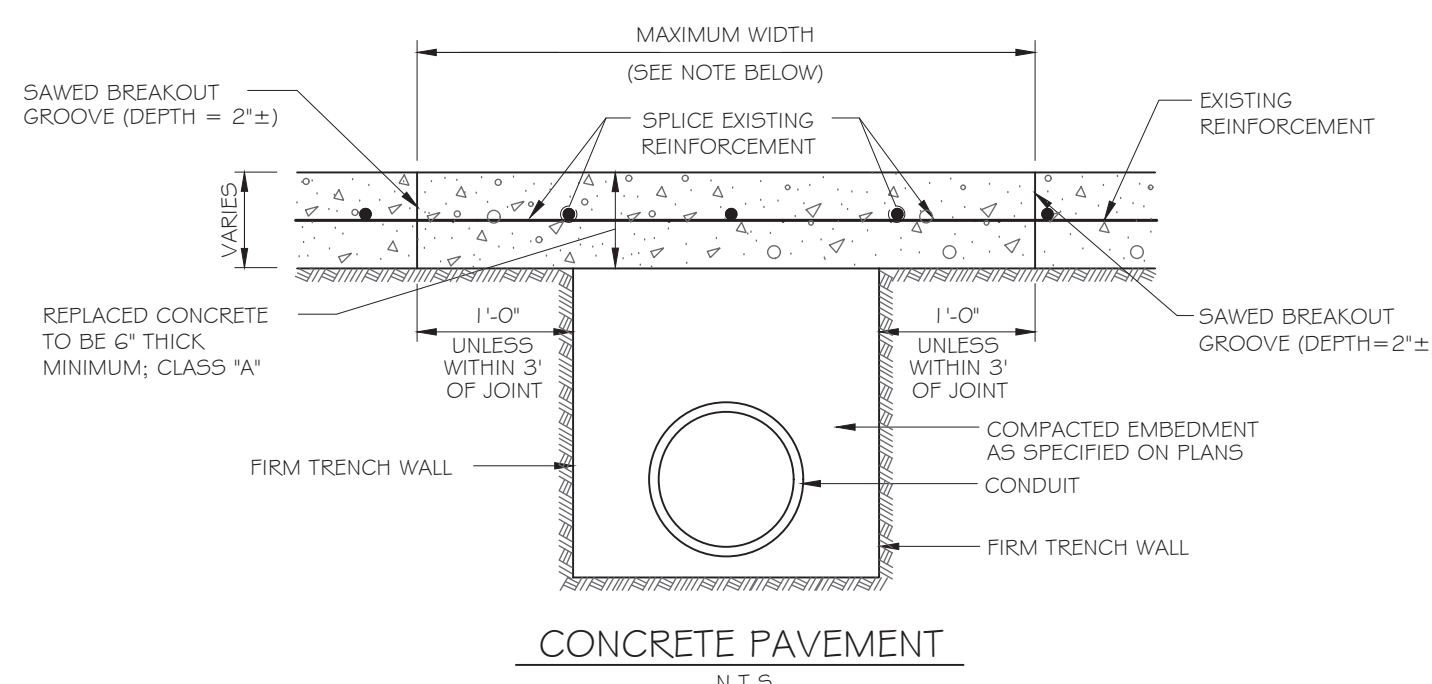
SIDEWALK PANEL SCHEDULE

| SIDEWALK WIDTH = W (FT.) | SIDEWALK THICKNESS = T (IN.) | JOINT SPACING = L (FT.) |
|--------------------------|------------------------------|-------------------------|
| 4 | 4 | 4 |
| 5 | 4 | 5 |
| 6 | 4 | 5 |
| 7 | 5 | 7 |
| 8 | 5 | 8 |
| 9 | 5 | 9 |
| 10 | 5 | 10 |

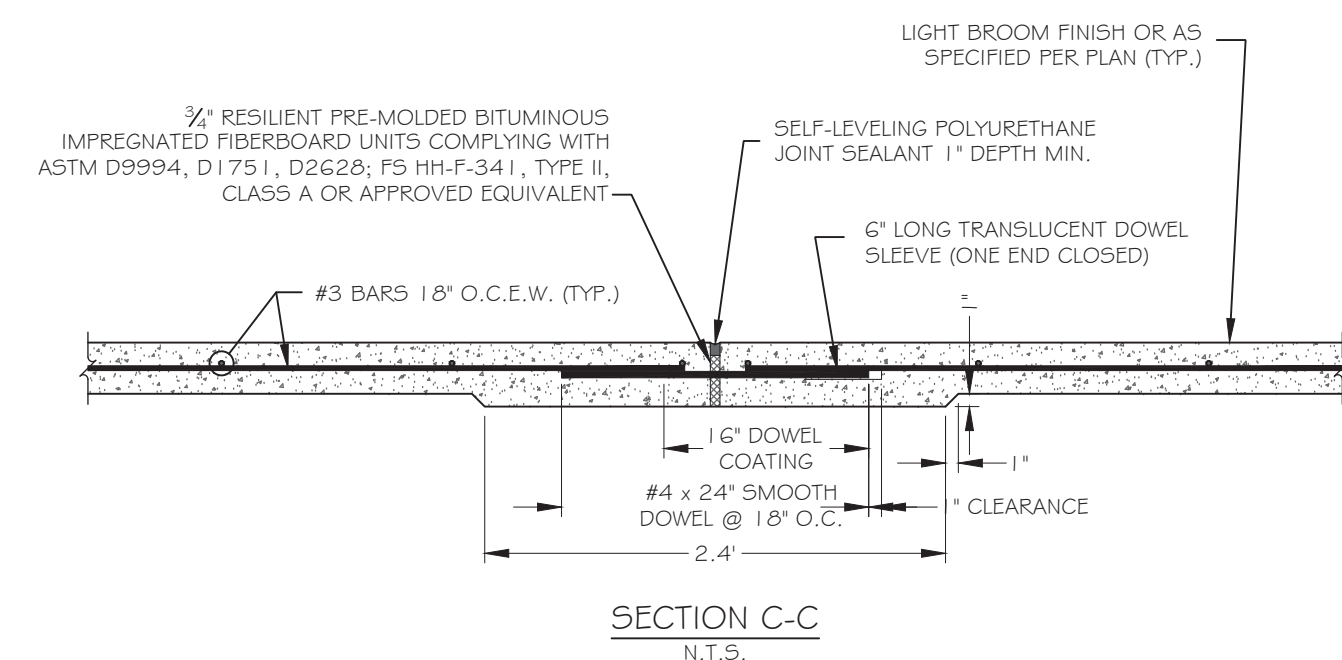
LANDSCAPE SIDEWALK
NOT TO SCALE



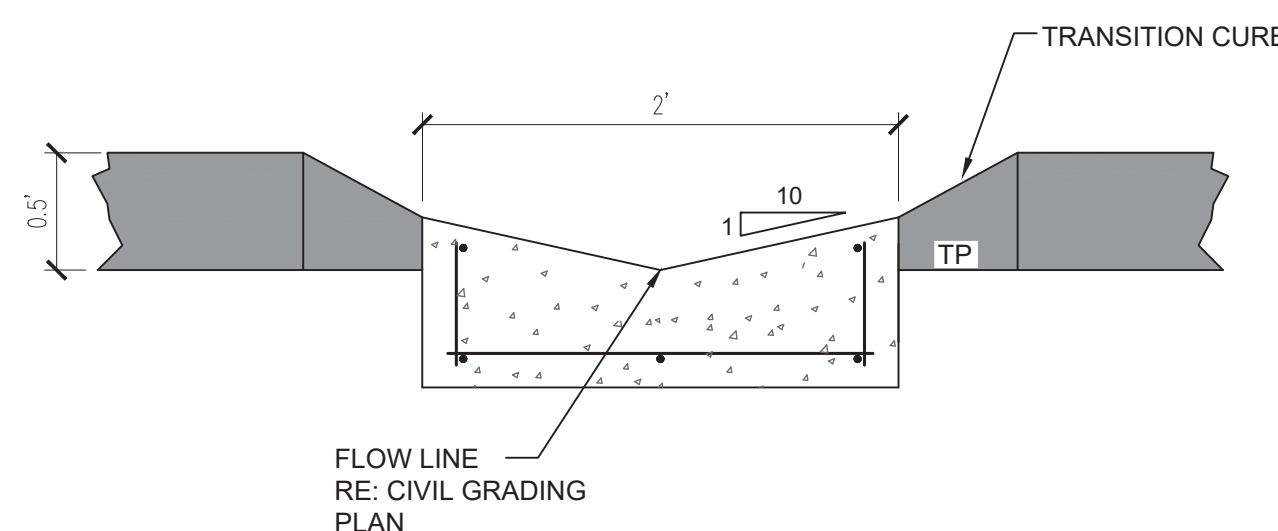
V-NOTCH CONCRETE SWALE IN GRASS AREA
NOT TO SCALE



CONCRETE PAVEMENT
N.T.S.



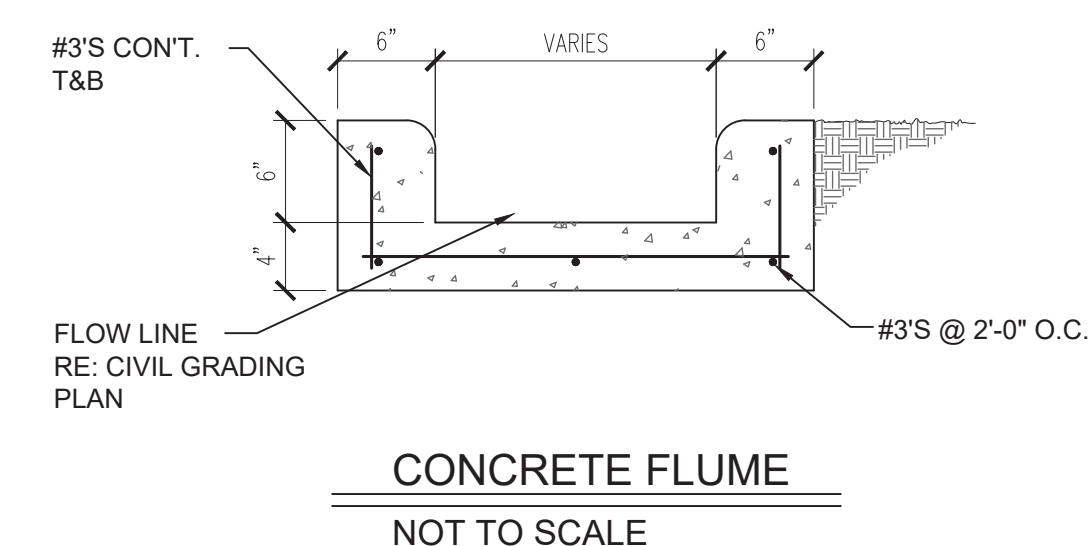
- NOTES:**
- SLEEVES FOR DOWELS SHALL HAVE AN INSIDE DIAMETER OF $\frac{1}{2}$ " GREATER THAN THE DIAMETER OF THE DOWELS AND SHALL BE SUBMITTED TO ENGINEER FOR APPROVAL PRIOR TO USE.
 - DOWEL COATING SHALL BE ASPHALTIC COATING.



V-NOTCH CONCRETE SWALE & TRANSITION CURB
NOT TO SCALE

- NOTE:**
- WHEN REMOVING CONCRETE PAVEMENT THE CONTRACTOR SHALL ENDEAVOR TO LIMIT DAMAGE TO EXISTING REINFORCEMENT SO IT MAY BE EMPLOYED IN THE REPLACEMENT OPERATION. IF ORIGINAL REINFORCEMENT IS CUT OR BROKEN, REPLACEMENT BARS OF THE SAME SIZE SHALL BE INSTALLED BY DRILLING AND DOWELING AS DIRECTED BY THE OWNER.

PAVEMENT CUT REMOVAL & REPLACEMENT
NOT TO SCALE



CONCRETE FLUME
NOT TO SCALE

| REV | DATE | DESCRIPTION |
|------------|------|---------------------|
| 07/06/2018 | | ISSUE FOR PERMIT |
| 06/05/2020 | | RE-ISSUE FOR PERMIT |



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4300 OSUNA NE
ALBUQUERQUE, NEW MEXICO

PLAN APPROVALS

| REGIONAL MGR. | DATE | SIGNATURE (2 REQUIRED) |
|---------------|------|------------------------|
| | | |
| | | |

CO-SIGN SIGNATURES

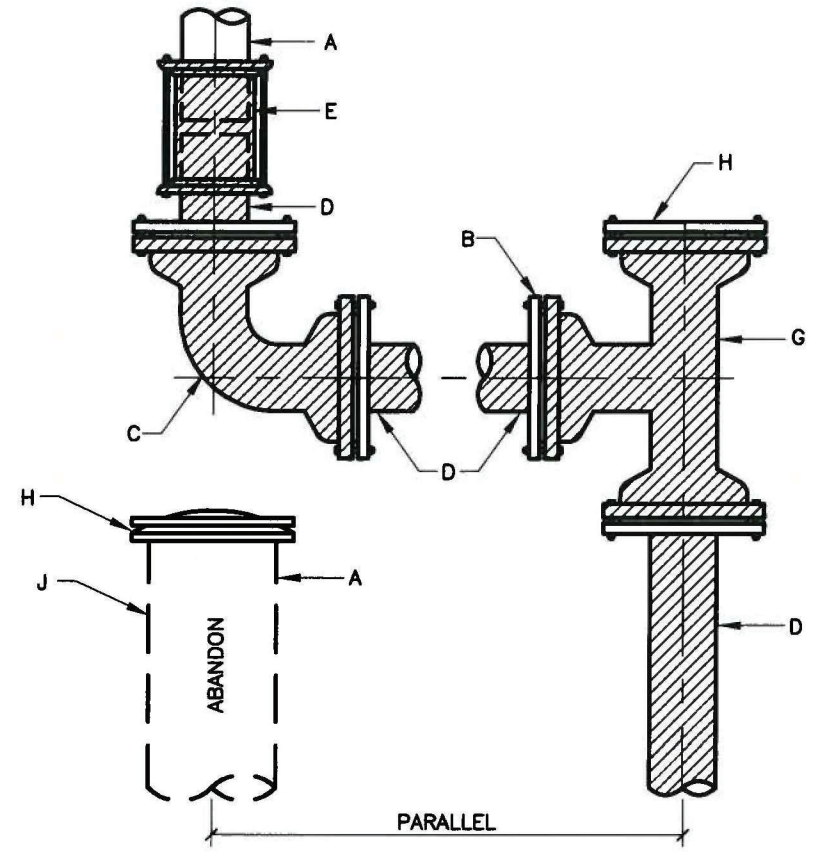
| CONTRACTOR | OWNER |
|------------|-------|
| | |

| DESIGNED | DATE | BY |
|----------|----------|-----|
| | JUL 2018 | BMB |
| DRAWN | DATE | BY |
| | JUL 2018 | BMB |
| CHECKED | DATE | BY |
| | JUL 2018 | DL |

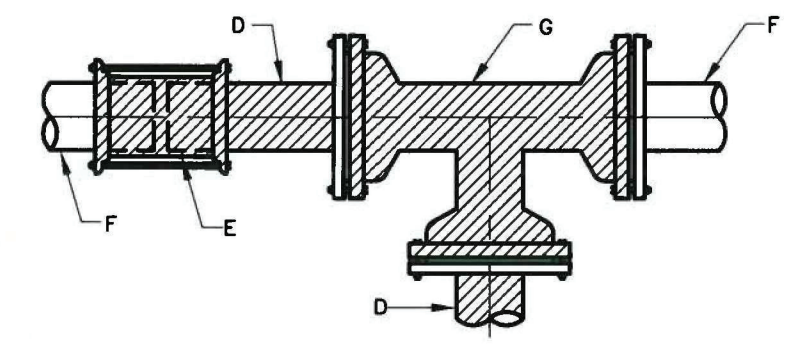
AS-BUILT

STANDARD DETAILS

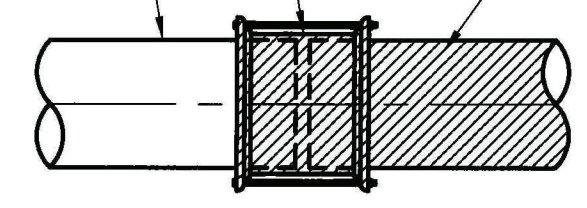
C7.3



REPLACEMENT OF STEEL LINES 4"-12"
CONNECTION DETAILS



TEE INSERTION D.I., P.V.C. OR A.C. PIPE

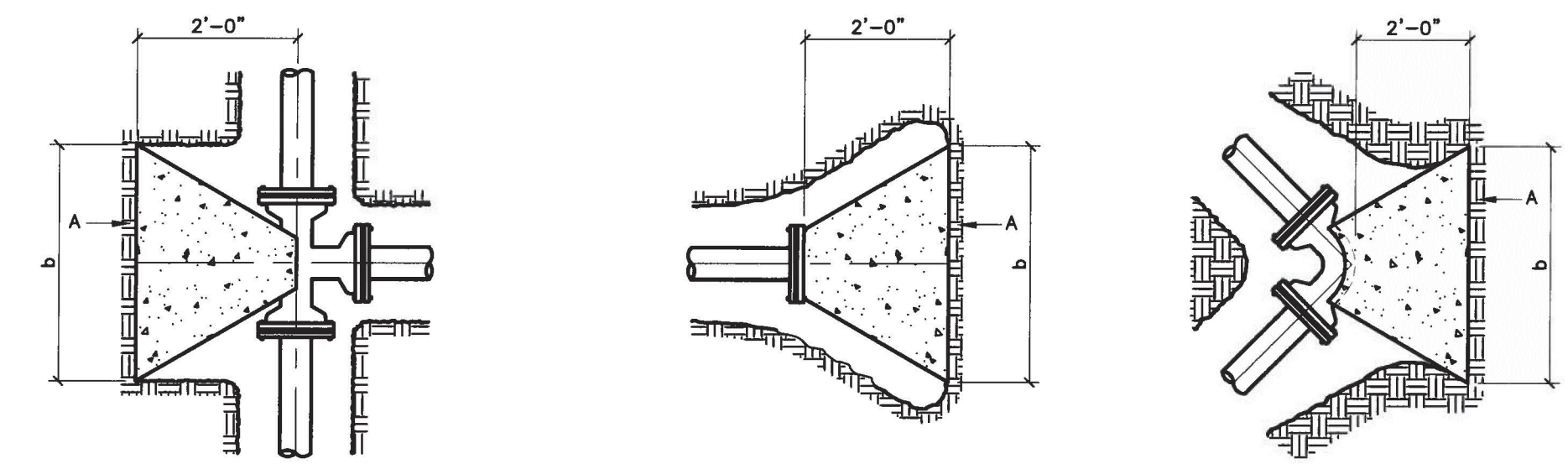


TRANSITION COUPLING
FROM D.I., P.V.C., OR A.C.
TO D.I. OR P.V.C.

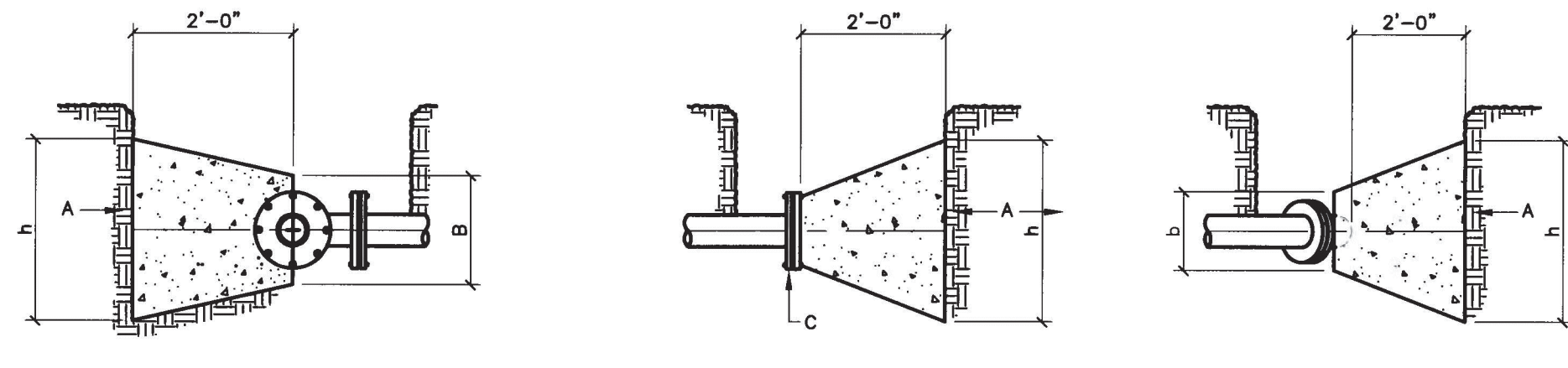
- GENERAL NOTES:**
- 1 ALL NEW PIPE AND FITTINGS SHALL BE PROVIDED WITH THRUST CONTROL.
 - 2 THRUST CONTROL SHALL BE BY RESTRAINED JOINTS ONLY UNLESS DIRECTED OTHERWISE BY ENGINEER.
 - 3 EMD'S ARE REQUIRED AT VALVES, TEES, FLANGED OUTLETS (ON CONCRETE CYLINDER PIPES), AND CAPPED OR PLUGGED ENDS. SEE SPECIFICATION SECTION 170 FOR LOCATIONS.

- CONSTRUCTION NOTES:**
- A EXISTING STEEL PIPE.
 - B REDUCE AT TEE, IF EXISTING LINE IS SMALLER THAN NEW LINE.
 - C M.J., C.I. ELBOW WITH JOINT RESTRAINT.
 - D NEW D.I. OR P.V.C., WITH VALVE AS DIRECTED.
 - E RESTRAINED TRANSITION COUPLING FOR A.C. RESTRAINED SOLID SLEEVE FOR D.I., C.I. AND PVC.
 - F EXISTING D.I., C.I., P.V.C., OR A.C. IF A.C., USE ADAPTER APPROVED BY ENGINEER OR AS APPROVED ON THE CURRENT WATER AUTHORITY APPROVED PRODUCTS LIST.
 - G M.J. D.I. TEE WITH JOINT RESTRAINT.
 - H M.J., C.I. PLUG OR CAP WITH JOINT RESTRAINT.
 - J REMOVE AT LEAST 10' OF PIPE TO BE ABANDONED AND CAP OR PLUG.

| | |
|-----------|--|
| REVISIONS | WATER AUTHORITY |
| | WATER WATERLINE CONNECTION DETAILS |
| | DWG. 2301 JANUARY 2011 |



PLAN PLAN PLAN



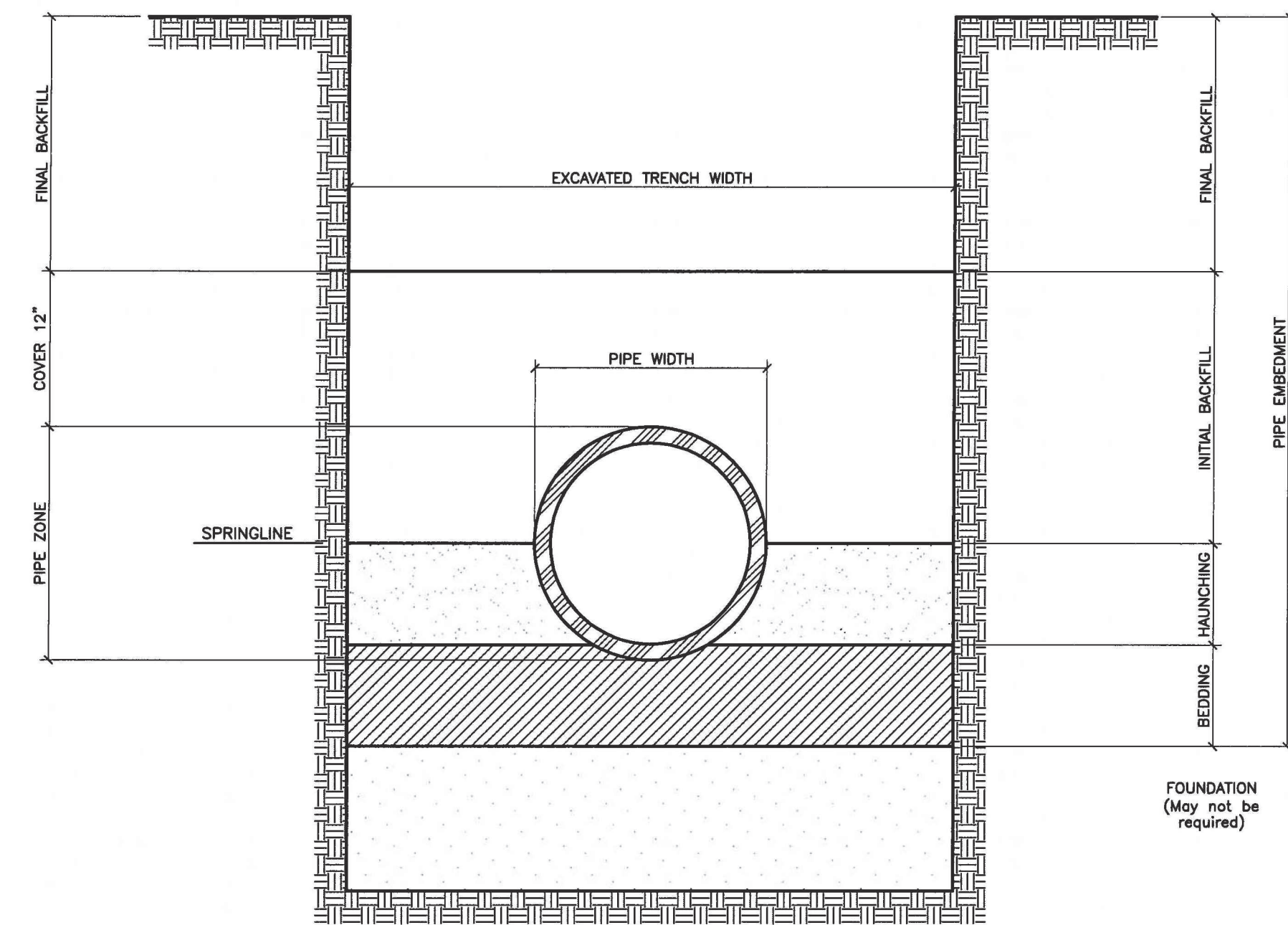
ELEVATION CONCRETE BLOCKING FOR TEE
ELEVATION CONCRETE BLOCKING FOR PLUG OR CAP
ELEVATION CONCRETE BLOCKING FOR ELBOW

- GENERAL NOTES:**
- 1 ALL THRUST CONTROL BY RESTRAINED JOINTS ONLY UNLESS DIRECTED BY ENGINEER, AND FOR "SPECIAL" SITUATIONS SPECIFIED BY THE WATER AUTHORITY.
 - 2 PIPE SIZE GREATER THAN 14" REQUIRES DESIGN BY ENGINEER TO BE SUBMITTED TO THE WATER AUTHORITY FOR APPROVAL.
 - 3 CONCRETE BLOCKING PER SEC. 101 EXTERIOR CONCRETE, f'c=3000 psi @ 28 DAYS.

| PIPE SIZE | ELBOW ANGLE | ELBOW (b) DIM. | ELBOW (h) DIM. | TEE OR PLUG (b) DIM. | TEE OR PLUG (h) DIM. |
|-----------|-------------|----------------|----------------|----------------------|----------------------|
| 4" | | | | 2' | 1' |
| 4" | 90° 45' | 2' | 2' | | |
| 4" | 22 1/2° | 2' | 2' | | |
| 6" | | | | 2' | 2' |
| 6" | 90° 45' | 2' | 2' | | |
| 6" | 22 1/2° | 2' | 2' | | |
| 8" | | | | 3' | 3' |
| 8" | 90° | 3' | 3' | | |
| 8" | 45° | 2' | 2' | | |
| 8" | 22 1/2° | 2' | 2' | | |
| 10" | | | | 3' | 3' |
| 10" | 90° | 3'-6" | 3'-6" | | |
| 10" | 45° | 3' | 3' | | |
| 10" | 22 1/2° | 2' | 2' | | |
| 12" | | | | 3'-6" | 3'-6" |
| 12" | 90° | 4' | 4' | | |
| 12" | 45° | 3'-6" | 3'-6" | | |
| 12" | 22 1/2° | 2' | 2' | | |
| 14" | | | | 4' | 4' |
| 14" | 90° | 5' | 5' | | |
| 14" | 45° | 3'-6" | 3'-6" | | |
| 14" | 22 1/2° | 3' | 3' | | |

- CONSTRUCTION NOTES:**
- A UNDISTURBED EARTH.
 - B O.D. OF PIPE + 8".
 - C O.D. OF CAP OR PLUG, MIN. 12"x12".
 - D ONLY FOR EXCEPTIONAL SITUATIONS, USE OF MECHANICAL RESTRAINTS TAKES PRECEDENCE.

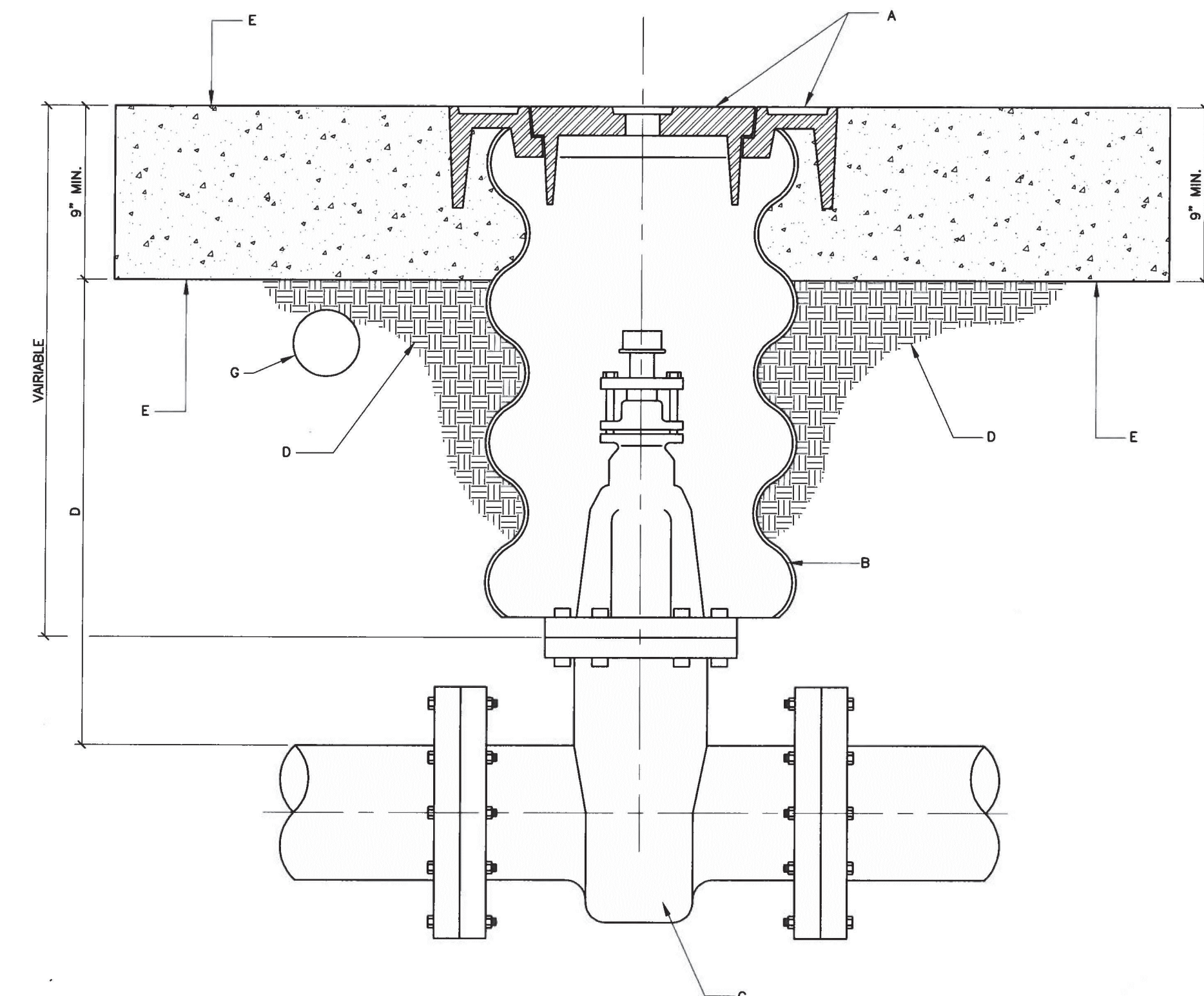
| | |
|-----------|--------------------------------------|
| REVISIONS | WATER AUTHORITY |
| | WATER CONCRETE BLOCKING DESIGN |
| | DWG. 2320 JANUARY 2011 |



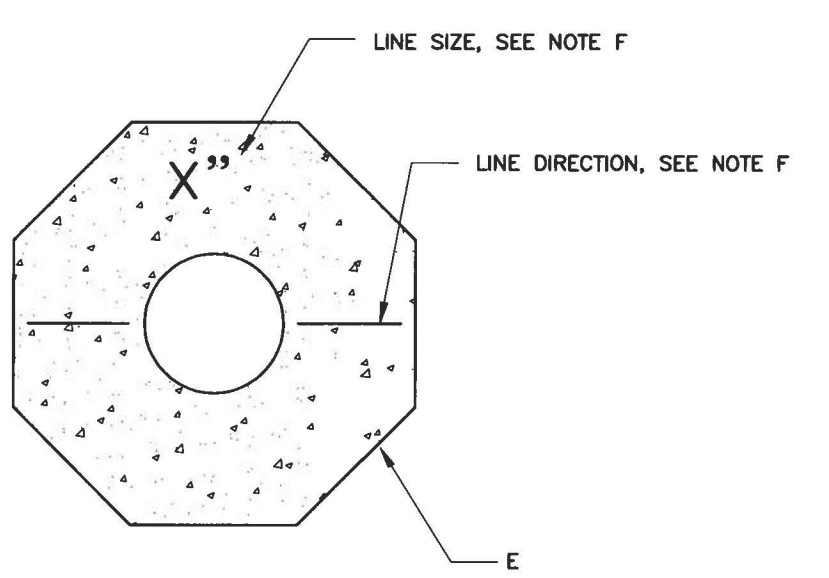
TRENCH CROSS-SECTION
SHOWING TERMINOLOGY

- GENERAL NOTES:**
- 1 MECHANICAL TAMPERS SHALL NOT BE USED IN THE INITIAL BACKFILL REGION FOR FLEXIBLE PIPE. WHEN FLEXIBLE PIPE IS USED, CONTRACTOR SHALL, PRIOR TO THE START OF CONSTRUCTION, PROVIDE THE PROPOSED COMPACTION METHOD IN THE INITIAL BACKFILL REGION TO THE WATER AUTHORITY FOR APPROVAL.
 - 2 MINIMUM CLASS "C" BEDDING WILL BE USED.
 - 3 ALL COMPACTION WILL BE TO 95% OF THE STANDARD PROCTOR.

| | |
|-----------|-------------------------------------|
| REVISIONS | WATER AUTHORITY |
| | WATER PIPE TRENCH TERMINOLOGY |
| | DWG. 2315 JANUARY 2011 |



- GENERAL NOTES:**
- A VALVE BOX RING AND COVER PER C.O.A. STD. DWG. 2328.
 - B 12" DIAMETER POLYMER COATED STEEL PIPE CMP.
 - C NEW OR EXISTING VALVE.
 - D COMPACTED BACKFILL, SOIL OR BASE COURSE MATERIAL (95% COMPACTION). SEE SECTION 701.
 - E CONCRETE COLLAR PER COA STANDARD DRAWING 2461. f'c = 4000 psi.
 - F TOP OF CONCRETE COLLAR SHALL BE STAMPED WITH LINE SIZE AND DIRECTION. MINIMUM LETTER SIZE SHALL BE 3" IN HEIGHT. INSTALL FIRE LINE RING AND COVER ON FIRE LINES PER COA STANDARD DRAWING 2329.
 - G ELECTRONIC MARKER DEVICE (EMD), SEE COA STANDARD SPECIFICATION SECTION 170.



LABEL REQUIREMENTS

- GENERAL NOTES:**
- 1 BEFORE THE WORK WILL BE ACCEPTED, WATER VALVE GPS COORDINATES SHALL BE PROVIDED ON THE RECORD DRAWINGS. GPS COORDINATES OBTAINED BY A PROFESSIONAL SURVEYOR LICENSED IN THE STATE OF NEW MEXICO SHALL BE TAKEN AT THE VALVE OPERATING NUT. USE THE NAD 1983 NM STATE PLANE CENTRAL ZONE FOR X AND Y COORDINATES AND NAVD 1988 FOR Z COORDINATE.

| | |
|-----------|------------------------|
| REVISIONS | WATER AUTHORITY |
| | WATER VALVE BOX |
| | DWG. 2326 JANUARY 2011 |

BY: HJM
DATE: 06/05/2020
DESCRIPTION: ISSUE FOR PERMIT
ADAMS JOB NO.: 2017.095
Adams
8951 Cypress Waters Blvd Suite 150 Dallas, Texas 75019 (817) 324-3300

06/08/2020
MATTHEW DAVID KORB
NEW MEXICO
25909
PROFESSIONAL ENGINEER

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McDonald's USA, LLC
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| | | |
|-------------------|------------------------|----|
| REGIONAL MGR. | DATE | BY |
| CONSTR. MGR. | SIGNATURE (2 REQUIRED) | |
| OPERATIONS DEPT. | | |
| REAL ESTATE DEPT. | | |
| CONTRACTOR | CO-SIGN SIGNATURES | |
| OWNER | | |

| | | |
|----------|----------|-----|
| DESIGNED | JUN 2020 | HJM |
| DRAWN | JUN 2020 | HJM |
| CHECKED | | |
| AS-BUILT | | |

CITY DETAILS
C7.5

