



DEVELOPMENT REVIEW APPLICATION

Effective 4/17/19

Please check the appropriate box a	nd refer to sup	plement	al forms for sub	mittal requirements. All fee	es must	be paid at the time of	application.
Administrative Decisions	D	ecisions	Requiring a Pul	blic Meeting or Hearing	Policy	Decisions	
☐ Archaeological Certificate (Form P3		Site Pla Form P1)		g any Variances – EPC		ption or Amendment of Facility Plan (Form Z)	Comprehensive
☐ Historic Certificate of Appropriatene (Form L)	ess – Minor] Master	Development Pla	n <i>(Form P1)</i>		ption or Amendment of ation (Form L)	Historic
☐ Alternative Signage Plan (Form P3)	, ,	Historic	: Certificate of App	oropriateness – Major	□ Ame	endment of IDO Text (F	form Z)
☑ Minor Amendment to Site Plan (For	rm P3)	Demoli	tion Outside of HF	PO (Form L)	□ Ann	exation of Land (Form 2	Z)
☐ WTF Approval (Form W1)		Historic	Design Standard	ls and Guidelines (Form L)	□ Ame	endment to Zoning Map	– EPC (Form Z)
		Wireles		ations Facility Waiver	□ Ame	endment to Zoning Map	- Council (Form Z)
					Appea	Is	
					□ Dec <i>A)</i>	ision by EPC, LC, ZHE	, or City Staff (Form
APPLICATION INFORMATION							
Applicant: McDonald's US					Ph	one: 972-869-1	.888
Address: 511 E John Car	penter F	¬wy				nail:	
City: Irving				State: TX	Zip	: 75062	
Professional/Agent (if any): Cesar	Segovia	a			Ph	one: 817-529-6	874
Address: 513 Main Stree	t #300				Em	nail: cesar@roguear	chitects.com
City: Fort Worth				State: TX	Zip	: 76102	
Proprietary Interest in Site:				List <u>all</u> owners:			
BRIEF DESCRIPTION OF REQUEST							
Administrative Ame	ndment f	or C	ity of Al	lbuquerque peri	mit ‡	BP-2020-271	06
SITE INFORMATION (Accuracy of the	ne existing lega	egal description is crucial! Attach a separate sheet if nec		necessa	rry.)		
Lot or Tract No.: LT A PLAT OF	ONE PRESI	DENTI	AL PLAZA	Block: D	Un	it: 2	
Subdivision/Addition:				MRGCD Map No.:	UP	C Code:	
Zone Atlas Page(s):		Existin	g Zoning:		Pro	pposed Zoning:	
# of Existing Lots:		# of Pr	roposed Lots:		Tot	tal Area of Site (acres):	
LOCATION OF PROPERTY BY STRE	ETS	-			-		
Site Address/Street: 4300 Osu:	na	Betwe	en: Jeffer:	son St NE	and: P	residential	Drive NE
CASE HISTORY (List any current or	prior project a	nd case	number(s) that	may be relevant to your re	quest.)		
Louis lough							
Signature:				Date: 7/29/2020			
Printed Name: Cesa	ar Segov	ia			☐ Applicant or ☒ Agent		
FOR OFFICIAL USE ONLY							
Case Numbers	Action		Fees	Case Numbers		Action	Fees
SI-2020-00688	AA		_				
Meeting/Hearing Date:					Fee	e Total:	
Staff Signature:				Date:	Pro	oject # PR-2020-004	4189

FORM P3: ADMINISTRATIVE DECISIONS AND MINOR AMENDMENTS

A single PDF file of the complete application including all plans and documents being submitted must be emailed to PLNDRS@cabg.gov prior to making a submittal. Zipped files or those over 9 MB cannot be delivered via email, in which case the PDF must be provided on a CD.

Management Information Required for all administrative decisions or amendments

- __ Letter of authorization from the property owner if application is submitted by an agent
- Zone Atlas map with the entire site clearly outlined and labeled

□ ARCHEOLOGICAL CERTIFICATE

- __ Archaeological Compliance Documentation Form with property information section completed
- Only the information above is required unless the City Archaeologist determines that the application does not qualify for a Certificate of No Effect, in which case a treatment plan prepared by a qualified archaeologist that adequately mitigates any archeological impacts of the proposed development must be submitted and reviewed for a Certificate of Approval per the criteria in IDO Section 14-16-6-5(A)(3)(b)

■ MINOR AMENDMENT TO SITE PLAN – ADMIN, EPC, or DRB

- __ Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-4(X)(2)
- ___ Three (3) copies of all applicable sheets of the approved Site Plan being amended, folded
- Copy of the Official Notice of Decision associated with the prior approval
- Three (3) copies of the proposed Site Plan, with changes circled and noted
 - Refer to the Site Plan Checklist for information needed on the proposed Site Plan.

Minor Amendments must be within the thresholds established in IDO TABLE 6-4-5. Any amendment beyond these thresholds is considered a Major Amendment and must be processed through the original decision-making body for the request.

MINOR AMENDMENT TO SITE DEVELOPMENT PLAN APPROVED PRIOR TO THE EFFECTIVE DATE OF THE IDO

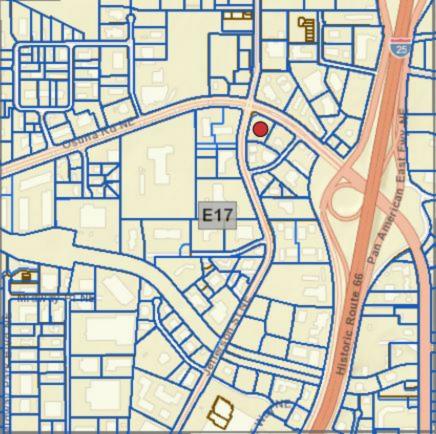
- __ Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-4(Y)(1)(a)
- ___ Three (3) copies of all applicable sheets of the approved Site Development Plan being amended, folded
- Copy of the Official Notice of Decision associated with the prior approval
- Three (3) copies of the proposed Site Development Plan, with changes circled and noted Refer to the Site Plan Checklist for information needed on the proposed Site Plan.

Minor Amendments must be within the thresholds established in IDO TABLE 6-4-5. Any amendment beyond these thresholds is considered a Major Amendment and must be processed through the original decision-making body for the request.

□ ALTERNATIVE SIGNAGE PLAN

- Proposed Alternative Signage Plan compliant with IDO Section 14-16-5-12(F)(5)
- Justification letter describing, explaining, and justifying the request per the criteria in IDO Section 14-16-6-5(F)(4)(c)
- Required notices with content per IDO Section 14-16-6-4(K)(6)
 - __ Office of Neighborhood Coordination notice inquiry response and proof of emailed notice to affected Neighborhood Association representatives
- Sign Posting Agreement

I, the applicant or agent, acknowledge that if any require scheduled for a public meeting or hearing, if required, or c		his application, the application will not be
Signature: Loga Jegalia >		Date: 7/23/2020
Printed Name Cesar Segovia		☐ Applicant or ☑ Agent
FOR OFFICIAL USE ONLY		
Project Number:	Case Numbers	THE PARTY OF THE P
PR-2020-004189	SI-2020-00688	
	-	
	-	(1706)
Staff Signature:		M E X September 1
Date:		TANA MARINE





Wednesday, August 12, 2020

City of Albuquerque Planning Department 600 2nd St. NW Albuquerque, NM 87102 (505) 924-3991

Re: McDonald's 4300 Osuna Rd. NE (PR 2020-004189)

Ms. Jeanne Wolfenbarger:

Thank you for taking the time to review the Administrative Amendment application for the McDonald's at 4300 Osuna Rd. NE. We have addressed your comments as follows:

- 1. Dimension all of the drive aisles.
 - All drive aisles are dimensioned. See C3.0 Site & Paving Plan.
- 2. Add "Do Not Enter" signs at the end of all of the one-way drive aisles.
 - A "Do Not Enter" sign has been added to the end of the one-way drive aisle. See C3.0 Site & Paving Plan.
- 3. On sidewalk and ADA accessible pathways as well as parking, list 2% maximum cross-slope in the details and/or general notes.
 - It's listed under the "Standard Accessibility Requirements" notes. See C3.0 Site & Paving Plan and C6.0 Grading Plan.
- 4. There were not any van accessible aisles shown (minimum 8 feet wide) for the ADA parking. List van accessible parking requirements and include as part of the design.
 - An 11' wide van accessible space with a 5' aisle are shown in accordance with ADA Standards 502.2 and 502.3.1. See C3.0 Site & Paving Plan.
- 5. Include C7.1, C7.3 and C7.5 standard details as part of this set. All details for curb, sidewalk, curb ramps, and signage have to be included.
 - Detail sheets have been included.
- 6. The ADA accessible parking sign must have the required language per 66-7-352.4C NMSA 1978 **"Violators Are Subject to a Fine and/or Towing."** Provide detail of sign.
 - Detail with required language is shown on Sheet C7.1 Standard Details.
- 7. Provide any bike rack details.
 - Bike rack detail is on Sheet C7.6 City Details.
- 8. Is existing curb ramp ADA compliant at southeast corner of McDonald's
 - The existing ramp at the northeast corner of the McDonald's building is ADA compliant. Plan view is rotated.
- 9. For any new concrete work tying to existing concrete pedestrian ways into the building (such as on northeast corner of the building), provide a note.
 - A note has been added. See Sheet C3.0 Site & Paving Plan.

Please let us know if any further corrections are needed.

Sincerely

Matthew D. Korte, P.E.



July 28, 2020

Mr. Brennon Williams City of Albuquerque Planning Department 600 2nd Street Albuquerque, NM 87102

RE: Amendment to Site Development Plan

McDonald's Restaurant - 4300 Osuna Rd. NE

Dear Mr. Williams:

This letter is in reference to our attached application and site plans for a remodel of the existing McDonald's restaurant at 4300 Osuna Rd. NE, near the southeast corner of the intersection with Jefferson Blvd. NE. The site was developed under the Site Development Plan ZA99-330, and project number 1000138.

The scope of work with this plan includes the remodel of the existing McDonald's restaurant, replacement of existing menu board, order point and gateway (clearance bar) and installation of a new (additional) menu board and order point in a side by side Drive Thru configuration. McDonald's has found that the addition of a second order point, in a side-by-side configuration, decreases the response time experienced by customers and thereby improves their satisfaction.

The scope also includes ADA accessibility improvements throughout the site. Sidewalks and ramps are being replaced where needed to provide compliant slopes and accessible routes to parking and the right-of-way. ADA parking spaces are being restriped to provide compliant configurations.

Should you have any questions or need additional information, please do not hesitate to reach out to us or the McDonald's Area Construction Manager for this project, Eddie Nunez.

Sincerely,

ADAMS

Matthew D. Korte, P.E. Program Director

Attachments

PROPERTY RECORDS E-FILE ASSESSOR'S FORMS ASSESSOR WEBSITE

CONTACT US | ?

PARID: 101706232437110303 CELCO INVESTMENTS LLC, 4300 OSUNA RD 1 of 1 Return to Search Results Class Tax Year 2020 V Class Non Residential Actions Tax District A1A Printable Summary Printable Version **Current Owner** Reports Tax Year 2020 CELCO INVESTMENTS LLC 2021 LOS POBLANOS PL NW Owner Property Attributes Owner Mailing Address Unit LOS RANCHOS DE ALBUQUERQUE City State NM 87107 Zip Code Foreign Mailling Address Ownership for Tax Year Selected Tax Year 2020 Owner Name CELCO INVESTMENTS LLC 2021 LOS POBLANOS PL NW Owner Mailing Address Unit LOS RANCHOS DE ALBUQUERQUE City State Zip Code 87107 Foreign Mailling Address Description 4300 OSUNA RD NE Location Address City ALBUQUERQUE State NM 87109 Zip Code Property Description LT A PLAT OF ONE PRESIDENTIAL PLAZA (REPL OF LT 1 BLK D UNIT 2 INTERSTATE INDUSTRIAL TRACT) CONT 2.0031 AC +-**Public Improvement District** Tax Increment Development Districts Document # Document #: 2010035346 042710 SW - ENTRY BY CBL 071310 **CODED BY LV 042810 Real Property Attributes** Primary Building SQ FT 6479 2001 Year Built Lot Size (Acres) 2.0031 CONVIENIENCE FOOD MARKET Land Use Code Style Manufactured Home Attributes Make: License : VIN: Year: Size:

Click here to view the Disclaimer

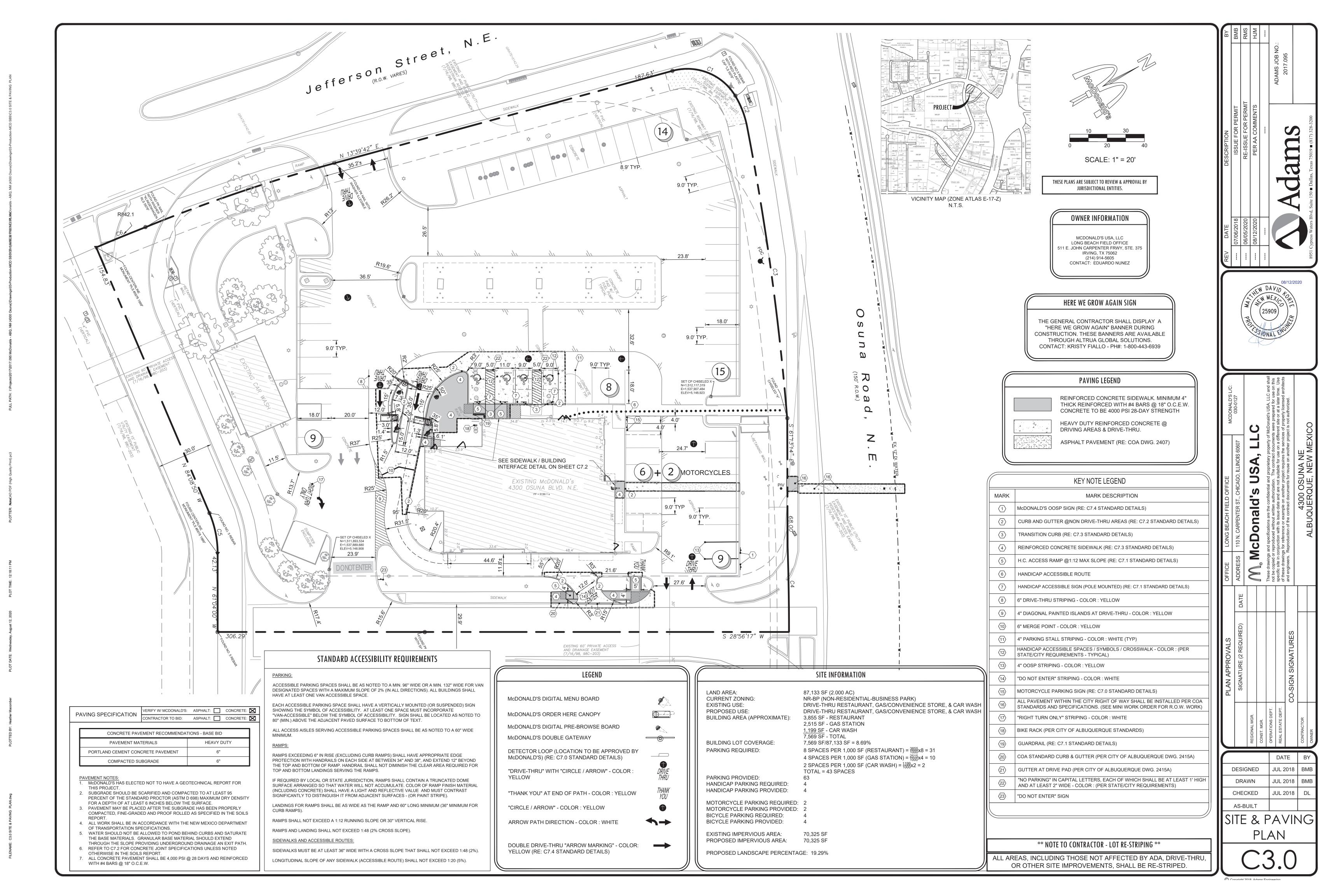
DISCLAIMER

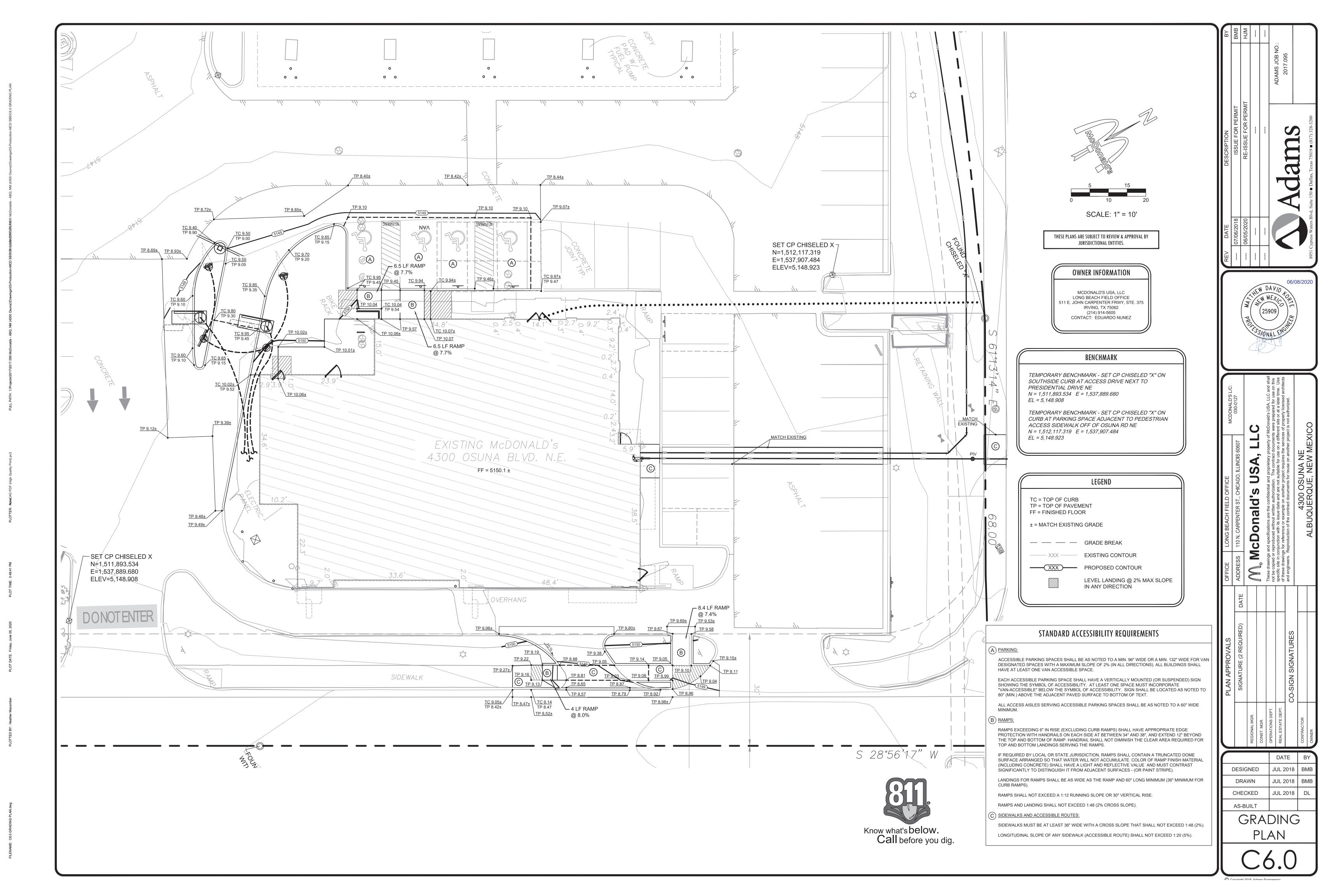
Profile

Мар

Satellite View

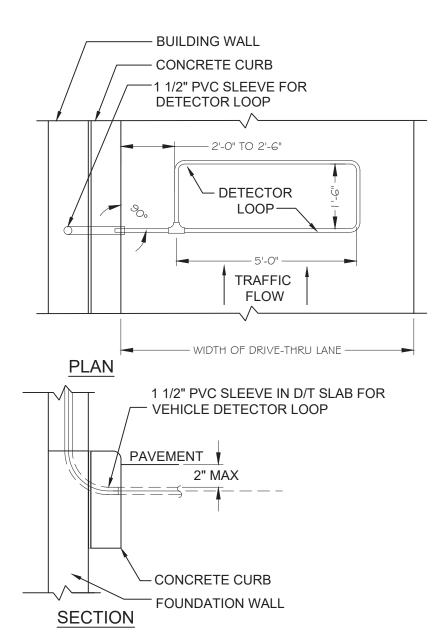
Tax Calculator



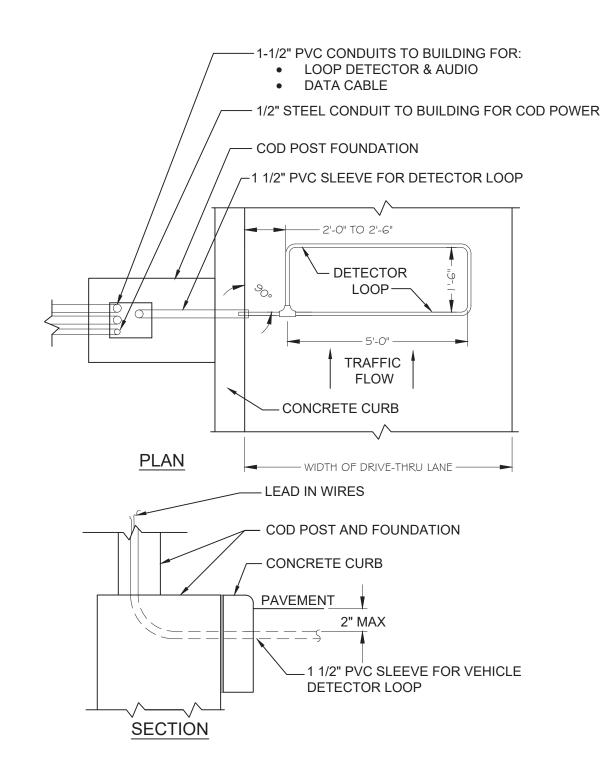


NOTES:

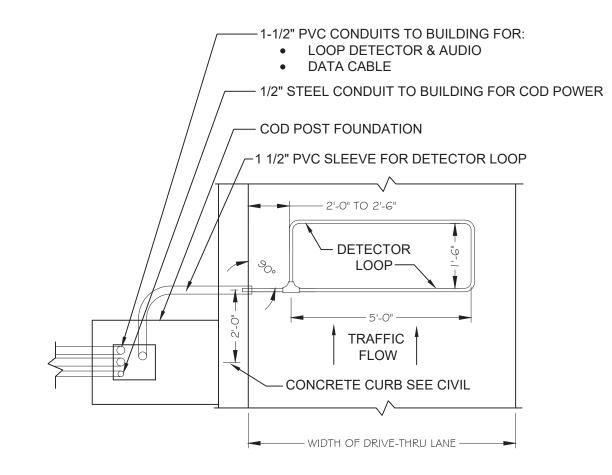
- 1. VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
- 2. CENTER VEHICLE DETECTOR LOOP IN DRIVE THRU LANE. INSTALL PER MANUFACTURERS RECOMMENDATIONS.
- 3. NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2' OF LOOP.
- 4. DETECTOR LOOP MANUFACTURERS: DETECTOR LOOPS MAY BE BY ONE OF THE FOLLOWINGS COMPANIES OR EQUAL.
- 3M: 1-800-328-0033 HME: 1-800-848-4468
- 5. <u>DETECTOR LOOP MATERIAL:</u> PVC TUBING 1/2" I.D. 100 PSI LOOP MADE FROM ONE LENGTH OF THIN FOURTEEN GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.
- 6. DETECTOR LOOP CONSTRUCTION: FORMED WITH ONE CONTINUOUS LENGTH OF PVC WITH NO SHARP CORNERS AS DETAILED. WIRE LOOPED, FORMED, & PIGTAILED AS DETAILED.

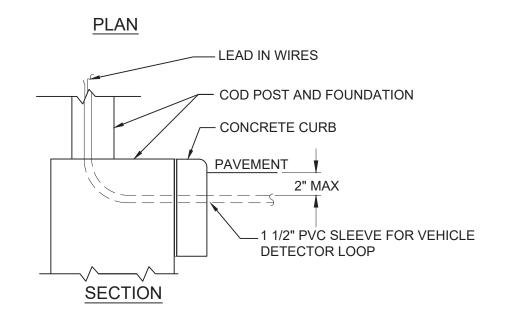


DETECTOR LOOP DETAIL AT DRIVE-THRU WINDOW NOT TO SCALE

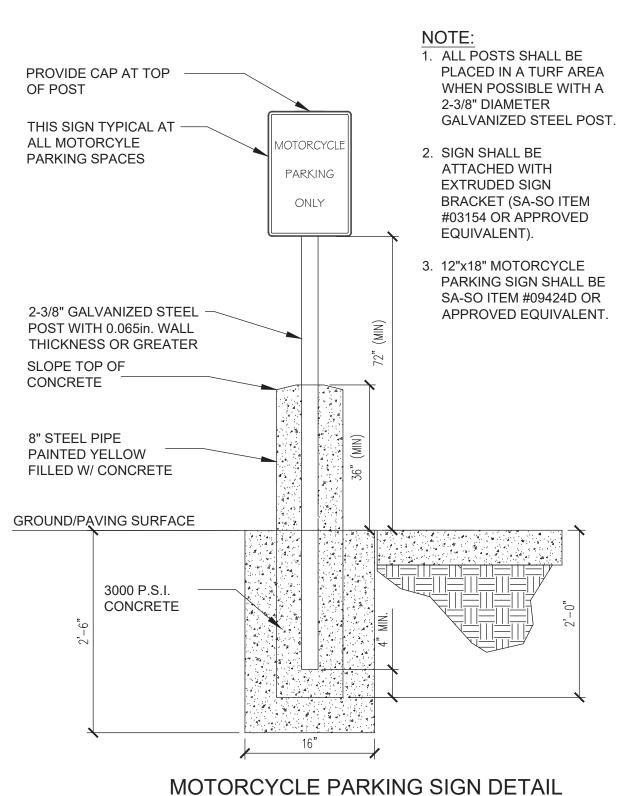


PRIMARY LANE DETECTOR LOOP DETAIL AT C.O.D. NOT TO SCALE



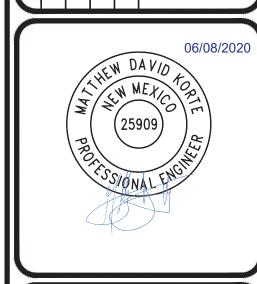


SECONDARY LANE DETECTOR LOOP DETAIL AT C.O.D. NOT TO SCALE



MOTORCYCLE PARKING SIGN DETAIL NOT TO SCALE

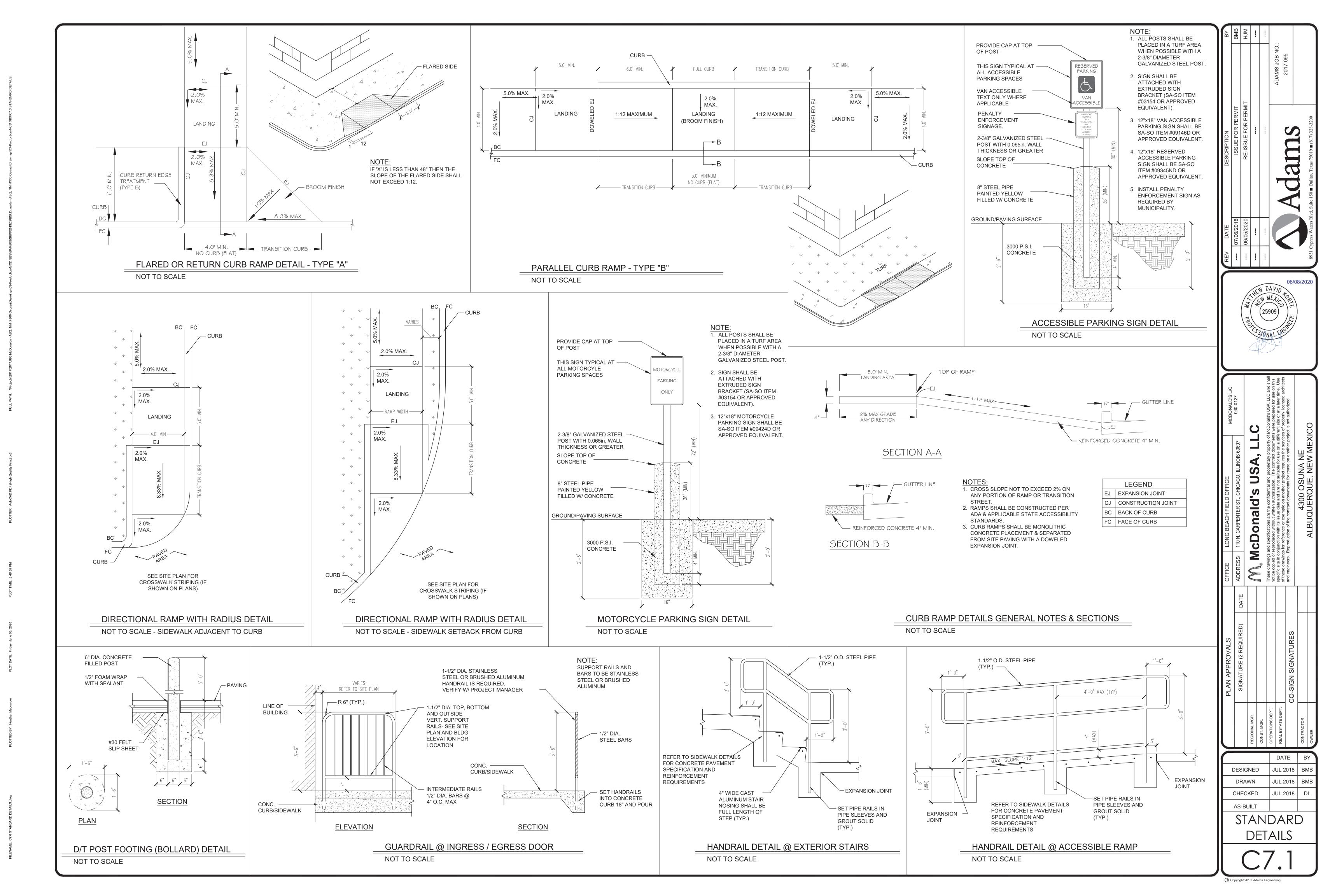
C Copyright 2018, Adams Engineering

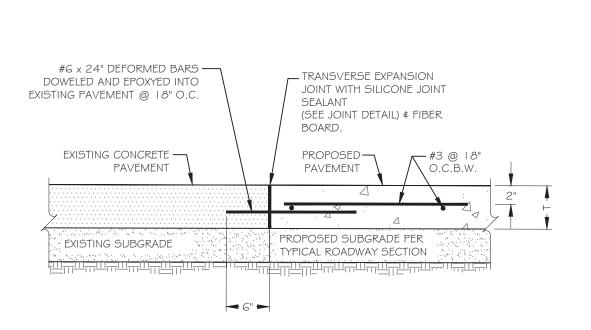


		PLAN APPROVALS		OFFICE	LONG BEACH FIELD OFFICE	MCDON
		SIGNATURE (2 REQUIRED)	DATE	ADDRESS	ADDRESS 110 N. CARPENTER ST., CHICAGO, ILLINOIS 60607	030
	REGIONAL MGR.				McDonald's IISA I	ر
	CONST. MGR.				Cooligia 3 Coo, Fr)
	OPERATIONS DEPT.			These drawings an or r	These drawings and specifications are the confidential and proprietary property of McDonald's US not be copied or reproduced without written authorization. The contract documents were preparec	McDonald's US s were prepared
	REAL ESTATE DEPT.			specific site in col	specific site in conjunction with its issue date and are not suitable for use on a different site or at a of these drawings for reference or example on another project requires the services of property lice	rent site or at a s of properly lic
		CO-SIGN SIGNATURES		and engineers. R	and engineers. Reproduction of the contract documents for reuse on another project is not author	ect is not author
T	CONTRACTOR				4300 OSUNA NE	
	OWNER				ALBUQUERQUE, NEW MEXICO	CO
Ĺ.						

	DATE	BY	
DESIGNED	JUL 2018	BMB	
DRAWN	JUL 2018	BMB	
CHECKED	JUL 2018	DL	
AS-BUILT			
STANI	DARE	\overline{C}	

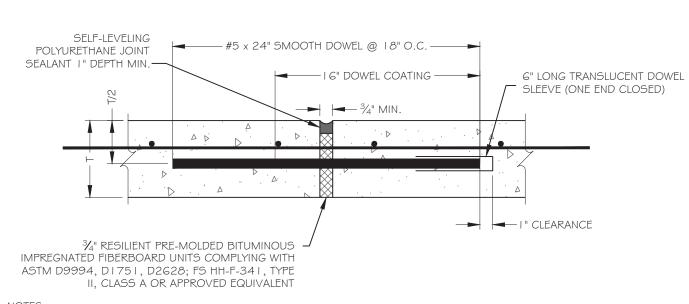
DETAILS





TRANSVERSE EXPANSION JOINT

NOT TO SCALE

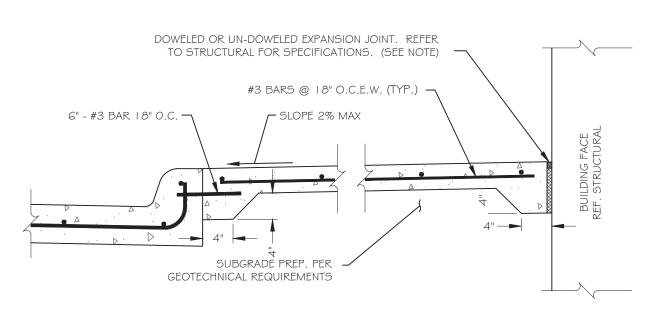


- . SLEEVES FOR DOWELS SHALL HAVE AN INSIDE DIAMETER OF N_0 " GREATER THAN THE DIAMETER OF THE DOWELS. EXPANSION JOINTS SHALL BE CONSTRUCTED A MAXIMUM OF 500' APART ON STRAIGHT PAVING, AND WHERE INDICATED PER THE AMERICAN CONCRETE PAVEMENT ASSOCIATION'S TECHNICAL PUBLICATION ACPA ISOG I .O I P (LATEST VERSION).
- 4. REFER TO SIDEWALK DETAILS THIS SHEET FOR EXPANSION JOINTS IN SIDEWALK AREAS. 5. ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS,
- DETAILS \$ REQUIREMENTS FOR PUBLIC PAVEMENT. 6. FINISHED SURFACES SHALL BE INSTALLED FLUSH WITH A DIFFERENTIAL ELEVATION NOT TO EXCEED 1/8".

EXPANSION JOINT (ISOLATION)

NOT TO SCALE

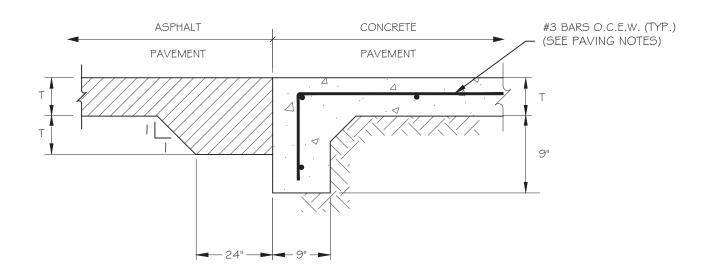
3. DOWELS SHALL NOT BE TIED TO OTHER REINFORCEMENT.



- . ALL SIDEWALK JOINTS LOCATED WITHIN 25' OF A BUILDING FACE OR ADJACENT TO ANY STRUCTURE SHALL BE SEALED.
- . REFER TO STRUCTURAL PLANS/DETAILS FOR DOWEL/HINGE JOINT AT ALL CONNECTIONS BETWEEN FOUNDATIONS/STOOPS 3. THE SUBGRADE PREPARATION FOR ANY FLATWORK OR SIDEWALK PAVEMENT WITHIN 25' FROM ANY BUILDING FACE OR ADJACENT TO ANY STRUCTURE SHALL AS SPECIFIED BY THE GEOTECHNICAL REPORT. IN THE EVENT THAT THE GEOTECHNICAL REPORT DOES NOT CONTAIN A RECOMMENDATION THE CONTRACTOR SHALL NOTIFY THE ENGINEER
- ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR PUBLIC PAVEMENT.
- 5. FINISHED SURFACES SHALL BE INSTALLED FLUSH WITH A DIFFERENTIAL ELEVATION NOT TO EXCEED 1/8".

SIDEWALK/BUILDING INTERFACE

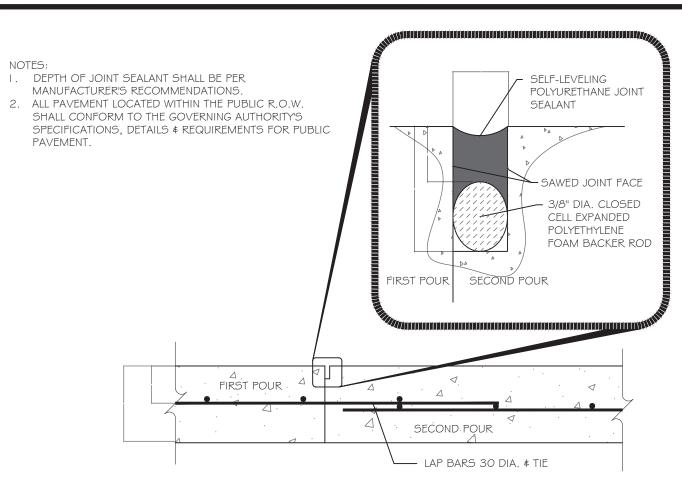
NOT TO SCALE



- . PAVEMENT BARS SHALL BE BENT DOWN INTO HEADER. HEADER AND PAVEMENT SHALL BE MONOLITHIC
- REINFORCEMENT STEEL SHALL BE PER PAVEMENT SECTION SPECIFICATIONS. 4. ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS,
- DETAILS & REQUIREMENTS FOR PUBLIC PAVEMENT. FINISHED SURFACES SHALL BE INSTALLED FLUSH WITH A DIFFERENTIAL ELEVATION NOT TO EXCEED 1/8".
- 5. SUBGRADE PREPARATION SHALL BE PER GEOTECHNICAL REPORT SPECIFICATIONS.

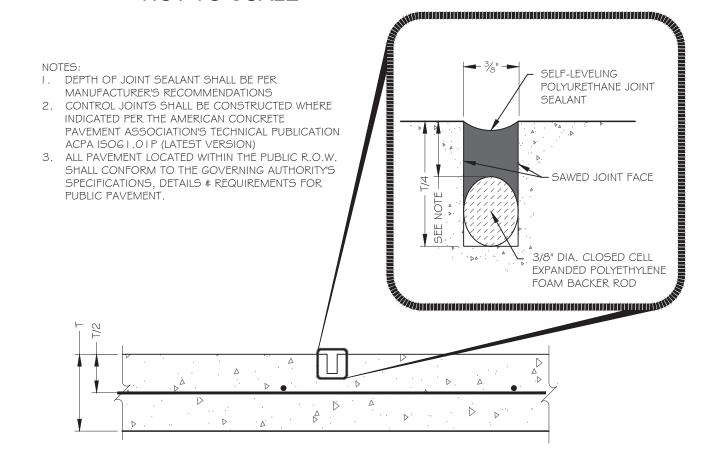
ASPHALT TO CONCRETE HEADER

NOT TO SCALE



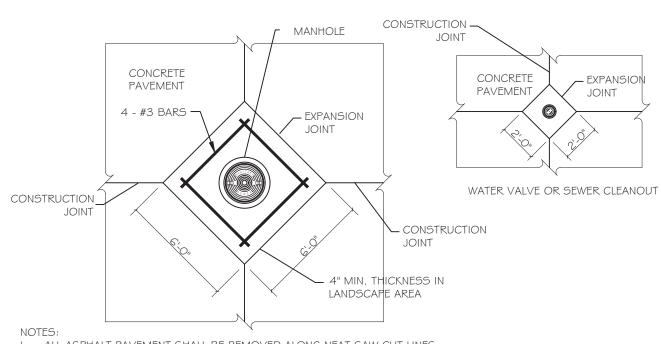
CONSTRUCTION BUTT JOINT

NOT TO SCALE



CONTROL JOINT (CONTRACTION)

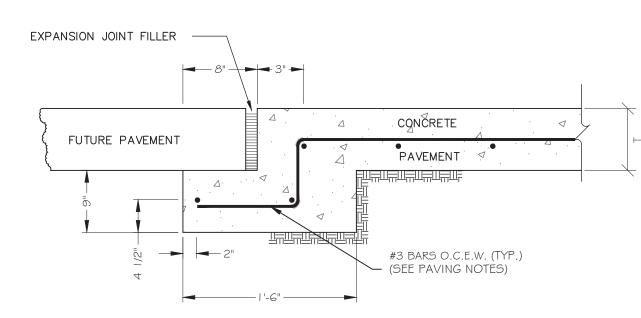
NOT TO SCALE



- ALL ASPHALT PAVEMENT SHALL BE REMOVED ALONG NEAT SAW CUT LINES. BOXOUT REQUIRED FOR ALL MANHOLES (STORM AND SANITARY) AND CLEANOUTS LOCATED IN PCC PAVEMENT.
- MANHOLE BOXOUT SHALL BE 6'x6' SQUARE AND ALIGNED WITH PAVEMENT JOINTING. WATER VALVE & SANITARY CLEANOUTS SHALL BE PLACED IN BOXOUTS 2'x2' SQUARE ALIGNED WITH PAVEMENT JOINTING. 5. MANHOLES OR VALVES LOCATED IN LANDSCAPE AREAS SHALL INCLUDE 4' MIN. CONCRETE COLLAR TO THE DIMENSIONS INDICATED ABOVE AND SHALL BE INSTALLED FLUSH WITH FINISHED GRADE.
- 6. ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR PUBLIC PAVEMENT.

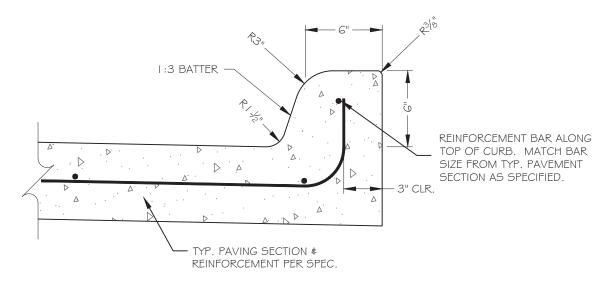
MANHOLE/VALVE/CLEANOUT JOINTING

NOT TO SCALE



STREET HEADER FOR FUTURE PAVEMENT

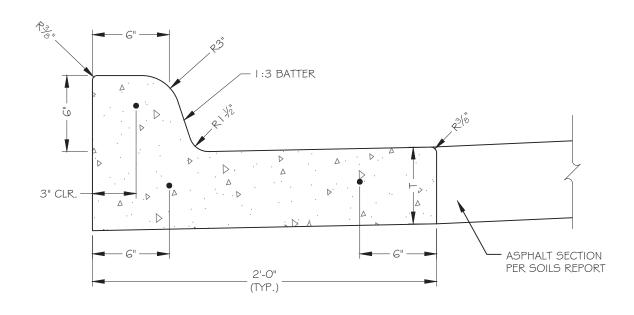
NOT TO SCALE



- I. ALL CURBS ARE CONSTRUCTED OF PORTLAND CEMENT CONCRETE UNLESS OTHERWISE SHOWN
- . GRADES SHALL BE MEASURED AT BACK OF CURB UNLESS OTHERWISE SPECIFIED. 3. ALL PAVEMENT LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR PUBLIC PAVEMENT.
- 4. CONTROL JOINTS THROUGH CURB SHALL BE SEALED IN THE PAVEMENT AND TERMINATE AT THE GUTTER

MONOLITHIC CURB

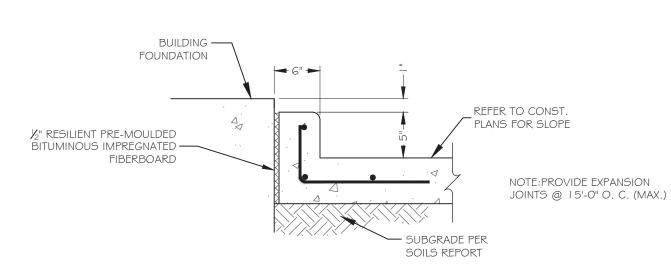
NOT TO SCALE



I. REINFORCEMENT SHALL BE NO. 4 BARS. 2. CONCRETE SHALL BE 3,500 PSI.

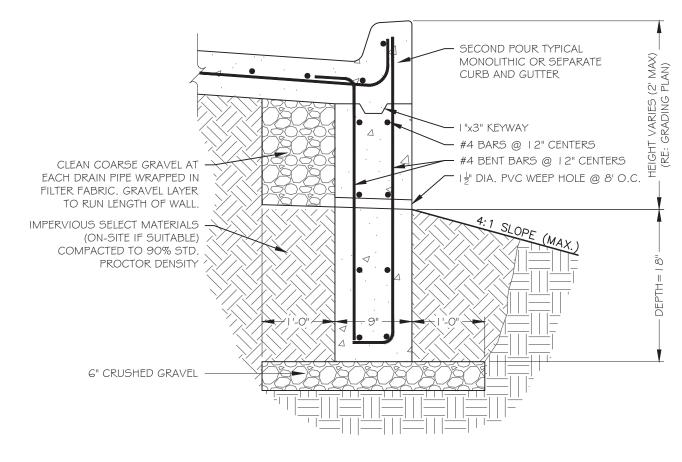
SEPARATE CURB & GUTTER

NOT TO SCALE



VERTICAL CURB @ DRIVE THRU

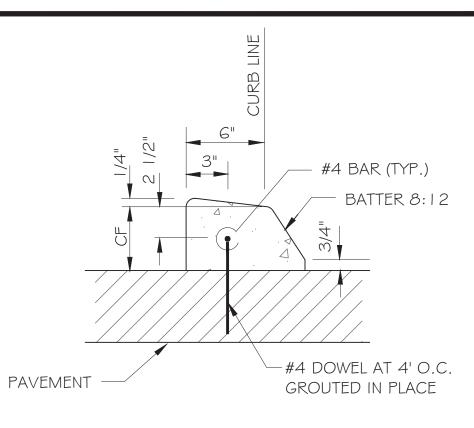
NOT TO SCALE



I. THE MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS SHALL BE 3,500 PSI AND SHALL COMPLY WITH ACI 318. 2. REINFORCING STEEL SHALL COMPLY WITH ASTM AG 15, GRADE GO. 3. BACKFILLING AGAINST REINFORCED DEEPENED CURB SHALL NOT BE PERMITTED UNTIL CONCRETE HAS REACHED ITS 28 DAY STRENGTH. CARE SHALL BE TAKEN TO AVOID EXERTING LARGE IMPACT FORCES ON THE DEEPENED CURB.

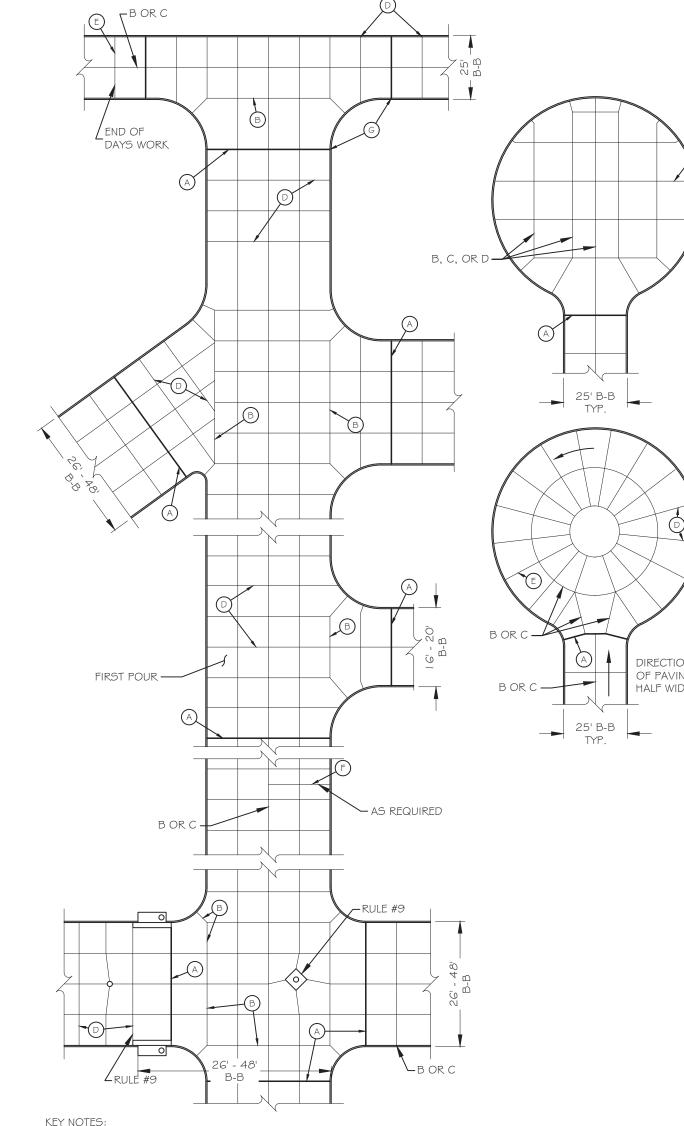
DEEPENED CURB

NOT TO SCALE



DOWELED CURB

NOT TO SCALE



- A. EXPANSION JOINT (ISOLATION)
- B. LONGITUDINAL CONSTRUCTION JOINT LONGITUDINAL CONTROL JOINT (CONTRACTION)
- TRANSVERSE CONTROL JOINT (CONTRACTION) E. PLANNED TRANSVERSE CONSTRUCTION JOINT
- F. EMERGENCY TRANSVERSE CONSTRUCTION JOINT G. PLACE 1/2" EXPANSION JOINT FILLER IN TOP OF CURB ONLY AT ALL RADIUS POINTS

- AVOID ODD-SHAPED SLABS. 2. MAXIMUM TRANSVERSE JOINT SPACING FOR PAVEMENT SHOULD EITHER BE 24 TO 30
- TIMES THE SLAB THICKNESS OR 15ft. LONGITUDINAL JOINT SPACING SHOULD NOT EXCEED 12.5ft
 KEEP SLABS AS SQUARE AS POSSIBLE. LONG NARROW SLABS TEND TO CRACK MORE
- THAN SQUARE ONES.
- 5. ALL TRANSVERSE CONTRACTION JOINTS MUST BE CONTINUOUS THROUGH THE CURB AND
- HAVE A DEPTH EQUAL TO 1/4 THE PAVEMENT THICKNESS.

 6. IN ISOLATION JOINTS, THE FILLER MUST BE FULL DEPTH AND EXTEND THROUGH THE CURB 7. IF THERE IS NO CURB, LONGITUDINAL JOINTS SHOULD BE TIED WITH DEFORMED BARS.
- 8. OFFSETS AT RADIUS POINTS SHROUD BE AT LEAST 1.5ft WIDE. JOINT INTERSECTION ANGLES LESS THAN 60° SHOULD BE AVOIDED. 9. MINOR ADJUSTMENTS IN JOINT LOCATION MADE BY SHIFTING OF SKEWING TO MEET INLETS
- AND MANHOLES WILL IMPROVE PAVEMENT PERFORMANCE IO. WHEN THE PAVEMENT AREA HAS DRAINAGE STRUCTURES, PLACE JOINTS TO MEET THE STRUCTURES IF POSSIBLE.

NOT TO SCALE

TYPICAL JOINTING LAYOUT



C Copyright 2018, Adams Engineering

DESIGNED

DRAWN

CHECKED

AS-BUILT

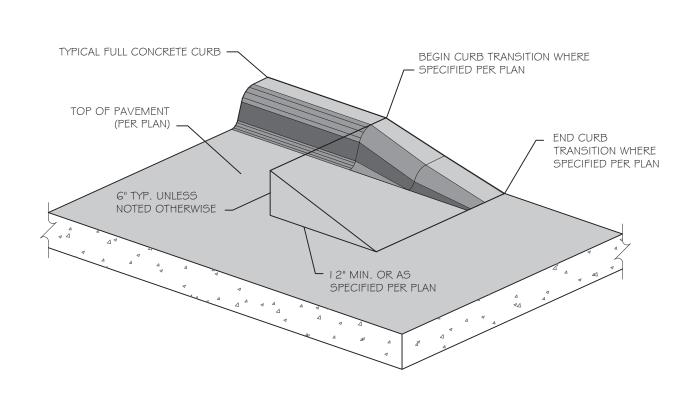
STANDARD

DATE

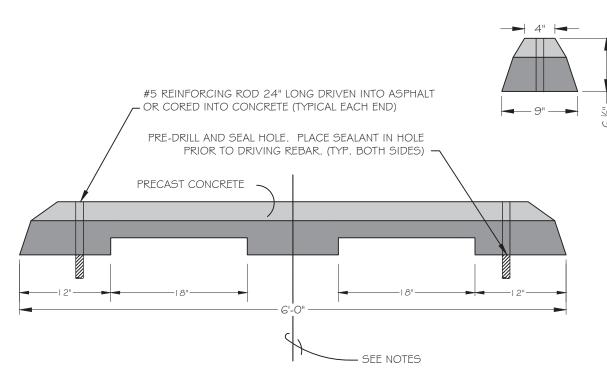
JUL 2018 | BME

JUL 2018 | BMI

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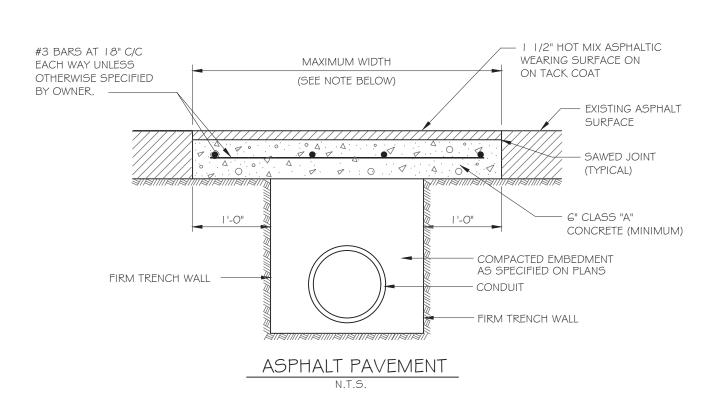
TRANSITION CURB NOT TO SCALE

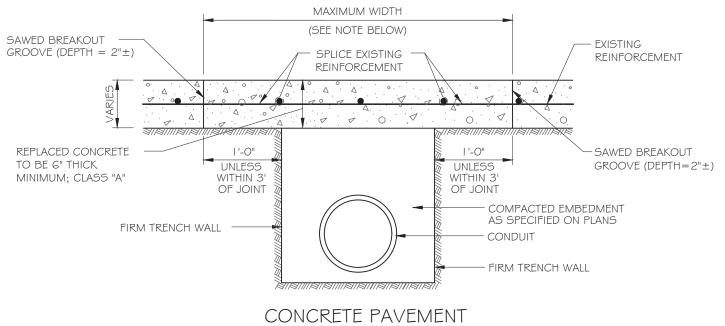


. DIMENSIONS MAY VARY AS APPROVED BY ENGINEER/OWNER WHEEL STOP SHALL BE CENTERED IN PARKING SPACE AS SHOWN ON SITE PLAN UNLESS NOTED OTHERWISE. 3. WHEELSTOP SHALL BE PLACED AT 3' FROM FACE OF CURB OR EDGE OF PAVEMENT IN 90° PARKING UNLESS NOTED OTHERWISE.

WHEEL STOP

NOT TO SCALE





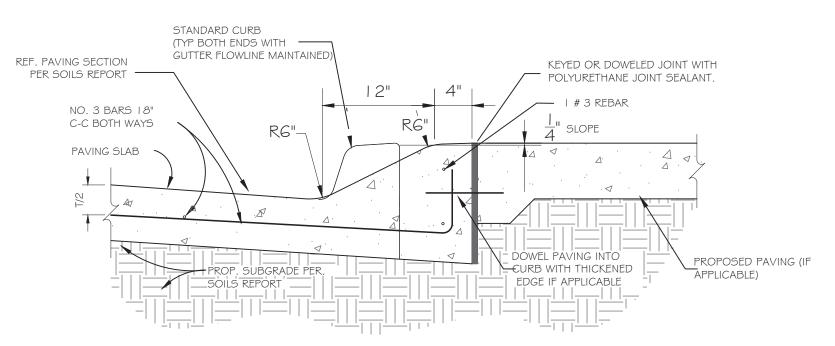
NOTE:

I. WHEN REMOVING CONCRETE PAVEMENT THE CONTRACTOR SHALL ENDEAVOR TO LIMIT DAMAGE TO EXISTING REINFORCEMENT SO IT MAY BE EMPLOYED IN THE REPLACEMENT OPERATION. IF ORIGINAL REINFORCEMENT IS CUT OR BROKEN, REPLACEMENT BARS OF THE SAME SIZE SHALL BE INSTALLED BY DRILLING AND DOWELING AS DIRECTED BY

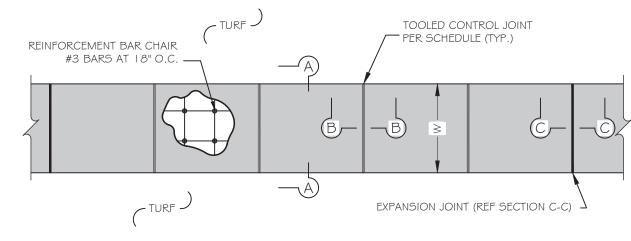
THE OWNER.

PAVEMENT CUT REMOVAL & REPLACEMENT

NOT TO SCALE

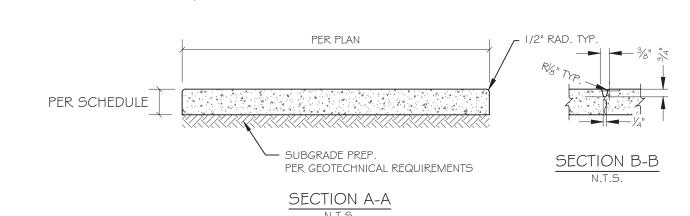


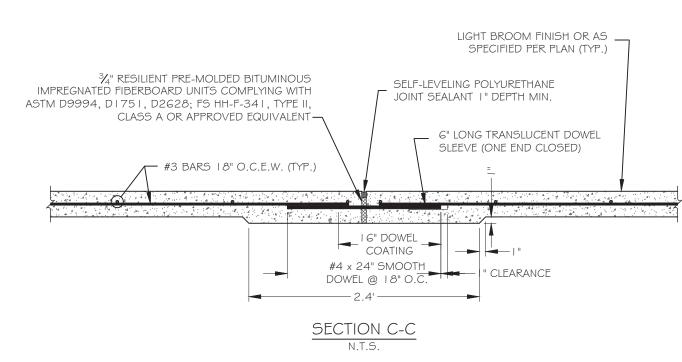
MONOLITHIC MOUNTABLE CURB NOT TO SCALE



EXPANSION JOINTS SHALL BE LOCATED EVERY 5 PANELS — L JOINT SPACING (TYP.)
EXPANSION JOINT (REF SECTION C-C)

- SAND CUSHION WILL NOT BE PERMITTED UNDER SIDEWALKS OR OTHER PAVEMENT. DOWEL WITH #4 BARS AT 18" C-C WHEN CONNECTING TO EXISTING SIDEWALKS, DRIVEWAYS, CURBS AND GUTTER.
- 3. CONCRETE STRENGTH FOR SIDEWALKS SHALL BE A MINIMUM OF 3,600 PSI @ 28 DAYS OR AS SPECIFIED BY THE AUTHORITY HAVING JURISDICTION, WHICHEVER IS GREATER.
- 4. ALL JOINTS LOCATED WITHIN LARGE AREAS OF CONCRETE FLATWORK (PLAZA AREAS OR PAVEMENT BETWEEN
- BUILDINGS) SHALL BE SEALED WITH POLYURETHANE JOINT SEALANT PER JOINT DETAILS THIS SHEET.
- CONTRACTOR SHALL SUBMIT FULL-SIZE SCALEABLE PLAZA AREAS JOINT LAYOUT FOR APPROVAL.
- . JOINT SEALANT IS NOT REQUIRED ON SIDEWALKS LOCATED IN TURF AREAS. ALL SIDEWALKS LOCATED WITHIN THE PUBLIC R.O.W. SHALL CONFORM TO THE GOVERNING AUTHORITY'S SPECIFICATIONS, DETAILS & REQUIREMENTS FOR SIDEWALKS.



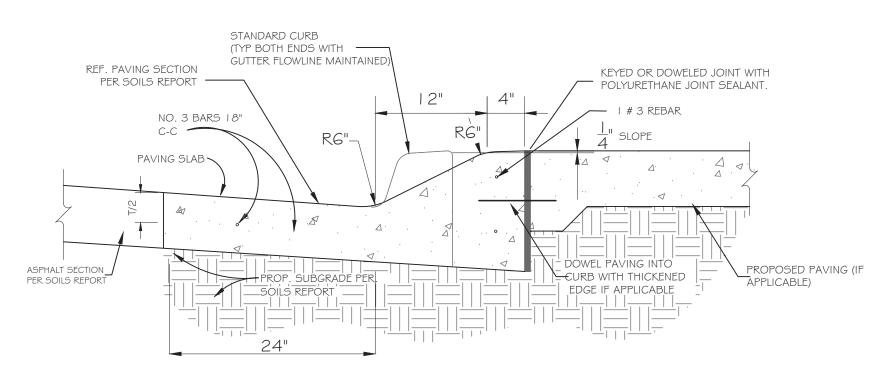


1. SLEEVES FOR DOWELS SHALL HAVE AN INSIDE DIAMETER OF /6" GREATER THAN THE DIAMETER OF THE DOWELS AND SHALL BE SUBMITTED TO ENGINEER FOR APPROVAL PRIOR TO USE. 2. DOWEL COATING SHALL BE ASPHALTIC COATING.

	WALK PA	— —
SIDEWALK WIDTH = W (FT.)	SIDEWALK THICKNESS = T (IN.)	JOINT SPACING = L (FT.)
4	4	4
5	4	5
6	4	5
7	5	7
8	5	8
9	5	9
10	5	10

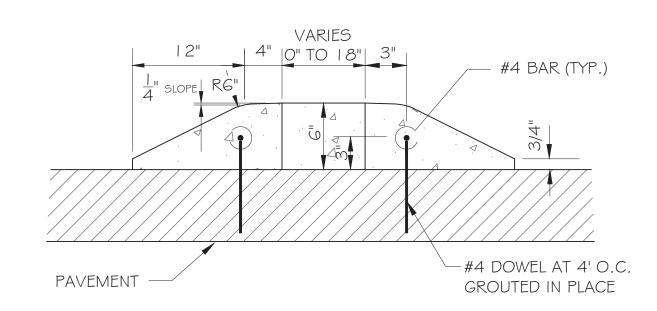
LANDSCAPE SIDEWALK

NOT TO SCALE



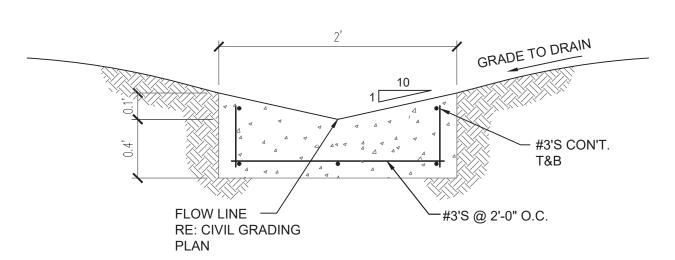
SEPARATE MOUNTABLE CURB & GUTTER

NOT TO SCALE



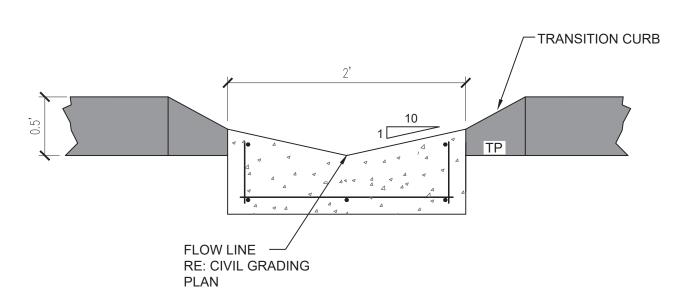
MOUNTABLE CURB RAISED ISLAND

NOT TO SCALE



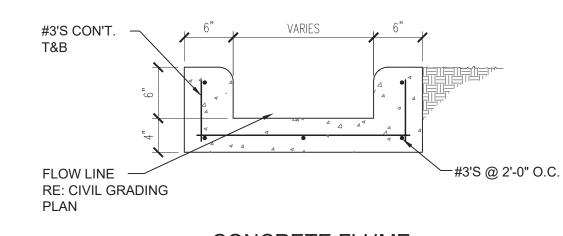
V-NOTCH CONCRETE SWALE IN GRASS AREA

NOT TO SCALE



V-NOTCH CONCRETE SWALE & TRANSITION CURB

NOT TO SCALE



CONCRETE FLUME NOT TO SCALE

	ess Waters Blvd. Suite 150 Dallas. Texas 75019 (817) 328-3200	ess Waters B
2017.09	Adame	
ADAMS JOE		
	RE-ISSUE FOR PERMIT	3/05/2020
	ISSUE FOR PERMIT	7/06/2018
	DESCRIPTION	DATE



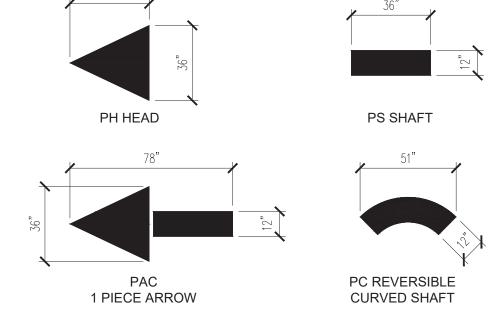
		PLAN APPROVALS		OFFICE	LONG BEACH FIELD OFFICE	
		SIGNATURE (2 REQUIRED)	DATE	ADDRESS	110 N. CARPENTER ST., CHICAGO, ILLINOIS 60607	
	REGIONAL MGR.				O I I VSII o'Lieno Com W	(
	CONST. MGR.				וכססוומות s ססע, דר	
	OPERATIONS DEPT.			These drawings a	These drawings and specifications are the confidential and proprietary property of McDo not be copied or reproduced without written authorization. The contract documents were	McDo
DA	REAL ESTATE DEPT.			specific site in co	specific site in conjunction with its issue date and are not suitable for use on a different s of these drawings for reference or example on another project requires the services of p	rent s
TE		CO-SIGN SIGNATURES		and engineers. F	and engineers. Reproduction of the contract documents for reuse on another project is	ct is
В	CONTRACTOR				4300 OSUNA NE	
Υ	OWNER				ALBUQUERQUE, NEW MEXICO	\mathcal{O}

	DATE	BY
DESIGNED	JUL 2018	вмв
DRAWN	JUL 2018	вмв
CHECKED	JUL 2018	DL
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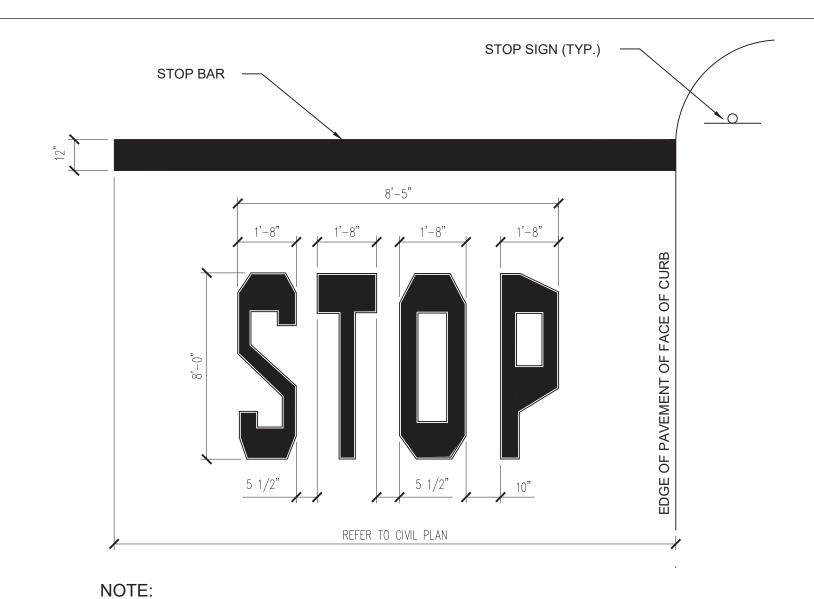
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ARROWS AND WORDS CAN BE ARRANGED IN OTHER COMBINATIONS THAN THOSE ILLUSTRATED HERE TO ACHIEVE DESIRED RESULT.



TYPICAL PAVEMENT MARKING

NOT TO SCALE



"STOP BAR"

NOT TO SCALE

REQUIREMENTS AS OUTLINED IN SECTION 3B OF THE MANUAL ON UNIFORM TRAFFIC

. WORDS AND ARROWS FOR DRIVEWAYS SHALL BE APPLIED ACCORDING TO

CONTROL DEVICES FOR STREETS AND HIGHWAYS

2. THESE WORDS AND BAR ARE TO BE PAINTED REFLECTIVE WHITE

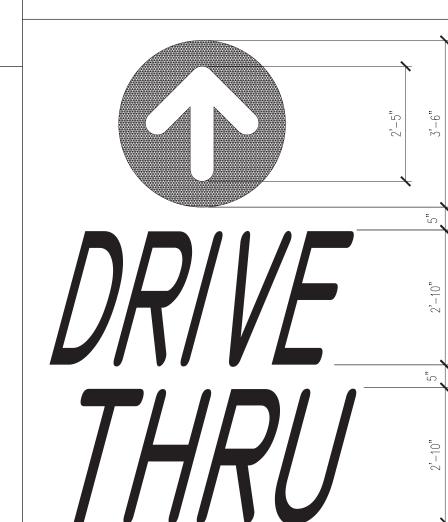
SYMBOL TO BE PAINTED PER ADA AND

PAINTED ACCESSIBLE PARKING SYMBOL

NOT TO SCALE



1. ALL TEXT SHALL BE PAINTED YELLOW (PMS 123).



ALL TEXT AND ARROWS SHALL BE PAINTED YELLOW (PMS 123)

> PAINTED "DRIVE THRU" MARKING

WIND SPEED (3 SEC. GUST) (ASCE 7-05) (MPH) 100 MPH 3'-9" PULL FORWARD PULL FORWARD 150 MPH 5'-3" SIGNAGE SIGNAGE 1 1/2 " DIA. 1 1/2 " DIA. **→** 1'-2" **→ →** 1'-2" **←** GALV. PIPE -GALV. PIPE -SLOPE TO SLOPE TO DRAIN -DRAIN -4" GROUT FILLED PIPE 4" GROUT FILLED PIPE **BOLLARD (PAINT PIPE &** BOLLARD (PAINT PIPE & **EXPOSED CONCRETE -EXPOSED CONCRETE -**YELLOW PMS 123C) YELLOW PMS 123C) SLOPE T.O. FND. 1/2" FOAM WRAP PATCH TO SHED WATER-CONCRETE WITH SEALANT TO MATCH FINISHED LANDSCAPE CONCRETE PAVING EXISTING GRADE #30 FELT SLIP SHEET (2)#3 REBAR TIES (2)#3 REBAR TIES @ 150 MPH WIND @ 150 MPH WIND SPEEDS SPEEDS 18" DIA. CONCRETE 18" DIA. CONCRETE PIER 7" -- 7" PIER -OOSP PARKING SIGN DETAIL (IN PAVING AREAS) OOSP PARKING SIGN DETAIL (IN LANDSCAPE AREAS)

OBJECTIVE OF STANDARDIZATION:

McDONALD'S OBJECTIVE IS TO STANDARDIZE OPTIMUM MARKINGS IN THE UNITED STATES TO ASSIST CUSTOMERS IN EASILY FINDING THE DRIVE-THRU LANES. THE MARKING POSITIONS ARE TO GUIDE THEM FROM ANY ENTRANCE ON THE PARKING LOT TO THE DRIVE-THRU LANE USING THE OPTIMUM ROUTE. THIS IS TYPICALLY AWAY FROM THE PRIMARY DRIVE AISLE, MOST COMMON ENTRANCE OR AROUND THE BUILDING TO INCREASE STACKING IN THE LANE. THE STANDARDIZATION FROM REGION TO REGION ASSISTS GUESTS WITH CONVENIENTLY FINDING THE DRIVE-THRU'S.

STANDARD LOT STRIPING STENCILS AND PAINT COLOR:

THESE ARE AVAILABLE FROM BETH BELL AT PAVEMENT STENCIL COMPANY, 4347-A AEROSPACE ROAD SE, ROANOKE, VA, 24014, 1-800-250-5547. THE FOLLOWING DESCRIPTION IS WHAT YOU WOULD SAY AS YOU ORDER. SHE HAS NO PART NUMBERS ASSOCIATED WITH THESE:

LOT STRIPING STENCILS DESCRIPTION

ROUND CIRCLE (DIRECTIONAL ARROW)

THANK YOU

DOUBLE HEADED ARROW FOR A DOUBLE DRIVE-THRU IS MADE UP OF THREE COMPONENTS.

PC SHAFT 12"W X 36"L, A PC REVERSIBLE CURVED SHAFT 12"W X 51"L

PH ARROW HEAD 38"L X 36"W

LANE STRIPE IS A 6" WIDE STRIPE DONE BY THE LOT STRIPING COMPANY.

39" ADA HANDICAP TEMPLATE PAINT COLOR: THIS IS FOR ALL DRIVE-THRU DIRECTIONAL STRIPING INCLUDING THE

PAINTED STRIPE FOR THE LANE. THE PAINT COLOR SHOULD MATCH PMS 123 YELLOW. PROVIDE YELLOW PAINT ON ALL DRIVE-THRU MARKINGS UNLESS NOTED OTHERWISE.

GUIDING PRINCIPLES

ALL ENTRANCES TO THE LOT

- THE WORD DRIVE THRU IS PLACED AT ALL ENTRANCES TO THE LOT APPROXIMATELY 25'-30' FROM THE CURB OR SIDEWALK. THEY SHOULD BE CENTERED IN THE DRIVEWAY (ON THE INGRESS SIDE OF THE DRIVE AISLE IF THERE IS TWO WAY TRAFFIC). RATIONALE: THIS ALLOWS THE CUSTOMER TO MOVE SAFELY ONTO THE LOT AND SEE THE DRIVE-THRU DIRECTIONAL ARROW WHEN THEY ARE SAFELY OFF THE STREET.
- ROUND CIRCLE (DIRECTIONAL ARROW): THE CIRCLE ARROW SHOULD BE CENTERED ABOVE THE WORD "DRIVE" APPROXIMATELY 5 FEET FURTHER INTO THE PARKING LOT. RATIONALE: THE STANDARD YELLOW COLOR WITH THE WORD DRIVE-THRU SEEN FIRST AND THE ROUND ARROW BEGINS TO BRAND THE MARKINGS.

LOT DIRECTIONAL MARKINGS:

- ROUND CIRCLE (DIRECTIONAL ARROW): THE ARROWS SHOULD BE SPACED EVERY 40 TO 60 FEET. THIS ALLOWS EVENLY POSITIONED ARROWS THROUGHOUT THE LOT. RATIONALE: THE STRATEGIC POSITIONING ALLOWS THE CAR TO REACH AN ARROW AND OFF IN THE DISTANCE SEE THE NEXT DIRECTIONAL ARROW. THIS LEADS THEM IN THE DESIRE DIRECTION.
- IF THERE ARE 5 OR MORE ARROWS ROUTING SOMEONE TO THE DRIVE-THRU THEN PLAN THE CORRECT PLACEMENT TO ADD THE WORD DRIVE-THRU AND POSITION THE ARROW CENTERED ABOVE THE WORD "DRIVE" SIMILAR TO THE ENTRANCE. THIS IS TO BE CENTERED BETWEEN THE ARROWS ON THE PARKING LOT. AN EXAMPLE IS IT TAKES EIGHT ARROWS TO GUIDE SOMEONE FROM THE ENTRANCE TO THE DRIVE-THRU ENTRANCE. YOU MIGHT DECIDE TO PLACE THE WORD DRIVE-THRU AT THE 4TH ARROW POSITION. RATIONALE: THE LONGER THE RUN TO THE DRIVE-THRU THIS REAFFIRMS THE COLOR AND DIRECTIONAL ARROWS ARE STEERING THEM IN THE RIGHT DIRECTION FOR THE DRIVE-THRU

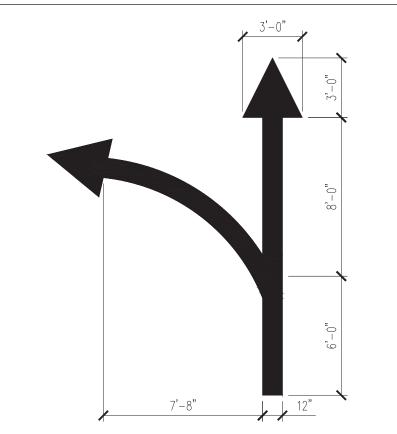
DRIVE-THRU ENTRANCE:

 THE DOUBLE HEADED ARROW FOR DOUBLE DRIVE-THRU SHOULD BE POSITIONED TO DIRECT TRAFFIC APPROPRIATELY TO EITHER LANE AS THEY APPROACH TO THE ISLAND. EACH ARROW MUST BE CUSTOMIZED TO FIT THE LANE CONFIGURATION. RATIONALE: EACH CUSTOMER READS FROM DIFFERENT LEVELS SOME BY LOT MARKINGS, SOME AT EYE LEVEL AND OTHERS LOOK ABOVE THE VEHICLES. BY ADDRESSING ALL OF THESE METHODS IN BRANDING, THE USE IS SIMPLIFIED FOR THE MAJORITY OF THE GUESTS. THE DOUBLE ARROW INCREASES THE USAGE OF THE OUTSIDE LANE IN OFF PEAK TIMES HELPING THE RESTAURANT MAXIMIZE THE CAPACITY.

PARKING LOT STRIPING NOT IN THE DRIVE-THRU:

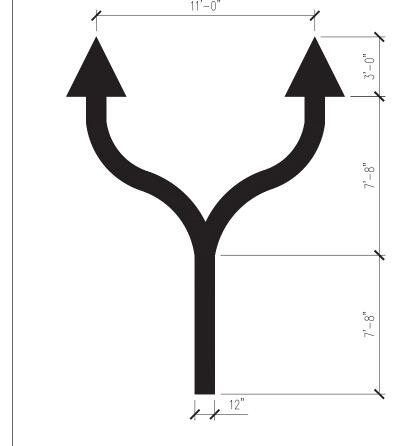
- ANY LOT STRIPING OTHER THAN THE DRIVE-THRU SHOULD BE WHITE. IF THE CITY CODE REQUIRES BLUE WITH THE HANDICAP PARKING STALLS THAT IS AN ACCEPTABLE DEVIATION. RATIONALE: THIS HELPS SUBTLY IDENTIFY WHAT IS A DRIVE-THRU MARKING FROM THE PARKING LOT MARKING AND CREATES A
- RUNWAY PATH TO THE LANE ENTRANCE. THE ENTRANCES WILL ALL HAVE A WHITE INGRESS/ EGRESS ARROW FOR THE CUSTOMER TO EASILY IDENTIFY IF IT IS A ONE WAY OR TWO-WAY ENTRANCE. THESE ARE WITHIN 10FT FROM INGRESS POINT AND IS DESIGNED TO BE SEEN CLEARLY BEFORE A CAR MAKES A TURNING COMMITMENT. RATIONALE: THE CONSISTENCY HERE WILL HELP CUSTOMERS IDENTIFY THE FLOW OF TRAFFIC ON ALL ENTRANCES FOR THE CONSUMER WHEN THEY ARE AT A DECISION POINT.
- ANY ADDITIONAL WHITE ARROWS NEEDED SHOULD BE POSITIONED IN BETWEEN THE DRIVE-THRU DIRECTIONAL MARKINGS. ANY WORDING NEEDED OTHER THAN FOR THE DRIVE-THRU AREA SHOULD BE IN
- WHITE AND POSITIONED SO IT DOES NOT INTERFERE WITH THE DRIVE-THRU

SHOULD YOU HAVE ADDITIONAL QUESTIONS FEEL FREE TO CONTACT THE McDONALD'S RESTAURANT DESIGN GROUP AT THE HOME OFFICE FOR FURTHER CLARIFICATIONS.



THIS IS TO BE USED ON ANY DESIGN THAT WOULD REQUIRE A "T" GATEWAY. ONLY DESIGNS THAT DO NOT HAVE A SINGLE ENTRY THAT LEADS A CAR RIGHT AT THE DECISION POINT OF THE TIP OF THE ISLAND. ONLY ONE OF OUR STANDARDS TEMPLATES HAS THIS DESIGN.

> T-GATEWAY DRIVE-THRU MARKING NOT TO SCALE



THIS IS TO BE USED ON ALL DESIGNS THAT HAVE A SINGLE POINT OF ENTRY THAT LEADS A CAR DIRECTLY TOWARD THE TIP OF THE ISLAND

> DOUBLE DRIVE-THRU MARKING NOT TO SCALE

DRAWN JUL 2018 BMB CHECKED JUL 2018 | DL AS-BUILT STANDARD **DETAILS**

DATE BY

JUL 2018 BMB

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N.T.S. N.T.S.

STATE REQUIREMENTS.

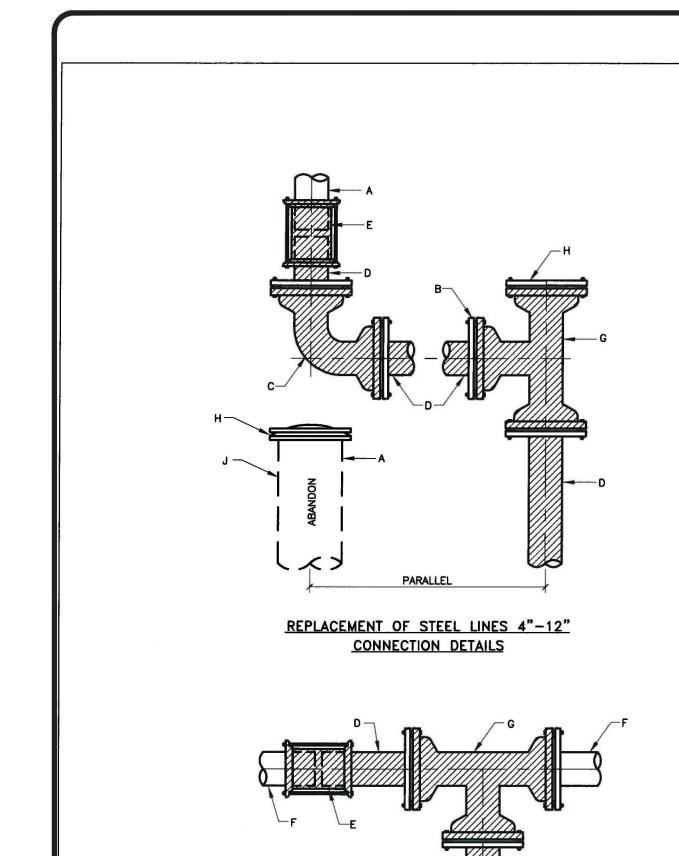
2. PLACE 30'-0" FROM CENTERLINE OF PICK UP BOOTH.

PAINTED "THANK YOU" NOT TO SCALE

NOT TO SCALE

C Copyright 2018, Adams Engineering

DESIGNED



TEE INSERTION D.I., P.V.C. OR A.C. PIPE

GENERAL NOTES:

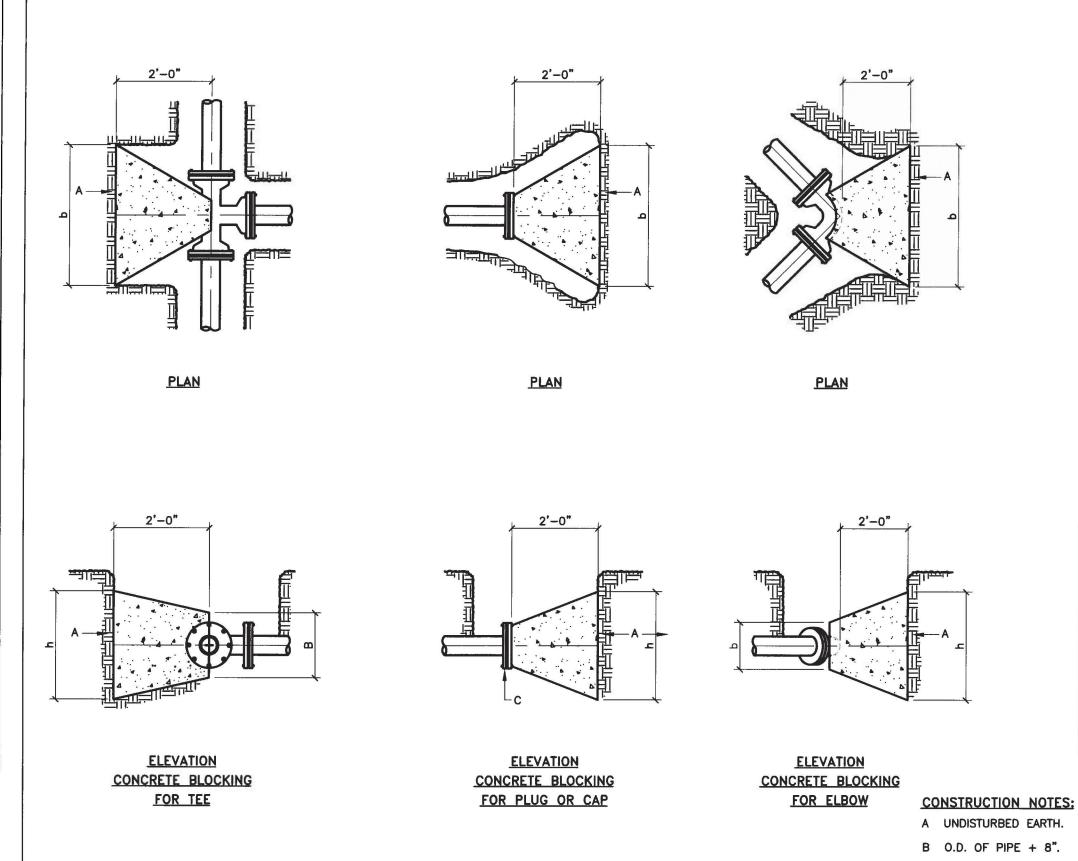
- 1 ALL NEW PIPE AND FITTINGS SHALL BE PROVIDED WITH THRUST CONTROL.
- 2 THRUST CONTROL SHALL BE BY RESTRAINED JOINTS ONLY UNLESS DIRECTED OTHERWISE BY ENGINEER.
- 3 EMD'S ARE REQUIRED AT VALVES, TEES, FLANGED OUTLETS (ON CONCRETE CYLINDER PIPES), AND CAPPED OR PLUGGED ENDS. SEE SPECIFICATION SECTION 170 FOR LOCATIONS.

CONSTRUCTION NOTES: A EXISTING STEEL PIPE.

- B REDUCE AT TEE, IF EXISTING LINE IS SMALLER THAN NEW LINE.
- C M.J., C.I. ELBOW WITH JOINT RESTRAINT.
- D NEW D.I. OR P.V.C., WITH VALVE AS DIRECTED.
- E RESTRAINED TRANSITION COUPLING FOR A.C. RESTRAINED SOLID SLEEVE FOR D.I., C.I. AND PVC.
- F EXISTING D.I., C.I., P.V.C., OR A.C.. IF A.C., USE ADAPTER APPROVED BY ENGINEER OR AS APPROVED ON THE CURRENT WATER AUTHORITY APPROVED
- G M.J. D.I. TEE WITH JOINT RESTRAINT.

REVISIONS

- H M.J., C.I. PLUG OR CAP WITH JOINT RESTRAINT.
- J REMOVE AT LEAST 10' OF PIPE TO BE ABANDONED AND CAP OR PLUG.



GENERAL NOTES:

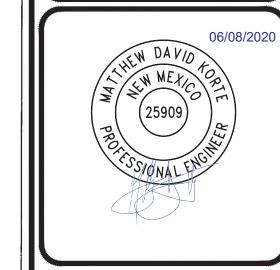
C O.D. OF CAP OR PLUG, MIN. 12"x12".

D ONLY FOR EXCEPTIONAL SITUATIONS,

USE OF MECHANICAL RESTRAINTS TAKES PRECEDENCE.

- 1 ALL THRUST CONTROL BY RESTRAINED JOINTS ONLY UNLESS DIRECTED BY ENGINEER, AND FOR "SPECIAL" SITUATIONS SPECIFIED BY THE WATER AUTHORITY.
- 2 PIPE SIZE GREATER THAN 14" REQUIRES DESIGN BY ENGINEER TO BE SUBMITTED TO THE WATER AUTHORITY FOR APPROVAL.
- 3 CONCRETE BLOCKING PER SEC. 101 EXTERIOR CONCRETE, f'c=3000 psi @ 28 DAYS.

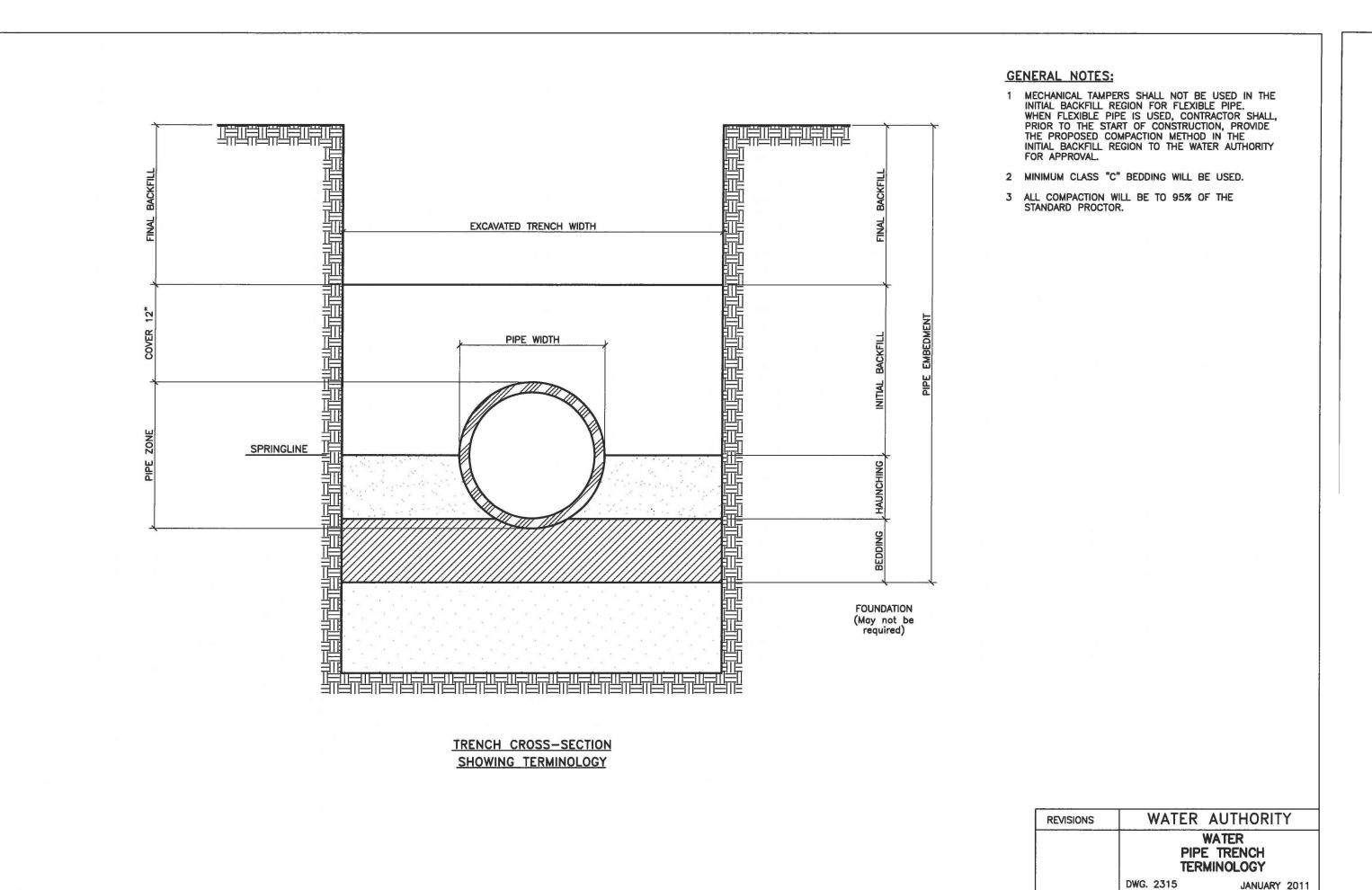
PIPE SIZE	ELBOW ANGLE	ELBOW (b) DIM.	ELBOW (h) DIM.	TEE OR PLUG (b) DIM.	TEE OR PLUC (h) DIM.
4"			N - 111 - 11	2'	1'
4"	90° 45°	2'	2'		
4"	22 1/2° 11 1/4°	2'	2'		
6"				2'	2'
6"	90° 45°	2'	2'		2000
6"	22 1/2	2'	2'		
8"				3'	3'
8"	90°	3'	3'		4
8"	45'	2'	2'		
8"	22 1/2° 11 1/4°	2'	2'		
10"				3'	3'
10"	90°	3'-6"	3'-6"		
10"	45°	3'	3'		•"
10"	22 1/2° 11 1/4°	2'	2'		
12"				3'-6"	3'-6"
12"	90.	4'	4'		
12"	45'	3'-6"	3'-6"		
12"	22 1/2° 11 1/4°	2'	2'	i i	
14"				4'	4'
14"	90*	5'	5'		
14"	45*	3'-6"	3'-6"		
14"	22 1/2° 11 1/4°	3'	3'		



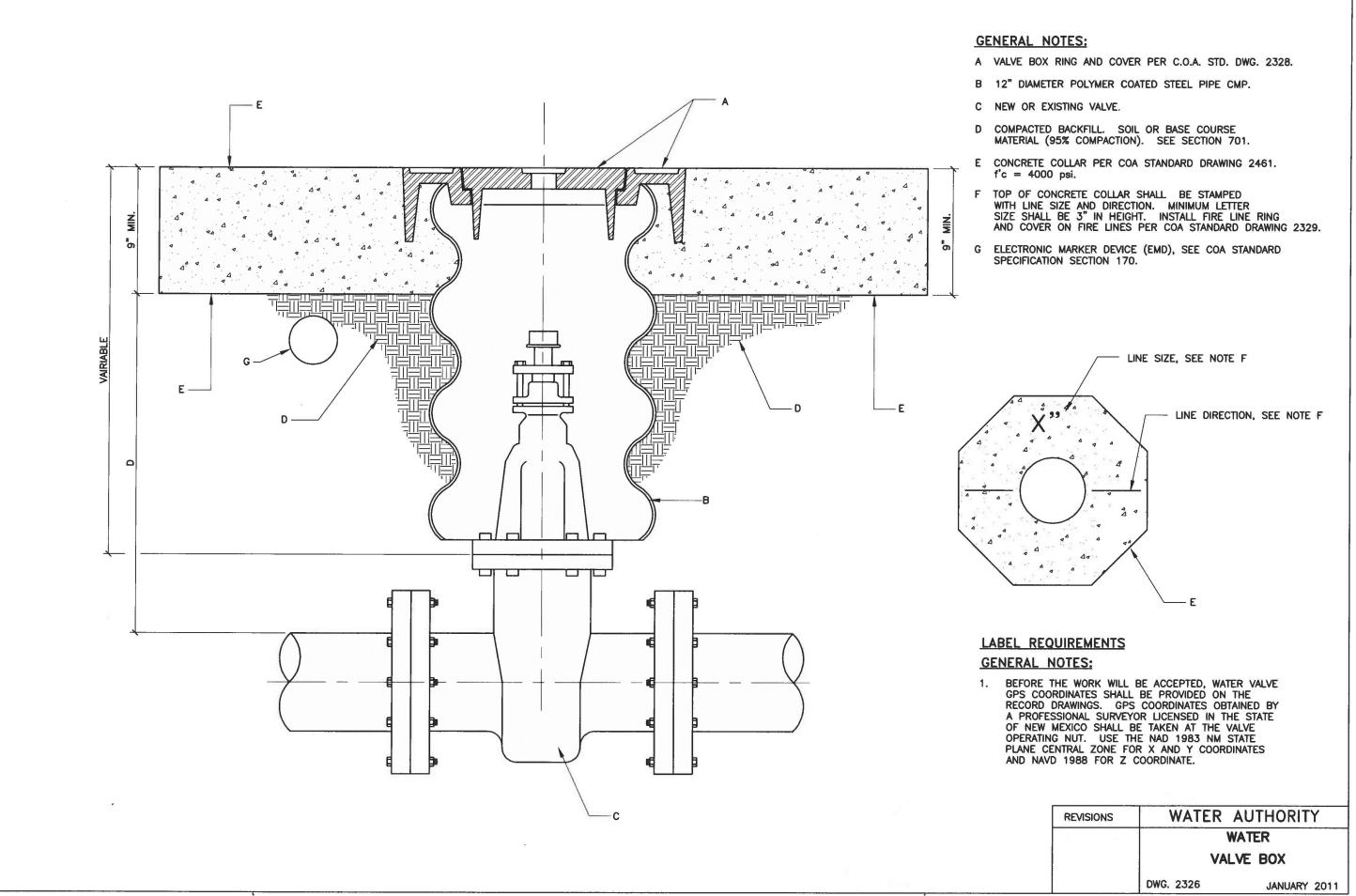
MCDONALD'S L/C: 030-0127	ty of McDonald's USA, LLC and shall ments were prepared for use on this different site or at a later time. Use rvices of properly licensed architects project is not authorized.	XICO

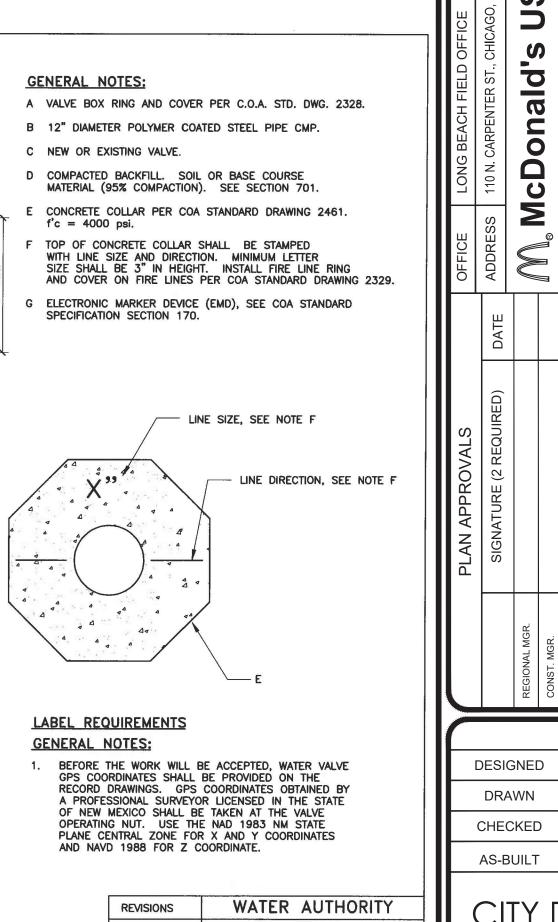
WATER WATERLINE CONNECTION **DETAILS** JANUARY 2011

WATER AUTHORITY



TRANSITION COUPLING FROM D.I., P.V.C., OR A.C. TO D.I. OR P.V.C





WATER AUTHORITY

WATER

CONCRETE BLOCKING

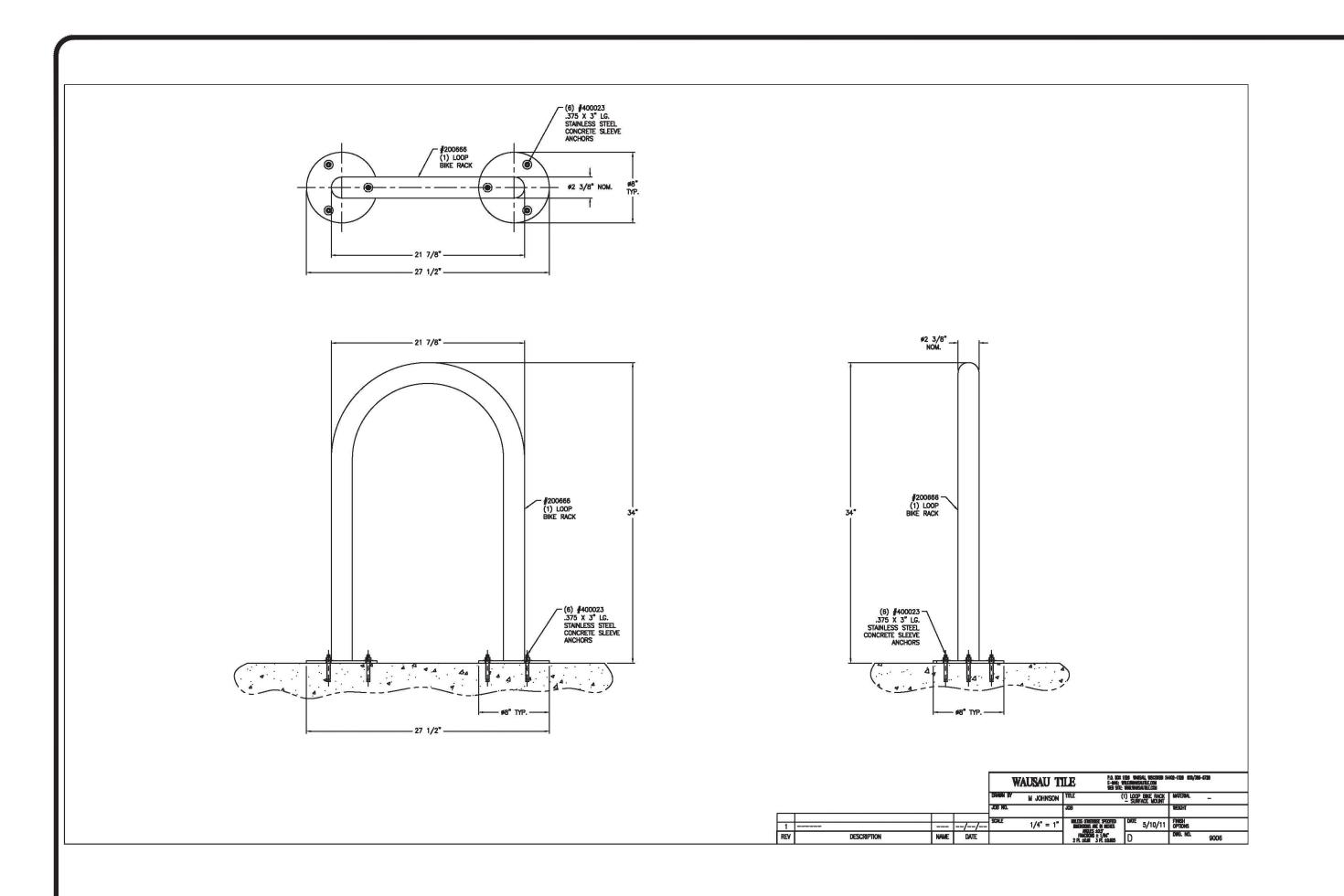
DESIGN

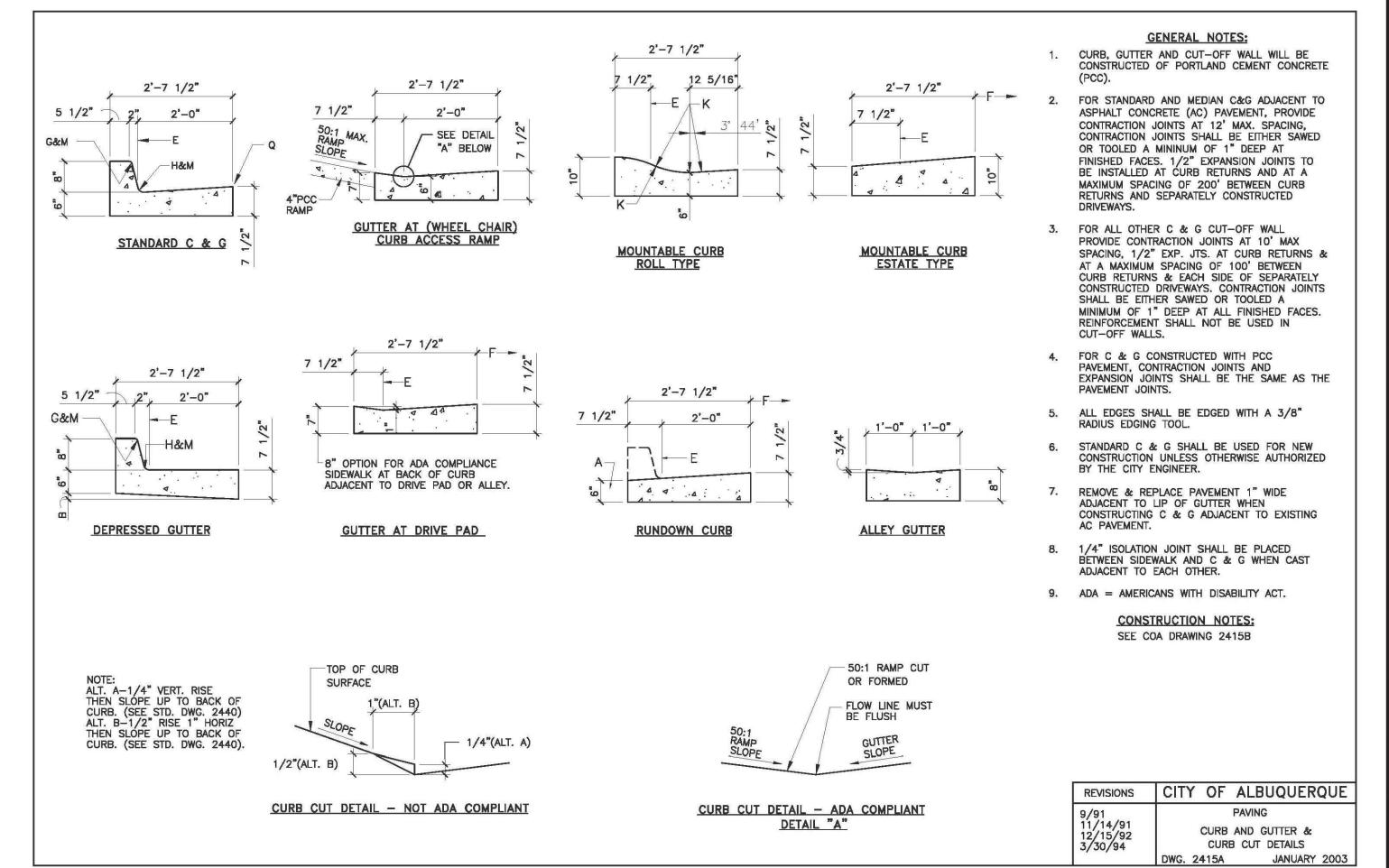
JANUARY 2011

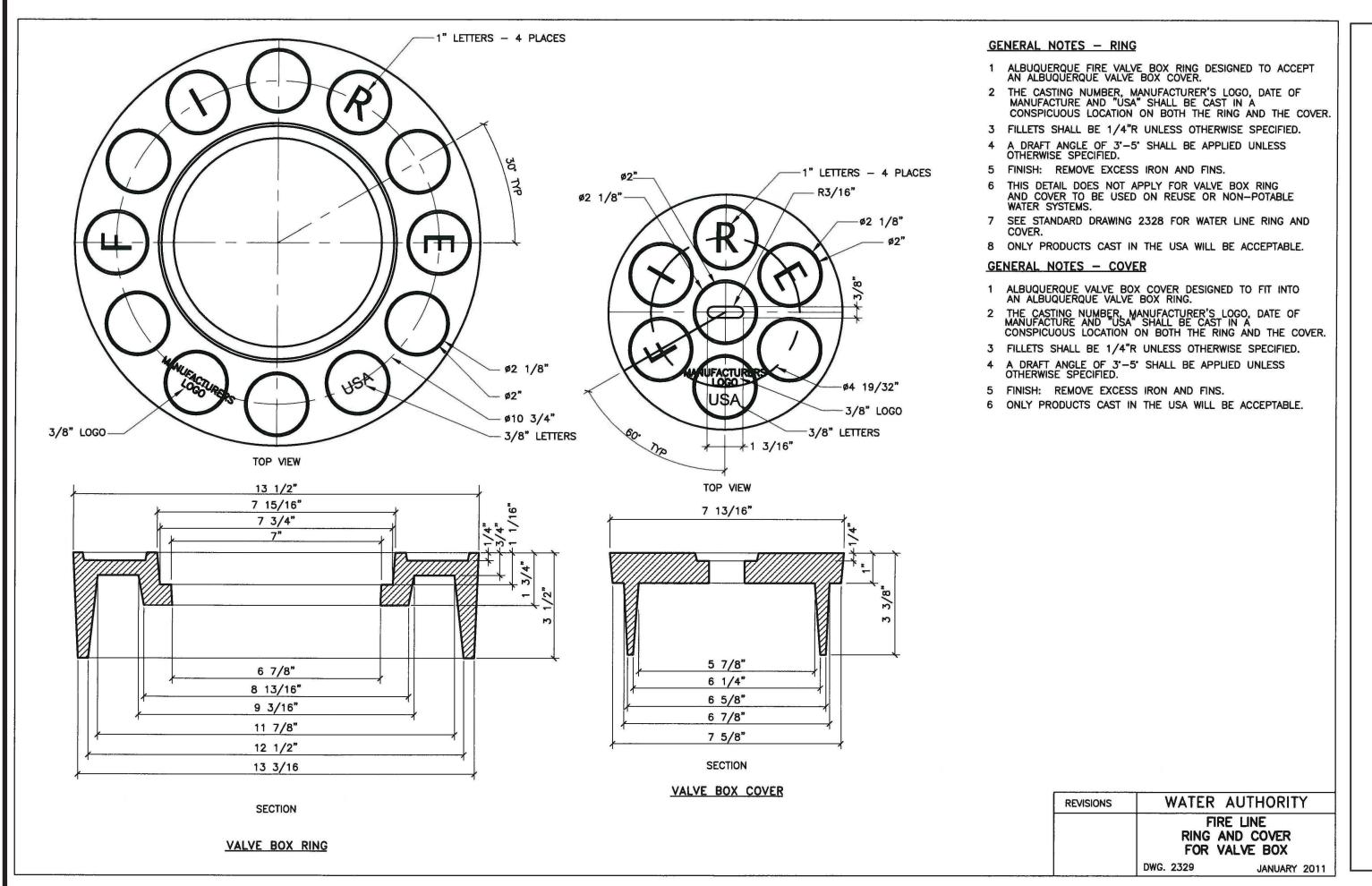
DATE DESIGNED JUN 2020 HJM DRAWN JUN 2020 HJN CHECKED AS-BUILT

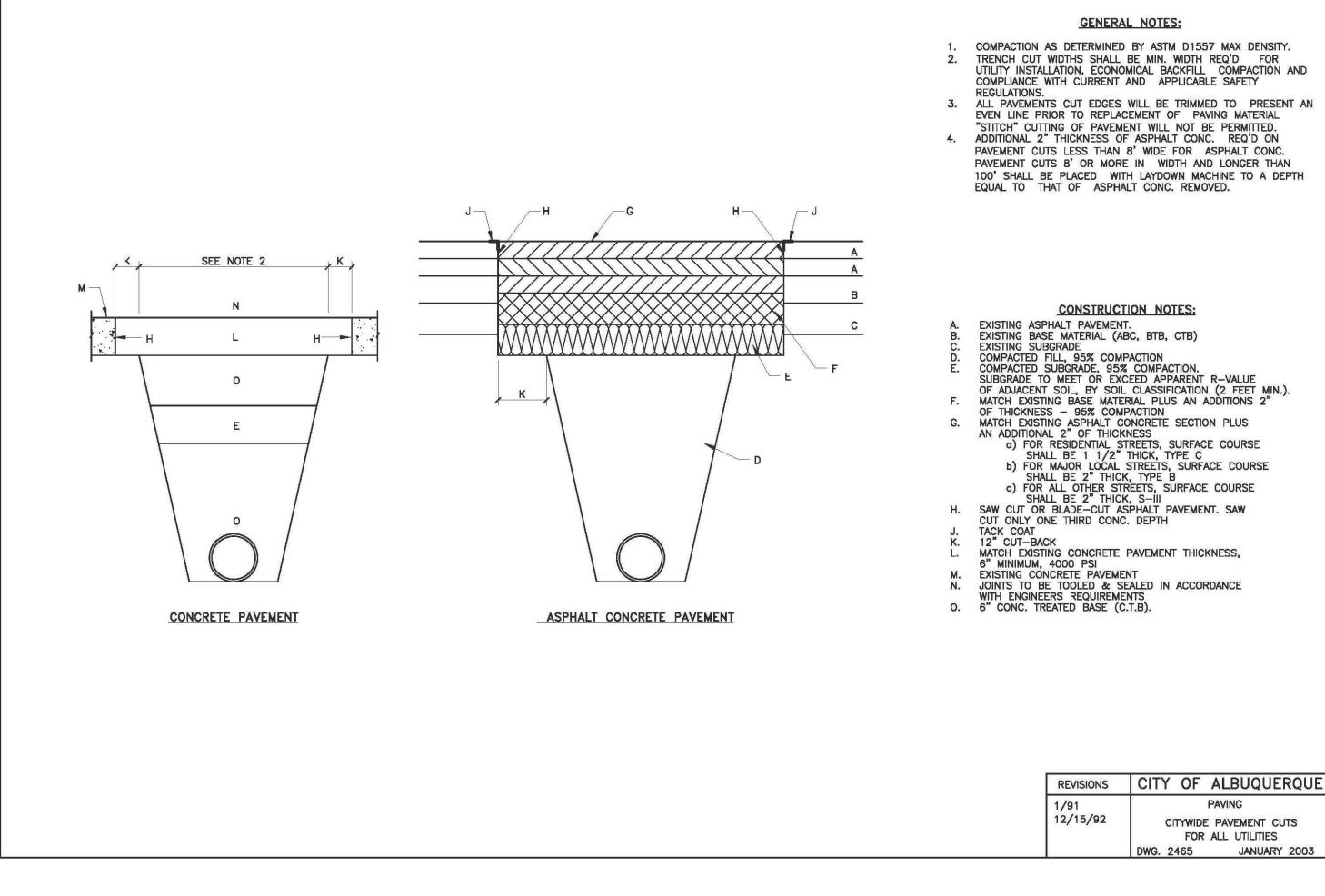
CITY DETAILS

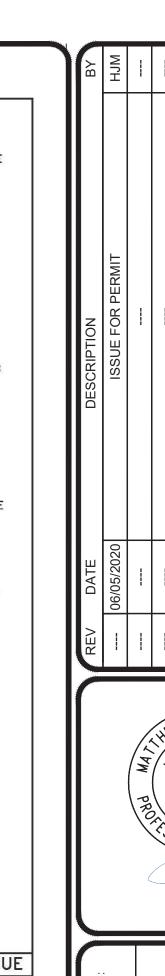












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06/08/202

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В	CONTRACTOR			
Υ	OWNER			

DESIGNED JUN 2020 HJM DRAWN JUN 2020 | HJN CHECKED AS-BUILT

CITY DETAILS