

Vicinity Map

SITE CONSTRUCTION DRAWINGS FOR LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM 87109

DEVELOPED BY

GROUP 1 AUTOMOTIVE®

800 GESSNER, SUITE 500
HOUSTON, TEXAS 77024
713-647-5700

PROJECT NUMBER: PR-2023-008423

Application Number: SI-2023-00606

Is an Infrastructure List required? () Yes () No If yes, then a set of approved DRC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DFT SITE DEVELOPMENT PLAN APPROVAL:

<i>Ernest Armijo</i>	Jul 25, 2023
Traffic Engineering, Transportation Division	Date
<i>Joe Galt</i>	Aug 2, 2023
ABCWUA	Date
<i>Walter Babin</i>	Jul 27, 2023
Parks and Recreation Department	Date
<i>Regina Cho</i>	Jul 25, 2023
Hydrology	Date
<i>Shi Pan</i>	Jul 25, 2023
Code Enforcement	Date
* Environmental Health Department (conditional)	Date
Solid Waste Management	Date
<i>Shirley</i>	Aug 4, 2023
Planning Department	Date

*Environmental Health, if necessary
12/18/22

CIVIL DRAWING INDEX	
SHEET	DRAWING TITLE
C00.0	SITE COVER SHEET
C00.1	EXISTING CONDITIONS
C01.0	DEMOLITION PLAN
C02.0	SITE PLAN
C02.1	PAVING PLAN
C03.0	EROSION CONTROL PLAN
C04.0	GRADING PLAN SOUTH
C04.1	GRADING PLAN NORTH
C05.0	UTILITY PLAN
C06.0	LANDSCAPE PLAN
C06.1	EXISTING LANDSCAPE PLAN
C07.0	LANDSCAPE DETAILS
C08.0	EROSION CONTROL DETAILS
C09.0	UTILITY DETAILS
C09.1	UTILITY DETAILS
C09.2	UTILITY DETAILS
C10.0	SITE DETAILS
C10.1	SITE DETAILS
C10.2	SITE DETAILS
C11.0	SIGHT DISTANCE EXHIBIT
C12.0	TEMPORARY TRAFFIC CONTROL PLAN
A-2.0	EXTERIOR ELEVATIONS
A-2.1	EXTERIOR ELEVATIONS
A-2.2	EXTERIOR ELEVATIONS
	NMDOT SITE DETAILS

LEGEND	
	PROPERTY CORNER
	CENTER LINE
	PROPERTY LINE
	RIGHT OF WAY
	SEPTIC TANK
	WATER METER
	CATV
	TELEPHONE PEDESTAL
	POWER POLE
	ELECTRICAL BOX
	BENCHMARK
	FIRE HYDRANT
	WATER VALVE
	BLOW OFF
	SANITARY SEWER MANHOLE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING SPOT ELEVATION
	PROPOSED SPOT ELEVATION
	TOP OF WALL ELEVATION
	BOTTOM OF WALL ELEVATION
	PROPOSED SWALE
	RIP RAP
	FLARED END SECTION
	STORM DRAIN DROP INLET
	STORM DRAIN JUNCTION BOX
	HANDICAP PARKING SPACE
	WATER LINE
	SANITARY SEWER
	SANITARY FORCE MAIN
	EXISTING OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	SILT FENCE
	INLET PROTECTION
	STORM DRAIN
	TREE SAVE / PROTECTION
	TREE TO BE REMOVED

GENERAL NOTES

- ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
- CONTRACTOR SHALL CONTACT "M ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING ANY EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
- SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
- ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
- FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
- GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
- BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
- BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
- WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
- ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN IDEQ APPROVED LANDFILL.
- CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
- ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
- CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
- ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
- TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROWLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
- ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSSWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A. CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
- CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
- REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.

AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnson Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
**LEXUS OF
ALBUQUERQUE**

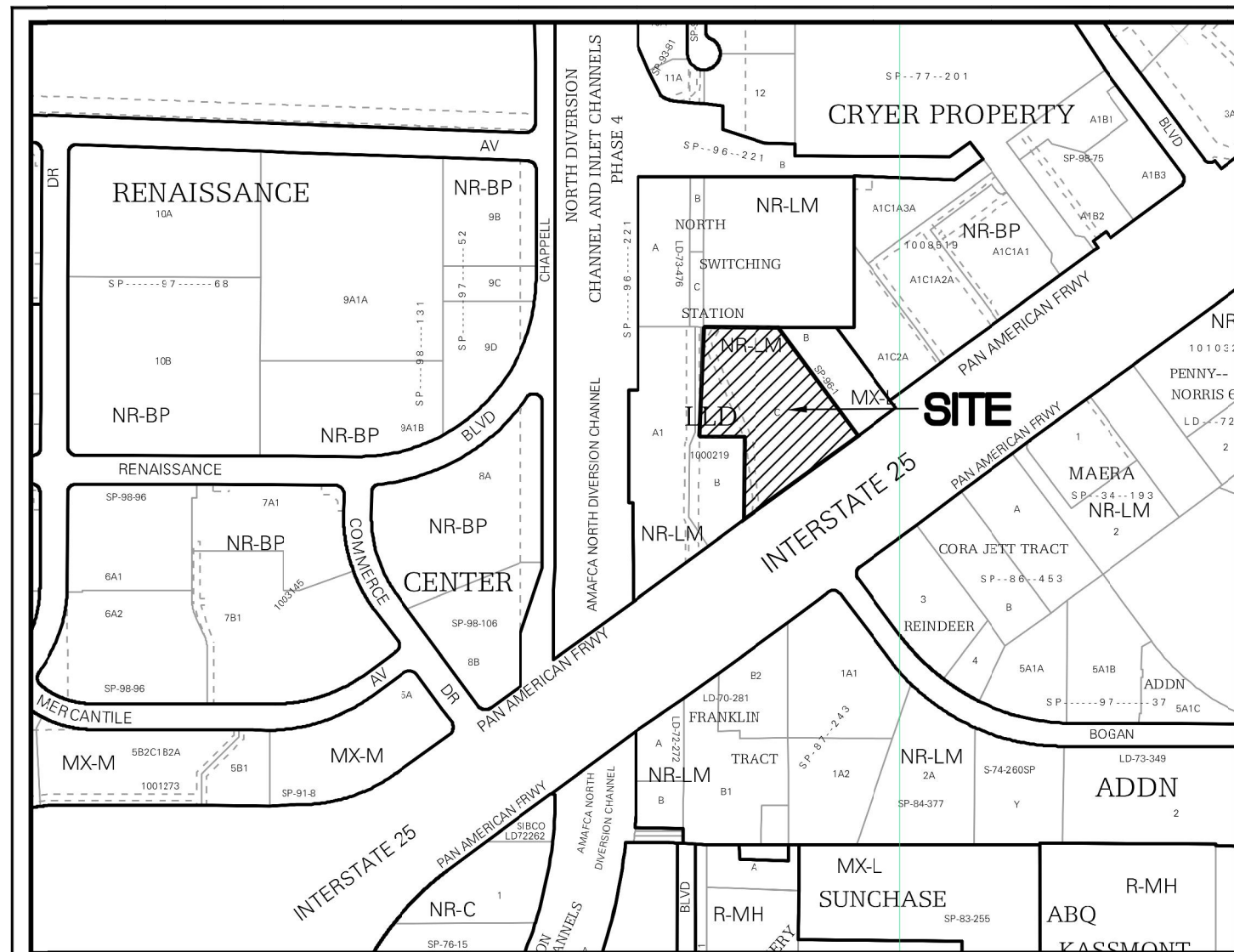
4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
**COVER
SHEET**

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/11/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/19/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23



C00.0



Vicinity Map

Easement Notes

- 1 EXISTING PNM EASEMENT FOR ROAD RIGHT OF WAY (4/10/2000, BK. A4, PG. 4328)
- 2 EXISTING 10' MST&T AND PNM EASEMENT (7/25/1976, BK. MISC. 622, PG. 132)
- 3 EXISTING 30' PRIVATE ACCESS CROSS LOT EASEMENT (4/2/2004, BK. A75, PG. 3899, DOC. NO. 2004044029), (4/2/2004, BK. A75, PG. 3901, DOC. NO. 2004044031) AND (4/2/2004, BK. A75, PG. 3902, DOC. NO. 2004044032)
- 4 EXISTING 30' SOUTHERN UNION GAS COMPANY EASEMENT (11/3/1955, VOL. D-332, FOL. 99)
- 5 EXISTING PNM AND MST&T EASEMENT (1/3/1974, BK. B9, PG. 24)
- 6 EXISTING 10' P.U.E. (1/4/1996, 96C-5)
- 7 EXISTING PRIVATE ACCESS EASEMENT (9/25/2014, 2014C-109) SHOWN HEREON AS [Symbol]
- 8 EXISTING 3.5' X 192' LANDSCAPE EASEMENT BENEFITING THAT PORTION OF TRACT B, LLD, LOCATED EAST OF THE EASEMENT (8/10/1999, 99C-225) AND (8/10/1999, BK. 9911, PG. 4699, DOC. NO. 19991049947) SHOWN HEREON AS [Symbol]
- 9 EXISTING 20' PNM (4/10/2000, BK. A4, PG. 4327, DOC. NO. 2000034475)
- 10 EXISTING 15' P.U.E. (1/3/1974, B9-24)
- 11 EXISTING 5' QWEST CORPORATION ENCROACHMENT AREA (1/4/2001, A1-1011)
- 12 EXISTING QWEST CORPORATION EASEMENT (1/4/2001, BK. A1, PG. 1010)
- 13 EXISTING 15' MST&T RIGHT OF WAY EASEMENT FOR ACCESS AND MAINTENANCE (6/1/1970, BK. MISC. 175, PG. 555)
- 14 EXISTING SIGN EASEMENT FOR BILLBOARD (4/2/2004, BK. A75, PG. 3899, DOC. NO. 2004044029) AND (4/2/2004, BK. A75, PG. 3901, DOC. NO. 2004044031) SHOWN HEREON AS [Symbol]
- 15 EXISTING 70' X 30' PNM EASEMENT (8/26/1994, BK. 94-24, PG. 9596-9597, DOC. NO. 94106089) SHOWN HEREON AS [Symbol]
- 16 EXISTING PNM EASEMENT (8/26/1994, BK. 94-24, PG. 9596-9597, DOC. NO. 94106089)
- 17 EXISTING 10' UNDERGROUND UTILITY PNM AND US WEST COMMUNICATIONS, INC. EASEMENT (2/1/1995, BK. 95-3, PG. 3907, DOC. NO. 95011050)
- 18 EXISTING 20' PNM AND US WEST EASEMENT (12/16/1998, BK. 9820, PG. 128, DOC. NO. 1998162004)
- 19 EXISTING 10' X 35' PNM AND US WEST EASEMENT (12/16/1998, BK. 9820, PG. 128, DOC. NO. 1998162004)
- 20 EXISTING 10' DRAINAGE EASEMENT (12/28/2015, DOC. NO. 2015111690)
- 21 EXISTING 10' PNM AND US WEST EASEMENT (2/8/1996, BK. 96-4, PG. 2640-2641) RELEASED BY QWEST CORPORATION BY QUITCLAIM DEED (1/4/2001, BK. A1, PG. 1009, DOC. NO. 2001001015)
- 22 INTENTIONALLY OMITTED
- 23 APPARENT P.U.E., ALTHOUGH SHOWN ON PLATS (8/10/1999, 99C-225) AND (3/26/2004, 2004C-102) AS EXISTING 10' P.U.E. GRANTED BY PLAT (1/4/1996, 96C-5), THIS IS INCORRECT. PLAT (1/4/1996, 96C-5) DID NOT GRANT ANY EASEMENT ACROSS WHAT IS NOW TRACT B, LLD.

THIS IS NOT A BOUNDARY SURVEY, APPARENT PROPERTY CORNERS ARE SHOWN FOR ORIENTATION ONLY. BOUNDARY DATA SHOWN IS FROM PREVIOUS SURVEY REFERENCED HEREON.

Legal Description

TRACTS LETTERED "C" OF THE PLAT OF TRACTS A, B AND C OF THE LLD SUBDIVISION, AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT OF SAID ADDITION, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO, ON MARCH 26, 2004, IN PLAT BOOK 2004C, FOLIO 102.

Benchmark -NAVD 88

ACS MONUMENT "NDC-14" HAVING AN ELEVATION OF 5107.996.

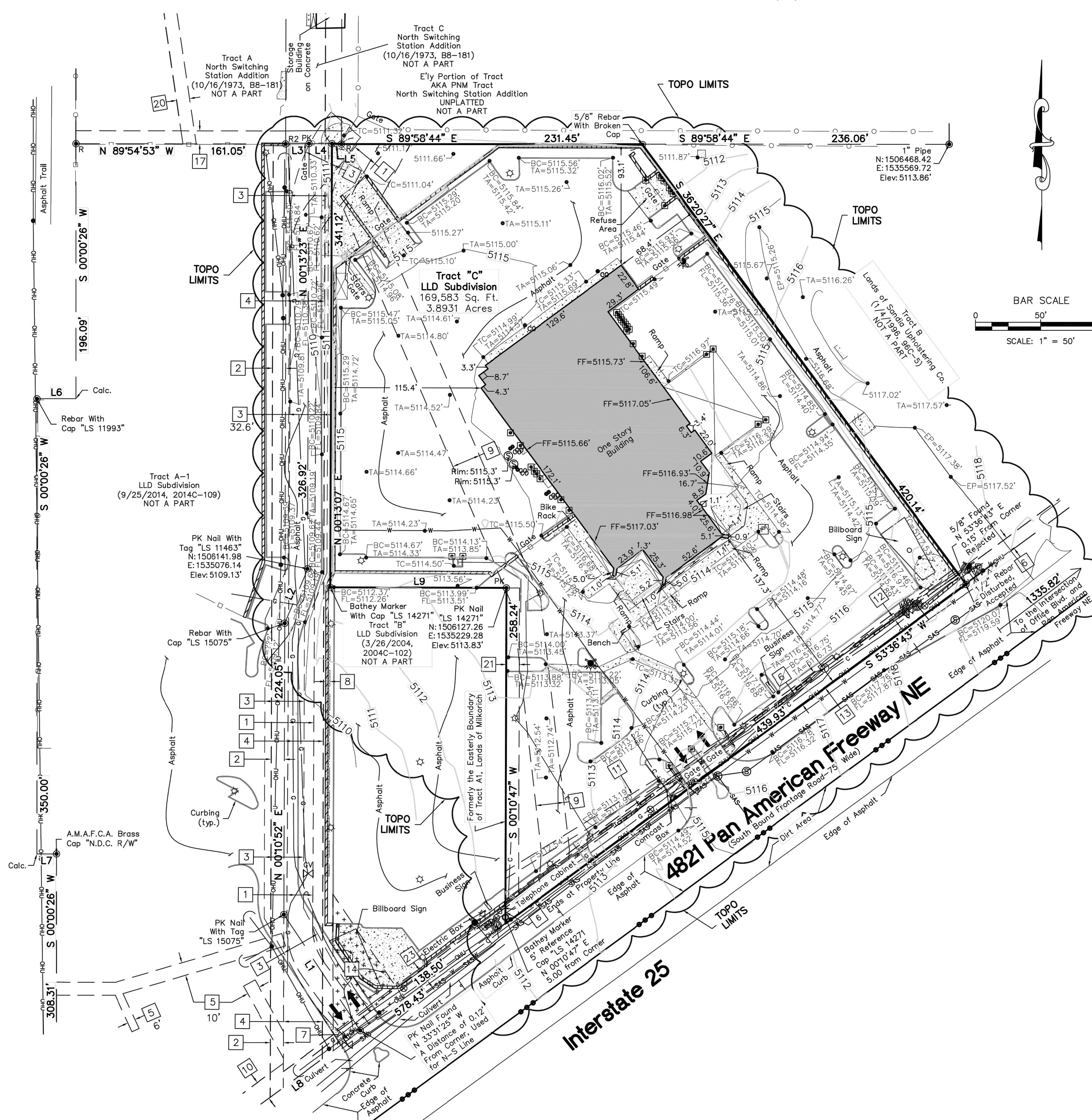
Notes

1. FIELD SURVEY PERFORMED IN OCTOBER 2019 AND TOPOGRAPHIC DATA ADDED FROM FIELD WORK CONDUCTED IN OCTOBER 2021.
2. ALL DISTANCES ARE GROUND DISTANCES: U.S. SURVEY FOOT.
3. THE BASIS OF BEARINGS REFERENCES NEW MEXICO STATE PLANE COORDINATES (NAD 83-CENTRAL ZONE).
4. ALTHOUGH THIS IS NOT A BOUNDARY SURVEY, A BOUNDARY SURVEY WAS PERFORMED ON THIS PROPERTY, SO THIS TOPOGRAPHIC SURVEY MAY REFLECT SOME CHARACTERISTICS OF A BOUNDARY SURVEY.
5. WITH REGARD TO UNDERGROUND UTILITY LOCATIONS, SOURCE INFORMATION FROM PLANS AND MARKINGS WERE COMBINED WITH OBSERVED EVIDENCE OF UTILITIES PURSUANT TO SECTION 5.E.I.V. TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES. HOWEVER, LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY AND RELIABLY DEPICTED. IN ADDITION, IN SOME JURISDICTIONS, 811 OR OTHER SIMILAR UTILITY LOCATE REQUESTS FROM SURVEYORS MAY BE IGNORED OR RESULT IN AN INCOMPLETE RESPONSE, WHICH MAY AFFECT THIS SURVEYOR'S ASSESSMENT OF THE LOCATION OF THE UTILITIES. WHERE ADDITIONAL OR MORE DETAILED INFORMATION IS REQUIRED, THE CLIENT IS ADVISED THAT THAT EXCAVATION AND/OR A PRIVATE UTILITY LOCATE REQUEST MAY BE NECESSARY. INVERT DATA SHOWN WITH A ± SHOULD BE CONSIDERED TAKEN FROM AS-BUILTS. (NMB11 TICKET NO. 21SE100530)
6. THE COORDINATES SHOWN HEREON REFERENCE MODIFIED NEW MEXICO STATE PLANE COORDINATES (NAD 83-GROUND-CENTRAL ZONE), USING GROUND TO GRID FACTOR OF 0.999671334 WITH AN ORIGIN OF (0,0).

Topographic Map
for
Tract C,
LLD Subdivision
City of Albuquerque
Bernalillo County, New Mexico
October 2021

Legend

PK	MEASURED BEARINGS AND DISTANCES	□	UTILITY PEDESTAL
PK	FOUND MONUMENT AS INDICATED	⊕	TELEPHONE MANHOLE
R	FOUND PK NAIL WITH TAG "LS 5708"	⊕	BOLLARD
R2	FOUND 1/2" REBAR	—	OVERHEAD UTILITY LINE
X	FOUND 1/2" REBAR WITH CAP "LS 5708"	•	UTILITY POLE
X	FOUND CHISELED "X"	—	ANCHOR
PK	SET BATHEY MARKER "LS 14271" UNLESS OTHERWISE NOTED	⊕	ELECTRIC METER
PK	SET PK NAIL WITH TAG "LS 14271" UNLESS OTHERWISE NOTED	⊕	TRANSFORMER
⊕	COVERED AREA	⊕	ELECTRIC CABINET
⊕	CONCRETE	⊕	A/C UNIT
⊕	ASPHALT	⊕	GAS METER
⊕	WOOD DECK	⊕	GAS VALVE
⊕	BLOCK WALL	⊕	TELEPHONE CABINET
⊕	GUARD RAIL	⊕	TELEPHONE MANHOLE
⊕	WOOD FENCE	⊕	WATER VALVE
⊕	METAL FENCE	⊕	WATER METER
⊕	CHAINLINK FENCE	⊕	FIRE HYDRANT
⊕	HANDRAIL	⊕	ROOF DRAIN
⊕	UNDERGROUND GAS UTILITY LINE	⊕	SANITARY SEWER MANHOLE
⊕	UNDERGROUND WATER UTILITY LINE	⊕	SAS CLEANOUT
⊕	UNDERGROUND SANITARY SEWER LINE	⊕	IRRIGATION BOX
⊕	UNDERGROUND ELECTRIC UTILITY LINE	⊕	STORM DRAIN MANHOLE
⊕	UNDERGROUND TELEPHONE UTILITY LINE	⊕	STORM DRAIN INLET
⊕	UNDERGROUND FIBER OPTIC UTILITY LINE	⊕	IRRIGATION VALVE
⊕	UNDERGROUND STORM DRAIN UTILITY LINE	⊕	SIGN
⊕	UNDERGROUND CABLE UTILITY LINE	⊕	CURB CUT/INDICATION OF ACCESS TO ROADWAY
⊕	NMOT ACCESS CONTROL LINE	⊕	
5075.50	SPOT ELEVATION		
BC 5075.50	BACK OF CURB ELEVATION		
FL 5075.50	FLOW LINE ELEVATION		
EP 5075.50	EDGE OF PAVEMENT ELEVATION		
TA 5075.50	TOP OF ASPHALT ELEVATION		
TC 5075.50	TOP OF CONCRETE ELEVATION		



Line Table

Line #	Direction	Length (ft)
L11	N 53°36'43" E	20.29'
L12	N 291°4'55" W	44.70'
L13	N 22°47'04" E	45.32'
L14	N 49°41'04" W	30.00'
L15	N 22°47'04" E	45.33'
L16	N 291°4'55" W	52.10'
L17	N 53°36'43" E	11.21'
L18	N 53°36'43" E	11.21'
L19	N 53°36'43" E	7.73'
L20	N 32°20'52" W	33.73'

Curve Table

Curve #	Length	Radius	Delta	Chord Length	Chord Direction
C1	58.47'	115.42'	29°01'35"	57.85'	S 41°50'38" E
C2	62.37'	145.42'	24°34'32"	61.90'	S 39°49'39" E
C3	73.23'	105.40'	39°48'30"	71.77'	S 20°24'41" W
C4	94.07'	135.40'	39°48'30"	92.19'	S 20°24'41" W
C5	34.15'	145.42'	13°27'16"	34.07'	S 45°25'42" E

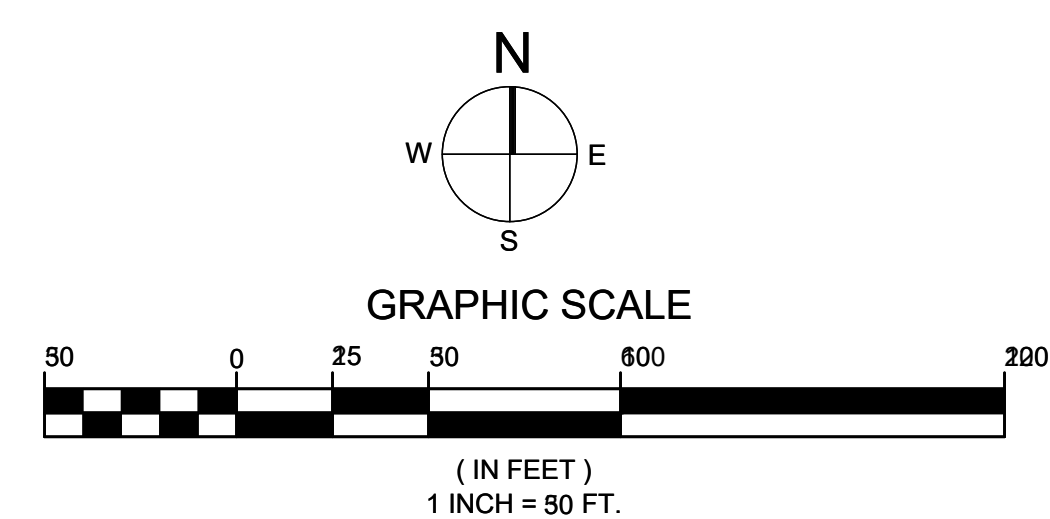
Surveyor's Certificate
I, WILL PLOTNER JR., A NEW MEXICO REGISTERED PROFESSIONAL LAND SURVEYOR DO HEREBY CERTIFY THAT THE TOPOGRAPHIC MAP SHOWN HEREON IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

Will Plotner Jr. 10/21/2021
N.M.R.P.S. No. 14271

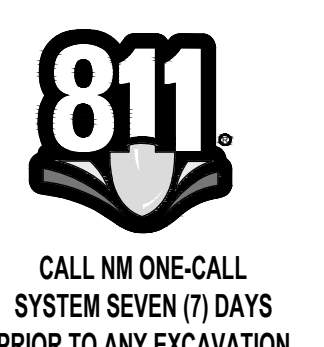
CSI-CARTESIAN SURVEYS INC.
P.O. BOX 44414 RIO RANCHO, N.M. 87174
Phone (505) 896-3050 Fax (505) 891-0244
wplotnerjr@gmail.com



Sheet 1 of 1
1916439



EXISTING CONDITIONS FROM SURVEY PROVIDED BY CSI-CARTESIAN SURVEYS INC.



AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE
4821 PAN AMERICAN FWY. ALBUQUERQUE, NM

SHEET DESCRIPTION:
EXISTING CONDITIONS PLAN

NO	ISSUE/REVISION	DATE
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C00.1

DEMOLITION NOTES

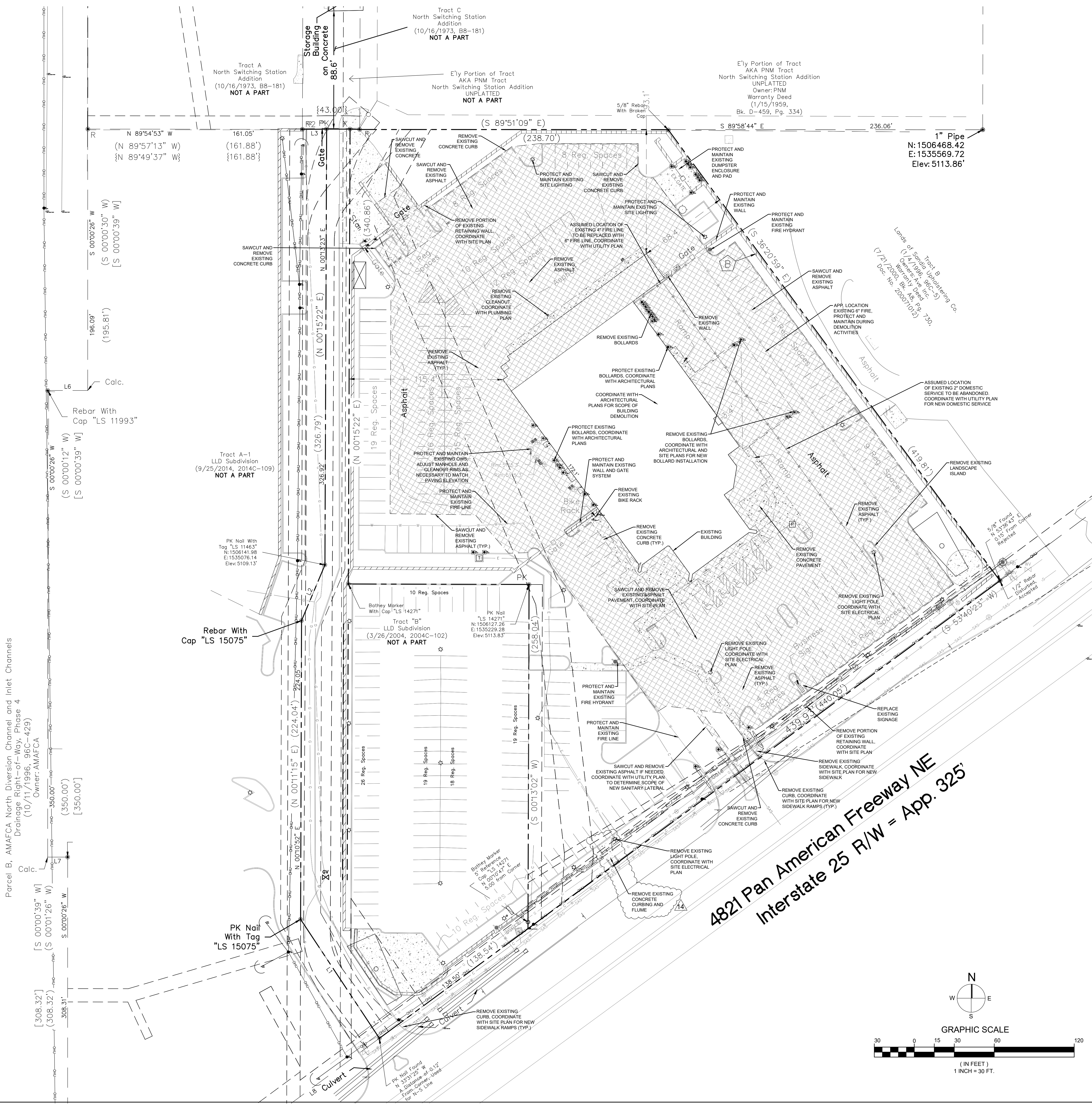
- PLANS MUST BE APPROVED. PRECONSTRUCTION MEETING NEEDS TO BE HELD AND THE PERMIT NEEDS TO BE ISSUED PRIOR TO ANY DEMOLITION ACTIVITIES.
- DEMOLITION IN AREAS INDICATED SHALL CONSIST OF COMPLETE REMOVAL OF EXISTING IMPROVEMENTS.
- ALL EXISTING FEATURES NOTED FOR DEMOLITION OR REMOVAL SHALL BE LEGALLY DISPOSED OF OFF-SITE.
- ELECTRICAL OR GAS UTILITY SERVICES TO BE REMOVED SHALL BE REMOVED AND RELOCATED (AS INDICATED) BY THE UTILITY PROVIDER. CONTRACTOR SHALL SCHEDULE AND COORDINATE THIS WORK WITH THE APPROPRIATE SERVICE PROVIDER. ALL SERVICES SHOULD BE INSTALLED PRIOR TO THE INSTALLATION OF PAVEMENT, SIDEWALKS, CURBS AND GUTTER, OR INSTALLED OTHER PERMANENT FEATURES, PARKING LOT LIGHTING PLAN TO BE COORDINATED WITH UTILITY PROVIDER DURING CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR ANY EXISTING WATER AND SEWER SERVICE RELOCATION. CONTRACTOR TO COORDINATE ANY RELOCATION WITH OWNERS REPRESENTATIVE ON-SITE.
- ANY EXCAVATION MUST BE FILLED IN AND TAMPED AT THE CONCLUSION OF EACH WORK PERIOD, AND EQUIPMENT AND SUPPLIES RETURNED TO THE CONSTRUCTION STAGING AREA.
- ALL MATERIALS AND CONSTRUCTION SHALL REMAIN WITHIN THE BOUNDARIES OF THE CONSTRUCTION FENCING. IF NECESSARY, CONSTRUCTION FENCING WHICH IS REMOVED TO FACILITATE ACCESS BY THE CONTRACTOR FOR CONSTRUCTION MUST BE REPLACED AT THE END OF EACH WORK PERIOD TO DIRECT PEDESTRIAN TRAFFIC AWAY FROM HAZARDOUS AREAS.
- REMOVE EXISTING CONCRETE (WHERE REQUIRED) TO FIRST COLD JOINT OR SAW CUT TO OBTAIN A CLEAN EDGE FOR NEW CONSTRUCTION.
- SAW CUT EXISTING CONCRETE AT LIMITS OF CONSTRUCTION TO OBTAIN A CLEAN EDGE.
- CONTRACTOR SHALL RESTORE THE LAY DOWN AND STAGING AREA TO ORIGINAL CONDITIONS AND TO THE SATISFACTION OF THE OWNER, PRIOR TO DEMOBILIZATION AT THE CONCLUSION OF THE PROJECT.
- DEMOLITION AND SUBSEQUENT CONSTRUCTION OF STORM DRAINAGE FACILITIES SHALL BE PERFORMED IN SUCH A MANNER THAT THE OLD PIPE AND STRUCTURES ARE REMOVED AND NEW STRUCTURES AND PIPING ARE IMMEDIATELY PUT INTO SERVICE. CONTRACTOR SHALL ENSURE THAT STORM DRAINAGE DOES NOT REMAIN OUT OF SERVICE FOR LONGER THAN 12 HOURS AT A TIME. PROVISIONS SHALL BE MADE TO MAINTAIN STORM WATER DRAINAGE DURING CONSTRUCTION.

HATCHED AREAS REPRESENT AREA TO BE DEMOLISHED.

GENERAL NOTES

- ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
- CONTRACTOR SHALL CONTACT "NM ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING ANY EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
- SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
- ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
- FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
- GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
- BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
- BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
- WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
- ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN IDEAL APPROVED LANDFILL.
- CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
- ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
- CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
- ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
- TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROWLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
- ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSSWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A. CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
- CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
- REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.

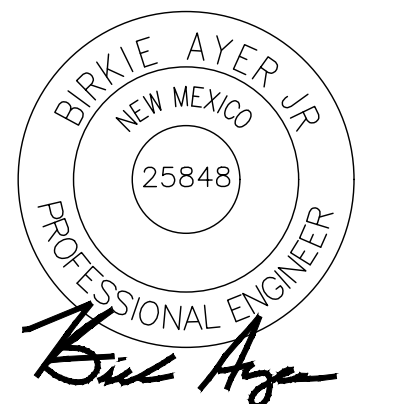
Parcel B, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA



1" Pipe
N: 1506468.42
E: 1535569.72
Elev: 5113.86'

Tract B LLD Subdivision
(10/11/1996, 96C-429)
Owner: PNM
Warranty Deed
(1/15/1959,
Bk. D-459, Pg. 334)

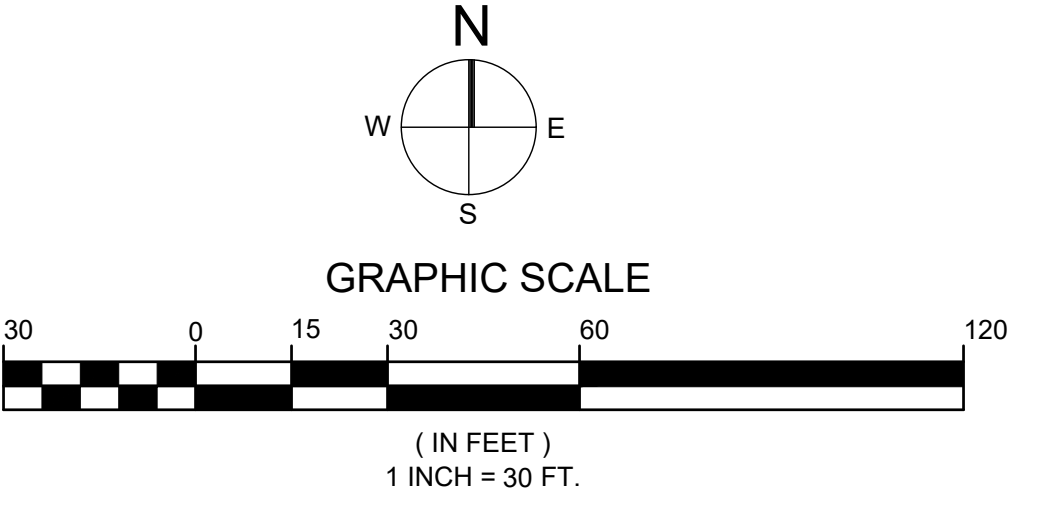
AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE
4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
DEMOLITION PLAN

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/13/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23



C01.0

SITE DESIGN DATA

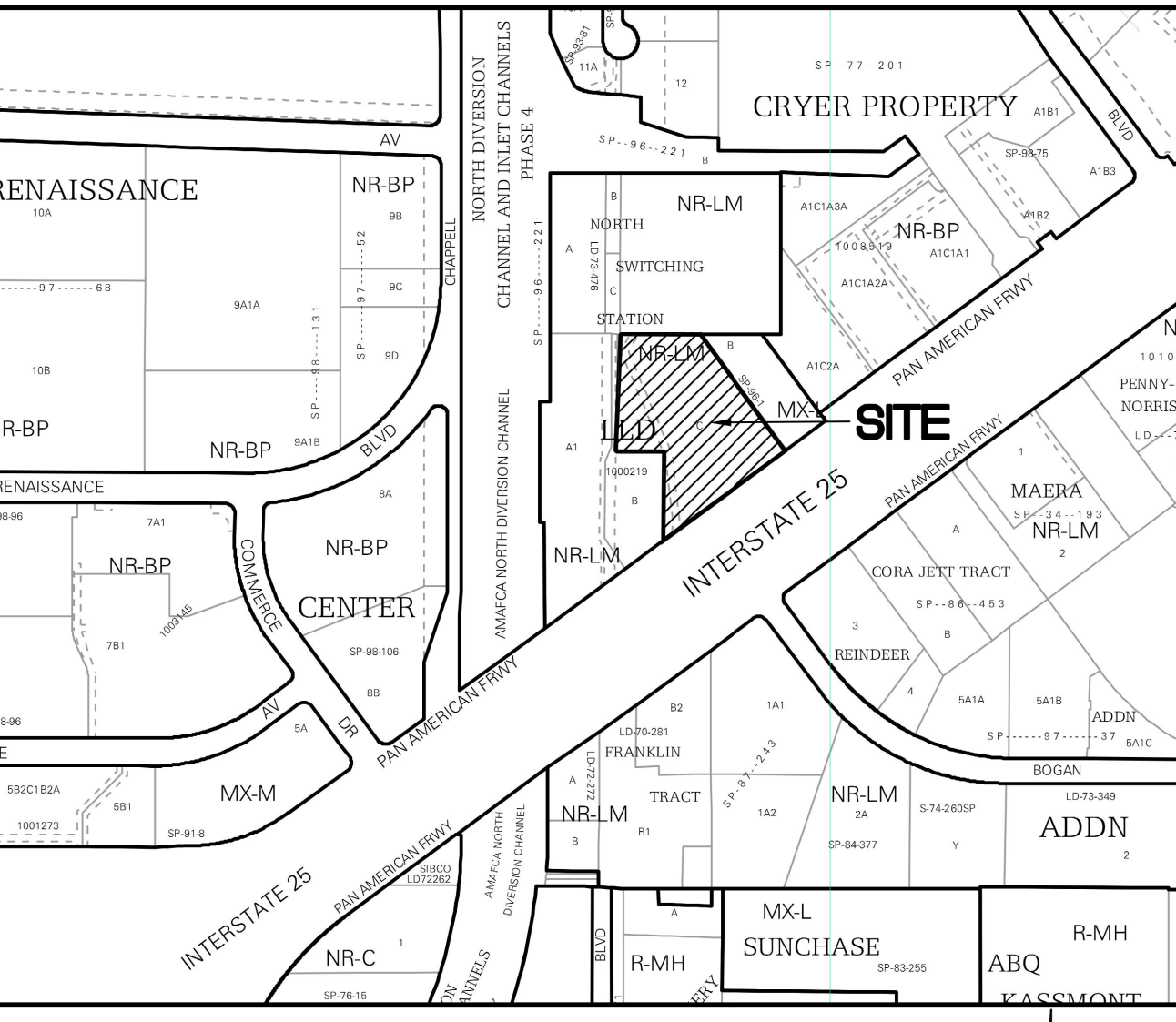
- PROPERTY IS LOCATED AT 4821 PAN AMERICAN FWY.
 - UPCR: 10170610272620107
 - CURRENT ZONING: NR-LM (NON-RESIDENTIAL LIGHT MANUFACTURING)
 - PROPOSED USE: AUTOMOTIVE SALES AND SERVICE
 - MINIMUM YARD & BUILDING SETBACKS PER ZONING ORDINANCE
- 6. PARKING DATA**
- USE: LIGHT VEHICLE SALES
REQUIRED PROVIDED: 2 SPACES PER 1,000 GFA
15,835/1,000 = 15.84 X 2 = 31.27 (32 SPACES REQUIRED)
- USE: LIGHT VEHICLE REPAIR
REQUIRED PROVIDED: 1 SPACE PER 1,000 GFA
19,400/1,000 = 19.4 (20 SPACES REQUIRED)
- REQUIRED PROVIDED: 52 SPACES
58 SPACES PROVIDED (LABELED WITH 'R')
180 SPACES TOTAL, INCLUDING DISPLAY SPACES
- ACCESSIBLE PARKING REQUIRED = 4 SPACES
MOTORCYCLE PARKING: 3 SPACES REQUIRED, 4 SPACES PROVIDED
BICYCLE PARKING: 5 SPACES REQUIRED, 6 SPACES PROVIDED
- MAXIMUM ALLOWABLE BUILDING HEIGHT = 65'
 - SUBJECT PROPERTY IS NOT WITHIN A SPECIAL FLOOD HAZARD AREA AS REFLECTED ON FEMA FLOOD RATE INSURANCE RATE MAP PANEL NO. 30010C138R, DATED EFFECTIVELY 8/16/12.
 - ADEQUATE LIGHTING SHALL BE PROVIDED FOR OFF-STREET PARKING.
 - SITE LIGHTING DESIGN BY OTHERS, SEE ELECTRICAL SITE PLAN. SITE LIGHTING POLES TO BE MAX. HEIGHT OF 30' WITH LED FIXTURES.
 - SIGNAGE WILL BE REVIEWED AND PERMITTED SEPARATELY.
 - SITE LOCATED AT 4821 PAN AMERICAN FWY.
 - INSTALL KNOX BOXES AT EXISTING AND PROPOSED GATES FOR FIRE ACCESS.
 - INSTALL ACCESS KEYPAD AT EXISTING AND PROPOSED GATES FOR REFUSE ACCESS.
 - LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
 - ALL REPAIR ACTIVITIES WILL ONLY BE CONDUCTED WITHIN THE FULLY ENCLOSED PORTION OF THE BUILDING.
 - ALL PARKING SPACES WITHIN 30 FEET OF THE FRONT LOT LINE ARE FOR VEHICLE SALES DISPLAY ONLY.
 - ALL IRRIGATION SYSTEMS SHALL BE DESIGNED TO MINIMIZE THE USE OF WATER.
 - ALL NON-RESIDENTIAL LANDSCAPE IRRIGATION SHALL HAVE AUTOMATIC TIMERS AND/OR PROGRAMMABLE SETTINGS TO AVOID OVERWATERING.
 - THE IRRIGATION SYSTEM SHALL NOT SPRAY OR IRRIGATE IMPERVIOUS SURFACES, INCLUDING SIDEWALKS, DRIVEWAYS, DRIVE AISLES, STREETS, AND PARKING AND LOADING AREAS.

FIRE ONE DATA

- CONSTRUCTION TYPE = II-B
- BUILDING SQUARE FOOTAGE = 38,336 SF
- FIRE FLOW = 1,200 GPM
- BUILDING HEIGHT = 27'-8"
- PROPOSED FIRE APPARATUS ROADS/DRIVES CONSTRUCTED TO SUPPORT IMPOSED LOAD OF FIRE APPARATUS WEIGHING AT LEAST 75,000 LBS.
- PROPOSED PAVEMENT DESIGN OF APPARATUS ROADS TO SUBSTANTIATE WEIGHT BEARING CAPACITY AND ALL WEATHER DRIVING CAPABILITIES FOR FIRE APPARATUS ACCESS ROADS.
- BUILDING TO BE SPRINKLERED
- FIRE APPARATUS ROADS SHALL HAVE AN UNOBSTRUCTED HEIGHT NOT LESS THAN 13 FEET 6 INCHES.

GENERAL NOTES

- ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
- CONTRACTOR SHALL CONTRACT "N1 ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING ANY EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
- SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
- ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
- FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
- GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
- BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
- BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
- WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
- ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN IDEQ APPROVED LANDFILL.
- CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
- ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
- CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
- ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
- CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
- TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROWLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
- ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSOWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A. CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
- CONTRACTOR SHALL MAINTAIN ONE ACCESS DRIVE INTO SITE FROM JEFFERSON STREET NE THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
- CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
- REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.



Vicinity Map

PROJECT NUMBER: PR-2023-008423
Application Number: SI-2023-00606

Is an Infrastructure List required? () Yes () No
If yes, then a set of approved DRC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

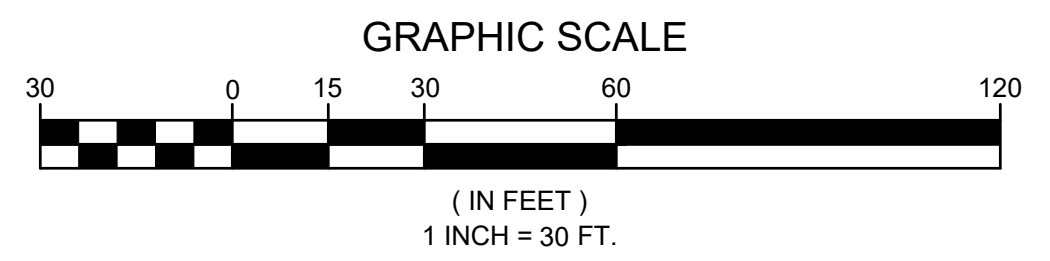
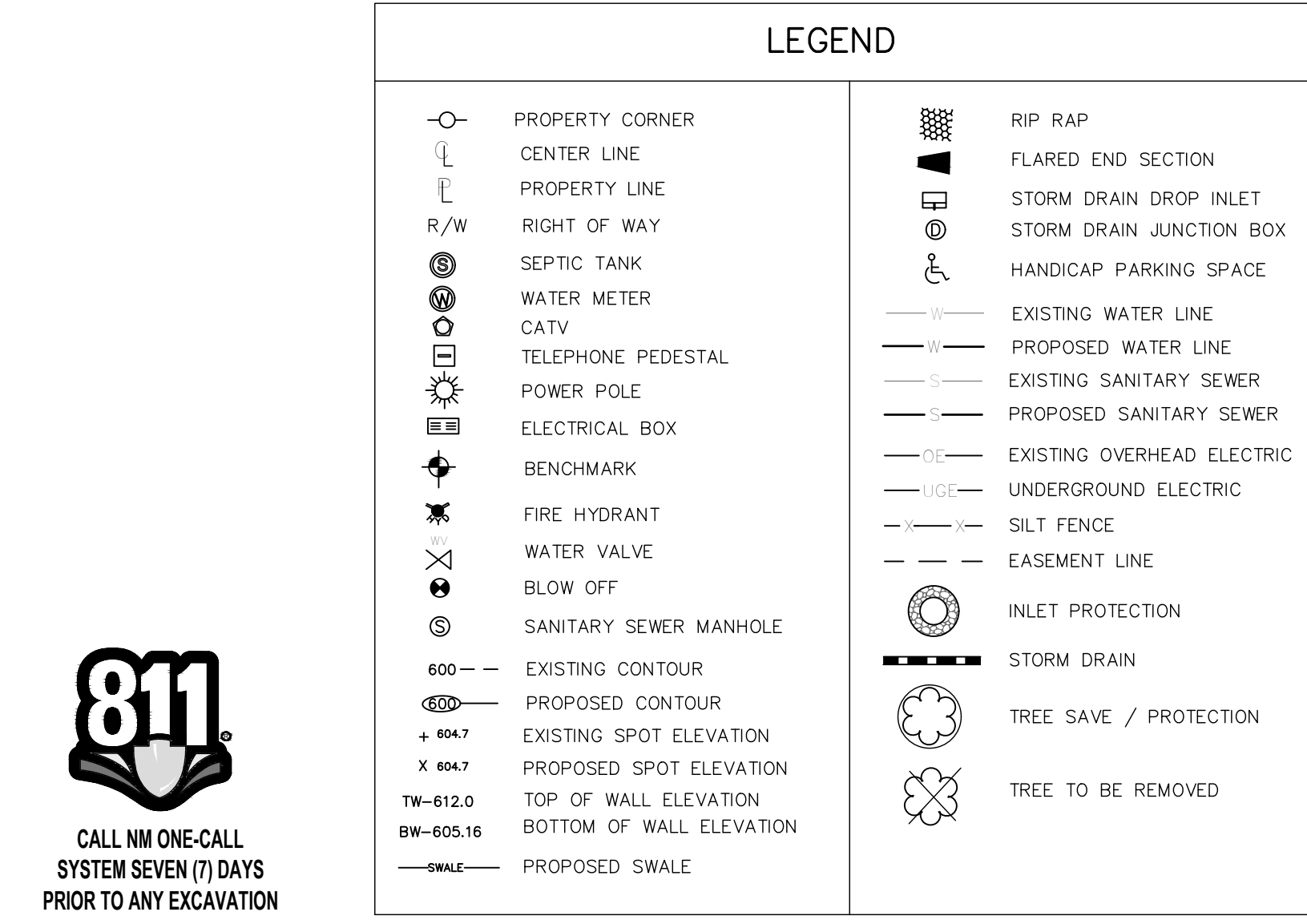
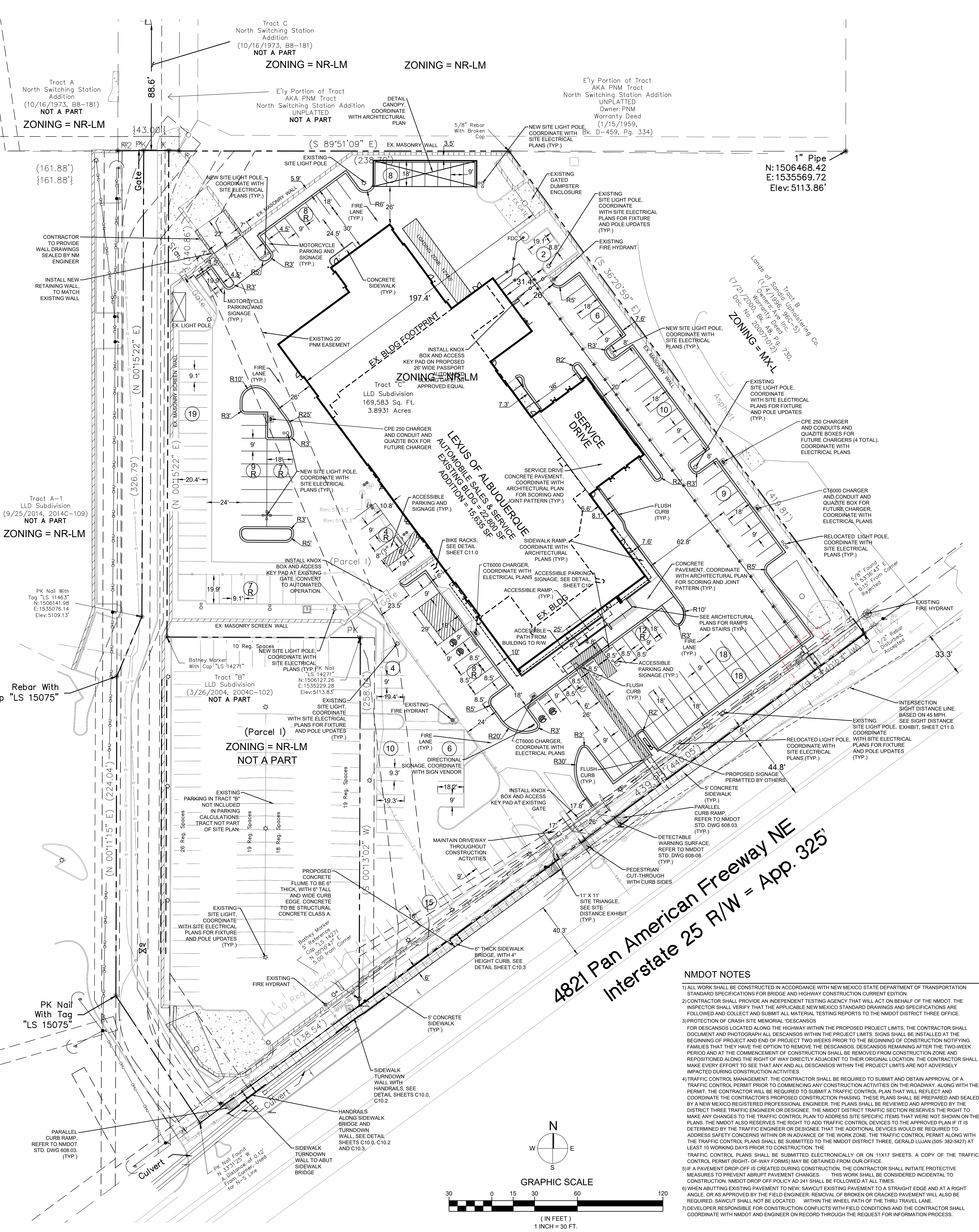
DFT SITE DEVELOPMENT PLAN APPROVAL:

Traffic Engineering, Transportation Division	Date
ABCWVA	Date
Parks and Recreation Department	Date
Hydrology	Date
Code Enforcement	Date
* Environmental Health Department (conditional)	Date
Solid Waste Management	Date
Planning Department	Date

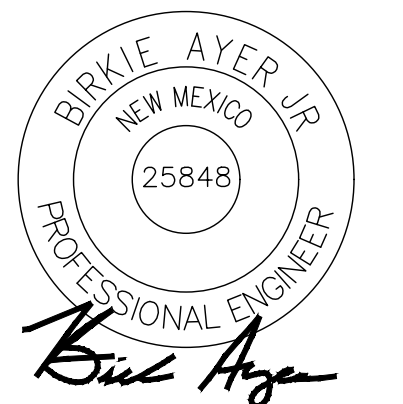
*Environmental Health, if necessary
12/16/22

EV CHARGER NOTES

- EV CHARGERS TO BE INSTALLED IN CONCRETE MOUNTING PAD PER MANUFACTURERS SPECIFICATIONS AND DETAILS.
- COORDINATE EV CHARGER INSTALLATION WITH ARCHITECTURAL AND ELECTRICAL PLANS.
- CONTRACTOR SHALL COORDINATE ALL UNDERGROUND CONDUIT ROUTING AND CONCRETE MOUNTING PAD LOCATIONS.
- CONTRACTOR TO ENSURE CONCRETE MOUNTING PADS ARE INSTALLED FLUSH AND MATCH ADJACENT SIDEWALK AND/OR CURB ELEVATIONS.
- CONTRACTOR TO INSTALL PAINTED PARKING SPACE EV CHARGER SYMBOLS PER PLAN. PROVIDE SUBMITTALS TO ENGINEER AND ARCHITECT FOR REVIEW.



AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnson Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME: 5-24-23

LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
SITE PLAN

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/13/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

NMDOT NOTES

- ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH NEW MEXICO STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR BRIDGE AND HIGHWAY CONSTRUCTION CURRENT EDITION.
- CONTRACTOR SHALL PROVIDE AN INDEPENDENT TESTING AGENCY THAT WILL ACT ON BEHALF OF THE NMDOT. THE INSPECTOR SHALL VERIFY THAT THE APPLICABLE NEW MEXICO STANDARD DRAWINGS AND SPECIFICATIONS ARE FOLLOWED AND COLLECT AND SUBMIT ALL MATERIAL TESTING REPORTS TO THE NMDOT DISTRICT THREE OFFICE.
- PROTECTION OF CRASH SITE MEMORIAL (DESCANSOS)
FOR DESCANSOS LOCATED ALONG THE HIGHWAY WITHIN THE PROPOSED PROJECT LIMITS, THE CONTRACTOR SHALL DOCUMENT AND PHOTOGRAPH ALL DESCANSOS WITHIN THE PROJECT LIMITS. SIGNS SHALL BE INSTALLED AT THE BEGINNING OF PROJECT AND END OF PROJECT TWO WEEKS PRIOR TO THE BEGINNING OF CONSTRUCTION NOTIFYING FAMILIES THAT THEY HAVE THE OPTION TO REMOVE THE DESCANSOS. DESCANSOS REMAINING AFTER THE TWO-WEEK PERIOD AND AT THE COMMENCEMENT OF CONSTRUCTION FROM CONSTRUCTION ZONE AND REPOSITIONED ALONG THE RIGHT OF WAY DIRECTLY ADJACENT TO THEIR ORIGINAL LOCATION. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO SEE THAT ANY AND ALL DESCANSOS WITHIN THE PROJECT LIMITS ARE NOT ADVERSELY IMPACTED DURING CONSTRUCTION ACTIVITIES.
- TRAFFIC CONTROL MANAGEMENT. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT AND OBTAIN APPROVAL OF A TRAFFIC CONTROL PLAN PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES ON THE ROADWAY. ALONG WITH THE PERMIT, THE CONTRACTOR WILL BE REQUIRED TO SUBMIT A TRAFFIC CONTROL PLAN THAT WILL REFLECT AND COORDINATE THE CONTRACTOR'S PROPOSED CONSTRUCTION PHASING. THESE PLANS SHALL BE PREPARED AND SEALED BY A NEW MEXICO REGISTERED PROFESSIONAL ENGINEER. THE PLANS SHALL BE REVIEWED AND APPROVED BY THE DISTRICT THREE TRAFFIC ENGINEER OR DESIGNER. THE NMDOT DISTRICT TRAFFIC SECTION RESERVES THE RIGHT TO MAKE ANY CHANGES TO THE TRAFFIC CONTROL PLAN TO ADDRESS SITE SPECIFIC ITEMS THAT WERE NOT SHOWN ON THE PLANS. THE NMDOT ALSO RESERVES THE RIGHT TO ADD TRAFFIC CONTROL DEVICES TO THE APPROVED PLAN IF IT IS DETERMINED BY THE TRAFFIC ENGINEER OR DESIGNER THAT THE ADDITIONAL DEVICES WOULD BE REQUIRED TO ADDRESS SAFETY CONCERNS WITHIN OR IN ADVANCE OF THE WORK ZONE. THE TRAFFIC CONTROL PERMIT ALONG WITH THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED TO THE NMDOT DISTRICT THREE, GERALD LUJAN (505-382-5427) AT LEAST 10 WORKING DAYS PRIOR TO CONSTRUCTION. THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED ELECTRONICALLY OR ON 11X17 SHEETS. A COPY OF THE TRAFFIC CONTROL PERMIT (RIGHT-OF-WAY FORMS) MAY BE OBTAINED FROM OUR OFFICE.
5) IF A PAVEMENT DROP-OFF IS CREATED DURING CONSTRUCTION, THE CONTRACTOR SHALL INITIATE PROTECTIVE MEASURES TO PREVENT ABRUPT PAVEMENT CHANGES. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION. NMDOT DROP-OFF POLICY 201 SHALL BE FOLLOWED AT ALL TIMES.
6) WHEN ABUTTING EXISTING PAVEMENT TO NEW SAWCUT PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER, REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED. SAWCUT SHALL NOT BE LOCATED WITHIN THE WHEEL PATH OF THE THRU TRAVEL LANES.
7) DEVELOPER RESPONSIBLE FOR CONSTRUCTION CONFLICTS WITH FIELD CONDITIONS AND THE CONTRACTOR SHALL COORDINATE WITH NMDOT AND ENGINEER ON RECORD THROUGH THE REQUEST FOR INFORMATION PROCESS.

C02.0

SITE DESIGN DATA

1. PROPERTY IS LOCATED AT 4821 PAN AMERICAN FWY.
2. UPCA: 10170810272820107
3. CURRENT ZONING: NR-LM (NON-RESIDENTIAL LIGHT MANUFACTURING)
4. PROPOSED USE: AUTOMOTIVE SALES AND SERVICE
5. MINIMUM YARD & BUILDING SETBACKS PER ZONING ORDINANCE

FRONT/STREET SIDE YARD	5 FT
REAR YARD	0 FT
SIDE YARD	0 FT

USE: LIGHT VEHICLE SALES	2 SPACES PER 1,000 GFA
REQUIRED	15,635/1,000 = 15.64 X 2 = 31.27 (32 SPACES REQUIRED)
PROVIDED	

USE: LIGHT VEHICLE REPAIR	1 SPACE PER 1,000 GFA
REQUIRED	19,400/1,000 = 19.4 (20 SPACES REQUIRED)
PROVIDED	
REQUIRED	52 SPACES
PROVIDED	55 SPACES PROVIDED (LABELLED WITH "R")
	180 SPACES TOTAL, INCLUDING DISPLAY SPACES

6. ACCESSIBLE PARKING REQUIRED = 4 SPACES
7. MOTORCYCLE PARKING, 3 SPACES REQUIRED, 4 SPACES PROVIDED
8. BICYCLE PARKING, 6 SPACES REQUIRED, 6 SPACES PROVIDED
9. MAXIMUM ALLOWABLE BUILDING HEIGHT = 65'
10. SUBJECT PROPERTY IS NOT WITHIN A SPECIAL FLOOD HAZARD AREA AS REFLECTED ON FEMA FLOOD RATE INSURANCE RATE MAP PANEL NO. 35001C0138H, DATED EFFECTIVELY 8/16/12.
11. ADEQUATE LIGHTING SHALL BE PROVIDED FOR OFF-STREET PARKING.
12. SITE LIGHTING DESIGN BY OTHERS, SEE ELECTRICAL SITE PLAN
13. SITE LIGHTING POLES TO BE MAX. HEIGHT OF 30' WITH LED FIXTURES.
14. SIGNAGE WILL BE REVIEWED AND PERMITTED SEPARATELY.
15. SIGNAGE WILL BE REVIEWED AND PERMITTED SEPARATELY.
16. SITE IS LOCATED AT 4821 PAN AMERICAN FWY.
17. INSTALL KNOX BOXES AT EXISTING AND PROPOSED GATES FOR FIRE ACCESS
18. INSTALL ACCESS KEYPAD AT EXISTING AND PROPOSED GATES FOR REFUSE ACCESS
19. LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
20. ALL REPAIR ACTIVITIES WILL ONLY BE CONDUCTED WITHIN THE FULLY ENCLOSED PORTION OF THE BUILDING.
21. ALL PARKING SPACES WITHIN 30 FEET OF THE FRONT LOT LINE ARE FOR VEHICLE SALES DISPLAY ONLY.

FIRE ONE DATA

1. CONSTRUCTION TYPE = II-B
2. BUILDING SQUARE FOOTAGE = 38,336 SF
3. FIRE FLOW = 1,200 GPM
4. BUILDING HEIGHT = 27' - 8"
5. PROPOSED FIRE APPARATUS ROADS/DRIVES CONSTRUCTED TO SUPPORT IMPOSED LOAD OF FIRE APPARATUS WEIGHING AT LEAST 75,000 LBS.
6. PROPOSED PAVEMENT DESIGN OF APPARATUS ROADS TO SUBSTANTIATE WEIGHT BEARING CAPACITY AND ALL WEATHER DRIVING CAPABILITIES FOR FIRE APPARATUS ACCESS ROADS.
7. BUILDING TO BE SPRINKLERED
8. FIRE APPARATUS ROADS SHALL HAVE AN UNOBSTRUCTED HEIGHT NOT LESS THAN 13 FEET 6 INCHES.

GENERAL NOTES

1. ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
2. CONTRACTOR SHALL CONTACT "NM ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING ANY EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
3. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
4. CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
5. SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
6. ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
7. FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
8. GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
9. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
10. BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
11. BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
12. WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
13. ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN IDEQ APPROVED LANDFILL.
14. CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
15. ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
16. CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
17. CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
18. ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
19. CONTRACTOR SHALL SUPPLY AS BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
20. TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROWLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
21. ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSSWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A., CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
22. CONTRACTOR SHALL MAINTAIN ONE ACCESS DRIVE INTO SITE FROM JEFFERSON STREET NE THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
23. CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
24. REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.

EV CHARGER NOTES

1. EV CHARGERS TO BE INSTALLED IN CONCRETE MOUNTING PAD PER MANUFACTURERS SPECIFICATIONS AND DETAILS.
2. COORDINATE EV CHARGER INSTALLATION WITH ARCHITECTURAL AND ELECTRICAL PLANS.
3. CONTRACTOR SHALL COORDINATE ALL UNDERGROUND CONDUIT ROUTING AND CONCRETE MOUNTING PAD LOCATIONS.
4. CONTRACTOR TO ENSURE CONCRETE MOUNTING PADS ARE INSTALLED FLUSH AND MATCH ADJACENT SIDEWALK AND/OR CURB ELEVATIONS.
5. CONTRACTOR TO INSTALL PAINTED PARKING SPACE EV CHARGER SYMBOLS PER PLAN. PROVIDE SUBMITTALS TO ENGINEER AND ARCHITECT FOR REVIEW.

PR-2023-008423



LEGEND

	PROPERTY CORNER		RIP RAP
	CENTER LINE		FLARED END SECTION
	PROPERTY LINE		STORM DRAIN DROP INLET
	RIGHT OF WAY		STORM DRAIN JUNCTION BOX
	SEPTIC TANK		HANDICAP PARKING SPACE
	WATER METER		EXISTING WATER LINE
	CATV		PROPOSED WATER LINE
	TELEPHONE PEDESTAL		EXISTING SANITARY SEWER
	POWER POLE		PROPOSED SANITARY SEWER
	ELECTRICAL BOX		EXISTING OVERHEAD ELECTRIC
	BENCHMARK		UNDERGROUND ELECTRIC
	FIRE HYDRANT		SILT FENCE
	WATER VALVE		EASEMENT LINE
	BLOW OFF		INLET PROTECTION
	SANITARY SEWER MANHOLE		STORM DRAIN
	EXISTING CONTOUR		TREE SAVE / PROTECTION
	PROPOSED CONTOUR		TREE TO BE REMOVED
	EXISTING SPOT ELEVATION		
	PROPOSED SPOT ELEVATION		
	TOP OF WALL ELEVATION		
	BOTTOM OF WALL ELEVATION		
	PROPOSED SWALE		

PAVING LEGEND

	LIGHT DUTY ASPHALT PAVEMENT. SEE PAVEMENT DETAIL, SHEET C10.0
	HEAVY DUTY ASPHALT PAVEMENT. SEE PAVEMENT DETAIL, SHEET C10.0
	CONCRETE PLAZA-DISPLAY AREA, TO MATCH BUILDING SLAB DESIGN 5" CONCRETE, 3,000 PSI - W.W.F. 6 X 6, W2.9 X W2.9 - FLAT SHEETS
	CONCRETE SIDEWALK. SEE SIDEWALK DETAIL, SHEET C10.0 & C10.2.
	EV CHARGER CONCRETE MOUNTING PAD - MEET EV CHARGER MANUFACTURERS SPECIFICATIONS AND DETAILS.



GRAPHIC SCALE



Parcel B, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel C, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel D, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel E, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

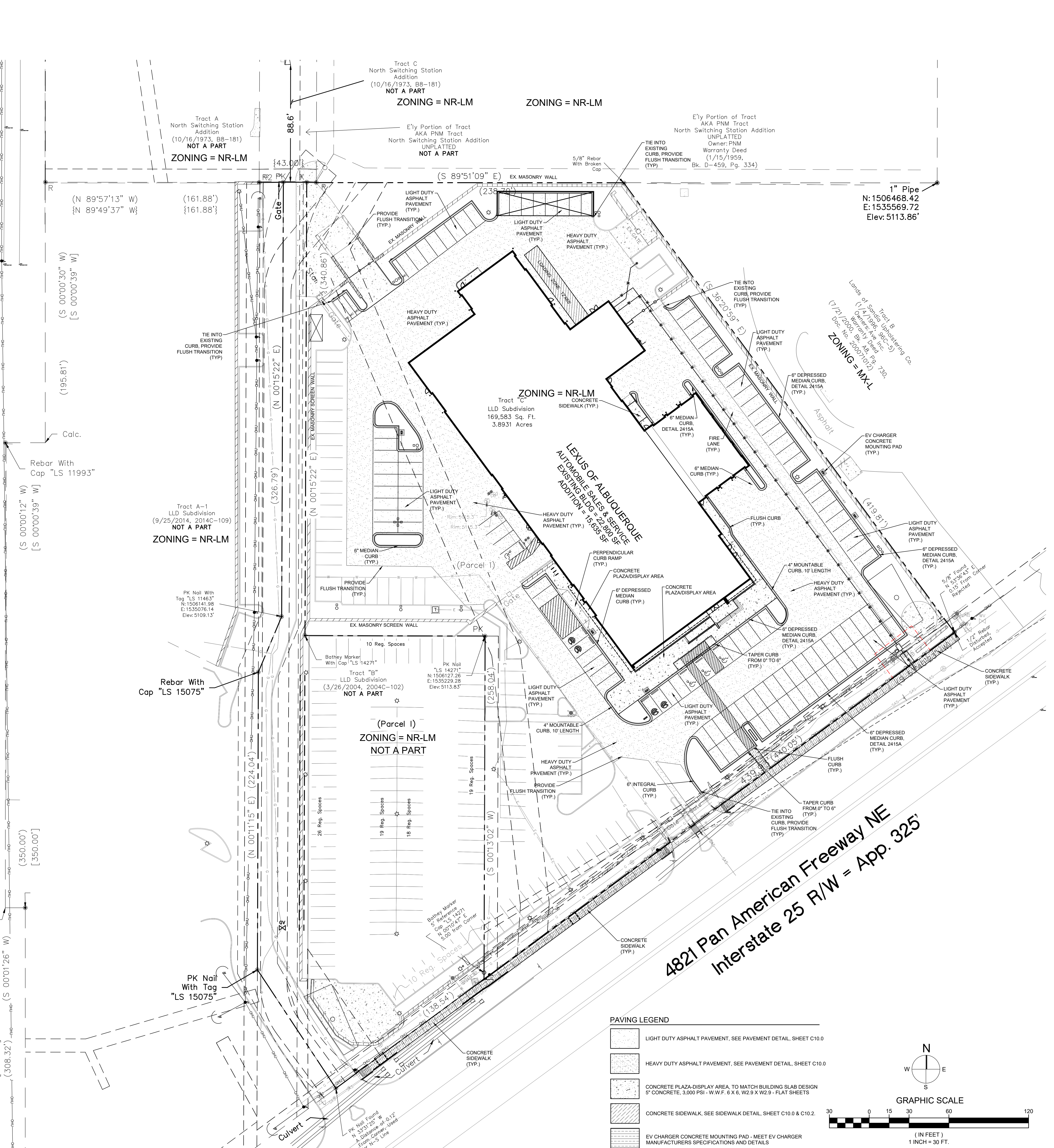
Parcel F, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel G, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel H, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel I, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA

Parcel J, AMAFCA North Diversion Channel and Inlet Channels
Drainage Right-of-Way, Phase 4
(10/11/1996, 96C-429)
Owner: AMAFCA



AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858

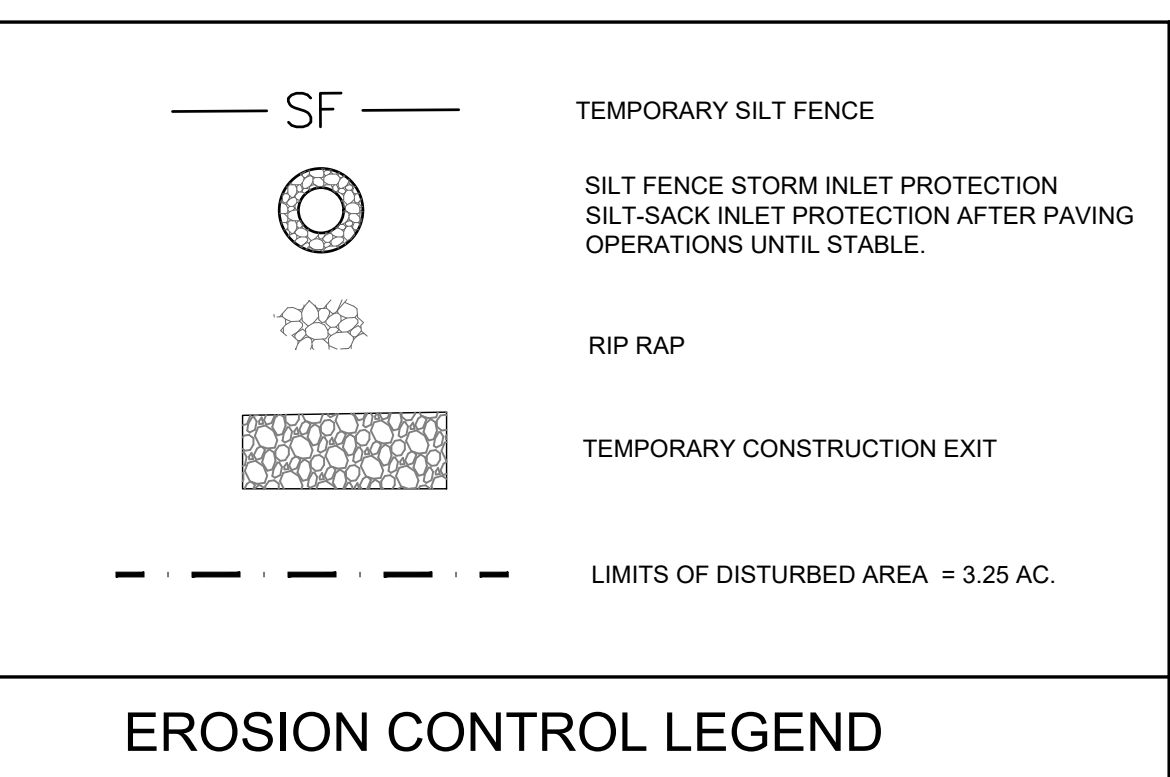


PROJECT NAME:
LEXUS OF ALBUQUERQUE
4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
PAVING PLAN

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/13/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C02.1



SWPPP PHASING INFORMATION

THE LOCATION OF THE LIMITS OF DISTURBANCE AND THE CORRESPONDING BMPs NEED TO BE PLANNED AHEAD OF TIME AND SHOWN ACCURATELY ON THE SWPPP MAPS FOR EACH PHASE. THE STAGING AREAS SHOULD BE INCLUDED INSIDE THE SILT FENCE DURING EACH PHASE.

EROSION CONTROL LEGEND

BEST MANAGEMENT PRACTICES

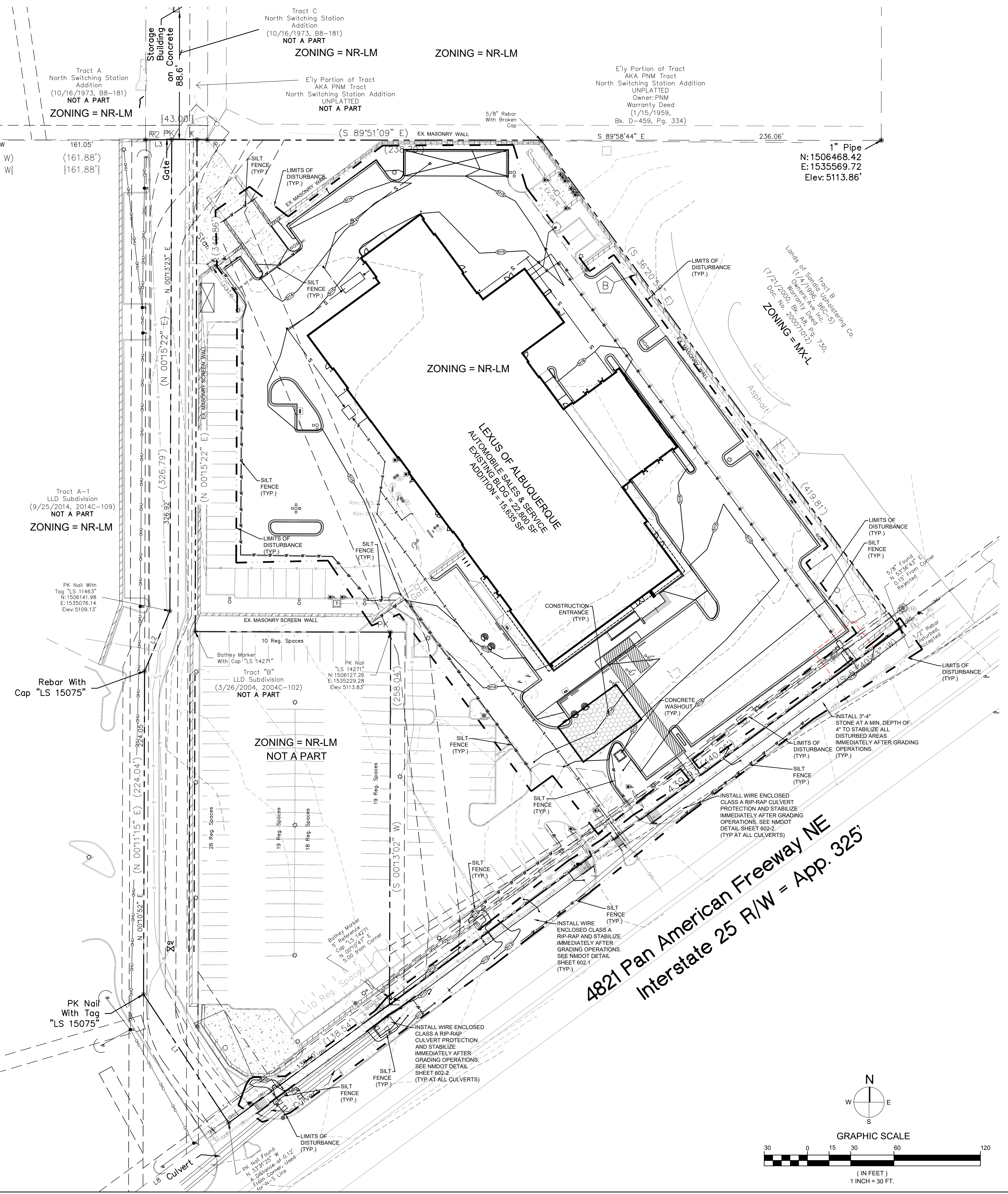
- STRUCTURAL MEASURES SHALL BE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT IN EFFECTIVE OPERATING CONDITION.
- DOCUMENTATION OF MAINTENANCE ACTIVITIES INCLUDING FREQUENCY, LOT DESIGNATION, INSPECTION OF STRUCTURAL CONTROLS, MATERIAL STORAGE AREAS, VEHICLES ENTRANCE AND EXITS, ACTIONS TAKEN AND INSPECTORS NAME.
- CONSTRUCTION SITE NOTICE WILL BE MAINTAINED ON SITE. YOU MUST POST A SIGN OR OTHER NOTICE OF THE PERMIT COVERAGE AT A SAFE, PUBLICLY ACCESSIBLE LOCATION IN CLOSE PROXIMITY TO THE CONSTRUCTION SITE. THE NOTICE MUST BE LOCATED SO IT IS VISIBLE FROM THE PUBLIC ROAD THAT IS NEAREST TO THE ACTIVE PART OF THE CONSTRUCTION SITE, AND IT MUST USE A FONT LARGE ENOUGH TO BE READILY VIEWED FROM A PUBLIC RIGHT-OF-WAY, CGP 15.
- COPY OF SWPPP SHALL BE KEPT ON SITE. SWPPP MUST BE KEPT UP-TO-DATE AND THE LOCATION OF THE LIMITS OF DISTURBANCE AND THE CORRESPONDING BMPs NEEDS TO BE PLANNED AHEAD OF TIME AND SHOWN ACCURATELY ON THE SWPPP MAPS FOR EACH PHASE. THE STAGING AREA SHOULD BE INCLUDED INSIDE THE SILT FENCE DURING EACH PHASE. CGP 7.1.
- PERMITEE MUST RETAIN THE SWPPPS NOI AND INSPECTION LOG FOR A MINIMUM OF 3 YEARS FROM THE TERMINATION AND FINAL STABILIZATION OF THE PROJECT.
- WASTE MATERIALS
 - ALL WASTE MATERIALS INCLUDING CONSTRUCTION DEBRIS, SHALL BE COLLECTED AND STORED IN A SECURED LOADED METAL DUMPSTER. NO CONSTRUCTION MATERIAL SHALL BE BURIED ON SITE. THE DUMPSTER SHALL BE EMPTIED AS NECESSARY OR AS REQUIRED BY ORDINANCE 9.04 (SOLID WASTE MANAGEMENT) AND THE TRASH HAULED TO A LICENSED LANDFILL.
 - HAZARDOUS WASTE
 - AT A MINIMUM, ANY PRODUCTS IN THE FOLLOWING CATEGORIES SHALL BE CONSIDERED HAZARDOUS: PAINT, ACIDS FOR CLEANING MASONRY SURFACES, CLEANING SOLVENTS, ASPHALT PRODUCTS, CHEMICAL ADDITIVES FOR SPILL STABILIZATION AND CURING COMPOUNDS AND ADDITIVES. IN EVENT OF SPILL WHICH MAY BE HAZARDOUS, THE CONTRACTOR SHALL TAKE IMMEDIATE ACTION AND CONTACT THE FIRED DEPARTMENT AND NMD.
- SANITARY WATER
 - ALL SANITARY WASTE SHALL BE COLLECTED FROM THE CONSTRUCTION PORTABLE UNITS AS NECESSARY BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR. ALL WASTE MATERIAL SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- SPILL PREVENTION
 - THE FOLLOWING PRACTICES SHALL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURES OF MATERIALS TO STORM WATER RUNOFF.
- GOOD HOUSEKEEPING
 - A. STORE ONLY ENOUGH PRODUCTS REQUIRED TO DO THE JOB.
 - B. NEATLY STORE MATERIAL ON-SITE IN A SECURED MANNER.
 - C. KEEP PRODUCTS IN THEIR ORIGINAL CONTAINER.
 - D. DO NOT MIX SUBSTANCES WITH ANOTHER, UNLESS OTHERWISE RECOMMENDED BY THE MANUFACTURER.
 - E. USE ENTIRE CONTENTS OF A PRODUCT BEFORE DISPOSING OF THE CONTAINER.
 - F. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR PROPER USE AND DISPOSAL.
- HAZARDOUS CONDITIONS (PRACTICES TO REDUCE RISK)
 - A. KEEP PRODUCTS IN THEIR ORIGINAL CONTAINER IF AT ALL POSSIBLE.
 - B. RETAIN ORIGINAL LABELS, PRODUCT INFORMATION AND MATERIAL SAFETY DATA SHEETS (MSDS).
 - C. DISPOSE OF SURPLUS PRODUCT IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- PETROLEUM PRODUCTS
 - PRODUCTS SHALL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED. ANY ASPHALT SUBSTANCES USED ON-SITE SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
- SPILL CONTROL PRACTICES
 - A. MANUFACTURERS RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED AND SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURE.
 - B. MATERIALS AND EQUIPMENT NECESSARY FOR SPILL CLEANUP SHALL BE KEPT IN THE MATERIAL STORAGE AREA ON-SITE.
 - C. ALL SPILLS SHALL BE CLEANED UP IMMEDIATELY AFTER DISCOVERY.
 - D. SPILL AREA SHALL BE WELL VENTILATED AND APPROPRIATE CLOTHING WILL BE WORN.
 - E. ANY SPILL SHALL BE REPORTED TO THE APPROPRIATE GOVERNMENTAL AGENCY.
 - F. MEASURES SHALL BE TAKEN TO PREVENT A SPILL FROM REOCCURRING.
- MAINTENANCE AND INSPECTION PROCEDURES
 - ALL POLLUTION PREVENTION MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY 14 DAYS AND FOLLOWING A STORM EVENT OR 1/25 INCHES OR MORE RAINFALL. MAINTENANCE PROCEDURES AND POLLUTION CONTROL PROCEDURES SHALL BE INSPECTED FOR ADEQUACY. A RECORD OF THE RESULTS OF THE INSPECTION OF THE SITE SHALL BE KEPT ON SITE.
- REMARKS
 - DISPOSAL AREAS, STOCKPILES, AND HAUL ROADS SHALL BE CONSTRUCTED IN A MANNER THAT WILL MINIMIZE AND CONTROL THE AMOUNT OF SEDIMENT THAT MAY ENTER RECEIVING WATERS. CONSTRUCTION STAGING

EROSION CONTROL NOTES

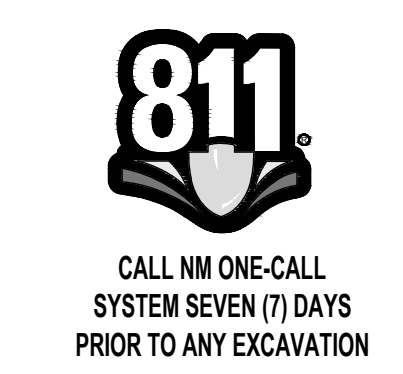
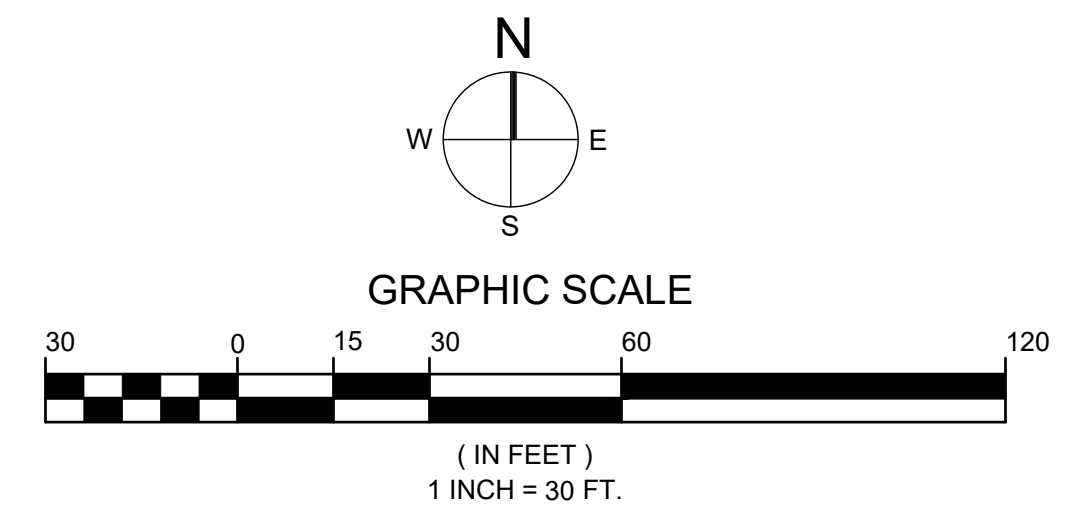
- IF NECESSARY, SLOPES WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS, IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINAGE DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED DAILY UNTIL THE SLOPE IS BROUGHT TO GRADE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
 - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS, STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED EVERY FOURTEEN (14) DAYS & WITHIN 24 HRS AFTER EACH RAINFALL OCCURRENCE THAT EXCEEDS ONE-QUARTER (0.25) INCH. IF SITE INSPECTIONS IDENTIFY BERMS THAT ARE DAMAGING OR NOT OPERATING EFFECTIVELY, MAINTENANCE MUST BE PERFORMED AS SOON AS PRACTICABLE OR AS REASONABLY POSSIBLE AND BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION, FILL COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE ANY SEDIMENTS BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- WHERE SEDIMENT HAS BEEN TRACKED-OUT FROM THE SITE ON PAVED ROADS AND SIDEWALKS, REMOVE THE DEPOSITED SEDIMENT BY THE END OF THE SAME BUSINESS DAY, CGP 2.4.f.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE PLACED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CANNOT BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
- DIRECT WASH WATER FROM CONCRETE, PAINT, AND STUCCO INTO A LEAK-PROOF CONTAINER, CGP 2.3.4.
- ALL CHEMICAL SPILLS, OIL SPILLS, OR FISH KILLS MUST BE REPORTED TO NEW MEXICO ENVIRONMENTAL DEPARTMENT AT 505-827-8929.
- TEMPORARY TOILET FACILITIES SHALL BE PROVIDED FOR ALL CONSTRUCTION WORKERS AND SITE VISITORS IN ACCORDANCE WITH 2009 INTERNATIONAL PLUMBING CODE GENERAL REGULATIONS, SECTION 311. PORTABLE FACILITIES SHALL BE PLACED ON LEVEL GROUND AND AWAY FROM STORM DRAINAGE SYSTEMS (DITCHES, CATCH BASINS, ETC.) DISPOSAL AND HANDLING OF SANITARY WASTE MUST COMPLY WITH ODEG REQUIREMENTS.
- FINAL GRADES FOR GRASSED AND LANDSCAPED AREAS SHALL REQUIRE A MINIMUM OF 4"-6" OF CLEAN TOP SOIL, FREE OF DEBRIS AND CONTAMINANTS, AND PREFERABLY OF NATIVE ORIGIN.
- CONTRACTOR SHALL COMPLY WITH THE CONSTRUCTION STORM WATER GENERAL PERMIT, INCLUDING PREPARATION OF A SWPPP AND NOI. CONTRACTOR TO PROVIDE A COPY OF SWPPP, THE SITE NOTICE, AND NOI TO CITY OF ALBUQUERQUE AND KEEP ON SITE UNTIL CONSTRUCTION COMPLETION.

CITY OF ALBUQUERQUE ESC PLAN STANDARD NOTES

- All Erosion and Sediment Control (ESC) work on these plans, except as otherwise stated or provided hereon shall be permitted, constructed, inspected, and maintained in accordance with:
 - a. The City Ordinance § 14-5-2-11, the ESC Ordinance,
 - b. The EPA's 2022 Construction General Permit (CGP), and
 - c. The City of Albuquerque Construction BMP Manual.
- All BMP's must be installed prior to beginning any earth moving activities except as specified hereon in the Phasing Plan. Construction of earthen BMP's such as sediment traps, sediment basins, and diversion berms shall be completed and inspected prior to any other construction or earthwork. Self-inspection is required after installation of the BMP's and prior to beginning construction.
- Self-inspectors - At a minimum a routine compliance self-inspection is required to review the project for compliance with the Construction General Permit once every 14 days and after any precipitation event of 1/4 inch or greater until the site construction has been completed and the site determined as stabilized by the city. Reports of these inspections shall be kept by the person or entity authorized to direct the construction activities on the site and made available upon request.
- Corrective action reports must be kept by the person or entity authorized to direct the construction activities on the site and made available upon request.
- Stabilization reports must be kept by the person or entity authorized to direct the construction activities on the site and made available upon request. Reports should include records of weed removal per City Ordinance (§ 9-8-1), sterilization, soil test results and recommendation, materials and manufacturer's specifications for application rates, estimated functional longevity, methods of application, inspection and maintenance. The reduced self-inspection schedule in CGP 4.4.1 applies to stabilized area and any damaged or worn stabilization must be identified in the reports along with weed problems. Corrective actions for stabilization shall be documented in a stabilization report including actual rates and dates of stabilization, and the materials and manufacturer's specifications used.
- BMP's shall be inspected and maintained until all disturbed areas are stabilized in accordance with the Final Stabilization Criteria (CGP 2.2.14.b). Generally, all disturbed areas, other than structures and impervious surfaces, must have uniform perennial vegetation that provides 70 percent or more of the cover provided by native vegetation or seed the disturbed area and provide non-vegetative mulch that provides cover for at least three years without active maintenance. Final stabilization must be approved by the City of Albuquerque prior to removal of BMP's and discontinuation of inspections.



**4821 Pan American Freeway NE
Interstate 25 R/W = App. 325'**



AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE
4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
EROSION CONTROL PLAN

NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/13/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C03.0

GRADING NOTES

- PROPOSED PROJECT WILL REDUCE EXISTING IMPERVIOUS COVER BY APP. 1,606 SF.
 - CONCRETE TRUCKS SHALL NOT TYPICALLY BE WASHED OUT ON SITE. SHOULD CONCRETE TRUCK WASHOUT BE PERMITTED ON SITE, COORDINATE LOCATION AND BMPs WITH SITE INSPECTOR.
 - A. DO NOT DISPOSE OF CONCRETE TRUCK WASHOUT WASTE BY DUMPING INTO A SANITARY SEWER, STORM DRAIN OR ONTO SOIL OR PAVEMENT THAT CARRIES STORM WATER RUNOFF.
 - B. THE WASHOUT FROM CONCRETE TRUCKS SHOULD BE DISPOSED OF INTO:
 - A DESIGNATED AREA THAT WILL LATER BE BACKFILLED (SLURRY PIT)
 - AN AREA WHERE THE CONCRETE WASH CAN HARDEN AND THEN BE DISPOSED OF AS SOLID WASTE
 - A LOCATION WHICH IS NOT SUBJECT TO WATER RUNOFF, AND MORE THAN 50 FEET AWAY FROM A STORM DRAIN, OPEN DITCH OR RECEIVING WATER WAY.
 - C. PUMP EXCESS CONCRETE IN CONCRETE PUMP BACK INTO CONCRETE MIXER TRUCK.
 - D. CONCRETE WASHOUT FROM CONCRETE PUMPER BINS CAN BE WASHED INTO CONCRETE PUMPER TRUCKS AND DISCHARGED INTO DESIGNATED WASHOUT AREA OR PROPERLY DISPOSED OF OFFSITE.
- TEMPORARY TOILET FACILITIES SHALL BE PROVIDED FOR ALL CONSTRUCTION WORKERS AND SITE VISITORS IN ACCORDANCE WITH 2006 INTERNATIONAL PLUMBING CODE GENERAL REGULATIONS, SECTION 311. PORTABLE FACILITIES SHALL BE PLACED ON LEVEL GROUND AND AWAY FROM STORM DRAINAGE SYSTEMS (DITCHES, CATCH BASINS, ETC.) DISPOSAL AND HANDLING OF SANITARY WASTE MUST COMPLY WITH TCEQ REQUIREMENTS.
- FINAL GRADES FOR GRASSED AND LANDSCAPED AREAS SHALL REQUIRE A MINIMUM OF 4'-6" OF CLEAN TOP SOIL, FREE OF DEBRIS AND CONTAMINANTS, AND PREFERABLY OF NATIVE ORIGIN.
- SPOT GRADES SHOWN ARE FINISHED SURFACE ELEVATIONS. CONTRACTOR MUST MAKE ALLOWANCE FOR PAVEMENT THICKNESS.
- WHEN PRECAST CONCRETE BOXES ARE USED FOR STORM STRUCTURES, THE TOP 12" SHALL BE CONSTRUCTED FROM BRICK OR BLOCK TO ALLOW ADJUSTMENTS TO FINAL GRADE.
- WHEN PRECAST CONCRETE BOXES ARE USED FOR STORM STRUCTURES, NO WAFFLE WALL BOXES OR KNOCK OUT PANELS WILL BE ALLOWED.
- M.E.G. = MATCH EXISTING GRADE
- CONTRACTOR SHALL COMPLY WITH THE CONSTRUCTION STORM WATER GENERAL PERMIT, INCLUDING PREPARATION OF A SWPPP AND FILING NOI. CONTRACTOR TO PROVIDE A COPY OF SWPPP, THE SITE NOTICE, AND NOI TO CITY OF ALBUQUERQUE AND KEEP ON SITE UNTIL CONSTRUCTION COMPLETION.

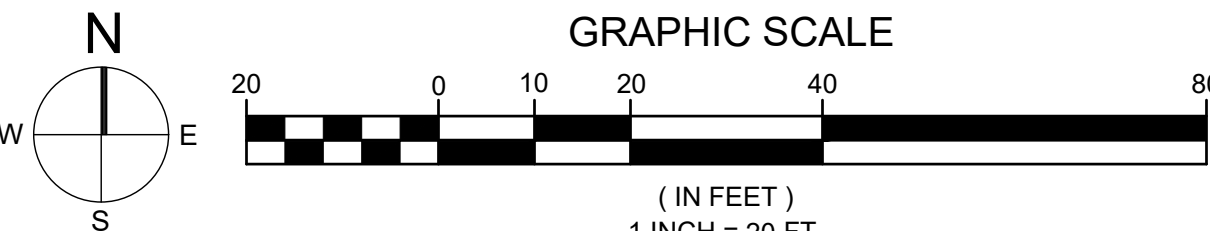
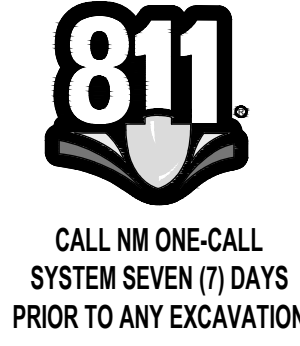
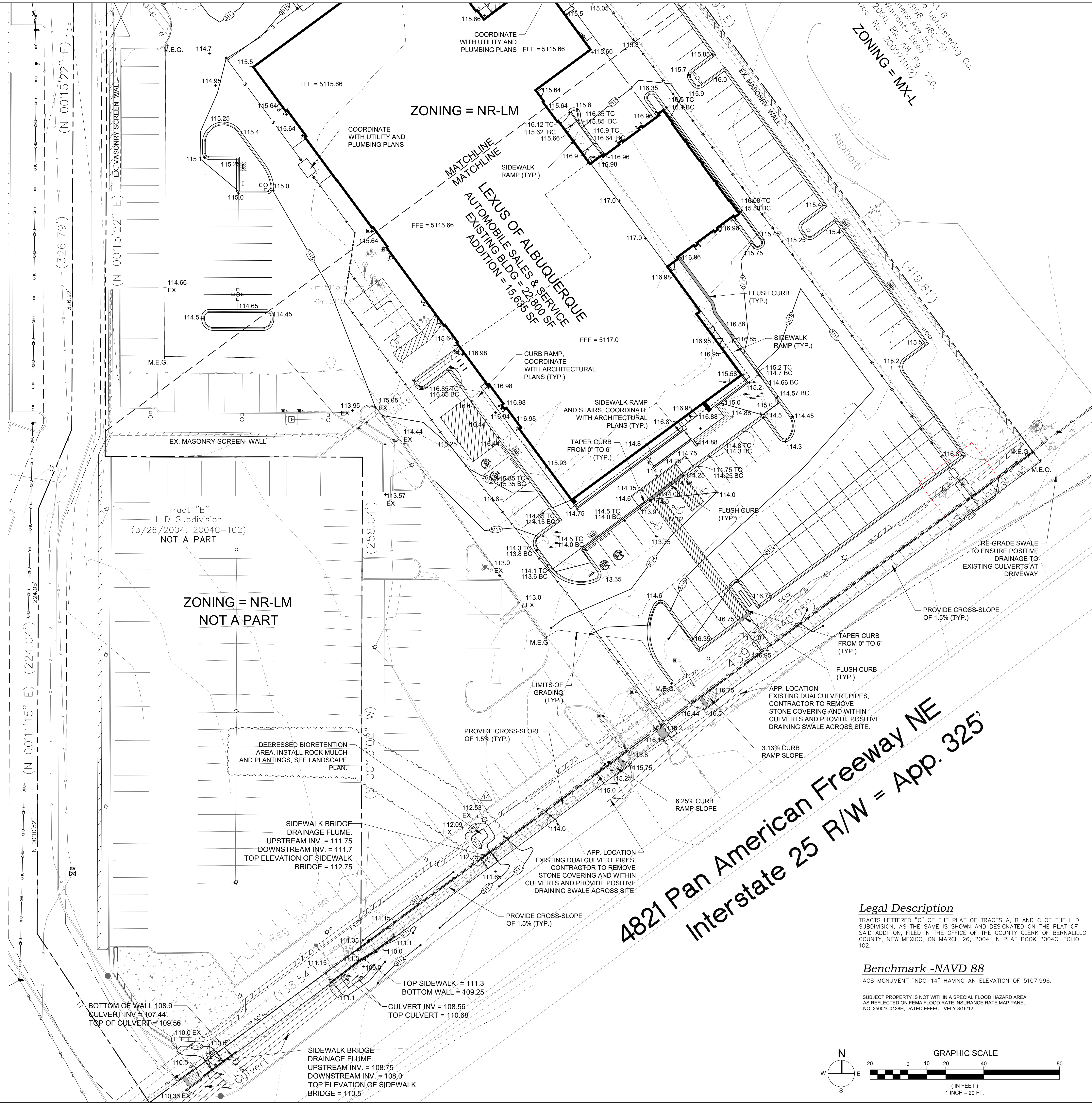
EROSION & SEDIMENT CONTROL NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING A GRADING PERMIT PRIOR TO BEGINNING WORK.
2. THE CONTRACTOR IS RESPONSIBLE FOR FILING FOR A NOTICE OF INTENT (NOI) WITH EPA PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR TO PREPARE AND PROVIDE A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) THAT IDENTIFIES ALL CONTROL MEASURES ON THE PROJECT.
3. THE CONTRACTOR SHALL ENSURE THAT NO SOIL ERODES INTO PUBLIC RIGHT-OF-WAY OR ONTO
4. THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET.
5. SPOILS FROM THE PROJECT SHALL NOT BE DEPOSITED OR STORED IN THE STREET OR ROADWAY.
6. SPOILS SHALL BE STAGED ON THE UPSTREAM SIDE OF TRENCHES WHEN TRENCHING IS REQUIRED.
7. THE CONTRACTOR SHALL CLEAN AND REMOVE ALL FUGITIVE DUST, SOIL AND DEBRIS RESULTING FROM THIS PROJECT FROM THE STREET AT THE END OF EACH DAY.
8. CONTRACTOR SHALL LEAVE THE AREA IMMEDIATELY BEHIND THE CURB DEPRESSED TO CONTAIN NUISANCE FLOWS AND SEDIMENT.
9. CONCRETE TRUCKS SHALL BE SENT BACK TO THE PLANT FOR WASHING. THE WASHING OF CONCRETE TRUCKS SHALL NOT BE PERMITTED WITHIN THE PUBLIC RIGHT-OF-WAY.
10. WHEN APPLICABLE, CONTRACTOR SHALL SECURE "TOPSOIL DISTURBANCE PERMIT" FROM THE CITY AND/OR FILE A NOTICE OF INTENT (NOI) WITH THE EPA PRIOR TO BEGINNING CONSTRUCTION. ALL EPA REGULATIONS WILL BE REQUIRED ON THE PROJECT.
11. UNLESS FINAL STABILIZATION IS OTHERWISE PROVIDED FOR, ANY AREAS OF EXCESS DISTURBANCE (TRAFFIC ACCESS, STORAGE YARD, EXCAVATED MATERIAL, ETC) SHALL BE RE-SEEDING ACCORDING TO CITY OF ALBUQUERQUE SPECIFICATION 1012 "MISCELLANEOUS SEEDING". THIS WILL BE CONSIDERED INCIDENTAL TO CONSTRUCTION, THEREFORE, NO SEPARATE PERMIT WILL BE MADE.
12. PROTECT EXISTING STORM DRAIN FACILITIES FROM SEDIMENT AS REQUIRED.
13. ALL EXPOSED EARTH SURFACES MUST BE PROTECTED FROM WIND AND WATER EROSION PRIOR TO FINAL (CITY) ACCEPTANCE OF ANY PROJECT.

CONSTRUCTION NOTES

1. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM, 811, FOR DESIGNATION (LINE-SPOTTING) OF EXISTING UTILITIES.
2. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL POTENTIAL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL INTERPRETATIONS IT MAKES WITHOUT FIRST CONTACTING THE ENGINEER AS REQUIRED ABOVE.
3. ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
4. ALL CONSTRUCTION WITHIN PUBLIC RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE CITY OF ALBUQUERQUE STANDARDS AND PROCEDURES.
5. UTILITY INFORMATION SHOWN HEREON IS BASED UPON ONSITE SURFACE EVIDENCE. IN ADDITION, UTILITY LINE-SPOTS WERE REQUESTED VIA THE NEW MEXICO ONE CALL SERVICE (TICKET NO. 200E020450). UTILITY LINES THAT ARE NOT SHOWN IN THESE DRAWINGS ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. IF ANY SUCH EXISTING LINES ARE SHOWN, THE LOCATION IS BASED UPON INFORMATION PROVIDED BY THE OWNER OF SAID UTILITY, AND THE INFORMATION MAY BE INCOMPLETE, OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES. THE ENGINEER HAS CONDUCTED ONLY PRELIMINARY INVESTIGATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES. THIS INVESTIGATION IS NOT CONCLUSIVE, AND MAY NOT BE COMPLETE, THEREFORE, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. THE CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. IN PLANNING AND CONDUCTING EXCAVATION, THE CONTRACTOR SHALL COMPLY WITH STATE STATUTES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES.
6. ALL LANDSCAPE AND PAVED AREAS ADJACENT TO THE BUILDING SHALL BE PROVIDED WITH POSITIVE DRAINAGE TO AVOID ANY PONDING ADJACENT TO THE STRUCTURE.

LEGEND	
	PROPERTY CORNER
	CENTER LINE
	PROPERTY LINE
	RIGHT OF WAY
	SEPTIC TANK
	WATER METER
	CATV
	TELEPHONE PEDESTAL
	POWER POLE
	ELECTRICAL BOX
	BENCHMARK
	FIRE HYDRANT
	WATER VALVE
	BLOW OFF
	SANITARY SEWER MANHOLE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING SPOT ELEVATION
	PROPOSED SPOT ELEVATION
	TOP OF WALL ELEVATION
	BOTTOM OF WALL ELEVATION
	PROPOSED SWALE
	RIP RAP
	FLARED END SECTION
	STORM DRAIN DROP INLET
	STORM DRAIN JUNCTION BOX
	HANDICAP PARKING SPACE
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	EASEMENT LINE
	INLET PROTECTION
	STORM DRAIN
	TREE SAVE / PROTECTION
	TREE TO BE REMOVED



4821 Pan American Freeway NE
Interstate 25 R/W = App. 325'

Legal Description

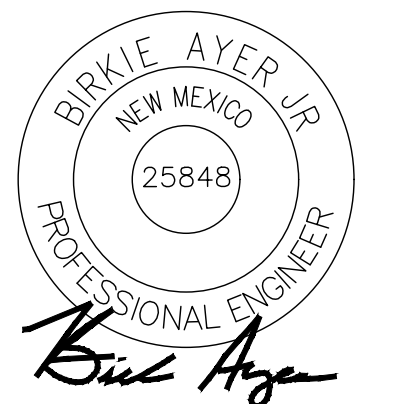
TRACTS LETTERED "C" OF THE PLAT OF TRACTS A, B AND C OF THE LLD SUBDIVISION, AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT OF SAID ADDITION, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO, ON MARCH 26, 2004, IN PLAT BOOK 2004C, FOLD 102.

Benchmark -NAVD 88

ACS MONUMENT "NDC-14" HAVING AN ELEVATION OF 5107.996.

SUBJECT PROPERTY IS NOT WITHIN A SPECIAL FLOOD HAZARD AREA AS REFLECTED ON FEMA FLOOD RATE INSURANCE RATE MAP PANEL NO. 35501C0138H, DATED EFFECTIVELY 8/16/12.

AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
GRADING PLAN SOUTH

NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C04.0

GRADING NOTES

- PROPOSED PROJECT WILL REDUCE EXISTING IMPERVIOUS COVER BY APP. 1,608 SF.
- CONCRETE TRUCKS SHALL NOT TYPICALLY BE WASHED OUT ON SITE. SHOULD CONCRETE TRUCK WASHOUT BE PERMITTED ON SITE, COORDINATE LOCATION AND BMPs WITH SITE INSPECTOR:
- DO NOT DISPOSE OF CONCRETE TRUCK WASHOUT WASTE BY DUMPING INTO A SANITARY SEWER, STORM DRAIN OR ONTO SOIL OR PAVEMENT THAT CARRIES STORM WATER RUNOFF.
- THE WASHOUT FROM CONCRETE TRUCKS SHOULD BE DISPOSED OF INTO:
 - A DESIGNATED AREA THAT WILL LATER BE BACKFILLED (SLURRY PIT)
 - AN AREA WHERE THE CONCRETE WASH CAN HARDEN AND THEN BE DISPOSED OF AS SOLID WASTE
 - A LOCATION WHICH IS NOT SUBJECT TO WATER RUNOFF AND MORE THAN 50 FEET AWAY FROM A STORM DRAIN, OPEN DITCH OR RECEIVING WATER WAY.
- PUMP EXCESS CONCRETE IN CONCRETE PUMP BACK INTO CONCRETE MIXER TRUCK.
- CONCRETE WASHOUT FROM CONCRETE PUMPER BINS CAN BE WASHED INTO CONCRETE PUMPER TRUCKS AND DISCHARGED INTO DESIGNATED WASHOUT AREA OR PROPERLY DISPOSED OF OFF SITE.
- TEMPORARY TOILET FACILITIES SHALL BE PROVIDED FOR ALL CONSTRUCTION WORKERS AND SITE VISITORS IN ACCORDANCE WITH 2006 INTERNATIONAL PLUMBING CODE GENERAL REGULATIONS, SECTION 311. PORTABLE FACILITIES SHALL BE PLACED ON LEVEL GROUND AND AWAY FROM STORM DRAINAGE SYSTEMS (DITCHES, CATCH BASINS, ETC.) DISPOSAL AND HANDLING OF SANITARY WASTE MUST COMPLY WITH TCEQ REQUIREMENTS.
- FINAL GRADES FOR GRASSED AND LANDSCAPED AREAS SHALL REQUIRE A MINIMUM OF 4"-6" OF CLEAN TOP SOIL, FREE OF DEBRIS AND CONTAMINANTS, AND PREFERABLY OF NATIVE ORIGIN.
- SPOT GRADES THINER ARE FINISHED SURFACE ELEVATIONS, CONTRACTOR MUST MAKE ALLOWANCE FOR PAVEMENT THICKNESS.
- WHEN PRECAST CONCRETE BOXES ARE USED FOR STORM STRUCTURES, THE TOP 12" SHALL BE CONSTRUCTED FROM BRICK OR BLOCK TO ALLOW ADJUSTMENTS TO FINAL GRADE.
- WHEN PRECAST CONCRETE BOXES ARE USED FOR STORM STRUCTURES, NO WAFFLE WALL BOXES OR KNOCK OUT PANELS WILL BE ALLOWED.
- M.E.G. = MATCH EXISTING GRADE
- CONTRACTOR SHALL COMPLY WITH THE CONSTRUCTION STORM WATER GENERAL PERMIT, INCLUDING PREPARATION OF A SWPPP AND FILING NOI. CONTRACTOR TO PROVIDE A COPY OF SWPPP, THE SITE NOTICE, AND NOI TO CITY OF ALBUQUERQUE AND KEEP ON SITE UNTIL CONSTRUCTION COMPLETION.

EROSION & SEDIMENT CONTROL NOTES

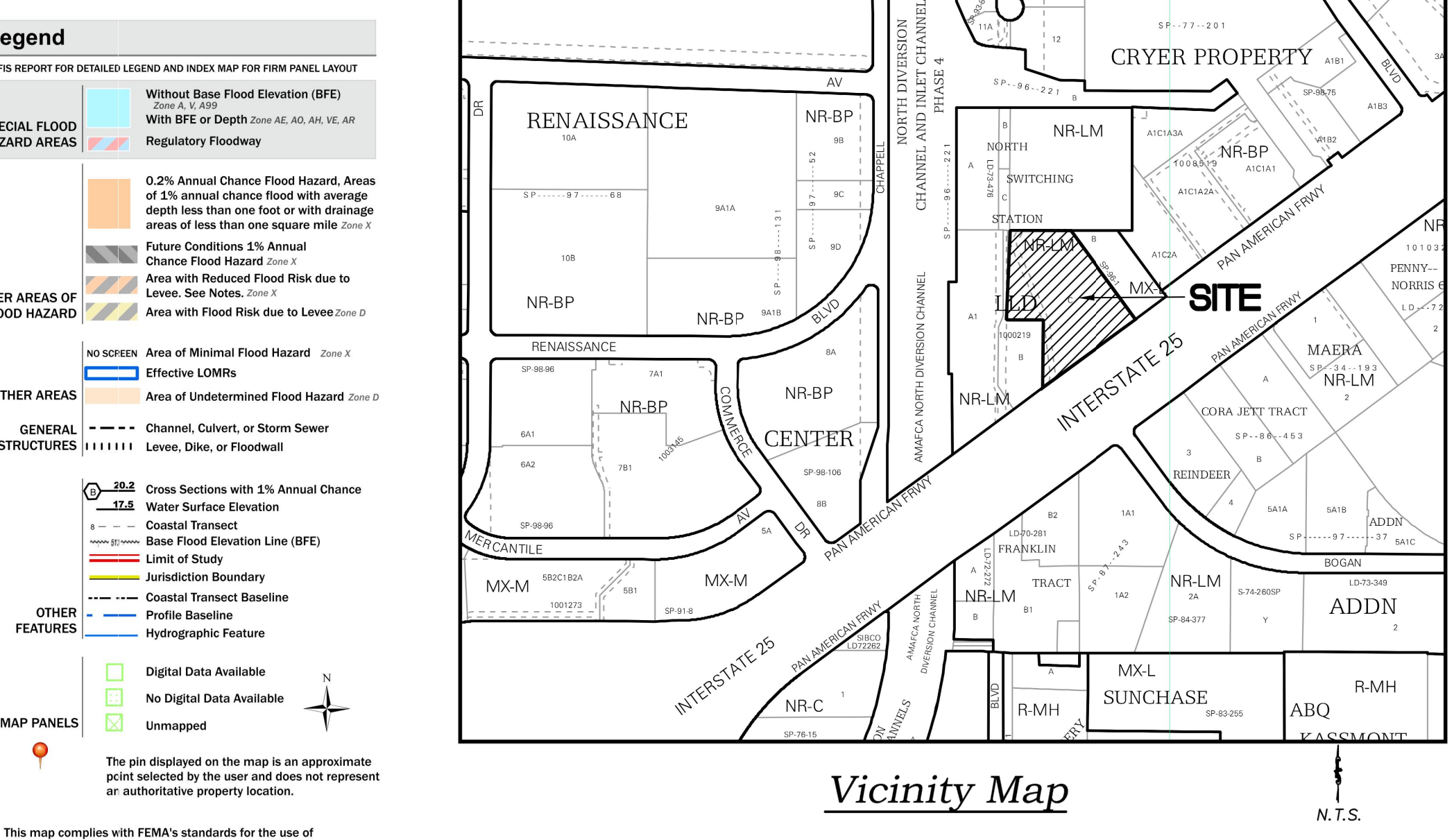
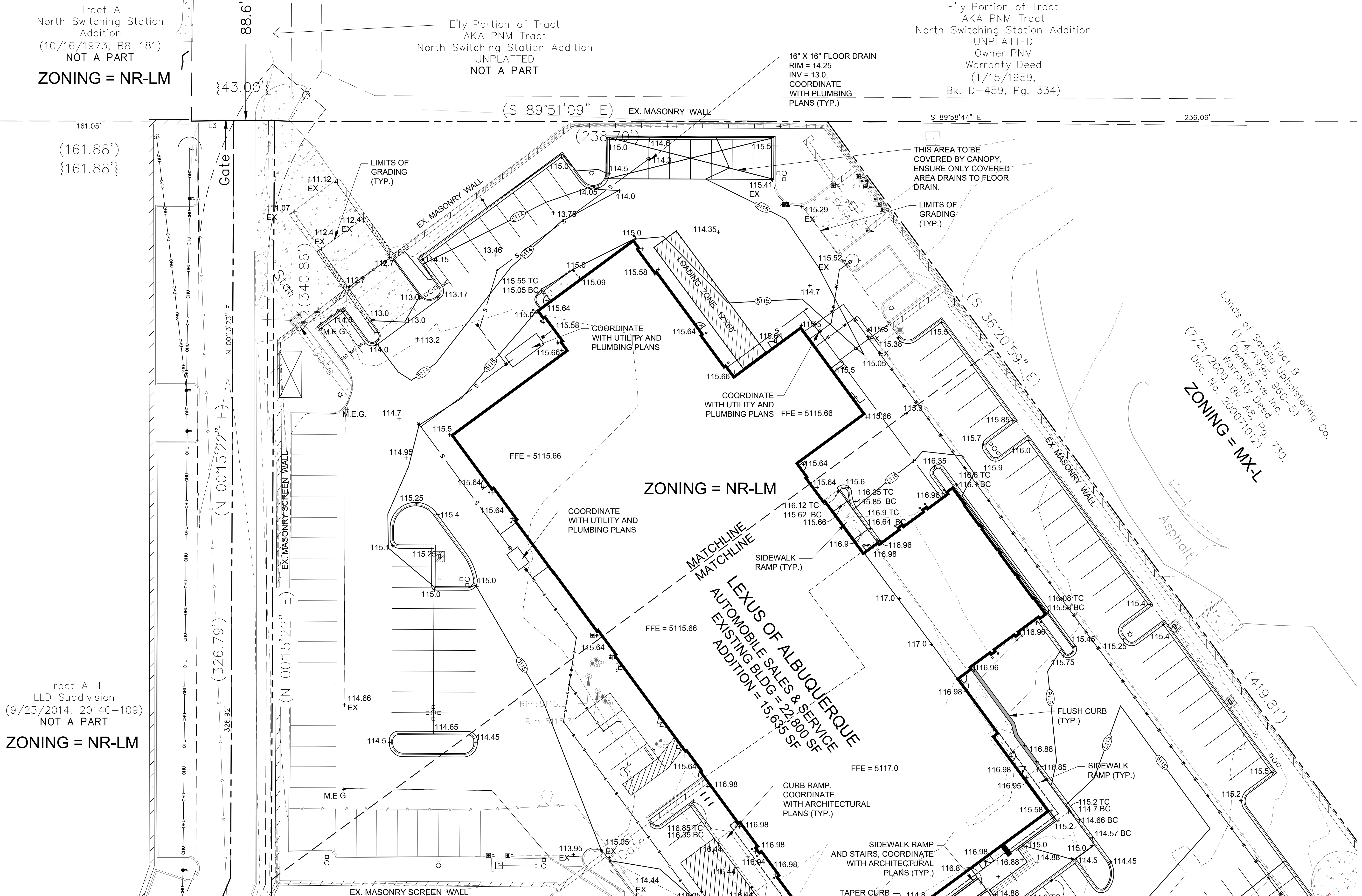
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING A GRADING PERMIT PRIOR TO BEGINNING WORK.
- THE CONTRACTOR IS RESPONSIBLE FOR FILING FOR A NOTICE OF INTENT (NOI) WITH EPA PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR TO PROVIDE A COPY OF SWPPP, THE SITE NOTICE, AND NOI TO CITY OF ALBUQUERQUE AND KEEP ON SITE UNTIL CONSTRUCTION COMPLETION.
- THE CONTRACTOR SHALL ENSURE THAT NO SOIL ERODES INTO PUBLIC RIGHT-OF-WAY OR ONTO
- THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET.
- SPOILS FROM THE PROJECT SHALL NOT BE DEPOSITED OR STORED IN THE STREET OR ROADWAY.
- SPOILS SHALL BE STAGED ON THE UPSTREAM SIDE OF TRENCHES WHEN TRENCHING IS REQUIRED.
- THE CONTRACTOR SHALL CLEAN AND REMOVE ALL FUGITIVE DUST, SOIL AND DEBRIS RESULTING FROM THIS PROJECT AT THE END OF EACH DAY.
- CONTRACTOR SHALL LEAVE THE AREA IMMEDIATELY BEHIND THE CURB DEPRESSED TO CONTAIN NUISANCE FLOWS AND SEDIMENT.
- CONCRETE TRUCKS SHALL BE SENT BACK TO THE PLANT FOR WASHING, THE WASHING OF CONCRETE TRUCKS SHALL NOT BE PERMITTED WITHIN THE PUBLIC RIGHT-OF-WAY.
- WHEN APPLICABLE, CONTRACTOR SHALL SECURE "TOPSOIL DISTURBANCE PERMIT" FROM THE CITY AND/OR FILE A NOTICE OF INTENT (NOI) WITH THE EPA PRIOR TO BEGINNING CONSTRUCTION. ALL EPA REGULATIONS WILL BE REQUIRED ON THE PROJECT.
- UNLESS FINAL STABILIZATION IS OTHERWISE PROVIDED FOR, ANY AREAS OF EXCESS DISTURBANCE (TRAFFIC ACCESS, STORAGE YARD, EXCAVATED MATERIAL, ETC.) SHALL BE RE-SEEDING ACCORDING TO CITY OF ALBUQUERQUE SPECIFICATION 1012 "MISCELLANEOUS SEEDING". THIS WILL BE CONSIDERED INCIDENTAL TO CONSTRUCTION, THEREFORE, NO SEPARATE PERMIT WILL BE MADE.
- PROTECT EXISTING STORM DRAIN FACILITIES FROM SEDIMENT AS REQUIRED.
- ALL EXPOSED EARTH SURFACES MUST BE PROTECTED FROM WIND AND WATER EROSION PRIOR TO FINAL (CITY) ACCEPTANCE OF ANY PROJECT.

CONSTRUCTION NOTES

- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM, 811, FOR DESIGNATION (LINE-SPOTTING) OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATION ALL POTENTIAL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL INTERPRETATIONS IT MAKES WITHOUT FIRST CONTACTING THE ENGINEER AS REQUIRED ABOVE.
- ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
- ALL CONSTRUCTION WITHIN PUBLIC RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE CITY OF ALBUQUERQUE STANDARDS AND PROCEDURES.
- UTILITY INFORMATION SHOWN HEREON IS BASED UPON ONSITE SURFACE EVIDENCE. IN ADDITION, UTILITY LINE-SPOTS WERE REQUESTED VIA THE NEW MEXICO ONE CALL SERVICE (TICKET NO. 2020020450). UTILITY LINES THAT APPEAR ON THESE DRAWINGS ARE SHOWN IN AN APPROXIMATE MANNER ONLY, AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. IF ANY SUCH EXISTING LINES ARE SHOWN, THE LOCATION IS BASED UPON INFORMATION PROVIDED BY THE OWNER OF SAID UTILITY, AND THE INFORMATION MAY BE INCOMPLETE, OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES. THE ENGINEER HAS CONDUCTED ONLY PRELIMINARY INVESTIGATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES. THIS INVESTIGATION IS NOT CONCLUSIVE, AND MAY NOT BE COMPLETE, THEREFORE, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. THE CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. IN PLANNING AND CONDUCTING EXCAVATION, THE CONTRACTOR SHALL COMPLY WITH STATE STATUTES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES.
- ALL LANDSCAPE AND PAVED AREAS ADJACENT TO THE BUILDING SHALL BE PROVIDED WITH POSITIVE DRAINAGE TO AVOID ANY PONDING ADJACENT TO THE STRUCTURE.

LEGEND

- | | | | |
|-----------|--------------------------|---|----------------------------|
| ○ | PROPERTY CORNER | ▬ | RIP RAP |
| — | CENTER LINE | ▬ | FLARED END SECTION |
| — | PROPERTY LINE | ▬ | STORM DRAIN DROP INLET |
| R/W | RIGHT OF WAY | ▬ | STORM DRAIN JUNCTION BOX |
| ⊙ | SEPTIC TANK | ▬ | HANDICAP PARKING SPACE |
| ⊙ | WATER METER | ▬ | EXISTING WATER LINE |
| ⊙ | CATV | ▬ | PROPOSED WATER LINE |
| ⊙ | TELEPHONE PEDESTAL | ▬ | EXISTING SANITARY SEWER |
| ⊙ | POWER POLE | ▬ | PROPOSED SANITARY SEWER |
| ⊙ | ELECTRICAL BOX | ▬ | EXISTING OVERHEAD ELECTRIC |
| ⊙ | BENCHMARK | ▬ | UNDERGROUND ELECTRIC |
| ⊙ | FIRE HYDRANT | ▬ | SILT FENCE |
| ⊙ | WATER VALVE | ▬ | EASEMENT LINE |
| ⊙ | BLOW OFF | ▬ | INLET PROTECTION |
| ⊙ | SANITARY SEWER MANHOLE | ▬ | STORM DRAIN |
| 600 | EXISTING CONTOUR | ⊙ | TREE SAVE / PROTECTION |
| 604.7 | EXISTING SPOT ELEVATION | ⊙ | TREE TO BE REMOVED |
| X 604.7 | PROPOSED SPOT ELEVATION | | |
| TW-612.0 | TOP OF WALL ELEVATION | | |
| BW-605.16 | BOTTOM OF WALL ELEVATION | | |
| — | PROPOSED SWALE | | |



SITE DESIGN DATA

- PROPERTY IS LOCATED AT 4821 PAN AMERICAN FWY.
- UPC# 101706102726220107
- CURRENT ZONING: NR-LM (NON-RESIDENTIAL LIGHT MANUFACTURING)
- PROPOSED USE: AUTOMOBILE SALES AND SERVICE
- MINIMUM YARD & BUILDING SETBACKS PER ZONING ORDINANCE

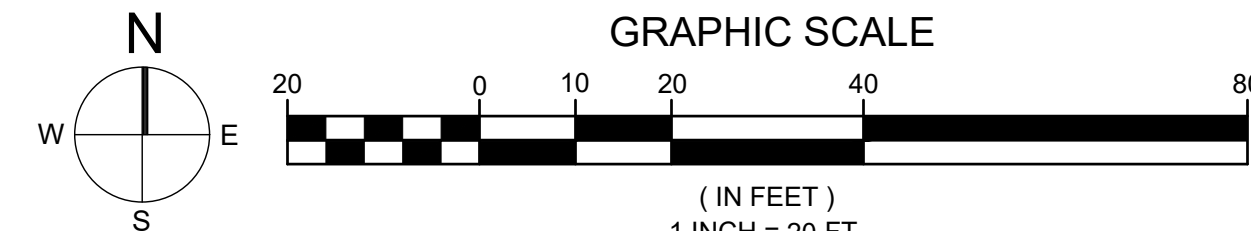
FRONT/STREET	5 FT
REAR YARD	0 FT
SIDE YARD	0 FT

Legal Description

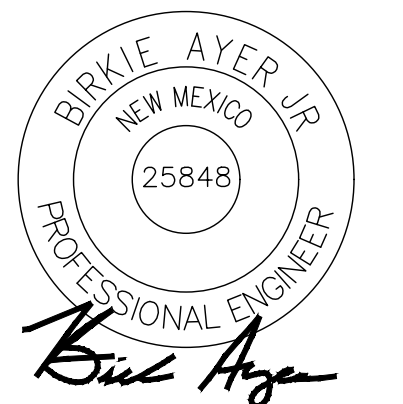
TRACTS LETTERED "C" OF THE PLAT OF TRACTS A, B AND C OF THE LLD SUBDIVISION, AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT OF SAID ADDITION, FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO, ON MARCH 26, 2004, IN PLAT BOOK 2004C, FOLIO 102.

Benchmark -NAVD 88

ACS MONUMENT "NDC-14" HAVING AN ELEVATION OF 5107.996.



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnson Street
 Rock Hill, SC 29730
 Phone: 803-328-5858



PROJECT NAME:

LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
GRADING PLAN NORTH

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
#1	FOR PERMIT	6/23/22
#2	PERMIT REVISIONS	7/11/22
#3	PERMIT REVISIONS	10/31/22
#4	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C04.1

WATER NOTES

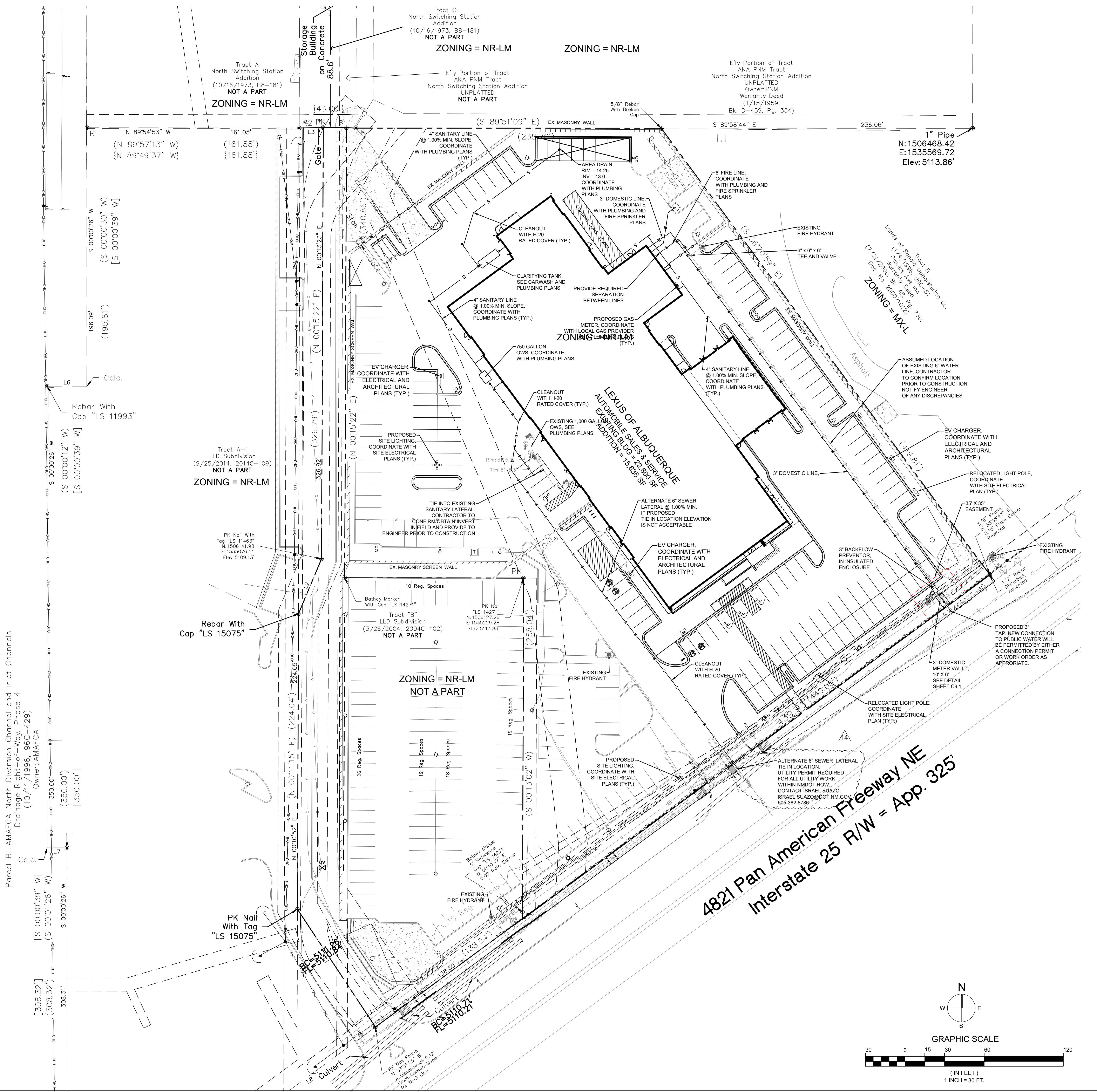
1. ALL NEW PUBLIC WATER MAIN IS TO BE 8" DIP EXCEPT NOTED OTHERWISE.
2. CONTRACTOR TO FIELD VERIFY SIZE AND TYPE OF EXISTING LINES AT THE POINTS PRIOR TO COMMENCING CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY IN WRITING IF DISCREPANCIES EXIST.
3. 2-INCH DOMESTIC WATER SERVICE TO BE TYPE K COPPER.
4. ALL SERVICES ARE TO BE LOCATED ON THE USER SIDE OF SIDEWALKS.
5. CONTRACTOR TO INSTALL COATED 14 GAUGE SOLID COPPER TRACER WIRE FOR ALL PVC WATERLINES. TRACER WIRE SHALL MAKE A MIN. 2' LOOP UP & OUT OF VALVE BOX TOP.
6. ALL DUCTILE IRON WATER PIPE TO HAVE MECHANICAL JOINT FITTINGS.
7. WATER FITTINGS FOR SERVICES TO HAVE A MINIMUM WORKING PRESSURE OF 150 PSI.
8. PIPE DEFLECTION AT EACH JOINT IS NOT TO EXCEED PIPE MANUFACTURER'S RECOMMENDATIONS. WHERE REQUIRED DEFLECTION EXCEEDS THE RECOMMENDATIONS, MAKE THE DEFLECTION OVER MULTIPLE PIPE JOINTS.
9. MAINTAIN 12" MINIMUM CLEARANCE BETWEEN WATER & STORM DRAIN PIPES. CENTER A 20' LENGTH OF DI WATER PIPE ON THE STORM DRAIN PIPE AT ALL CROSSINGS.
10. SEE DETAIL SHEET FOR TYPICAL SEWER LINE CROSSINGS.
11. ALL WATER WORK, PLANS & MATERIALS SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF ABCWUA UTILITIES AND THE CITY OF ALBUQUERQUE.
12. WATER & SANITARY SEWER SEPARATION SHALL BE 10 FT. (CLEAR) IN ACCORDANCE WITH ALL LOCAL AND STATE REGULATIONS.
13. WATER FEES AS APPLICABLE ARE TO BE PAID TO ABCWUA.
14. COORDINATION WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY CROSS CONNECTION SECTION WILL BE REQUIRED TO ENSURE PROPER BACKFLOW CONTAINMENT IS IN PLACE PRIOR TO RELEASE OF THE METER FOR THE SITE. CONTACT THE CROSS CONNECTION SECTION AT 505-268-3464.
15. COORDINATION WITH THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY PRE-TREATMENT ENGINEER IS REQUIRED TO ENSURE THE SITE COMPLIES WITH THE WATER AUTHORITY SEWER ORDINANCE. CONTACT THE PRE-TREATMENT ENGINEER AT 505-268-3439.
16. NEW CONNECTION TO PUBLIC WATER AND/OR PUBLIC SANITARY SEWER WILL BE CONSTRUCTED BY EITHER A CONNECTION PERMIT OR WORK ORDER AS APPROPRIATE.

SEWER NOTES

1. ALL SANITARY SEWER SERVICES ARE TO BE SCHEDULE 40 PVC.
2. STONE BEDDING IS REQUIRED FOR ALL PVC SEWER PIPE.
3. ALL SEWER SERVICES ARE TO BE LOCATED AS INDICATED ON THE PLANS AND 10' FROM WATER SERVICE UNLESS NOTED OTHERWISE.
4. PLEASE NOTE ALL CLEANOUTS HAVE NOT BEEN INDICATED ON THE PLAN. CLEANOUTS SHALL BE LOCATED PER THE INTERNATIONAL PLUMBING CODE CURRENT VERSION AND THE STANDARDS AND SPECIFICATIONS OF EL PASO WATER UTILITIES AND THE CITY OF EL PASO.
5. DRAIN OR FIXTURE OUTLETS WHICH ARE AT AN ELEVATION LOWER THAN THE TOP OF THE NEAREST DOWNSTREAM SANITARY SEWER MANHOLE SHALL BE PROTECTED FROM SEWAGE BACKUP WITH THE INSTALLATION OF A BACKFLOW PREVENTION DEVICE.
6. ALL SEWER WORK, PLANS & MATERIALS SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF ABCWUA UTILITIES AND THE CITY OF ALBUQUERQUE.
7. ALL NEW MANHOLES IN FLOOD PLAIN ARE REQUIRED TO BE SEALED AND VENTED.
8. SEWER FEES AS APPLICABLE ARE TO BE PAID TO ABCWUA.
9. THE CONTRACTOR, PRIOR TO POURING THE BUILDING SLAB AND FOUNDATION, SHALL VERIFY THE ACCESSIBILITY OF THE SANITARY SEWER SERVICE AND OBTAIN CONFIRMATION FROM THE DESIGN ENGINEER REGARDING SANITARY SEWER SERVICE FUNCTIONALITY.

GENERAL NOTES

1. ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
2. CONTRACTOR SHALL CONTACT "NM ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING ANY EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
3. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
4. CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
5. SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
6. ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
7. FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
8. GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
9. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
10. BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
11. BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
12. WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
13. ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN IDEO APPROVED LANDFILL.
14. CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
15. ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
16. CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
17. CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
18. ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
19. CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
20. TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROWLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
21. ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSSWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A. CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
22. CONTRACTOR SHALL MAINTAIN ONE ACCESS DRIVE INTO SITE FROM JEFFERSON STREET NE THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
23. CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
24. REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858

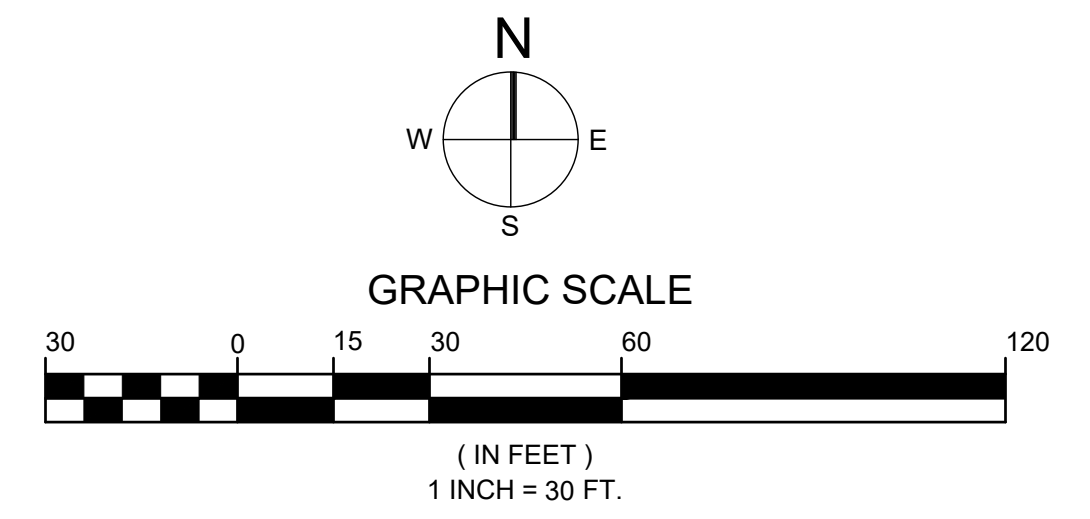
BIRKIE AYER JR.
 NEW MEXICO
 25848
Birkie Ayer
 PROFESSIONAL ENGINEER

PROJECT NAME:
LEXUS OF ALBUQUERQUE
 4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
UTILITY PLAN

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/13/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C05.0



LANDSCAPE NOTES

1. ALL DISTURBED AREAS NOT SPECIFIED AS LANDSCAPE AREA OR HARDSCAPE TO BE MULCHED WITH ROCK MULCH.
2. CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO INSTALLATION. NOTIFY ENGINEER/ARCHITECT IF FIELD CONDITIONS WARRANT ADJUSTMENT OF PLANT LOCATIONS.
3. SEE SHEET C07.0 FOR LANDSCAPE DETAILS AND SPECIFICATIONS.
4. CONTRACTOR IS RESPONSIBLE FOR LOCATING EXISTING UTILITIES AND IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGE TO EXISTING UTILITIES THAT MAY OCCUR.
5. CONTRACTOR TO PROVIDE IRRIGATION PLAN TO ENGINEER/ARCHITECT FOR REVIEW AND APPROVAL.
6. COORDINATE LANDSCAPE INSTALLATION WITH IRRIGATION CONSTRUCTION.
7. ENSURE FINAL GRADES IN LANDSCAPED AND GRASSED AREAS PROVIDE POSITIVE DRAINAGE.
8. FINAL GRADES FOR GRASSED AND LANDSCAPED AREAS SHALL REQUIRE A MINIMUM OF 4" OF UNCOMPACTED, CLEAN TOPSOIL, FREE OF DEBRIS AND CONTAMINANTS, AND PREFERABLY OF NATIVE ORIGIN.
9. ALL TREE PITS, SHRUB BEDS AND PREPARED PLANTING BEDS ARE TO BE COMPLETELY EXCAVATED IN ACCORDANCE WITH THE PLANTING DETAILS.
10. SOIL REQUIRED FOR PLANTING MIXES SHALL BE PROVIDED BY THE CONTRACTOR WHO SHALL LOAD, HAUL, MIX AND DISTRIBUTE ALL TOPSOIL AND AMENDMENTS SUCH AS ORGANICS, HUMUS, COMPOSTED MATERIAL OR NATIVE MATERIAL AS REQUIRED.
11. FOR NEW PLANTING AREAS, REMOVE ALL PAVEMENT, GRAVEL SUB BASE AND CONSTRUCTION DEBRIS. LOOSEN COMPACTED SOIL TO MINIMUM 24" DEPTH AND PROVIDE SOIL AMENDMENTS MIXED INTO THE TOP 18" MINIMUM OR FOR UNCOMPACTED AREAS, AMEND THE TOP 18" OF EXISTING SOIL TO MEET TOPSOIL/PLANTING MIX STANDARDS.
12. PLANTING MIX SHALL BE NATURAL, FERTILE AGRICULTURAL TOPSOIL, CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT MAY BE DEVELOPED BY AMENDING THE EXISTING SOIL OR REMOVING THE EXISTING SOIL AND REPLACING WITH NEW SOIL. IT SHALL BE FREE OF STONES AND OTHER LIVE PLANTS OR THEIR ROOTS, STICKS AND/OR ANY OTHER OTHER THAN FRAGILE SOIL.
13. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR ANY FIELD ADJUSTMENTS REQUIRED TO AVOID DRAINAGE PATTERNS OR STRUCTURES, UTILITIES, UNDERGROUND OR OVERHEAD OR RELOCATIONS OF PROPOSED VEGETATION BECAUSE OF SITE CONDITIONS.
14. STONE MULCH BE 3" GRANITE "GREY ROCK" AT A DEPTH OF 3"-4", OR APPROVED EQUAL.
15. STONE MULCH WITHIN NMDOT RW TO BE 3"-4" GREY ROCK AT A MIN. DEPTH OF 4", OR APPROVED EQUAL.
16. WOOD MULCH TO BE ORGANIC WOOD OR BARK MULCH AT A DEPTH OF 3"-4", OR APPROVED EQUAL.
17. INSTALL METAL EDGING, OR APPROVED EQUAL, BETWEEN AREAS OF STONE AND WOOD MULCH.
18. LEXUS OF ALBUQUERQUE OWNER WILL BE RESPONSIBLE FOR MAINTAINING LANDSCAPING AND ASSOCIATED IRRIGATION SYSTEMS ONCE CONSTRUCTION PROJECT IS COMPLETED.
19. LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL, (AS MEASURED FROM THE CUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
20. CONTRACTOR TO PROVIDE HARDWOOD MULCH AROUND ALL PROPOSED AND EXISTING TREES. MULCH RING TO BE A MIN. 5' RADIUS AROUND THE TREE.
21. CONTRACTOR TO REMOVE AND REPLACE ALL DEAD AND DISEASED PLANT MATERIAL.

PLANTING LEGEND & SCHEDULE							
SYMBOL	BOTANICAL NAME	COMMON NAME	AVG. SIZE HEIGHT X WIDTH	COVERAGE	QUANTITY	TOTAL COVERAGE	SIZE WATER USAGE
	QUERCUS FUSIFORMIS	ESCAPMENT LIVE OAK	25' X 30'	707 SF	5	3,535 SF	2" CAL. MEDIUM
	GINKGO BILOBA	GINKGO	40' X 25'	491 SF	4	1,964 SF	2" CAL. MEDIUM
	CERCIS RENIFORMIS	OKLAHOMA REDBUD	15' X 12'	113 SF	7	791 SF	2" CAL. MEDIUM
	ILEX VOMITORIA	YAUPOH HOLLY	15' X 15'	177 SF	5	855 SF	2" CAL. MEDIUM
	JUNIPERUS SABINA 'TAM'	TAM JUNIPER	2' X 7'	38 SF	23	874 SF	3 GAL. LOW
	ERICAMERIA LARICIFOLIA	TURPENTINE BUSH	3' X 4'	13 SF	31	403 SF	3 GAL. LOW
	SALVIA CHAMAEDRYOIDES	MEXICAN BLUE SAGE	1' X 3'	7 SF	48	336 SF	1 GAL. LOW
	MUHLENBERGIA CAPILLARIS	REGAL MIST MUHLY	3' X 3'	7 SF	47	329 SF	1 GAL. LOW
COVERAGE AREA = 9,117 SF							

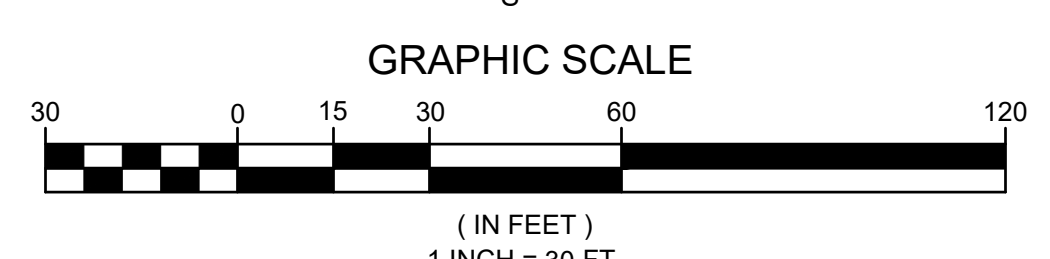
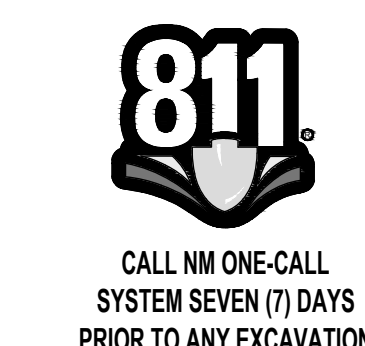
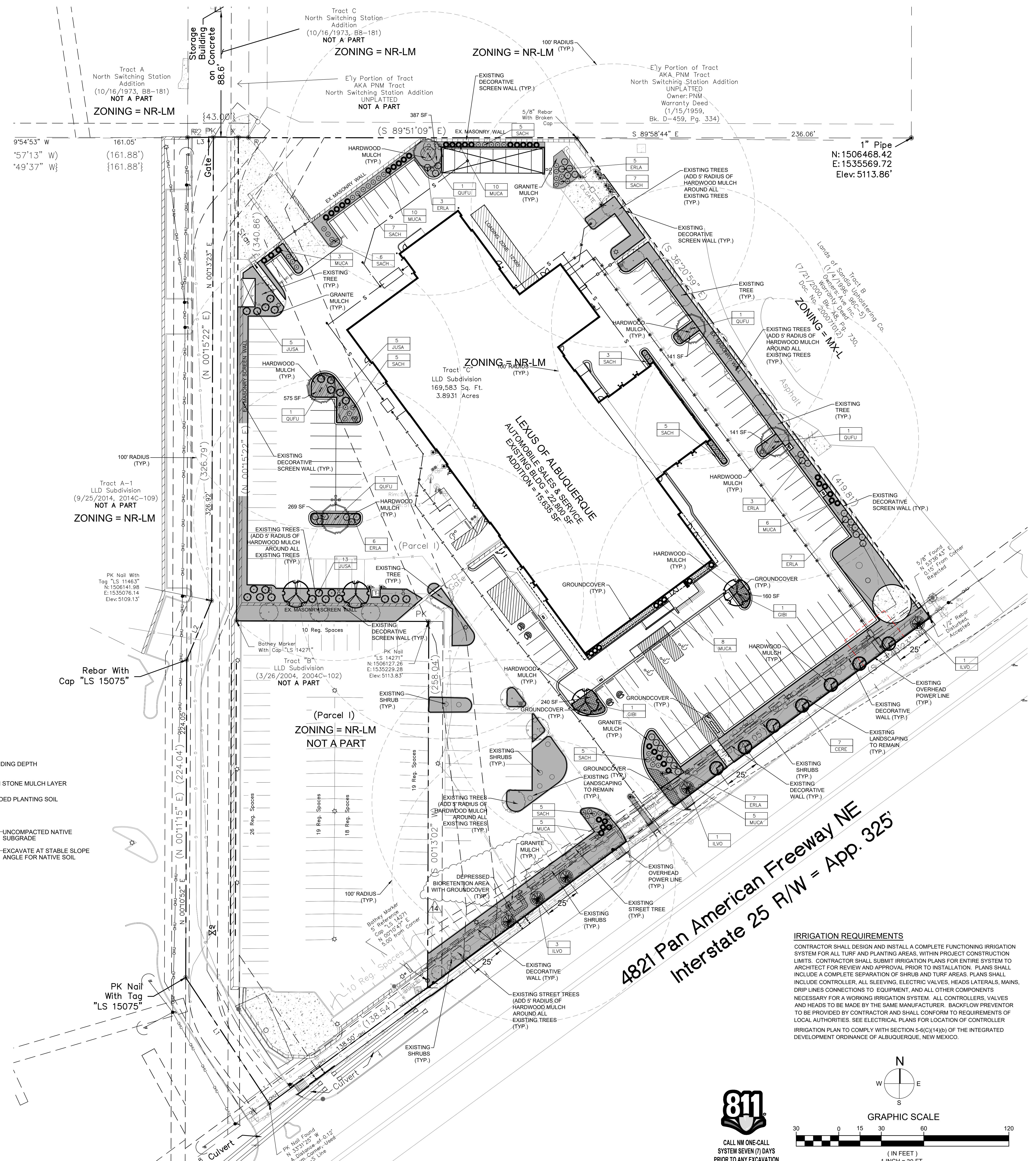
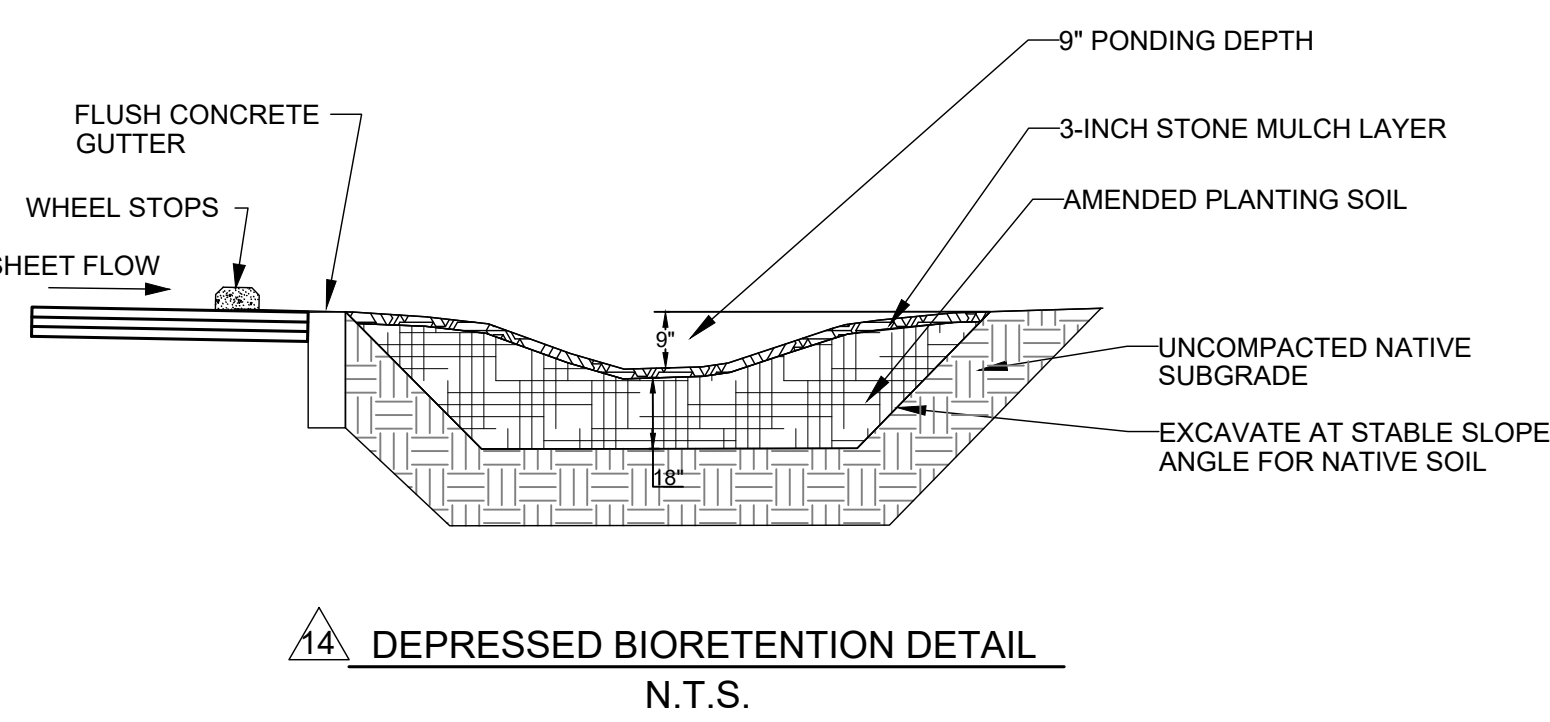
- SANTOLINA CHAMAECYPARISSUS "GRAY LAVENDER COTTON" (OR APPROVED EQUAL), SPACED AT 3' O.C. COVERAGE AREA = 922 SF
- HARDWOOD MULCH SYMBOL
- LANDSCAPE AREA HATCH

LANDSCAPE REQUIREMENTS

- LANDSCAPE AREA REQUIREMENTS:**
 LOT: 169,583 SF
 TOTAL BUILDING FOOTPRINT = 38,336 SF
 NET LOT AREA: 169,583 SF - 38,336 SF = 131,247 SF
 REQUIRED LANDSCAPE AREA = 15% OF NET LOT AREA
 131,247 SF X 15% = 19,687 SF
 LANDSCAPE AREA PROVIDED = 20,302 SF
- INTERNAL PARKING LOT LANDSCAPE REQUIREMENTS:**
 PARKING AREA: 75,041 SF
 REQ. PARKING LANDSCAPE AREA = 15% OF PARKING AREA
 75,041 SF X 15% = 11,256 SF
 LANDSCAPE AREA PROVIDED = 20,302 SF
- TREE REQUIREMENTS:**
 1 TREE PER 10 PARKING SPACES
 PARKING SPACES = 180 SPACES
 180 SPACES / 10 = 18.0
 27 TREES TOTAL, INCLUDING 18 EXISTING
- LANDSCAPE COVERAGE REQUIREMENTS:**
 LANDSCAPE AREA = 20,302 SF
 REQ. VEGETATIVE COVERAGE = 75% OF REQUIRED LANDSCAPE AREA
 20,302 SF X 75% = 15,227 SF REQUIRED COVERAGE
 EXISTING PLANT MATERIAL COVERAGE = 12,466 SF
 PROPOSED PLANT MATERIAL COVERAGE = 9,117 + 922 = 10,039 SF
 TOTAL PLANT MATERIAL COVERAGE = 12,466 + 10,039 = 22,505 SF
- REQ. GROUND LEVEL COVERAGE = 25% OF REQUIRED LANDSCAPE AREA**
 15,227 X 25% = 3,807 SF REQUIRED COVERAGE
 EXISTING GROUND PLANT MATERIAL COVERAGE = 1,350 SF
 PROPOSED GROUND PLANT MATERIAL COVERAGE = 2,864 SF
 TOTAL GROUND PLANT MATERIAL COVERAGE = 1,350 + 2,864 = 4,214 SF
- STREET TREE REQUIREMENTS**
 1 TREE PER 25 LF, 440 LF TOTAL FRONTAGE
 440/25 = 17.6, USE 18 TREES REQUIRED
 4 TREES @ 2" CAL. = 4 CREDITS, 2 TREES @ 8" CAL. = 4 CREDITS
 EXISTING TREE CREDITS = 8 TREES
 10 PROPOSED TREES + 8 TREE CREDITS = 18 TREES

GENERAL NOTES

1. ALL PAVEMENT MARKINGS & SIGNAGE SHALL COMPLY WITH THE "NEW MEXICO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS".
2. CONTRACTOR SHALL CONTACT "NM ONE CALL" AT 811 FOR ASSISTANCE IN LOCATING EXISTING UTILITIES AT LEAST 72 HRS PRIOR TO BEGINNING CONSTRUCTION.
3. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATION & PROTECTION OF EXISTING ABOVE & BELOW GROUND UTILITIES AND STRUCTURES. ANY AND ALL MAINS, LINES, OR INDIVIDUAL SERVICES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AT NO ADDITIONAL EXPENSE TO THE OWNER.
4. CONTRACTOR SHALL VERIFY ALL CONDITIONS & DIMENSIONS ON-SITE & NOTIFY THE ENGINEER OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES IN WRITING PRIOR TO BEGINNING WORK.
5. SIDEWALK CROSS-SLOPE SHALL NOT EXCEED 50:1.
6. ALL DIMENSIONAL CALL OUTS ARE MEASURED FROM FACE OF CURB.
7. FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY TO ENSURE A SMOOTH TRANSITION BOTH VERTICALLY & HORIZONTALLY FROM EXISTING TO PROPOSED PAVEMENT & CURB SECTIONS.
8. GUTTERS & PAVING SHALL BE INSTALLED IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE IN ALL AREAS.
9. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL LOCAL, STATE & FEDERAL CONSTRUCTION SAFETY REGULATIONS ARE FOLLOWED DURING THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
10. BENCHMARK LOCATION & ELEVATION SHALL BE VERIFIED PRIOR TO BEGINNING CONSTRUCTION. BENCHMARKS USED FOR SURVEY ARE LISTED ON THE EXISTING CONDITIONS PLAN.
11. BOUNDARY AND ALTA SURVEY INFORMATION TAKEN FROM SURVEY BY PRECISION SURVEYS, INC.
12. WATER & SEWER FEES (AS APPLICABLE) ARE TO BE PAID AT THE ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY OFFICE.
13. ALL DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF-SITE IN AN TDEQ APPROVED LANDFILL.
14. CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO NMDOT & ENGINEER FOR APPROVAL PRIOR TO COMMENCING CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
15. ALL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
16. CONTRACTOR TO ADJUST ALL EXISTING UTILITIES RIMS, COVERS, GRATES, AND WATER METER BOXES TO MATCH PROPOSED GRADES.
17. CONTRACTOR TO COORDINATE UTILITY RELOCATIONS WITH ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY.
18. ALL MATERIALS & PLANS ARE TO COMPLY WITH CURRENT CITY OF ALBUQUERQUE, ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY AND THE NEW MEXICO DEPARTMENT OF TRANSPORTATION AS APPLICABLE.
19. CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS OF STREET, DRAINAGE, WATER & SANITARY SEWER SYSTEMS AFTER INSTALLATION.
20. TRANSITION PROPOSED CONCRETE CURB, PAVEMENT AND SIDEWALK INTO EXISTING CURB, PAVEMENT AND SIDEWALK AND PROVIDE A FLUSH 1/8" TROVLED JOINT WHERE PROPOSED CONCRETE MEETS EXISTING CONCRETE.
21. ALL EXISTING AND PROPOSED LANDINGS, SIDEWALKS, RAMPS, ACCESSIBLE PARKING, DRIVEWAY CROSSWALKS AND ACCESSIBLE ROUTES SHALL COMPLY WITH A.D.A., CITY OF ALBUQUERQUE, AND NMDOT REQUIREMENTS. EXISTING INFRASTRUCTURE NOT COMPLYING SHALL BE REMOVED AND REPLACED TO MEET STANDARDS.
22. CONTRACTOR SHALL MAINTAIN ACCESS DRIVE INTO SITE FROM PAN AMERICAN FWY THROUGHOUT DEMOLITION AND CONSTRUCTION ACTIVITIES.
23. REFER TO PROJECT SPECIFICATION MANUAL FOR ADDITIONAL REQUIREMENTS AND PROJECT SPECIFICATIONS.



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE
 4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
LANDSCAPE PLAN

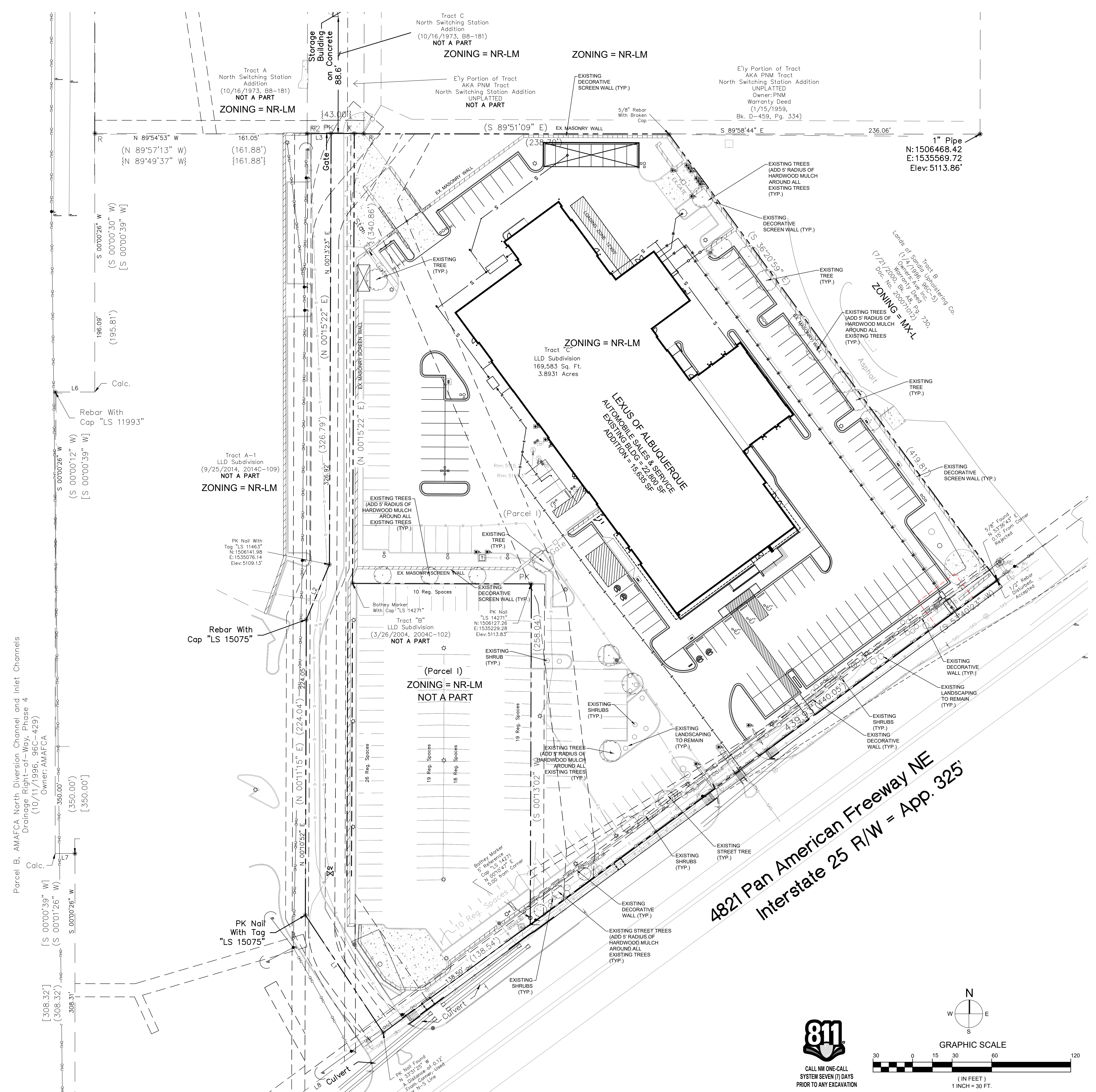
PROJECT #	DATE
2021-12	
FOR PERMIT	6/23/22
#1 PERMIT REVISIONS	7/13/22
#2 PERMIT REVISIONS	10/31/22
#3 PERMIT REVISIONS	12/16/22
#8 CLIENT CHANGES	12/30/22
#9 NMDOT REVISIONS	2/21/23
#10 PERMIT REVISIONS	3/10/23
#11 PERMIT REVISIONS	4/13/23
#12 PERMIT REVISIONS	4/26/23
#13 PERMIT REVISIONS	5/9/23
#14 PERMIT REVISIONS	5/24/23

C06.0

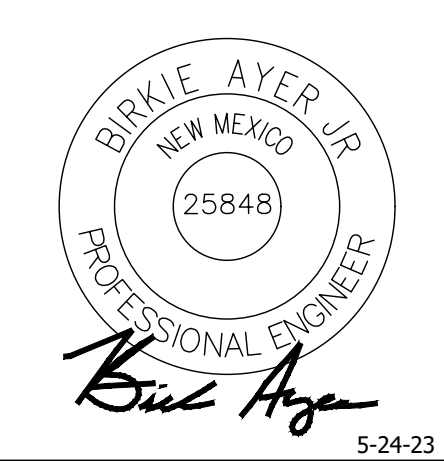
LANDSCAPE NOTES

1. ALL DISTURBED AREAS NOT SPECIFIED AS LANDSCAPE AREA OR HARDSCAPE TO BE MULCHED WITH ROCK MULCH.
2. CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO INSTALLATION. NOTIFY ENGINEER/ARCHITECT IF FIELD CONDITIONS WARRANT ADJUSTMENT OF PLANT LOCATIONS.
3. SEE SHEET C07.0 FOR LANDSCAPE DETAILS AND SPECIFICATIONS.
4. CONTRACTOR IS RESPONSIBLE FOR LOCATING EXISTING UTILITIES AND IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGE TO EXISTING UTILITIES THAT MAY OCCUR.
5. CONTRACTOR TO PROVIDE IRRIGATION PLAN TO ENGINEER/ARCHITECT FOR REVIEW AND APPROVAL.
6. COORDINATE LANDSCAPE INSTALLATION WITH IRRIGATION CONSTRUCTION.
7. ENSURE FINAL GRADES IN LANDSCAPED AND GRASSED AREAS PROVIDE POSITIVE DRAINAGE.
8. FINAL GRADES FOR GRASSED AND LANDSCAPED AREAS SHALL REQUIRE A MINIMUM OF 4" OF UNCOMPACTED, CLEAN TOPSOIL, FREE OF DEBRIS AND CONTAMINANTS, AND PREFERABLY OF NATIVE ORIGIN.
9. ALL TREE PITS, SHRUB BEDS AND PREPARED PLANTING BEDS ARE TO BE COMPLETELY EXCAVATED IN ACCORDANCE WITH THE PLANTING DETAILS.
10. SOIL REQUIRED FOR PLANTING MIXES SHALL BE PROVIDED BY THE CONTRACTOR WHO SHALL LOAD, HAUL, MIX AND DISTRIBUTE ALL TOPSOIL AND AMENDMENTS SUCH AS ORGANICS, HUMUS, COMPOSTED MATERIAL OR NATIVE MATERIAL, AS REQUIRED.
11. FOR NEW PLANTING AREAS, REMOVE ALL PAVEMENT, GRAVEL SUB BASE AND CONSTRUCTION DEBRIS. LOOSEN COMPACTED SOIL TO MINIMUM 24" DEPTH AND PROVIDE SOIL AMENDMENTS MIXED INTO THE TOP 18" MINIMUM OR FOR UNCOMPACTED AREAS, AMEND THE TOP 18" OF EXISTING SOIL TO MEET TOPSOIL PLANTING MIX STANDARDS.
12. PLANTING MIX SHALL BE NATURAL, FERTILE AGRICULTURAL TOPSOIL, CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT MAY BE DEVELOPED BY AMENDING THE EXISTING SOIL OR REMOVING THE EXISTING SOIL AND REPLACING WITH NEW SOIL. IT SHALL BE FREE OF STONES AND OTHER LIVE PLANTS OR THEIR ROOTS, STICKS AND/OR ANY OTHER THAN FRIABLE SOIL.
13. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR ANY FIELD ADJUSTMENTS REQUIRED TO AVOID DRAINAGE PATTERNS OR STRUCTURES, UTILITIES, UNDERGROUND OR OVERHEAD OR RELOCATIONS OF PROPOSED VEGETATION BECAUSE OF SITE CONDITIONS.
14. STONE MULCH TO BE 3/4" GRANITE "GREY ROCK" AT A DEPTH OF 3"-4", OR APPROVED EQUAL.
15. WOOD MULCH TO BE ORGANIC WOOD OR BARK MULCH AT A DEPTH OF 3"-4", OR APPROVED EQUAL.
16. INSTALL METAL EDGING, OR APPROVED EQUAL, BETWEEN AREAS OF STONE AND WOOD MULCH.
17. LEXUS OF ALBUQUERQUE OWNER WILL BE RESPONSIBLE FOR MAINTAINING LANDSCAPING AND ASSOCIATED IRRIGATION SYSTEMS ONCE CONSTRUCTION PROJECT IS COMPLETED.
18. LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
19. CONTRACTOR TO PROVIDE HARDWOOD MULCH AROUND ALL PROPOSED AND EXISTING TREES. MULCH RING TO BE A MIN. 5' RADIUS AROUND THE TREE.
20. CONTRACTOR TO REMOVE AND REPLACE ALL DEAD AND DISEASED PLANT MATERIAL.

EXISTING PLANT LEGEND				
SYMBOL	PLANT TYPE	QUANTITY	COVERAGE	TOTAL COVERAGE
●	DECIDUOUS SHADE TREE	23	491 SF	11,293 SF
○	EVERGREEN SHRUB	54	25 SF	1,350 SF
COVERAGE SUM			12,643 SF	



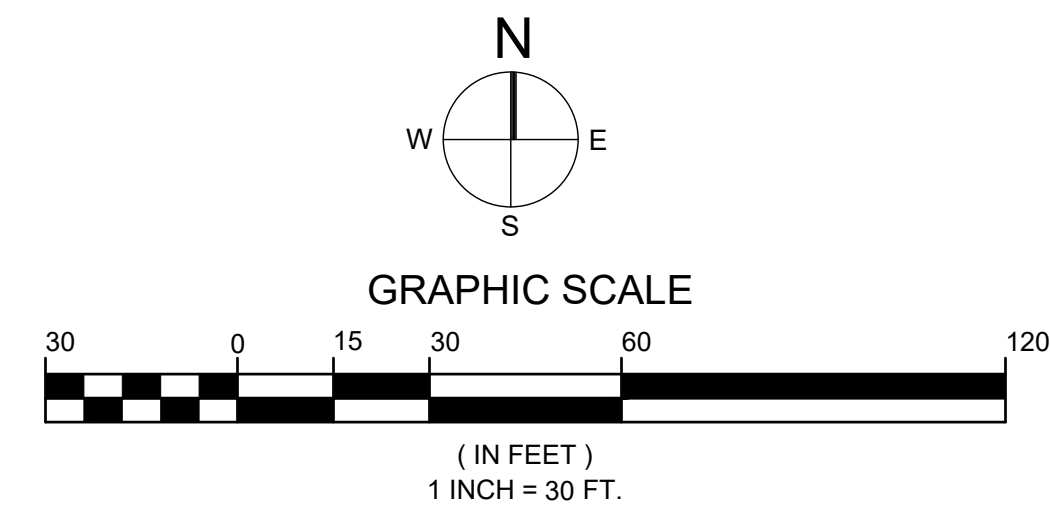
AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE
 4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
EXISTING LANDSCAPE PLAN

NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/11/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23



C06.1

PLANTING SPECIFICATIONS

GENERAL

- A. QUALIFICATIONS OF LANDSCAPE CONTRACTOR**
- ALL LANDSCAPE WORK SHOWN ON THESE PLANS SHALL BE PERFORMED BY A SINGLE FIRM SPECIALIZING IN LANDSCAPE PLANTING.
 - A LIST OF SUCCESSFULLY COMPLETED PROJECTS OF THIS TYPE, SIZE AND NATURE MAY BE REQUESTED BY THE OWNER FOR FURTHER QUALIFICATION MEASURES.
 - THE LANDSCAPE CONTRACTOR SHALL HOLD A VALID NURSERY AND FLORAL CERTIFICATE ISSUED BY THE NEW MEXICO DEPARTMENT OF AGRICULTURE AS WELL AS OPERATE UNDER A COMMERCIAL PESTICIDE APPLICATOR LICENSE ISSUED BY EITHER THE NEW MEXICO DEPARTMENT OF AGRICULTURE OR THE NEW MEXICO STRUCTURAL PEST CONTROL BOARD.
- B. SCOPE OF WORK**
- WORK COVERED BY THESE SECTIONS INCLUDES THE FURNISHING AND PAYMENT OF ALL MATERIALS, LABOR, SERVICES, EQUIPMENT, LICENSES, TAXES AND ANY OTHER ITEMS THAT ARE NECESSARY FOR THE EXECUTION, INSTALLATION AND COMPLETION OF ALL WORK, SPECIFIED HEREIN AND / OR SHOWN ON THE LANDSCAPE PLANS, NOTES, AND DETAILS.
 - ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE LAWS, CODES AND REGULATIONS REQUIRED BY AUTHORITIES HAVING JURISDICTION OVER SUCH WORK, INCLUDING ALL INSPECTIONS AND PERMITS REQUIRED BY FEDERAL, STATE AND LOCAL AUTHORITIES IN SUPPLY, TRANSPORTATION AND INSTALLATION OF MATERIALS.
 - THE LANDSCAPE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITY LINES (WATER, SEWER, ELECTRICAL, TELEPHONE, GAS, CABLE, TELEVISION, ETC.) PRIOR TO THE START OF ANY WORK.

PRODUCTS

- A. ALL MANUFACTURED PRODUCTS SHALL BE NEW.**
- B. CONTAINER AND BALLED-AND-BURLAPED PLANTS:**
- FURNISH NURSERY-GROWN PLANTS COMPLYING WITH ANSI Z60.1-2014. PROVIDE WELL-SHAPED, FULLY BRANCHED, HEALTHY, VIGOROUS STOCK FREE OF DISEASE, INSECTS, EGGS, LARVAE, AND DEFECTS SUCH AS KNOTS, SUN SCALD, INJURIES, ABRASIONS, AND DISFIGUREMENT. ALL PLANTS WITHIN A SPECIES SHALL HAVE SIMILAR SIZE, AND SHALL BE OF A FORM TYPICAL FOR THE SPECIES. ALL TREES SHALL BE OBTAINED FROM SOURCES WITHIN 200 MILES OF THE PROJECT SITE, AND WITH SIMILAR CLIMATIC CONDITIONS.
 - ROOT SYSTEMS SHALL BE HEALTHY, DENSELY BRANCHED ROOT SYSTEMS, NON-POT-BOUND, FREE FROM ENCRUSTING AND/OR GIRDLING ROOTS, AND FREE FROM ANY OTHER ROOT DEFECTS (SUCH AS J-SHAPED ROOTS).
 - ANY PLANT DEEMED UNACCEPTABLE BY THE LANDSCAPE ARCHITECT OR OWNER SHALL BE IMMEDIATELY REMOVED FROM THE SITE AND SHALL BE REPLACED WITH AN ACCEPTABLE PLANT OF LIKE TYPE AND SIZE AT THE CONTRACTOR'S OWN EXPENSE. ANY PLANTS APPEARING TO BE UNHEALTHY, EVEN IF DETERMINED TO STILL BE ALIVE, SHALL NOT BE ACCEPTED. THE LANDSCAPE ARCHITECT AND OWNER SHALL BE THE SOLE JUDGES AS TO THE ACCEPTABILITY OF PLANT MATERIAL.
 - ALL TREES SHALL BE STANDARD IN FORM, UNLESS OTHERWISE SPECIFIED. TREES WITH CENTRAL LEADERS WILL NOT BE ACCEPTED IF LEADER IS DAMAGED OR REMOVED. PRUNE ALL DAMAGED TWIGS AFTER PLANTING.
 - CALIPER MEASUREMENTS FOR STANDARD (SINGLE TRUNK) TREES SHALL BE AS FOLLOWS: SIX INCHES ABOVE THE ROOT FLARE FOR TREES UP TO AND INCLUDING FOUR INCHES IN CALIPER, AND TWELVE INCHES ABOVE THE ROOT FLARE FOR TREES EXCEEDING FOUR INCHES IN CALIPER.
 - MULTI-TRUNK TREES SHALL BE MEASURED BY THEIR OVERALL HEIGHT, MEASURED FROM THE TOP OF THE ROOT BALL.
 - ANY TREE OR SHRUB SHOWN TO HAVE EXCESS SOIL PLACED ON TOP OF THE ROOT BALL, SO THAT THE ROOT FLARE HAS BEEN COMPLETELY COVERED, SHALL BE REJECTED.
- C. SOD:** PROVIDE WELL-ROOTED SOD OF THE VARIETY NOTED ON THE PLANS. SOD SHALL BE CUT FROM HEALTHY, MATURE TURF WITH SOIL THICKNESS OF 3/4" TO 1". EACH PALLET OF SOD SHALL BE ACCOMPANIED BY A CERTIFICATE FROM SUPPLIER STATING THE COMPOSITION OF THE SOD.
- D. SEED:** PROVIDE BLEND OF SPECIES AND VARIETIES AS NOTED ON THE PLANS, WITH MAXIMUM PERCENTAGES OF PURITY, GERMINATION, AND MINIMUM PERCENTAGE OF WEED SEED AS INDICATED ON PLANS. EACH BAG OF SEED SHALL BE ACCOMPANIED BY A TAG FROM THE SUPPLIER INDICATING THE COMPOSITION OF THE SEED.
- E. TOPSOIL:** SANDY TO CLAY LOAM TOPSOIL, FREE OF STONES LARGER THAN 1/2" INCH, FOREIGN MATTER, PLANTS, ROOTS, AND SEEDS.
- F. COMPOST:** WELL-COMPOSTED, STABLE, AND WEED-FREE ORGANIC MATTER, pH RANGE OF 5.5 TO 8; MOISTURE CONTENT 35 TO 55 PERCENT BY WEIGHT; 100 PERCENT PASSING THROUGH 3/4" INCH SIEVE; SOLUBLE SALT CONTENT OF 5 TO 10 CCM/CM3; NOT EXCEEDING 1.5 PERCENT INERT CONTAMINANTS AND FREE OF SUBSTANCES TOXIC TO PLANTINGS. NO MANURE OR ANIMAL-BASED PRODUCTS SHALL BE USED.
- G. FERTILIZER:** GRANULAR FERTILIZER CONSISTING OF NITROGEN, PHOSPHORUS, POTASSIUM, AND OTHER NUTRIENTS IN PROPORTIONS, AMOUNTS, AND RELEASE RATES RECOMMENDED IN A SOIL REPORT FROM A QUALIFIED SOIL TESTING AGENCY (SEE BELOW).
- H. MULCH:** SIZE AND TYPE AS INDICATED ON PLANS, FREE FROM DELETERIOUS MATERIALS AND SUITABLE AS A TOP DRESSING OF TREES AND SHRUBS.
- I. WEED FABRIC:** 5 MIL, KNOCK, WOVEN, NEEDLE-PUNCHED FABRIC, SUCH AS DEWITT PROS LANDSCAPE FABRIC (OR APPROVED EQUAL).
- J. TREE STAKING AND GUYING**
- STAKES: 6' LONG GREEN METAL T-POSTS.
 - GUY AND TIE WIRE: ASTM A 641, CLASS 1, GALVANIZED-STEEL WIRE, 2-STRAND, TWISTED, 0.106 INCH DIAMETER.
 - STRAP CHAFING GUARD: REINFORCED NYLON OR CANVAS AT LEAST 1-1/2 INCH WIDE, WITH GROMMETS TO PROTECT TREE TRUNKS FROM DAMAGE.
- K. STEEL EDGING:** PROFESSIONAL STEEL EDGING, 1/4 GAUGE THICK X 4 INCHES WIDE, FACTORY PAINTED DARK GREEN. ACCEPTABLE MANUFACTURERS INCLUDE COLMIET OR APPROVED EQUAL.
- L. PRE-EMERGENT HERBICIDES:** ANY GRANULAR, NON-STAINING PRE-EMERGENT HERBICIDE THAT IS LABELED FOR THE SPECIFIC ORNAMENTALS OR TURF ON WHICH IT WILL BE UTILIZED. PRE-EMERGENT HERBICIDES SHALL BE APPLIED PER THE MANUFACTURER'S LABELED RATES.
- METHODS**
- A. SOIL PREPARATION**
- BEFORE STARTING WORK, THE LANDSCAPE CONTRACTOR SHALL VERIFY THAT THE GRADE OF ALL LANDSCAPE AREAS ARE WITHIN +0.1' OF FINISH GRADE. THE CONTRACTOR SHALL NOTIFY THE OWNER IMMEDIATELY SHOULD ANY DISCREPANCIES EXIST.
 - SOIL TESTING:
 - AFTER FINISH GRADES HAVE BEEN ESTABLISHED, CONTRACTOR SHALL HAVE SOIL SAMPLES TESTED BY AN ESTABLISHED SOIL TESTING LABORATORY FOR THE FOLLOWING: SOIL TEXTURAL CLASS, GENERAL SOIL FERTILITY, pH, ORGANIC MATTER CONTENT, SALT (CEC), LIME, SODIUM ADSORPTION RATIO (SAR) AND BORON CONTENT. EACH SAMPLE SUBMITTED SHALL CONTAIN NO LESS THAN ONE QUART OF SOIL.
 - CONTRACTOR SHALL ALSO SUBMIT THE PROJECT'S PLANT LIST TO THE LABORATORY ALONG WITH THE SOIL SAMPLES.
 - THE SOIL REPORT PRODUCED BY THE LABORATORY SHALL CONTAIN RECOMMENDATIONS FOR THE FOLLOWING (AS APPROPRIATE): GENERAL SOIL PREPARATION AND BACKFILL MIXES, PRE-PLANT FERTILIZER APPLICATIONS, AND ANY OTHER SOIL RELATED ISSUES. THE REPORT SHALL ALSO PROVIDE A FERTILIZER PROGRAM FOR THE ESTABLISHMENT PERIOD AND FOR LONG-TERM MAINTENANCE.
 - THE CONTRACTOR SHALL INSTALL SOIL AMENDMENTS AND FERTILIZERS PER THE SOILS REPORT RECOMMENDATIONS. ANY CHANGE IN COST DUE TO THE SOIL REPORT RECOMMENDATIONS, EITHER INCREASE OR DECREASE, SHALL BE SUBMITTED TO THE OWNER WITH THE REPORT.
 - FOR BIDDING PURPOSES ONLY, THE SOIL PREPARATION SHALL CONSIST OF THE FOLLOWING:
 - TURF: INCORPORATE THE FOLLOWING AMENDMENTS INTO THE TOP 8" OF SOIL BY MEANS OF ROTOTILLING AFTER CROSS-RIPPING:
 - NITROGEN STABILIZED ORGANIC AMENDMENT - 4 CU. YDS. PER 1,000 S.F.
 - AMMONIUM PHOSPHATE 16-20-0 - 15 LBS PER 1,000 S.F.
 - AGRICULTURAL GYPSUM - 100 LBS PER 1,000 S.F.
 - TREES, SHRUBS, AND PERENNIALS: INCORPORATE THE FOLLOWING AMENDMENTS INTO THE TOP 8" OF SOIL BY MEANS OF ROTOTILLING AFTER CROSS-RIPPING:
 - NITROGEN STABILIZED ORGANIC AMENDMENT - 4 CU. YDS. PER 1,000 S.F.
 - 12-12-12 FERTILIZER - 10 LBS. PER CU. YD.
 - AGRICULTURAL GYPSUM - 10 LBS. PER CU. YD.
 - IRON SULFATE - 2 LBS. PER CU. YD.
 - CONTRACTOR SHALL ENSURE THAT THE GRADE IN SOD AREAS SHALL BE 1" BELOW FINISH GRADE BEFORE INSTALLING SOIL AMENDMENTS, AND 2" BELOW FINISH GRADE IN SHRUB AREAS BEFORE INSTALLING SOIL AMENDMENTS. MULCH COVER WITHIN 6" OF CONCRETE WALKS AND CURBS SHALL NOT PROTRUDE ABOVE THE FINISH SURFACE OF THE WALKS AND CURBS. MULCH COVER WITHIN 12" OF WALLS SHALL BE AT LEAST 3" LOWER THAN THE TOP OF WALL.
 - ONCE SOIL PREPARATION IS COMPLETE, THE LANDSCAPE CONTRACTOR SHALL ENSURE THAT THERE ARE NO DEBRIS, TRASH, OR STONES LARGER THAN 1" REMAINING IN THE TOP 6" OF SOIL.

- B. GENERAL PLANTING**
- REMOVE ALL NURSERY TAGS AND STAKES FROM PLANTS.
 - EXCEPT IN AREAS TO BE PLANTED WITH ORNAMENTAL GRASSES, APPLY PRE-EMERGENT HERBICIDES AT THE MANUFACTURER'S RECOMMENDED RATE.
 - TRENCHING NEAR EXISTING TREES:
 - CONTRACTOR SHALL NOT DISTURB ROOTS 1-1/2" AND LARGER IN DIAMETER WITHIN THE CRITICAL ROOT ZONE (CRZ) OF EXISTING TREES, AND SHALL EXERCISE ALL POSSIBLE CARE AND PRECAUTIONS TO AVOID INJURY TO TREE ROOTS, TRUNKS, AND BRANCHES. THE CRZ IS DEFINED AS A CIRCULAR AREA EXTENDING OUTWARD FROM THE TREE TRUNK, WITH A RADIUS EQUAL TO 1" FOR EVERY 1" OF TRUNK DIAMETER AT BREAST-HIGHT (4.5) ABOVE THE AVERAGE GRADE AT THE TRUNK.
 - ALL EXCAVATION WITHIN THE CRZ SHALL BE PERFORMED USING HAND TOOLS. NO MACHINE EXCAVATION OR TRENCHING OF ANY KIND SHALL BE ALLOWED WITHIN THE CRZ.
 - ALTER ALIGNMENT OF PIPE TO AVOID TREE ROOTS 1-1/2" AND LARGER IN DIAMETER. WHERE TREE ROOTS 1-1/2" AND LARGER IN DIAMETER ARE ENCOUNTERED IN THE FIELD, TUNNEL UNDER SUCH ROOTS, WRAP EXPOSED ROOTS WITH SEVERAL LAYERS OF BURLAP AND KEEP MOIST. CLOSE ALL TRENCHES WITHIN THE CANOPY DRIP LINES WITHIN 24 HOURS.
 - ALL SEVERED ROOTS SHALL BE HAND PRUNED WITH SHARP TOOLS AND ALLOWED TO AIR-DRY. DO NOT USE ANY SORT OF SEALERS OR WOUND PAINTS.

- C. TREE PLANTING**
- TREE PLANTING HOLES SHALL BE EXCAVATED TO MINIMUM WIDTH OF TWO TIMES THE WIDTH OF THE ROOTBALL, AND TO A DEPTH EQUAL TO THE DEPTH OF THE ROOTBALL LESS TWO TO FOUR INCHES.
 - SCARIFY THE SIDES AND BOTTOM OF THE PLANTING HOLE PRIOR TO THE PLACEMENT OF THE TREE. REMOVE ANY GLAZING THAT MAY HAVE BEEN CAUSED DURING THE EXCAVATION OF THE HOLE.
 - FOR CONTAINER AND BOX TREES, TO REMOVE ANY POTENTIALLY GIRDLING ROOTS AND OTHER ROOT DEFECTS, THE CONTRACTOR SHALL SHAVE A 1" LAYER OFF OF THE SIDES AND BOTTOM OF THE ROOTBALL OF ALL TREES JUST BEFORE PLACING INTO THE PLANTING PIT. DO NOT "TEASE" ROOTS OUT FROM THE ROOTBALL.
 - INSTALL THE TREE ON UNDISTURBED SUBGRADE SO THAT THE TOP OF THE ROOTBALL IS TWO TO FOUR INCHES ABOVE THE SURROUNDING GRADE.
 - BACKFILL THE TREE HOLE UTILIZING THE EXISTING TOPSOIL FROM ON-SITE. ROCKS LARGER THAN 1" DIA. AND ALL OTHER DEBRIS SHALL BE REMOVED FROM THE SOIL PRIOR TO THE BACKFILL. SHOULD ADDITIONAL SOIL BE REQUIRED TO ACCOMPLISH THIS TASK, USE STORED TOPSOIL FROM ON-SITE OR IMPORT ADDITIONAL TOPSOIL FROM OFF-SITE AT NO ADDITIONAL COST TO THE OWNER. IMPORTED TOPSOIL SHALL BE OF SIMILAR TEXTURAL CLASS AND COMPOSITION IN THE ON-SITE SOIL.
 - THE TOTAL NUMBER OF TREE STAKES (BEYOND THE MINIMUMS LISTED BELOW) WILL BE LEFT TO THE LANDSCAPE CONTRACTOR'S DISCRETION. SHOULD ANY TREES FALL OR LEAN, THE LANDSCAPE CONTRACTOR SHALL STRAIGHTEN THE TREE, OR REPLACE IT SHOULD IT BECOME DAMAGED. TREE STAKING SHALL ADHERE TO THE FOLLOWING GUIDELINES:
 - 1"-2" TREES: TWO STAKES PER TREE
 - 2-1/2"-4" TREES: THREE STAKES PER TREE
 - TREES OVER 4" CALIPER: GUY AS NEEDED
 - MULTI-TRUNK TREES: THREE STAKES PER TREE MINIMUM. QUANTITY AND POSITIONS AS NEEDED TO STABILIZE THE TREE
 - UPON COMPLETION OF PLANTING, CONSTRUCT AN EARTH WATERING BASIN AROUND THE TREE. COVER THE INTERIOR OF THE TREE RING WITH THE WEED BARRIER CLOTH AND TOPRESS WITH MULCH (TYPE AND DEPTH PER PLANS).

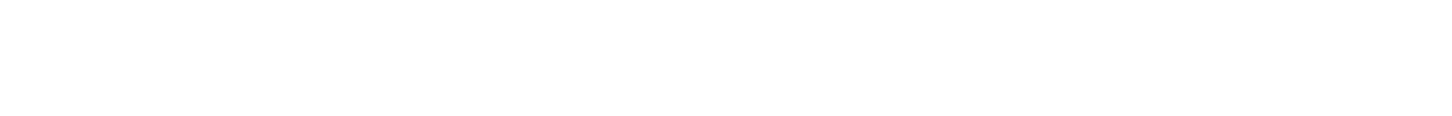
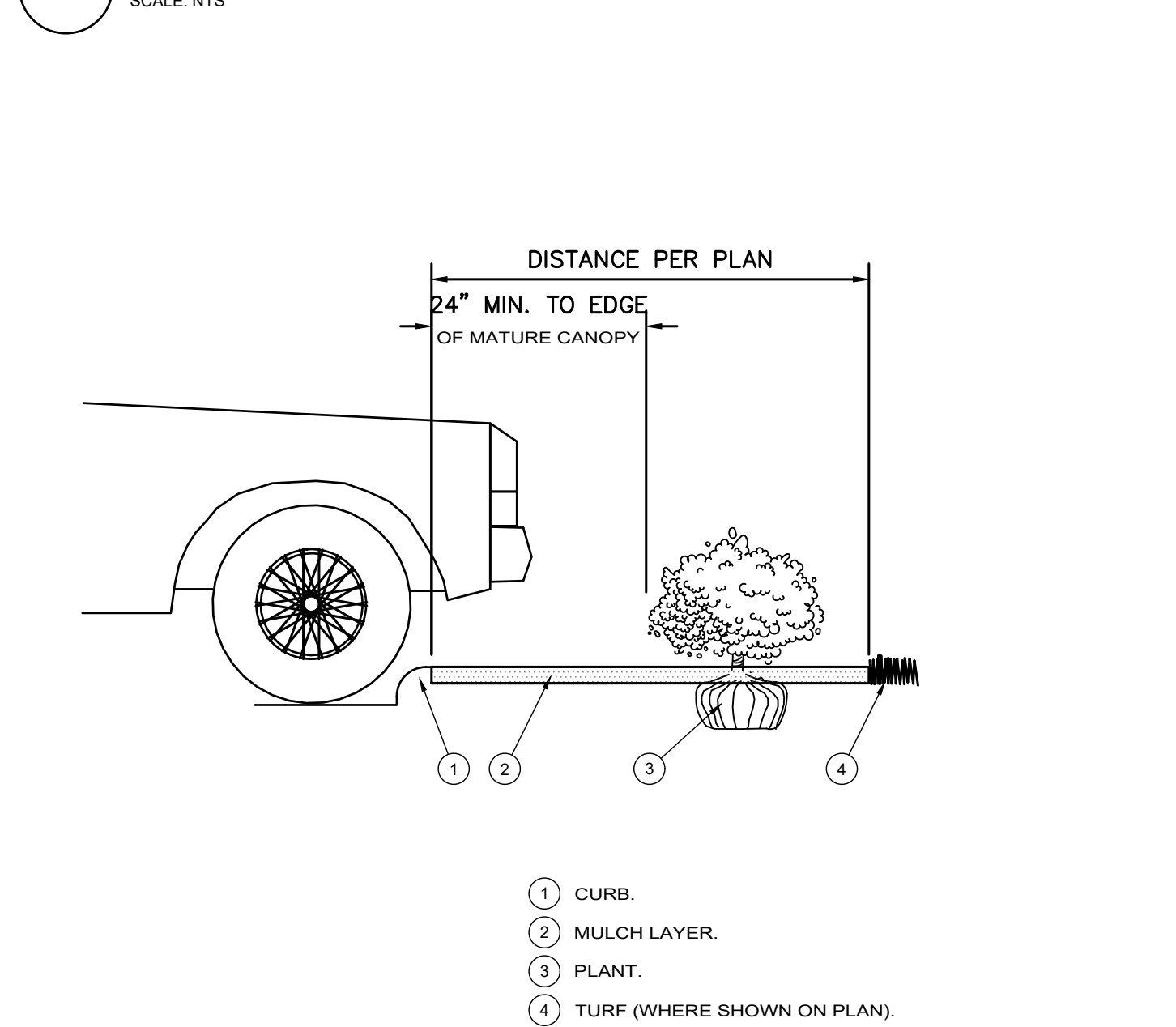
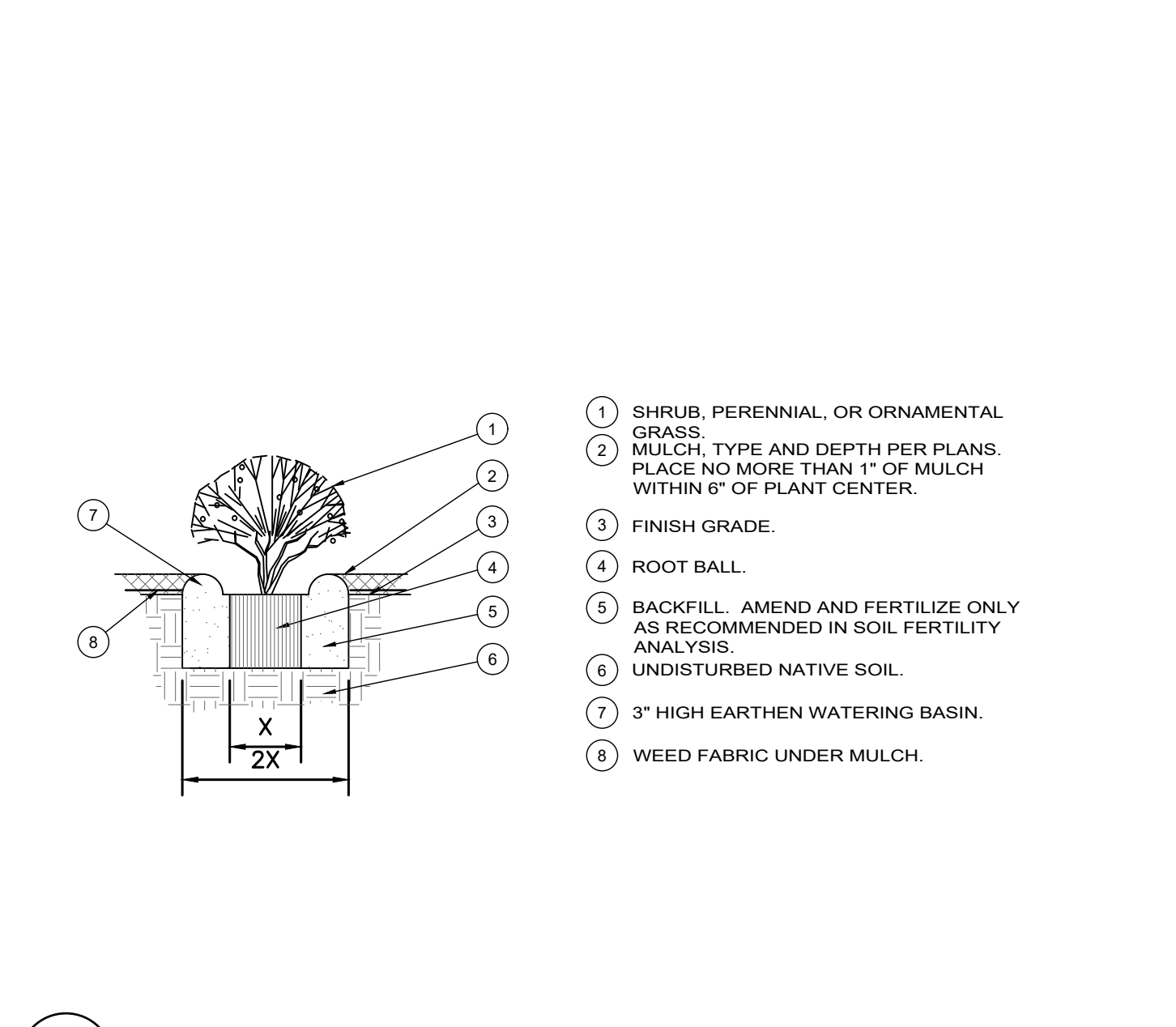
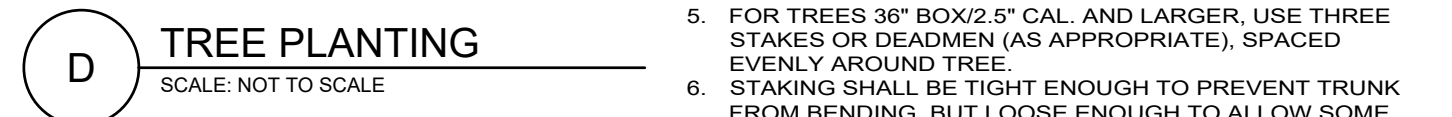
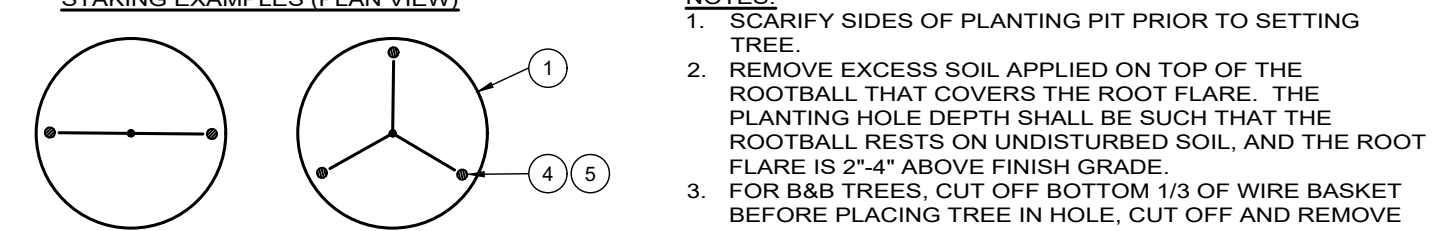
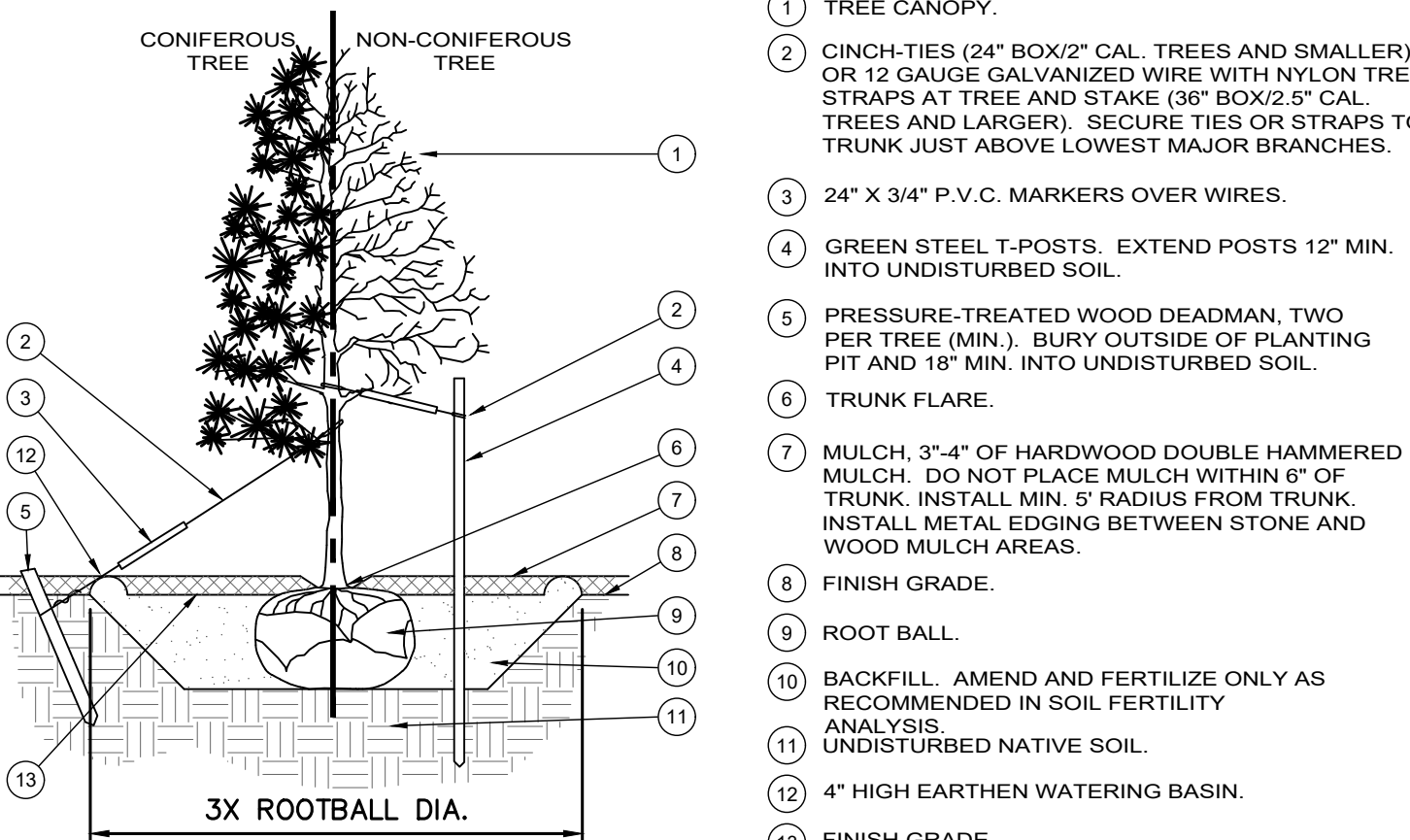
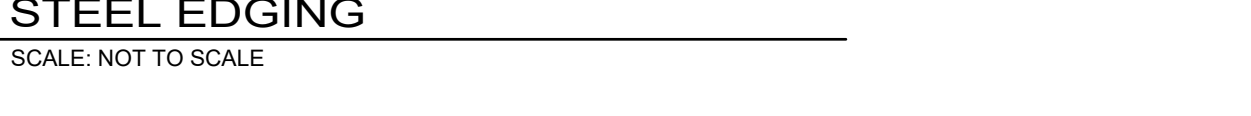
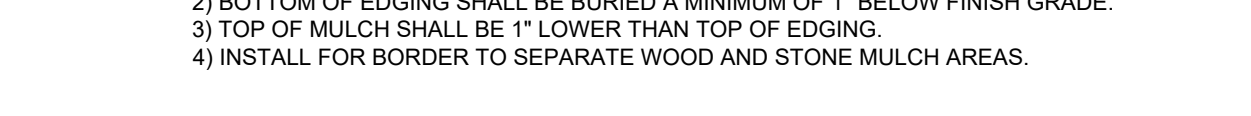
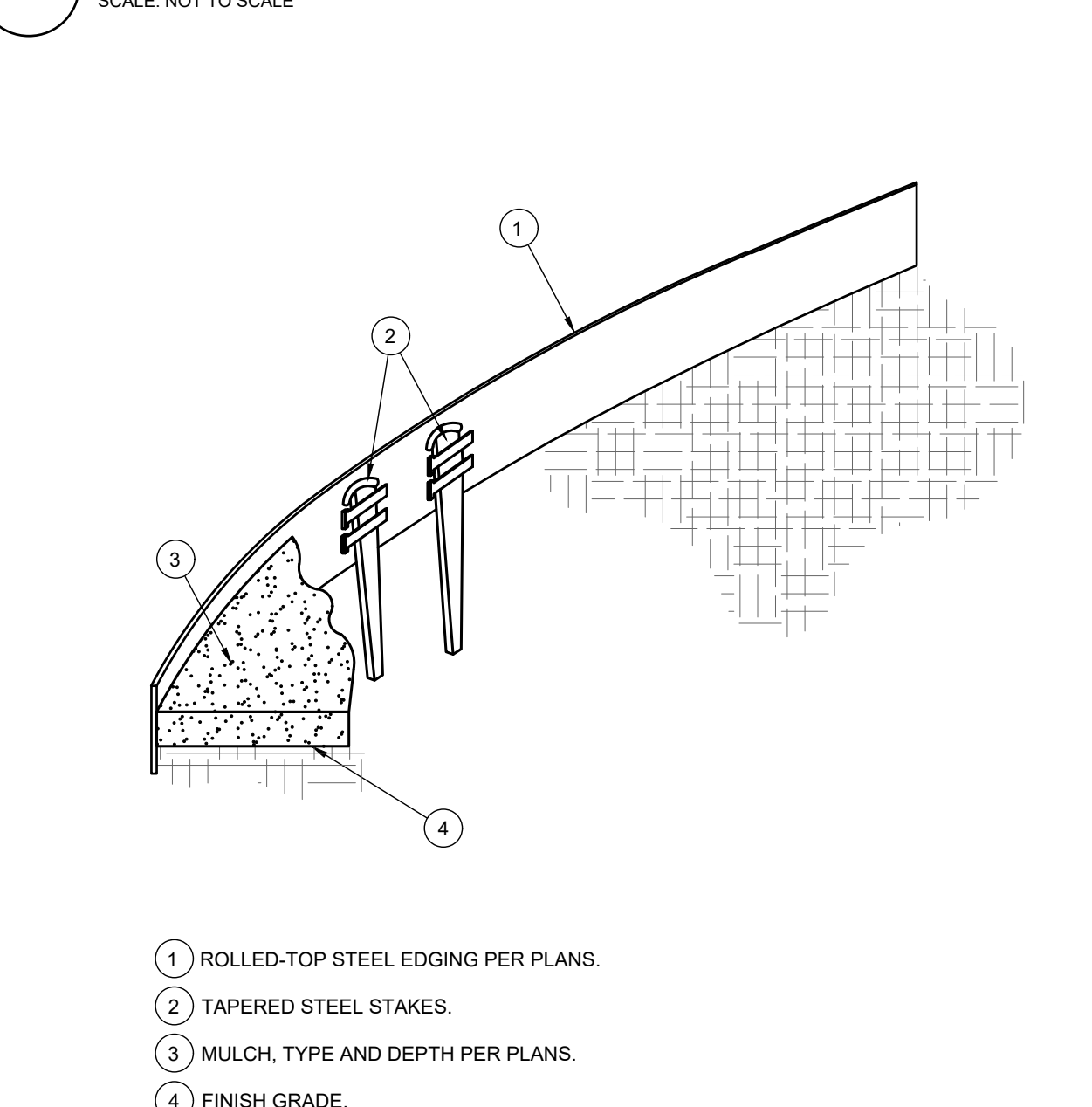
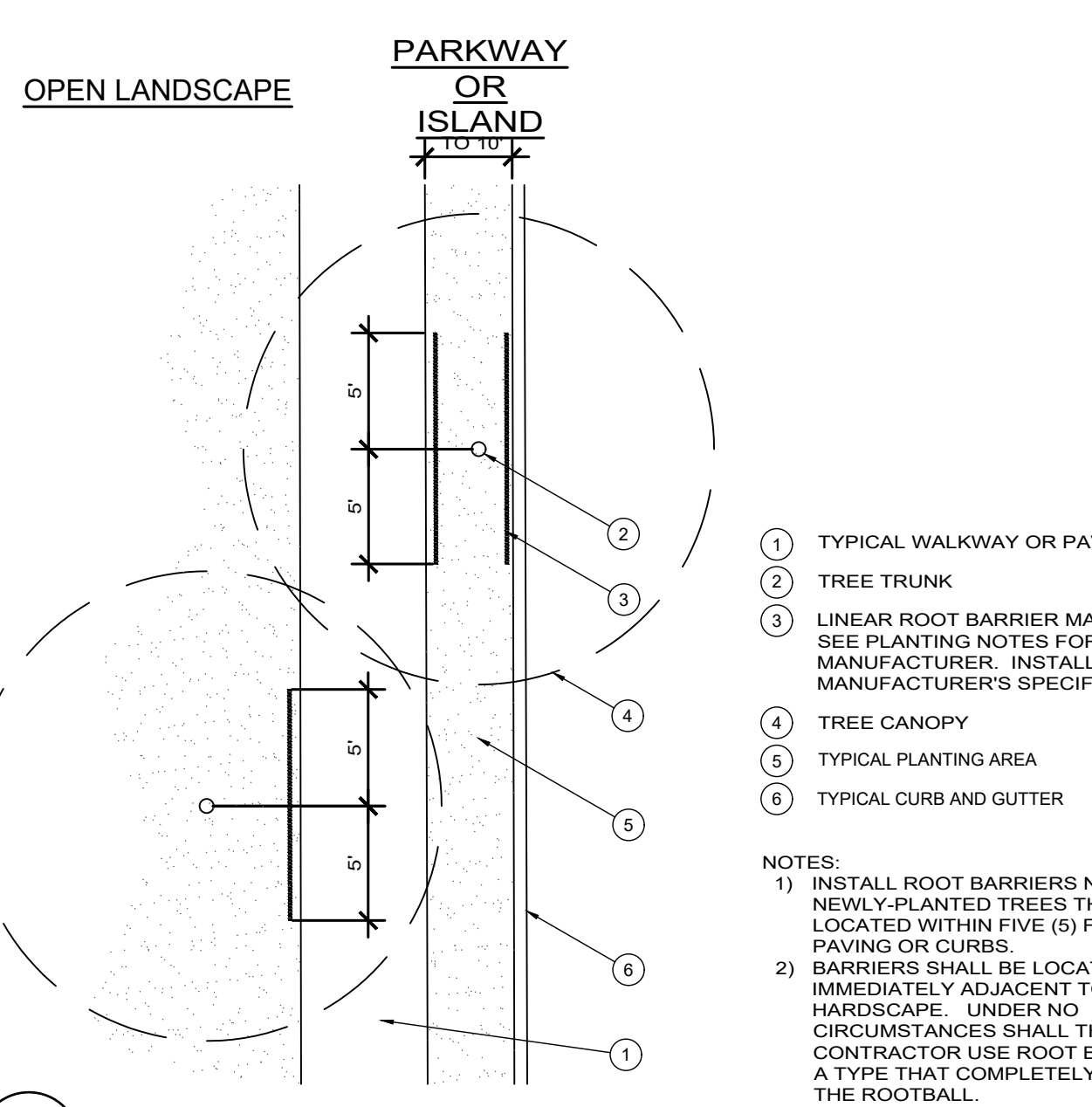
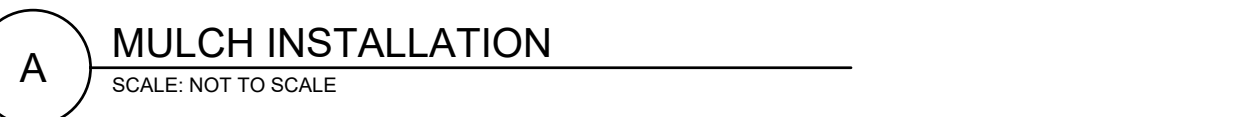
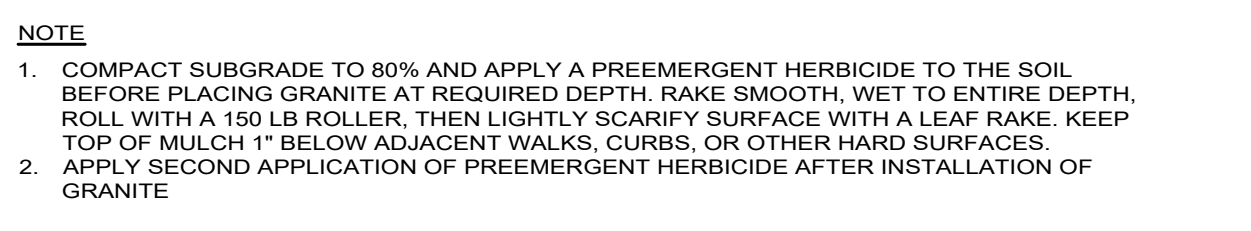
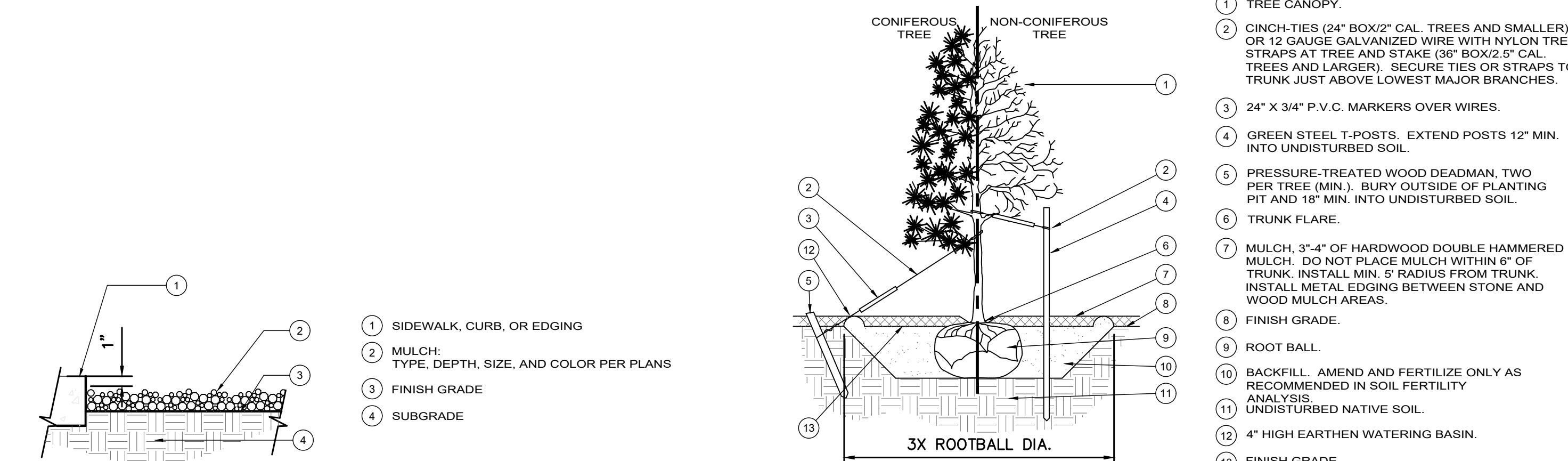
- D. SHRUB, PERENNIAL AND GROUND COVER PLANTING**
- DIG THE PLANTING HOLES TWICE AS WIDE AND 2" LESS DEEP THAN EACH PLANT'S ROOTBALL. INSTALL THE PLANT IN THE HOLE. BACKFILL AROUND THE PLANT WITH SOIL AMENDED PER SOIL TEST RECOMMENDATIONS.
 - INSTALL THE WEED BARRIER CLOTH, OVERLAPPING IT AT THE ENDS. UTILIZE STEEL STAPLES TO KEEP THE WEED BARRIER CLOTH IN PLACE.
 - WHEN PLANTING IS COMPLETE, INSTALL MULCH (TYPE AND DEPTH PER PLANS) OVER ALL PLANTING BEDS, COVERING THE ENTIRE PLANTING AREA.
- E. HYDROMULCHING**
- TURF HYDROMULCH MIX (PER 1,000 SF) SHALL BE AS FOLLOWS:
 - WINTER MIX (OCTOBER 1 - MARCH 31)
 - 50# CELLULOSE FIBER MULCH
 - 2# UNHULLED BERMUDA SEED
 - 2# ANNUAL RYE SEED
 - 5# 15-15-15 WATER SOLUBLE FERTILIZER
 - SUMMER MIX (APRIL 1 - SEPTEMBER 30)
 - 50# CELLULOSE FIBER MULCH
 - 2# HULLED BERMUDA SEED
 - 5# 15-15-15 WATER SOLUBLE FERTILIZER

- F. CLEAN UP**
- DURING LANDSCAPE PREPARATION AND PLANTING, KEEP ALL PAVEMENT CLEAN AND ALL WORK AREAS IN A NEAT, ORDERLY CONDITION.
 - NEARLY ALL DEBRIS, WOODEN, AND METAL DEBRIS SHALL BE REMOVED FROM THE PROJECT SITE.
- G. INSPECTION AND ACCEPTANCE**
- UPON COMPLETION OF THE WORK, THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE SITE CLEAN, FREE OF DEBRIS AND TRASH, AND SUITABLE FOR USE AS INTENDED. THE LANDSCAPE CONTRACTOR SHALL THEN REQUEST AN INSPECTION BY THE OWNER TO DETERMINE FINAL ACCEPTABILITY.
 - WHEN THE INSPECTED PLANTING WORK DOES NOT COMPLY WITH THE CONTRACT DOCUMENTS, THE LANDSCAPE CONTRACTOR SHALL REPLACE AND/OR REPAIR THE REJECTED WORK TO THE OWNER'S SATISFACTION WITHIN 24 HOURS.
 - THE LANDSCAPE MAINTENANCE PERIOD WILL NOT COMMENCE UNTIL THE LANDSCAPE WORK HAS BEEN RE-INSPECTED BY THE OWNER AND FOUND TO BE ACCEPTABLE. AT THAT TIME, A WRITTEN NOTICE OF FINAL ACCEPTANCE WILL BE ISSUED BY THE OWNER, AND THE MAINTENANCE AND GUARANTEE PERIODS WILL COMMENCE.

- H. LANDSCAPE MAINTENANCE**
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL WORK SHOWN ON THESE PLANS FOR 90 DAYS BEYOND FINAL ACCEPTANCE OF ALL LANDSCAPE WORK BY THE OWNER. LANDSCAPE MAINTENANCE SHALL INCLUDE WEEKLY SITE VISITS FOR THE FOLLOWING ACTIONS (AS APPROPRIATE): PROPER PRUNING, RESTAKING OF TREES, RESETTING OF PLANTS THAT HAVE SETTLED, MOVING AND AERATION OF LAWN, WEEDING, RESEEDING AREAS WHICH HAVE NOT GERMINATED WELL, TREATING FOR INSECTS AND DISEASES, REPLACEMENT OF MULCH, REMOVAL OF LITTER, REPAIRS TO THE IRRIGATION SYSTEM DUE TO FAULTY PARTS AND/OR WORKMANSHIP, AND THE APPROPRIATE WATERING OF ALL PLANTINGS. THE LANDSCAPE CONTRACTOR SHALL MAINTAIN THE IRRIGATION SYSTEM IN PROPER WORKING ORDER, WITH SCHEDULING ADJUSTMENTS BY SEASON TO MAXIMIZE WATER CONSERVATION.
 - SHOULD SEEDING AND/OR SODDED AREAS NOT BE COVERED BY AN AUTOMATIC IRRIGATION SYSTEM, THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR WATERING THESE AREAS AND OBTAINING A FULL, HEALTHY STAND OF GRASS AT NO ADDITIONAL COST TO THE OWNER.
 - TO ACHIEVE FINAL ACCEPTANCE AT THE END OF THE MAINTENANCE PERIOD, ALL OF THE FOLLOWING CONDITIONS MUST OCCUR:
 - THE LANDSCAPE SHALL SHOW ACTIVE, HEALTHY GROWTH WITH EXCEPTIONS MADE FOR SEASONAL DORMANCY. ALL PLANTS NOT MEETING THIS CONDITION SHALL BE REJECTED AND REPLACED BY HEALTHY PLANT MATERIAL PRIOR TO FINAL ACCEPTANCE.
 - ALL HARDSCAPE SHALL BE CLEANED PRIOR TO FINAL ACCEPTANCE.
 - SODDED AREAS MUST BE ACTIVELY GROWING AND MUST REACH A MINIMUM HEIGHT OF 1 1/2 INCHES BEFORE FIRST MOWING. HYDROMULCHED AREAS SHALL SHOW ACTIVE, HEALTHY GROWTH. BARE AREAS LARGER THAN TWELVE SQUARE INCHES MUST BE RESODDED OR RESEEDED (AS APPROPRIATE) PRIOR TO FINAL ACCEPTANCE. ALL SODDED TURF SHALL BE NEATLY MOWED.

- I. WARRANTY PERIOD, PLANT GUARANTEE AND REPLACEMENTS**
- THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL TREES, SHRUBS, PERENNIALS, SOD, SEEDING/HYDROMULCHED AREAS, AND IRRIGATION SYSTEMS FOR A PERIOD OF ONE YEAR FROM THE DATE OF THE OWNER'S FINAL ACCEPTANCE (90 DAYS FOR ANNUAL PLANTS). THE CONTRACTOR SHALL REPLACE, AT HIS OWN EXPENSE AND TO THE SATISFACTION OF THE OWNER, ANY PLANTS WHICH DIE IN THAT TIME, OR REPAIR ANY PORTIONS OF THE IRRIGATION SYSTEM WHICH OPERATE IMPROPERLY.
 - AFTER THE INITIAL MAINTENANCE PERIOD AND DURING THE GUARANTEE PERIOD, THE LANDSCAPE CONTRACTOR SHALL ONLY BE RESPONSIBLE FOR REPLACEMENT OF PLANTS WHEN PLANT DEATH CANNOT BE ATTRIBUTED DIRECTLY TO COVERABLE OR OTHER DAMAGE BY HUMAN ACTIONS.

- J. PROVIDE A MINIMUM OF (2) COPIES OF RECORD DRAWINGS TO THE OWNER UPON COMPLETION OF WORK. A RECORD DRAWING IS A RECORD OF ALL CHANGES THAT OCCURRED IN THE FIELD AND THAT ARE DOCUMENTED THROUGH CHANGE ORDERS, ADDENDA, OR CONTRACTOR/CONSULTANT DRAWING MARKUPS.**



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnson Street
 Rock Hill, SC 29730
 Phone: 803-328-5858

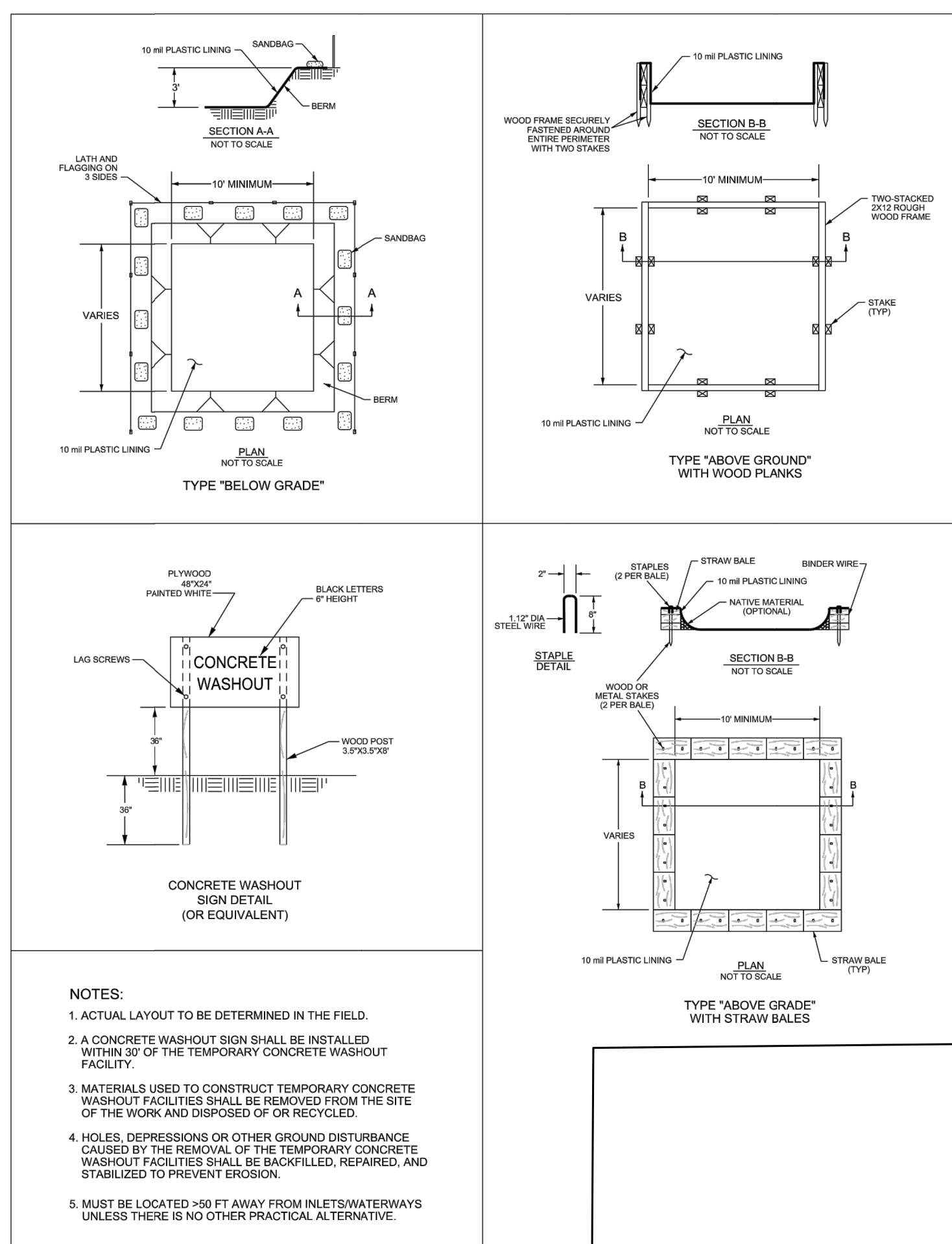
BIRKIE AYER JR.
 NEW MEXICO
 25848
 PROFESSIONAL ENGINEER
 Bill Ayer
 5-24-23

PROJECT NAME:
LEXUS OF ALBUQUERQUE
 4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
LANDSCAPE DETAILS

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

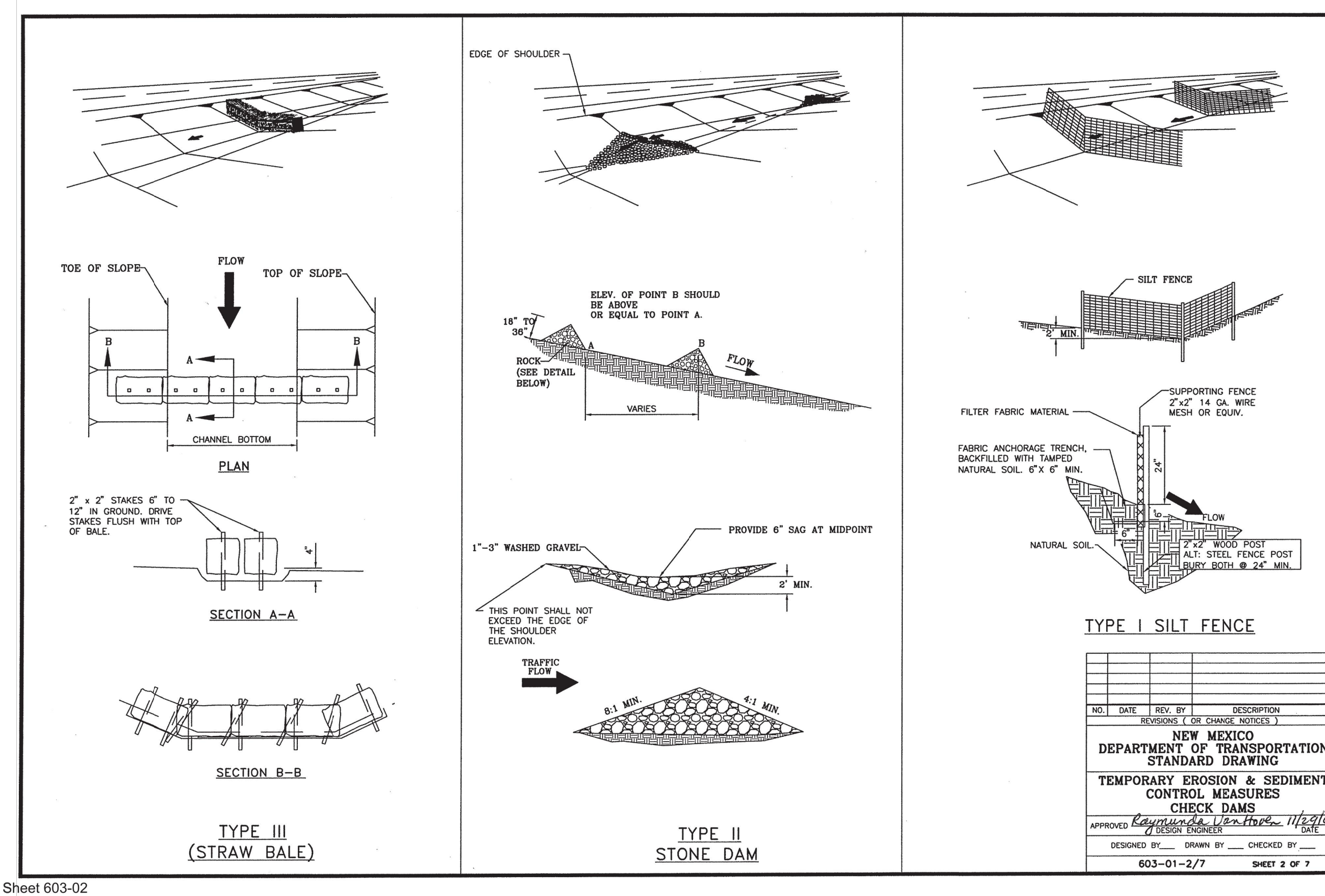
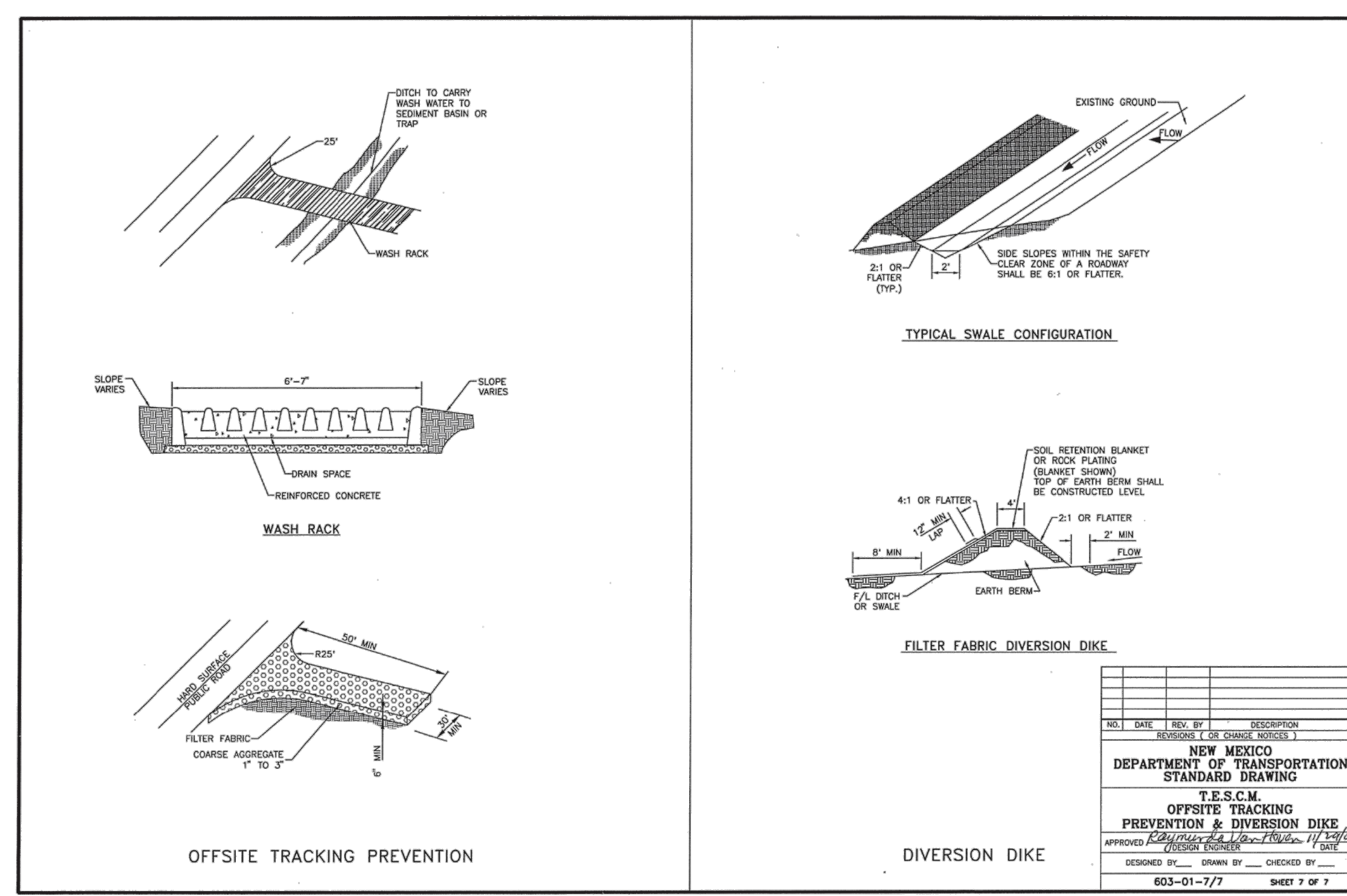
C07.0



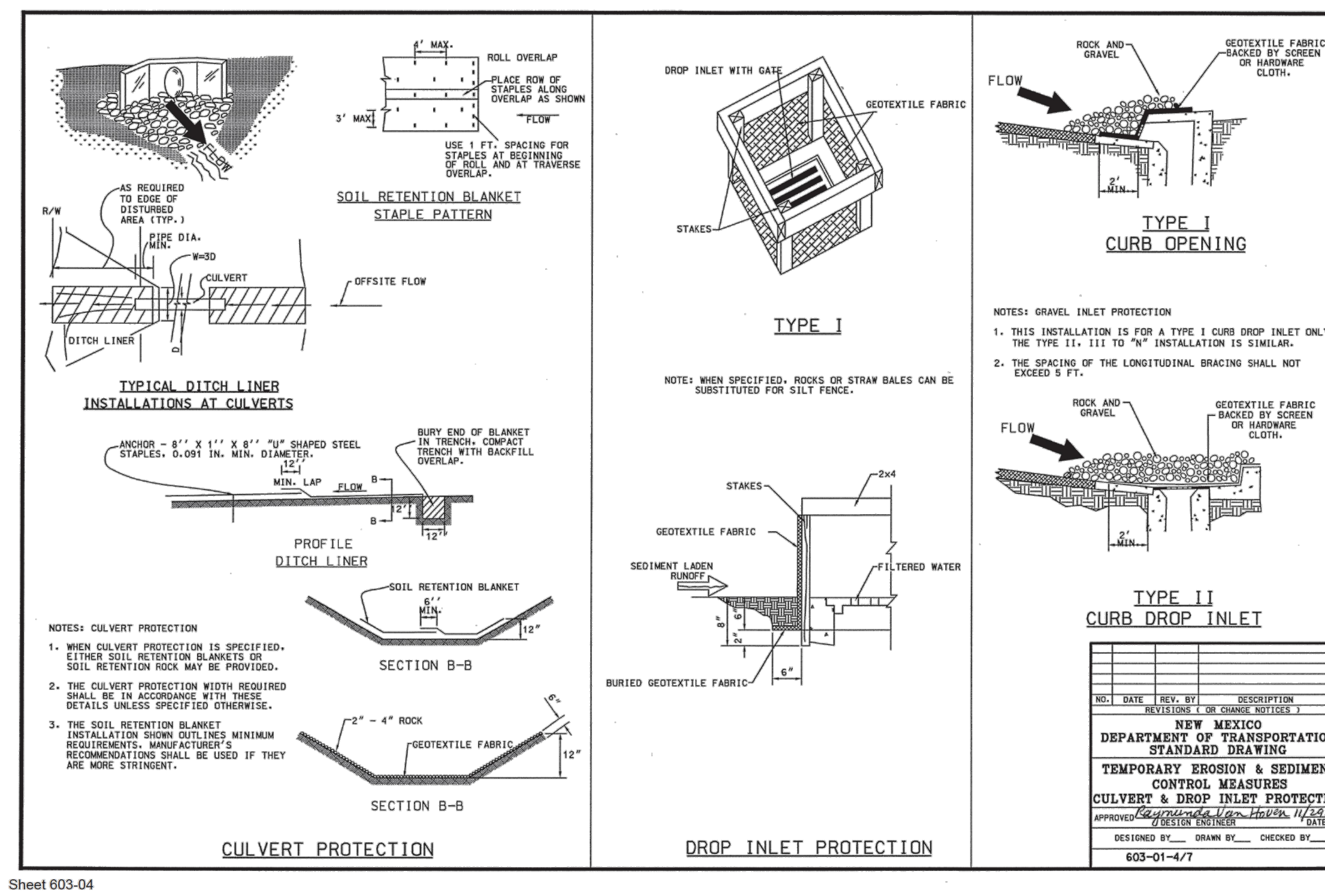
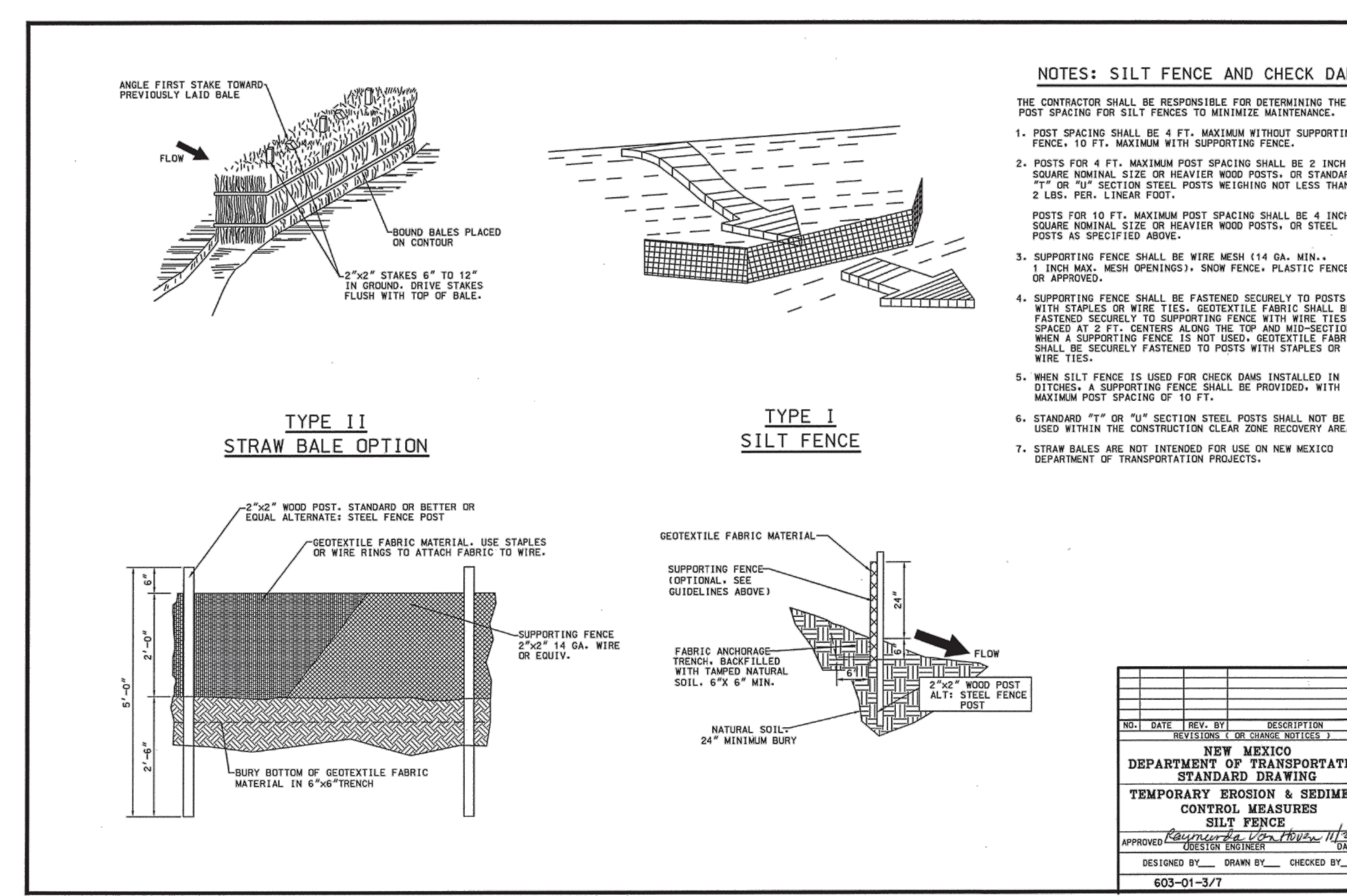
CONCRETE WASHOUT



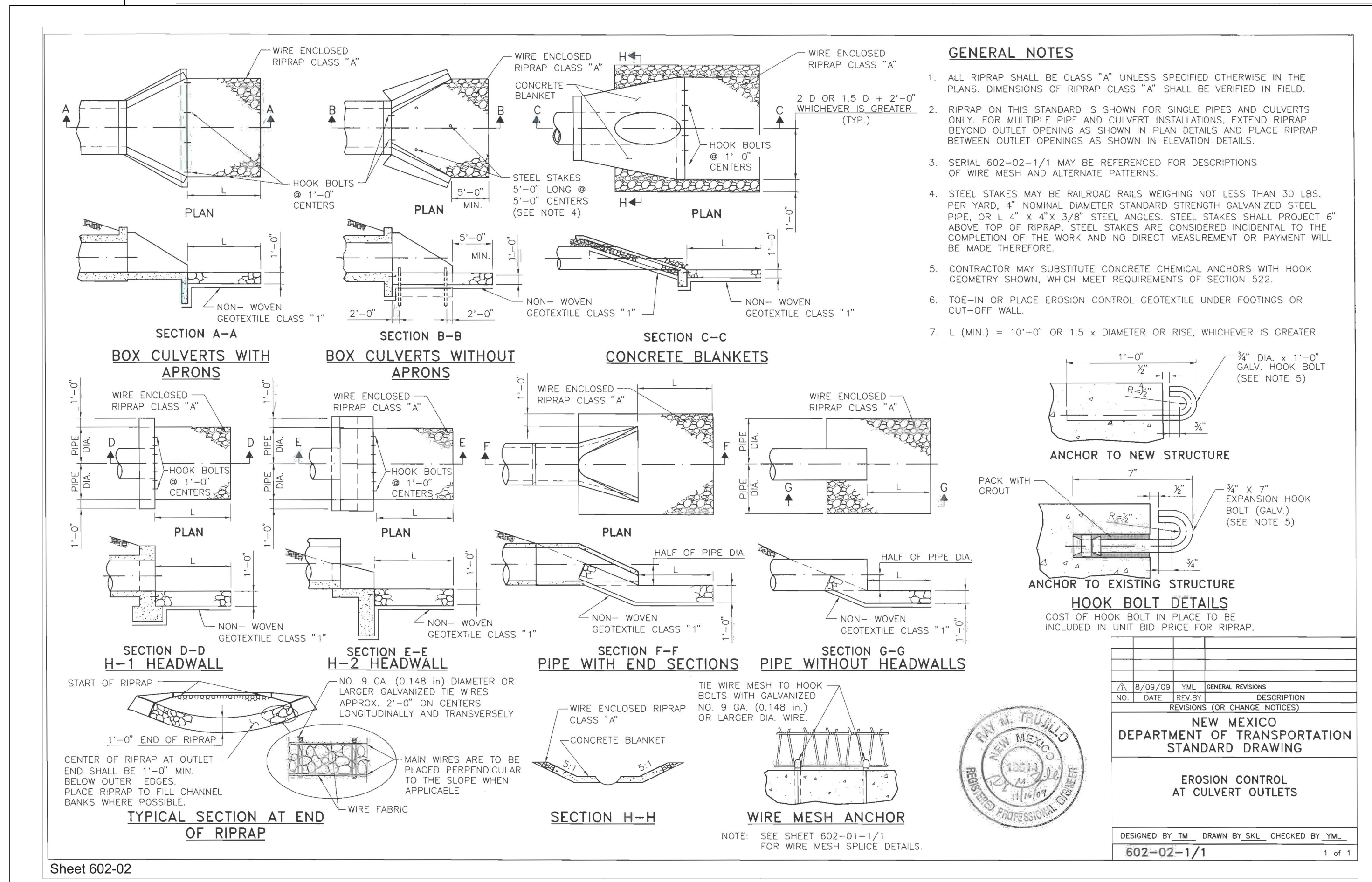
Sheet 603-02



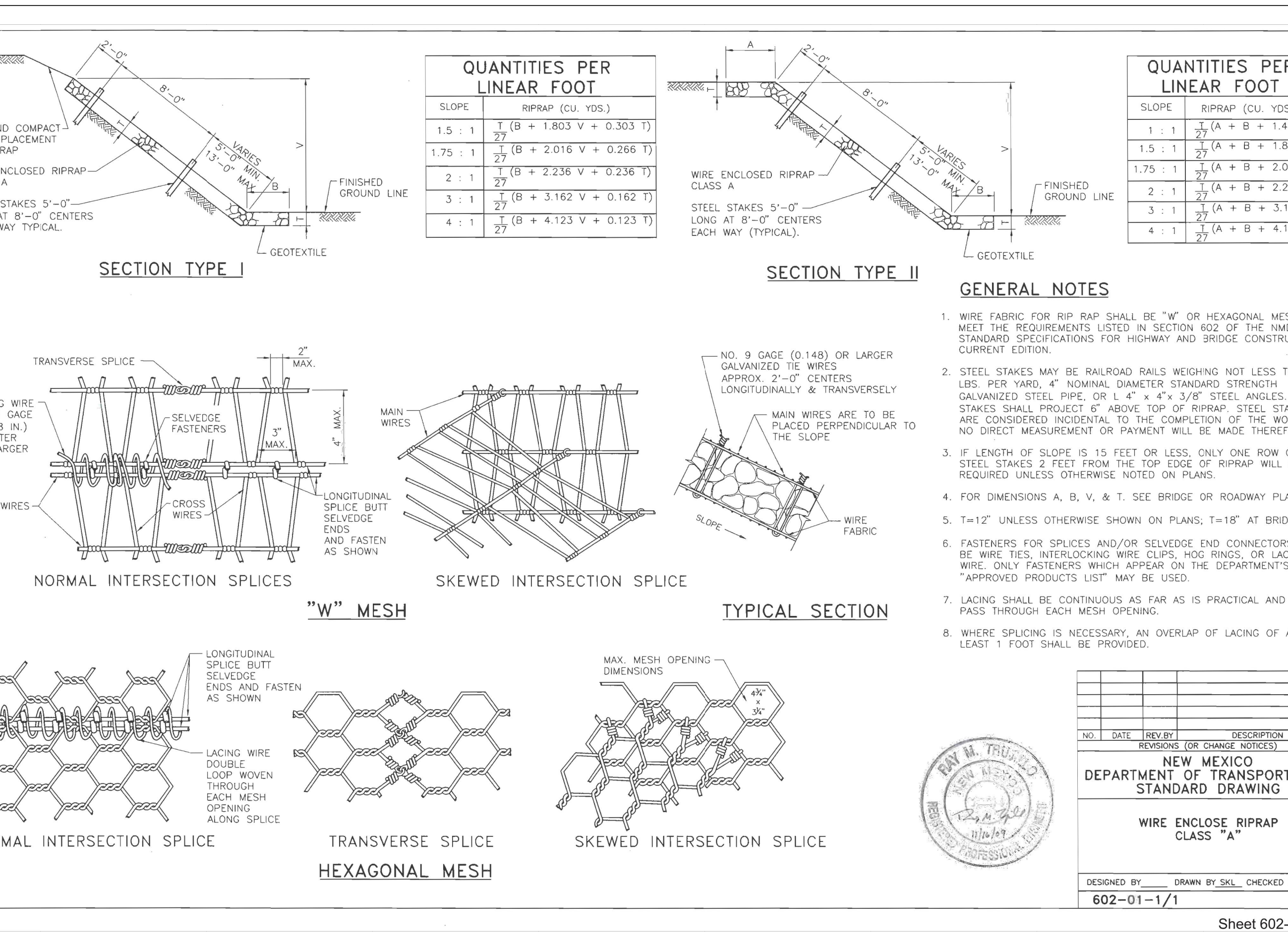
Sheet 603-02



Sheet 603-04



Sheet 602-02



Sheet 602-01

AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858

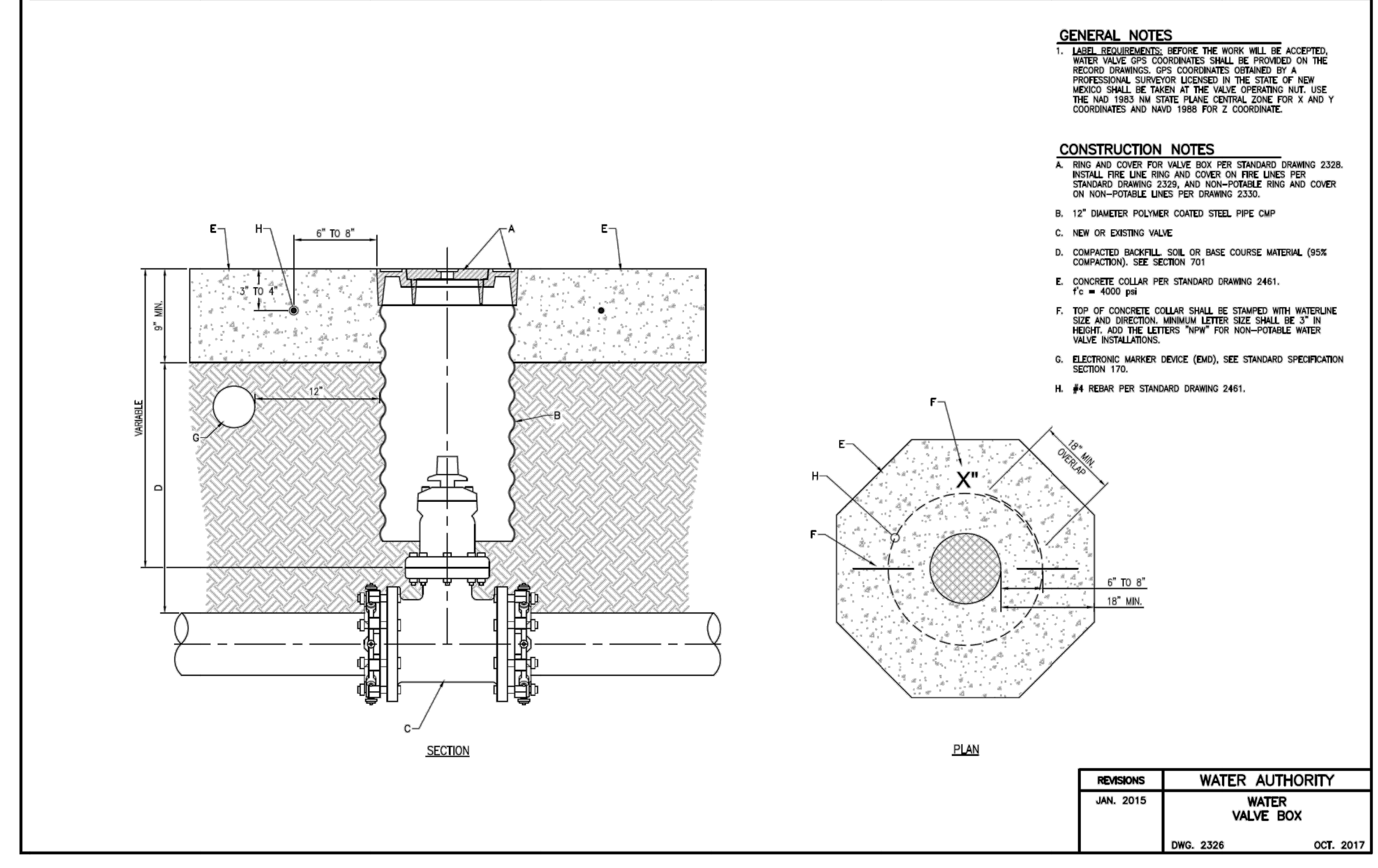
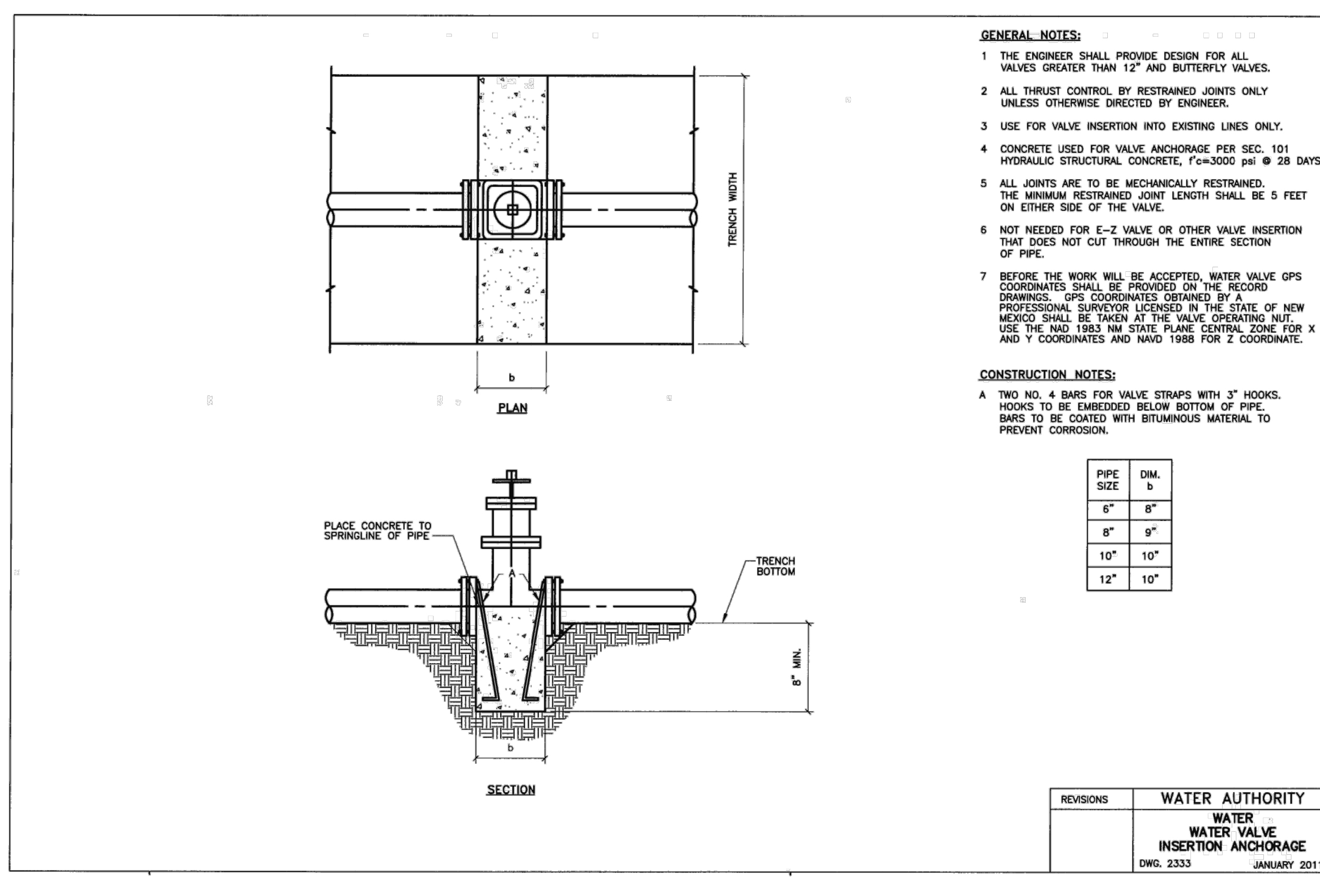
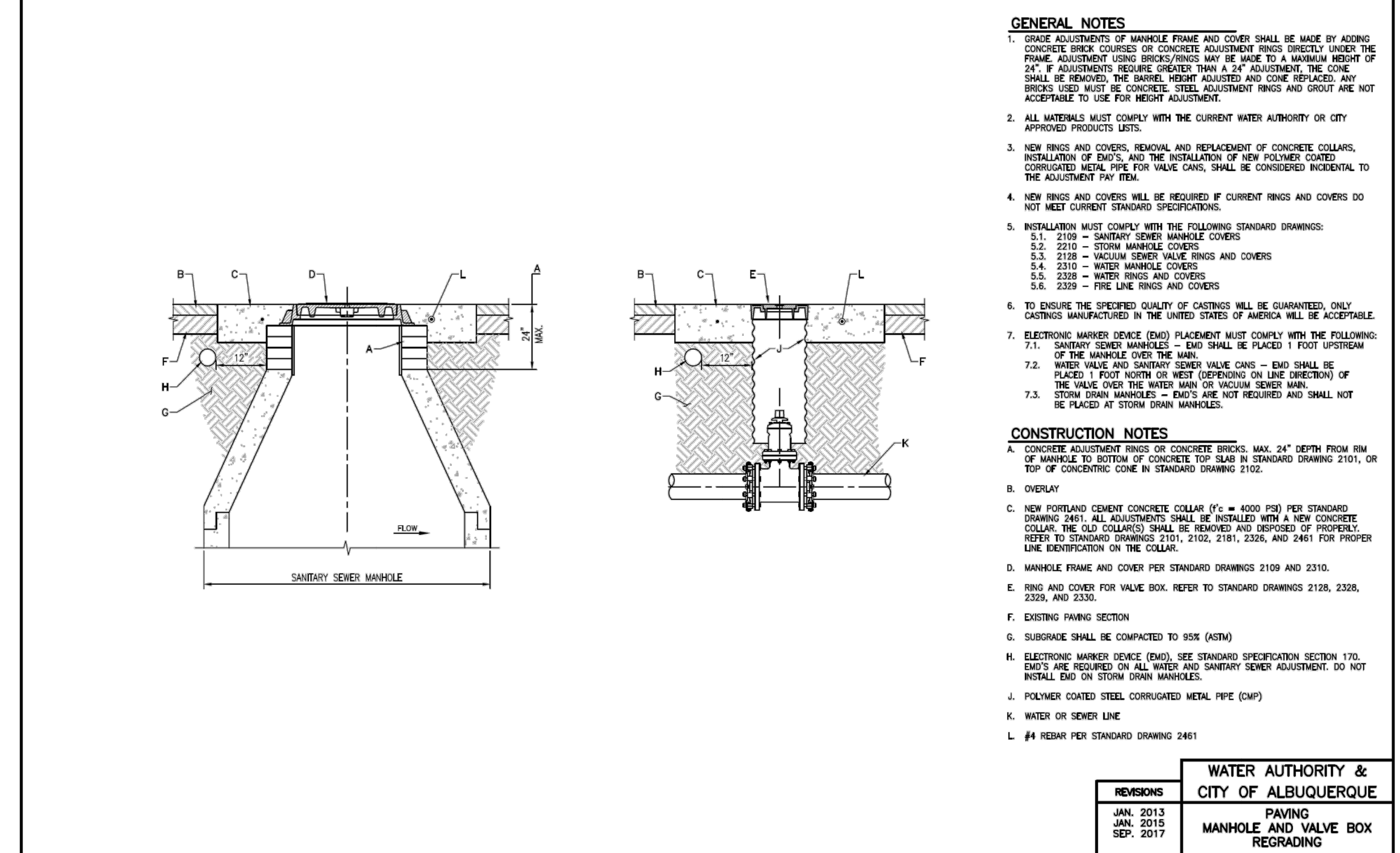
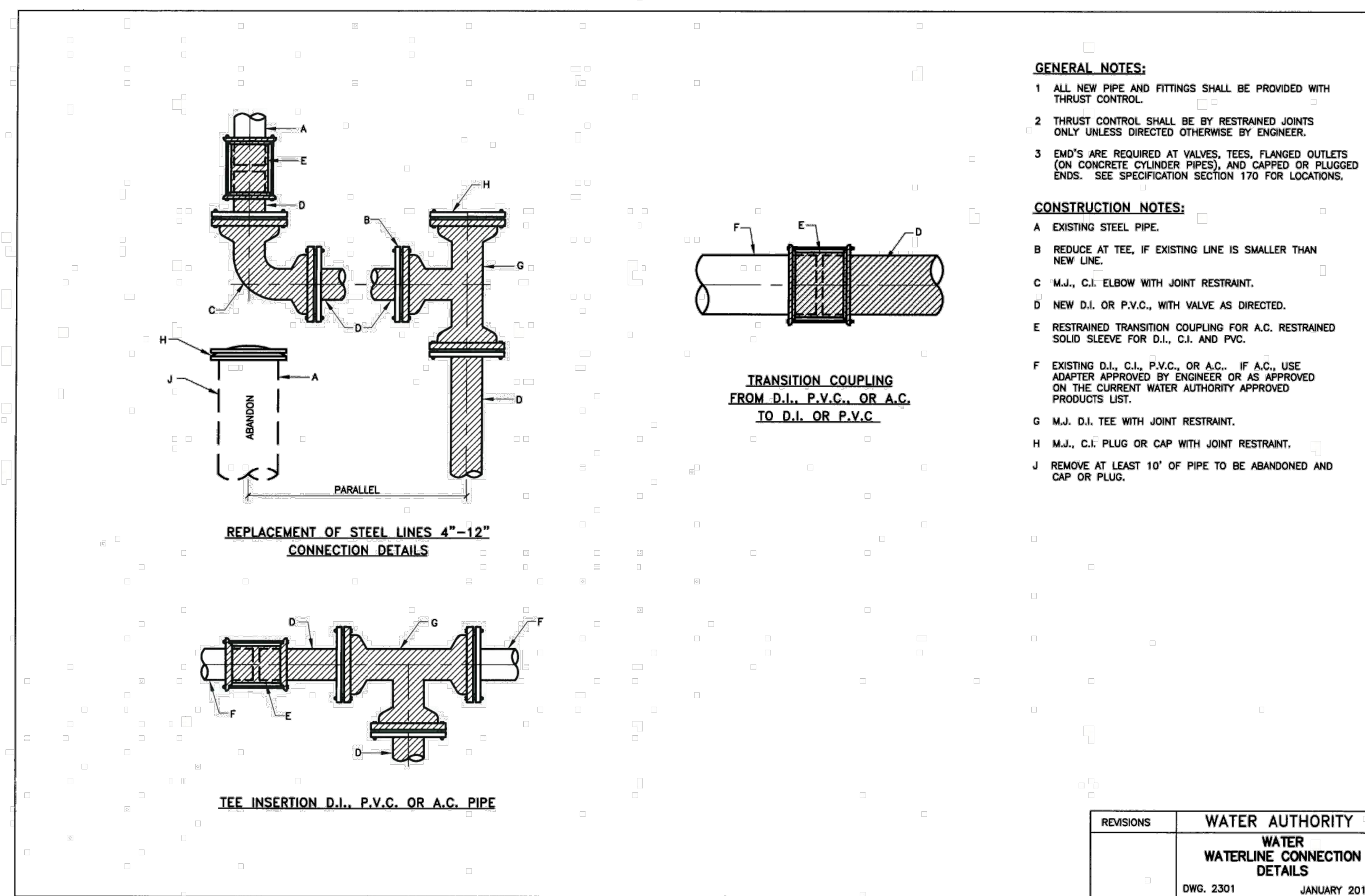
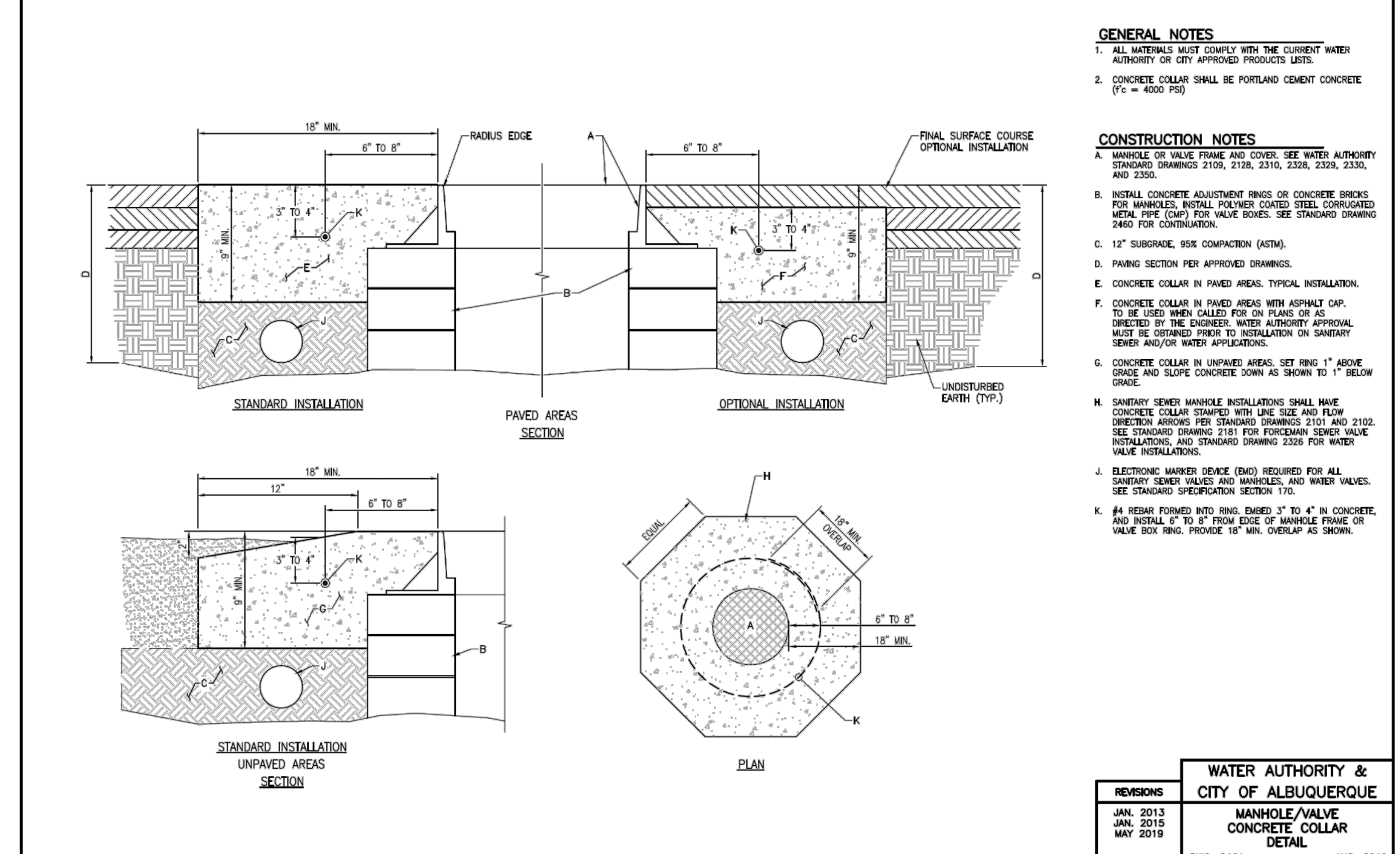
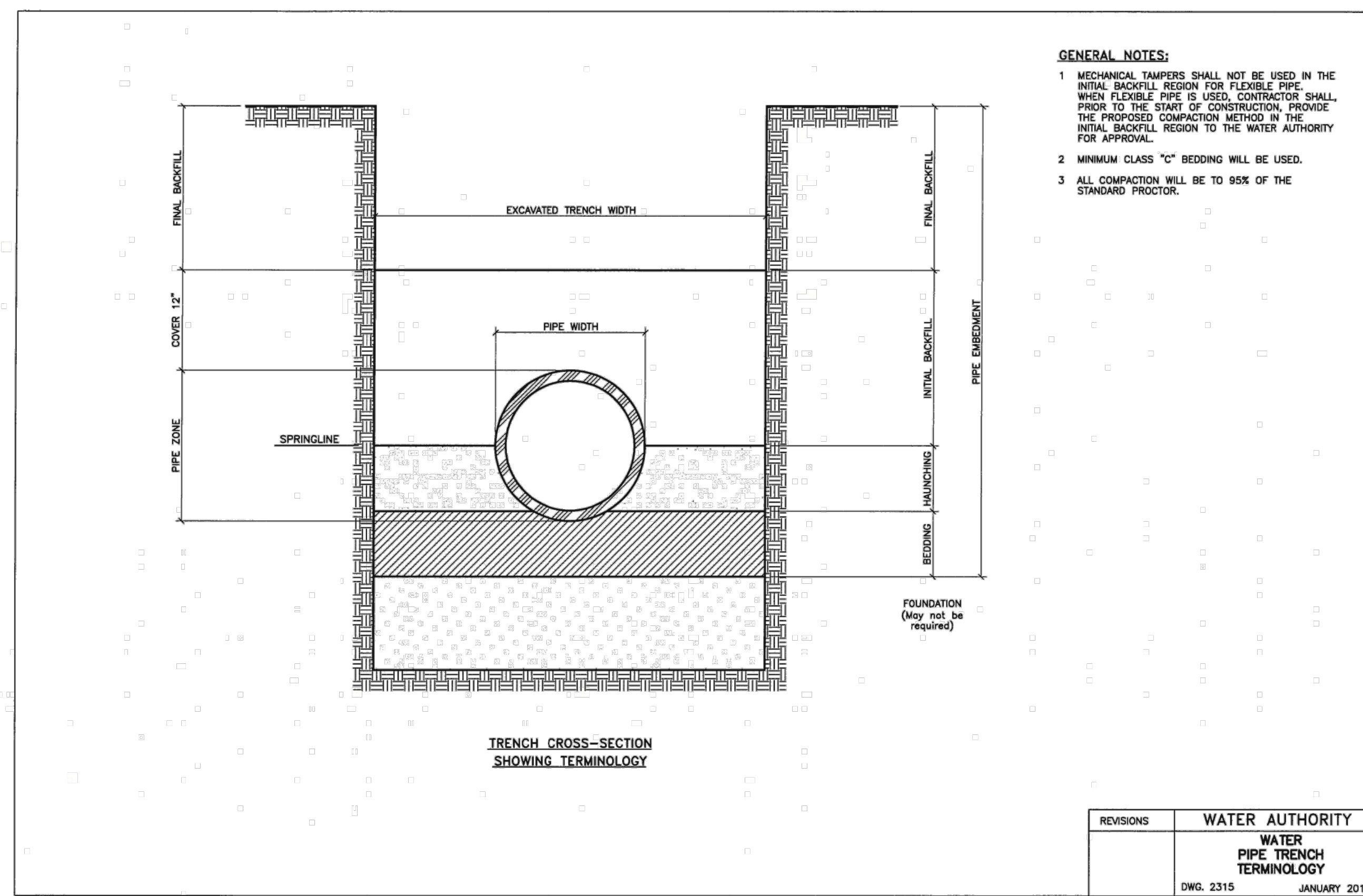
BIRKIE AYER JR.
 NEW MEXICO
 25848
 Professional Engineer
 Bill Ayer

PROJECT NAME:
LEXUS OF ALBUQUERQUE
 4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

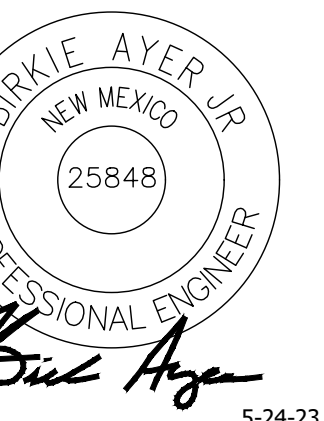
SHEET DESCRIPTION:
EROSION CONTROL DETAILS

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
1	FOR PERMIT	6/23/22
2	PERMIT REVISIONS	10/31/22
3	PERMIT REVISIONS	12/16/22
8	CLIENT CHANGES	12/30/22
9	NMDOT REVISIONS	2/21/23
10	PERMIT REVISIONS	3/10/23
11	PERMIT REVISIONS	4/13/23
12	PERMIT REVISIONS	4/26/23
13	PERMIT REVISIONS	5/9/23
14	PERMIT REVISIONS	5/24/23

Sheet 608.0



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858



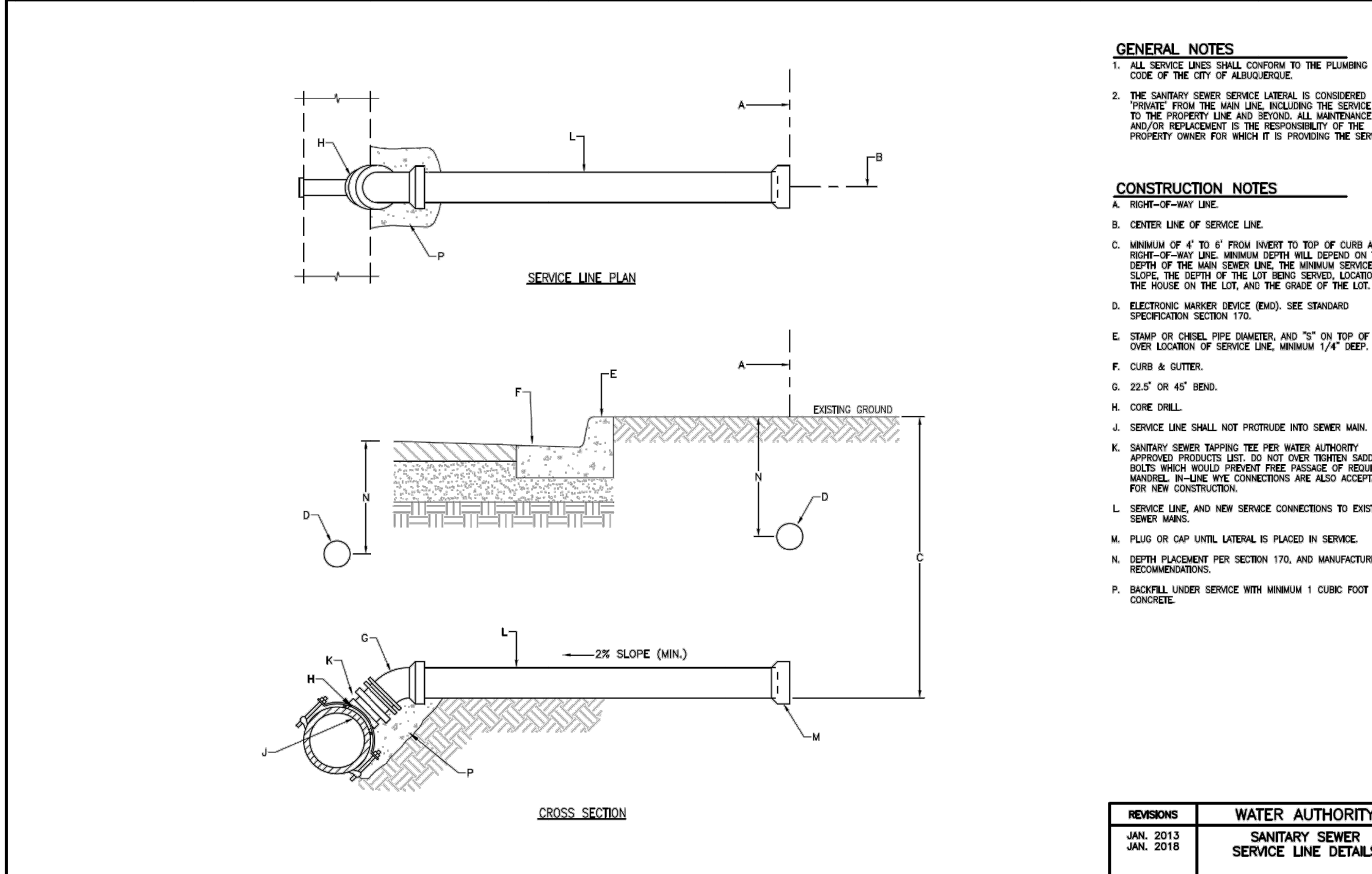
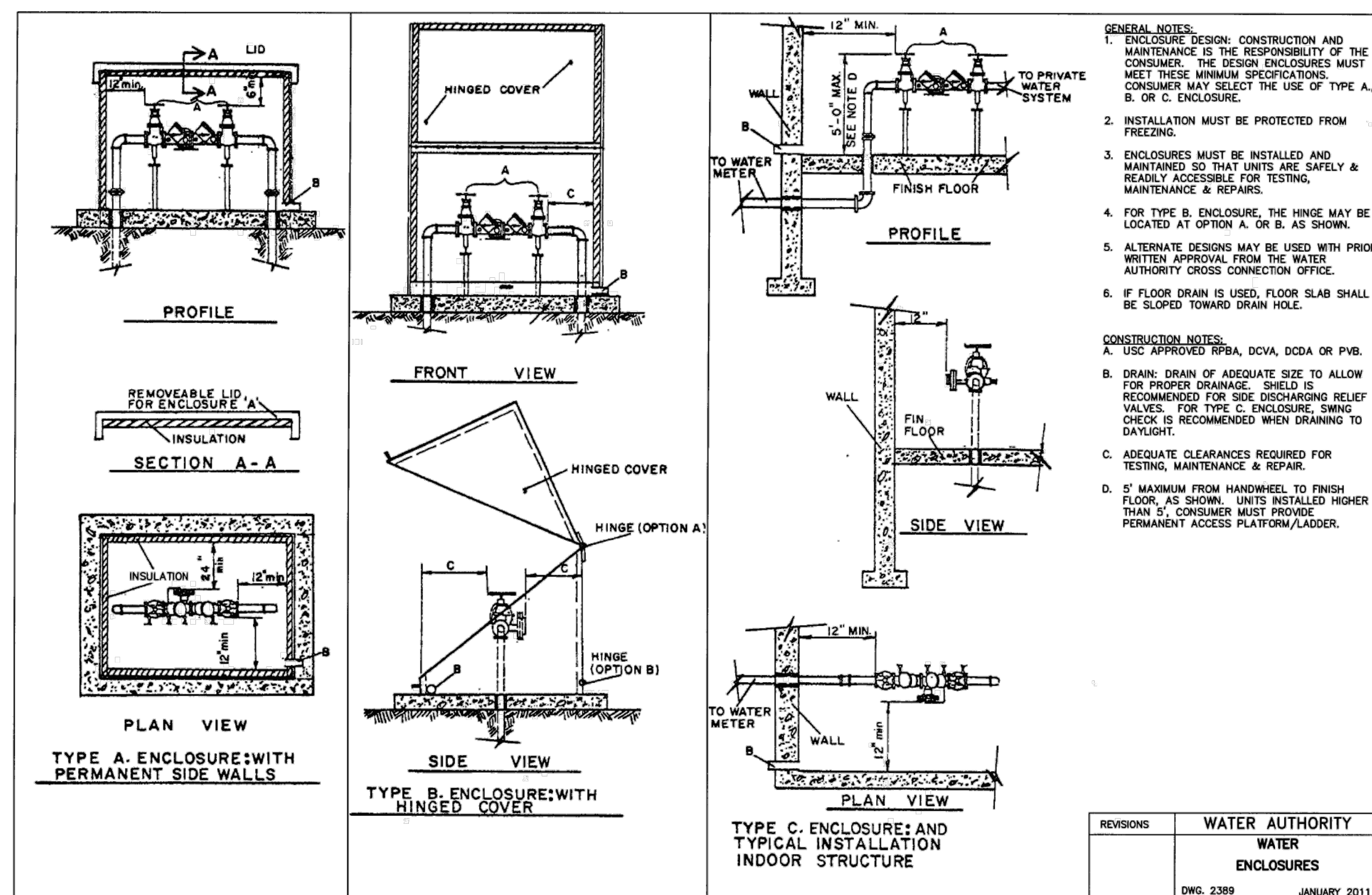
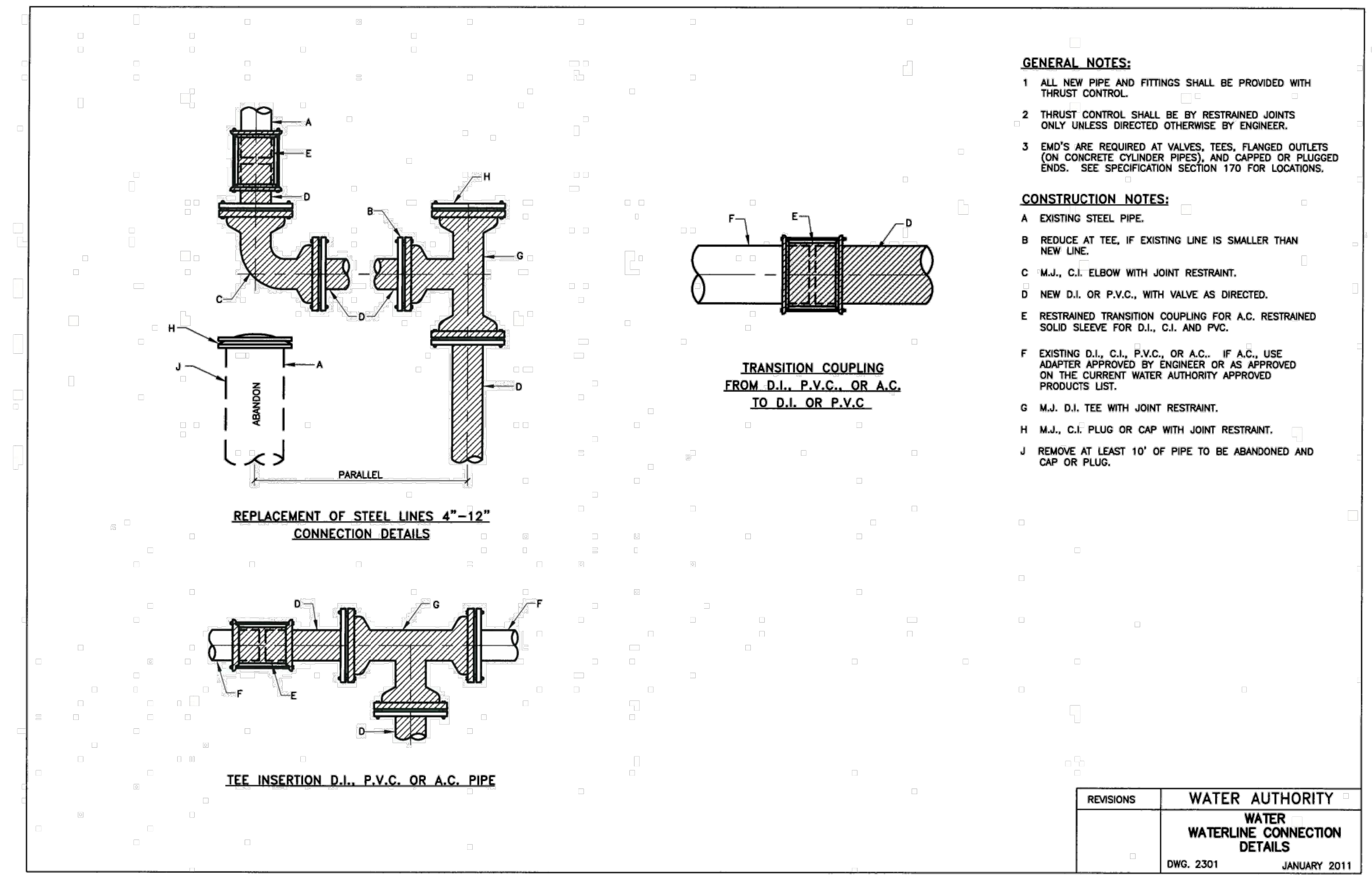
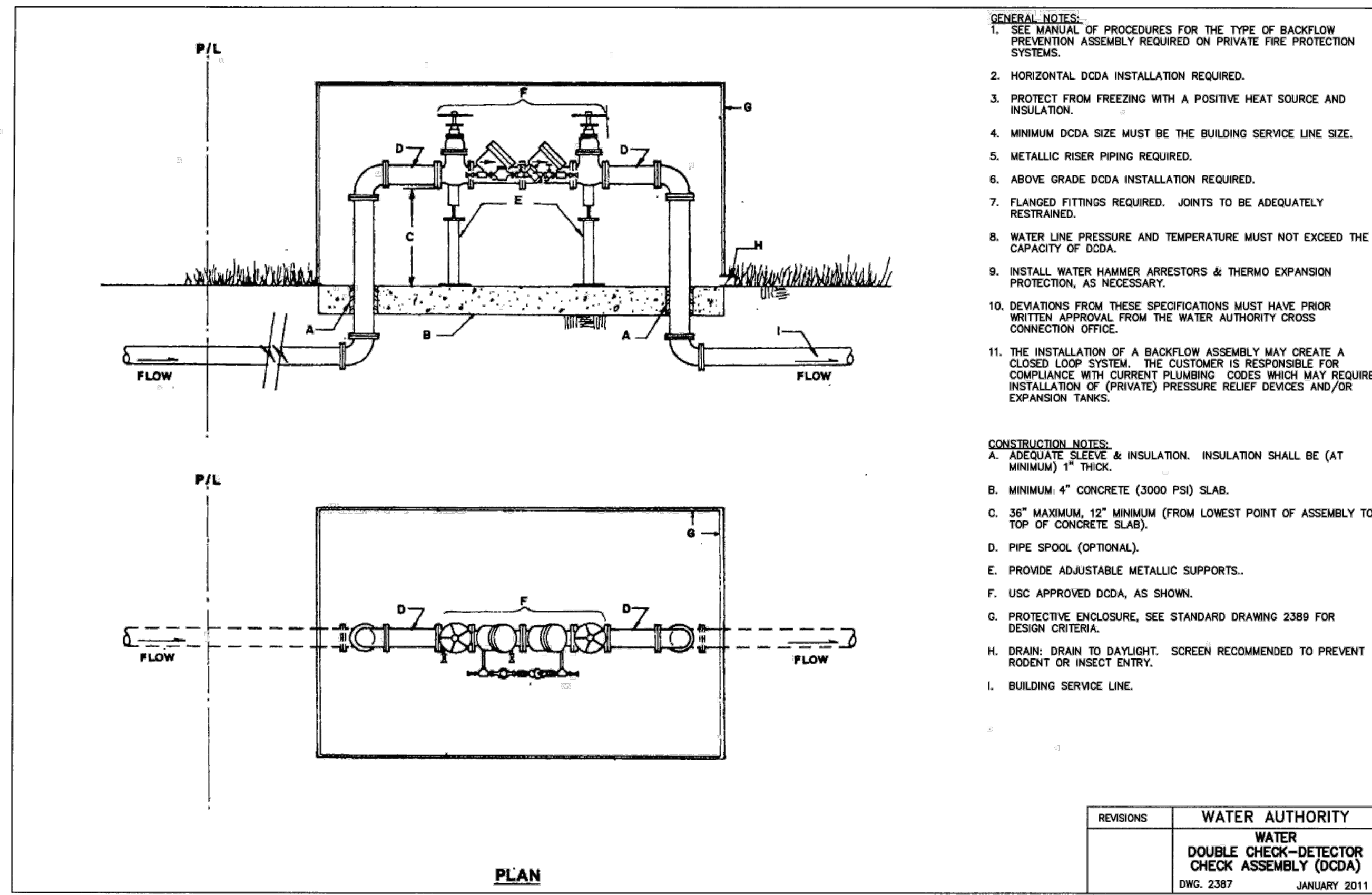
PROJECT NAME:
LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
UTILITY DETAILS

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#4	CLIENT CHANGES	12/30/22
#9	NM DOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C9.0



AYER DESIGN GROUP
 PROFESSIONAL ENGINEERING SERVICES
 215 Johnston Street
 Rock Hill, SC 29730
 Phone: 803-328-5858



PROJECT NAME:

LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
 ALBUQUERQUE, NM

SHEET DESCRIPTION:
UTILITY DETAILS

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

C9.2

PART 1 GENERAL

1.01 Scope
The work covered by this Specification consists of furnishing all labor, tools, materials, supervision, and equipment in performing all operations in connection with constructing the site infrastructure as specified herein or shown on Drawings.

1.02 Protection of Facilities
Existing utility lines, sidewalks, fencing, pavement or other structures shown on the Drawings, as shown to the Contractor or mentioned in the Plans and Specifications shall be kept free of damage by the Contractor's operations. It shall be the responsibility of the Contractor to verify the existence and location of all underground utilities on the site and insure that they are adequately marked to prevent damage.

PART 2 CLEARING AND GRUBBING
2.01 General
All vegetation, trees, stumps, roots, rubbish, trash, and other debris shall be cleared and grubbed within the clearing limits of all areas to receive structures or utility lines before beginning grading operations. All material from clearing and grubbing operations shall be removed from the site in conformance to applicable Federal, State and Local laws and regulations.

2.02 Stripping and Topsoiling
Areas to be graded shall be stripped of topsoil. All topsoil shall be stripped and spread in non-critical grading areas. Topsoil shall be stockpiled for use on finished grades or other areas graded or disturbed by Contractor.

PART 3 EROSION CONTROL
3.01 General
Soil erosion and sediment control shall be provided by the Contractor at his expense for all areas of the site which are disturbed by construction operations throughout the life of the project. Control measures shall be in place prior to any land disturbing activity and expanded as needed during the project. Maintenance of these facilities shall be continuous by the Contractor until the disturbed area is protected by ground cover (Final Stabilization).

PART 4 STORM DRAINAGE SYSTEM
4.01 General
Each pipe shall be laid on a firm bed, true to line and grade, and in such manner as to form a close concentric joint with the adjoining pipe and to prevent sudden offsets of the flow line or disturbance of the line and grade. All pipes shall be laid with the bell end pointing up grade. Each section of storm pipe installation shall begin at the lowest point and must be laid continuously between manholes, headwalls, or to completion. Where pipe is to be laid in material of poor supporting value, the unsuitable material shall be replaced with a layer of No. 57 crushed stone.

4.02 Products
Reinforced concrete pipe (RCP) shall comply with the requirements of ASTM Designation, C76, Class III or Class IV as indicated on the Drawings. Joints shall be tongue-and-groove joints with Butyl rubber mastic sealant, which shall comply with Federal Specification SS-S-210. All pipes shall be circular with circular reinforcement. Precast drainage structures are not allowed without prior written approval. Waffle or knock-out boxes are never to be used.

PART 5 PARKING LOT CONSTRUCTION
5.01 General
All lots shall be graded as shown on the Drawings. Sub-grade shall be sloped for effective drainage at all times. Equipment ruts shall be leveled at the end of each work day. Water-holding depressions shall not be left in grading areas overnight. All sub-grade shall be proof rolled with a fully loaded tandem, rubber-tired roller or similar approved equipment. The proof roller should make at least two (2) passes over each location. Any areas which wave, rut or deflect excessively and continue to do so after several passes of the proof roller shall be considered as waste and shall be disposed of off-site in an approved land fill. The excavations for footings and slabs shall be carried to the exact grades as shown on the Drawings. Undercutting will not be permitted. Should the Contractor excavate below the proper grade under concrete slabs or footings, earth fill will not be permitted, and concrete shall be poured to the bottom of excavation. No extra compensation will be granted the Contractor for extra concrete used for re-fill. Embankment and fill compaction shall be accomplished by thoroughly compacting each layer with sheeps foot rollers, pneumatic rollers, and mechanical tampers in places inaccessible to rollers, or other equipment. When material has too much moisture, grading operations shall be limited to drying soil by spreading and turning for drying by the sun and aeration. When material is dry, moisture shall be added by sprinkling by approved means.

5.02 Products
Concrete curb and gutter, sidewalk, and driveways shall be poured with minimum compressive strength of 3000 PSI 28 days, four inch slump per ASTM C39, air content of 3%-6%. Location, width and thickness shall be as indicated on the Drawings. Aggregate base course, binder and surface course shall be installed and compacted in accordance with NM DOT Specifications. Type and thickness shall be as indicated on the drawings.

PART 6 GRADING AND EXCAVATION
6.01 General
After clearing, grubbing and stripping have been completed, excavation for buildings, parking lots, drainage ditches and all other construction, shall be performed to the lines and grades indicated by the Drawings. Site excavation shall be unclassified regardless of the nature of material encountered. When material encountered within the limits of the work is considered unsuitable by the Engineer, such material shall be excavated below the grades shown on the Drawings or as directed by the Engineer and shall be replaced with suitable material. Excavation materials which are not required for fills shall be considered as waste and shall be disposed of off-site in an approved land fill. The excavations for footings and slabs shall be carried to the exact grades as shown on the Drawings. Undercutting will not be permitted. Should the Contractor excavate below the proper grade under concrete slabs or footings, earth fill will not be permitted, and concrete shall be poured to the bottom of excavation. No extra compensation will be granted the Contractor for extra concrete used for re-fill. Embankment and fill compaction shall be accomplished by thoroughly compacting each layer with sheeps foot rollers, pneumatic rollers, and mechanical tampers in places inaccessible to rollers, or other equipment. When material has too much moisture, grading operations shall be limited to drying soil by spreading and turning for drying by the sun and aeration. When material is dry, moisture shall be added by sprinkling by approved means.

All embankments and fills shall be compacted to the following percentages of the maximum dry density and within the following moisture range, in terms of optimum moisture as determined by the Standard Proctor Density Test, ASTM, D698.

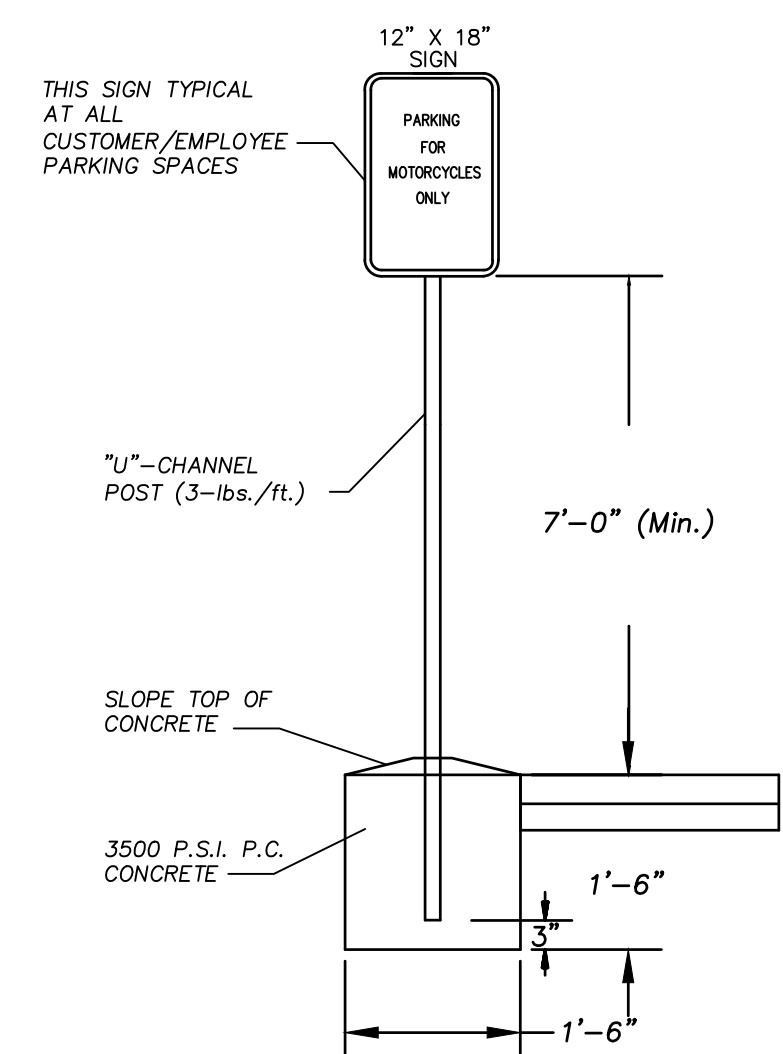
The following table shall be used unless otherwise specified.

TABLE OF COMPACTION AND MOISTURE			
Class of Fills or Embankments	Zone	Minimum Density%	Moisture Range
Structure	Top 12-inches	98	+1 to -2
	Remainder	95	+4 to -3
Roadway	Top 30-inches	98	+1 to -2
	Remainder	95	+2 to -3
Parking Lot	Top 18-inches	98	+1 to -2
	Remainder	95	+3 to -3
Dam/Dike	All Depths	95	+4 to -3
	Common	92	+4 to -4

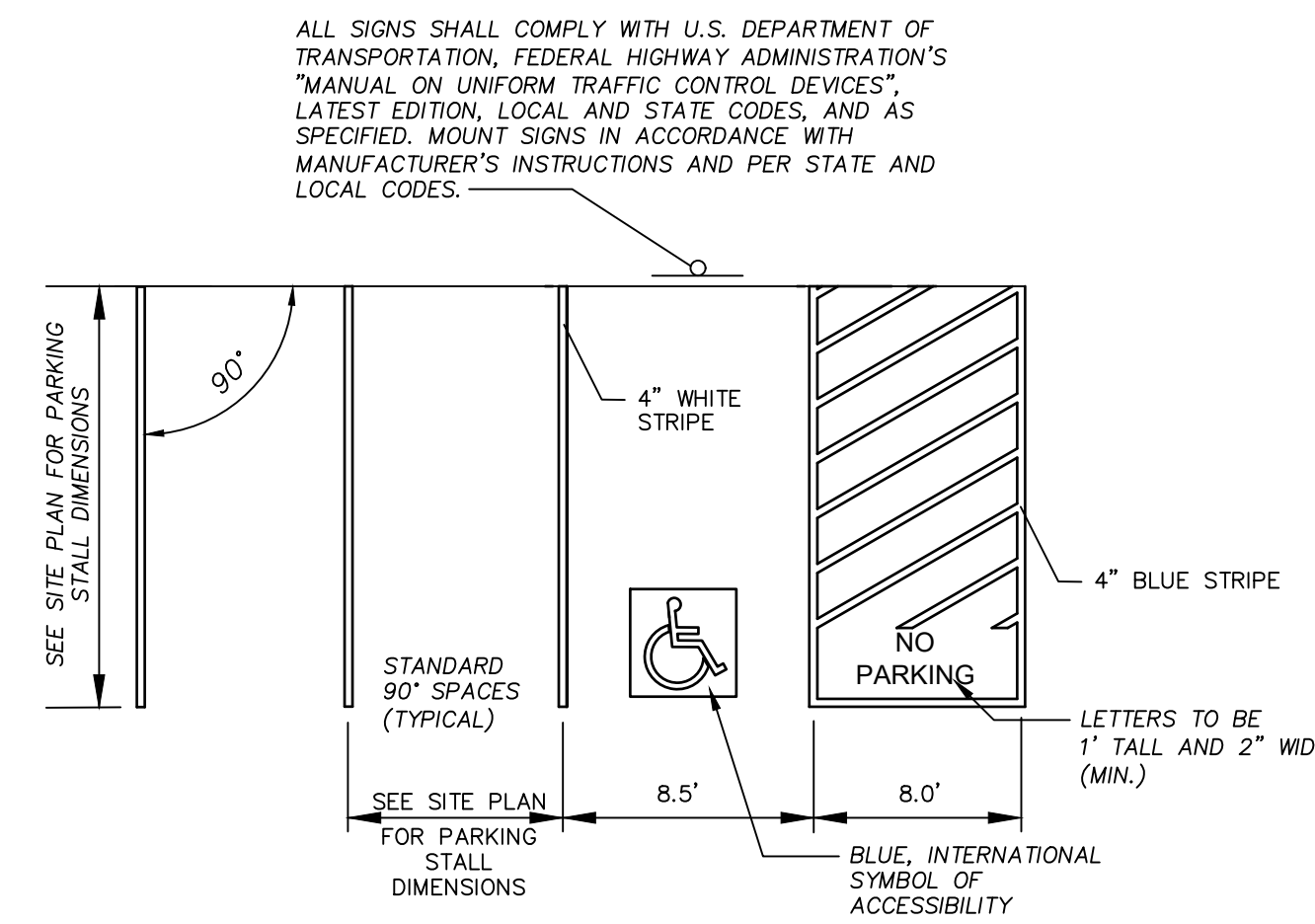
Class of fills or Embankments are defined as follows:
Structure - beneath concrete slabs of buildings, pumping stations, floors, foundations, etc.
Roadway - beneath all roads, streets, truck operations.
Parking Lot - automobile parking lots.
Dam/Dike - embankment for ponds, lagoons, etc.
Common - all other embankments.

Any required borrow material shall be provided by the contractor at no additional cost to the owner. Excavated materials not suited for backfill and excavated material in excess of that needed to complete the work shall be wasted on the project site where directed by the Owner. Waste areas shall be left in a graded and sloped condition to allow natural drainage of the surrounding area.

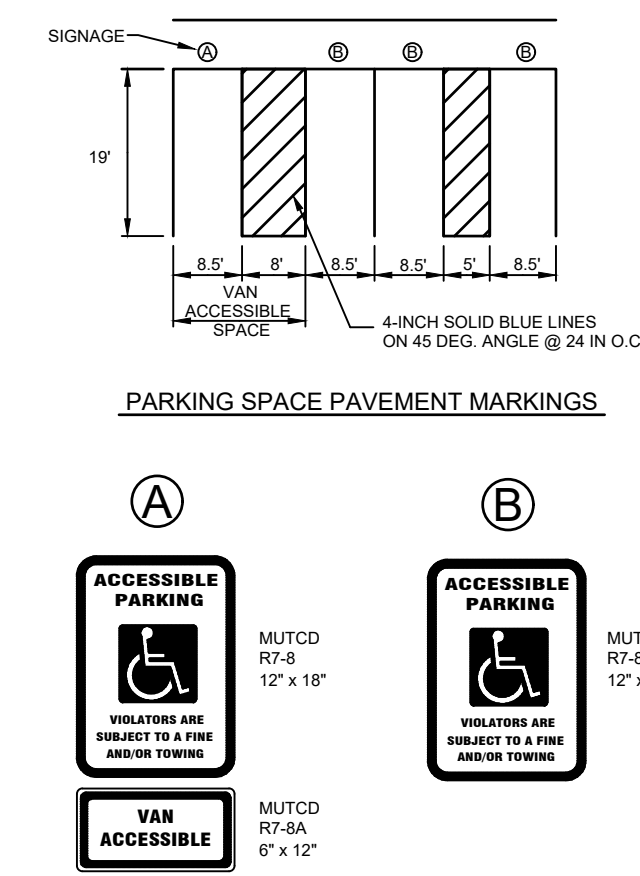
SITE WORK SPECIFICATIONS



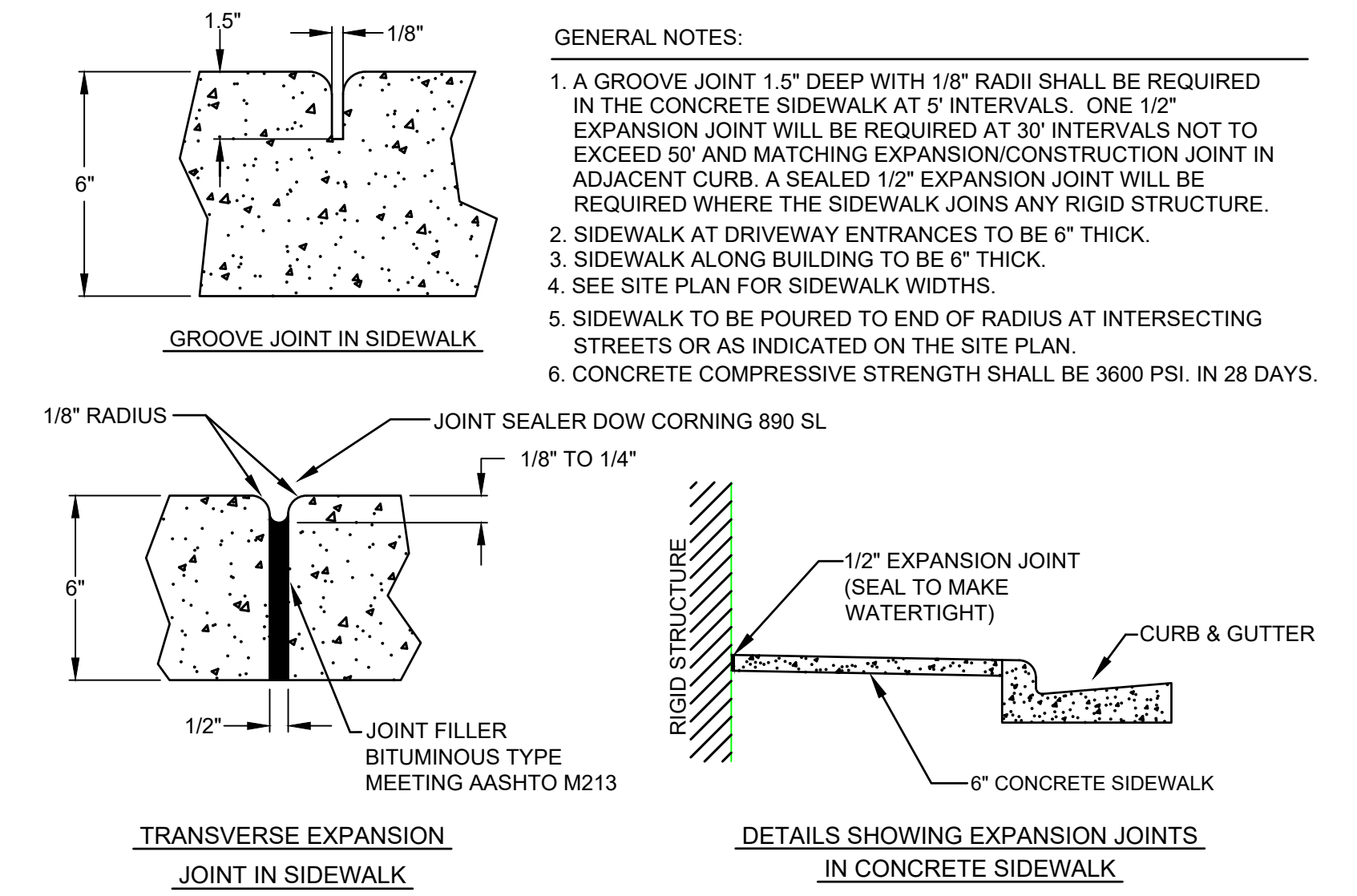
MOTORCYCLE PARKING SIGNAGE



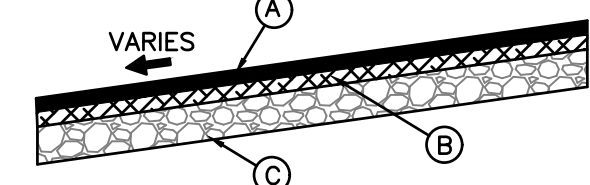
ACCESSIBLE PAINT STRIPING



ACCESSIBLE PARKING & SIGNAGE

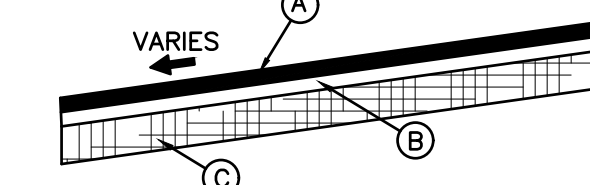


CONCRETE SIDEWALK SECTIONS



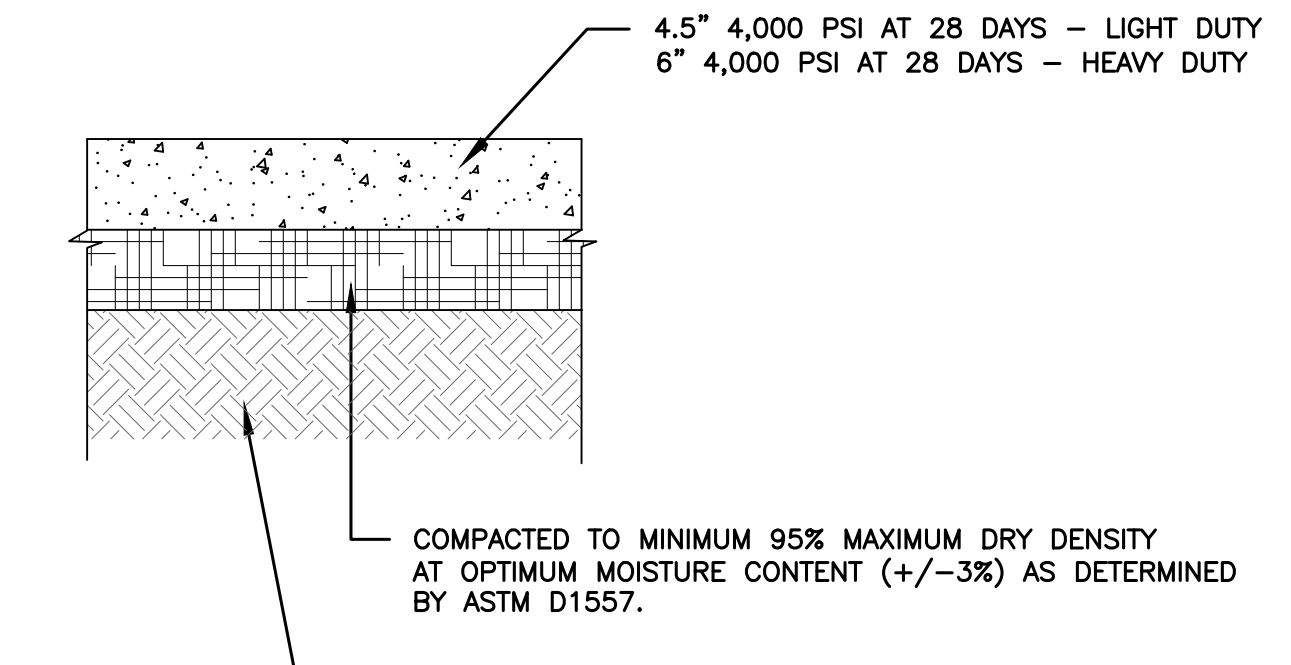
- PAVEMENT SCHEDULE BASED ON GEOTECH REPORT**
- Ⓐ 2" ASPHALTIC CONCRETE SURFACE COURSE (AC), TYPE B, PLACED AFTER ALL MANHOLE, VALVE COVERS/RINGS, AND STORMWATER RIMS/GRATES ARE SET TO GRADE
 - Ⓑ 2" ASPHALTIC CONCRETE COURSE (AC), TYPE B
 - Ⓒ 4" AGGREGATE BASE, COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DENSITY AS DETERMINED BY ASTM D-1557.

HEAVY DUTY ASPHALT PAVEMENT SECTION



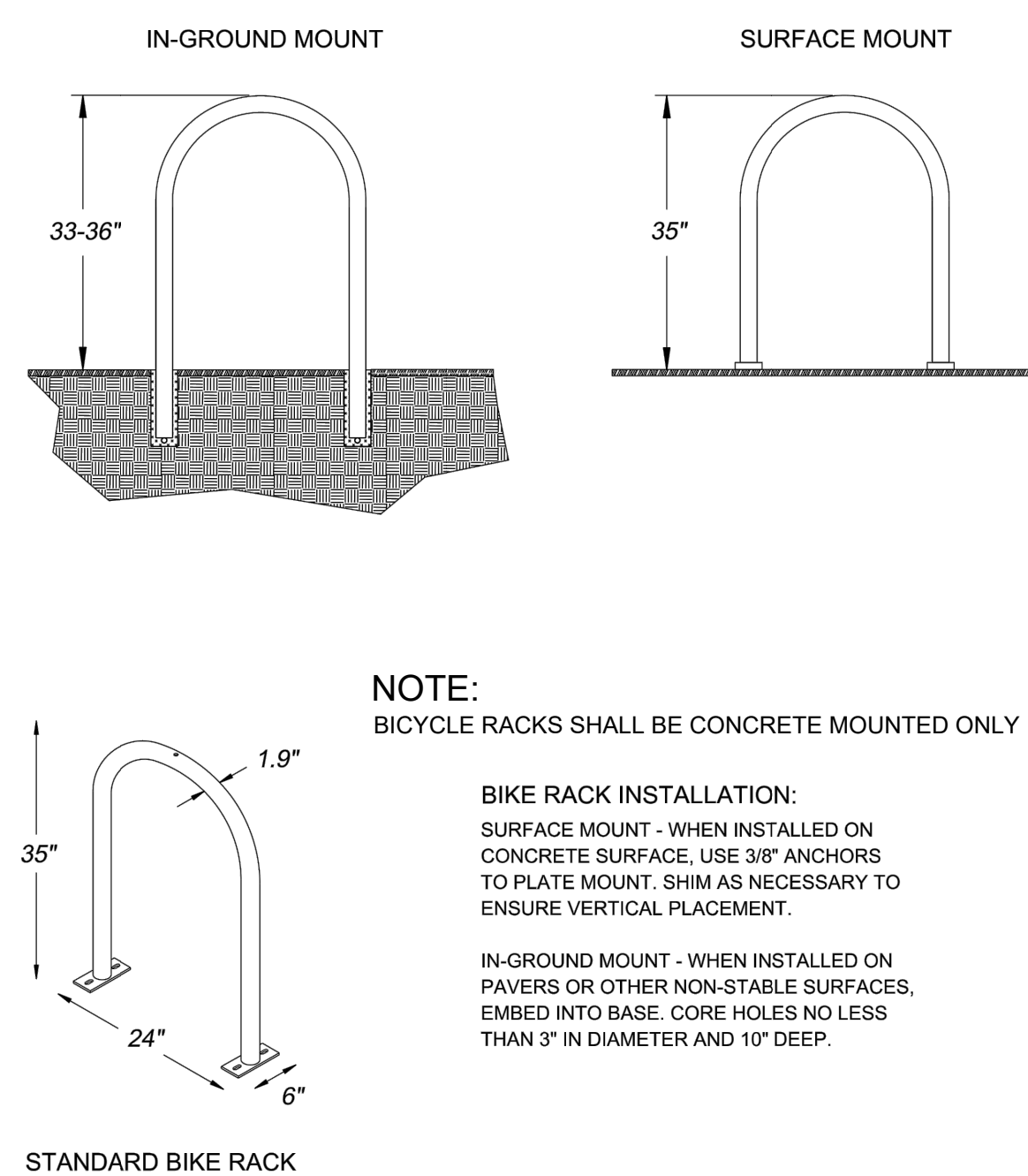
- PAVEMENT SCHEDULE BASED ON GEOTECH REPORT**
- Ⓐ 2" ASPHALTIC CONCRETE SURFACE COURSE (AC), TYPE B, PLACED AFTER ALL MANHOLE, VALVE COVERS/RINGS, AND STORMWATER RIMS/GRATES ARE SET TO GRADE
 - Ⓑ 2" ASPHALTIC CONCRETE COURSE (AC), TYPE B
 - Ⓒ COMPACTED TO MINIMUM 95% MAXIMUM DRY DENSITY AT OPTIMUM MOISTURE CONTENT (+/-3%) AS DETERMINED BY ASTM D1557.

LIGHT DUTY ASPHALT PAVEMENT SECTION

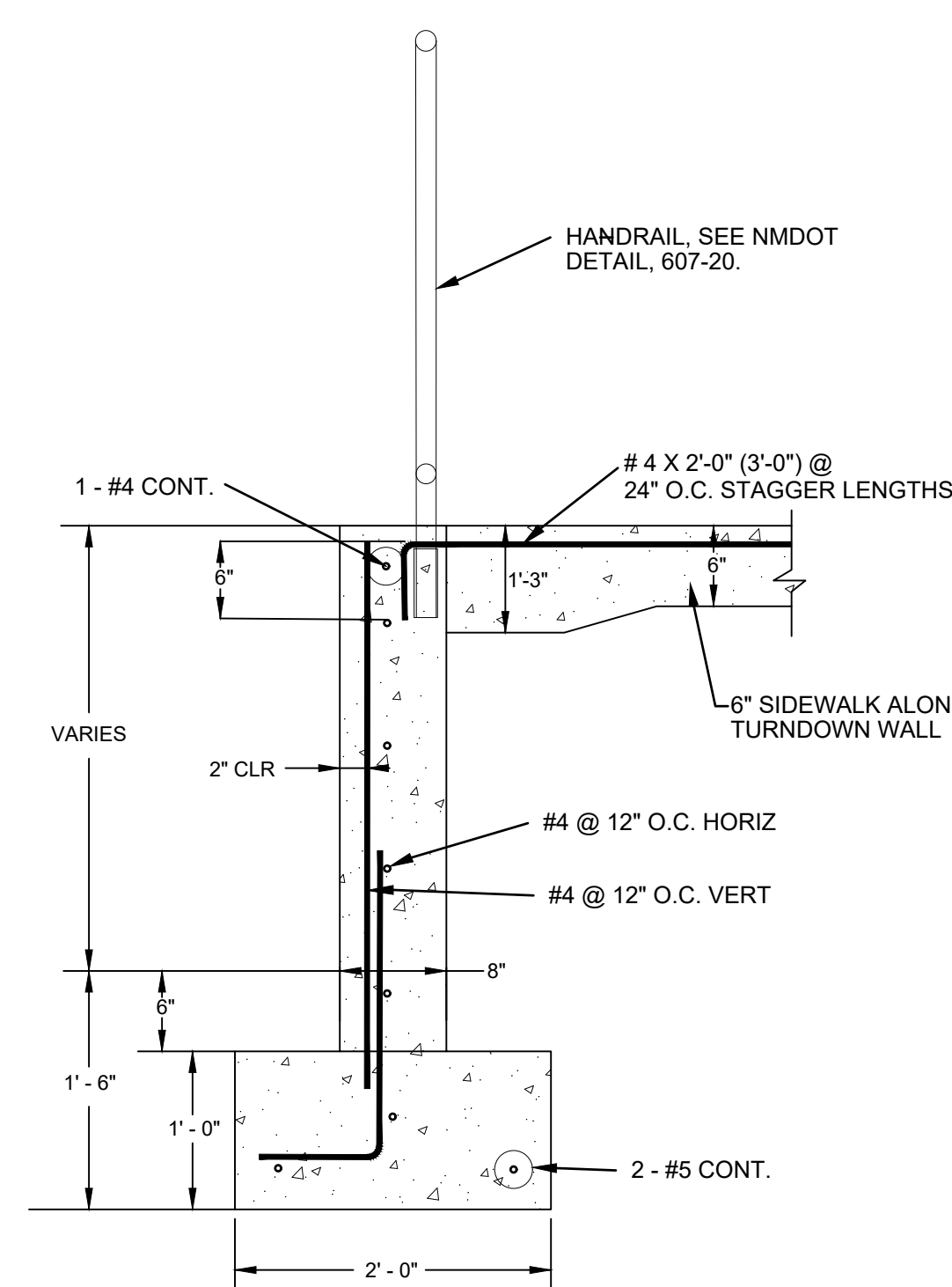


- NOTES:**
- REFER TO GEOTECHNICAL REPORT
 - CONTROL JOINTS SHALL BE SPACED AT 12' MAX. ODD SHAPES SHALL BE AVOIDED.
 - JOINTS SHALL BE CUT WITHIN 24 HOURS OF PLACEMENT OF CONCRETE.
 - CONCRETE MATERIALS AND PLACEMENT INCLUDING JOINTING SHOULD BE IN ACCORDANCE WITH RECOMMENDATIONS IN THE LATEST EDITION OF ACI-330R.

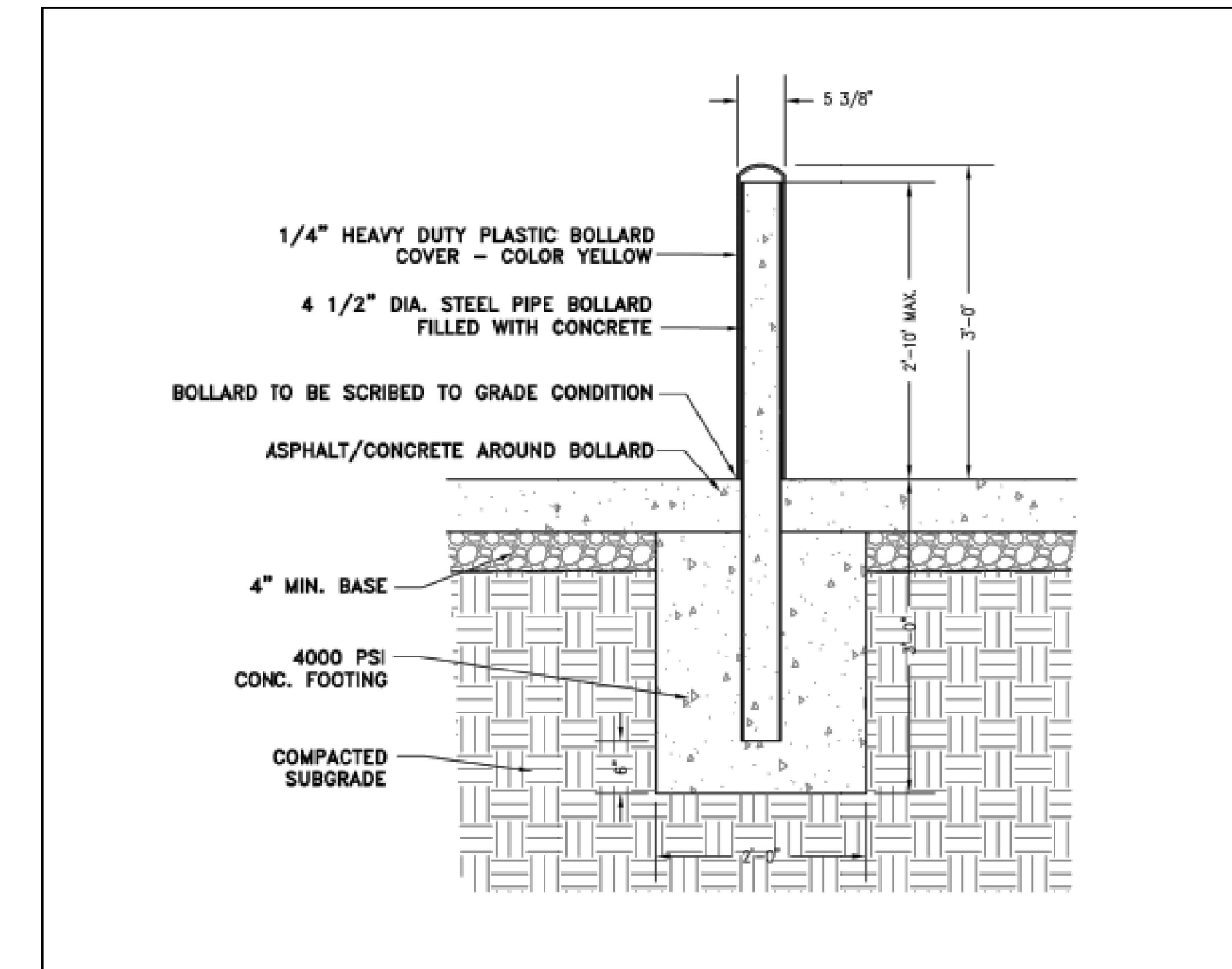
CONCRETE PAVEMENT SECTION



BIKE RACK

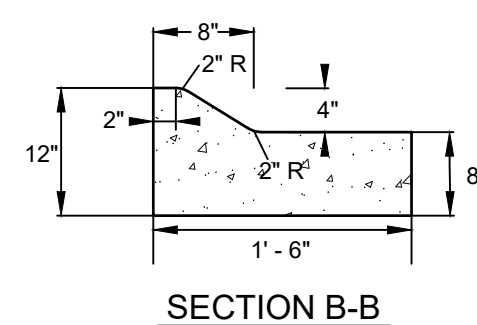


SIDEWALK TURNDOWN WALL SECTION

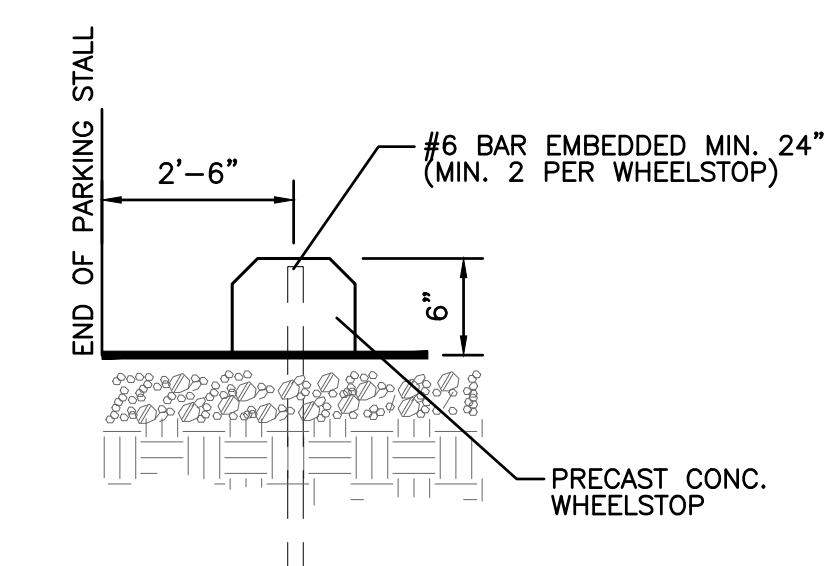


STEEL BOLLARD DETAIL

- GENERAL NOTES:**
- CONTRACTION JOINTS SHALL BE SPACED AT 10-FOOT INTERVALS.
 - CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS, WHERE SUCH JOINTS ARE NOT BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2" SHALL BE OBTAINED.
 - ALL EXPANSION JOINTS SHALL BE SPACED AT 90-FOOT INTERVALS, AND ADJACENT TO ALL RIGID OBJECTS. JOINTS SHALL MATCH LOCATIONS WITH JOINTS IN ABUTTING SIDEWALK.
 - CONCRETE COMPRESSIVE STRENGTH SHALL BE 3,600 P.S.I. IN 28 DAYS.
 - TOP 6" OF SUBGRADE BENEATH THE CURB AND GUTTER SHALL BE COMPACTED TO 100% STANDARD PROCTOR DENSITY.



4" MOUNTABLE CURB



CONCRETE WHEELSTOP

AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF ALBUQUERQUE

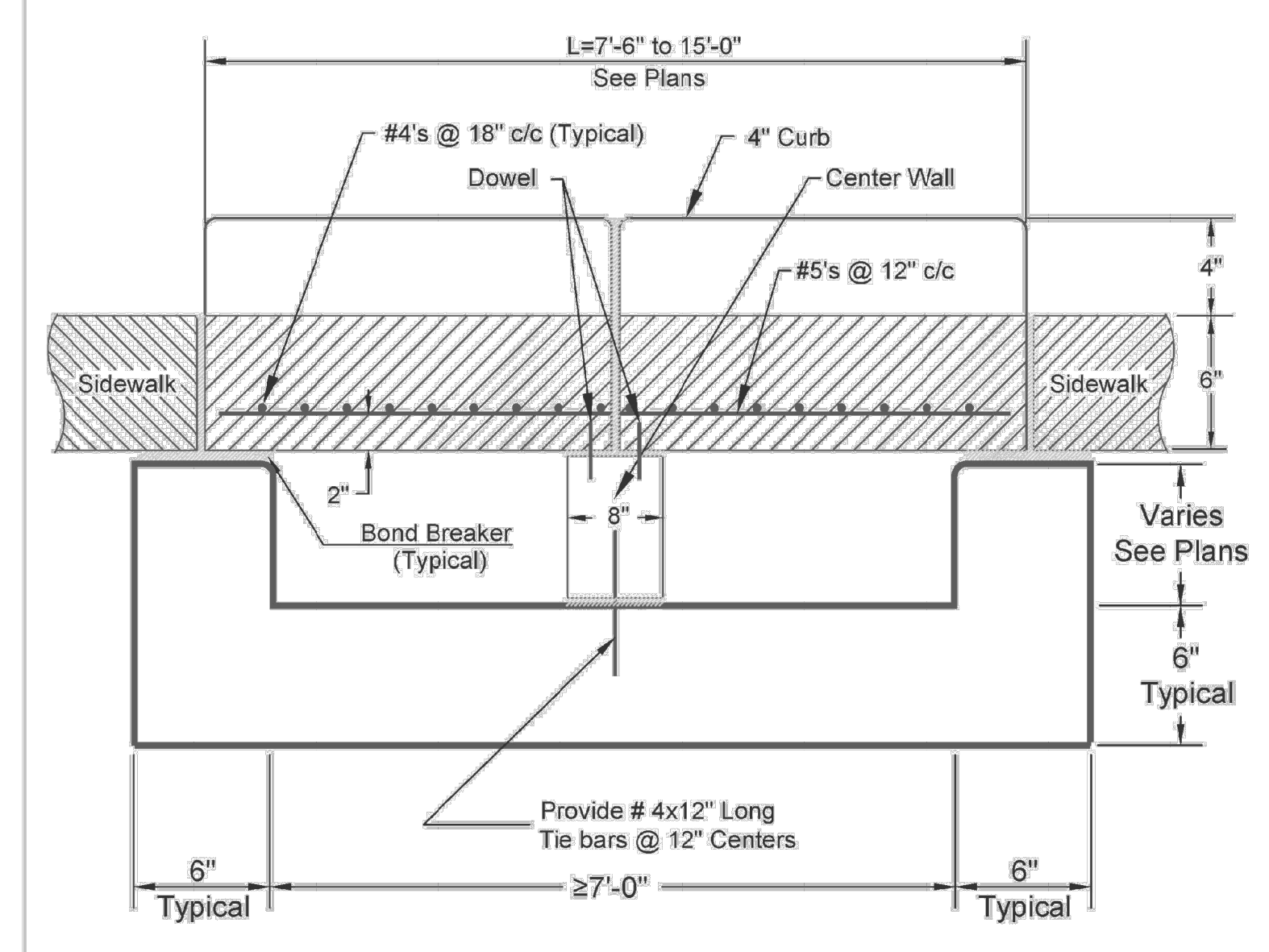
4821 PAN AMERICAN FWY
ALBUQUERQUE, NM

SHEET DESCRIPTION:
SITE DETAILS

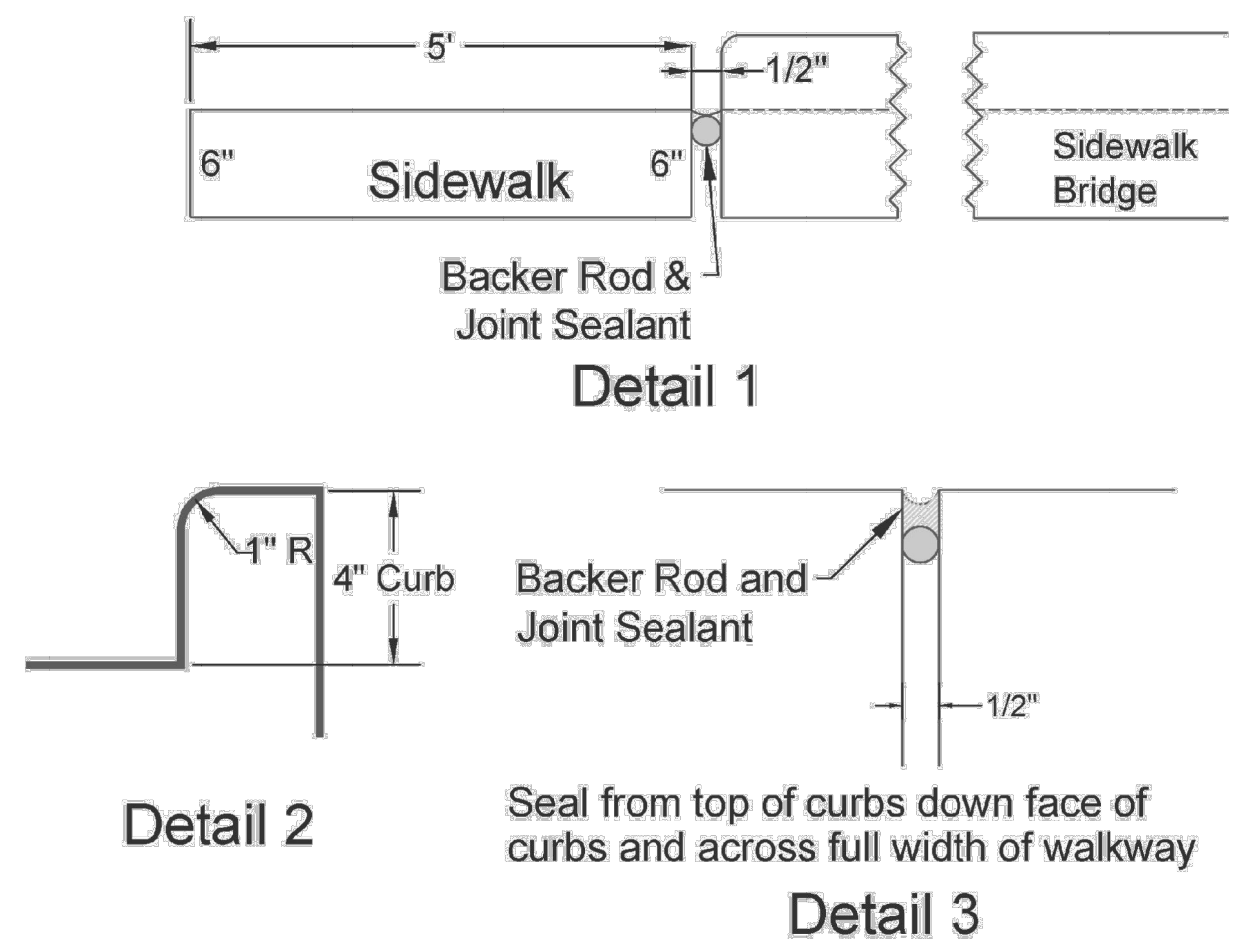
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/
#1	PERMIT REVISIONS	7/11/2
#2	PERMIT REVISIONS	10/31
#3	PERMIT REVISIONS	12/16
#8	CLIENT CHANGES	12/30
#9	NMDDOT REVISIONS	2/21/
#10	PERMIT REVISIONS	3/10/
#11	PERMIT REVISIONS	4/13/
#12	PERMIT REVISIONS	4/26/
#13	PERMIT REVISIONS	5/9/2
#14	PERMIT REVISIONS	5/24/

C10.0

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/1/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

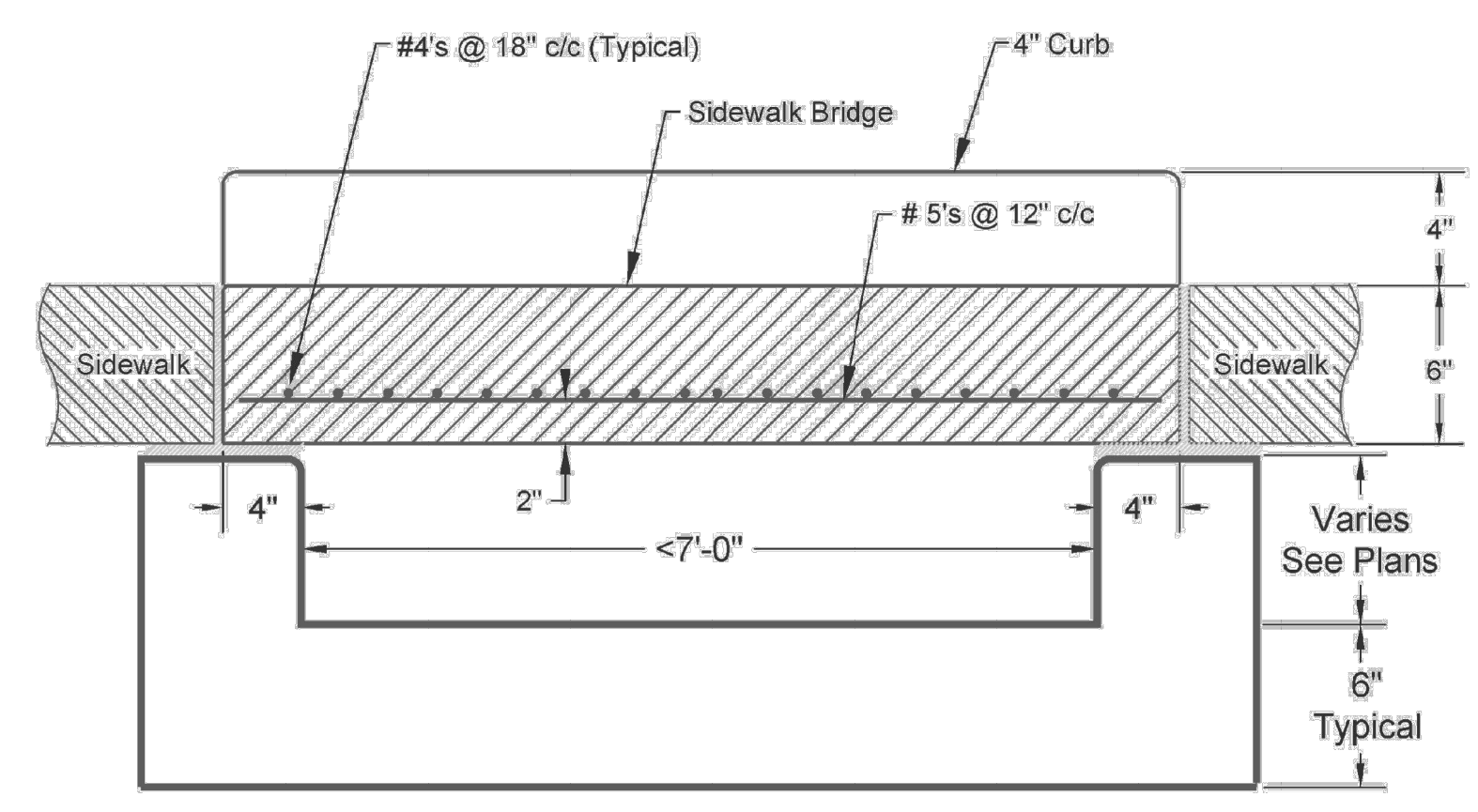


Section A-A for 7'-0" or greater span length

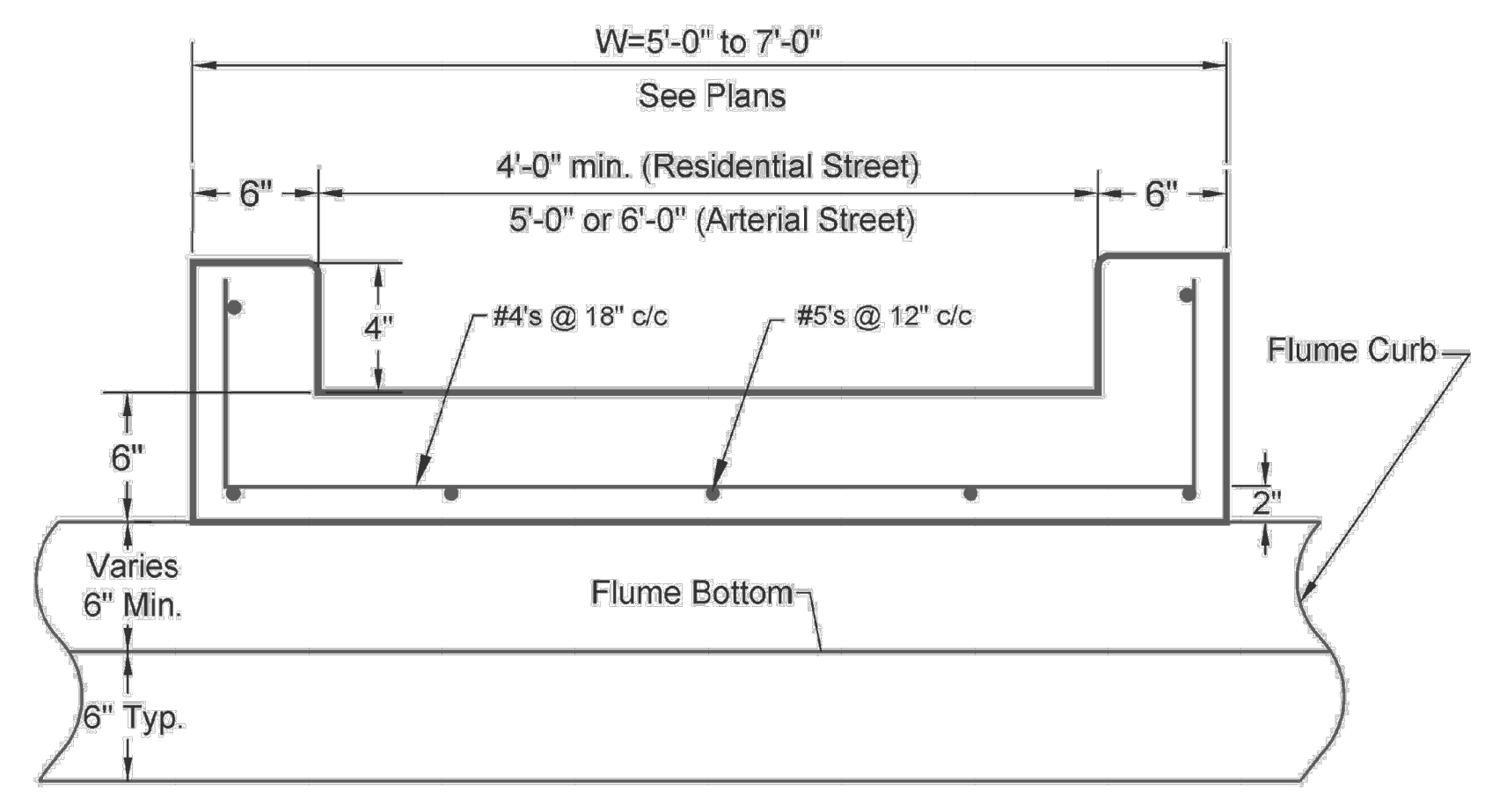


Sidewalk Bridge Notes:

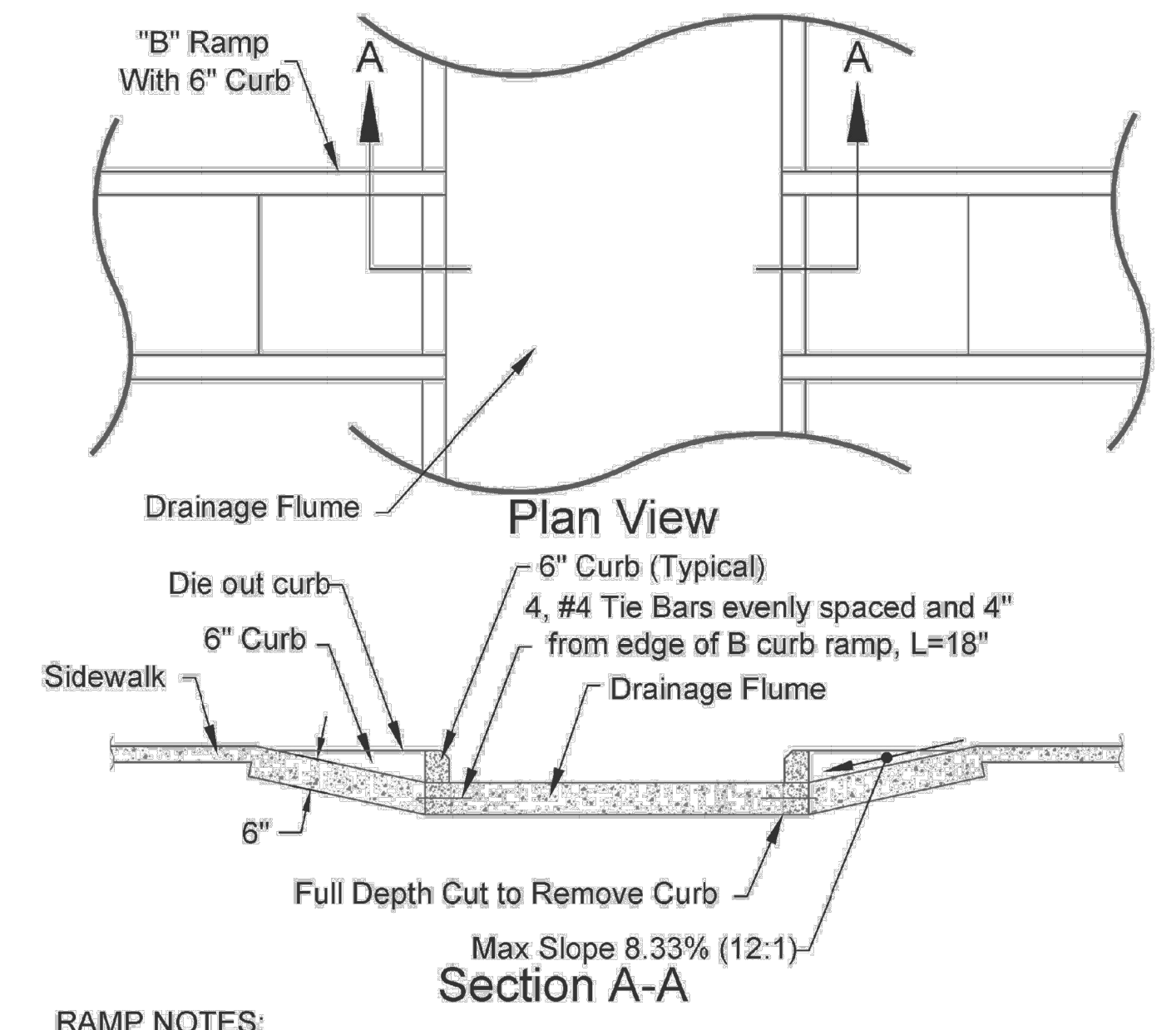
1. The width of the sidewalk bridge must be at least as wide as the adjacent sidewalk.
2. # 5 bars must extend to within 1 1/2" of the end forms.
3. Flume crossing with sidewalk bridges having a walking path greater than 5' wide require a separate design
4. Flume crossing with a dimension of 18" or greater from flume surface to the walkway surface will require pedestrian guardrail.
5. 3/4" chamfer (typical) required on all exposed edges.



Section A-A for less 7'-0" span length

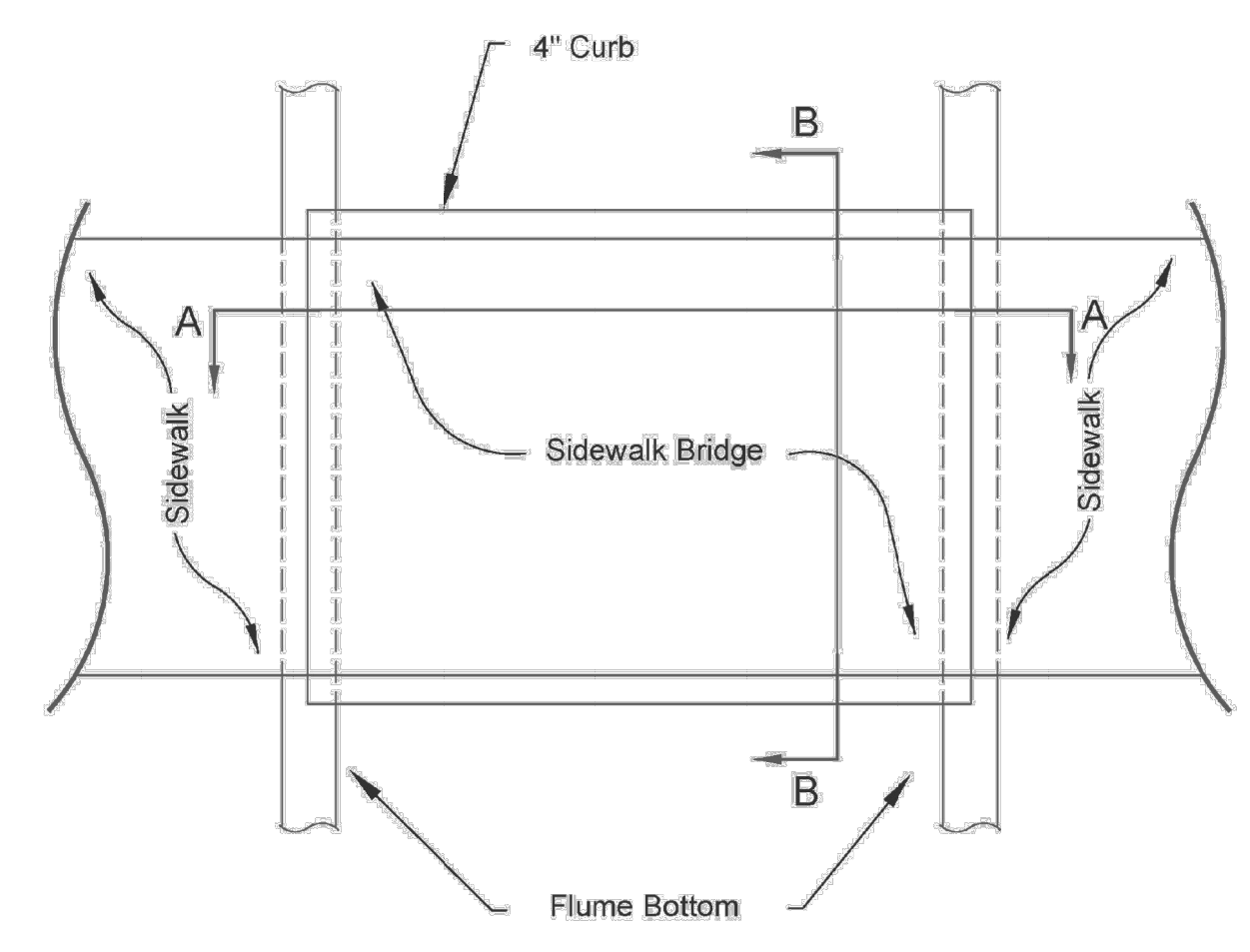


Section B-B



RAMP NOTES:

1. In most cases, to be constructed as shown above unless grade or other factors require the construction of a sidewalk bridge.
2. If flume grade is more than 2%, then bottom of flume must be reconstructed so the longitudinal grade is 2% or less to meet ADA requirements for sidewalk cross slope. The transition on each side of sidewalk in bottom of flume must be at least 2' wide and a maximum grade equal to 2x the longitudinal flume grade.



Sidewalk at Flume Crossing

ADA SIDEWALK AND CURB RAMP AT FLUME DETAILS

SIDEWALK AT FLUME DETAILS
 State Job No. 26985(04) Sheet No. R015

OKLAHOMA COUNTY COVELL & MACARTHUR BLVD.

sp "LS 11993"

Tract A-1
LLD Subdivision
(9/25/2014, 2014C-109)
NOT A PART
ZONING = NR-LM

PK Nail With
Tag "LS 11463"
N: 1506141.98
E: 1535076.14
Elev: 5109.13'

Rebar With
Cap "LS 15075"

PK Nail
With Tag
"LS 15075"

10 Reg. Spaces
Bathery Marker
With Cap "LS 14271"
Tract "B"
LLD Subdivision
(3/26/2004, 2004C-102)
NOT A PART

(Parcel I)
ZONING = NR-LM
NOT A PART

19 Reg. Spaces
18 Reg. Spaces
26 Reg. Spaces

Bathery Marker
Reference
Cap "LS 14271"
N: 1506127.26
E: 1535229.28
Elev: 5113.83'

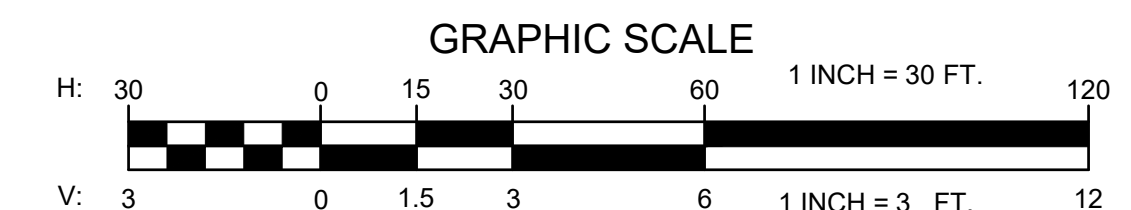
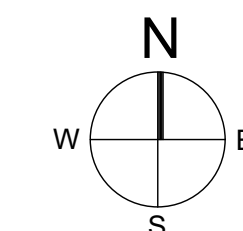
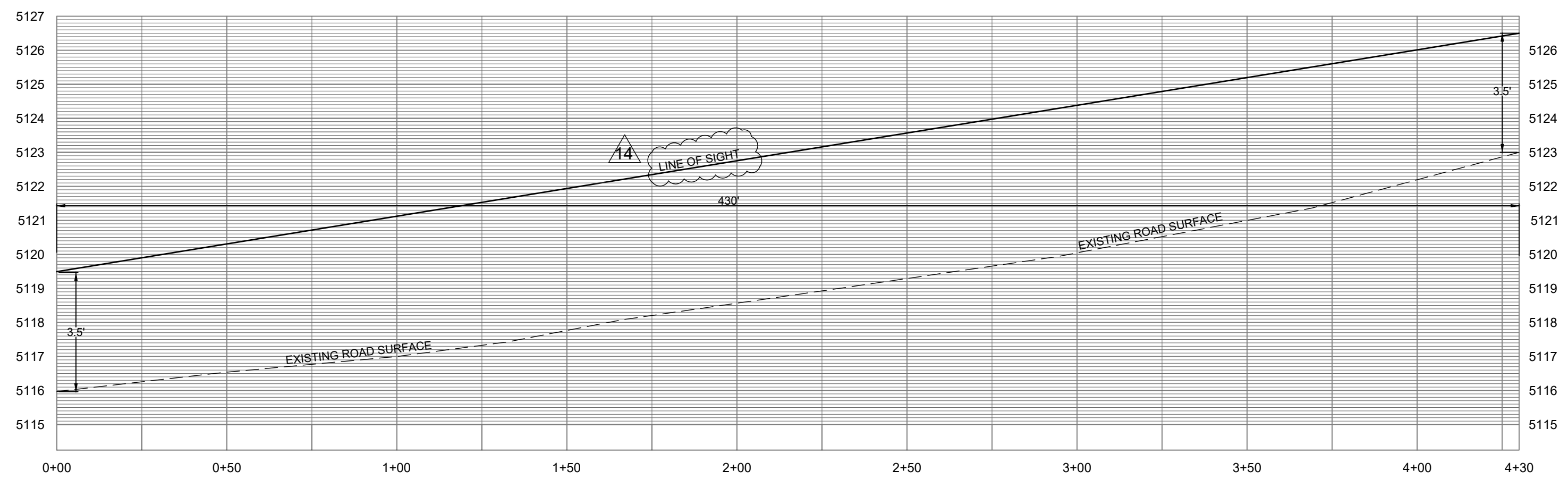
10 Reg. Spaces
19 Reg. Spaces

S OF ALBUQUERQUE
MOBILE SALES & SERVICE
EXISTING BLDG = 22,800 SF
ADDITION = 15,635 SF

4821 Pan American Freeway NE
Interstate 25 R/W = App. 325'

INTERSECTION
SIGHT DISTANCE LINE
BASED ON 45 MPH.
SEE SIGHT DISTANCE
EXHIBIT, SHEET C11.0.

PAN AMERICAN FWY. FRONTAGE ROAD (45 MPH)
INTERSECTION SIGHT DISTANCE



AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



PROJECT NAME:
LEXUS OF
ALBUQUERQUE

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

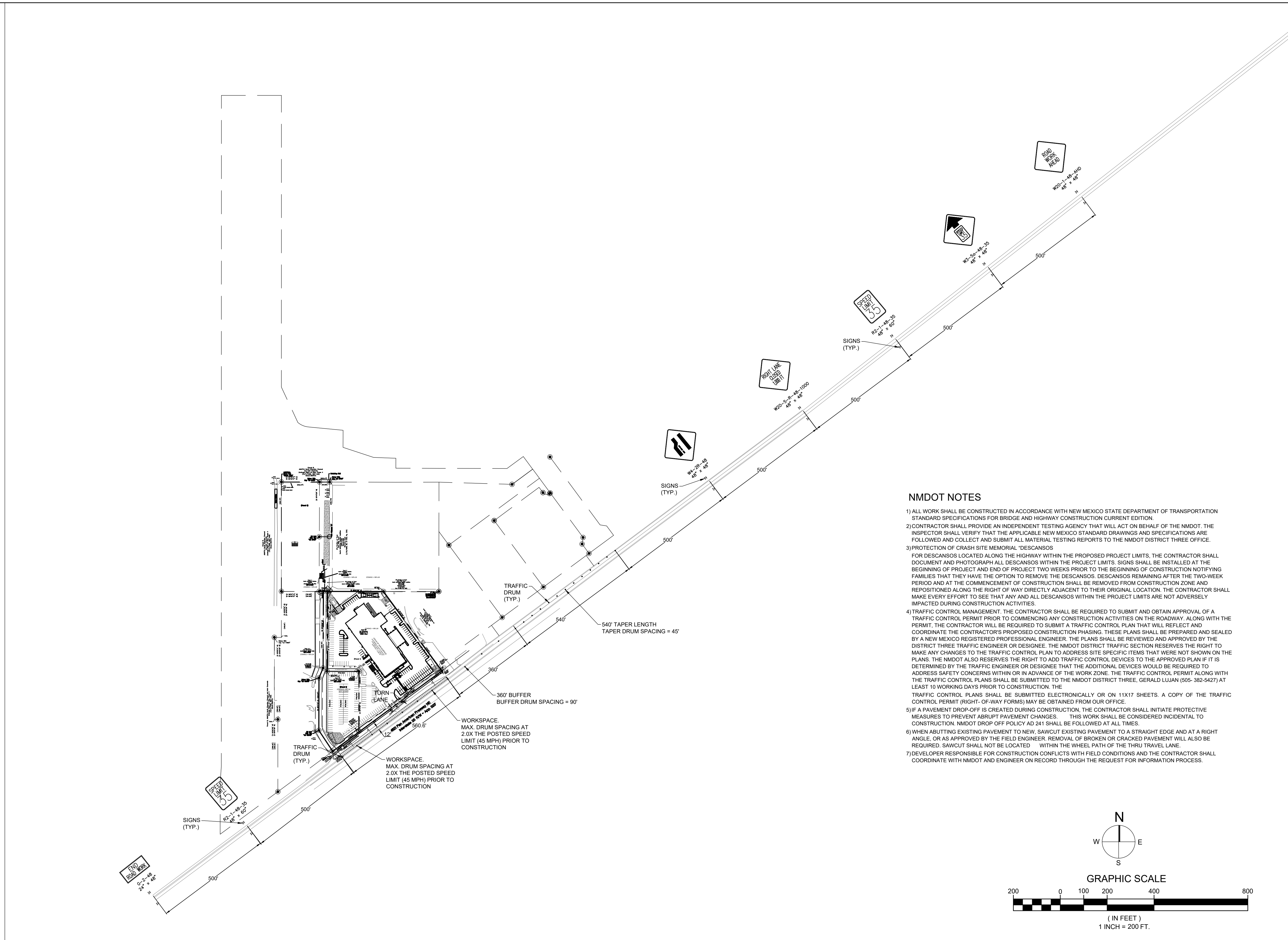
SHEET DESCRIPTION:
SIGHT DISTANCE
EXHIBIT

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
#1	FOR PERMIT	6/23/22
#2	PERMIT REVISIONS	7/11/22
#3	PERMIT REVISIONS	10/31/22
#4	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23



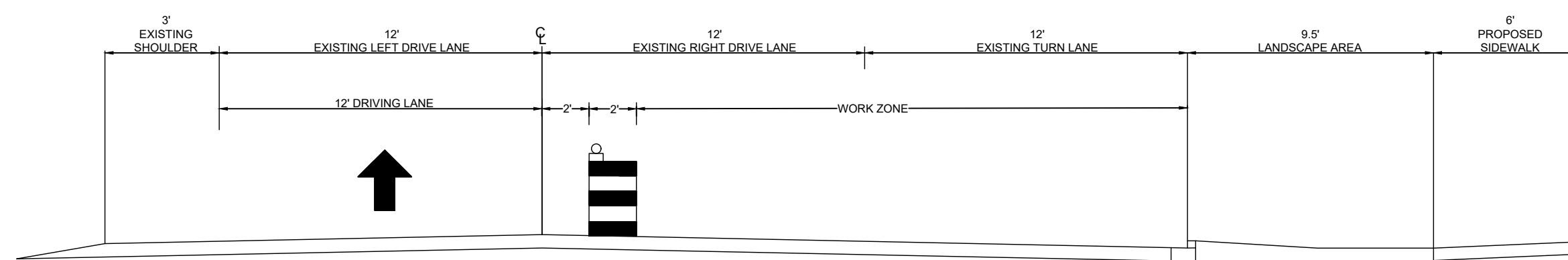
CALL NM ONE-CALL
SYSTEM SEVEN (7) DAYS
PRIOR TO ANY EXCAVATION

C11.0



NMDOT NOTES

- 1) ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH NEW MEXICO STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR BRIDGE AND HIGHWAY CONSTRUCTION CURRENT EDITION.
- 2) CONTRACTOR SHALL PROVIDE AN INDEPENDENT TESTING AGENCY THAT WILL ACT ON BEHALF OF THE NMDOT. THE INSPECTOR SHALL VERIFY THAT THE APPLICABLE NEW MEXICO STANDARD DRAWINGS AND SPECIFICATIONS ARE FOLLOWED AND COLLECT AND SUBMIT ALL MATERIAL TESTING REPORTS TO THE NMDOT DISTRICT THREE OFFICE.
- 3) PROTECTION OF CRASH SITE MEMORIAL "DESCANSOS"
FOR DESCANSOS LOCATED ALONG THE HIGHWAY WITHIN THE PROPOSED PROJECT LIMITS, THE CONTRACTOR SHALL DOCUMENT AND PHOTOGRAPH ALL DESCANSOS WITHIN THE PROJECT LIMITS. SIGNS SHALL BE INSTALLED AT THE BEGINNING OF PROJECT AND END OF PROJECT TWO WEEKS PRIOR TO THE BEGINNING OF CONSTRUCTION NOTIFYING FAMILIES THAT THEY HAVE THE OPTION TO REMOVE THE DESCANSOS. DESCANSOS REMAINING AFTER THE TWO-WEEK PERIOD AND AT THE COMMENCEMENT OF CONSTRUCTION SHALL BE REMOVED FROM CONSTRUCTION ZONE AND REPOSITIONED ALONG THE RIGHT OF WAY DIRECTLY ADJACENT TO THEIR ORIGINAL LOCATION. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO SEE THAT ANY AND ALL DESCANSOS WITHIN THE PROJECT LIMITS ARE NOT ADVERSELY IMPACTED DURING CONSTRUCTION ACTIVITIES.
- 4) TRAFFIC CONTROL MANAGEMENT. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT AND OBTAIN APPROVAL OF A TRAFFIC CONTROL PERMIT PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES ON THE ROADWAY. ALONG WITH THE PERMIT, THE CONTRACTOR WILL BE REQUIRED TO SUBMIT A TRAFFIC CONTROL PLAN THAT WILL REFLECT AND COORDINATE THE CONTRACTOR'S PROPOSED CONSTRUCTION PHASING. THESE PLANS SHALL BE PREPARED AND SEALED BY A NEW MEXICO REGISTERED PROFESSIONAL ENGINEER. THE PLANS SHALL BE REVIEWED AND APPROVED BY THE DISTRICT THREE TRAFFIC ENGINEER OR DESIGNEE. THE NMDOT DISTRICT TRAFFIC SECTION RESERVES THE RIGHT TO MAKE ANY CHANGES TO THE TRAFFIC CONTROL PLAN TO ADDRESS SITE SPECIFIC ITEMS THAT WERE NOT SHOWN ON THE PLANS. THE NMDOT ALSO RESERVES THE RIGHT TO ADD TRAFFIC CONTROL DEVICES TO THE APPROVED PLAN IF IT IS DETERMINED BY THE TRAFFIC ENGINEER OR DESIGNEE THAT THE ADDITIONAL DEVICES WOULD BE REQUIRED TO ADDRESS SAFETY CONCERNS WITHIN OR IN ADVANCE OF THE WORK ZONE. THE TRAFFIC CONTROL PERMIT ALONG WITH THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED TO THE NMDOT DISTRICT THREE, GERALD LUNAM (605-862-8627) AT LEAST 10 WORKING DAYS PRIOR TO CONSTRUCTION. THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED ELECTRONICALLY OR ON 11X17 SHEETS. A COPY OF THE TRAFFIC CONTROL PERMIT (RIGHT-OF-WAY FORMS) MAY BE OBTAINED FROM OUR OFFICE.
- 5) IF A PAVEMENT DROP-OFF IS CREATED DURING CONSTRUCTION, THE CONTRACTOR SHALL INITIATE PROTECTIVE MEASURES TO PREVENT ABRUPT PAVEMENT CHANGES. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION. NMDOT DROP-OFF POLICY 42.241 SHALL BE FOLLOWED AT ALL TIMES.
- 6) WHEN ABUTTING EXISTING PAVEMENT TO NEW, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER, REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED. SAWCUT SHALL NOT BE LOCATED WITHIN THE WHEEL PATH OF THE THRU TRAVEL LANE.
- 7) DEVELOPER RESPONSIBLE FOR CONSTRUCTION CONFLICTS WITH FIELD CONDITIONS AND THE CONTRACTOR SHALL COORDINATE WITH NMDOT AND ENGINEER ON RECORD THROUGH THE REQUEST FOR INFORMATION PROCESS.



14 WORK ZONE TYPICAL SECTION

NOT TO SCALE

AYER DESIGN GROUP
PROFESSIONAL ENGINEERING SERVICES
215 Johnston Street
Rock Hill, SC 29730
Phone: 803-328-5858



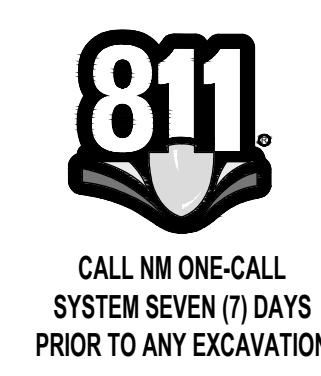
PROJECT NAME: 5-24-23

LEXUS OF ALBUQUERQUE

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM

SHEET DESCRIPTION:
TEMPORARY TRAFFIC CONTROL PLAN

PROJECT #	2021-12	
DRAWN BY	CC	
CHECKED BY	BA	
NO	ISSUE/REVISION	DATE
	FOR PERMIT	6/23/22
#1	PERMIT REVISIONS	7/11/22
#2	PERMIT REVISIONS	10/31/22
#3	PERMIT REVISIONS	12/16/22
#8	CLIENT CHANGES	12/30/22
#9	NMDOT REVISIONS	2/21/23
#10	PERMIT REVISIONS	3/10/23
#11	PERMIT REVISIONS	4/13/23
#12	PERMIT REVISIONS	4/26/23
#13	PERMIT REVISIONS	5/9/23
#14	PERMIT REVISIONS	5/24/23

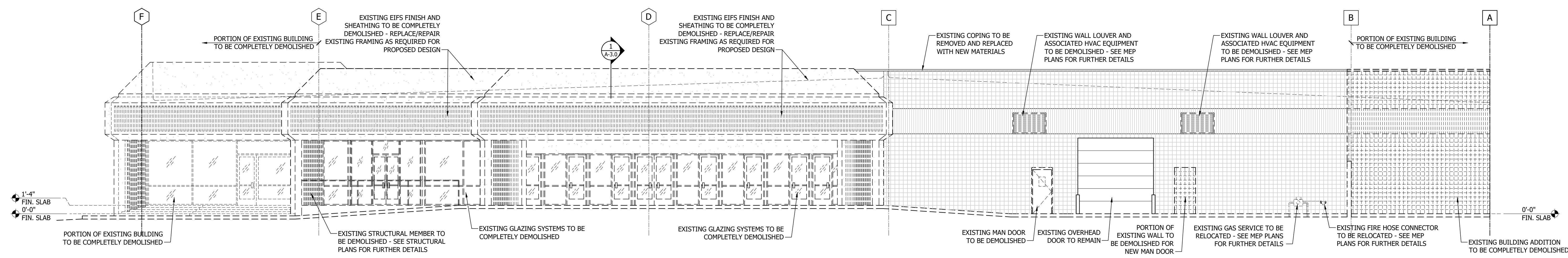


C12.0

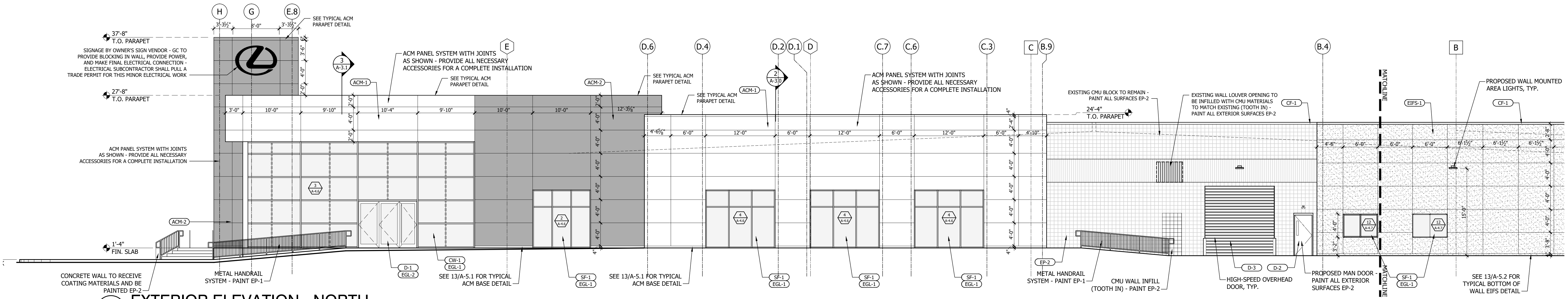
PROJECT #	1921	
DRAWN BY	DKC	
CHECKED BY	DKC	
NO	ISSUE/REVISION	DATE
1	PERMIT SET	04/08/22
2	PERMIT COMMENTS	06/17/22
3	BID SET	07/05/22

THESE DRAWINGS ARE TO BE USED IN CONJUNCTION WITH THE WRITTEN SPECIFICATION BOOK ISSUED AS THE COMBINED CONSTRUCTION DOCUMENTS PACKAGE. GC SHALL NOTIFY THE ARCHITECT DURING THE BIDDING PROCESS IF THERE IS A DISCREPANCY BETWEEN THE DRAWINGS AND SPECIFICATION BOOK.

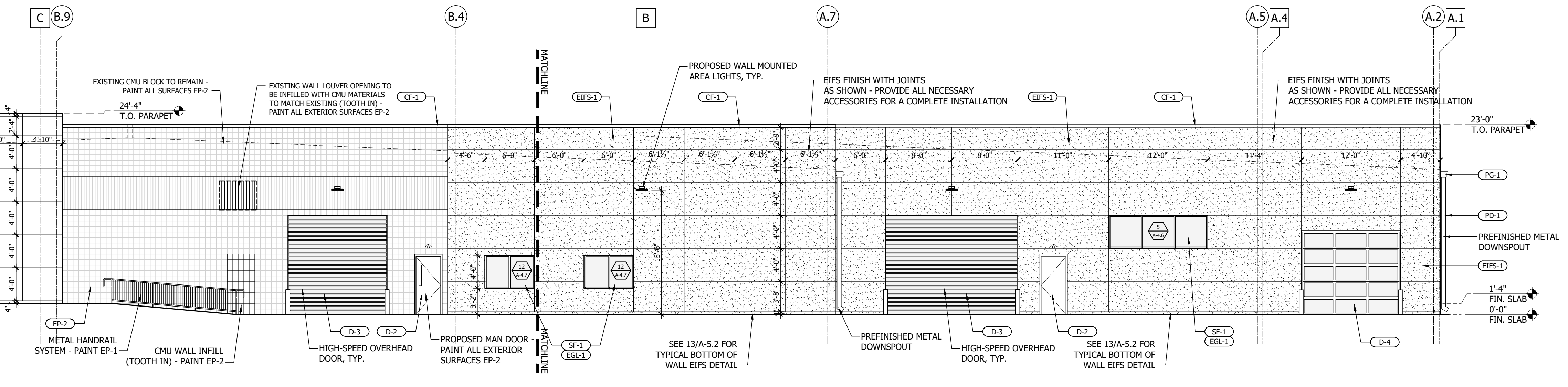
NOTE: ALL SIGNAGE INDICATED ON THIS SHEET IS FOR REPRESENTATIONAL PURPOSES ONLY. THE OWNER SHALL SECURE A CONTRACT WITH A SIGNAGE VENDOR WHO WILL BE RESPONSIBLE FOR APPLYING FOR AND OBTAINING A SIGNAGE PERMIT. THE GC SHALL BE RESPONSIBLE FOR COORDINATING THE SIGNAGE INSTALLATION AND ALL FINAL CONNECTIONS.



1
EXTERIOR ELEVATION - NORTH (DEMO)
SCALE: 1/8" = 1'-0"



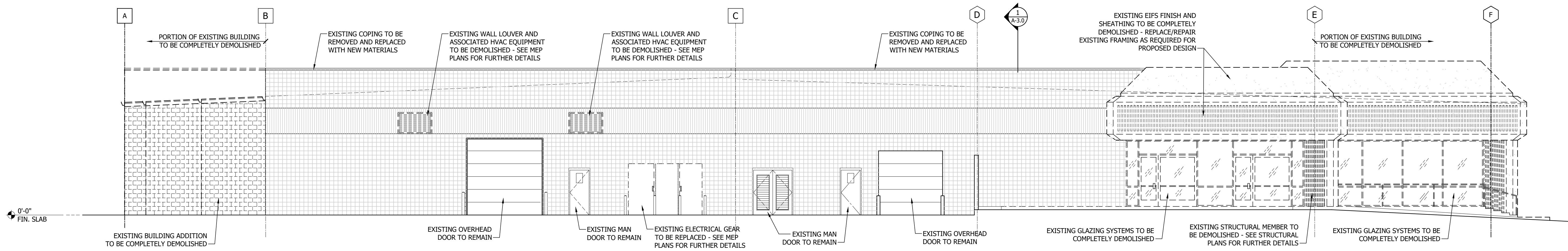
2
EXTERIOR ELEVATION - NORTH
SCALE: 1/8" = 1'-0"



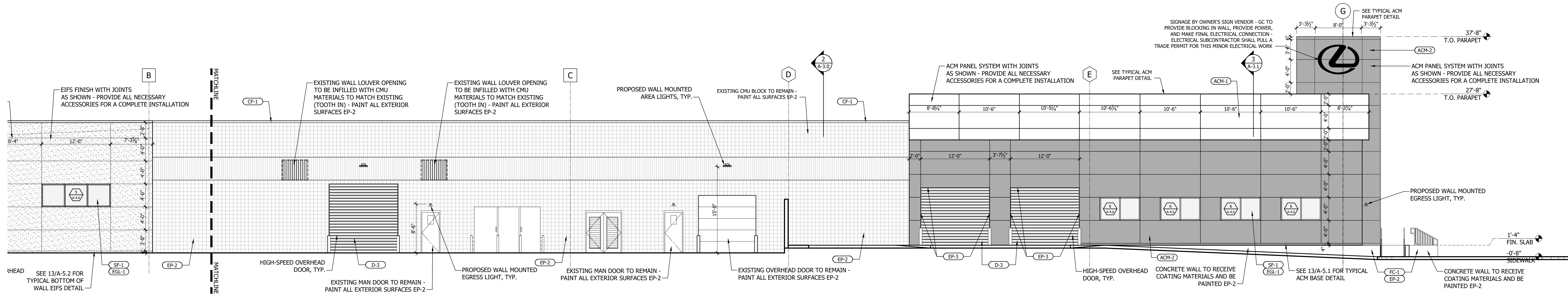
2
EXTERIOR ELEVATION - NORTH
SCALE: 1/8" = 1'-0"

EXTERIOR MATERIAL SCHEDULE

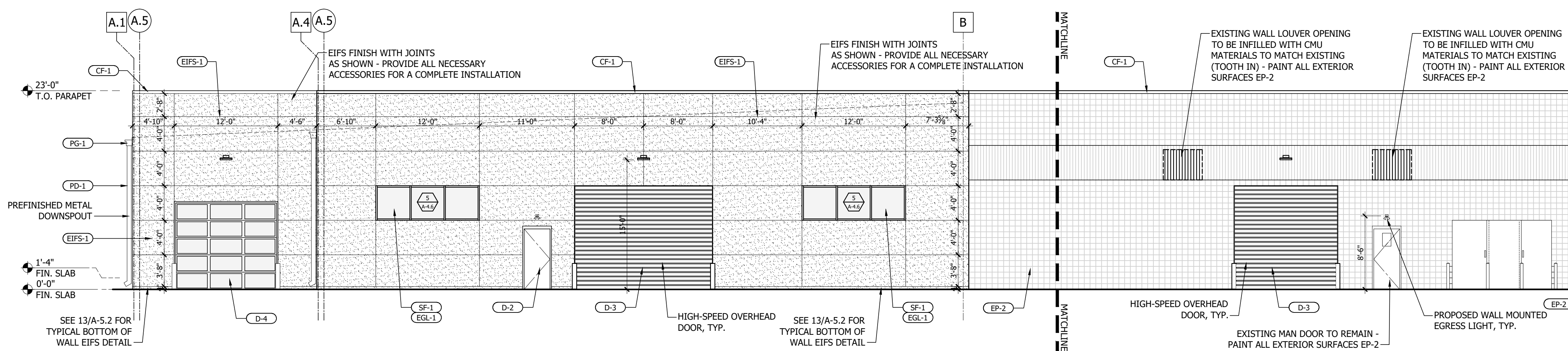
TAG	DESCRIPTION	MANUFACTURER	MODEL NUMBER/COLOR	NOTES / REMARKS
ACM-1	COMPOSITE METAL CLADDING	ALPOLIC	MICA ANODIC CLEAR (SILVER), SOBOTEC SL-2000 DRYJOINT SYSTEM, MAX 1/2" WIDE JOINTS - SEE SPECIFICATION FOR REQUIREMENTS	JOINT LOCATIONS AS INDICATED ON ELEVATIONS, FLASHING TO MATCH ADJACENT WALL PANELS, PROVIDE ALL NECESSARY ACCESSORIES FOR A COMPLETE INSTALLATION
ACM-2	COMPOSITE METAL CLADDING	ALPOLIC	MICA GREY (CHARCOAL) - SOBOTEC SL-2000 DRYJOINT SYSTEM - MAX 1/2" WIDE JOINTS - SEE SPECIFICATION FOR REQUIREMENTS	JOINT LOCATIONS AS INDICATED ON ELEVATIONS, FLASHING TO MATCH ADJACENT WALL PANELS - PROVIDE ALL NECESSARY ACCESSORIES FOR A COMPLETE INSTALLATION
EIFS-1	EXTERIOR INSULATING FINISH SYSTEM	DRYVIT	OUTSULATION - SANDPERBLE FINE FINISH - COLOR #133 DRIFTWOOD (MATCH EP-2)	BARRIER SYSTEM WITH HIGH TRAFFIC REINFORCING MESH (PANZER 20) TO BE USED FROM SIDEWALK/GRADE UP TO 6'-0" A.F.F. MINIMUM
CF-1	CAP FLASH / METAL COPING	DMG ROOFING PRODUCTS	4" TALL (SEE ELEVATIONS) - COLOR 'SILVERSMITH' A-30 (PREMIUM FINISH OPTION)	EXISTING CMU WALLS RECEIVING NEW COPING AND ALL PROPOSED EIFS FINISHED WALLS, ANY EXISTING COPING SCHEDULED TO REMAIN SHALL BE PAINTED TO MATCH THIS MATERIAL
CW-1	CURTAIN WALL SYSTEM	KAWNEER OR APPROVED EQUAL	1600 SYSTEM 2 - 555 VERTICAL 10-1/2" TOTAL DEPTH PARTIAL EXTERIOR BUTT GLAZED SYSTEM - CLEAR ANODIZED	CURTAIN WALL PROVIDER TO DETERMINE REINFORCING REQUIREMENTS
D-1	GLASS & ALUMINUM MAN DOOR	KAWNEER OR APPROVED EQUAL	STANDARD MEDIUM STYLE CENTER HUNG DOOR - CLEAR ANODIZED	SEE DOOR SCHEDULE FOR SIZE AND LOCATIONS
D-2	METAL MAN DOOR	-	PAINTED TO MATCH ADJACENT WALL	SEE DOOR SCHEDULE FOR SIZE AND LOCATIONS
D-3	HIGH SPEED OVERHEAD DOOR	RYTEC	METAL COLOR TO MATCH CW-1, CLEAR ANODIZED	SEE DOOR SCHEDULE FOR SIZE AND LOCATIONS
D-4	OVERHEAD DOOR	OVERHEAD DOOR COMPANY	METAL COLOR TO MATCH CW-1, CLEAR ANODIZED	SEE DOOR SCHEDULE FOR SIZE AND LOCATIONS
EGL-1	EXTERIOR GLASS - VERTICAL	PPG / VITRO	1" THICK INSULATING GLASS - 6MM SOLARBAN 90 (2) ON CLEAR (LOW IRON) + 1/2" AIR SPACE + 6MM CLEAR (LOW IRON)	SILICONE EDGE SEALANT - INTER-PANE SPACE TO CONTAIN PURGED, DRY HERMETIC AIR - MAX. U-VALUE (WINTER) - .28, MAX. SHGC - .23, MAX. SHADING COEFFICIENT - .32, VISIBLE LIGHT TRANSMITTANCE - 51%
EGL-2	EXTERIOR GLASS - OPERABLE DOORS	PPG / VITRO	9/16" THICK LAMINATED GLASS - 6MM CLEAR (LOW IRON) + .060 PVB INTERLAYER + 6MM CLEAR (LOW IRON)	SILICONE EDGE SEALANT - MAX. U-VALUE (WINTER) - .95, MAX. SHGC - .71, MAX. SHADING COEFFICIENT - .82, VISIBLE LIGHT TRANSMITTANCE - 86% (TO BE USED AT OPERABLE MAN DOORS ONLY - ALL OTHER LOCATIONS TO BE EGL-1)
EP-1	EXTERIOR PAINT	BENJAMIN MOORE	GRANITE #AF-660 - SEMI-GLOSS - COLOR TO MATCH ACM-2	SEE ELEVATIONS FOR LOCATIONS
EP-2	EXTERIOR PAINT	BENJAMIN MOORE	PORTLAND GRAY #2109-60 - LOW LUSTER AT CMU/EIFS - SEMI-GLOSS AT ALL METAL	SEE ELEVATIONS FOR LOCATIONS
EP-3	EXTERIOR PAINT	SHERWIN WILLIAMS	GRAY CLOUDS SW-7658 - SEMI-GLOSS - COLOR TO MATCH ACM-1	SEE ELEVATIONS FOR LOCATIONS
PD-1	PRE-FORMED DOWNSPOUT	VARIOUS	6" x 6" x .024 RECTANGULAR ALUMINUM DOWNSPOUT - FINISH ALL SIDES TO MATCH EP-2	PROVIDE ALL ACCESSORIES NECESSARY FOR A COMPLETE SYSTEM
PG-1	PRE-FORMED GUTTER	VARIOUS	8" ALUMINUM BOX GUTTER - FINISH ALL SIDES TO MATCH EP-2	PROVIDE ALL ACCESSORIES NECESSARY FOR A COMPLETE SYSTEM
SF-1	STOREFRONT GLAZING SYSTEM	KAWNEER OR APPROVED EQUAL	TRIFAB 451T (EXTERIOR) / 451 (INTERIOR) - 4-1/2" TOTAL DEPTH - FRONT GLAZED - CLEAR ANODIZED ALUMINUM	GLAZING PROVIDER TO DETERMINE REINFORCING REQUIREMENTS
FC-1	FOUNDATION COATING SYSTEM	MASTERSSEAL OR APPROVED EQUAL	MASTERSSEAL 582 (FORMERLY THOROSEAL) WATERPROOF CEMENT-BASED COATING, ADD MASTERSSEAL 666 ADMIXTURE, TOP COAT WITH EP-2 (ELASTOMERIC PAINT)	TAPE AND SEAL ALL JOINTS IN EXTERIOR CEMENT BOARD SHEATHING PRIOR TO APPLICATION



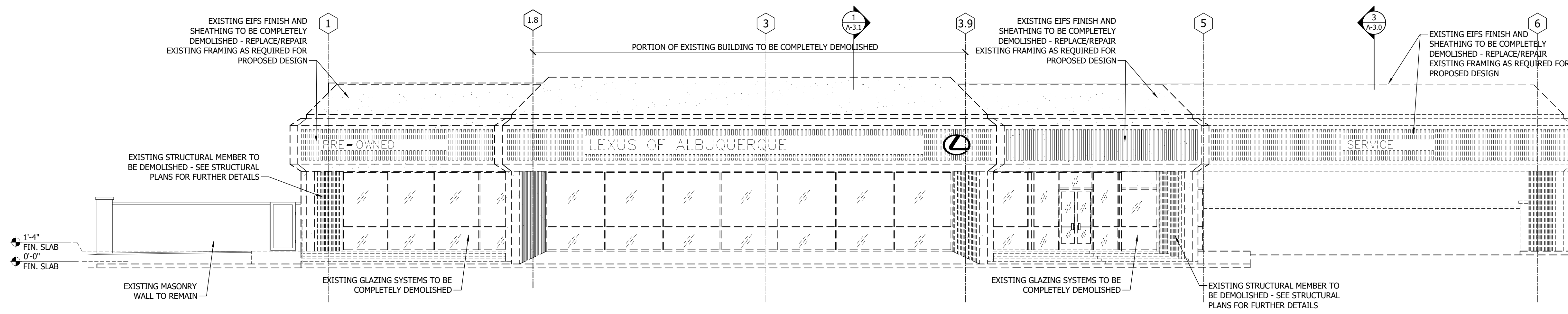
1 EXTERIOR ELEVATION - SOUTH (DEMO)
A-2.1 SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - SOUTH
A-2.1 SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - SOUTH
A-2.1 SCALE: 1/8" = 1'-0"



3 EXTERIOR ELEVATION - EAST (DEMO)
A-2.1 SCALE: 1/8" = 1'-0"

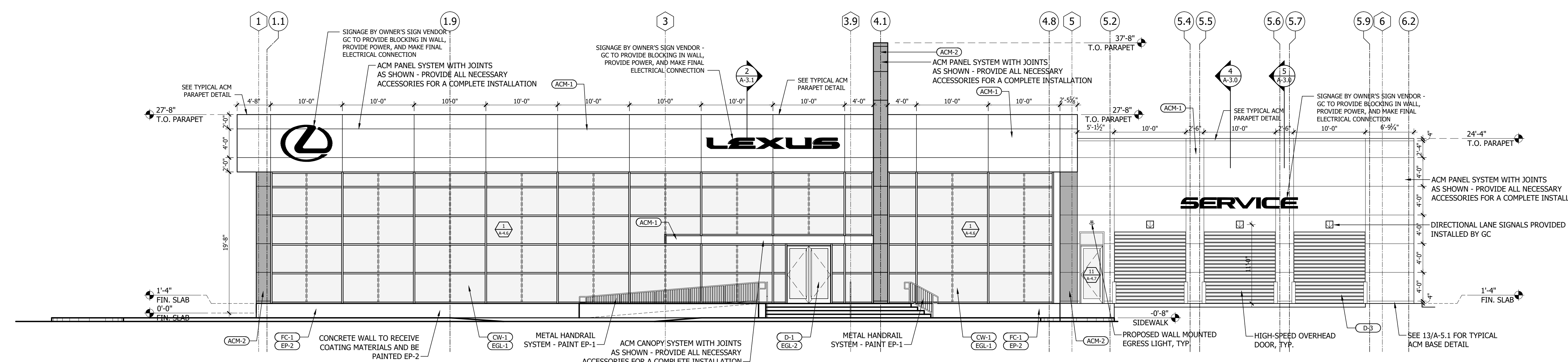
PROJECT NAME:
**LEXUS
OF
ALBUQUERQUE**
4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM 87109

SHEET DESCRIPTION:
EXTERIOR ELEVATIONS

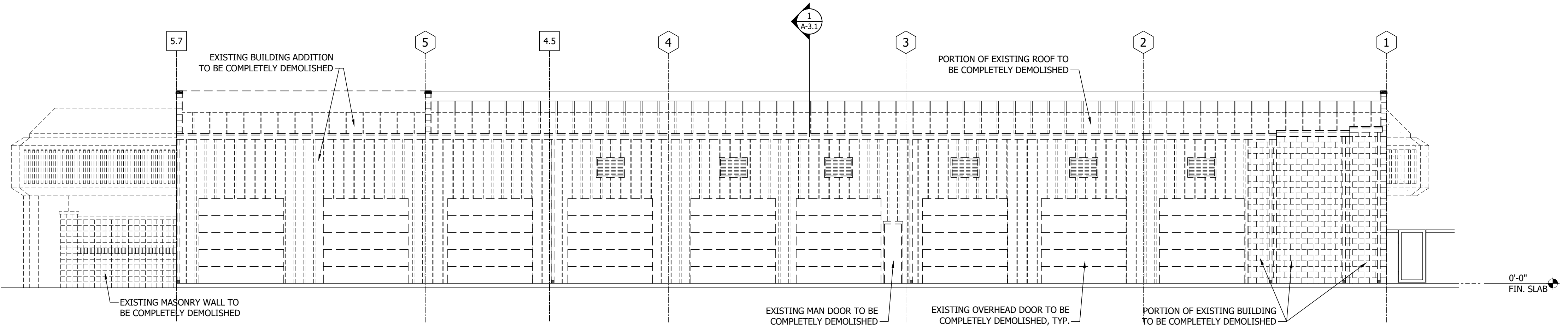
NO.	ISSUE/REVISION	DATE
	PERMIT SET	04/08/22
	PERMIT COMMENTS	06/17/22
	BID SET	07/05/22

THESE DRAWINGS ARE TO BE USED IN CONJUNCTION WITH THE WRITTEN SPECIFICATION BOOK ISSUED AS THE COMBINED CONSTRUCTION DOCUMENTS PACKAGE. GC SHALL NOTIFY THE ARCHITECT DURING THE BIDDING PROCESS IF THERE IS A DISCREPANCY BETWEEN THE DRAWINGS AND SPECIFICATION BOOK.

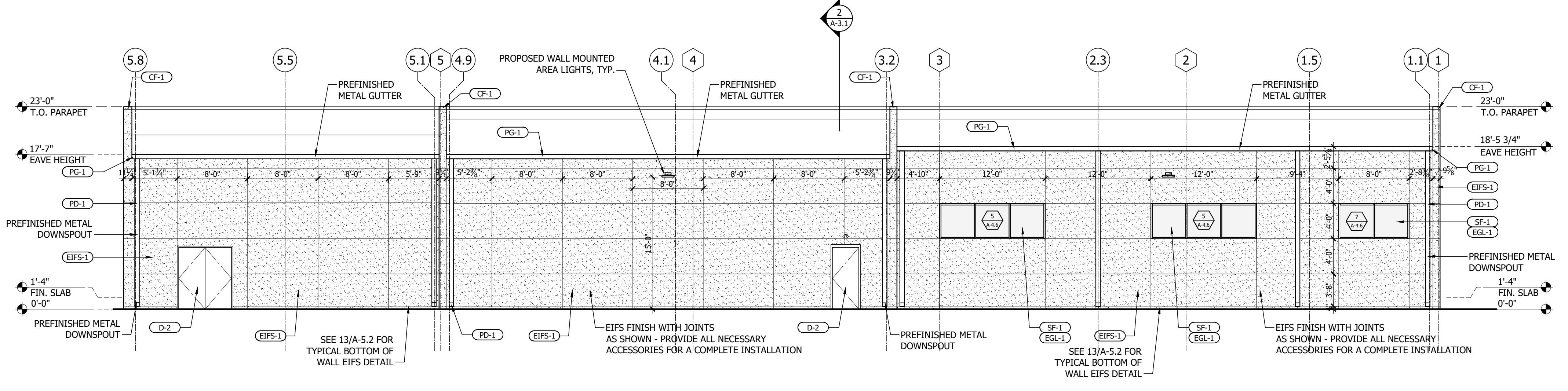
NOTE:
ALL SIGNAGE INDICATED ON THIS SHEET IS FOR REPRESENTATIONAL PURPOSES ONLY. THE OWNER SHALL SECURE A CONTRACT WITH A SIGNAGE VENDOR WHO WILL BE RESPONSIBLE FOR APPLYING FOR AND OBTAINING A SIGNAGE PERMIT. THE GC SHALL BE RESPONSIBLE FOR COORDINATING THE SIGNAGE INSTALLATION AND ALL FINAL CONNECTIONS.



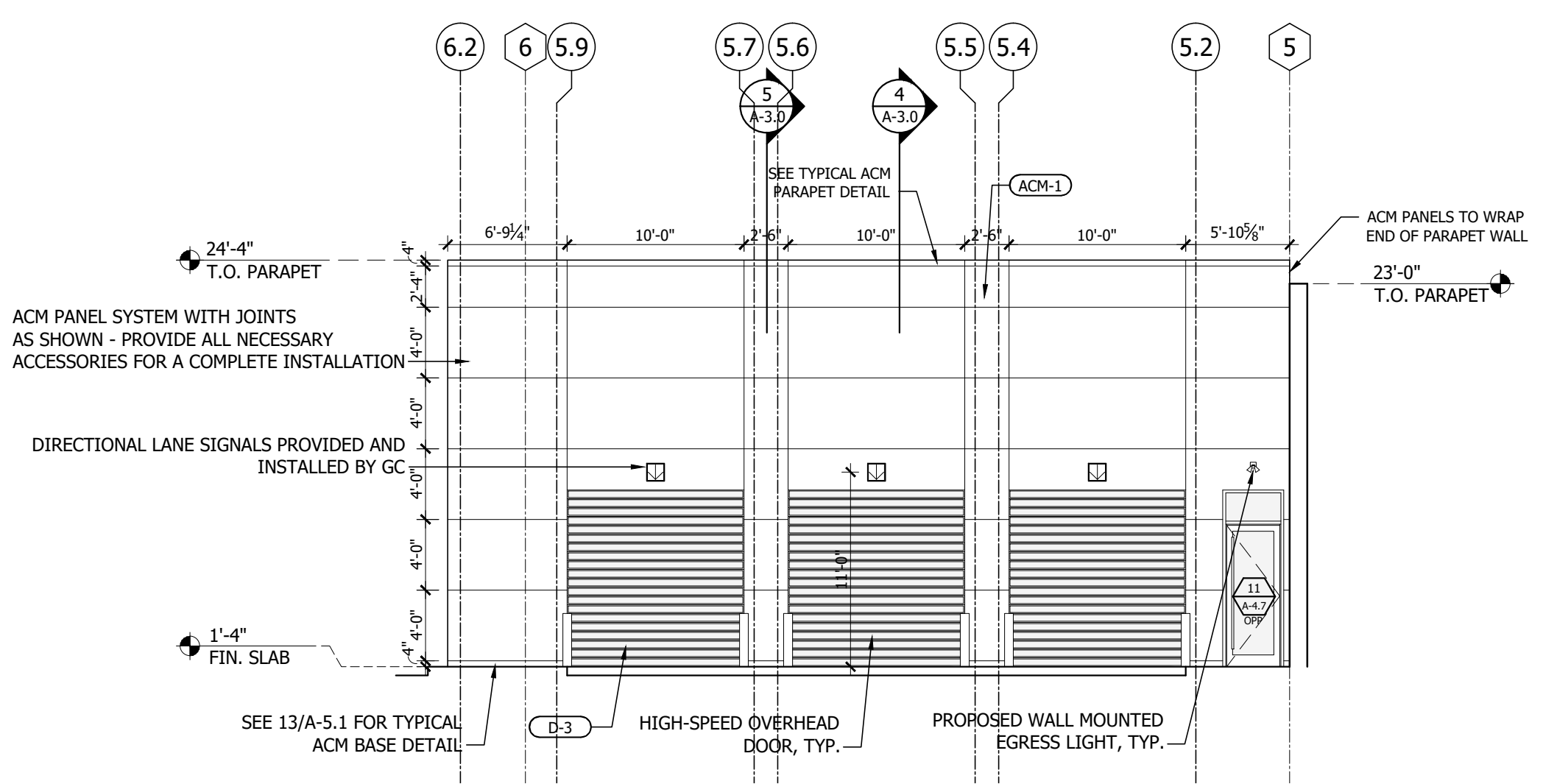
1 EXTERIOR ELEVATION - EAST
SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - WEST (DEMO)
SCALE: 1/8" = 1'-0"



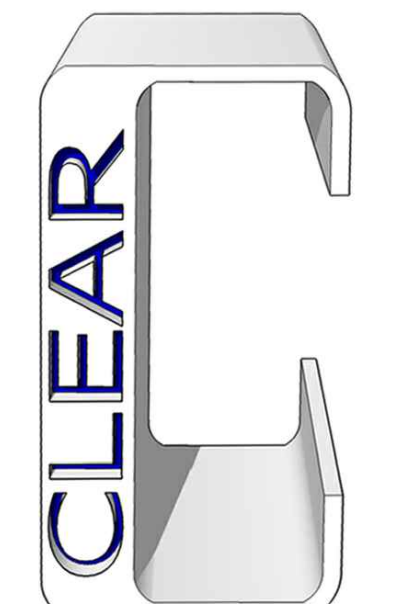
3 EXTERIOR ELEVATION - WEST
SCALE: 1/8" = 1'-0"



4 EXTERIOR ELEVATION - WEST
SCALE: 1/8" = 1'-0"

THESE DRAWINGS ARE TO BE USED IN CONJUNCTION WITH THE WRITTEN SPECIFICATION BOOK ISSUED AS THE COMBINED CONSTRUCTION DOCUMENTS PACKAGE. GC SHALL NOTIFY THE ARCHITECT DURING THE BIDDING PROCESS IF THERE IS A DISCREPANCY BETWEEN THE DRAWINGS AND SPECIFICATION BOOK.

NOTE: ALL SIGNAGE INDICATED ON THIS SHEET IS FOR REPRESENTATIONAL PURPOSES ONLY. THE OWNER SHALL SECURE A CONTRACT WITH A SIGNAGE VENDOR WHO WILL BE RESPONSIBLE FOR APPLYING FOR AND OBTAINING A SIGNAGE PERMIT. THE GC SHALL BE RESPONSIBLE FOR COORDINATING THE SIGNAGE INSTALLATION AND ALL FINAL CONNECTIONS.



**architectural
DESIGN**

Clear Architectural Design, LLC
10605 Concord St., Suite 440
Kensington, MD 20895
240.396.2453



PROJECT NAME:

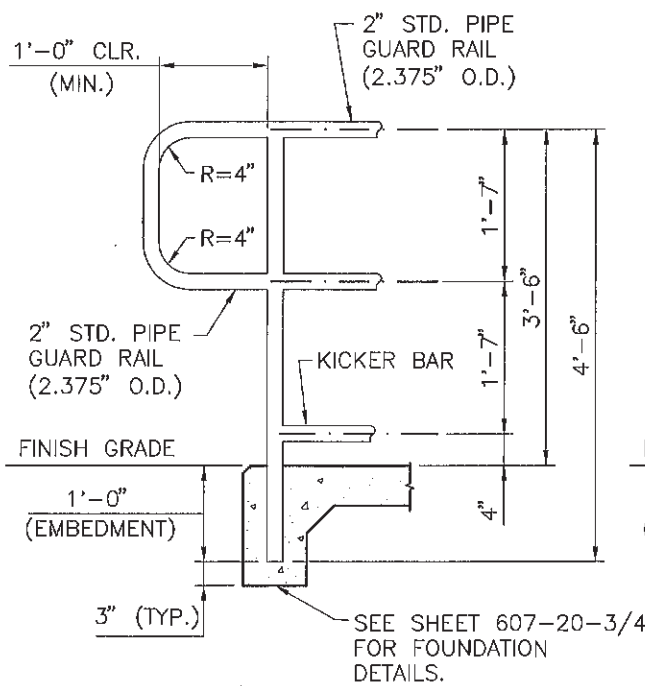
**LEXUS
OF
ALBUQUERQUE**

4821 PAN AMERICAN FWY.
ALBUQUERQUE, NM 87109

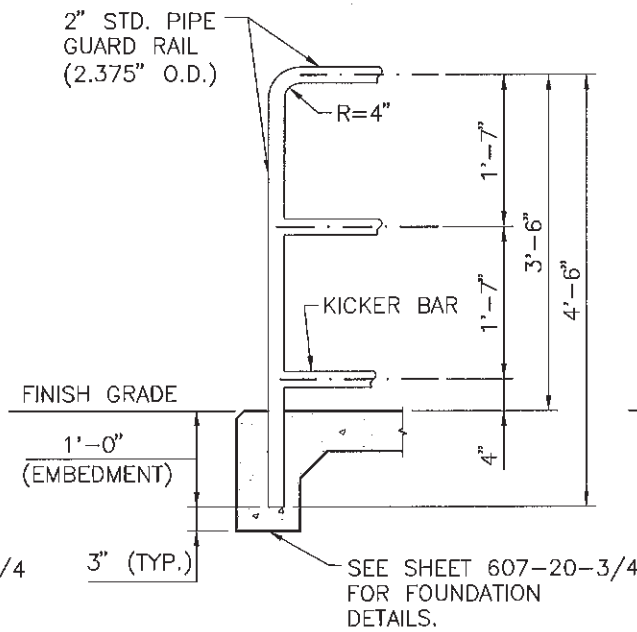
SHEET DESCRIPTION:
EXTERIOR ELEVATIONS

PROJECT #	1921	
DRAWN BY	DKC	
CHECKED BY	DKC	
NO	ISSUE/REVISION	DATE
1	PERMIT SET	04/08/22
2	PERMIT COMMENTS	06/17/22
3	BID SET	07/05/22

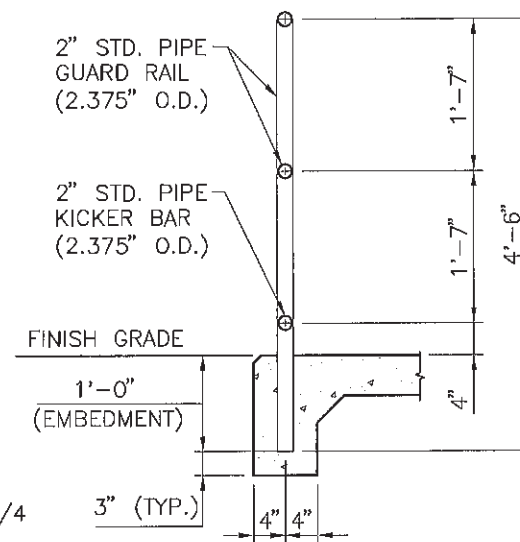
A-2.2



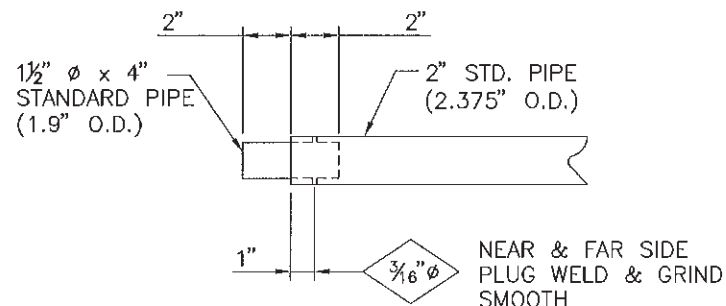
**DETAIL @ RAIL END
TYPE A**



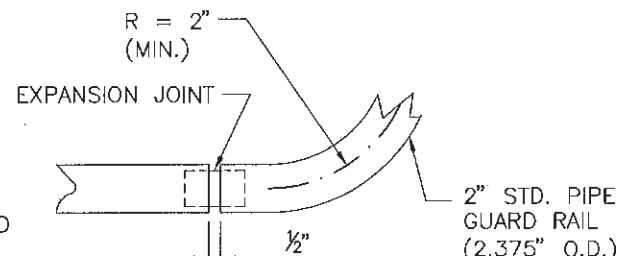
**DETAIL @ RAIL END
TYPE B**



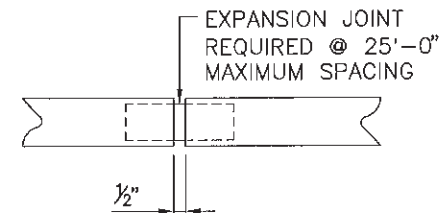
**TYPICAL SECTION
THRU RAIL**



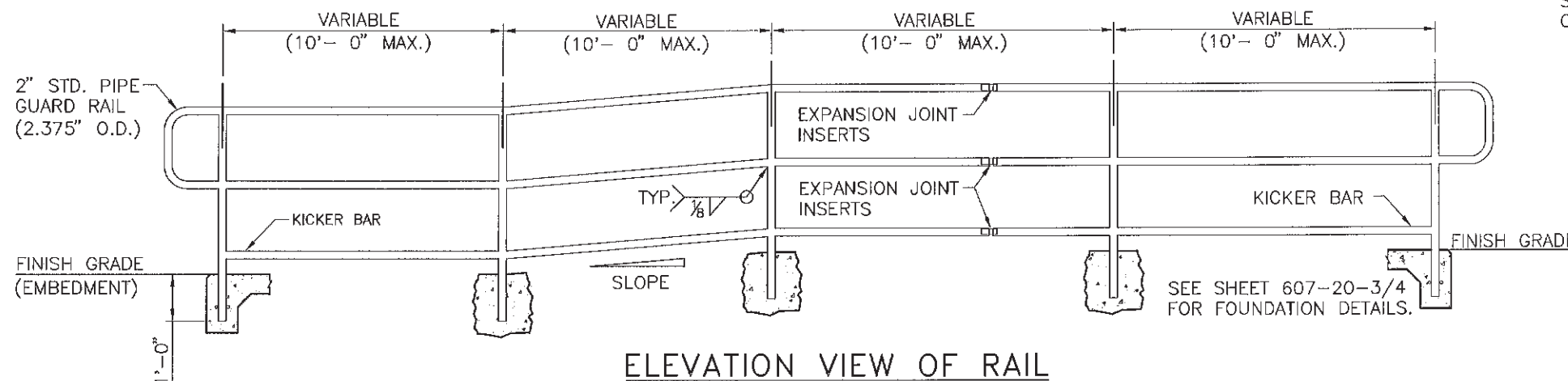
**DETAIL @ EXPANSION
JOINT INSERTS**



**DETAIL OF RAIL TRANSITION
AT HORIZONTAL CURVE
(PLAN VIEW SHOWN)**



DETAIL @ EXP. JOINT

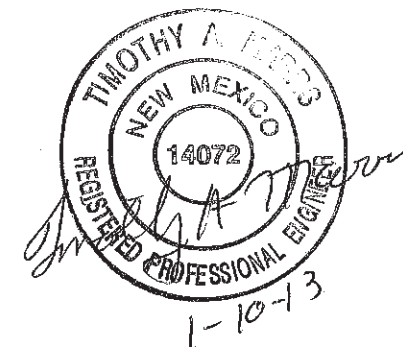


ELEVATION VIEW OF RAIL

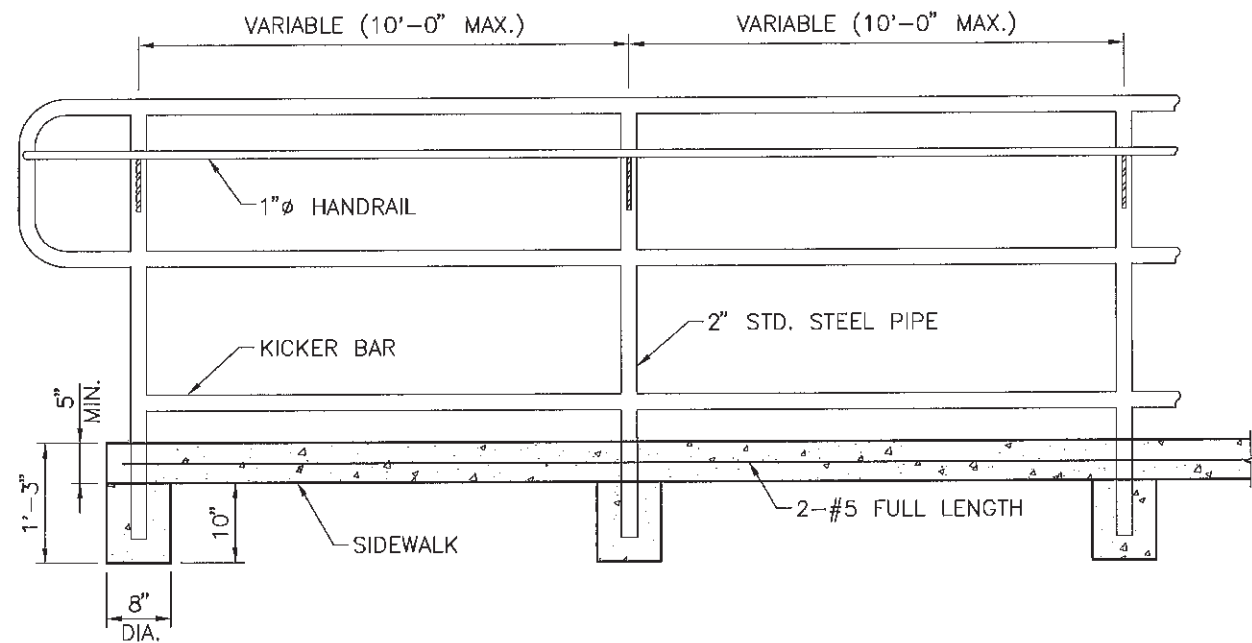
NOTE: POSTS TO REMAIN VERTICAL, RAIL PARALLEL TO GRADE.

GENERAL NOTES:

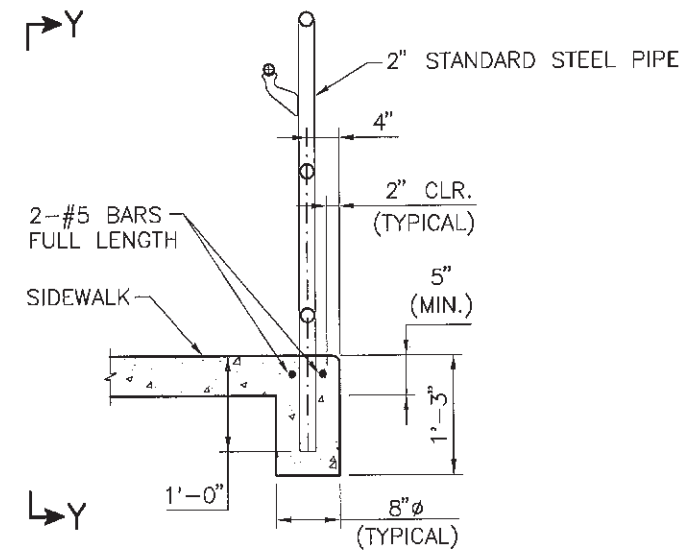
- DESIGN AND CONSTRUCTION SHALL CONFORM TO THE NEW MEXICO STATE DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, CURRENT EDITION AND SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
- STEEL STRUCTURES SHALL CONFORM TO SECTION 541 - STEEL STRUCTURES OF THE STANDARD SPECIFICATIONS AND AASHTO M 270, GRADE 50. TUBING SHALL CONFORM TO ASTM A 500, GRADE B.
- BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF AASHTO M 111 OR AASHTO M 298.
- WELDING SHALL MEET THE REQUIREMENTS OF ANSI/AWS D 1.1 STRUCTURAL WELDING CODE, AND SECTION 541 OF THE STANDARD SPECIFICATIONS. ALL WELDS SHALL BE GROUND SO AS TO REMOVE ALL BURS OR PROTUBERANCES WHICH COULD TRAP FOREIGN MATERIALS OR CAUSE HARM TO A PEDESTRIAN.
- THE FABRICATOR SHALL FURNISH TO THE BRIDGE ENGINEER SIX COPIES OF MILL TEST REPORTS OF ALL STRUCTURAL STEEL ITEMS, WELDING ELECTRODES, AND FLUX COMBINATIONS IN CONFORMANCE WITH SUBSECTION 541.3.2.1 OF THE STANDARD SPECIFICATIONS.
- UNLESS OTHERWISE NOTED IN THE CONTRACT, THE PROJECT MANAGER SHALL SELECT EITHER TYPE A OR TYPE B RAILING ENDS, AND SCHEME 1 OR SCHEME 2 FOR FOUNDATIONS.
- 1" STANDARD PIPE HANDRAIL SHALL BE USED FOR GRADES EQUAL TO OR EXCEEDING 5%. 1" STANDARD PIPE HANDRAIL SHALL BE USED FOR GRADES LESS THAN 5%, WHEN SPECIFIED IN THE PLANS.
- POSTS SHALL BE VERTICAL. ALL RAILS SHALL BE PARALLEL TO EACH OTHER AND TO GRADE.
- BREAKS IN VERTICAL ALIGNMENTS SHALL BE AT THE CENTER LINES OF THE VERTICAL POSTS.
- CONCRETE SHALL BE CLASS "AA" AND SHALL BE INCLUDED IN THE BID PRICE FOR SIDEWALKS. IN INSTANCES WHERE THERE ARE NO SIDEWALKS, THE BID PRICE OF THE PEDESTRIAN RAILING INCLUDES THE COST OF CONCRETE TO SUPPORT THE POSTS.
- RAIL POSTS SHALL BE CAST-IN-PLACE, OR SET IN OVERSIZED HOLES (3" IN DIAMETER) AND GROUTED AS OUTLINED IN SECTION 522 OR 523 OF THE STANDARDS SPECIFICATIONS.
- AFTER GROUTING, THE CONTRACTOR SHALL SEAL THE JOINTS BETWEEN THE RAIL POSTS AND THE CONCRETE WITH SILICONE SEALANT APPROVED BY THE PROJECT MANAGER.
- PAINTING AND/OR POWDER COATING OF STRUCTURAL STEEL SHALL CONFORM TO SECTION 545 OR SPECIAL PROVISIONS OF THE STANDARD SPECIFICATIONS. COLOR SHALL BE VERIFIED BY PROJECT MANAGER. REINFORCING BARS SHALL BE EPOXY COATED CONFORMING TO SECTION 540.



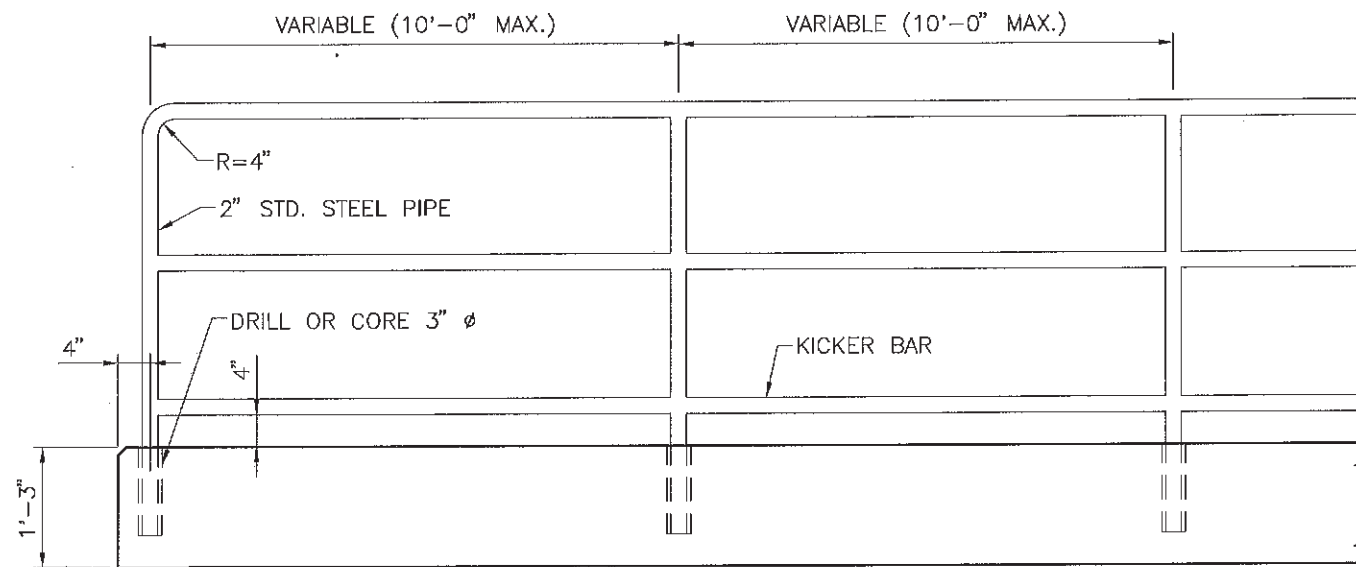
DATE	BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)		
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING		
PEDESTRIAN RAILING WITHOUT HANDRAIL		
DESIGNED BY: TM DRAWN BY: SKL CHECKED BY: YML/TM		
607-20-1/4		
1 of 4		



SCHEME # 2
(VIEW Y-Y)

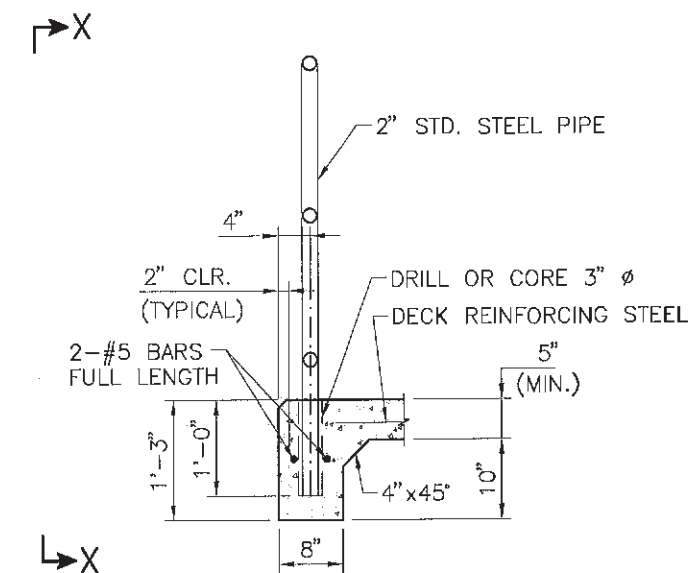


FOUNDATION SCHEME # 2
(FOR EDGE OF SIDEWALK INSTALLATION)

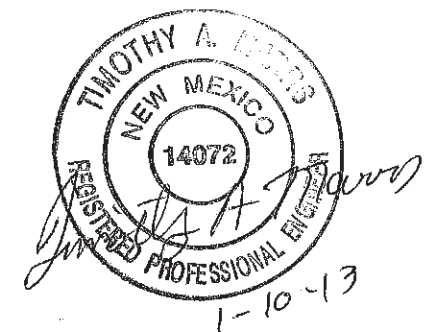


SCHEME # 1
(VIEW X-X)

NOTE: HANDRAILING OMITTED FROM VIEW. REFER TO GENERAL NOTE 7 ON DRAWING 607-20-1/4 WHEN HANDRAILING IS REQUIRED.



FOUNDATION SCHEME # 1
(FOR BRIDGE DECK OR EDGE OF SIDEWALK INSTALLATION)



DATE	BY	DESCRIPTION

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO
DEPARTMENT OF TRANSPORTATION
STANDARD DRAWING

PEDESTRIAN RAILING
FOUNDATION DETAILS

DESIGNED BY: TM DRAWN BY: SKL CHECKED BY: YML/TM
607-20-3/4 3 of 4

GENERAL NOTES:

1. NMDOT IS RECOGNIZED AS A TITLE II PUBLIC ENTITY UNDER THE AMERICANS WITH DISABILITIES ACT (ADA), OF 1990 (PUBLIC LAW 101-336). A TITLE II ENTITY IS DEFINED AS ANY STATE OR LOCAL GOVERNMENT ENTITY AND PROHIBITS DISCRIMINATION ON THE BASIS OF DISABILITY. THE ADA EXTENDS THE PRINCIPLES OF SECTION 504 OF THE REHABILITATION ACT, OF 1973, AS AMENDED, TO PROTECT PERSONS WITH DISABILITIES IN ALL PUBLIC FACILITIES AND PROGRAMS IRRESPECTIVE OF THE FUNDING SOURCE.
2. THESE DRAWINGS PROVIDE GUIDANCE FOR COMPLIANCE WITH THE PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG), JULY 26, 2011, OR LATEST EDITION. THESE GUIDELINES SHALL APPLY TO ALL NEW AND ALTERED PEDESTRIAN ACCESS ROUTES (PAR).
3. REFER TO CONSTRUCTION PLANS FOR THE DETAILED LAYOUTS AND DETAILS.
4. PEDESTRIAN ACCESS ROUTES (PAR) SHALL BE FIRM, STABLE, AND SLIP RESISTANT. PROVIDE SLIP RESISTANT TEXTURE ON SIDEWALKS AND CURB RAMPS BY BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP AND/OR PERPENDICULAR TO PEDESTRIAN TRAVEL. EXTEND TEXTURE THE FULL WIDTH AND LENGTH OF THE CURB RAMP INCLUDING SIDE FLARES. DO NOT SCORE OR MAKE GROOVES IN SLOPED SURFACE. LINES SHOWN ON STANDARD DETAILS ARE FOR ILLUSTRATIONS ONLY.
5. VERTICAL SURFACE DISCONTINUITIES SHALL BE 0.5 INCHES MAXIMUM. VERTICAL DISCONTINUITIES BETWEEN 0.25 INCHES AND 0.5 INCHES SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 50 PERCENT. THE BEVEL SHALL BE APPLIED ACROSS THE ENTIRE VERTICAL SURFACE DISCONTINUITY.
6. HORIZONTAL OPENINGS IN GRATINGS AND JOINTS SHALL NOT PERMIT PASSAGE OF A SPHERE MORE THAN 0.5 INCHES IN DIAMETER. ELONGATED OPENINGS IN GRATES SHALL BE PLACED SO THAT THE LONG DIMENSION IS PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL.
7. PROVIDE EXPANSION JOINT MATERIAL 0.5 INCHES THICK WHERE CURB RAMP ADJOINS ANY RIGID PAVEMENT, SIDEWALK OR STRUCTURE WITH THE TOP OF JOINT FILLER FLUSH WITH ADJACENT CONCRETE SURFACE.
8. SEAL ALL JOINTS WITH AN APPROVED SEALING MATERIAL.
9. INSTALL JOINTS WHERE CURB RAMPS, TURNING SPACES, FLARES, AND SIDEWALKS ABUT. ALL JOINTS AND TRANSITIONS SHALL BE FLUSH.
10. VERTICAL WALLS OR HEADER CURBS ARE PERMITTED WHEN ADJACENT TO NON-WALK AREAS OR ELEVATION DIFFERENCES CANNOT BE ACCOMMODATED BY CURB RAMP FLARES OR GRADING. GRADE NON-WALK AREAS AT 3:1 OR FLATTER.
11. CONSTRUCTION TOP / BOTTOM OF CURB TO BE FLUSH WITH ADJACENT SURFACES (CURB RAMPS, SIDEWALKS, AND FLARES). VERTICAL LIPS NOT PERMITTED AT THE BOTTOM OF CURB RAMP WHERE THE RAMP MEETS STREET LEVEL.

SIDEWALKS

12. SIDEWALK, AND CURB AND GUTTER CONSTRUCTION SHALL BE IN ACCORDANCE WITH SERIAL 609-01-1/1.
13. SIDEWALK CROSS SLOPE IS RECOMMENDED TO BE CONSTRUCTED FOR CROSS SLOPE OF 1.5% TYPICAL, BUT SHALL NOT EXCEED 2.0% CROSS SLOPE ON THE PEDESTRIAN ACCESS ROUTE (PAR).
14. SIDEWALK SHALL HAVE A MINIMUM WIDTH OF 5.0 FT, EXCLUSIVE OF THE WIDTH OF THE CURB RETURN.
EXCEPTION: WHERE SIDEWALK WIDTH NEEDS TO BE REDUCED TO NO LESS 4.0 FT, PASSING SPACES SHALL BE PROVIDED AT INTERVALS OF 200 FT MAXIMUM. PASSING SPACES SHALL BE 5.0 FT MINIMUM BY 5.0 FT MINIMUM.
15. ANY SIGNS POSTS, UTILITY POLES, FIRE HYDRANTS, TRAFFIC SIGNALS, STREET FURNITURE, AND OTHER OBJECTS SHALL NOT REDUCE THE CLEAR WIDTH TO LESS THAN 4.0 FT.
16. THE CLEAR WIDTH OF PEDESTRIAN ACCESS ROUTES (PAR) WITHIN MEDIANS AND PEDESTRIAN REFUGE ISLANDS SHALL BE 5.0 FT MINIMUM.

CURB RAMPS

17. FOR NEW CONSTRUCTION AND ALTERATIONS, CONSTRUCT CURB RAMP AND FLARE SLOPES WITH THE FLATTEST SLOPE FEASIBLE. THE MAXIMUM SLOPE ALLOWABLE IS INDICATED IN NOTE 18 OF THE CURB RAMP STANDARD DETAILS. SLOPES THAT EXCEED THOSE INDICATED IN THE CURB RAMP STANDARD DETAILS, OR CONSTRUCTION PLANS, WILL NOT BE ACCEPTED AND WILL BE REMOVED AND RECONSTRUCTED.
18. RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.3% MAX (RECOMMENDED 7.0%) BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15.0 FT TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAX LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE EXTENDED AS FLAT AS MAXIMUM EXTENT PRACTICABLE.
19. CONSTRUCT THE CLEAR WIDTH OF CURB RAMP RUNS (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITIONS, AND TURNING SPACES AS TYPICAL 5.0 FT X 5.0 FT AND MINIMUM 4.0 FT X 4.0 FT CLEAR SPACE BEYOND THE CURB FACE, WITHIN THE WIDTH OF THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
20. CURB RAMP AND SIDE FLARE LENGTHS ARE VARIABLE AND BASED ON CURB HEIGHT AND THE SIDEWALK SLOPE.
21. THE CHANGE IN GRADE AT THE BOTTOM OF THE CURB RAMP AND ADJOINING ROAD SURFACE SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 13.3%. THE COUNTER SLOPE OF THE GUTTER OR ROAD AT THE FOOT OF A CURB RAMP RUNS, TURNING SPACE OR BLENDED TRANSITION IS NOT TO EXCEED 5.0%.
22. CONSTRUCT CURB RAMPS FLUSH TO ADJACENT ROADWAY. GRADE EDGE OF ROAD ELEVATIONS AT THE FLOW LINE TO ENSURE POSITIVE DRAINAGE AND PREVENT PONDING. FOR LEVEL TURNING SPACES BEHIND CURB, ADJUST SLOPES TO PROVIDE POSITIVE DRAINAGE.
23. GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE CURB RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF CURB RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
24. ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE. THEREFORE, THE LENGTH OF CURB RAMP IS NOT SOLELY DEPENDENT ON THE HEIGHT OF CURB. (FOR EXAMPLE, A 6" CURB DOES NOT NECESSARILY MEAN A RAMP LENGTH OF 6.0 FT FOR AN 8.3% SLOPE).

CROSSWALKS

25. PROVIDE A SEPARATE CURB RAMP FOR EACH MARKED OR UNMARKED CROSSWALK. CURB RAMP LOCATIONS SHALL BE PLACED WITHIN THE WIDTH OF THE MARKED OR UNMARKED CROSSWALK AS SHOWN IN THE CONSTRUCTION PLANS.

DETECTABLE WARNING

26. DETECTABLE WARNING SURFACES (DWS) CONSISTING OF TRUNCATED DOMES SHALL BE UTILIZED WHERE CURB RAMPS, BLENDED TRANSITIONS, OR TURNING SPACE PROVIDE A FLUSH PEDESTRIAN CONNECTION TO THE STREET OR WHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CROSSES A STREET, ALLEY, TRAFFIC ISLAND, MEDIAN, OR RAILROAD. DETECTABLE WARNING SURFACES (DWS) WILL NOT BE INSTALLED AT RESIDENTIAL DRIVEWAYS. DETECTABLE WARNING SURFACE MUST BE PROVIDED AT THE JUNCTION BETWEEN THE PAR AND COMMERCIAL DRIVEWAYS THAT ARE STOP OR YIELD CONTROLLED OR ARE CONTROLLED BY A SIGNAL.
27. DETAILS OF DETECTABLE WARNING SURFACE ARE SHOWN IN CONTRACT PLANS AND SHEET 608-001-8/12 OF THE STANDARD DRAWINGS.

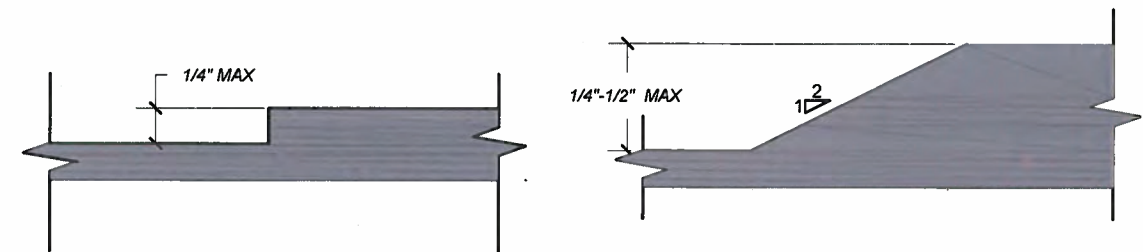
ACCESSIBLE PEDESTRIAN SIGNALS (APS) AND PEDESTRIAN PUSHBUTTONS

28. FOR ALTERATION PROJECTS, PROVIDE ACCESS TO EXISTING PEDESTRIAN PUSHBUTTONS TO THE MAXIMUM EXTENT PRACTICABLE. INSTALL PEDESTRIAN STUB POLES, WHERE APPLICABLE, SO AS NOT TO CREATE PEDESTRIAN OBSTRUCTIONS. REFER TO THE MUTCD FOR FURTHER GUIDANCE.
29. PEDESTRIAN SIGNAL PUSH BUTTONS SHALL COMPLY WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND LOCATED WITHIN A HORIZONTAL REACH OF 0" TO 10" AND SHALL BE WITHIN 36" TO 46" ABOVE THE SIDEWALK SURFACE.
30. PEDESTRIAN SIGNAL SHALL HAVE 4FTx4FT MIN TURNING SPACE TO PROVIDE ACCESS TO PUSH BUTTONS.

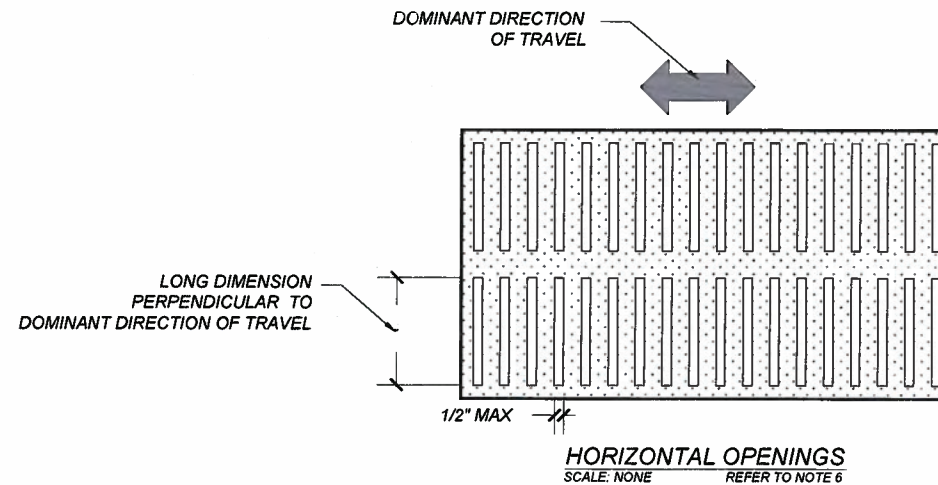
ALTERATIONS TO EXISTING FACILITIES - GENERAL NOTES:

ADDITIONS OR ALTERATIONS TO ANY FACILITY SHALL CONFORM TO THE REQUIREMENTS OF THE NEW CONSTRUCTION STANDARDS WITHIN THE NMDOT PEDESTRIAN ACCESS STANDARDS AND PROWAG 2011 OR LATEST EDITION. ANY DESIGN / CONSTRUCTION DEVIATION THAT IS DEEMED AN VARIANCE OR TECHNICALLY INFEASIBLE BY THE DEFINITION BELOW SHALL REQUIRE SUBMITTAL AND APPROVAL OF ADA DESIGN VARIANCE PROCEDURES.

31. **EXCEPTION:** IN ALTERATION WORK, IF COMPLIANCE IS TECHNICALLY INFEASIBLE, THE ALTERATION SHALL PROVIDE ACCESSIBILITY TO THE MAXIMUM EXTENT PRACTICABLE. ANY ELEMENTS OR FEATURES OF THE BUILDING OR FACILITY THAT IS BEING ALTERED AND CAN BE MADE ACCESSIBLE SHALL BE MADE ACCESSIBLE WITHIN THE SCOPE OF THE ALTERATION.
32. **TECHNICAL INFEASIBILITY:** MEANS, WITH RESPECT TO AN ALTERATION OF A BUILDING OR A FACILITY, THAT IT HAS LITTLE LIKELIHOOD OF BEING ACCOMPLISHED BECAUSE EXISTING STRUCTURAL CONDITIONS WOULD REQUIRE REMOVING OR ALTERING A LOAD-BEARING MEMBER WHICH IS AN ESSENTIAL PART OF THE STRUCTURAL FRAME; OR BECAUSE OTHER EXISTING PHYSICAL OR SITE CONSTRAINTS PROHIBIT.
33. IN ALTERATIONS WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT COMPLIANCE TO PROVIDE A CURB RAMP FOR EACH PEDESTRIAN CROSSING A SINGLE DIAGONAL CURB RAMP SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS.



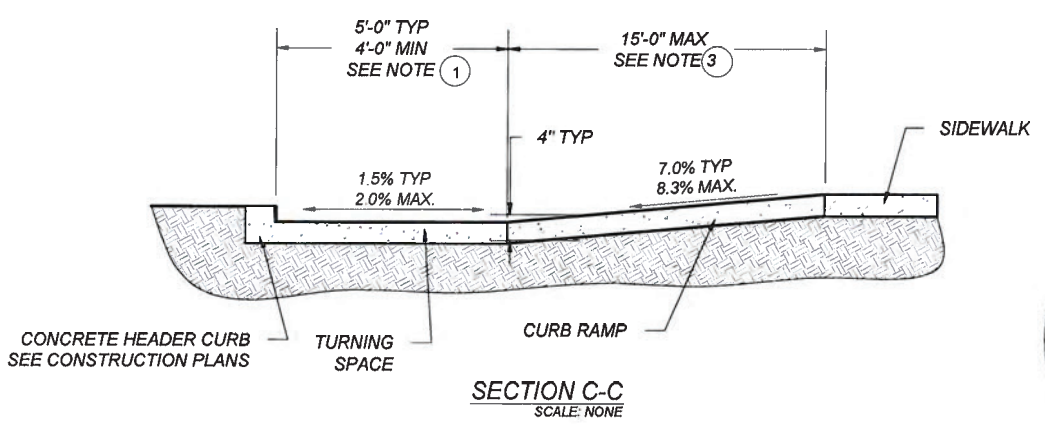
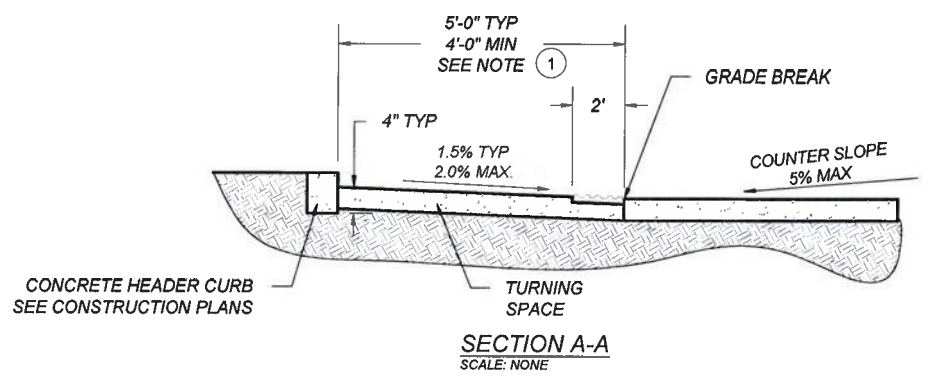
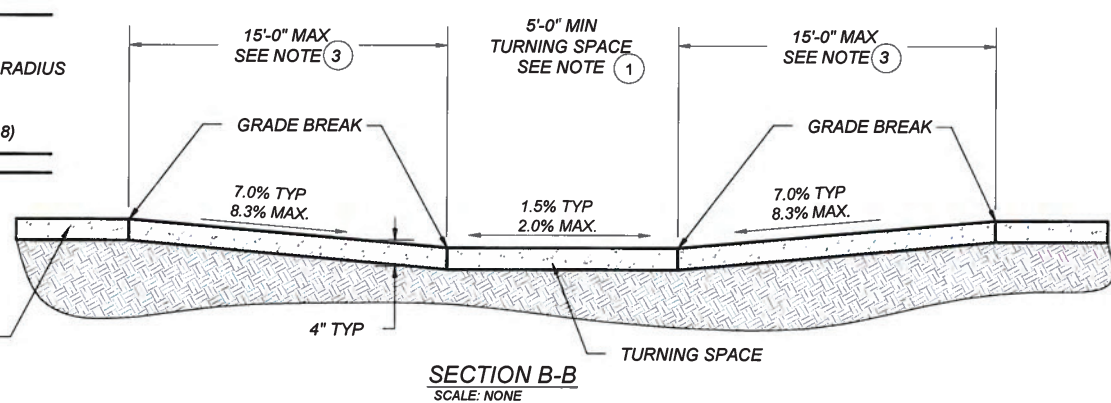
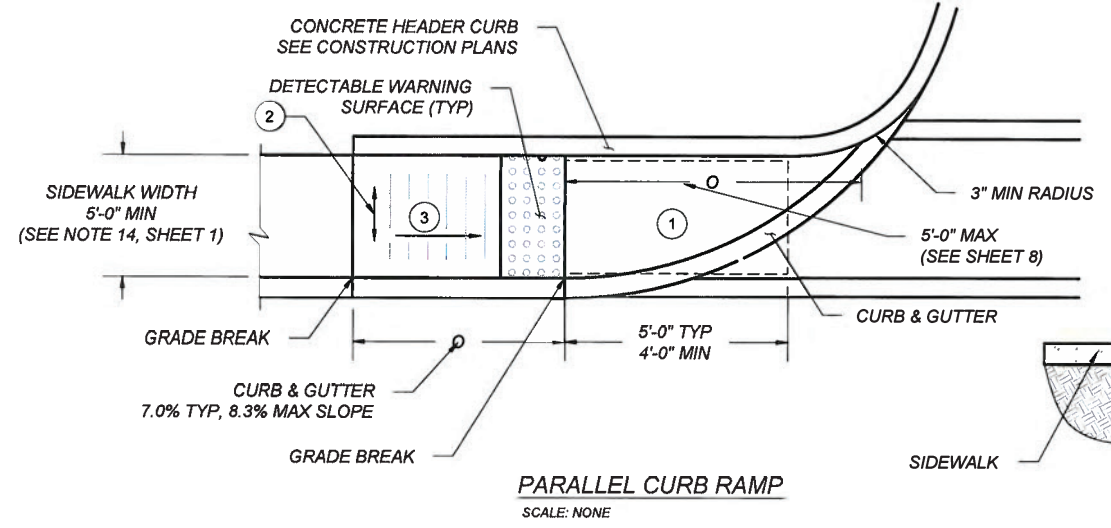
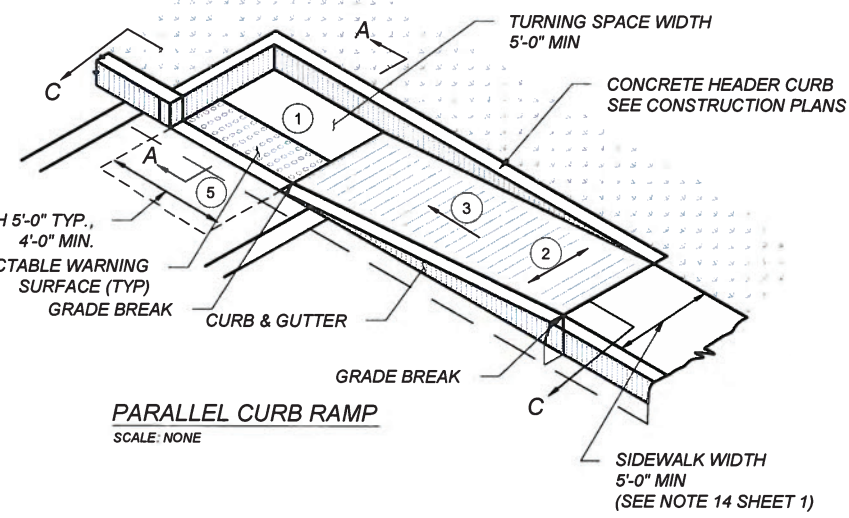
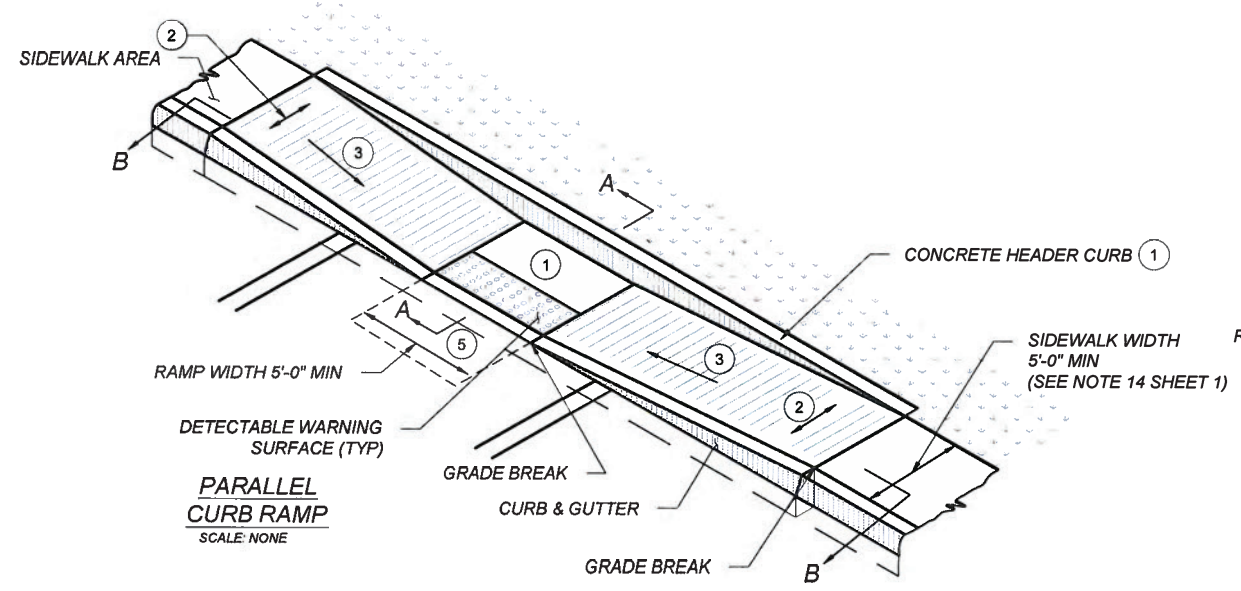
VERTICAL SURFACE DISCONTINUITIES
SCALE: NONE REFER TO NOTE 5



HORIZONTAL OPENINGS
SCALE: NONE REFER TO NOTE 6



NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
PEDESTRIAN ACCESS ROUTE GENERAL NOTES			
APPROVED	<i>Michael J. Smelker</i>		DATE
	DESIGN ENGINEER		1-13-15
608-001-1		608-1 of 12	

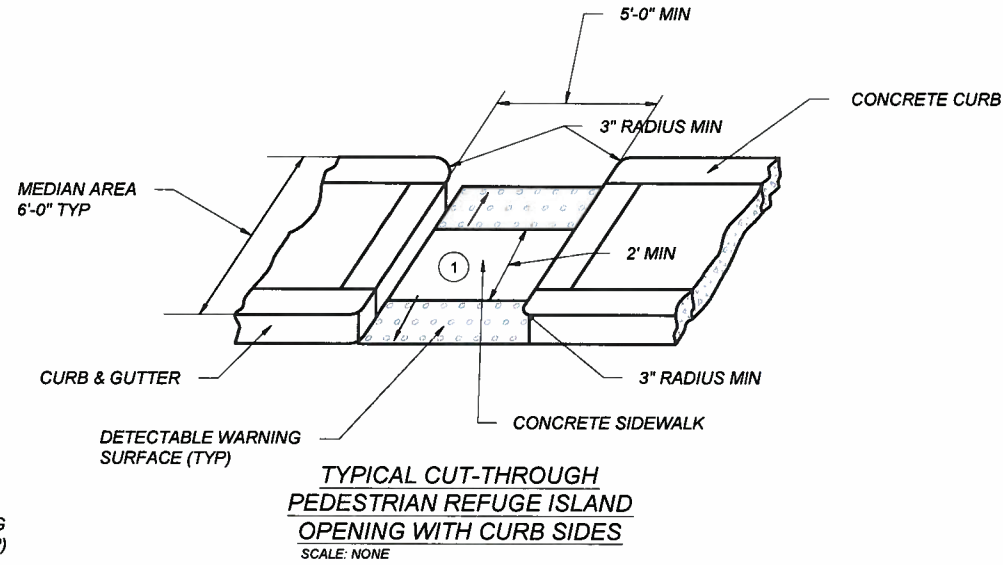
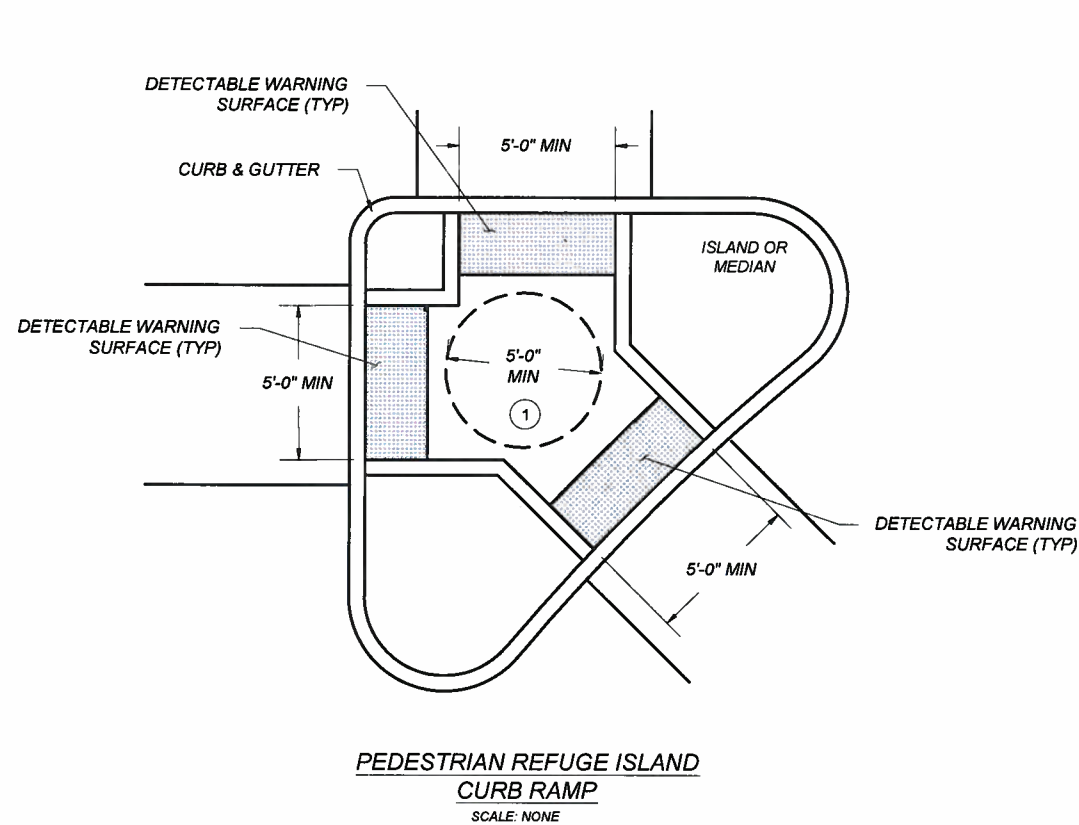


KEYED NOTES

- 1 TURNING SPACE SHALL HAVE MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.0% (RECOMMEND 1.5%). TURNING SPACE SHALL BE 4.0 FT BY 4.0 FT MIN (RECOMMEND 5.0 FT BY 5.0 FT) AT THE TOP OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE 4.0 FT MIN BY 5.0 FT MIN. THE 5.0 FT SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
 - 2 GROSS SLOPE SHALL BE 2.0% MAX (RECOMMEND 1.5%). EXCEPTION: THE CROSS SLOPE OF CURB RAMPS AT PEDESTRIAN STREET CROSSING WITHOUT YIELD OR STOP CONTROL, TRAFFIC SIGNALS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK PEDESTRIAN STREET CROSSING, THE CROSS SLOPE IS PERMITTED TO MATCH STREET OR HIGHWAY GRADE.
 - 3 RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.3% MAX (RECOMMENDED 7.0%) BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15.0 FT TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAX LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE EXTENDED AS FLAT AS MAXIMUM EXTENT PRACTICABLE.
 - 4 GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMPS RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACE. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
 - 5 COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF A CURB RAMP, RUN OR TURNING SPACE SHALL BE 5% MAX.
 - 6 FLARED SIDES ARE TO HAVE A SLOPE OF 10% MAX (RECOMMEND 9%), MEASURED PARALLEL TO THE BACK OF THE CURB, UNLESS THE FLARED SIDES ARE PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, CHAINS, FENCING, OR RAILINGS.
- NOTES:**
- A DO NOT SCORE OR MAKE GROOVES IN SLOPED SURFACE. LINES SHOWN ON STANDARD DETAILS ARE FOR ILLUSTRATION ONLY.
 - B DETAILS OF THE DETECTABLE WARNING SURFACE ARE SHOWN IN THE CONSTRUCTION PLANS AND SHEET 608-001-8/12 OF THE STANDARD DRAWINGS.
 - C IN ALTERATIONS WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT COMPLIANCE TO PROVIDE A CURB RAMP FOR EACH PEDESTRIAN CROSSING A SINGLE DIAGONAL CURB RAMP SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS.
 - D CONCRETE HEADER CURBS CONSTRUCTED AS PART OF THE CURB RAMP WILL BE CONSIDERED INCIDENTAL TO ITEM NUMBER 608004 AND NO SEPARATE PAYMENT WILL BE MADE.



NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
PARALLEL CURB RAMPS			
APPROVED			1-13-15 DATE
			DESIGN ENGINEER
608-001-3			608- 3 of 12



KEYED NOTES

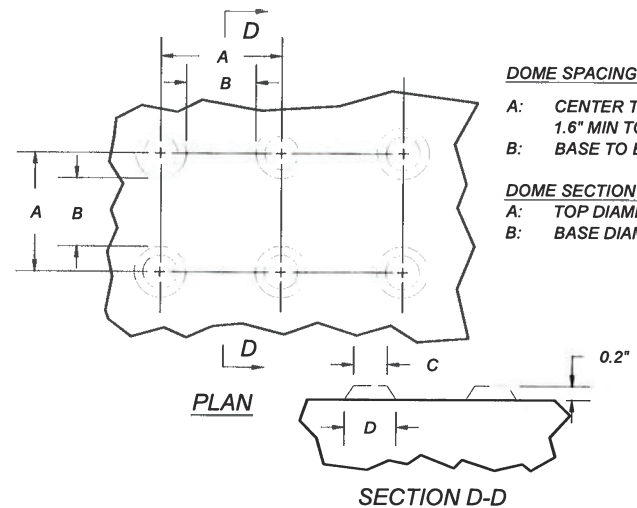
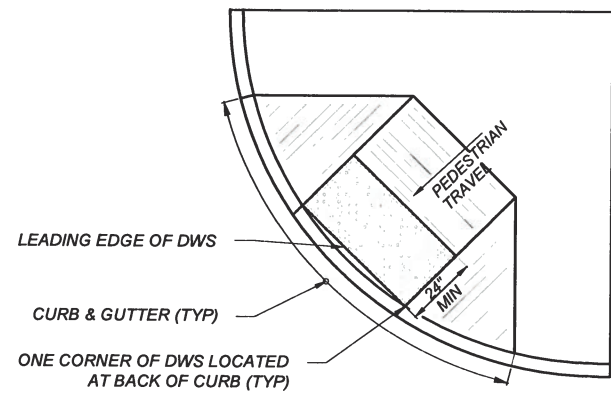
- 1 TURNING SPACE SHALL HAVE MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.0% (RECOMMEND 1.5%). TURNING SPACE SHALL BE 4.0 FT BY 4.0 FT MIN (RECOMMEND 5.0 FT BY 5.0 FT) AT THE TOP OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE 4.0 FT MIN BY 5.0 FT MIN. THE 5.0 FT SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- 2 CROSS SLOPE SHALL BE 2.0% MAX (RECOMMENDED 1.5%). EXCEPTION: THE CROSS SLOPE OF CURB RAMPS AT PEDESTRIAN STREET CROSSING WITHOUT YIELD OR STOP CONTROL, TRAFFIC SIGNALS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK PEDESTRIAN STREET CROSSING, THE CROSS SLOPE IS PERMITTED TO MATCH STREET OR HIGHWAY GRADE.
- 3 RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.3% MAX (RECOMMENDED 7.0%) BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15.0 FT TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAX LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE EXTENDED AS FLAT AS MAXIMUM EXTENT PRACTICABLE.
- 4 GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMPS RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACE. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
- 5 COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF A CURB RAMP, RUN OR TURNING SPACE SHALL BE 5% MAX.
- 6 FLARED SIDES ARE TO HAVE A SLOPE OF 10% MAX (RECOMMEND 9%), MEASURED PARALLEL TO THE BACK OF THE CURB, UNLESS THE FLARED SIDES ARE PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, CHAINS, FENCING, OR RAILINGS.

NOTES:

- A DO NOT SCORE OR MAKE GROOVES IN SLOPED SURFACE. LINES SHOWN ON STANDARD DETAILS ARE FOR ILLUSTRATION ONLY.
- B DETAILS OF THE DETECTABLE WARNING SURFACE ARE SHOWN IN THE CONSTRUCTION PLANS AND SHEET 608-001-8/12 OF THE STANDARD DRAWINGS.
- C IN ALTERATIONS WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT COMPLIANCE TO PROVIDE A CURB RAMP FOR EACH PEDESTRIAN CROSSING A SINGLE DIAGONAL CURB RAMP SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS.
- D CONCRETE HEADER CURBS CONSTRUCTED AS PART OF THE CURB RAMP WILL BE CONSIDERED INCIDENTAL TO ITEM NUMBER 608004 AND NO SEPARATE PAYMENT WILL BE MADE.

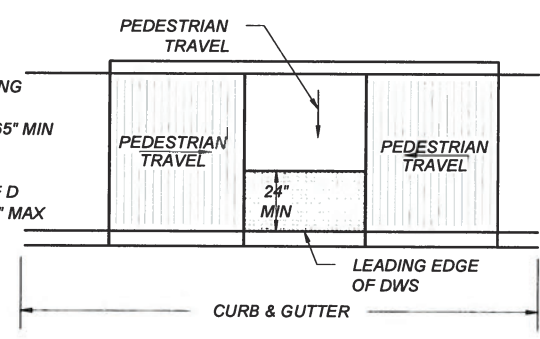


NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
PEDESTRIAN REFUGE ISLAND			
APPROVED			1-13-15
	DESIGN ENGINEER		DATE
608-001-6			608- 6 of 12



DOME SPACING
 A: CENTER TO CENTER SPACING
 1.6" MIN TO 2.4" MAX
 B: BASE TO BASE SPACING 0.65" MIN

DOME SECTION
 A: TOP DIAMETER 50%-65% OF D
 B: BASE DIAMETER 0.9" TO 1.4" MAX

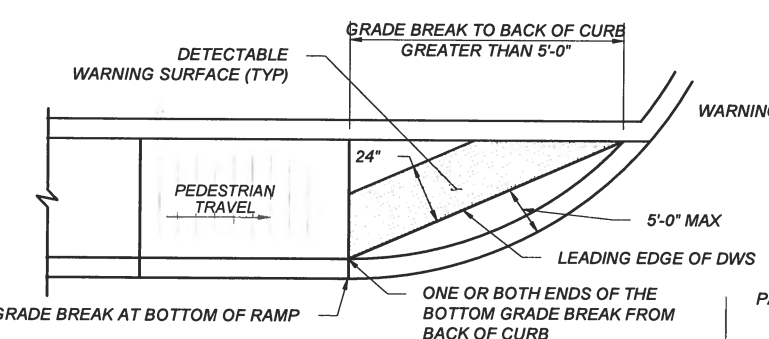
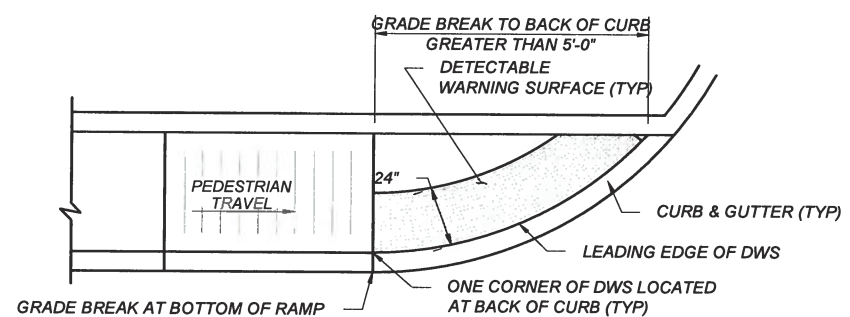
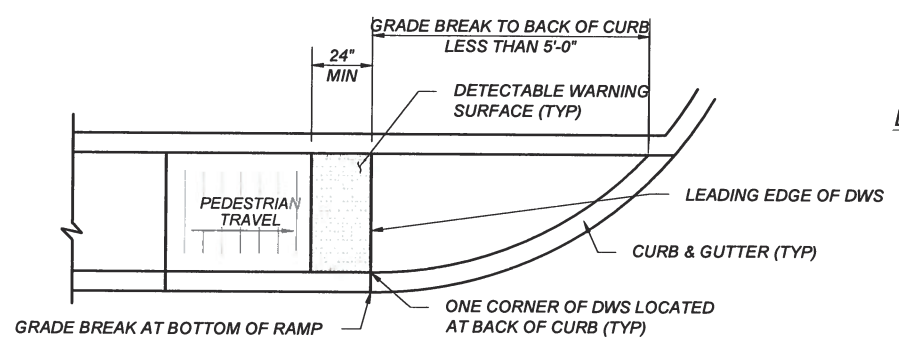


DETECTABLE WARNING SURFACE
 SCALE: NONE

DETECTABLE WARNING SURFACE (DWS):
 A STANDARDIZED TRUNCATED DOME GRID SURFACE BUILT IN OR APPLIED TO THE PEDESTRIAN ACCESS ROUTE TO WARN VISUALLY IMPAIRED PEOPLE OF HAZARDS. THE SURFACE IS PLACED WHERE DETECTABLE WARNING SURFACE (DWS): A STANDARDIZED TRUNCATED DOME GRID SURFACE BUILT IN OR APPLIED TO THE PEDESTRIAN ACCESS ROUTE TO WARN VISUALLY IMPAIRED PEOPLE OF HAZARDS. THE SURFACE IS PLACED WHERE PEDESTRIANS WILL ENCOUNTER THE PRESENCE OF HAZARDS IN THE LINE OF TRAVEL, SUCH AS THE EDGE OF ROADWAY AND AT-GRADE RAIL CROSSINGS, INDICATING THEY SHOULD STOP AND DETERMINE THE NATURE OF THE HAZARD BEFORE PROCEEDING.

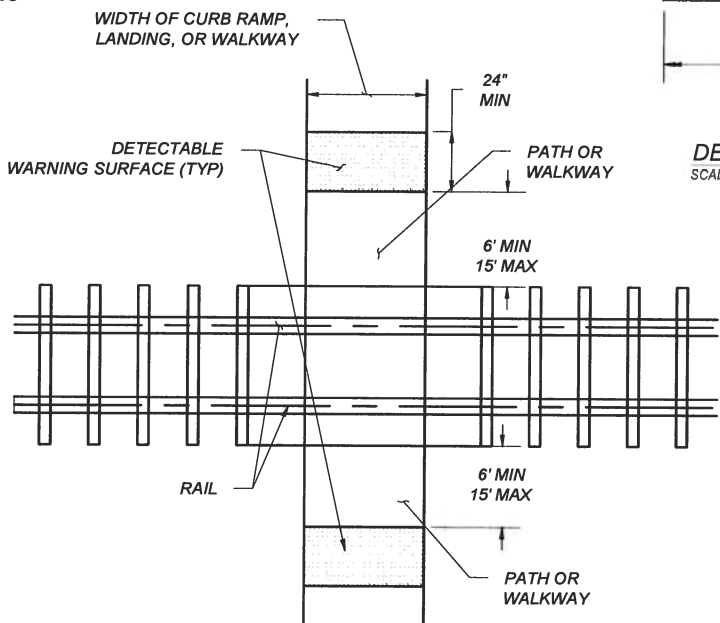
- LOCATION:**
1. THE DETECTABLE WARNING SURFACE (DWS) SHALL BE 2.0 FT MINIMUM WIDTH AND EXTENDED THE FULL WIDTH OF THE CURB RAMP RUN, TURNING SPACE, BLENDED TRANSITION, AN EXCLUDING ANY THE FLARED SIDES
 2. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BACK OF THE CURB.
 3. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PARALLEL TO THE DIRECTION OF TRAVEL.
 4. IF CURB AND GUTTER ARE NOT PRESENT, SUCH AS A SHARED-USE PATH CONNECTION, THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE PAVEMENT EDGE.
 5. PEDESTRIAN REFUGE ISLANDS SHALL HAVE DETECTABLE WARNINGS. DETECTABLE WARNINGS AT CUT THROUGH ISLANDS SHALL BE SEPARATED BY A 24 INCH MINIMUM LENGTH OF THE WALKWAY WITHOUT MARKINGS.
- EXCEPTION:** DETECTABLE WARNINGS SHALL NOT BE REQUIRED ON CUT THROUGH ISLANDS WHERE THE CROSSING IS LESS THAN 6 FT IN THE DIRECTION OF PEDESTRIAN TRAVEL.

- NOTES:**
1. DETAILS SPECIFIED ON THIS PLAN APPLY TO ALL CONSTRUCTION OR RECONSTRUCTION OF STREETS, CURBS, OR SIDEWALKS BY ALL PUBLIC AGENCIES AND BY ALL PRIVATE ORGANIZATIONS CONSTRUCTING FACILITIES FOR PUBLIC USE.
 2. DETECTABLE WARNING SURFACE SHALL CONTRAST VISUALLY WITH ADJACENT GUTTER, WALKWAY SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT FOR THE FULL WIDTH OF RAMP.
 3. ALL PRODUCTS USED FOR DETECTABLE WARNING SURFACES SHALL BE ON THE DEPARTMENT'S APPROVED PRODUCT LIST.

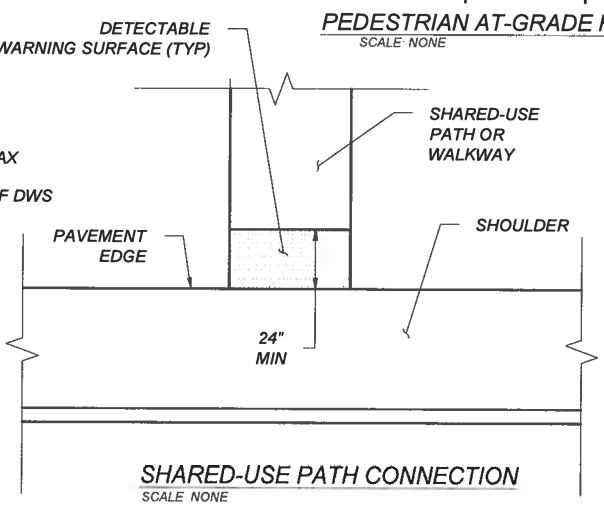


DETECTABLE WARNING SURFACE (DWS) ON CURVED SURFACES
 SCALE: NONE

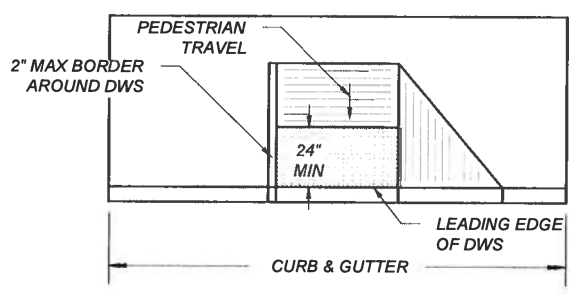
DETECTABLE WARNING SURFACE (DWS) TRUNCATED DOME DETAILS
 SCALE: NONE



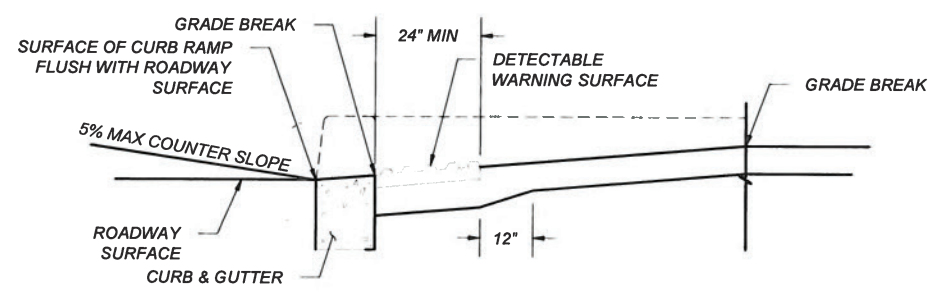
PEDESTRIAN AT-GRADE RAIL CROSSINGS
 SCALE: NONE



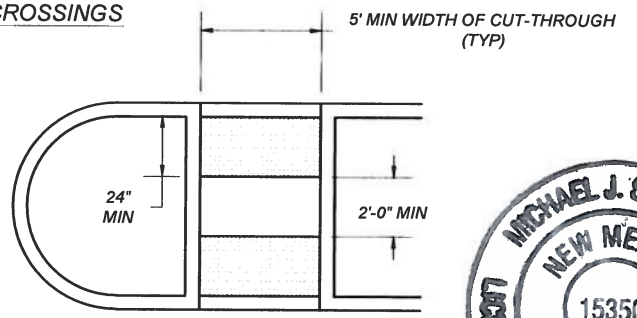
SHARED-USE PATH CONNECTION
 SCALE: NONE



DETECTABLE WARNING SURFACE
 SCALE: NONE



DETECTABLE WARNING SURFACE
 SCALE: NONE



MEDIAN CUT-THROUGH
 SCALE: NONE
 EXCEPTION: IF THE LENGTH BETWEEN TWO DWS SURFACE IS LESS THAN 2' THEN DETECTABLE WARNING SURFACE WILL NOT BE INSTALLED



NO.	DATE	REV. BY	DESCRIPTION
REVISIONS (OR CHANGE NOTICES)			
NEW MEXICO DEPARTMENT OF TRANSPORTATION STANDARD DRAWING			
DETECTABLE WARNING SURFACE			
APPROVED	[Signature]		1-13-15 DATE
			DESIGN ENGINEER
608-001-8			608-8 of 12











PR-2023-008423_SI-2023-00606_Site_Plan_Approved_5-9-23_Sheet_1


Final Audit Report


2023-08-04


Created:	2023-07-24
By:	Jay Rodenbeck (jrodenbeck@cabq.gov)
Status:	Signed
Transaction ID:	CBJCHBCAABAACVUWHw5-quL-GqWHNBKUsj5UF9vtN9z8


"PR-2023-008423_SI-2023-00606_Site_Plan_Approved_5-9-23_Sheet_1" History


-  Document created by Jay Rodenbeck (jrodenbeck@cabq.gov)
2023-07-24 - 11:55:03 PM GMT - IP address: 198.206.237.4
-  Document emailed to Jolene Wolfley (jwolfley@cabq.gov) for signature
2023-07-24 - 11:59:07 PM GMT
-  Document emailed to Ernest Armijo (earmijo@cabq.gov) for signature
2023-07-24 - 11:59:07 PM GMT
-  Document emailed to Tiequan Chen (tchen@cabq.gov) for signature
2023-07-24 - 11:59:08 PM GMT
-  Document emailed to David G. Gutierrez (dggutierrez@abcwua.org) for signature
2023-07-24 - 11:59:08 PM GMT
-  Document emailed to Jeff Palmer (jppalmer@cabq.gov) for signature
2023-07-24 - 11:59:08 PM GMT
-  Document emailed to Whitney Phelan (wphelan@cabq.gov) for signature
2023-07-24 - 11:59:08 PM GMT
-  Email viewed by Ernest Armijo (earmijo@cabq.gov)
2023-07-25 - 1:30:09 PM GMT - IP address: 143.120.132.81
-  Document e-signed by Ernest Armijo (earmijo@cabq.gov)
Signature Date: 2023-07-25 - 1:30:25 PM GMT - Time Source: server- IP address: 143.120.132.81
-  Email viewed by Jeff Palmer (jppalmer@cabq.gov)
2023-07-25 - 1:55:36 PM GMT - IP address: 198.206.237.4


 Document e-signed by Jeff Palmer (jppalmer@cabq.gov)
Signature Date: 2023-07-25 - 1:55:56 PM GMT - Time Source: server- IP address: 198.206.237.4


 Email viewed by Tiequan Chen (tchen@cabq.gov)
2023-07-25 - 2:37:51 PM GMT- IP address: 97.123.93.197

 Document e-signed by Tiequan Chen (tchen@cabq.gov)
Signature Date: 2023-07-25 - 2:38:29 PM GMT - Time Source: server- IP address: 97.123.93.197


 Email viewed by Whitney Phelan (wphelan@cabq.gov)
2023-07-26 - 0:17:28 AM GMT- IP address: 172.59.1.62

 Email viewed by Whitney Phelan (wphelan@cabq.gov)
2023-07-27 - 9:28:08 PM GMT- IP address: 104.47.65.254


 Document e-signed by Whitney Phelan (wphelan@cabq.gov)
Signature Date: 2023-07-27 - 9:28:31 PM GMT - Time Source: server- IP address: 198.206.237.4


 Email viewed by Jolene Wolfley (jwolfley@cabq.gov)
2023-07-31 - 3:47:56 PM GMT- IP address: 143.120.133.169


 Email viewed by David G. Gutierrez (dggutierrez@abcwua.org)
2023-08-02 - 2:23:50 PM GMT- IP address: 142.202.67.2


 New document URL requested by David G. Gutierrez (dggutierrez@abcwua.org)
2023-08-02 - 2:46:29 PM GMT- IP address: 142.202.67.2

 Document e-signed by David G. Gutierrez (dggutierrez@abcwua.org)
Signature Date: 2023-08-02 - 2:47:26 PM GMT - Time Source: server- IP address: 142.202.67.2

 Email viewed by Jolene Wolfley (jwolfley@cabq.gov)
2023-08-04 - 8:27:09 PM GMT- IP address: 143.120.133.169

 New document URL requested by Jolene Wolfley (jwolfley@cabq.gov)
2023-08-04 - 8:27:14 PM GMT- IP address: 143.120.133.169

 Document e-signed by Jolene Wolfley (jwolfley@cabq.gov)
Signature Date: 2023-08-04 - 8:27:46 PM GMT - Time Source: server- IP address: 143.120.133.169

 Agreement completed.
2023-08-04 - 8:27:46 PM GMT