CITY OF ALBUQUERQUE



March 9, 2015

Re:

Scott Steffen, P.E. Bohannan Huston Inc. 7500 Jefferson St NE, Ctyd 1 Albuquerque, NM 87109

Valle Vista at the Trails Unit 2 Lots 1-5 and 10-19 and Unit 3 all Lots Pad

Certification.

Engineer's Stamp Date 6-23-14 (C09D009)

Certification dated 2-28-15, Supplemental Information 3-9-15

Dear Mr. Steffen,

Based upon the information provided in your submittal received 3-2-15 and supplemental information received 3-9-15, the above referenced plan and information is accepted for Building Permit approval on above mentioned lots.

If you have any questions, you can contact me at 924-3986.

PO Box 1293

Sincerely,

Albuquerque

Curtis Cherne, P.E.

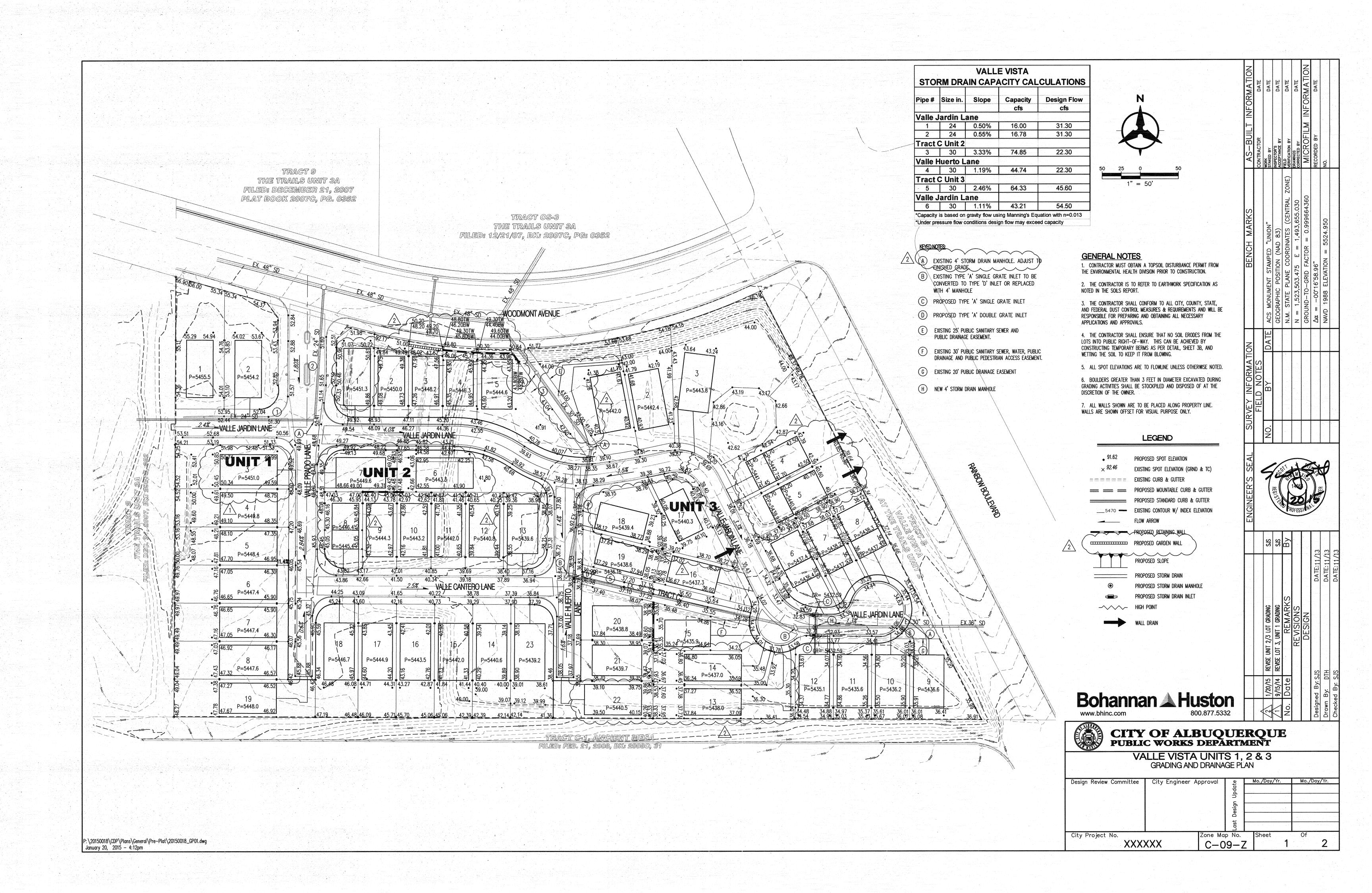
Principal Engineer, Hydrology

Planning Dept.

New Mexico 87103

www.cabq.gov

C: e-mail





Courtyard I 7500 Jefferson St. NE Albuquerque, NM 87109-4335

www.bhinc.com

voice: 505.823.1000 facsimile: 505.798.7988 toll free: 800.877.5332

February 28, 2015

Mr. Curtis Cherne Hydrology Section City of Albuquerque 600 2nd Street NW Albuquerque, NM 87102

Re: Grading and Drainage Certification, Valle Vista Units 2 and 3

DRB Case No. 1004606, (C09/D009)

Dear Curtis:

We are submitting grading and drainage certifications for Valle Vista Units 2 and 3. The certifications include a partial certification of Valle Vista Unit 2 (Lots 1-5 and 10-19) and a final certification for Valle Vista Unit 3. Enclosed for your review is the approved grading and drainage plan dated 6/23/14, with as-built elevations for the above lots in Unit 2 and all lots in Unit 3. Several minor field modifications were made during construction based on field conditions and homebuilder requirements. These are shown on the as-built grading plan and described below.

Unit 2

- Lot 1 Pad depth increased from 65' to 75'.
- Lot 3 Lower rear yard retaining wall is no longer required as the existing wall along the rear property line is able to accept a greater split than originally thought. The retaining wall is replaced by a garden wall to be built by the homebuilder.
- Lot 4 Pad was built as a split pad to reduce the height of the lower rear yard retaining wall and reduce the driveway slope.
- Lot 13 Pad was reduced from 70' to 65' to remove the pad from the 10' Public Utility Easement.
- Lot 18 Pad was built as a split pad to reduce the driveway slope as the driveway needs to be on the low side of the lot. The upper pad matches the original pad elevation.

Unit 3

- Lot 4 Pad elevation decreased by 1.5' as it was determined that the depth to basalt rock was deeper than originally thought. This lower elevation also reduces the driveway slope.
- Lot 5 Pad elevation decreased by 1.0' as it was determined that the depth to basalt rock was deeper than originally thought. This lower elevation also reduces the driveway slope.
- The lowering of the Lot 4 and 5 pad elevations reduced the Lot 4/5 side yard split to less than 2.0', therefore eliminating the side yard retaining wall.
 - **Engineering A**
 - Spatial Data
 - Advanced Technologies A

Curtis Cherne, P.E. Planning Department February 28, 2015 Page 2

- The lowering of the Lot 5 pad elevation reduced the Lot 5 side yard and Lots 6-8 rear yard splits to less than 2.0', therefore eliminating the side/rear yard retaining wall.
- Lot 17 Reposition the pad to increase the pad depth from 57' to 60' to meet the homebuilder minimum pad depth requirement. The entire back yard now drains to the low point wall drain as shown on the original grading plan. This allowed the pad elevation to be lowered by 1.4', reducing the side yard split next to Valle Jardin Lane and the Lot 16 side yard and Lot 17 rear yard split to less than 2.0', eliminating the retaining walls in these locations.
- Lot 18 Pad depth increased from 57' to 65'. Reconfiguration of the Lot 17/18 common lot line, such that Lots 16-19 now share a common rear corner, eliminated the need for the Lot 19 side yard retaining wall.
- Lot 22 Pad depth increased from 65' to 75'.

After reviewing these as-built elevations and visiting the site on 02/25/15, it is my belief that the above lots have been graded in substantial compliance with the approved grading and drainage plan and that street and drainage improvements allow runoff to be managed in the interim condition.

Your review and approval is requested for Building Permit Approval for Lots 1-5 and 10-19, Valle Vista Unit 2 and for SIA/Financial Guarantee Release for Valle Vista Unit 3. I appreciate your time and consideration. If you have questions or require additional information, please contact me at 823-1000.

Sincerely,

Scott J. Steffen, P.E.

Vice President

Community Development & Planning

Enclosure

cc: Rick Beltramo, Wexford Construction



City of Albuquerque

Planning Department

Development & Building Services Division DRAINAGE AND TRANSPORTATION INFORMATION SHEET

(REV 02/2013)

Project Title: DMP FOR VALLE VISTA AT THE TRAILS	Building Permit #:	City Drainage #: C09/D009	
DRB#: 1004606 EPC	·#:	Work Order#:	
Legal Description: VALLE VISTA UNITS 2 AND 3			
City Address:			
Engineering Firm: BOHANNAN HUSTON INC		Contact: SCOTT STEFFEN	
Address: 7500 JEFFERSON ST NE COURTYARD 1 ALBU	QUERQUE NM 87109		
Phone#: 823-1000 Fax#	‡ :	E-mail: SSTEFFENN@BHINC.COM	
Owner: WOODMONT PASEO, LLC		Contact: RICK BELTRAMO	
Address: 6330 RIVERSIDE PLAZA LANE #160 ALBUQUE	RQUE NM 87120		
Phone#: 505-761-9911 Fax#	‡ :	E-mail:	
Architect:		Contact:	
Address:			
		E-mail:	
Surveyor:		Contact:	
Address:			
Phone#: Fax#	# :	E-mail:	
Contractor:		Contact:	
Address:			
Phone#: Fax#	# :	E-mail:	
TYPE OF SUBMITTAL:	CHECK TYPE OF APPROV	AL/ACCEPTANCE SOUGHT:	
DRAINAGE REPORT		X SIA/FINANCIAL GUARANTEE RELEASE	
DRAINAGE PLAN 1st SUBMITTAL			
DRAINAGE PLAN RESUBMITTAL	S. DEV. PLAN FOR SUB'D APPROVAL		
CONCEPTUAL G & D PLAN	S. DEV. FOR BLDG. PERMIT APPROVAL		
GRADING PLAN	SECTOR PLAN APPROVAL		
	ON & SEDIMENT CONTROL PLAN (ESC) FINAL PLAT APPROVAL		
× ENGINEER'S CERT (HYDROLOGY)			
CLOMR/LOMR		CERTIFICATE OF OCCUPANCY (TCL TEMP)	
TRAFFIC CIRCULATION LAYOUT (TCL)		FOUNDATION PERMIT APPROVAL	
ENGINEER'S CERT (TCL)		X BUILDING PERMIT APPROVAL	
ENGINEER'S CERT (DRB SITE PLAN)	GRADING PERMIT APPRO		
ENGINEER'S CERT (ESC)	PAVING PERMIT APPROV	-	
SO-19		WORK ORDER APPROVAL ESC CERT. ACCEPTANCE	
OTHER (SPECIFY)	GRADING CERTIFICATION		
, ,		D	
WAS A PRE-DESIGN CONFERENCE ATTENDED:			
DATE SUBMITTED: 3/2/15	By: Scott Steffen		

Requests for approvals of Site Development Plans and/or Subdivision Plats shall be accompanied by a drainage submittal. The particular nature, location, and scope to the proposed development defines the degree of drainage detail. One or more of the following levels of submittal may be required based on the followin

1. Conceptual Grading and Drainage Plan: Required for approval of Site Development Plans greater than five (5) acres and Sector Plans

- Drainage Plans: Required for building permits, grading permits, paving permits and site plans less than five (5) acres
- **Drainage Report**: Required for subdivision containing more than ten (10) lots or constituting five (5) acres or more
- Erosion and Sediment Control Plan: Required for any new development and redevelopment site with 1-acre or more of land disturbing area, including project less than 1-acre than are part of a larger common plan of development

