



February 1, 2019

Mr. Terry Brown  
P.O Box 92051  
Albuquerque, New Mexico 87199

**Subject: Alameda Car Wash Traffic Impact Study  
NM 528 (MP 0.25 to 2) between 4<sup>th</sup> Street and 2<sup>nd</sup> Street  
Albuquerque, Bernalillo County, New Mexico**

Dear Mr. Brown:

This letter is to inform you that the **FINAL** Traffic Impact Study (TIS) for the proposed Alameda Car Wash on the north side of NM 528 between 4<sup>th</sup> Street and 2<sup>nd</sup> Street dated January 16, 2019 has been reviewed.

The NMDOT has no objection to the (1) partial Right-In/Out, Left-In access on NM 528 located approximately 500 feet east of the signalized intersection of NM 528 and 4<sup>th</sup> Street. See Exhibit A for the conceptual site plan.

This TIS proposed the recommendations shown in Exhibit B. It should be noted, that the TIS did not reflect actual demand, therefore it does not reflect the existing conditions of the delays on NM 528 during peak hours. NMDOT gives final concurrence of this development given the conditions below are met.

1. NMDOT concurs with the site plan that maximizes on-site queuing by positioning the building closest to NM 528.
2. The Property Owner shall submit a Commercial Driveway Application for Driveway "A". Access shall be located on the most eastern side of the property.
3. The development shall build a westbound deceleration lane/taper for as long as possible at Driveway "A".
4. The development shall modify the existing inside median curb and gutter and landscaping on NM 528 at 4<sup>th</sup> Street to maximize the potential need for the westbound left turn lane.
5. The development shall remove the existing driveway and replace the sidewalk, curb and gutter in kind.
6. The development shall reconstruct any existing damaged sidewalk along its frontage of NM 528.

**Michelle Lujan  
Grisham**  
Governor

**Michael R. Sandoval**  
Cabinet Secretary-Designate

**Commissioners**

**Ronald Schmeits**  
Chairman  
District 4

**Dr. Kenneth White**  
Commissioner  
District 1

**David Sepich**  
Commissioner  
District 2

**Keith Mortensen**  
Commissioner  
District 3

**Butch Mathews**  
Commissioner  
District 5

**Billy Moore**  
Commissioner  
District 6

7. The development shall restripe the northbound approach at NM 528 and 2<sup>nd</sup> Street to provide for additional northbound left turn storage for as long as needed and available.
8. The NMDOT shall provide final approval on the design for the offsite improvements. The developer shall agree to incorporate all the comments requested by the NMDOT.

In addition to the TIA, all improvements are based on other factors, including but not limited to, the State Access Management Manual (SAMM) design criteria, Pedestrian Right of Way Accessibility Guidelines (PROWAG), roadway design references and any local jurisdiction planning documents.

The following information will be required in combination with the approval of the development:

- a. All geometric details associated with the proposed offsite improvements as shown in Exhibit B must be approved by the NMDOT. Any schematic layout(s) for the proposed improvements that is contained in the report is for informational purposes only and should not be considered as an approved final design. These proposed improvements may include, but are not limited to:
  - Acceleration/deceleration lanes
  - Roadway widening
  - Traffic signal
- b. Detailed construction plans, including traffic control plans, for the proposed roadway improvements shall be submitted to Margaret Haynes, P.E at [Margaret.Haynes@state.nm.us](mailto:Margaret.Haynes@state.nm.us) prior to any driveway application submittals. The roadway design shall be compliant with proposed right-of-way accessibility guidelines (PROWAG) for pedestrian facilities.
- c. Grading and drainage plans, shall be submitted with the driveway application for review and approval by Mr. Keith Thompson, PE District 3 Engineer. Mr. Thompson can be reached at [Keith.Thompson@state.nm.us](mailto:Keith.Thompson@state.nm.us)
- d. Cultural resource approval will need to be obtained from Ms. Kasey Flavin for disturbance to the state right-of-way. Ms. Flavin can be reached at [Kasey.Flavin@state.nm.us](mailto:Kasey.Flavin@state.nm.us)
- e. All utility and traffic control permits, within state right-of-way related to the proposed development shall be submitted to Mr. Peter Kubiak. Mr. Kubiak can be reached at [Peter.Kubiak@state.nm.us](mailto:Peter.Kubiak@state.nm.us)

If you have any questions, please feel free to call me at 505.206.1069 or email me at [Nancy.Perea@state.nm.us](mailto:Nancy.Perea@state.nm.us)

Sincerely,



Nancy R. Perea, P.E.  
District 3 Traffic Engineer

Copies:

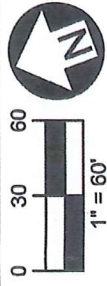
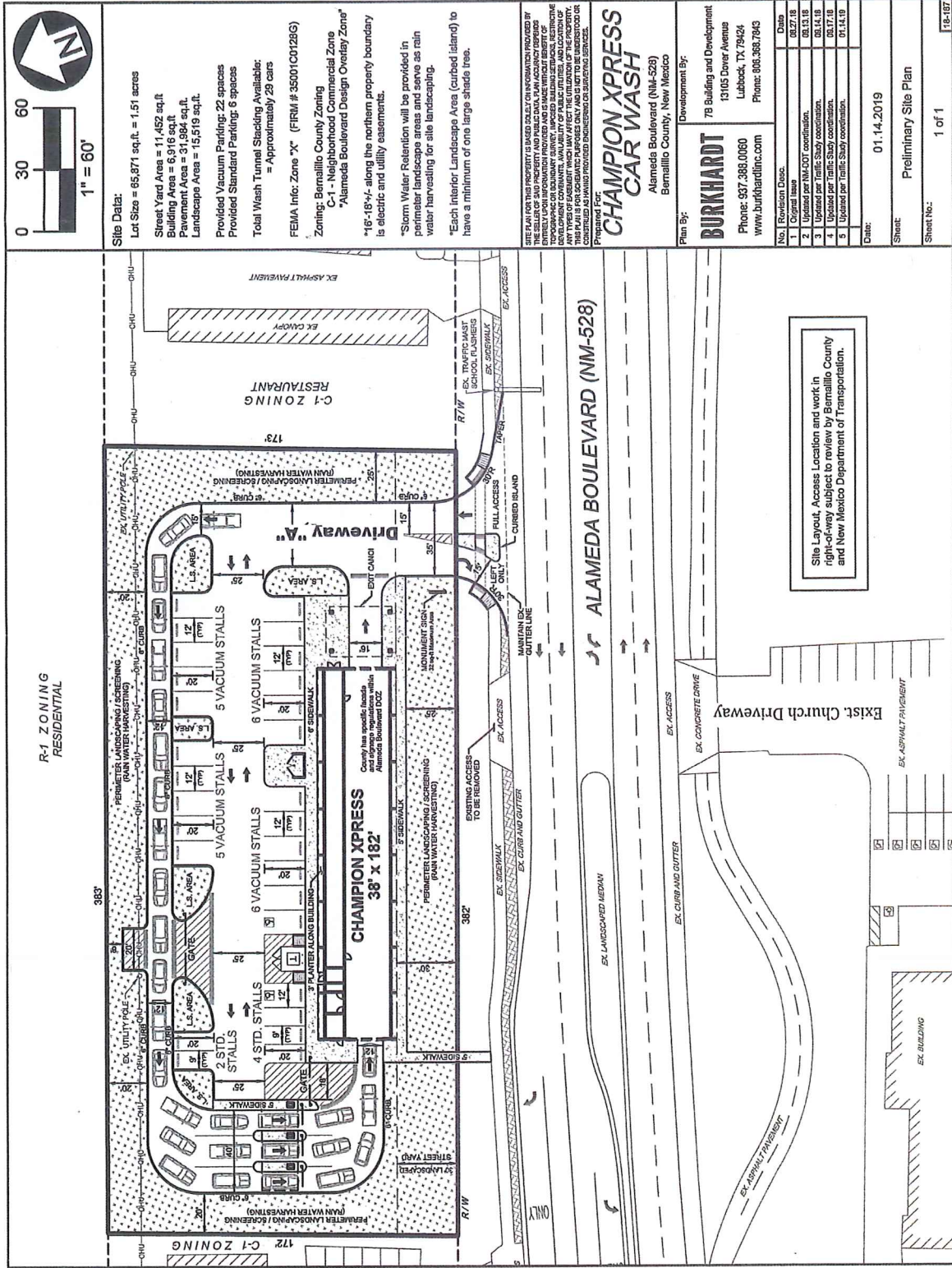
Jill Mosher, NMDOT D3 ADE  
Margaret Haynes, NMDOT D3 ADTE  
Kasey Flavin, NMDOT GO  
Racquel Michel, COA  
Ernest Armijo, COA  
Tim Brown, COA  
Julie Luna, BC  
Bobby Baker, BC  
file

Attachments:

Exhibit A – Schematic of proposed site plan  
Exhibit B – TIS page 18 – Summary of Deficiencies, Anticipated Impacts, and Recommendations

# EXHIBIT A





**Site Data:**  
Lot Size = 65,871 sq.ft. = 1.51 acres  
Street Yard Area = 11,452 sq.ft.  
Building Area = 6,916 sq.ft.  
Pavement Area = 31,584 sq.ft.  
Landscape Area = 15,519 sq.ft.  
Provided Vacuum Parking: 22 spaces  
Provided Standard Parking: 6 spaces  
Total Wash Tunnel Stacking Available:  
= Approximately 29 cars  
FEMA Info Zone "X" (FIRM # 35001C0128G)  
Zoning: Bernalillo County Zoning  
C-1 Neighborhood Commercial Zone  
"Alameda Boulevard Design Overlay Zone"  
"16'-18'-1/2" along the northern property boundary  
is electric and utility easements.  
"Storm Water Retention will be provided in  
perimeter landscape areas and serve as rain  
water harvesting for site landscaping.  
"Each Interior Landscape Area (curbed island) to  
have a minimum of one large shade tree.

THIS PLAN FOR THIS PROPERTY IS BASED ONLY ON INFORMATION PROVIDED BY THE OWNER AND THE PROPERTY AND PUBLIC DATA. AN ACCURATE RECORD OF THE PROPERTY AND PUBLIC DATA IS NOT GUARANTEED. THE OWNER AND THE PROPERTY AND PUBLIC DATA ARE NOT GUARANTEED. THE OWNER AND THE PROPERTY AND PUBLIC DATA ARE NOT GUARANTEED. THE OWNER AND THE PROPERTY AND PUBLIC DATA ARE NOT GUARANTEED.

**CHAMPION XPRESS  
CAR WASH**  
Alameda Boulevard (NM-528)  
Bernalillo County, New Mexico

Plan By: **BURKHARDT**  
Development By: 7B Building and Development  
13105 Dwyer Avenue  
Lubbock, TX 79424  
Phone: 937.388.0060  
www.burkhardtline.com

No.	Revision Desc.	Date
1	Original Issue	08/27/18
2	Updated per NMCOT conditions	08/14/19
3	Updated per NMCOT conditions	08/14/19
4	Updated per Traffic Study coordination	08/17/19
5	Updated per Traffic Study coordination	01/14/19

Date: 01.14.2019  
Sheet: Preliminary Site Plan  
Sheet No.: 1 of 1  
18-187

Site Layout, Access Location and work in right-of-way subject to review by Bernalillo County and New Mexico Department of Transportation.

## EXHIBIT B

## Summary of Deficiencies, Anticipated Impacts, and Recommendations

The existing 2020 analysis did not determine any significant deficiencies in the adjacent transportation system. The LOS / Volumes / Geometry map at the end of this report summarize the 2020 NO BUILD Conditions and the 2020 BUILD Conditions analyzed.

### Recommendations:

Based on 2020 (Implementation Year) Analysis:

- An eastbound left turn deceleration lane and a westbound right turn deceleration lane are both warranted at the proposed Driveway "A" accessing the new car wash facility. Based on the posted speed limit of 35 MPH on Alameda Blvd. along the frontage of this project, the New Mexico Department of Transportation's State Access Management Manual stipulates that the right turn deceleration lane should be designed and constructed to a length of 230 feet plus transition. It stipulates that the left turn deceleration lane should be designed and constructed to a length of 250 feet plus transition. Field constraints imposed by urban conditions preclude construction of the right turn and left turn deceleration lanes warranted. This study recommends that the westbound right turn deceleration lane be designed and constructed to begin at the west curb return of the adjacent Sonic Drive-In Restaurant driveway and extend about 65 feet to the new car wash driveway. Also recommended is that the existing center two-way left turn lane be utilized to serve as the eastbound left turn deceleration lane for the new car wash driveway. The available queuing for the eastbound left turn in the TWLTL is approximately 80 feet. The calculated 95<sup>th</sup> percentile queue length associated with the eastbound left turn volumes into the car wash driveway is one vehicle (actually, less than one). See Page A-43 for HCM 6<sup>th</sup> Edition TWSC analysis with queuing report.
- Re-stripe the northbound approach on Second St. south of Alameda Blvd. to provide optimum northbound left turn storage by extending the northbound outside left turn lane by an additional 250 feet if possible.