

CITY OF ALBUQUERQUE



October 15, 2019

Treveston Elliott, RA
Treveston Elliott Architect
811 12th St NW
Albuquerque, NM 87102

Re: Rexel
5300 Eagle Rock NE
30-Day Temporary Certificate of Occupancy
Transportation Development Final Inspection
Engineer's/Architect's Stamp dated 4-1-19 (C18D037C)
Certification dated 10-07-19

Dear Mr. Elliott

Based upon the information provided in your submittal received 10-14-18, Transportation Development has no objection to a 30-day Temporary Certificate of Occupancy based. This letter serves as a "green tag" from Transportation Development for a 30-day Temporary Certificate of Occupancy to be issued by the Building and Safety Division.

Prior to the issuance of a permanent Certificate of Occupancy, the following items must be addressed:

- Motorcycle parking spaces shall be designated by its own conspicuously posted upright sign, either free-standing or wall mounted per the zoning code. (Each stall requires its own individual sign)
- Please relocate bike racks. Picnic table blocking required clearance. (See attached bike rack details.)

Once these corrections are complete, email pictures showing the changes to PLNDRS@cabq.gov, and submit an administrative fee of \$40.

Once corrections are complete resubmit

1. The approved and stamped TCL for Building Permit with changes drawn in red.
2. Transportation Certification letter on either the plan or applicants letterhead.
3. A Drainage Transportation Information Sheet (DTIS)
4. Send an electronic copy of your submittal to PLNDRS@cabq.gov.

Sincerely,

A handwritten signature in red ink, appearing to read 'Ernie Gomez', with a stylized flourish extending to the right.

Ernie Gomez,
Plan Checker, Planning Dept.
Development Review Services

EG via: email
C: CO Clerk, File

TREVESTON ELLIOTT ARCHITECT

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87102

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TRAFFIC CERTIFICATION

I, TREVESTON ELLIOTT ARCHITECT HEREBY CERTIFY THAT THE PROJECT AT REXEL CORP. AT 5300 EAGLE ROCK AVE. NE 87113 IS IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED APRIL 1, 2019. THE RECORD INFORMATION EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED BY TREVESTON ELLIOTT OF THE FIRM TREVESTON ELLIOTT ARCHITECT. I FURTHER CERTIFY THAT I HAVE PERSONALLY VISITED THE PROJECT SITE ON AND HAVE DETERMINED BY VISUAL INSPECTION THAT THE SURVEY DATA PROVIDED IS REPRESENTATIVE OF ACTUAL SITE CONDITIONS AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR CERTIFICATE OF OCCUPANCY.

WITH ONE EXCEPTIONS.

THE BIKE RACKS RELOCATED SOUTH APPROXIMATELY 130' TO EXISTING CONCRETE PAD.

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE TRAFFIC ASPECTS OF THIS PROJECT. THOSE RELYING ON THE RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.

 10.7.19 Treveston Elliott RA



10.7.19

3. Design Standards

3.11 Off-Street Parking and Site Design

This section provides guidance on site design and off-street parking layout. The overall site design shall accommodate all modes of transportation including automobiles, pedestrians, bicyclists, and motorcyclists. To facilitate efficient parking operations, the designer shall also consider the interface of the site with adjacent development areas.

3.11.1 General Provisions

1. All sites and off-street parking areas shall be designed to comply with ADA/PROWAG standards.
2. The number of off-street, vehicle, bicycle, and motorcycle parking spaces shall be provided as established in the IDO.
3. Site design shall comply with design requirements and landscape buffers established by the IDO.
4. Parking and site layout shall be designed such that vehicles do not back into the public right-of-way, except single-family dwellings may back into local streets.

3.11.2 Bicycle Parking

Off-street bicycle parking location, layout, and rack options vary widely. The following guidelines shall be considered when placing and designing bicycle parking areas and choosing rack options. Alternative rack design, placement, or installation methods not meeting the guidelines below may be considered and are reviewed on a case-by-case basis by the City Engineer.

1. All bicycle racks shall be designed according to the following guidelines:
 - a. The rack shall be a minimum 30 inches tall and 18 inches wide.
 - b. The bicycle frame shall be supported horizontally at two or more places. Comb/toaster racks are not allowed.
 - c. The rack shall be designed to support the bicycle in an upright position. See the IDO for additional information.
 - d. The rack allows varying bicycle frame sizes and styles to be attached.
 - e. The user is not required to lift the bicycle onto the bicycle rack.
 - f. Each bicycle parking space is accessible without moving another bicycle.
2. Bicycle parking spaces shall be located in a well-lit area, visible from and, where feasible, located within 50 feet of the primary pedestrian entrance it

serves. Bicycle rack placement shall meet the following placement requirements (also see Figure 3.11-1 for direction on bicycle stall layout):

- a. Bicycle parking shall be separated from vehicle parking areas and driveways by a barrier, such as a curb, rail, or bollard, or be located to minimize the possibility of vehicles striking parked bicycles.
 - b. Bicycle racks shall be placed in a designated area and shall not infringe upon the width of the required clear pedestrian access route (see Section 23-3.5 Pedestrian Facilities).
 - c. Bicycle racks shall not be placed directly in front of entrances or in locations that impede pedestrian flow.
3. Bicycle racks shall be sturdy and anchored to a concrete pad.
 4. A 1-foot clear zone around the bicycle parking stall shall be provided.
 5. Bicycle parking spaces shall be at least 6 feet long and 2 feet wide.

Figure 3.11-1: Bicycle Parking Stall Layout Options

