

# CITY OF ALBUQUERQUE



October 15, 2019

Treveston Elliott, RA  
Treveston Elliott Architect  
811 12<sup>th</sup> St NW  
Albuquerque, NM 87102

**Re: Rexel**  
**5300 Eagle Rock NE**  
**30-Day Temporary Certificate of Occupancy**  
**Transportation Development Final Inspection**  
Engineer's/Architect's Stamp dated 4-1-19 (C18D037C)  
Certification dated 10-07-19

Dear Mr. Elliott

Based upon the information provided in your submittal received 10-14-18, Transportation Development has no objection to a 30-day Temporary Certificate of Occupancy based. This letter serves as a "green tag" from Transportation Development for a 30-day Temporary Certificate of Occupancy to be issued by the Building and Safety Division.

PO Box 1293

Prior to the issuance of a permanent Certificate of Occupancy, the following items must be addressed:

Albuquerque

- Motorcycle parking spaces shall be designated by its own conspicuously posted upright sign, either free-standing or wall mounted per the zoning code. (Each stall requires its own individual sign)
- Please relocate bike racks. Picnic table blocking required clearance. (See attached bike rack details.)

NM 87103

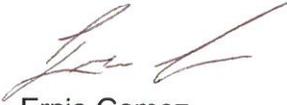
www.cabq.gov

Once these corrections are complete, email pictures showing the changes to [PLNDRS@cabq.gov](mailto:PLNDRS@cabq.gov), and submit an administrative fee of \$40.

Once corrections are complete resubmit

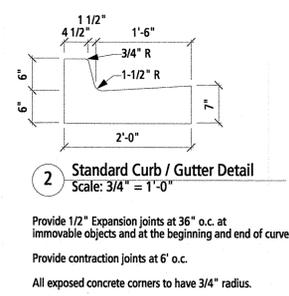
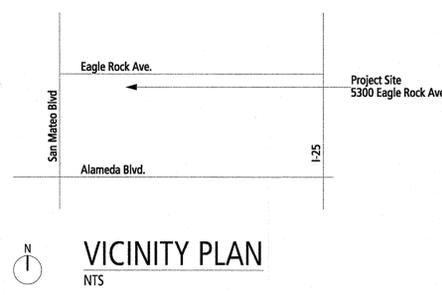
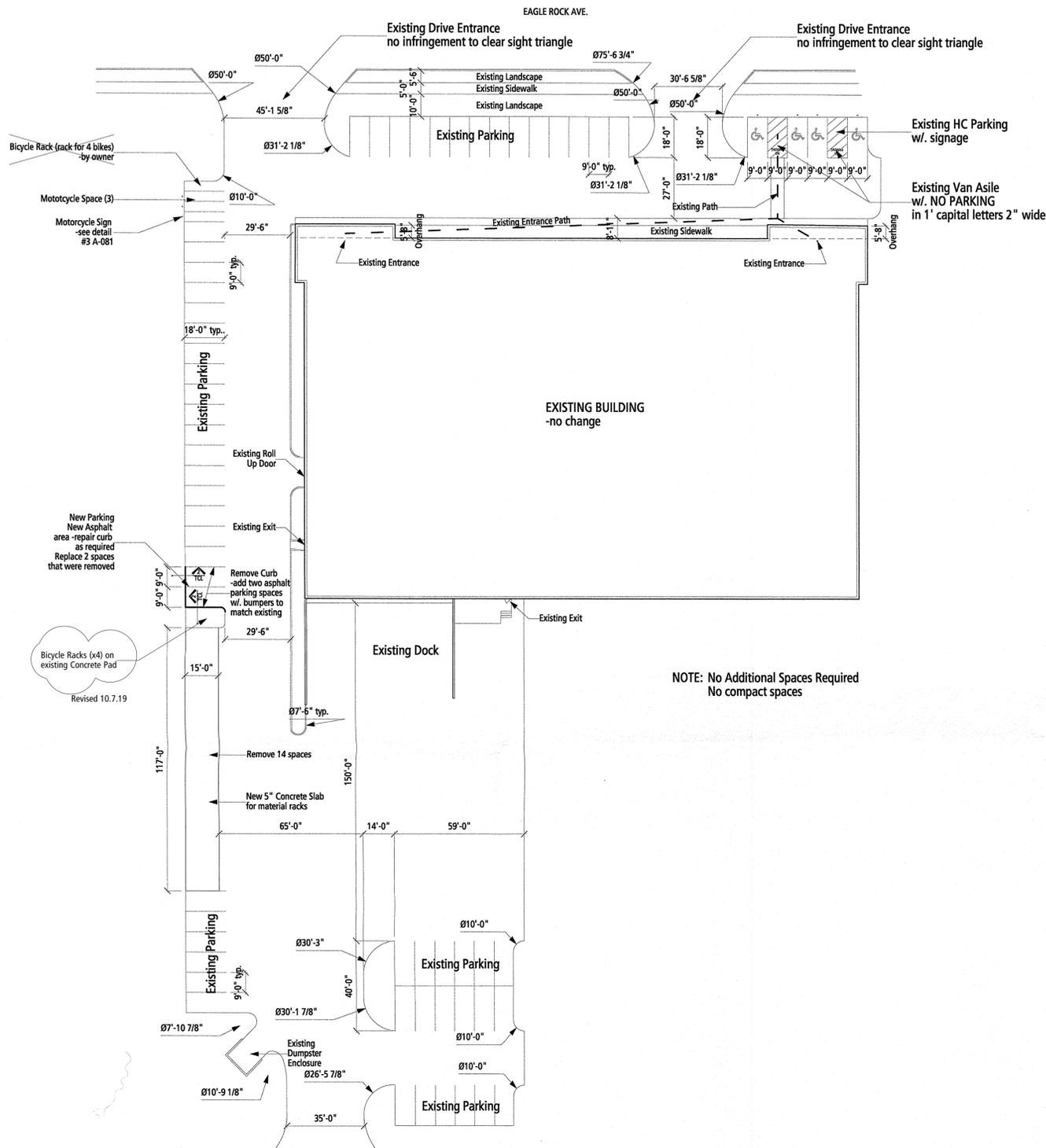
1. The approved and stamped TCL for Building Permit with changes drawn in red.
2. Transportation Certification letter on either the plan or applicants letterhead.
3. A Drainage Transportation Information Sheet (DTIS)
4. Send an electronic copy of your submittal to [PLNDRS@cabq.gov](mailto:PLNDRS@cabq.gov).

Sincerely,

A handwritten signature in red ink, appearing to read "Ernie Gomez", with a long horizontal stroke extending to the right.

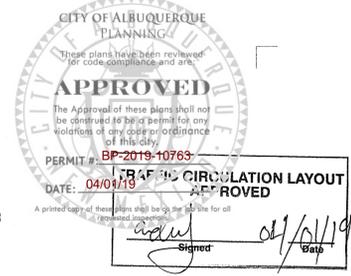
Ernie Gomez,  
Plan Checker, Planning Dept.  
Development Review Services

EG via: email  
C: CO Clerk, File



**General Note:** No change to Entrance / Exit of existing Parking area, or pedestrian circulation. 12 Spaces removed per this plan.

**CODE DATA**  
 2015 International Building Code  
 2015 Uniform Plumbing Code  
 2018 Uniform Mechanical Code  
 2017 National Electrical Code  
 2009 International Energy Conservation Code



City of Albuquerque  
 Location: 5300 Eagle Rock Ave. Albuquerque, New Mexico 87113  
 Zoning: NR-LM  
 Zoning Atlas Page: C-18  
 Setbacks: Existing- no change to building footprint  
 Height: 1-Stories no change (existing mezzanine)  
 Parking: 3,511,000 GSF (office) 6,773 GSF= 23.7 no requirement for warehouse area 1/1,000 GSF (adjacent tenant) (event facility) 18,326 GSF= 18.3  
 Total Spaces required = 42  
 Total Spaces provided = 63 (entire bldg.)  
 Bld. Area: 44,218.5 sq.ft.  
 Occupancy Type: B / S-1  
 Occupancy: B Office @2,950 sq.ft. / 100 = 29.5  
 M Will Call @1,513 sq.ft. / 60 = 25.2  
 S Will Call @1,416 sq.ft. / 300 = 4.7  
 S Warehouse @15,709 sq.ft. / 500 = 32  
 S Mezzanine @3,339sq.ft. / 500 = 6.7  
 Total Occ = 98 occ.  
 Construction Type: Type Vb  
 Separation: 1 Hour  
 Seismic: C  
 Sprinkler: Existing  
 PLUMBING REQUIREMENTS (B most restrictive)  
 B= 126 occ.  
 WC 1/25 first 50 1/50 remainder 4 required 4 provided  
 LAV. 1/40 3 required 4 provided  
 DF 1/100 2 required 2 provided  
 SS 1 1 required 1 provided  
 Alteration level 3- Remodel existing warehouse add a will call counter area and two new restrooms.

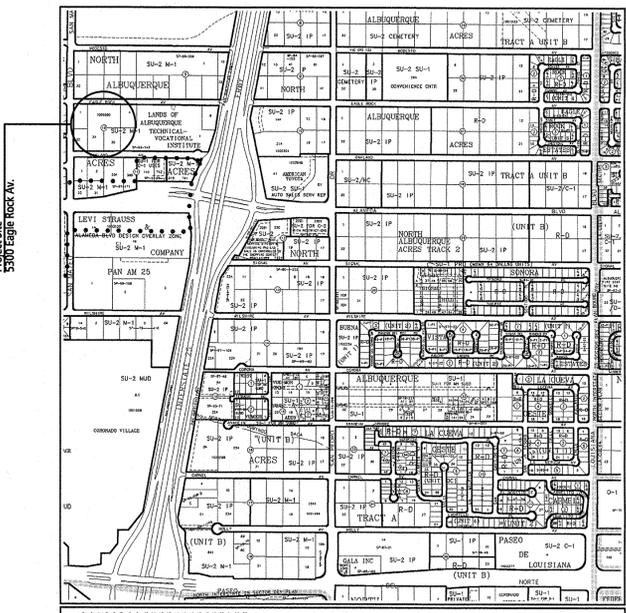
ALL WHEELCHAIR RAMP LOCATED WITHIN THE PUBLIC RIGHT OF WAY. MUST HAVE TRUNCATED DOMES.

**TREVESTON ELLIOTT ARCHITECT**  
 811 17TH ST. NW  
 ALBUQUERQUE, NEW MEXICO 87102  
 C-505.259.4617  
 www.treveston.com

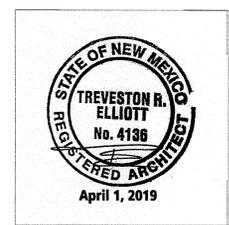


Sheet Index

#	Plan	Scale
A-081	Site Plan	1" = 30'-0"
	Demo Plan	1/16" = 1'-0"
A-101	Floor Plan	3/32" = 1'-0"
	Wall Sections	1/4" = 1'-0"
Fire-2	Fire-2	1/16" = 1'-0"
E-101	Electrical Plan	3/32" = 1'-0"
M/P-101	Plumbing Plan	3/32" = 1'-0"
	Mechanical Plan	3/32" = 1'-0"

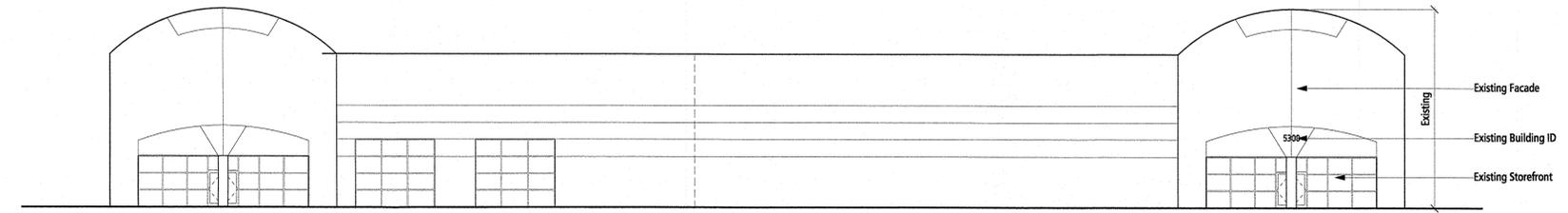


**REXEL CORP**  
 5300 Eagle Rock Ave.  
 ALBUQUERQUE, NEW MEXICO 87113



Date: April 1, 2019  
 Sheet: Traffic Circulation Layout

1 Site Plan - TCL Proposed  
 Scale: 1" = 30'-0"



3 Elevation  
 Scale: 1/16" = 1'-0"

GRAPHIC SCALE IN FEET  
 0 25 50 100  
**Zone Atlas Page**  
**C-18-Z**  
 Map Amended through December 03, 2014

**TCL**

TREVESTON ELLIOTT ARCHITECT

811 12TH STREET NW  
ALBUQUERQUE NM  
87102

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**TRAFFIC CERTIFICATION**

I, TREVESTON ELLIOTT ARCHITECT HEREBY CERTIFY THAT THE PROJECT AT REXEL CORP. AT 5300 EAGLE ROCK AVE. NE 87113 IS IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED APRIL 1, 2019. THE RECORD INFORMATION EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED BY TREVESTON ELLIOTT OF THE FIRM TREVESTON ELLIOTT ARCHITECT . I FURTHER CERTIFY THAT I HAVE PERSONALLY VISITED THE PROJECT SITE ON AND HAVE DETERMINED BY VISUAL INSPECTION THAT THE SURVEY DATA PROVIDED IS REPRESENTATIVE OF ACTUAL SITE CONDITIONS AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED EN SUPPORT OF A REQUEST FOR CERTIFICATE OF OCCUPANCY.

WITH ONE EXCEPTIONS.

THE BIKE RACKS RELOCATED SOUTH APPROXIMATELY 130' TO EXISTING CONCRETE PAD.

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE TRAFFIC ASPECTS OF THIS PROJECT. THOSE RELYING ON THE RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.

 10.7.17 Treveston Elliott RA



10.7.12

### **3. Design Standards**

#### **3.11 Off-Street Parking and Site Design**

This section provides guidance on site design and off-street parking layout. The overall site design shall accommodate all modes of transportation including automobiles, pedestrians, bicyclists, and motorcyclists. To facilitate efficient parking operations, the designer shall also consider the interface of the site with adjacent development areas.

##### **3.11.1 General Provisions**

1. All sites and off-street parking areas shall be designed to comply with ADA/PROWAG standards.
2. The number of off-street, vehicle, bicycle, and motorcycle parking spaces shall be provided as established in the IDO.
3. Site design shall comply with design requirements and landscape buffers established by the IDO.
4. Parking and site layout shall be designed such that vehicles do not back into the public right-of-way, except single-family dwellings may back into local streets.

##### **3.11.2 Bicycle Parking**

Off-street bicycle parking location, layout, and rack options vary widely. The following guidelines shall be considered when placing and designing bicycle parking areas and choosing rack options. Alternative rack design, placement, or installation methods not meeting the guidelines below may be considered and are reviewed on a case-by-case basis by the City Engineer.

1. All bicycle racks shall be designed according to the following guidelines:
  - a. The rack shall be a minimum 30 inches tall and 18 inches wide.
  - b. The bicycle frame shall be supported horizontally at two or more places. Comb/toaster racks are not allowed.
  - c. The rack shall be designed to support the bicycle in an upright position. See the IDO for additional information.
  - d. The rack allows varying bicycle frame sizes and styles to be attached.
  - e. The user is not required to lift the bicycle onto the bicycle rack.
  - f. Each bicycle parking space is accessible without moving another bicycle.
2. Bicycle parking spaces shall be located in a well-lit area, visible from and, where feasible, located within 50 feet of the primary pedestrian entrance it

serves. Bicycle rack placement shall meet the following placement requirements (also see Figure 3.11-1 for direction on bicycle stall layout):

- a. Bicycle parking shall be separated from vehicle parking areas and driveways by a barrier, such as a curb, rail, or bollard, or be located to minimize the possibility of vehicles striking parked bicycles.
  - b. Bicycle racks shall be placed in a designated area and shall not infringe upon the width of the required clear pedestrian access route (see Section 23-3.5 Pedestrian Facilities).
  - c. Bicycle racks shall not be placed directly in front of entrances or in locations that impede pedestrian flow.
3. Bicycle racks shall be sturdy and anchored to a concrete pad.
  4. A 1-foot clear zone around the bicycle parking stall shall be provided.
  5. Bicycle parking spaces shall be at least 6 feet long and 2 feet wide.

**Figure 3.11-1: Bicycle Parking Stall Layout Options**

