CITY OF ALBUQUERQUE

Planning Department Brennon Williams, Director



March 5, 2021

Mark H. Burak, P.E. 1512 Sagebrush Trail SE Albuquerque, NM 87123

RE: 8920 Eagle Rock NE

Grading and Drainage Plan Engineer's Stamp Date: 02/10/21

Hydrology File: C20D087

Dear Mr. Burak:

Based upon the information provided in your submittal received 02/22/2021, the Grading and Drainage Plan is approved for Grading Permit Grading Permit (earthwork can get started for the earth pad on the house) and Work Order.

PO Box 1293

Once the grading is complete, a pad certification (meaning that the earthwork is complete) will be required prior to release from Hydrology during the Building Permit process. Also, at the time of pad certification approval, Hydrology will concurrently approve the Grading & Drainage Plan for Building Permit.

Albuquerque

NM 87103

Prior to approval in support of Permanent Release of Occupancy by Hydrology, Engineer Certification per the DPM checklist will be required.

www.cabq.gov

As a reminder, if the project total area of disturbance (including the staging area and any work within the adjacent Right-of-Way) is 1 acre or more, then an Erosion and Sediment Control (ESC) Plan and Owner's certified Notice of Intent (NOI) is required to be submitted to the Stormwater Quality Engineer (Doug Hughes, PE, jhughes@cabq.gov, 924-3420) 14 days prior to any earth disturbance.

If you have any questions, please contact me at 924-3995 or rbrissette@cabq.gov.

Sincerely,

Renée C. Brissette, P.E. CFM

Renée C. Brissette

Senior Engineer, Hydrology Planning Department



City of Albuquerque

Planning Department

Development & Building Services Division

DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 6/2018)

Project Title: _	8920 Eagle Rock N	NE Building Pe	rmit #:	Hydrology File #: C20D087	
DRB#:		EPC#:		Work Order#:	
Legal Description	on: Lot 13, Bloc	k 2, Tract 3, Unit	3, North Albuque	rque Acres	
City Address: _	8900 Eagle	Rock NE			
Applicant:	Mark Burak, PE			Contact:	
Address:	1512 Sagebrush T	r SE, Albuquerqu	ie, NM 87123		
				E-mail: mburak@comcast.ne	
Other Contact:				Contact:	
				E-mail:	
TYPE OF DEVE	LOPMENT:I	PLAT (# of lots)	RESIDENCE	X DRB SITE ADMIN SITE	
IS THIS A RESUB	BMITTAL? X	Yes No			
DEPARTMENT _	TRANSPORTAT	ION X HYI	DROLOGY/DRAINA	AGE	
Check all that Appl	ly:		TYPE OF API	PROVAL/ACCEPTANCE SOUGHT:	
11				G PERMIT APPROVAL	
TYPE OF SUBM			CERTIFIC	CATE OF OCCUPANCY	
	ARCHITECT CERTIFIC	ATION			
PAD CERTIF			PRELIMI	NARY PLAT APPROVAL	
CONCEPTUA			SITE PLA	N FOR SUB'D APPROVAL	
_X_GRADING P			SITE PLAN FOR BLDG. PERMIT APPROVAL		
DRAINAGE			FINAL PLAT APPROVAL		
DRAINAGE					
FLOODPLAI	N DEVELOPMENT PER	RMIT APPLIC	SIA/ REL	EASE OF FINANCIAL GUARANTEE	
ELEVATION	CERTIFICATE			TION PERMIT APPROVAL	
CLOMR/LON	МR			G PERMIT APPROVAL	
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TRAFFIC IM	IPACT STUDY (TIS)			PERMIT APPROVAL	
STREET LIG	HT LAYOUT			G/ PAD CERTIFICATION	
OTHER (SPE	ECIFY)		S-10-10-10-10-10-10-10-10-10-10-10-10-10-	RDER APPROVAL	
PRE-DESIGN	MEETING?		CLOMR/I		
				LAIN DEVELOPMENT PERMIT	
				SPECIFY)	
DATE SUBMITT	ED: 02/18/2021	By:	Mark Burak, PE	<u> </u>	
COA	STAFF:	ELECTRONIC	SUBMITTAL RECEIVE	D:	

FEE PAID:_____

ABBREVIATIONS

- A = AIR LINE
- AREA = DRAIN
- AIP = ABANDONED IN PLACE
- BLDG = BUILDING
- BM = BENCHMARK
- CATV = CABLE TELEVISION LINE
- CIP = CAST IRON PIPE
- CMP = CORRUGATED PIPE
- CMPA = CORRUGATED METAL PIPE ARCH
- CO = CLEANOUT
- COA = CITY OF ALBUQUERQUE
- CONC = CONCRETE
- = CENTERLINE
- = DIAMETER
- = DUCTILE IRON PIPE
- DTL = DETAIL
- DWG = DRAWING
- EG = EXISTING GRADE
- ELEC = ELECTRIC
- ELEV = ELEVATION
- EX = EXISTING
- FF = FINISH FLOOR ELEVATION
- FG = FINISH GRADE
- FH = FIRE HYDRANT
- = FLOW LINE
- = GAS PIPE
- GV = GATE VALVE

GM = GAS METER

- HI PT = HIGH POINT
- INV = INVERT ELEVATION
- = LIGHT POLE
- L/S = LANDSCAPING
- = MANHOLE = NATURAL GRADE
- = OVERHEAD ELECTRIC LINE = OVERHEAD TELEPHONE LINE
- PB = ELECTRIC PULL BOX
- PCC = PORTLAND CEMENT CONCRETE
- PP = POWER POLE
- PROP = PROPOSEDPVC = POLYVINYL CHLORIDE PIPE
- RCP = REINFORCED CONCRETE PIPE
- R/W = RIGHT-OF-WAY
- = SLOPE SAS = SANITARY SEWER
- SD = STORM DRAIN
- STA = STATION
- STD = STANDARD
- SW = SIDEWALK
- = TELEPHONE
- TA = TOP OF ASPHALT PAVEMENT
- TAC = TOP OF ASPHALT CURB
- TC = TOP OF CONCRETE SLAB (PAVEMENT)
- TCC = TOP OF CONCRETE CURB
- TS = TOP OF SIDEWALK
- TW = TOP OF WALL
- TYP = TYPICAL
- = UNDERGROUND ELECTRIC
- = UNDERGROUND TELEPHONE
- = WATER
- WM = WATER METER
- WV = WATER VALVE

8920 EAGLE ROCK AVENUE GRADING & DRAINAGE PLAN CURB, GUTTER & SIDEWALK ADDITION

GENERAL NOTES:

- 1. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED, EXCEPT AS OTHERWISE STATED OR APPROVED HEREON, SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION THROUGH MOST RECENT UPDATE AND WILL BE REFERRED TO HERE ON AS "STANDARD
- 2. ALL CONSTRUCTION WITHIN CITY RIGHT-OF-WAY OR EASEMENTS MUST BE DONE FROM APPROVED WORK ORDER DOCUMENTS FROM THE COUNTY AND/OR CITY OF ALBUQUERQUE.
- 3. ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, ORDINANCES, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH
- 4. CONTRACTOR AGREES THAT HE SHALL ASSUME THE SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD HARMLESS THE OWNER AND ENGINEER FRON ANY AND ALL LIABILITY REAL OR ALLEGED, IN CONNECTION WITH PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.
- 5. ALL EXCAVATION, TRENCHING, AND SHORING ACTIVITIES MUST BE ACCOMPLISHED IN ACCORDANCE WITH OSHA 29CFR 1926.650 SUBPART P.
- 6. AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN COUNTY RIGHT-OF-WAY.
- 7. CONTRACTOR SHALL SECURE A "TOPSOIL DISTURBANCE PERMIT" PRIOR TO BEGINNING CONSTRUCTION (IF REQUIRED BY THE CITY OF ALBUQUERQUE).
- 8. PERMIT REQUESTS MAY BE DENIED OR DELAYED DUE TO CONFLICTS WITH OTHER PROJECTS IN THE AREA.
- 9. THE CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY MAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. ONLY THE CITY SURVEYOR SHALL REPLACE SURVEY MONUMENTS. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO STANDARD SPECIFICATIONS SECTION 4.4.
- 10. CONTRACTOR SHALL COORDINATE WITH A.B.C.W.U.A. (857-8200) SEVEN (7) WORKING DAYS PRIOR TO ANY WORK THAT MAY AFFECT EXISTING ABCWUA PUBLIC WATER OR SEWER UTILITIES. CONTRACTOR IS REFERRED TO SECTION 18 OF THE COA STANDARD SPECIFICATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR TIMING AND COORDINATION OF WATER SHUTOFF. EXISTING ABCWUA VALVES TO BE OPERATED BY CITY PERSONNEL ONLY.
- 11. FIVE (5) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, CONTRACTOR SHALL SUBMIT TO ABCWUA A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE ABCWUA. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (848-1504) PRIOR TO OCCUPYING AN INTERSECTION. REFER TO SECTION 19 OF THE STANDARD SPECIFICATIONS.
- 12. CONTRACTOR SHALL DETERMINE IN ADVANCE OF HIS CONSTRUCTION IF OVERHEAD UTILITIY LINES, SUPPORT STRUCTURES, POLES, GUYS, ETC. ARE AN OBSTRUCTION TO CONSTRUCTION OPERATIONS. IF ANY OF OBSTRUCTION TO CONSTRUCTION OPERATIONS IS EVIDENT, CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE APPROPRIATE UTILITY OWNER TO REMOVE OR SUPPORT THE UTILITY OBSTRUCTION. ANY COST ASSOCIATED WITH THIS EFFORT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

NEW CURB AND GUTTER & SIDEWALK

PROPOSED FIRE HYDRANT

PROPOSED WATER VALVE

PROPOSED WATER VALVE

EXISTING CURB AND GUTTER & SIDEWALK

LEGEND

- 13. PNM WILL PROVIDE AT NO COST TO THE CITY OR THE CONTRACTOR THE REQUIRED PERSONNEL FOR INSPECTION OR OBSERVATION DEEMED NECESSARY BY PNM WHILE THE CONTRACTOR IS EXPOSING PNM'S CABLES. HOWEVER, THE CONTRACTOR SHALL BE CHARGED THE TOTAL COST ASSOCIATED WITH REPAIRS TO ANY DAMAGED CABLES OR FOR ANY COST ASSOCIATED WITH SUPPORTING OR RELOCATING THE POLES AND CABLES DURING CONSTRUCTION.
- 14. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
- 15. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL PERTINENT EXISTING UTILITIES AND/OR OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- 16. EXISTING UTILITY LINE LOCATIONS ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. THE LOCATION OF ANY SUCH EXISTING LINE IS BASED UPON INFORMATION PROVIDED BY THE OTHER UTILITY COMPANY, THE OWNER, OR BY OTHERS, AND THE INFORMATION MAY BE INCOMPLETE OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES.
- 17. THE ENGINEER HAS UNDERTAKEN NO FIELD VERIFICATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UNDERGROUND UTILITY LINES, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE IN OR NEAR THE ARE OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY, AND PRESERVE ANY AND ALL EXISTING UTILITIES.
- 18. CONTRACTOR SHALL SUPPORT ALL EXISTING, UNDERGROUND UTILTY LINES WHICH BECOME EXPOSED DURING CONSTRUCTION. PAYMENT FOR SUPPORTING WORK SHALL BI INCIDENTAL TO WATER LINE AND/OR SEWER LINE COSTS.
- 19. CONTRACTOR IS TO SUPPORT AND MAINTAIN THE INTEGRITY OF ALL UNDERGROUND TELEPHONE, ELECTRIC CABLES AND CABLE TELEVISION UTILITIES AT NO ADDITIONAL COST TO THE OWNER. CABLE IS TO BE SUPPORTED AT A MAXIMUM OF EVERY FIFTEEN (15) FEET. CONTRACTOR SHALL COORDINATE WITH AND MAKE NECESSARY PAYMENT (IF ANY) TO UTILITY OWNER FOR DE-ENERGIZATION OF CABLES OR SUPPORT OF CABLES BY THE UTILITY OWNER.
- 20 CONTRACTOR SHALL ASSIST THE ENGINEER/INSPECTOR IN THE RECORDING OF DATA ON ALL UTILITY LINES AND ACCESSORIES AS REQUIRED BY THE CITY OF ALBUQUERQUE FOR THE PREPARATION OF "AS-CONSTRUCTED" DRAWINGS. CONTRACTOR SHALL NOT COVER UTILITY LINES AND ACCESSORIES UNTIL ALL DATA HAS BEEN RECORDED.
- 21. ALL UTILITIES AND UTILITY SERVICE LINES SHALL BE INSTALLED PRIOR TO
- 22. ALL FINAL BACKFILL TRENCHES SHALL BE COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY PER ASTM-D1557 AND AS DIRECTED BY STANDARD SPECIFICATIONS SECTION 701.14.2 AND STANDARD DRAWING NUMBER 2315
- 23. CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY OR PRIVATE ROADWAY EASEMENTS SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET OR INTO ANY PUBLIC DRAINAGE FACILITY.
- 24 REMOVALS SHALL BE DISPOSED OF OFF-SITE AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR

PROPOSED SAS MANHOLE

PROPOSED WATER METER

EXISTING SAS MANHOLE

(\$ 83°39'25" W) RECORD BEARING AND DISTANCE

25. THE CONTRACTOR SHALL DISPOSE OF ALL UNSUITABLE MATERIAL IN AN ENVIRONMENTALLY ACCEPTABLE MANNER AT A LOCATION ACCEPTABLE TO THE PROJECT MANAGER. THERE WILL NO DIRECT COMPENSATION FOR THIS

S 83°39'25" W MEASURED BEARING AND DISTANCE

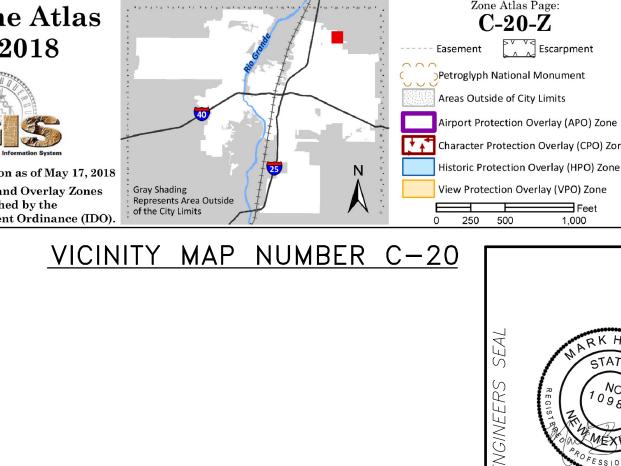
--6"W -- Existing water line

- 26. CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN A MANNER WHICH WILL MINIMIZE INTERFERENCE WITH LOCAL TRAFFIC.
- 27. ANY WORK AFFECTING AN ARTERIAL ROADWAY REQUIRES TWENTY-FOUR (24) HOURS OF NOTICE TO CITY OF ALBUQUERQUE PRIOR TO COMMENCEMENT
- 28. ALL EXISTING SIGNS, MARKERS, DELINEATORS, ETC. WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED, STORED AND RE-SET BY THE
- 29. WHEN ABUTTING EXISTING PAVEMENT TO NEW, SAW CUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER. REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED. THE CONTRACTOR SHALL CALL CITY OF ALBUQUERQUE PUBLIC WORKS TO REQUEST AN INSPECTOR TO VERIFY PAVEMENT THICKNESS.
- REMOVAL OF EXISTING CURB AND GUTTER OR SIDEWALK SHALL BE TO THE NEAREST JOINT OR SAW CUT.
- 31. AT HIS OWN EXPENSE, CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENT, PAVEMENT MARKINGS, CURB AND GUTTER, HANDICAP RAMPS, AND SIDEWALK DURING CONSTRUCTION APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS AND SHALL REPAIR OR REPLACE. PER STANDARD SPECIFICATIONS.
- 32. ALL STREET STRIPING, ALTERED OR DESTROYED, SHALL BE REPLACED WITH PLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR TO SAME LOCATION AS EXISTING, OR AS INDICATED BY THIS PLAN SET.
- 33. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHEATHER PERMANENT OR TEMPORARY.
- 34. ELECTRONIC MARKER DISKS (EMD) SHALL BE PLACED ACCORDING TO SECTION 170 OR THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION AS REVISED THROUGH UPDATE #7.
- 35. THE CONTRACTOR WILL CONFINE HIS WORK WITHIN THE CONSTRUCTION EASEMENT LIMITS AND/OR RIGHT-OF-WAY OR PROVIDE COPIES OF AGREEMENTS WITH ADJACENT LANDOWNERS TO THE CITY.
- THE CONTRACTOR SHALL PERFORM ALL WORK IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE SYSTEM (NPDES) REQUIREMENTS INCLUDING, BUT NOT LIMITED TO, OBTAINING AN NPDES PERMIT DURING CONSTRUCTION, SUBMISSION OF A COMPLETED NOTICE OR INTENT (NOI) APPLICATION, AND SUBMISSION OF A COMPLETED NOTICE OF TERMINATION (NOT). THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR TE IMPLÉMENTATION OF AND INSPECTION REPORTS FOR THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP). IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE THE SWPPP RÉVIEWED AND APPROVED BY CITY PUBLIC WORKS DIVISION, AT NO ADDITIONAL COST TO THE CITY, PRIOR TO IMPLEMENTATION OF THE SWPPP.
- 37 C.O.A. HAS JURISDICTION OVER EAGLE ROCK AVENUE. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION WORK WITHIN THE EAGLE ROCK RIGHT-OF-WAY WITH C.O.A. AND OBTAIN ALL PERMITS FOR SAID CONSTRUCTION WORK.

INDEX OF DRAWINGS **DESCRIPTION** SHEET NO.

COVER, GENERAL NOTES AND VICINITY MAP

ALBUQUERQUE DESERT RIDGE DG WELD RIDONTH O For more details about the Integrated Development Ordinance visit; http://www.cabg.gov/planning/codes-policies-regulations/integrated-development-ordinance IDO Zone Atlas C-20-ZMay 2018 Petroglyph National Monument Areas Outside of City Limits Airport Protection Overlay (APO) Zone Character Protection Overlay (CPO) Zor View Protection Overlay (VPO) Zone The Zone Districts and Overlay Zones are established by the tegrated Development Ordinance (IDO).





Mark H. Burak, 1512 Sagebrush Trail SE Albuquerque, New Mexico, 87123 (505) 235-2256 . mburak@comcast.net CITY OF ALBUQUERQUE

> ENGINEERING GROUP 8920 EAGLE ROCK AVE. NE

> > C20D087

GRADING AND DRAINAGE PLAN

Zone Map NO. | Sheet

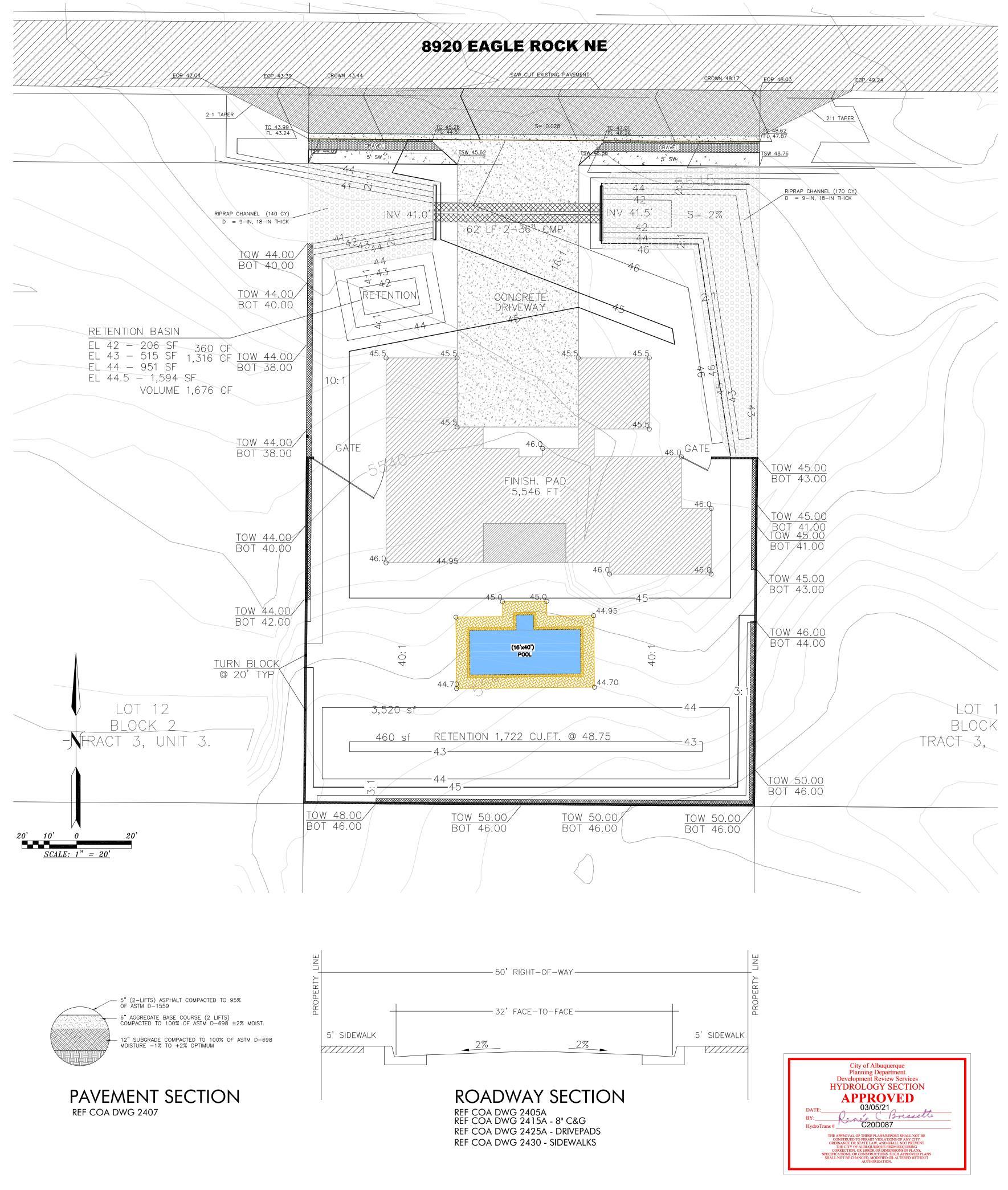
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Of

PUBLIC WORKS DEPARTMENT

SITE LAYOUT AND GRADING PLAN DRAINAGE CALCULATIONS PLAN & PROFILE SHEET

TRAFFIC CONTROL DETAILS City Project No.



Site Location - As shown by the Vicinity Map (Zone Atlas Map C-20), the proposed residential project site is located on a single 0.88-acre parcel on the south side of Eagle Rock NE and west side of Ventura in North Albuquerque Acres. At present, the site is undeveloped. Single homes are adjacent to the west of the property and across Eagle Rock to the north.

Legal Description: Lot 13 Block 2 NORTH ALBQ ACRES TR3 UNIT #3, 8900 Eagle Rock NE

Benchmark - Basis of elevation is ACS Station "5-D21" Elevation 5,653.331 NAVD 1988.

Flood Zone - As shown by Panels 35001-C0133H of the National Flood Insurance Program Flood Insurance Rate Maps (FIRM) for the City of Albuquerque, New Mexico, dated 08/16/2012 none of this site lies within a designated flood hazard Zone.

Existing Conditions - Currently, the project site slopes at about three percent and drains from east to west across the undeveloped site in a small arroyo towards the west. The arroyo runoff is intercepted by the upstream property and discharged through a double 36" cmp culvert battery near the northeastern corner of the lot adjacent to the subject property. The small arroyo discharge crosses the property in a southwesterly direction basically through the middle portion of the lot. Eagle Rock is paved with no culverts, curb or gutter. The offsite runoff generated east of the adjacent subdivision impacts the property in an historical/allowable discharge rate of 104 cfs according to the calculations. The property two lots to the east is surrounded by a retaining/garden wall which blocks all runoff from impacting the subject property other than the culvert discharge. The vacant lot to the east drains to the arroyo and through the subject property.

Proposed Grading - The Grading and Drainage Plan shows 1) existing and proposed grades indicated by spot elevations and contours; 2) the limit of existing and proposed improvements.

The upstream watershed is collected and discharged through an existing culvert battery under the driveway of the property two lots to the east of the subject parcel. This runoff will traverse the adjacent property and will impact the subject approximately 100-feet south of Eagle Rock NE. The grading of the lot will elevate the residence about three feet above the bottom of the arroyo and will divert the runoff towards the north to cross under the proposed driveway utilizing two 36" diameter CMP culverts. The culverts have the capacity to carry the entire peak runoff rate with a headwall headwater depth of 4.2-feet. The riprap channel upstream and downstream of the driveway culverts has a slope of two percent, and a ten foot bottom width with 2:1 side slopes. The maximum velocity was calculated at 7.4-fps which will require a riprap nominal size of nine inches. The riprap bedding should extend a minimum of eighteen inches below the channel invert.

Most runoff on the property is to be discharged into two retention basins that are to be located either along the southern property boundary or in the northwest corner of the property. These retention basins will provide adequate storage for the 100-year event. To achieve adequate pad and fill height, a six foot retaining wall is proposed along a portion of the western and southern sides of the property. An additional shorter segment will be located along a portion of the eastern property boundary.

Cut and or fill slopes along the east and south boundary areas were set at a maximum of 3:1 so that no armoring will be required. The northern retention pond is two feet deep and will be sloped at a 3:1 and will require armoring with cobble.

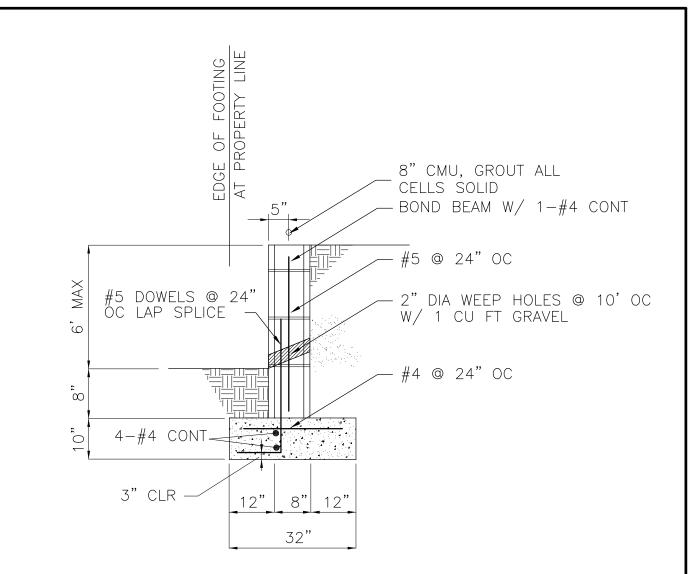
As part of this construction, the southern half of Eagle Rock Avenue will be installed including curb, gutter, and sidewalk. The proposed roadway section is 32-feet face to face. Since the existing pavement is mostly located on the northern side of the right-of-way, the new asphalt will extend 14-feet to the centerline of the roadway. The curb, gutter, sidewalk and roadway will be sloped at 2.8-percent to match existing conditions.

Hydrologic Methods - The drainage basin map shows six separate sub-basins (offsite, and A-E) impacting the project area to assess peak flow rates at various points around the project site culminating at the retention basins or outfall channel. The calculations which appear hereon analyze both the existing and developed conditions for the 100-year, 6-hour rainfall event. The process outlined in the DPM, Section 22.2 was used to quantify the peak flow rates and volumes. As shown by these calculations, the fully developed improvements will result in a slight increase in runoff generated by the site. When incorporating the proposed ponding, the downstream impact is similar when comparing to existing and/or historical conditions.

The proposed improvements will increase the existing peak runoff due to the higher percentage of impervious area proposed by the development. By controlling the calculated runoff within the retention areas, scour and erosion is expected to be reduced to a minimum amount. A spreadsheet for Precipitation Zone 3 is included on this plan. This spreadsheet outlines the peak runoff and volume generated for each sub-basin for existing and proposed fully developed conditions. Percentage of each land treatment is shown to illustrate the addition of impervious area related to the proposed construction. By routing the proposed developed discharge rates and volumes through the retention basins, the outfall is to be controlled by incorporating a limited capacity discharge. In this case, the discharge structure consists of a number of turned blocks in the west wall along with channelization from the south pond to the north pond. The northern spillway is to be a riprap lined weir section that will discharge into the channel south of the Eagle Rock right-of-way. The majority of the southern basin overflow will discharge through the turned blocks in the western wall to discharge onto the property to the west.

The proposed retention basins will have a maximum capacity of 667 cubic feet and 1,722 cubic feet as shown on the Plan. The cumulative volume generated for the six-hour storm for all onsite sub-basins was estimated as 6,526 cubic feet for the northern pond and 2,293 cubic feet for the southern pond. The required six-hour retention for each pond was estimated as 316 cu. ft. for the north pond and 320 cu. ft. for the southern pond. Northern retention area will pond to a depth of 1.0-foot and the southern retention area will pond at a depth of 1.0-foot.

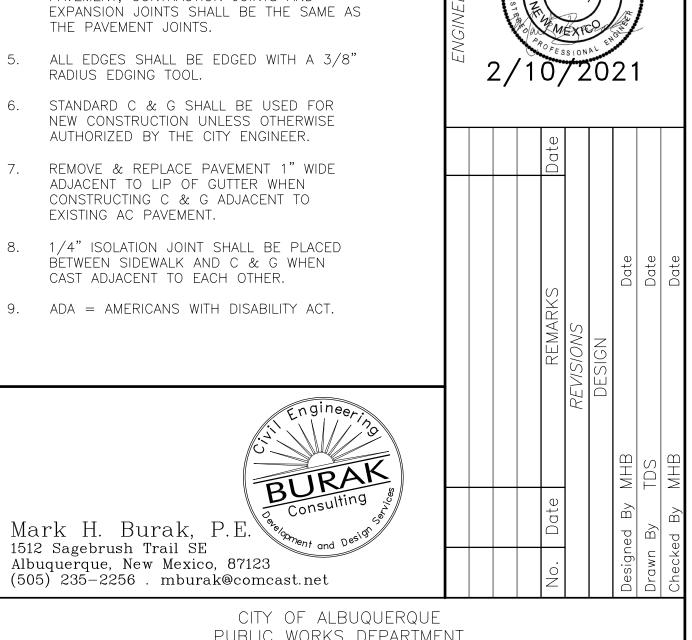
90% Compensatory Volume Management – The first flush has been mitigated based on the impervious areas listed on the attached spreadsheet. This equates to the total impervious area of the site multiplied by 0.615-inches or about 232 and 236 cubic feet for each of the two ponding areas. This storage has been provided on the plan by the retention basins as shown.



RETAINING WALL DETAIL

GENERAL NOTES:

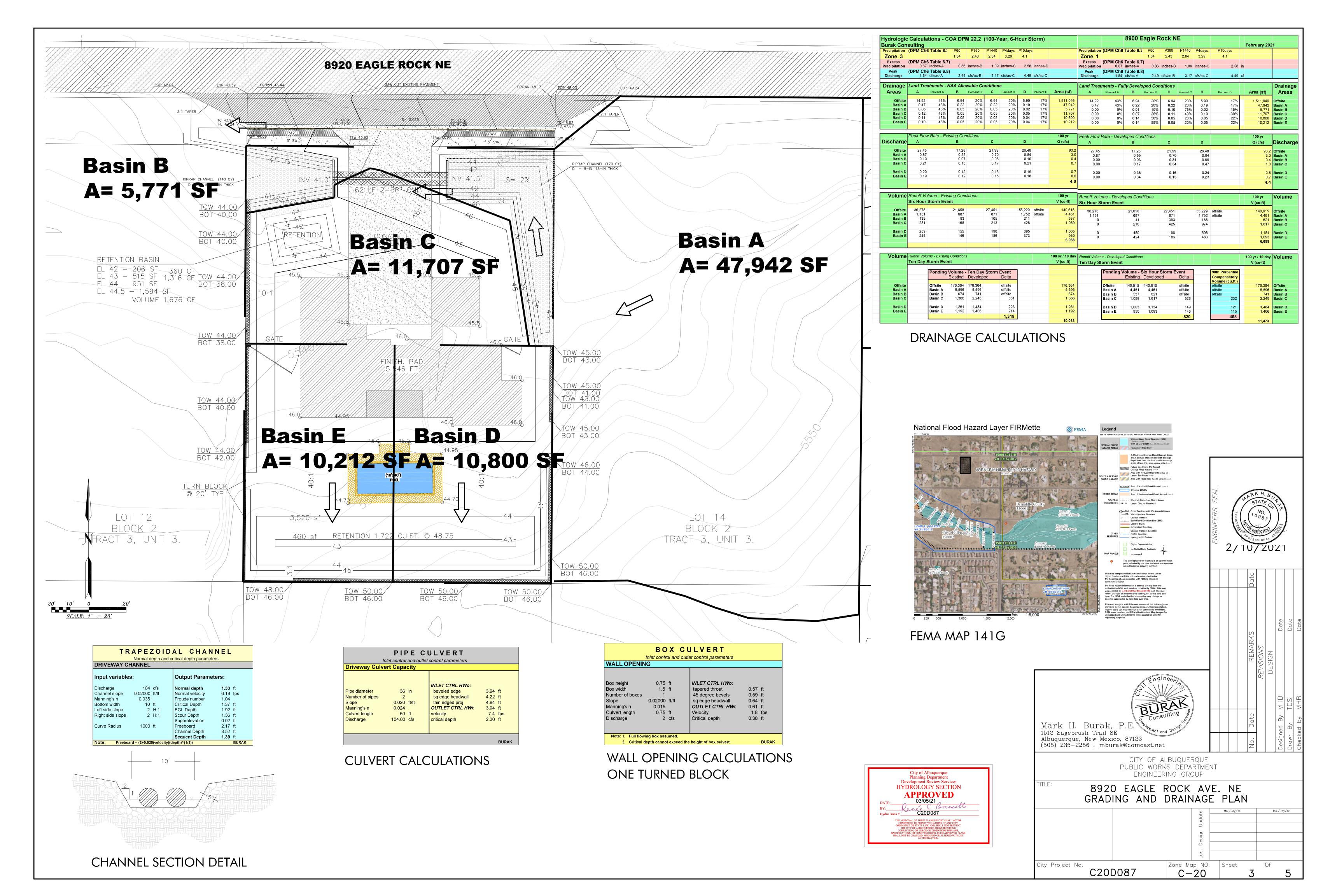
- CURB, GUTTER AND CUT-OFF WALL WILL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE (PCC).
- FOR STANDARD AND MEDIAN C & G ADJACENT TO ASPHALT CONCRETE (AC) PAVEMENT, PROVIDE CONTRACTION JTS. AT 12" MAX SPACING, 1/2" EXP JOINTS AT CURB RETURNS & AT A MAXIMUM SPACING OF 120' BETWEEN CURB RETURNS & EACH SIDE OF SEPARATELY CONSTRUCTED DRIVEWAYS. CONTRACTION JOINTS SHALL BE EITHER SAWED OR TOOLED A MINIMUM OF 1" DEEP AT FINISHED FACES.
- FOR ALL OTHER C & G CUT-OFF WALL PROVIDE CONTRACTION JOINTS AT 10' MAX SPACING, 1/2" EXP. JTS. AT CURB RETURNS & AT A MAXIMUM SPACING OF 100' BETWEEN CURB RETURNS & EACH SIDE OF SEPARATELY CONSTRUCTED DRIVEWAYS. CONTRACTION JOINTS SHALL BE EITHER SAWED OR TOOLED A MINIMUM OF 1" DEEP AT ALL FINISHED FACES. REINFORCEMENT SHALL NOT BE USED IN CUT-OFF WALLS.
- FOR C & G CONSTRUCTED WITH PCC PAVEMENT, CONTRACTION JOINTS AND EXPANSION JOINTS SHALL BE THE SAME AS THE PAVEMENT JOINTS.
- ALL EDGES SHALL BE EDGED WITH A 3/8"
- STANDARD C & G SHALL BE USED FOR NEW CONSTRUCTION UNLESS OTHERWISE
- REMOVE & REPLACE PAVEMENT 1" WIDE ADJACENT TO LIP OF GUTTER WHEN CONSTRUCTING C & G ADJACENT TO EXISTING AC PAVEMENT.
- 1/4" ISOLATION JOINT SHALL BE PLACED BETWEEN SIDEWALK AND C & G WHEN
- ADA = AMERICANS WITH DISABILITY ACT.

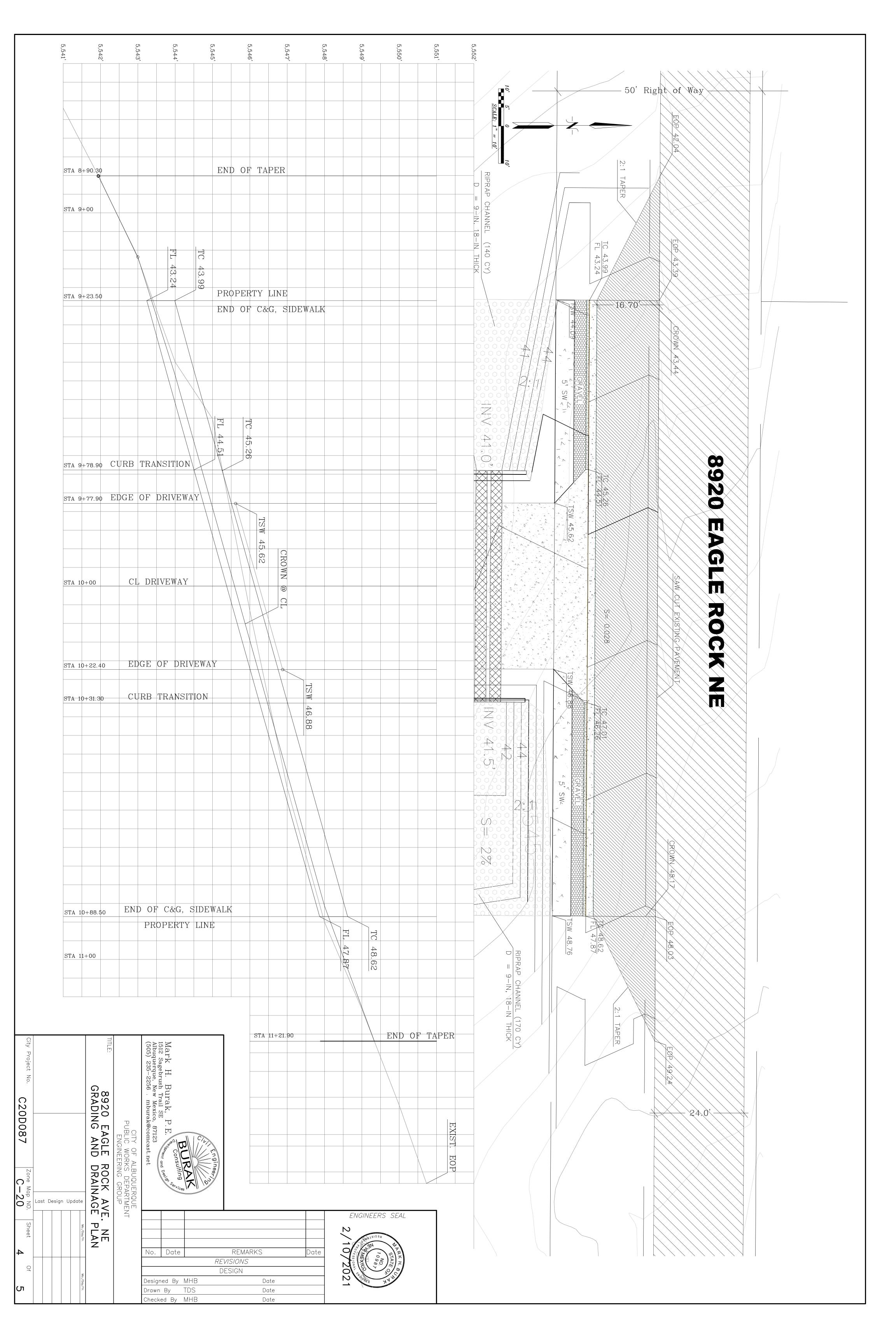


PUBLIC WORKS DEPARTMENT ENGINEERING GROUP

8920	EA(GLE	ROCK	AV	E. NE
GRADI	NG A	AND	DRAIN	IAGE	E PLAN
			I	4)	Mo./Dav/Yr.

Project No. Amo./Day/Yr. Mo./Day/Yr. Mo./Day/Yr.	,	C20D087	C-	20		2	5
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Mo./Day/Yr. Mo./Day/Yr.				.ppd			
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TEMPORARY TRAFFIC CONTROL GENERAL NOTES 20.

- CONTRACTOR MUST OBTAIN FROM CONSTRUCTION COORDINATION AN EXCAVATION/BARRICADING PERMIT AT LEAST TWO WORKING DAYS BEFORE ENGAGING IN ANY CONSTRUCTION, MAINTENANCE OR REPAIR WORK IN ANY OF THE CITY OF ALBUQUERQUE'S RIGHTS—OF—WAY. EMERGENCY WORK THAT WOULD PRESERVE LIFE OR PROPERTY IS EXCLUDED WITH THE UNDERSTANDING THAT A PERMIT SHALL BE OBTAINED WITHIN 24 HOURS.
- 5 CONTRACTOR SHALL AT THE TIME OF PERMIT REQUEST, SUBMIT FOR APPROVAL BY CONSTRUCTION COORDINATION, A TRAFFIC CONTROL PLAN DETAILING ALL EXISTING TOPOGRAPHY SUCH AS LANE WIDTHS, DRIVEWAYS, AND BUSINESS/RESIDENTIAL ACCESSES. THE TRAFFIC CONTROL PLAN SHALL INCLUDE ALL PHASES OF WORK AND SCHEDULES INVOLVED IN THE CONSTRUCTION PROJECT. ANY SEPARATE PHASES OF A CONSTRUCTION PROJECT SHALL BE GIVEN AN INDIVIDUAL PERMIT EACH. BLANKET PERMITS WILL NOT BE ISSUED.
- THESE TYPICAL TRAFFIC CONTROL PLANS DO NOT REFLECT THE EXISTING TOPOGRAPHY SUCH AS DRIVEWAYS, LANE WIDTHS, AND BUSINESS/RESIDENTIAL ACCESSES. EVERY LOCATION THAT REQUIRES CONSTRUCTION TRAFFIC CONTROL SHALL HAVE A DETAILED TRAFFIC CONTROL PLAN SHOWING ALL EXISTING TOPOGRAPHY.
- CONSTRUCTION SHALL NOT BEGIN UNLESS A TRAFFIC CONTROL PLAN HAS BEEN APPROVED AND VERIFIED BY CONSTRUCTION COORDINATION.
- $\dot{\Omega}$ CONSTRUCTION COORDINATION SHALL BE NOTIFIED 48 HOURS PRIOR TO ANY TRAFFIC CONTROL CHANGES NEEDED BY CONTRACTOR THAT WERE NOT PREVIOUSLY APPROVED. THESE TRAFFIC CONTROL CHANGES SHALL BE REQUESTED IN WRITING ACCOMPANIED WITH A TRAFFIC CONTROL PLAN REFLECTING SUCH CHANGES.

25.

- <u></u>6 ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL COMPLY TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL, SERVICE AND MAINTAIN ALL TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHALL NOT BE REMOVED OR ALTERED IN ANY WAY WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION, PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- THE CONSTRUCTION TRAFFIC CONTROL INITIAL SETUP SHALL BE BY AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA), OR EQUIVALENT, CERTIFIED WORK SITE TRAFFIC SUPERVISOR. THE MAINTENANCE AND SERVICING SHALL ALSO BE DONE BY AN ATSSA CERTIFIED WORK SITE TRAFFIC SUPERVISOR OR EQUIVALENT.
- CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND SERVICE ALL TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK THROUGHOUT LENGTH OF PROJECT. CONTRACTOR IS RESPONSIBLE THAT ALL TRAFFIC CONTROL DEVICES COMPLY WITH THE MUTCD, LATEST EDITION.
- ALL ADVANCE WARNING SIGNS SHALL BE DOUBLE INDICATED WHENEVER THERE ARE MULTI—LANE TRAFFIC IN ANY ONE GIVEN DIRECTION AND THERE IS SUFFICIENT MEDIAN SPACE.
- 10. ALL BARRICADES IN ALL TAPERS AND TANGENTS SHALL BE PLACED APART, A DISTANCE MEASURED IN FEET, EQUAL TO THAT OF THE POSTED SPEED LIMIT. NO EXCEPTIONS UNLESS APPROVED BY CONSTRUCTION COORDINATION PER MUTCD SECTION 6A-4.

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- 12. CONTRACTOR IS RESPONSIBLE TO PROVIDE CONSTRUCTION COORDINATION, A WEEKLY LOG OF DAILY INSPECTIONS OF BARRICADE AND MAINTENANCE SCHEDULES ON PROJECTS THAT ARE OVER ONE WEEK DURATION. ALL WORK IN ARTERIAL ROADWAYS SHALL BE ON A CONTINUOUS 24 HOUR PER DAY BASIS UNTIL COMPLETED.
- 13. EQUIPMENT OR MATERIALS SHALL NOT BE STORED WITHIN 15 FEET OF A TRAVELED TRAFFIC LANE DURING NON—WORKING HOURS WITHOUT THE APPROVAL OF CONSTRUCTION COORDINATION.
- <u>1</u> 4 CONTRACTOR SHALL PROVIDE AND MAINTAIN A SAFE AND ADEQUATE MEANS OF CHANNELIZING PEDESTRIAN AND BICYCLE TRAFFIC AROUND AND THROUGH THE CONSTRUCTION AREA.
- 15. CONTRACTOR IS RESPONSIBLE FOR OBLITERATION OF ANY CONFLICTING STRIPING AND RESPONSIBLE FOR ALL TEMPORARY STRIPING.

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SIGN FACE DETAILS

TYPE I BARRICADE COLLAPSIBLE
NOTE: STRIPES SHALL SLOPE DOWNWARD
TOWARD DIRECTION OF TRAVEL

TRAFFIC CONTROL ELEMENTS

3' MIN.

- 17. <u>1</u>6. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FACILITIES, BUSINESSES AND/OR RESIDENCES AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE ACCESS SIGNS FOR BUSINESSES LOCATED WITHIN THE CONSTRUCTION AREA UNDER THE SUPERVISION OF CONSTRUCTION COORDINATION. EACH ACCESS SIGN SHALL HAVE 5 INCH, WHITE OPAQUE LETTERING ON BLUE REFLECTORIZED BACKGROUND. ACCESS SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE BID AND NOT PART OF THE CONTRACT UNLESS OTHERWISE STATED. NO MORE THAN 3 BUSINESSES SHALL BE LISTED ON A ACCESS SIGN. SHOPPING CENTERS AND MALLS SHALL BE LISTED AS SUCH.

30" SPEED 14

24"
KEEP
RIGHT

CLOSED AHEAD 0-5(L)

- ALL ADVANCE WARNING SIGNS SHALL MEET THE MINIMUM REFLECTIVE INTENSITY REQUIREMENTS SET FORTH BY THE CITY OF ALBUQUERQUE. CONSTRUCTION COORDINATION SHALL DETERMINE ALL REQUIREMENTS AND APPROVE OR DISAPPROVE ANY ADVANCE WARNING SIGN PER SECTION 6A-4 OF THE MUTCD, LATEST EDITION.
- 48 HOURS PRIOR TO OCCUPYING OR CLOSING A RIGHT—OF—WAY, CONTRACTOR SHALL NOTIFY: POLICE, FIRE DEPARTMENT, SCHOOLS, HOSPITALS, TRANSIT AUTHORITY, BUSINESSES AND/OR RESIDENTS THAT WILL BE AFFECTED BY THE CONSTRUCTION.

PREPARED STOP

ACCESS TO SPECIAL SIGN 50

W11-1S

12" SHARE THE ROAD SP-STR

ALL CONSTRUCTION WARNING SIGNS
SHALL HAVE A BLACK LEGEND ON A DETOUR DETOUR
ORANGE BACKGROUND

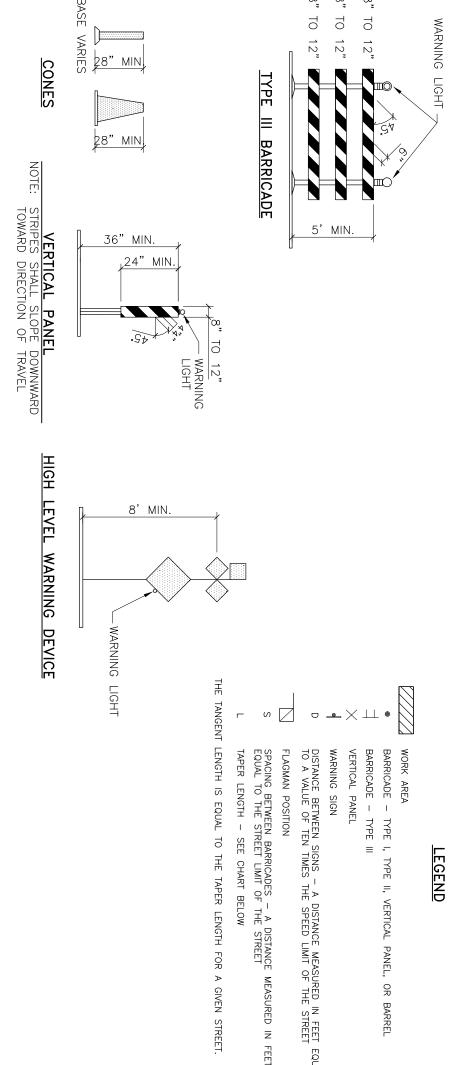
M4-10(R)

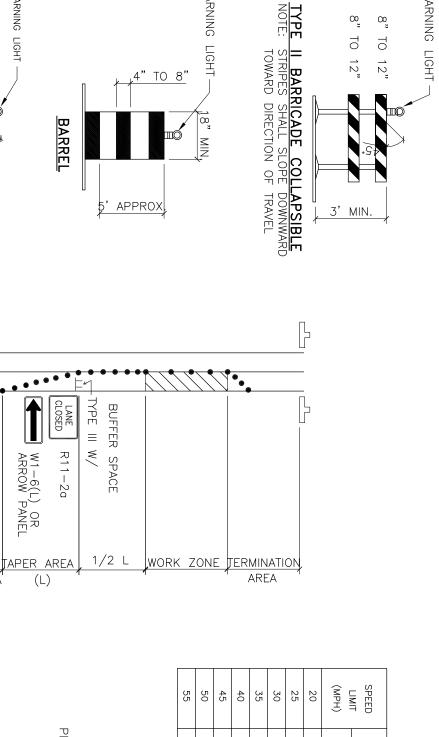
M4-10(L)

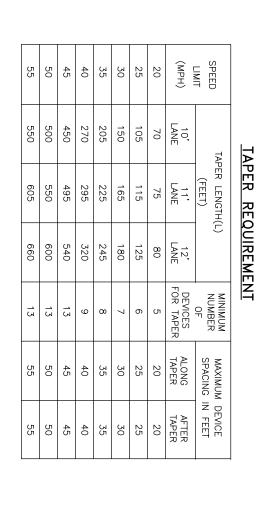
21. EXCAVATIONS SHALL BE PLATED, TEMPORARILY PATCHED OR RESURFACED PRIOR TO OPENING OF TRAFFIC. A MINIMUM OF 11 FEET SHALL BE PROVIDED FOR TRAFFIC IN ANY GIVEN DIRECTION. CONTRACTOR IS RESPONSIBLE FOR ANY WORK INVOLVED IN SATISFYING THESE REQUIREMENTS. ANY FIELD ADJUSTMENTS COORDINATION.

APPROVED BY CONSTRUCTION

- CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE FOLLOWING:
- THE CITY OF ALBUQUERQUE STANDARDS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. TRAFFIC CODE, LATEST EDITION.
- SECTIONS 19, 1200, AND 2800 OF THE CITY OF ALBUQUERQUE'S STANDARD SPECIFICATIONS FOR PUBLIC WORK CONSTRUCTION, AS WELL AS OTHER SECTIONS.
- FAILURE TO COMPLY WITH ANY OF THE ABOVE MENTIONED WILL BE ADEQUATE CAUSE TO CEASE ALL WORK ON ANY CONSTRUCTION PROJECT. WORK WILL NOT RESUME UNTIL ALL REQUIREMENTS ARE ADDRESSED AND APPROVED BY CONSTRUCTION COORDINATION.
- ALL TRAFFIC CONTROL DEVICES SHALL BE KEPT IN NEW/CLEAN CONDITION, WASHING OF EQUIPMENT IS INCIDENTAL TO ITS PLACEMENT AND MAINTENANCE.
- TRAFFIC CONTROL STANDARDS APPLY ONLY WHERE THE TEMPORARY TRAFFIC CONTROL PLANS ARE NOT SPECIFIC.
- 26. ADVANCE WARNING SIGNS SHALL BE 36"x36" MIN. WITH SUPER ENGINEERING GRADE SHEETING OR BETTER. MOUNTING HEIGHT AT TOP OF SIGN SHALL BE THE SAME AS FOR A 48" SIGN AS INDICATED IN THE M.U.T.C.D.
- CONTRACTOR SHALL MAINTAIN A GRAFFITI FREE WORK SITE. ALL GRAFFITI SHALL BE PROMPTLY REMOVED FROM ALL EQUIPMENT, BOTH PERMANENT AND TEMPORARY.







	SPEED MILES PER HOUR 0-20 25-30 30-35 40-45 50-60	RECOMA ADVANC
TAPER CRITERIA	MINIMUM DISTANCE IN FEET BETWEEN SIGNS FROM LA SIGN TO TI 10 X SPEED LIMIT 10 X SPEET	RECOMMENDED SIGN SPACING(D) FOR ADVANCE WARNING SIGN SERIES
	ROM LAST SIGN TO TAPER 10 X SPEED LIMIT	ACING(D) FOR N SERIES

TAPER LENGTH TAPER COMPUTATION L MINIMUM

1/2 L MINIMUM

1/2 L MINIMUM

100 FEET MAXIMUM

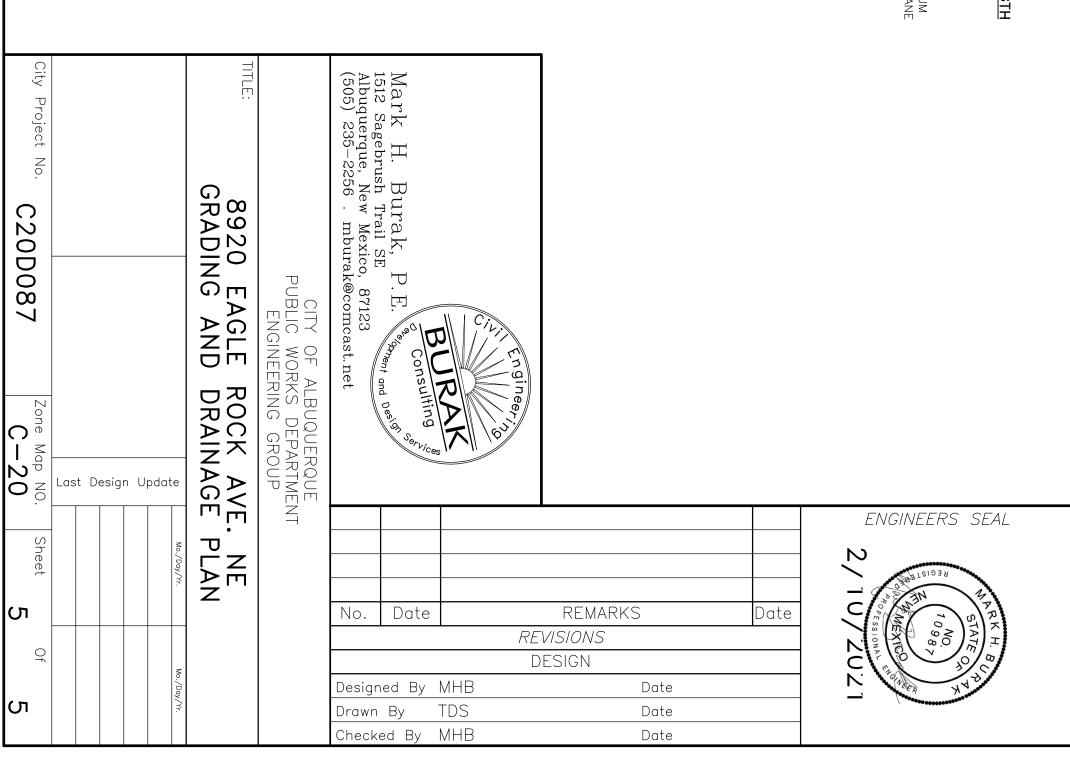
100 FEET PER LANE TAPER LENGTH



OUTLET W14-2

₩1-4bR

W1-4bL



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