

DRAINAGE PLAN

I. INTRODUCTION AND EXECUTIVE SUMMARY

THIS PROJECT, LOCATED ALONG THE NORTH I-25/JEFFERSON CORRIDOR REPRESENTS MODIFICATIONS TO AN EXISTING SITE WITHIN AN INFILL AREA. THE SITE, LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF JEFFERSON STREET NE AND ELLISON STREET NE IS CURRENTLY DEVELOPED AS A HOSPITAL SITE WITH RELATED USES. IT IS PROPOSED TO ADD SEVERAL BUILDING ADDITIONS AND MODEST PARKING LOT IMPROVEMENTS. UPGRADES TO THE EXISTING LANDSCAPING ARE ALSO PROPOSED. THE DRAINAGE CONCEPT FOR THIS SITE WILL BE THE CONTINUED FREE DISCHARGE TO THE PINO ARROYO, A CITY OWNED AND OPERATED CONCRETE DRAINAGE CHANNEL.

THIS SUBMITTAL IS MADE IN SUPPORT OF BUILDING PERMIT APPROVAL.

II. PROJECT DESCRIPTION

AS SHOWN BY THE VICINITY MAP, THE SITE IS LOCATED AT THE SOUTHEAST CORNER OF JEFFERSON STREET NE AND ELLISON STREET NE. THE CURRENT LEGAL DESCRIPTION IS TRACTS 4A AND 4B1, INTERSTATE INDUSTRIAL TRACT, UNIT 5. AS INDICATED BY PANEL 137 OF 825 OF THE NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAPS PUBLISHED BY FEMA FOR BERNALILLO COUNTY, NEW MEXICO, NOVEMBER 19, 2003, THIS SITE DOES NOT LIE WITHIN A DESIGNATED FLOOD HAZARD ZONE. A DESIGNATED FLOOD HAZARD ZONE IS IDENTIFIED TO THE SOUTH OF THE SITE. THE PUBLIC STORM DRAIN PINO ARROYO MAKES UP THE LIMITS OF THE FLOOD HAZARD ZONE. APPROXIMATELY TWO THIRDS OF THE EXISTING SITE SURFACE DRAINS NORTH TO SOUTH, DISCHARGING DIRECTLY INTO PINO ARROYO VIA CURB OPENINGS ALONG THE SOUTH PARKING LOT CURB. THE REMAINING WESTERN THIRD OF THE SITE DRAINS EAST TO WEST INTO JEFFERSON STREET NE, WHERE IT COLLECTS INTO AN EXISTING PUBLIC STORM DRAIN SYSTEM AND ULTIMATELY DISCHARGES INTO PINO ARROYO. THE CONSTRUCTION PROPOSED FOR THIS SITE WILL NOT ADVERSELY IMPACT DOWNSTREAM FLOW OR DOWNSTREAM PROPERTIES AND WILL NOT ALTER THE EXISTING APPROVED DRAINAGE PATTERNS.

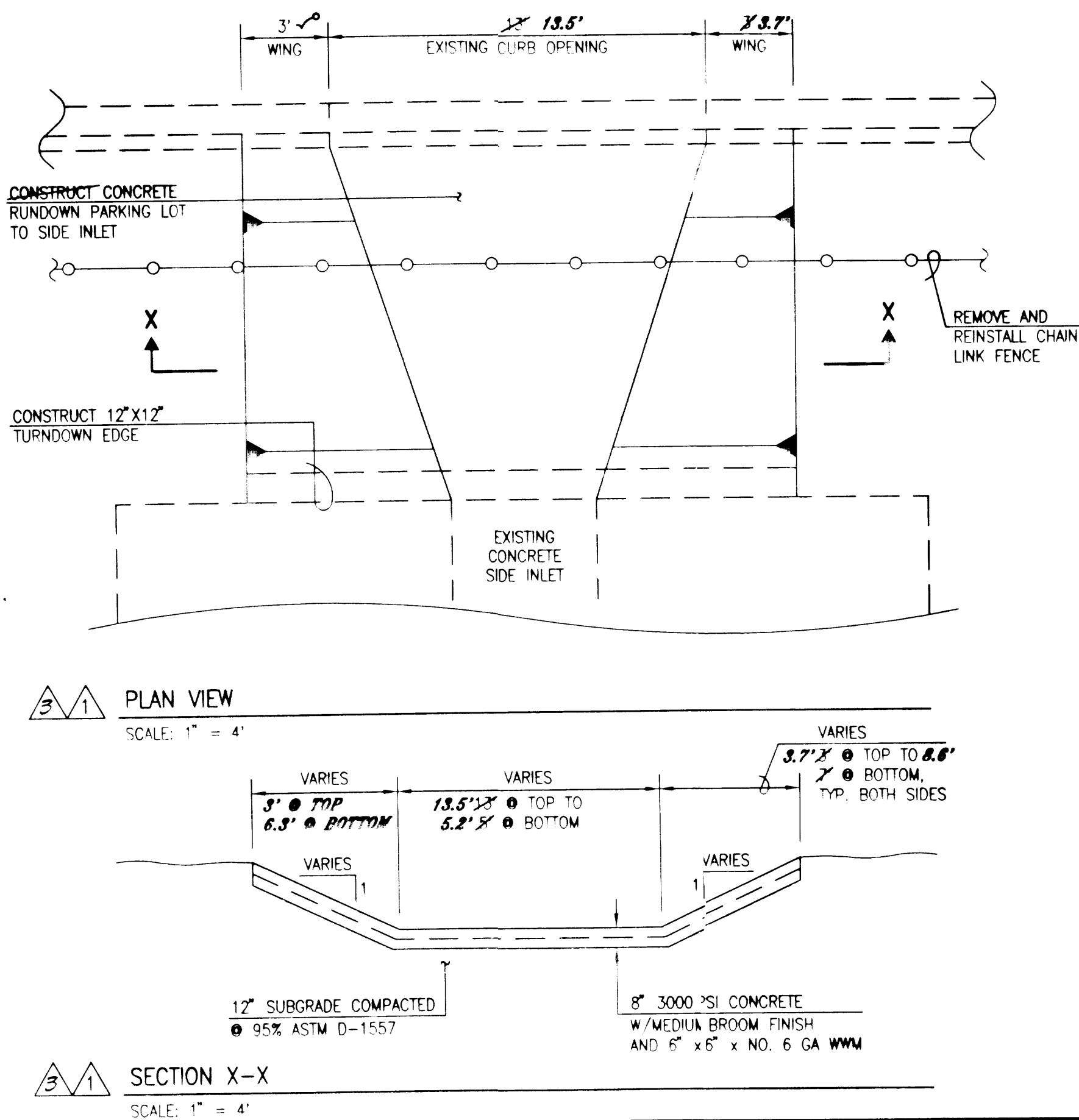
III. BACKGROUND DOCUMENTS & RESEARCH

THE FOLLOWING ITEMS WERE REVIEWED IN THE PREPARATION OF THIS SUBMITTAL:

- A BOUNDARY AND TOPOGRAPHIC SURVEY OF THE EXISTING SITE PREPARED BY HIGH MESA CONSULTING GROUP, DATED 10/07/2008. THE SUBJECT SURVEY SHOWS THE EXISTING SITE IMPROVEMENTS.
- CITY AS-BUILT PLANS OF JEFFERSON STREET NE AND ELLISON STREET NE RECEIVED FROM THE CITY RECORDS. THE AS-BUILT PLANS SHOW THE EXISTING PUBLIC PAVING AND INFRASTRUCTURE IMPROVEMENTS WITHIN THESE PUBLIC ROADWAYS.
- GRADING AND DRAINAGE PLAN PREPARED BY ESPEY, HUSTON & ASSOCIATES, INC. DATED 11/13/87 (D17/D61A). THIS APPROVED DRAINAGE PLAN PERMITS FREE DISCHARGE INTO JEFFERSON STREET NE AND THE PINO ARROYO.
- GRADING AND DRAINAGE PLAN FOR THE HEALTHSOUTH SATELLITE PARKING LOT PREPARED BY THIS OFFICE, NMPE 8547, DATED 12-29-2005 AND REVISED 01-31-2006. THIS 2006 SUBMITTAL ADDRESSED THE DESIGN AND DEVELOPMENT OF A STAFF PARKING LOT AT THE NORTHWEST CORNER OF TRACT 4B1. THE DEVELOPED RUNOFF FROM TRACT 4B1 DISCHARGES DIRECTLY TO ELLISON DRIVE NE VIA SIDEWALK CULVERT. THE 2006 DRAINAGE SUBMITTAL REPRESENTS THE DRAINAGE CONDITIONS OF TRACT 4B1 THAT LIES IMMEDIATELY ADJACENT TO THE ORIGINAL HOSPITAL SITE, TRACT 4A.
- SITE DEVELOPMENT PLAN FOR BUILDING PERMIT APPROVAL PREPARED BY BOULDER ASSOCIATES AND HIGH MESA CONSULTING GROUP, DATED NOVEMBER 6, 2008, REVISED DECEMBER 1, 2008 AND SUBMITTED TO EPC FOR REVIEW AND APPROVAL. THE PLAN WAS APPROVED FOR DRB ACTION ON DECEMBER 18, 2008 BY THE CITY OF ALBUQUERQUE ENVIRONMENTAL PLANNING COMMISSION.

RECORD DRAWING LEGEND

| CONSTRUCT | RECORD INFORMATION (VERIFIED BY ENGINEER) |
|-----------|------------------------------------------------------------|
| ✓ | AS-DESIGNED (VERIFIED BY ENGINEER) |
| 3' 42" | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| + 25.2 | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| 28.9842 | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| ✓ | AS-CONSTRUCTED = AS-DESIGNED (VERIFIED BY AS-BUILT SURVEY) |
| 31.8 | RECORD INFORMATION FROM AS-BUILT SURVEY |
| + 26.9 | RECORD INFORMATION FROM AS-BUILT SURVEY |
| 31.25.22 | RECORD INFORMATION FROM AS-BUILT SURVEY |



IV. EXISTING CONDITIONS

AT PRESENT, TRACT 4A IS DEVELOPED AS A HOSPITAL SITE WITH RELATED USES. THE SITE IS CHARACTERIZED BY TWO DRAINAGE BASINS. BASIN A DISCHARGES TO THE ADJACENT CITY STREETS. FROM THIS POINT, THE RUNOFF IS CARRIED WITHIN THE IMPROVED STREETS TO ENTER THE EXISTING PUBLIC STORM DRAIN SYSTEM THAT OUTFALLS TO THE PINO ARROYO. BASIN B DRAINS INTERNALLY AND DISCHARGES DIRECTLY TO THE PINO ARROYO VIA AN EXISTING SIDE INLET SITUATED ON THE NORTH EDGE OF THE CONCRETE CHANNEL.

TRACT 4B1 IS ONLY PARTLY DEVELOPED AS A SATELLITE PARKING LOT FOR HOSPITAL STAFF. REFER TO THE 2006 DRAINAGE SUBMITTAL REFERENCED ABOVE FOR THE EXISTING CONDITIONS OF TRACT 4B1. THE DEVELOPED PORTIONS OF TRACT 4B1 FREE DISCHARGE TO ELLISON DRIVE NE VIA A SIDEWALK CULVERT SITUATED AT THE NORTHWEST CORNER OF THE STAFF PARKING LOT.

OFFSITE FLOWS DO NOT ENTER EITHER SITE. THE ADJACENT CITY STREETS LIE TOPOGRAPHICALLY LOWER, PLUS ARE FULLY IMPROVED WITH CURB AND GUTTER AND PUBLIC STORM DRAIN FACILITIES. TRACT 4B1 IS BOUNDED ON ITS EAST EDGE BY A PRIVATE STREET THAT INTERCEPTS OFFSITE FLOWS FROM THE EAST AND CONVEYS THOSE FLOWS BOTH NORTH AND SOUTH. THE PINO ARROYO LIES TO THE SOUTH OF THE COLLECTIVE SITE. REVIEW OF FIRM PANEL 137 INDICATES THAT FLOWS ARE CONFINED TO THE CONSTRUCTED CHANNEL AND HENCE DOES NOT GENERATE ANY OFFSITE FLOWS.

V. DEVELOPED CONDITIONS

THE PROPOSED IMPROVEMENTS CONSIST OF SEVERAL BUILDING ADDITIONS TO THE ORIGINAL HOSPITAL BUILDING. MODEST PARKING LOT IMPROVEMENTS ARE ALSO PROPOSED ALONG WITH LANDSCAPING UPGRADES. THE NET RESULT OF THE PROPOSED IMPROVEMENTS WILL BE A MINOR INCREASE IN IMPERVIOUS AREA RESULTING IN A MINOR INCREASE IN DEVELOPED RUNOFF. REGARDLESS, THE PROPOSED IMPROVEMENTS WILL NOT ALTER DRAINAGE BASIN BOUNDARIES NOR WILL THE PROPOSED IMPROVEMENTS ALTER EXISTING POINTS OF DISCHARGE FROM THE SITE.

VI. GRADING PLAN

THE GRADING PLAN SHOWS 1.) EXISTING AND PROPOSED GRADES INDICATED BY SPOT ELEVATIONS AND CONTOURS AT 1'-0" INTERVALS, 2.) THE LIMIT AND CHARACTER OF THE EXISTING IMPROVEMENTS, 3.) THE LIMIT AND CHARACTER OF THE PROPOSED IMPROVEMENTS, AND 4.) CONTINUITY BETWEEN EXISTING AND PROPOSED GRADES. AS SHOWN BY THIS PLAN, THE PROPOSED IMPROVEMENTS WILL MAINTAIN EXISTING DRAINAGE PATTERNS AND POINTS OF DISCHARGE.

VII. CALCULATIONS

CALCULATIONS ANALYZING THE EXISTING AND DEVELOPED CONDITIONS FOR THE 100-YEAR, 6-HOUR RAINFALL EVENT HAVE BEEN PREPARED FOR THIS PROJECT. THE PROCEDURE FOR 40 ACRE AND SMALLER BASINS, AS SET FORTH IN THE REVISION OF SECTION 22.2, HYDROLOGY OF THE DEVELOPMENT PROCESS MANUAL, VOLUME 2, DESIGN CRITERIA, DATED JANUARY, 1993, HAS BEEN USED TO QUANTIFY THE PEAK RATE OF DISCHARGE AND VOLUME OF RUNOFF GENERATED. AS SHOWN BY THE RESULTS PRESENTED HEREON, THERE WILL BE A MINIMAL INCREASE IN PEAK DISCHARGE AND RUNOFF VOLUME ASSOCIATED WITH THE PROPOSED DEVELOPMENT ON TRACT 4A. CALCULATIONS FOR TRACT 4B1 HAVE NOT BEEN INCLUDED AS THERE IS NO CHANGE PROPOSED FROM THE 2006 SUBMITTAL REFERENCED ABOVE.

VIII. CONCLUSION

THE CONTINUED FREE DISCHARGE OF RUNOFF FROM THIS SITE TO THE PINO ARROYO IS JUSTIFIED DUE TO THE FOLLOWING FACTORS:

- MODIFICATION TO AN EXISTING SITE WITHIN AN INFILL AREA
- MINIMAL INCREASE IN PEAK DISCHARGE AND VOLUME OF RUNOFF GENERATED
- PROPOSED DEVELOPMENT IS CONSISTENT WITH THE PREVIOUSLY APPROVED 1987 AND 2006 DRAINAGE PLANS REFERENCED ABOVE
- NO ADVERSE IMPACT ON DOWNSTREAM CAPACITY OR DOWNSTREAM PROPERTIES
- THE EXISTING PREVIOUSLY APPROVED DRAINAGE PATTERNS (STATUS Q00) WILL NOT BE ALTERED AND HENCE MAINTAINED

CALCULATIONS

I. SITE CHARACTERISTICS

| | |
|----------------------------------------|------------|
| A. PRECIPITATION ZONE= | 2 |
| B. $P_{100/6 \text{ hr.}} = P_{300} =$ | 2.35 |
| $P_{100/6 \text{ hr.}} =$ | 3.95 |
| C. TOTAL PROJECT AREA (A_T) = | 253,750 SF |
| | 5.83 AC |

D. LAND TREATMENTS

| 1. EXISTING LAND TREATMENT | TREATMENT | AREA (SF/AC) | % |
|-----------------------------|-----------|----------------|----|
| B | | 73,920 / 1.70 | 29 |
| C | | 10,600 / 0.24 | 4 |
| D | | 170,320 / 3.91 | 67 |
| 2. DEVELOPED LAND TREATMENT | TREATMENT | AREA (SF/AC) | % |
| B | | 58,350 / 1.34 | 23 |
| D | | 195,400 / 4.49 | 77 |

II. HYDROLOGY

A. EXISTING CONDITION

- VOLUME
 $E_w = (E_a A_a + E_b A_b + E_c A_c + E_d A_d) / A_T$
 $E_w = (0.78 \times 1.70) + (1.13 \times 0.24) + (2.12 \times 3.91) / 5.85 = 1.69 \text{ IN}$
 $V_{100, 6 \text{ hr.}} = (E_w / 12) A_T = (1.69 / 12) 5.85 = 0.8239 \text{ AC-FT} = 35,890 \text{ CF}$
- PEAK DISCHARGE
 $Q_p = Q_m A_a + Q_m A_b + Q_m A_c + Q_m A_d$
 $Q_p = Q_{100} = (2.28 \times 1.70) + (3.14 \times 0.24) + (4.70 \times 3.91) = 23.0 \text{ CFS}$

B. DEVELOPED CONDITION

- VOLUME
 $E_w = (E_a A_a + E_b A_b + E_c A_c + E_d A_d) / A_T$
 $E_w = (0.78 \times 1.34) + (2.12 \times 4.49) / 5.83 = 1.81 \text{ IN}$
 $V_{100, 6 \text{ hr.}} = (E_w / 12) A_T = (1.81 / 12) 5.83 = 0.8794 \text{ AC-FT} = 38,300 \text{ CF}$
- PEAK DISCHARGE
 $Q_p = Q_m A_a + Q_m A_b + Q_m A_c + Q_m A_d$
 $Q_p = Q_{100} = (2.28 \times 1.34) + (4.70 \times 4.49) = 24.1 \text{ CFS}$

C. COMPARISON

- VOLUME
 $\Delta V_{100, 6 \text{ hr.}} = 38,300 - 35,890 = 2,410 \text{ CF}$ (INCREASE)
- PEAK DISCHARGE
 $\Delta V_{100} = 24.1 - 23.0 = 1.1 \text{ CFS}$ (INCREASE)

SO #19 SIGNOFF BLOCK

| APPROVALS | NAME | DATE |
|-------------------------|------|------|
| HYDROLOGY | | |
| SIDEWALK INSPECTOR | | |
| STORM DRAIN MAINTENANCE | | |

EROSION CONTROL MEASURES:

- THE CONTRACTOR SHALL ENSURE THAT NO SOIL ERODES FROM THE SITE INTO PUBLIC RIGHT-OF-WAY OR ONTO PRIVATE PROPERTY.
- THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET.
- WHEN APPLICABLE, CONTRACTOR SHALL SECURE "TOPSOIL DISTURBANCE PERMIT" FROM THE CITY AND/OR FILE A NOTICE OF INTENT (N.O.I.) WITH THE EPA PRIOR TO BEGINNING CONSTRUCTION.
- UNLESS FINAL STABILIZATION IS OTHERWISE PROVIDED FOR, ANY AREAS OF EXCESS DISTURBANCE (TRAFFIC ACCESS, STORAGE YARD, EXCAVATED MATERIAL, ETC.) SHALL BE RE-SEEDING ACCORDING TO C.O.A. SPECIFICATION 1012 "NATIVE GRASS SEEDING". THIS WILL BE CONSIDERED INCIDENTAL TO CONSTRUCTION, THEREFORE, NO SEPARATE PAYMENT WILL BE MADE.

ENGINEER'S DRAINAGE CERTIFICATION FOR TEMPORARY C.O.

I, JEFFREY G. MORTENSEN, NMPE 8547, OF THE FIRM HIGH MESA CONSULTING GROUP, HEREBY CERTIFY THAT THIS PROJECT HAS BEEN GRADED AND DRAINED IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED 03-25-2009 AND REVISED 04-06-2009 WITH EXCEPTIONS AS NOTED BELOW. THE RECORD INFORMATION EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED FROM THE VERIFICATION SURVEY CONDUCTED UNDER THE DIRECT SUPERVISION OF CHARLES G. CALA, JR., NMPS 11184, OF THE FIRM HIGH MESA CONSULTING GROUP, AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR TEMPORARY CERTIFICATE OF OCCUPANCY.

THE FOLLOWING EXCEPTIONS ARE NOTED AND MUST BE CORRECTED AND RE-CERTIFIED AS A CONDITION FOR PERMANENT CERTIFICATE OF OCCUPANCY:

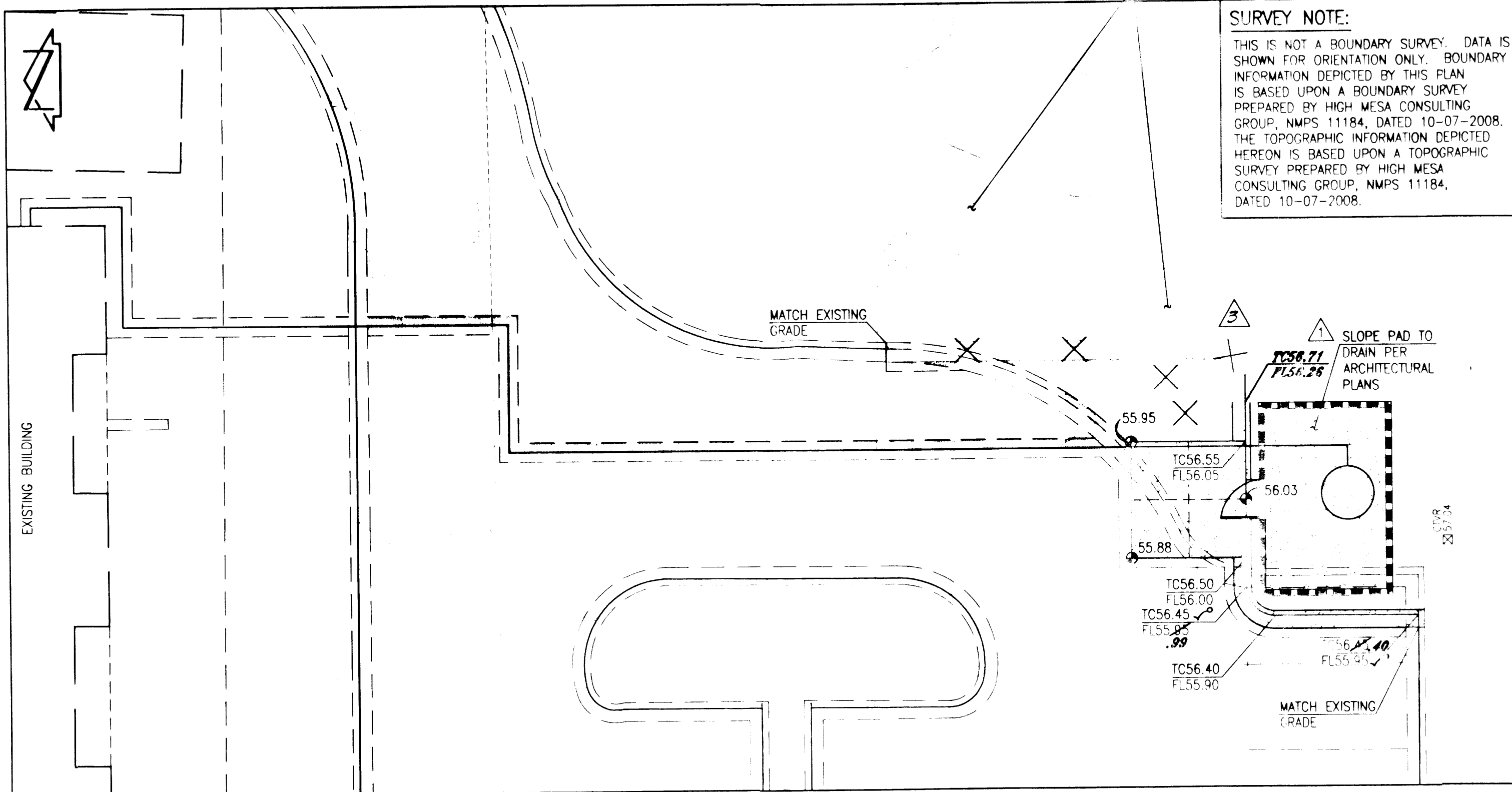
- THE 4" PRIVATE STORM DRAIN AT THE NORTHEAST CORNER OF THE SITE WAS CONSTRUCTED TOO FLAT ($S = 0.002 \pm$) AND MUST BE MODIFIED TO A MINIMUM SLOPE OF $S = 0.0050$ AS DESIGNED AND APPROVED.
- UPON COMPLETION OF THE ABOVE REFERENCED CORRECTION, RE-CERTIFICATION BY THE ENGINEER IS REQUIRED TO SUPPORT ISSUANCE OF A PERMANENT CERTIFICATE OF OCCUPANCY.

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE GRADING AND DRAINAGE ASPECTS OF THIS PROJECT. THIS CERTIFICATION DOES NOT ADDRESS ADA COMPLIANCE WHICH IS BEYOND THE SCOPE OF GRADING AND DRAINAGE. THOSE RELYING ON THIS RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.

JEFFREY G. MORTENSEN
NMPE 8547
12-08-2009
12-14-2009
FOR CERTIFICATION FOR PERMANENT C.O., SEE SHEET C1B
FOR PARKING LOT EXPANSION, SEE SHEET C1S

SURVEY NOTE:

THIS IS NOT A BOUNDARY SURVEY. DATA IS SHOWN FOR ORIENTATION ONLY. BOUNDARY INFORMATION DEPICTED BY THIS PLAN IS BASED UPON A BOUNDARY SURVEY PREPARED BY HIGH MESA CONSULTING GROUP, NMPS 11184, DATED 10-07-2008. THE TOPOGRAPHIC INFORMATION DEPICTED HEREON IS BASED UPON A TOPOGRAPHIC SURVEY PREPARED BY HIGH MESA CONSULTING GROUP, NMPS 11184, DATED 10-07-2008.



DETAIL B
C1.5
SCALE: 1" = 10' - 0"
(BY SEPARATE PERMIT)

PROJECT NUMBER
081570.00

DRAWN BY

DATE

MARCH 6, 2009

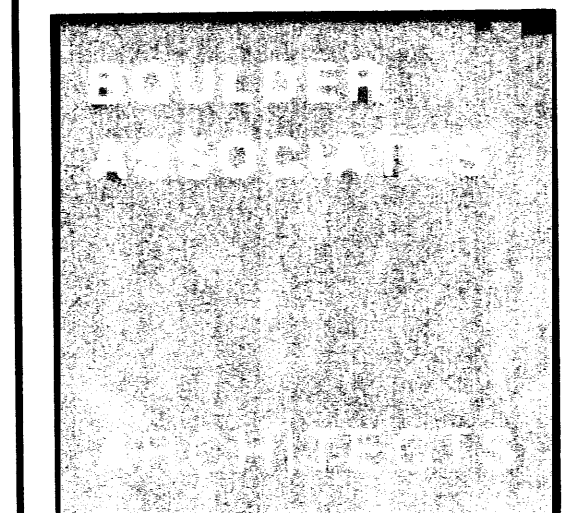
REVISIONS

- HYDROLOGY COMMENTS
- RECORD DRAWING & CERT.
- REVERT FOR FIRM C.O.

HIGH MESA
Consulting Group

6010-B MIDWAY PARK BLVD. NE
ALBUQUERQUE, NEW MEXICO 87109
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2008.039.3



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04-06-2009

JEFFREY G. MORTENSEN
REGISTERED PROFESSIONAL ENGINEER
NEW MEXICO
8547
03-25-2009

PROJECT

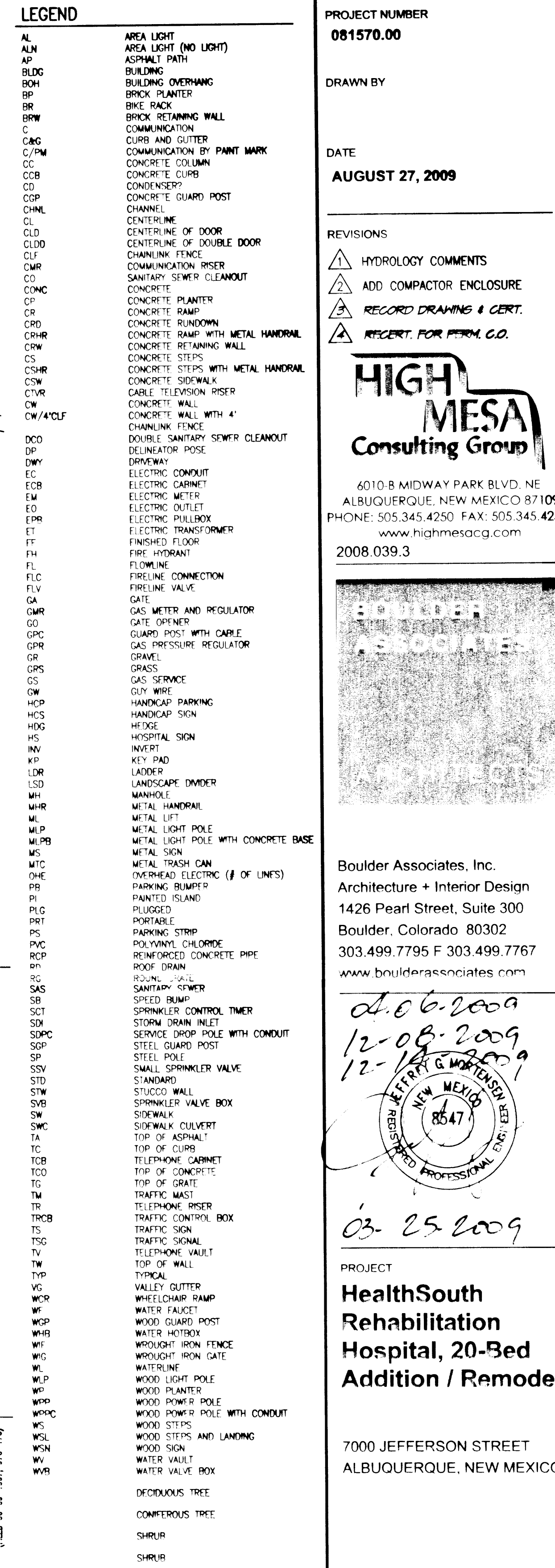
HealthSouth
Rehabilitation
Hospital, 20-Bed
Addition / Remodel

7000 JEFFERSON STREET
ALBUQUERQUE, NEW MEXICO

SHEET TITLE
DRAINAGE PLAN &
CALCULATIONS

SHEET NUMBER

C1.4



SEASEMENTS - OFFSITE

(8) 50'X20' PRIVATE ACCESS, PUBLIC WATERLINE AND PUBLIC SANITARY SEWER EASEMENT GRANTED BY PLAT 92C-50

(9) 10' PNM AND US WEST COMMUNICATIONS EASEMENT GRANTED BY PLAT 97C-178

(10) PUBLIC DRAINAGE, UTILITY AND ACCESS EASEMENT DEPICTED ON PLAT 2001C-260

(11) 30' PUBLIC DRAINAGE AND UTILITY EASEMENT DEPICTED ON PLAT 2001C-260

SEASEMENTS - OFFSITE (CONTINUED)

(12) 7' PUBLIC UTILITY EASEMENT DEPICTED ON PLAT 2001C-260

(13) 7' PUBLIC UTILITY EASEMENT GRANTED BY PLAT 2001C-260

(14) 30' DRAINAGE EASEMENT DEPICTED ON PLAT C10-129

(15) 10' PUBLIC UTILITY EASEMENT DEPICTED ON PLAT C10-129

25.41

PROPOSED SPOT ELEVATION

EXISTING FLOWLINE ELEVATION

PROPOSED CONTOUR

EXISTING CONTOUR

PROPOSED FLOWLINE

ROOF DRAINAGE

EXISTING ROOF DRAINAGE

BASIN BOUNDARY LINE

PUBLIC EASEMENT LINE

PROPOSED CONCRETE

PROPOSED ASPHALT

HIGH POINT

PROJECT NUMBER
081570.00

DRAWN BY

DATE
AUGUST 27, 2009

REVISIONS

△ HYDROLOGY COMMENTS

△ ADD COMPACTOR ENCLOSURE

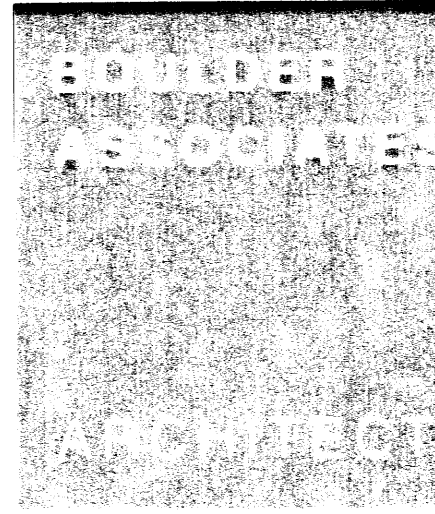
△ RECORD DRAWING & CERT.

△ RECERT. FOR PERM. C.D.

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06-2009
12-08-2009
12-18-2009

JEFFERY G. MONTGOMERY
NEW MEXICO
REGISTERED
PROFESSIONAL ENGINEER
8547

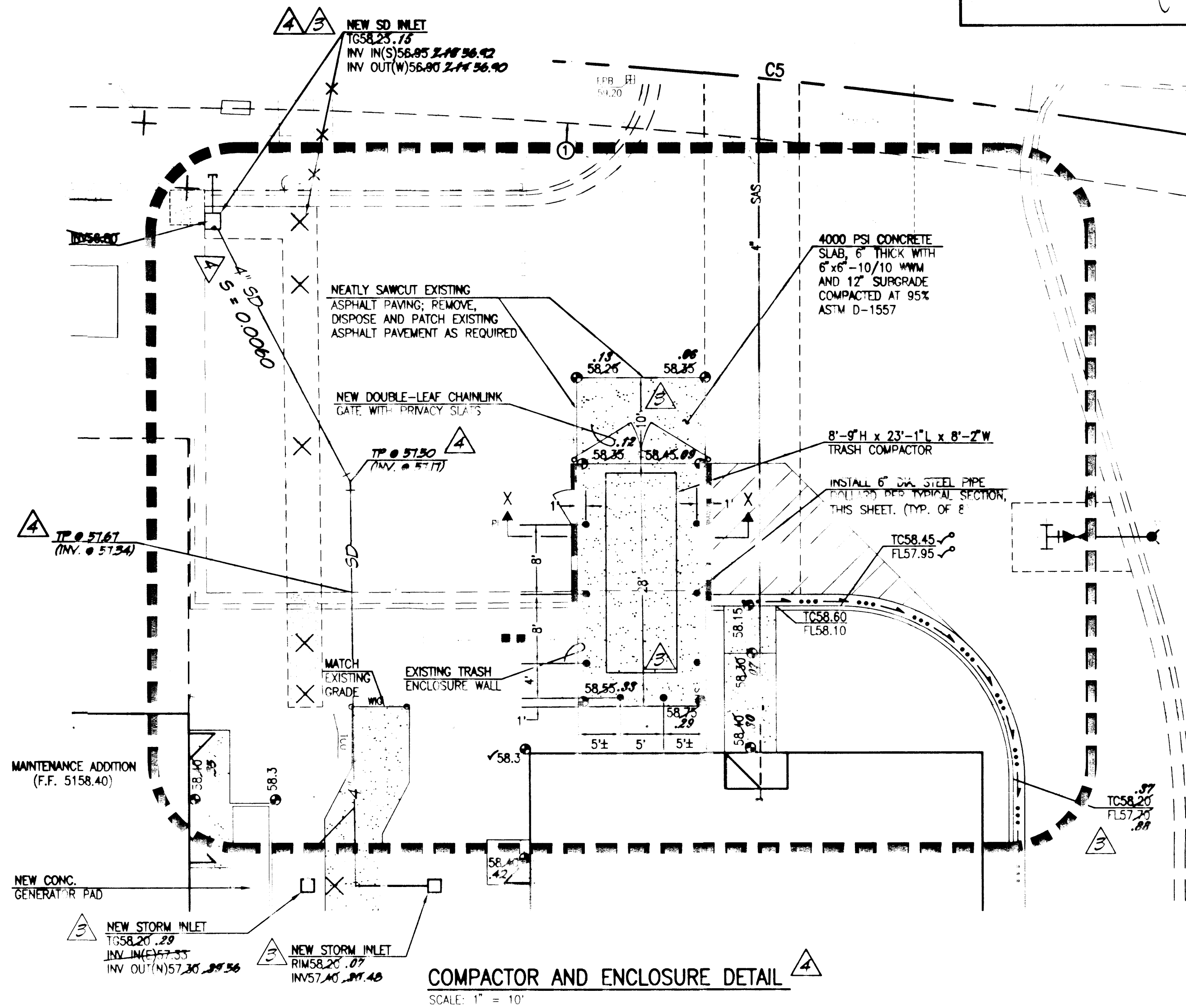
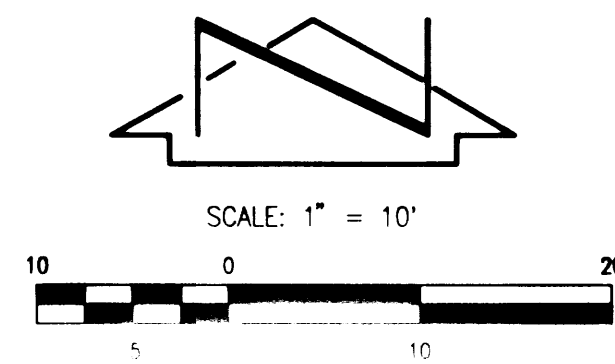
03-25-2009

PROJECT
**HealthSouth
Rehabilitation
Hospital, 20-Bed
Addition / Remode**

7000 JEFFERSON STREET
ALBUQUERQUE, NEW MEXICO

File Path: C:\JEN\2008\34 Plot Date: 12-14-2009
File Name: 80597CED-R4.DWG Plot Time: 1:32 pm

NOTE:
THIS IS NOT A BOUNDARY SURVEY. DATA IS SHOWN FOR ORIENTATION ONLY. BOUNDARY INFORMATION DEPICTED BY THIS PLAN IS BASED UPON A BOUNDARY SURVEY PREPARED BY HIGH MESA CONSULTING GROUP, NMPS 11184, DATED 10-07-2008, JOB NO. 2008.038.1. THE TOPOGRAPHIC INFORMATION DEPICTED HEREON IS BASED UPON A TOPOGRAPHIC SURVEY PREPARED BY HIGH MESA CONSULTING GROUP, NMPS 11184, DATED 10-07-2009, JOB NO. 2008.038.1.

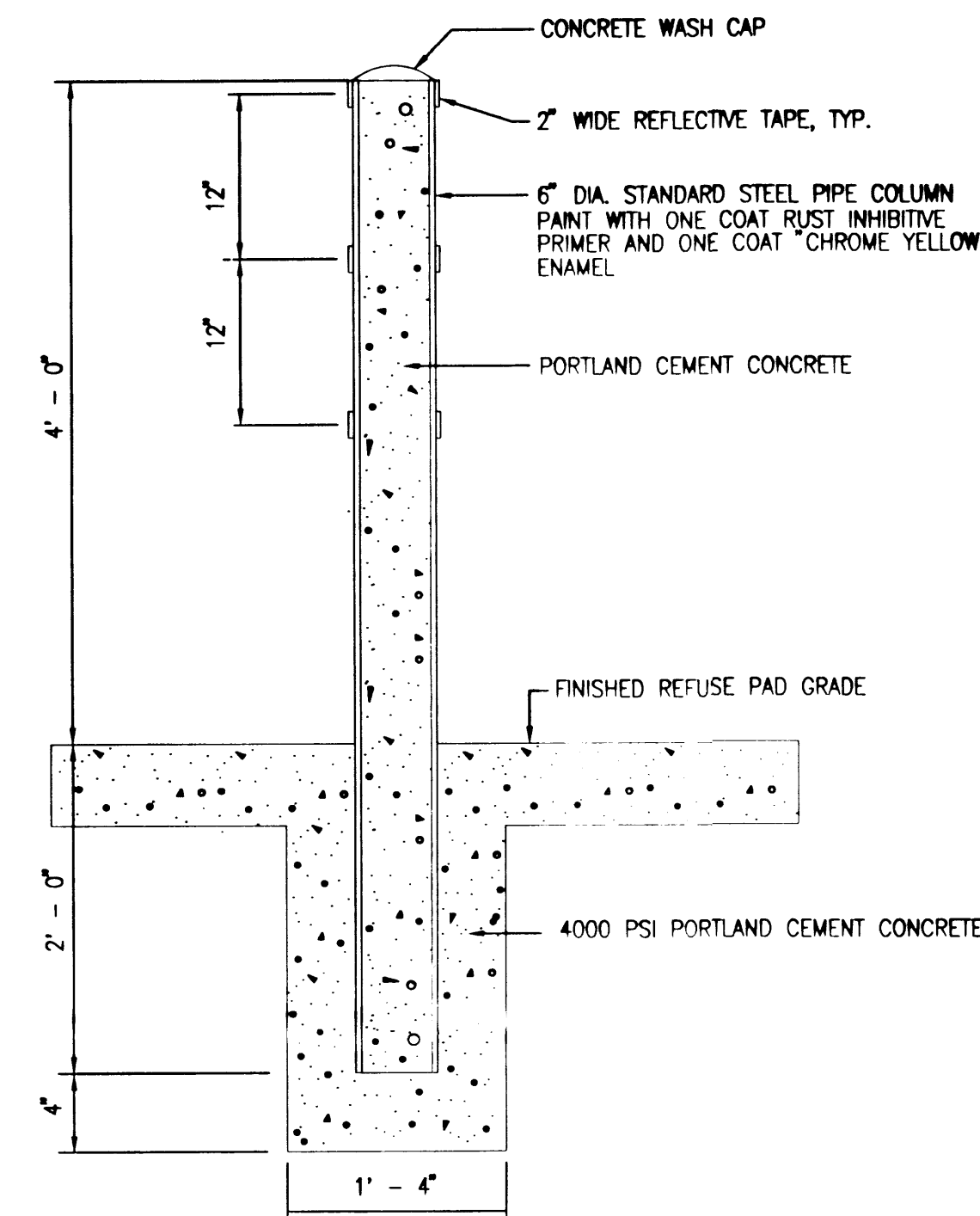


COMPACTOR AND ENCLOSURE DETAIL
SCALE: 1" = 10'

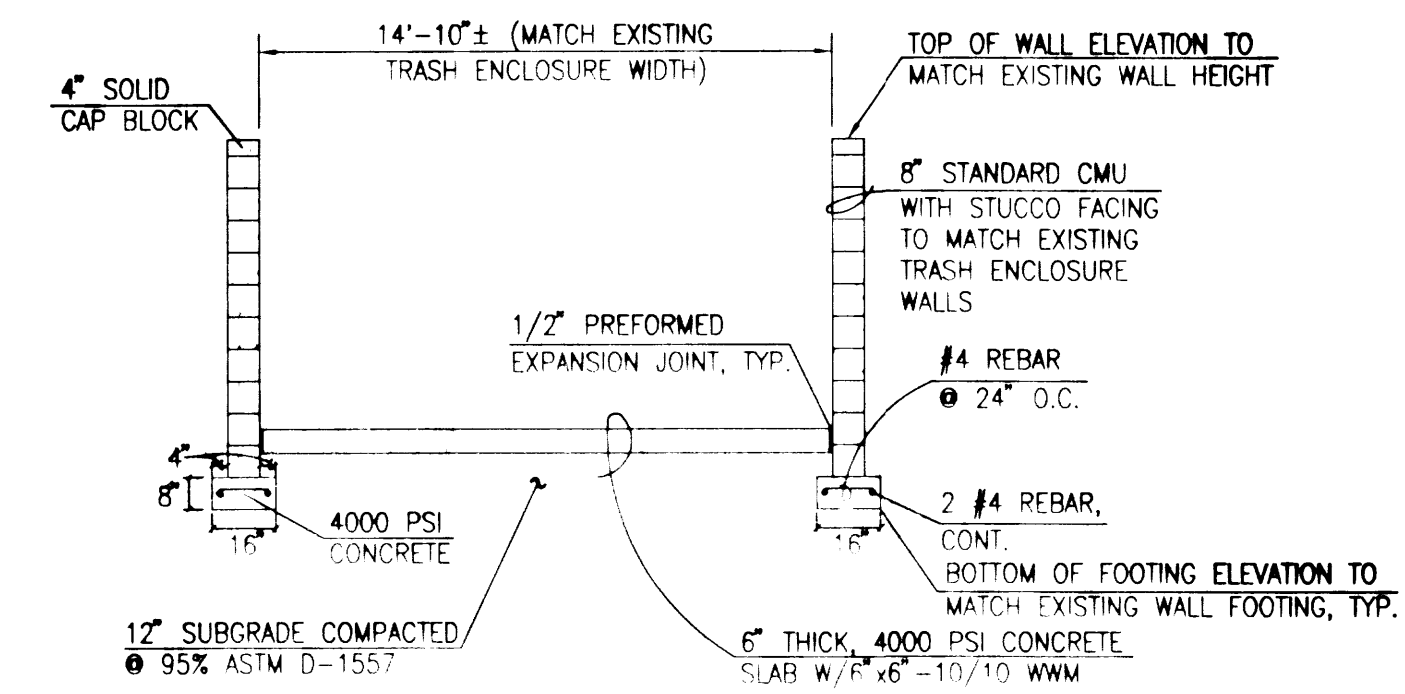
| 4/3 | RECORD DRAWING LEGEND |
|--------------|------------------------------------------------------------|
| CONSTRUCT | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| ✓ | AS-CONSTRUCTED = AS-DESIGNED (VERIFIED BY ENGINEER) |
| 3/5 42" | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| +25.2 | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| 3/5 38.92 | RECORD INFORMATION (VERIFIED BY ENGINEER) |
| ✓ | AS-CONSTRUCTED = AS-DESIGNED (VERIFIED BY AS-BUILT SURVEY) |
| 3/5 31.8 | RECORD INFORMATION FROM AS-BUILT SURVEY |
| +26.9 | RECORD INFORMATION FROM AS-BUILT SURVEY |
| 3/5 31.25.22 | RECORD INFORMATION FROM AS-BUILT SURVEY |

ENGINEER'S DRAINAGE CERTIFICATION FOR PERMANENT C.O.
I, JEFFREY G. MORTENSEN, NMPE 8547, OF THE FIRM HIGH MESA CONSULTING GROUP, HEREBY CERTIFY THAT THIS PROJECT HAS BEEN GRADED AND DRAINED IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED 03-25-2009 AND REVISED 04-06-2009, AND THAT THE CORRECTIONS SET FORTH IN THE ENGINEER'S DRAINAGE CERTIFICATION FOR TEMPORARY C.O. DATED 12-08-2009 HAVE BEEN SATISFACTORILY ADDRESSED. THE RECORD INFORMATION EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED FROM THE VERIFICATION SURVEY CONDUCTED UNDER THE DIRECT SUPERVISION OF CHARLES G. CALA, JR., NMPS 11184, OF THE FIRM HIGH MESA CONSULTING GROUP, ALONG WITH SUPPLEMENTAL DATA OBTAINED BY THE UNDERSIGNED ENGINEER TO VERIFY THE COMPLETION OF THE REQUIRED CORRECTIONS, AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR PERMANENT CERTIFICATE OF OCCUPANCY.
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JEFFREY G. MORTENSEN
NMPE 8547
12-14-2009
DATE



TYPICAL BOLLARD SECTION
SCALE: 1" = 1'-0"



SECTION X-X
SCALE: 1" = 4'-0"

RECORD DRAWING
FOR CERTIFICATION, SEE SHEET C1.4

PROJECT NUMBER
081570.00

DRAWN BY

DATE
AUGUST 27, 2009

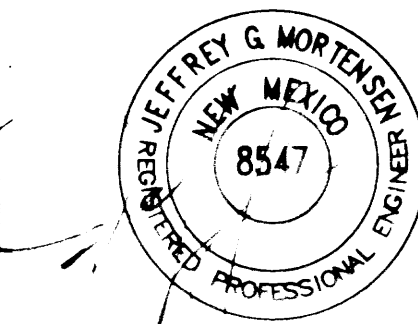
REVISIONS
2 ADD THIS SHEET
3 DRAINAGE CERT.
4 REVERT FOR PERM. C.O.

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2008.039.8



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12-08-2009



PROJECT
**HealthSouth
Rehabilitation
Hospital, 20-Bed
Addition / Remodel**

7000 JEFFERSON STREET
ALBUQUERQUE, NEW MEXICO

SHEET TITLE
**COMPACTOR PAD
AND ENCLOSURE
DETAIL**

SHEET NUMBER

C1.8

PARKING CALCULATIONS

THIS SITE IS ZONED S-H SPECIAL USE ZONE, FOR HOSPITAL AND RELATED HOSPITAL USES. THIS USE IS A REHABILITATION HOSPITAL. PARKING QUANTITY IS AS REQUIRED BY THE EPC PER ALBUQUERQUE ZONING CODE PART 3 GENERAL REGULATIONS 14-16-3-22.

THE FOLLOWING PARKING CALCULATIONS ARE BASED UPON CITY OF ALBUQUERQUE ZONING CODE PART 3. GENERAL REGULATIONS 14-16-3-1 OFF-STREET PARKING REGULATIONS.

PARKING SPACES FOR AUTOMOBILES AND LIGHT TRUCKS:

| USE | REQUIREMENT | UNITS | SPACES |
|----------|------------------|-----------|--------|
| OFFICE | 1 SPACE/200 SF. | 4,402 SF. | 22 |
| HOSPITAL | 1 SPACE/TWO BEDS | 81 BEDS | 44 |

| | |
|------------------------------------------------------|-----|
| TOTAL SPACES | 66 |
| 10% REDUCTION FOR ADJACENCY TO PUBLIC TRANSPORTATION | -6 |
| TOTAL REQUIRED SPACES | 60 |
| TOTAL EXISTING SPACES | 191 |
| TOTAL PROPOSED SPACES | 191 |

ACCESSIBLE/DISABLED PARKING: 4 SPACES REQUIRED, 13 SPACES PROVIDED BASED UPON CITY OF ALBUQUERQUE ZONING CODE PART 3. SEC. 14-16-3-1 (F) (1) (a) AND AMERICANS WITH DISABILITIES ACT 61.412 (5) (d).

VAN ACCESSIBLE PARKING: 2 SPACES REQUIRED, 4 SPACES PROVIDED (1 SPACE FOR EVERY 6 REQUIRED ACCESSIBLE SPACES)

MOTORCYCLE PARKING: 3 SPACES REQUIRED, 4 SPACES PROVIDED (FOR 3:100 REQUIRED OFF-STREET PARKING SPACES)

BICYCLE SPACES: 3 SPACES REQUIRED, 4 SPACES PROVIDED (1 SPACE FOR EVERY 20 REQUIRED OFF-STREET PARKING SPACES)

SITE DEVELOPMENT PLAN for BUILDING PERMIT Healthsouth Rehabilitation Hospital Tracts 4A and 4B1, Interstate Industrial Tract, Unit 5

AREAS:

| | | | |
|------------------------|-----------------------------|---------------------------------------|------------|
| SITE AREA | (11.0703 AC +/-) 482,220 SF | EXISTING BUILDING AREA | 61,734 GSF |
| DEVELOPED SITE AREA | 327,000 SF | NEW BUILDING AREA | |
| LANDSCAPE AREA | 58,350 SF | 20-BED PATIENT WING ADDITION | 10,705 GSF |
| | | DAY ROOM AND HALL ADDITION | 1,782 GSF |
| | | THERAPY GYM EXPANSION | 916 GSF |
| | | PATIENT ENTRY EXPANSION | 399 GSF |
| | | MAINTENANCE ADDITION | 517 GSF |
| | | TOTAL NEW BUILDING AREA: | 14,319 GSF |
| PUBLIC AREA REQUIRED | 1,014 SF MINIMUM | TOTAL EXISTING AND NEW BUILDING AREA: | 76,053 GSF |
| PUBLIC AREA PROVIDED | 5,404 SF | | |
| EMPLOYEE AREA REQ'D | 300 SF MINIMUM | | |
| EMPLOYEE AREA PROVIDED | 357 SF | TOTAL BEDS (EXISTING+NEW): | 87 BEDS |

PROJECT DESCRIPTION:

- PHASE 1, HEALTHSOUTH FACILITY IMPROVEMENTS:
 - ADDITION OF A ONE-STORY, 20-BED PATIENT WING TO THE EXISTING 67-BED, ONE STORY BUILDING
 - DAY ROOM AND HALL ADDITION
 - NEW BULK STORAGE OXYGEN TANK AND ENCLOSURE
 - NEW GENERATOR
 - NEW PATIENT DROP-OFF
- PHASE 1, REQUIRED IMPROVEMENTS TO SITE:
 - ACCESSIBLE SIDEWALK TO BUS STOP
 - PARKING LOT CROSSWALKS AND RESTRIPING
 - LANDSCAPING IMPROVEMENTS
- PHASE 2: NEW THERAPY GYM EXPANSION AND PATIENT ENTRY EXPANSION UNDER EXISTING PORCH STRUCTURES AND RELATED SITE/LANDSCAPING IMPROVEMENTS
- PHASE 3: NEW ONE-STORY MAINTENANCE ADDITION
- PHASE 4: PARKING EXPANSION FOR 14 SPACES AND RELATED LANDSCAPING IMPROVEMENTS
- PHASE 5: NEW PAVILLION STRUCTURE AT ENTRY DROP-OFF

DRAWING INDEX:

- SITE DEVELOPMENT PLAN FOR BUILDING PERMIT
- BUILDING AND STRUCTURE ELEVATIONS
- BUILDING AND STRUCTURE ELEVATIONS
- LANDSCAPING PLAN
- LANDSCAPING DETAILS AREA 1
- LANDSCAPING DETAILS AREA 2
- LANDSCAPING DETAILS AREA 3
- LANDSCAPING DETAILS AREA 4
- PRELIMINARY GRADING PLAN
- DRAINAGE PLAN AND CALCULATIONS
- CONCEPTUAL UTILITY PLAN
- PREVIOUSLY APPROVED SITE PLAN
- PREVIOUSLY APPROVED LANDSCAPING PLAN
- SITE PLAN PHOTOMETRICS
- LIGHTING FIXTURES AND POLE BASE DETAILS

PROJECT NUMBER
081570.00

DRAWN BY
M. Bensky

DATE
November 6, 2008

REVISIONS
December 1, 2008
February 16, 2009

DECEMBER 07, 2009
(TCL CERTIFICATION)

BOULDER ASSOCIATES

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PROJECT
HealthSouth
Rehabilitation
Hospital
Addition / Remodel

7000 Jefferson Street
Albuquerque, New Mexico

PRELIMINARY
DRAWING
NOT FOR
CONSTRUCTION

SHEET TITLE
Site Development Plan
for Building Permit

2008.03.9.9 (TCL)

SHEET NUMBER
1

RECEIVED
DEC 08 2009
HYDROLOGY SECTION

PROJECT NUMBER: 1004614

Application Number: 09DRB-70111

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated [redacted], and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List Required? () Yes () No If yes, then a set of approved DRG plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DRB SITE DEVELOPMENT PLAN SIGNOFF APPROVAL:

Environmental Health, if necessary

04-03-09
Traffic Engineering, Transportation Division
Date

3-25-09
Water-Utility Authority
Date

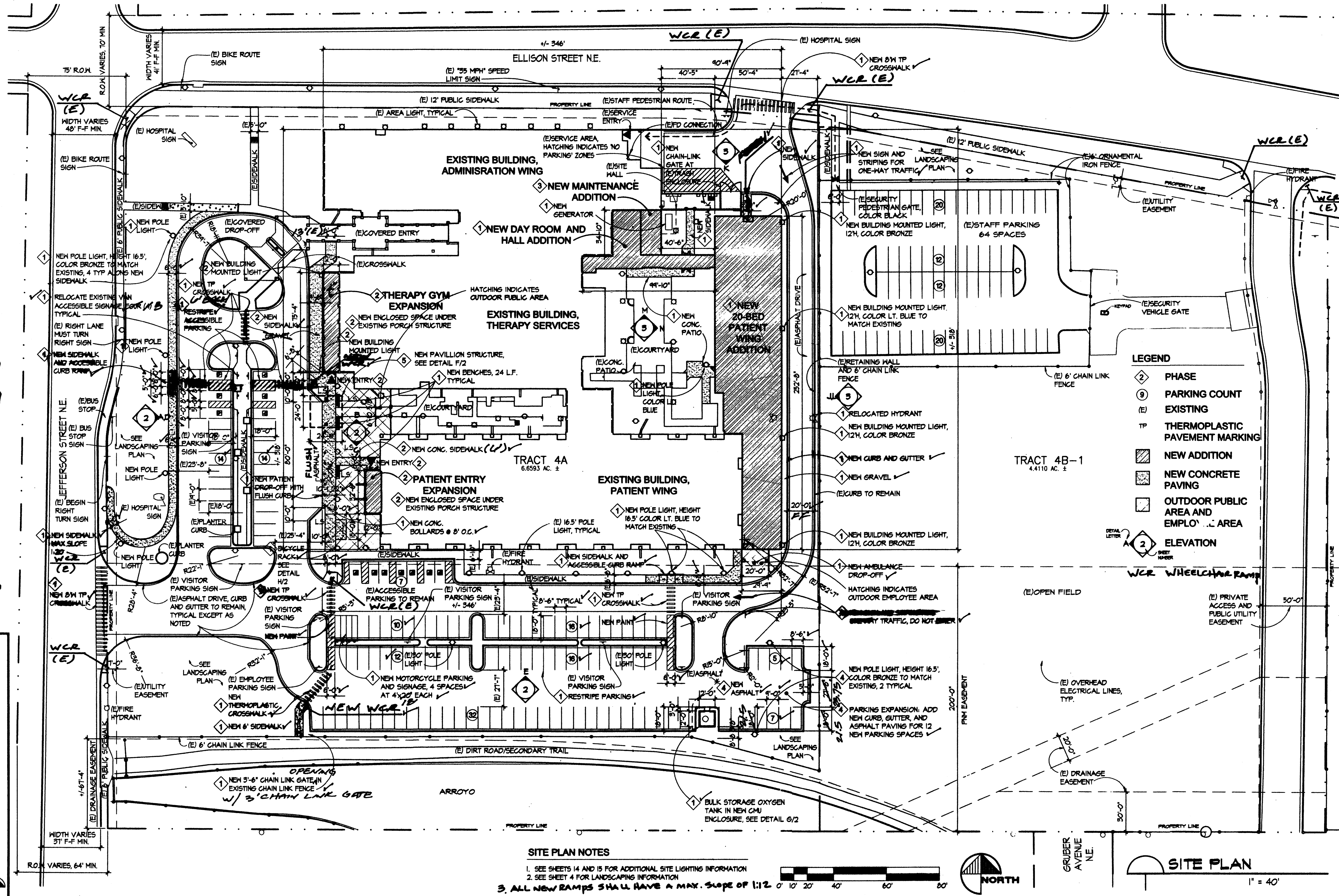
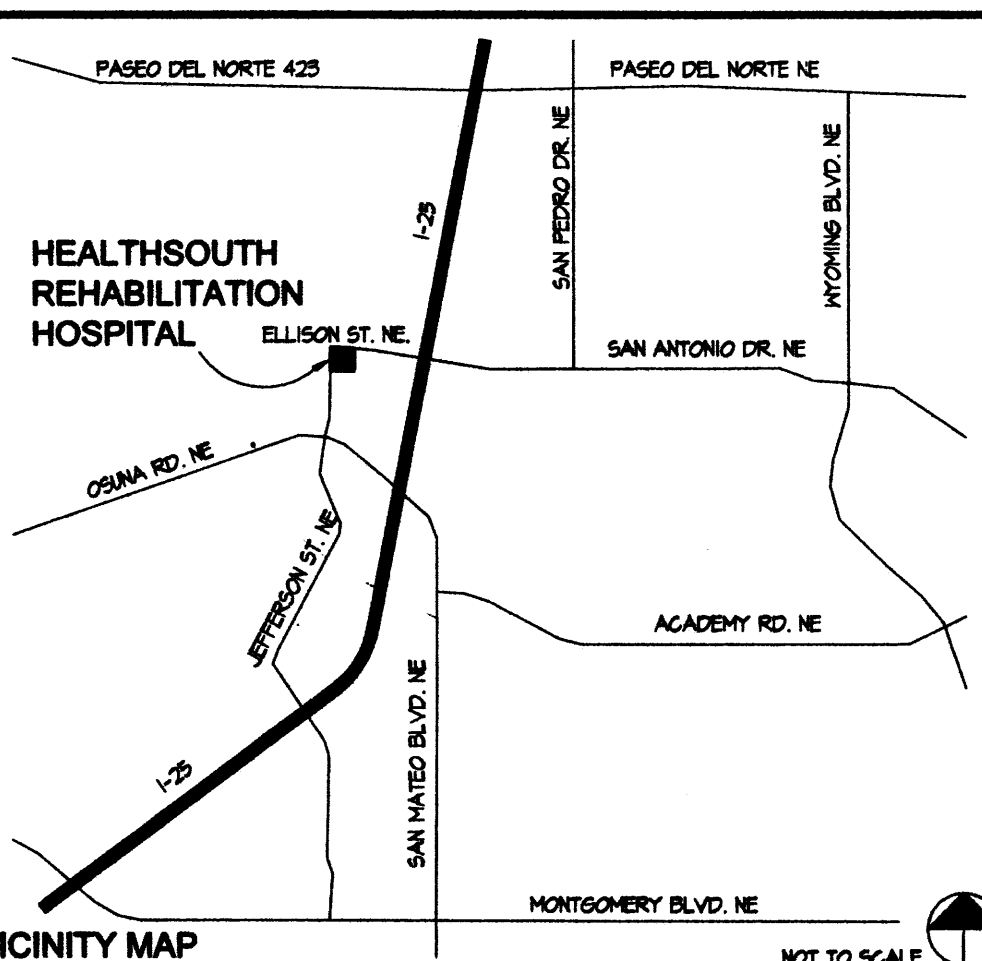
3/25/09
Christina Sandoval
Parks and Recreation Department
Date

3/25/09
Bradley D. Bingham
City Engineer
Date

3-10-09
Environmental Health Department (conditional)
Date

3-10-09
Lee Whistle
Solid Waste Management
Date

3-03-09
Bill Dine
DRB Chairperson, Planning Department
Date



SITE PLAN NOTES

- SEE SHEETS 14 AND 15 FOR ADDITIONAL SITE LIGHTING INFORMATION
- SEE SHEET 4 FOR LANDSCAPING INFORMATION
- ALL NEW RAMPS SHALL HAVE A MAX. SLOPE OF 1:12



SITE PLAN

1" = 40'