

DRB ADMINISTRATIVE
SITE PLAN AMENDMENT
PROJECT NO. 1081946
APPLICATION NO. 0724-10177
APPROVED: [Signature] 07-03-10
PLANNING DIRECTOR DATE

ISSUED FOR
PERMIT

PROJECT

**LOS ANGELES CENTER
RETAIL SHOPS**
6220 Paseo Del Norte & San Pedro NE
Albuquerque, New Mexico

GENERAL NOTES

- A. GENERAL CONTRACTOR IS TO FIELD VERIFY EXISTING SITE CONDITIONS AND IS RESPONSIBLE FOR DEMOLITION AND REMOVAL OF DEBRIS AS REQUIRED TO COMPLETE THE SCOPE OF WORK
- B. FOR HYDRANT LOCATIONS RE: CIVIL, MASTER UTILITY PLAN
- C. IF LAYOUT DIFFERS SIGNIFICANTLY FROM CIVIL DRAWINGS, NOTIFY ARCHITECT IMMEDIATELY.
- D. DIMENSIONS ARE FROM FACE OF CURB.
- E. ALL PAINT ON EXTERIOR FINISHES TO BE WEATHER RESISTANT & NON CORROSIVE.
- F. ALL TREES, IRRIGATION AND SITE LIGHTING TO BE BE PUT BACK IN LIKE AND IN-KIND CONDITION.

KEYED NOTES

1. ACCESSIBLE PARKING SPACE W/ LOADING ZONE, PAINT WITH 4" WIDE WHITE STRIPES AT 45° HATCH LINES AT 18" O.C., RE: D5/A091.
2. NEW LIGHT POLE TO MATCH EXISTING, RE: ELECTRICAL
3. EXISTING LIGHT POLE TO REMAIN
4. EXISTING 6" SIDEWALK TO REMAIN
5. BIKE RACK, RE: C2/A091
6. COMPACT CAR SPACE
7. FIRE HYDRANT LOCATION, RE: CIVIL
8. MOTORCYCLE PARKING, RE: D3/A091 FOR SIGNAGE DETAIL
9. METAL BENCH BY BELSON - THERMOPLASTIC COATED STEEL SLAT STYLE BENCH, 6' LONG, S6NB-S, SURFACE MOUNT, COLOR: GREY. SECURE TO CONCRETE. PROVIDED AND INSTALLED BY CONTRACTOR.
10. DUMPSTER/COMPACTOR LOCATION W/ RECYCLE AREA, RE: B4/A091
11. MATCH EXISTING SIDEWALK ELEVATION.
12. NEW 6" HIGH CONCRETE CURB, SEE A3/A091
13. 4" CONCRETE SIDEWALK, RE: A4/A091
14. SIDEWALK CONTROL JOINT, EVERY 6'-0" UNLESS SHOWN OTHERWISE.
15. ACCESSIBLE RAMP, RE: C5/A091
16. ACCESSIBLE SIGNAGE, RE: D3/A091
17. 4" WIDE PARKING STALL STRIPING, TYP. COLOR: WHITE
18. 6" HIGH PARKING BUMPER, RE: D2/A091
19. CONCRETE RAMP, SLOPE TO GRADE AT 1:20, MAX (NO GUARDRAIL REQUIRED)
20. TRANSFORMER, RE: CIVIL UTILITIES PLAN
21. A.D.A. PAVEMENT SIGNAGE, RE: D4/A091
22. LANDSCAPE AREA, RE: LANDSCAPE PLANS
23. PROPERTY LINE
24. NEW BUILDING
25. EXISTING PARKING LOT
26. ASPHALTIC PAVING OVER PREPARED SUBGRADE, RE: CIVIL FOR EXTENT
27. CONCRETE LANDING
28. TRASH RECEPTACLE BY BELSON - THERMOPLASTIC TRASH RECEPTACLE - DECORATIVE EXPANDED STEEL 32 GALLON WITH RIGID LINER, MODEL R32L WITH FLAT TOP MODEL R32FC, COLOR GREY. SECURE TO CONCRETE PER MANUFACTURERS DIRECTIONS WITH SURFACE POST MOUNT KIT MODEL RSP.
29. 18" + WIDE CONCRETE MOW STRIP. RE: A3/A091.
30. TIMBER RETAINING WALL, RE: CIVIL
31. IRRIGATION BACKFLOW PREVENTER/MASTER VALVE HOT BOX ENCLOSURE.
32. WATER METER
33. EXISTING DROP INLET, RE: CIVIL PLANS

SITE ANALYSIS AND DATA

LEGAL DESCRIPTION
PORTION OF TRACT A-1-B

ZONING
M-1 (SC)

TOTAL ACREAGE .6721 AC

ACTUAL BUILDING AREA WITH HORIZONTAL PROJECTIONS

6,969 GSF

FAR: .238

BUILDING USE RETAIL

PARKING (2007 ALBUQUERQUE CITY ZONING CODE)

RETAIL 30 (6,000 N.S.F. @ 1:200SF)

TOTAL PARKING REQUIRED 30 (INCLUDES 2 ACCESS. SPACES, 1 BEING VAN)

TOTAL PARKING PROVIDED 38 (INCLUDES 2 ACCESS. SPACES, 1 BEING VAN)

MOTORCYCLE PARKING (2007 ALBUQ. CITY ZONING CODE)

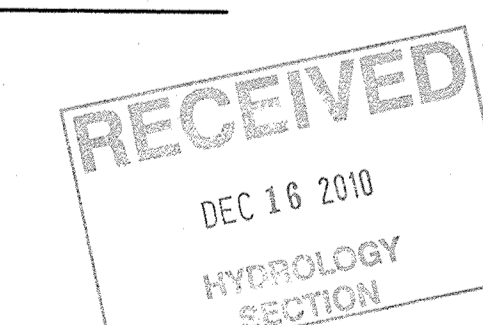
REQUIRED: = 2

PROVIDED: = 2

BICYCLE PARKING (2007 ALBUQUERQUE CITY ZONING CODE)

REQUIRED: 30/20 = 1.5

PROVIDED: = 5



REVISIONS
11/10/09 Admin. Amendment

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- ▲
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DRAWN BY SAS

REVIEWED BY CG

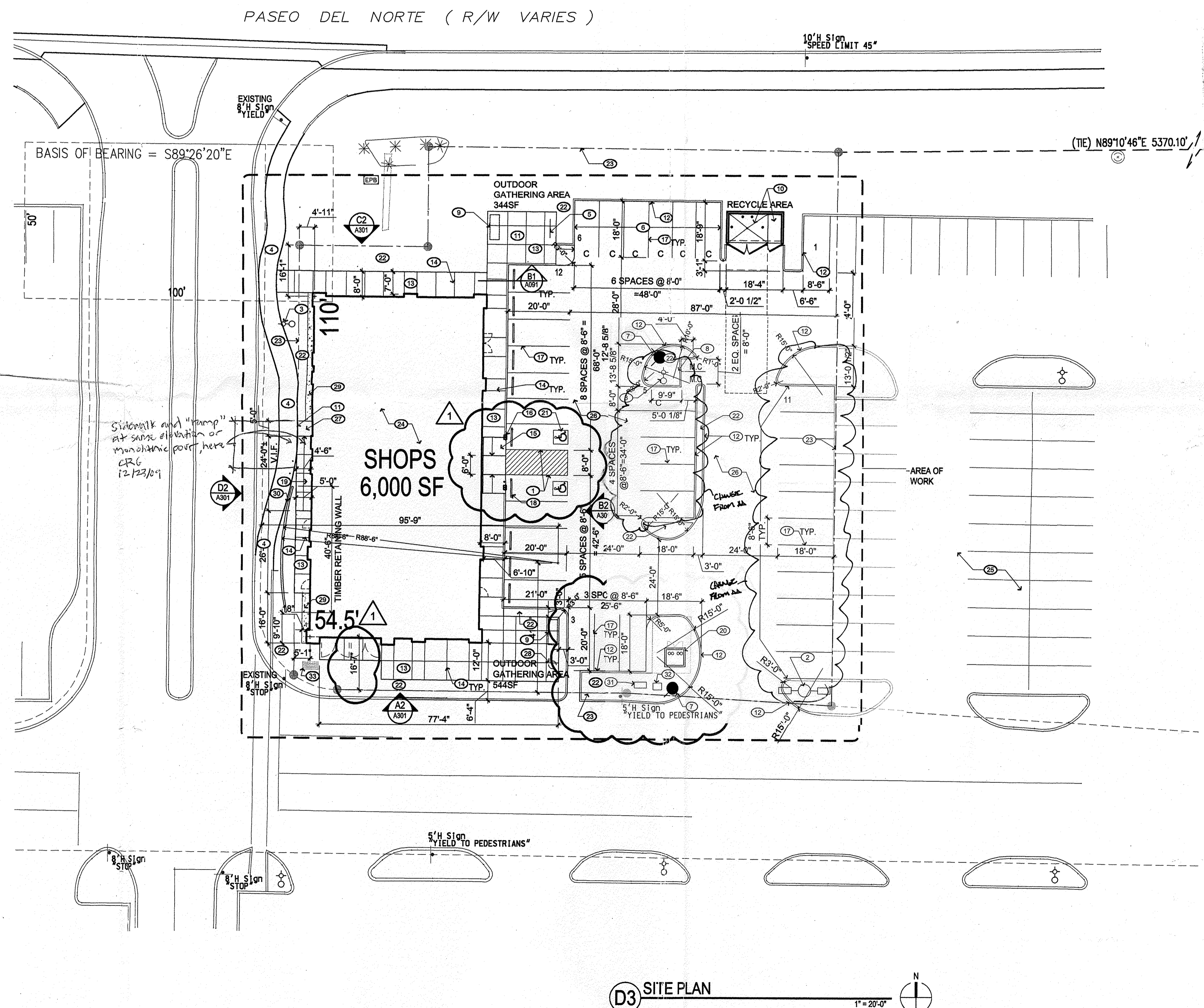
DATE 11-18-09

PROJECT NO. 09-0060

DRAWING NAME

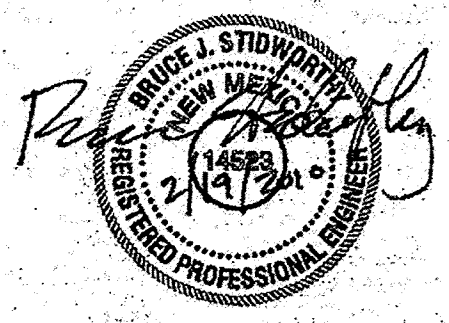
SITE PLAN

SHEET NO.



D3 SITE PLAN
1" = 20'-0"

ISSUED FOR
PERMIT



PROJECT
**LOS ANGELES CENTER
RETAIL SHOPS**
6220 Paseo Del Norte & San Pedro NE
Albuquerque, New Mexico

REVISIONS

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DRAWN BY
REVIEWED BY
DATE 11-18-09
PROJECT NO. 09-0060
DRAWING NAME

GRADING AND
DRAINAGE PLAN
C-101
SHEET NO. OF

GENERAL NOTES:

- ALL WORK DETAILED ON THESE PLANS AND PERFORMED UNDER THIS CONTRACT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS AND THE PROJECT GEOTECHNICAL REPORT, WHERE APPLICABLE, CITY OF ALBUQUERQUE PUBLIC WORKS STANDARDS SHALL APPLY.
- THE CONTRACTOR SHALL ABIDE BY ALL LOCAL, STATE, AND FEDERAL LAWS, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS, INCLUDING EPA REQUIREMENTS WITH RESPECT TO STORM WATER DISCHARGE.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL POTENTIAL OBSTRUCTIONS INCLUDING ALL UNDERGROUND UTILITIES. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE CONSTRUCTION OBSERVER OR ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR SHALL CONTACT LINE LOCATING SERVICE FOR LOCATION OF EXISTING UTILITIES.
- ALL ELECTRICAL, TELEPHONE, CABLE TV, GAS AND OTHER UTILITY LINES, CABLES, AND APPURTENANCES ENCOUNTERED DURING CONSTRUCTION THAT REQUIRE RELOCATION, SHALL BE COORDINATED WITH THAT UTILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL NECESSARY UTILITY ADJUSTMENTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR DELAYS OR INCONVENIENCES CAUSED BY UTILITY COMPANY WORK CREWS. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE HIS ACTIVITIES TO ALLOW UTILITY CREWS TO PERFORM THEIR REQUIRED WORK.
- THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITY LINES AND ELECTRICAL CONDUIT SERVING THE EXISTING SITE LIGHTING WITHIN THE CONSTRUCTION AREA. ANY DAMAGE TO EXISTING FACILITIES CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE AND APPROVED BY THE CONSTRUCTION OBSERVER.
- CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT PROPERTIES RESULTING FROM THE CONSTRUCTION PROCESS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- OVERNIGHT PARKING OF CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS OR DESIGNATED TRAFFIC LANES. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL WITHIN THE PUBLIC RIGHT-OF-WAY.
- THE CONTRACTOR SHALL OBTAIN ALL THE NECESSARY PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION (I.E., BARRICADING, TOPSOIL DISTURBANCE, EXCAVATION PERMITS, EPA STORM WATER PERMITS, ETC.).
- ALL PROPERTY CORNERS DESTROYED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. ALL PROPERTY CORNERS MUST BE RESET BY A REGISTERED LAND SURVEYOR.
- THE CONTRACTOR SHALL PREPARE A CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN AND OBTAIN APPROVAL OF SUCH PLAN FROM THE CITY OF ALBUQUERQUE, TRAFFIC ENGINEERING DEPARTMENT, PRIOR TO BEGINNING ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.
- ALL BARRICADES AND CONSTRUCTION SIGNING SHALL CONFORM TO APPLICABLE SECTIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), US DEPARTMENT OF TRANSPORTATION, LATEST EDITION.
- THE CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION BARRICADES AND SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADES AT THE END AND BEGINNING OF EACH DAY.
- THE CONTRACTOR SHALL TAKE ALL STEPS NECESSARY TO CONFORM WITH EPA REQUIREMENTS, INCLUDING COMPLIANCE WITH NPDES PHASE 2 REQUIREMENTS.
- CONSTRUCTION WORK REQUIRING RE-ROUTING OF TRAFFIC UTILIZING THE MAIN EAST-WEST DRIVE AISLE SHALL BE COORDINATED WITH THE PROPERTY OWNER AND LOWE'S. CONTRACTOR TO CONTACT GREG GARNER, THE LOWE'S STORE MANAGER, CONTACT AT (505) 797-4666, 2 WORK DAYS PRIOR TO BEGINNING WORK.

GRADING NOTES:

- EXCEPT AS PROVIDED HEREIN, GRADING SHALL BE PERFORMED AT THE ELEVATIONS AND IN ACCORDANCE WITH THE DETAILS SHOWN ON THIS PLAN.
- THE COST FOR REQUIRED CONSTRUCTION DUST AND EROSION CONTROL MEASURES SHALL BE INCIDENTAL TO THE PROJECT COST.
- ALL WORK RELATIVE TO FOUNDATION CONSTRUCTION, SITE PREPARATION, PAVEMENT INSTALLATION, AND METHANE GAS MITIGATION AS SHOWN ON THIS PLAN, SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "GEOTECHNICAL INVESTIGATION," AS PROVIDED BY THE ARCHITECT OR OWNER. ALL OTHER WORK SHALL, UNLESS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT (FIRST PRIORITY) SPECIFICATIONS, AND/OR THE CITY OF ALBUQUERQUE (COA) STANDARD SPECIFICATIONS FOR PUBLIC WORKS (SECOND PRIORITY).
- EARTH SLOPES SHALL NOT EXCEED 3 HORIZONTAL TO 1 VERTICAL UNLESS SHOWN OTHERWISE.
- IT IS THE INTENT OF THESE PLANS THAT THIS CONTRACTOR SHALL NOT PERFORM ANY WORK OUTSIDE OF THE PROPERTY BOUNDARIES EXCEPT AS REQUIRED BY THIS PLAN.
- THE CONTRACTOR IS TO ENSURE THAT NO SOIL ERODES FROM THE SITE ONTO ADJACENT PROPERTY OR PUBLIC RIGHT-OF-WAY. THIS SHOULD BE ACHIEVED BY CONSTRUCTING TEMPORARY BERMS OR SILT FENCE AT THE PROPERTY LINES AND WETTING THE SOIL TO PROTECT IT FROM WIND EROSION.
- A DISPOSAL SITE FOR ANY & ALL EXCESS EXCAVATION MATERIAL, AND UNSUITABLE MATERIAL AND/OR A BORROW SITE CONTAINING ACCEPTABLE FILL MATERIAL SHALL BE OBTAINED BY THE CONTRACTOR IN COMPLIANCE WITH APPLICABLE ENVIRONMENTAL REGULATIONS AND APPROVED BY THE OBSERVER. ALL COSTS INCURRED IN OBTAINING A DISPOSAL OR BORROW SITE AND HAUL TO OR FROM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.
- PAVING AND ROADWAY GRADES SHALL BE +/- 0.1' FROM PLAN ELEVATIONS. PAD ELEVATION SHALL BE +/- 0.05' FROM BUILDING PLAN ELEVATION.
- ALL PROPOSED CONTOURS REFLECT TOP OF PAVEMENT ELEVATIONS IN THE PARKING AREA AND MUST BE ADJUSTED FOR MEDIANS AND ISLANDS.
- VERIFY ALL ELEVATIONS SHOWN ON PLAN FROM BASIS OF ELEVATION CONTROL STATION PRIOR TO BEGINNING CONSTRUCTION.

OVEREXCAVATION NOTES:

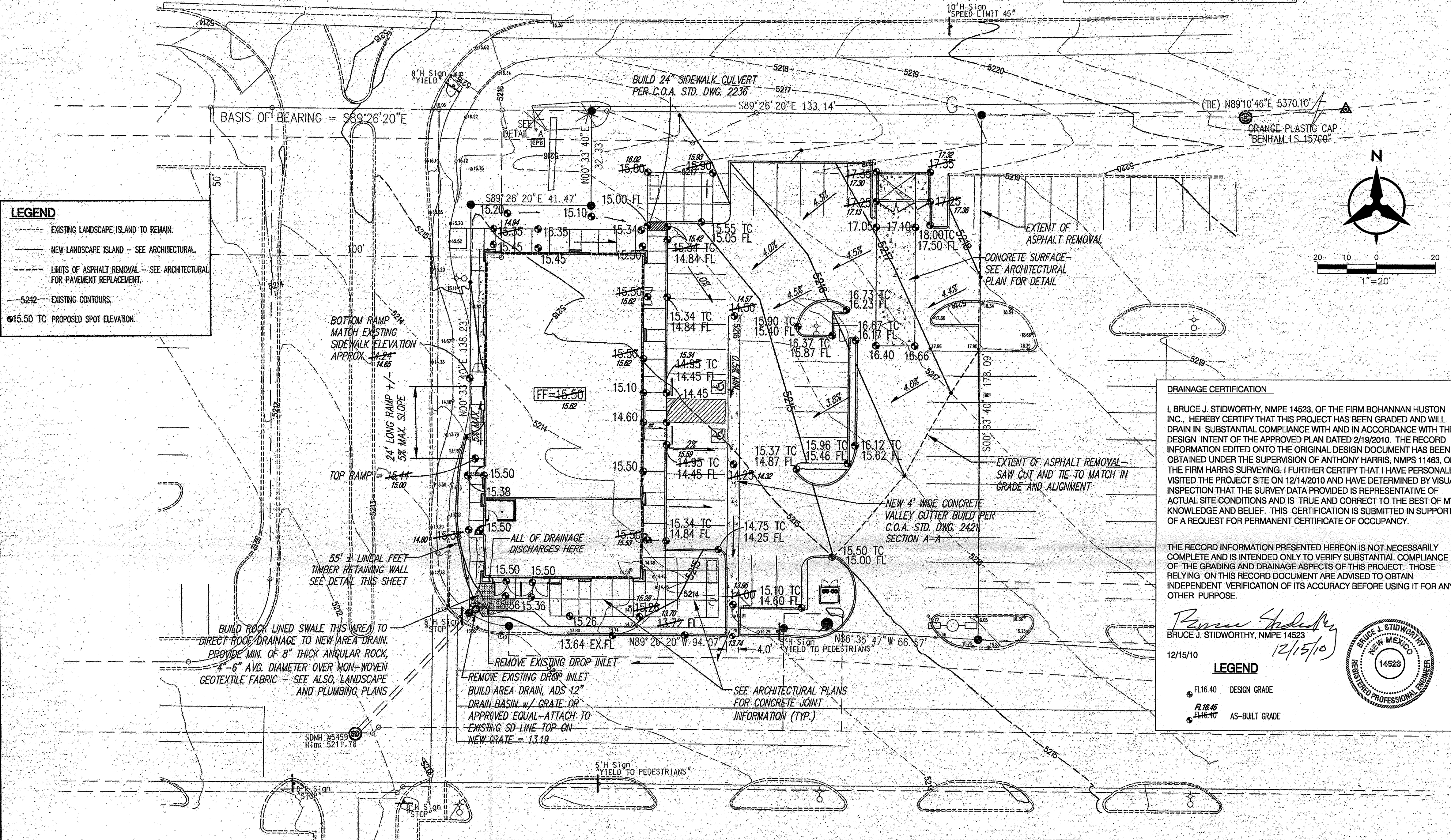
- GEOTECHNICAL ENGINEERING ANALYSIS REQUIRES OVEREXCAVATION OF APPROXIMATELY 11'-0" AT THE BUILDING PAD. SEE THE GEOTECHNICAL EVALUATION REPORT BY WESTERN TECHNOLOGIES, INC. DATED 01/15/2009 FOR FURTHER DETAILS.
- CONTRACTOR TO PROTECT EXISTING CURB, GUTTER, SIDEWALK, LANDSCAPING, LIGHT POLES, UNDERGROUND UTILITIES, ETC., AS A PART OF THE OVEREXCAVATION PROCESS.
- CONTRACTOR TO REPLACE EXISTING SIDEWALK, LANDSCAPING, LIGHT POLES, UNDERGROUND UTILITIES, ETC., WITH LIKE SIZE AND TYPE IF SUCH ITEMS ARE REMOVED OR DAMAGED AS A PART OF THE OVEREXCAVATION WORK.
- CURB AND GUTTER AROUND PERIMETER OF SITE MUST REMAIN UNDISTURBED THROUGHOUT THE OVEREXCAVATION WORK.

PROJECT DESCRIPTION:

THE PROPOSED DEVELOPMENT WILL CONSIST OF A SINGLE STORY MULTI-TENANT COMMERCIAL BUILDING OF APPROXIMATELY 6000 SQUARE FEET. THE SITE IS LOCATED NORTH ON THE EXISTING LOWE'S HOME IMPROVEMENT CENTER AT 6200 PASEO DEL NORTE NEAR THE INTERSECTION OF SAN PEDRO DRIVE. A RECENT SUBDIVISION REQUEST FOR THIS SITE, CITY PROJECT NUMBER 1001946, 0908B-70070, IS RELATED TO THIS REQUEST AS IT CREATES THE PARCEL THAT THE PROPOSED NEW BUILDING WILL BE BUILT ON.

USC&GS BRASS DISK STAMPED "HEAVEN 1969"
GEOGRAPHIC POSITION (NAD 1983)
NM STATE PLANE GRID COORDINATES (CENTRAL ZONE)
X=1,547,297.145 Y=1,518,799.515
GROUND TO GRID FACTOR = 0.999655810
DELTA ALPHA = -0°10'46.22"
ELEVATION = 5378.235 (NAVD 1988)

PASEO DEL NORTE (RAW VARIES)



LEGEND

- EXISTING LANDSCAPE ISLAND TO REMAIN
- NEW LANDSCAPE ISLAND - SEE ARCHITECTURAL
- LIMITS OF ASPHALT REMOVAL - SEE ARCHITECTURAL FOR PAVEMENT REPLACEMENT
- 5212 EXISTING CONTOURS
- 15.50 TC PROPOSED SPOT ELEVATION

DRAINAGE CERTIFICATION

I, BRUCE J. STIDWORTHY, NMPE 14523, OF THE FIRM BOHANNAN HUSTON INC. HEREBY CERTIFY THAT THIS PROJECT HAS BEEN GRADED AND WILL DRAIN IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED 2/19/2010. THE RECORD INFORMATION EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED UNDER THE SUPERVISION OF ANTHONY HARRIS, NMPS 11463, OF THE FIRM HARRIS SURVEYING. I FURTHER CERTIFY THAT I HAVE PERSONALLY VISITED THE PROJECT SITE ON 12/14/2010 AND HAVE DETERMINED BY VISUAL INSPECTION THAT THE SURVEY DATA PROVIDED IS REPRESENTATIVE OF ACTUAL SITE CONDITIONS AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR PERMANENT CERTIFICATE OF OCCUPANCY.

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND IS INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE GRADING AND DRAINAGE ASPECTS OF THIS PROJECT. THOSE RELYING ON THIS RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.

Bruce J. Stidworthy
BRUCE J. STIDWORTHY, NMPE 14523
12/15/10

LEGEND

- FL16.40 DESIGN GRADE
- FL16.45 AS-BUILT GRADE

METHANE GAS MITIGATION

METHANE GAS MITIGATION IS REQUIRED AS A PART OF THIS PROJECT. THIS MITIGATION SHALL BE AS DESCRIBED IN THE LANDFILL GAS ASSESSMENT REPORT DATED APRIL 3, 2009 AND PREPARED BY GEO-TEST, INC. REFERENCE ALSO, THE BOHANNAN-HUSTON LETTER SPECIFYING ADDITIONAL MITIGATION INFORMATION DATED APRIL 23, 2009. IN ACCORDANCE WITH THE DETAIL SHOWN ON THIS SHEET, MITIGATION EFFORTS INCLUDE BUT ARE NOT LIMITED TO INSTALLING A METHANE BARRIER LINING BENEATH THE NEW BUILDING AND INSTALLING UTILITY TRENCH VENTING IN ACCORDANCE WITH THE DETAIL SHOWN ON SHEET C-102. THE BARRIER SHOULD BE ATTACHED TO THE CONCRETE STEELWALLS USING AN APPROPRIATE ADHESIVE PER MANUFACTURER'S SPECIFICATIONS. IF IMPERMEABLE MEMBRANE SHEETS OVERLAP, THE OVERLAP SHOULD BE A MINIMUM OF 4 INCHES AND THE SHEET SEAMS SHOULD BE ATTACHED (MASTIC, FUSION, OR SOLVENT WELDED) PER THE MANUFACTURER'S SPECIFICATIONS. IF THE MEMBRANE SHEETS ARE "S"-FOLDED, THEN THE INITIAL OVERLAP SHOULD BE A MINIMUM OF 2 FEET AND THE TOP FLAP SHOULD BE SEALED WITH NONHARDENING CAULK. IF DESIRED, TWO INCHES OF CLEAN SAND MAY BE PLACED OVER THE MEMBRANE TO PROTECT IT DURING CONSTRUCTION AND TO AID SLAB CURING. SAND SHOULD BE PLACED AHEAD OF EQUIPMENT SO THAT THE MEMBRANE IS NOT TORN OR PUNCTURED. THE CONTRACTOR SHOULD VERIFY PRIOR TO CONSTRUCTION THAT THE MEMBRANE IS NOT BREACHED, TEARS OR RIPS SHOULD BE REPAIRED. UTILITY PENETRATIONS THROUGH THE MEMBRANE SHOULD BE APPROPRIATELY SEALED WITH UTILITY BOOTS AND MASTIC OR NONHARDENING CAULK.

DRAINAGE MANAGEMENT PLAN:

EXISTING CONDITIONS:

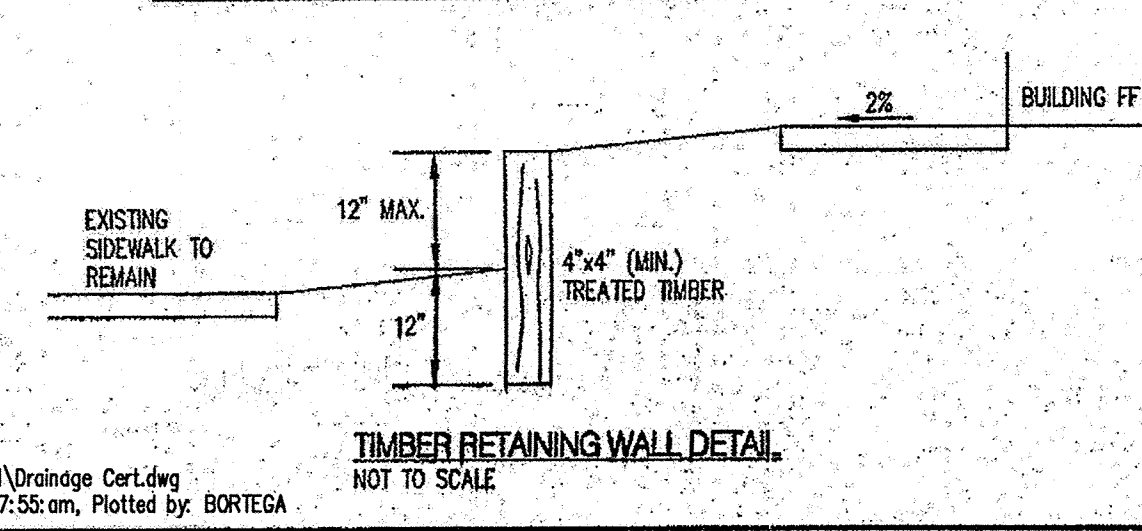
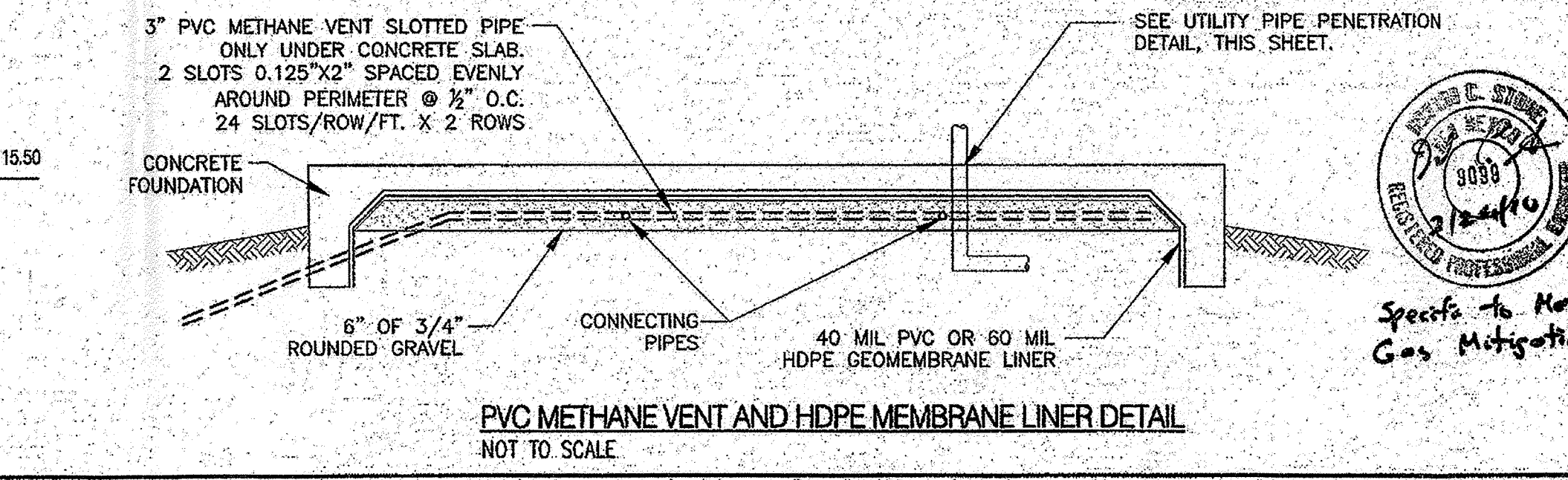
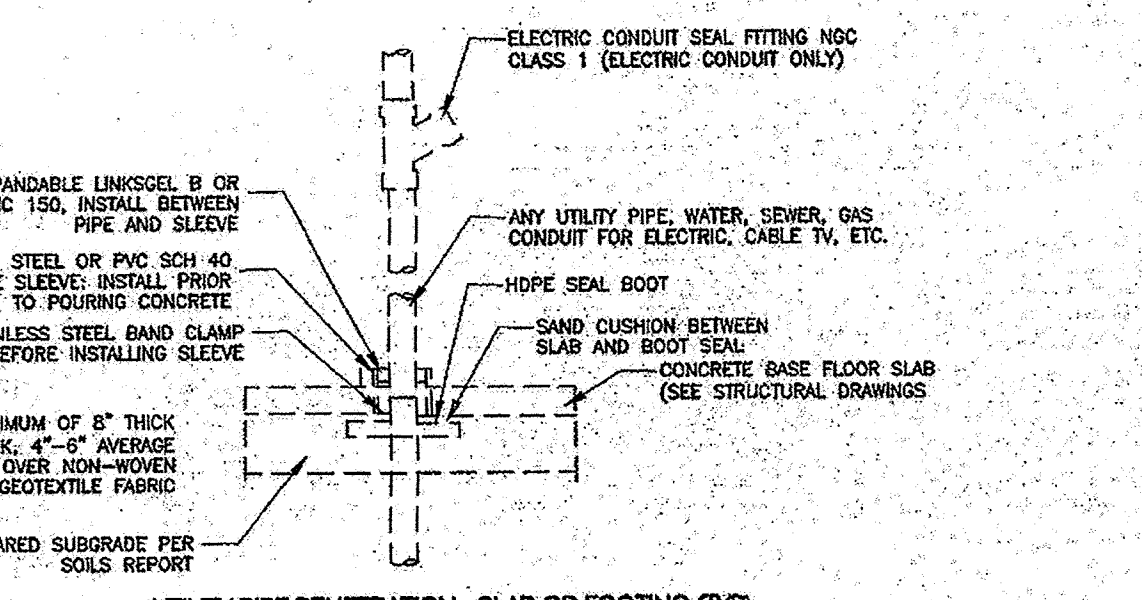
THE SITE IS A PAVED PARKING LOT WITH LANDSCAPE ISLANDS. MUCH OF THE SITE CURRENTLY DRAINS TO AN EXISTING DROP INLET LOCATED NEAR THE SOUTHWEST CORNER OF THE PROPOSED BUILDING. THIS FLOWRATE ENTERING THAT INLET UNDER EXISTING CONDITIONS 5.34CFS (SEE CALCULATIONS FOR INLET #10 IN THE ORIGINAL LOWE'S DRAINAGE REPORT - SEE CITY HYDROLOGY FILE # 018/D14). THE REMAINDER OF THE SITE FOR THE PROPOSED PROJECT AND MUCH OF THE EXISTING LOWE'S PARKING LOT DRAINS TO INLET #5 AS SHOWN IN THE LOWE'S DRAINAGE REPORT. THE TOTAL FLOWRATE TO INLET #5 IS 16.0CFS.

PROPOSED CONDITIONS:

UNDER PROPOSED CONDITIONS THE ROOF DRAINS FROM THE BUILDING WILL DRAIN TO EXISTING INLET #9 AND THE FLOWRATE TO THAT INLET WILL BE 0.8CFS. THE REMAINING FLOW WHICH PREVIOUSLY ENTERED INLET #9 WILL NOW ENTER INLET #5 WHICH IS DOWNSTREAM OF INLET #9 ON THE SAME STORM DRAIN SYSTEM. INLET #5 HAS SIGNIFICANTLY MORE EXCESS CAPACITY THAN REQUIRED TO ACCOMMODATE THE ADDITIONAL FLOW WHICH PREVIOUSLY ENTERED INLET #9 (SEE PAGE A2-5 OF THE ORIGINAL LOWE'S DRAINAGE REPORT).

CONCLUSION:

THE MODIFICATIONS TO THE EXISTING DRAINAGE PLAN WHICH ARE PROPOSED WITH THIS PROJECT ARE MINOR AND ARE NOT DETRIMENTAL TO THE OPERATION OF THE EXISTING DRAINAGE SYSTEM. THE OVERALL RUNOFF FROM THE SITE DOES NOT INCREASE SINCE THE SITE IS AN EXISTING PAVED PARKING LOT.



INLET #5 - LOWE'S STANDARD - TYPE C

AVAILABLE HEAD = 0.67'

WEIR LENGTH = 6' + 6' + 2' + 2' = 16'

$$Q = 2.68 \cdot 16 \cdot 0.67^{3/2}$$

CAPACITY = 32.7 cfs

Q_{100} to the inlet = 75% of BZ
= 16.0 cfs

$$32.7 > 16.0 \Rightarrow \underline{\underline{OK}}$$

INLET #5
Capacity

Bohannon & Huston



A2-5

PROJECT NAME _____ SHEET _____ OF _____

PROJECT NO. _____ BY _____ DATE _____

SUBJECT _____ CH'D _____ DATE _____

ENGINEERS PLANNERS PHOTOGRAMMETRISTS
SURVEYORS SOFTWARE DEVELOPERS

INLET #10
CAPACITY

Lowe's Standard Type 'B' Inlet - (Similar to CoA Type 'C')

Neenah R-3246-F

- *Open Area (for orifice calc in sq. ft. 1.8
- *Length of Weir (feet): 5.9
- Weir Coefficient : 2.68

*Note: Numbers are take from Neenah Catalog

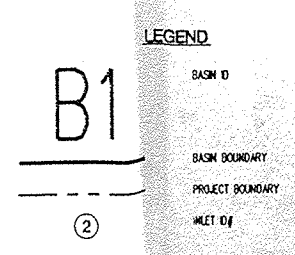
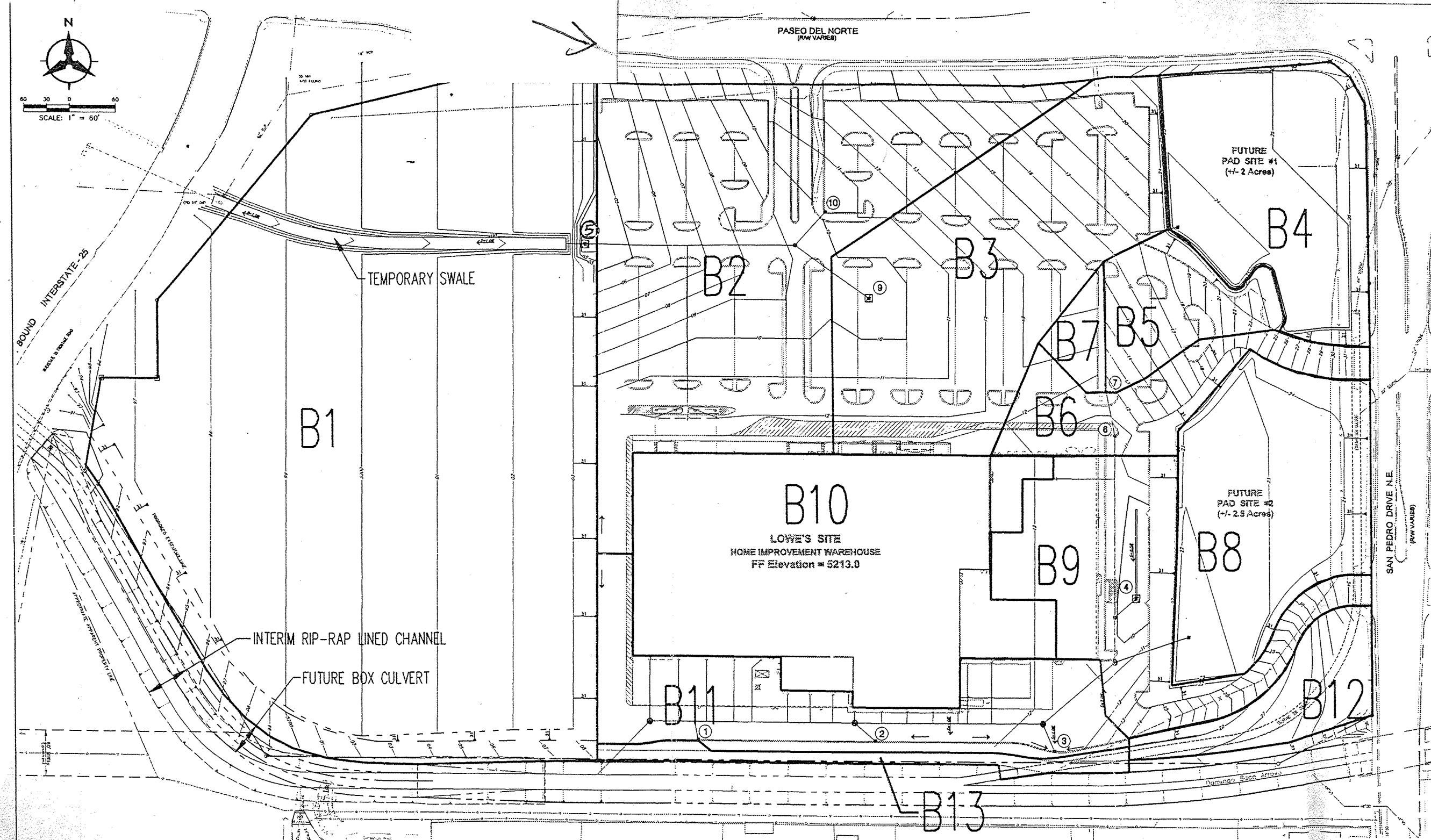
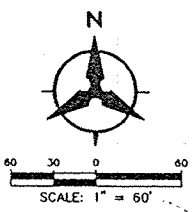
Head (ft)	Head (in)	Weir Q	Orifice Q	Control Q
0.05	0.6	0.18	1.94	0.18
0.1	1.2	0.50	2.74	0.50
0.15	1.8	0.92	3.36	0.92
0.2	2.4	1.41	3.88	1.41
0.25	3	1.98	4.33	1.98
0.3	3.6	2.60	4.75	2.60
0.35	4.2	3.27	5.13	3.27
0.4	4.8	4.00	5.48	4.00
0.45	5.4	4.77	5.81	4.77
0.5	6	5.59	6.13	5.59
0.55	6.6	6.45	6.43	6.43
0.6	7.2	7.35	6.71	6.71
0.65	7.8	8.29	6.99	6.99
0.7	8.4	9.26	7.25	7.25
0.75	9	10.27	7.51	7.51
0.8	9.6	11.31	7.75	7.75
0.85	10.2	12.39	7.99	7.99
0.9	10.8	13.50	8.22	8.22
0.95	11.4	14.64	8.45	8.45
1	12	15.81	8.67	8.67

INLET CAPACITY TABLE

INLET ID#	BASINS	RESIDUAL TO INLET	TOTAL Q ₁₀₀	HEAD AVAILABLE	CAPACITY	OVERFLOW
1	40% of B11	—	3.42	0.5	5.59	—
2	20% of B11	—	1.71	0.3	2.60	—
3	30% of B11	—	2.57	0.6	6.71	—
6	B6	6.41	10.52	0.5'	5.59	4.93
7	B4, B5, B7	—	12.00	0.5'	5.59	6.41
8	NOT USED					
10	25% of B2	—	5.34	0.5	5.59	—
TRUCK WELL TRENCH DR.	10% of B1	—	0.86	72.0'		

DESIGN OF THIS INLET BY MECH. ENGR FOR BLDG. A2-4

INLET #10
LOCATION

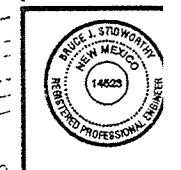


Continuum Houston
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ENGINEERS PLANNERS ARCHITECTS SURVEYORS SOFTWARE DEVELOPERS

NO.	DATE	REVISIONS:

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LOWE'S
Companies, Inc.
LOWE'S OF NORTH EAST



PROPOSED CONDITIONS
BASIN MAP