

CITY OF ALBUQUERQUE



May 17, 2016

Asa Nilsson-Weber, PE
Isaacson & Arfman, PA
128 Monroe NE
Albuquerque, NM 87108

**Re: Broadstone Promenade, Bldg 14, Phase 17
6400 San Pedro NE
Request for Certificate of Occupancy- Transportation Development
Engineer's/Architect's Stamp dated 6-19-14 (D18-D054A)
Certification dated 5-12-16**

Dear Ms. Nilsson-Weber,

Based upon the information provided in your submittal received 5-13-16, Transportation Development has no objection to the issuance of a Permanent Certificate of Occupancy. This letter serves as a "green tag" from Transportation Development for a Permanent Certificate of Occupancy to be issued by the Building and Safety Division.

If you have any questions, please contact Gary Sandoval at (505) 924-3675 or me at (505)924-3991.

Sincerely,

Racquel M. Michel, P.E.
Traffic Engineer, Planning Dept.
Development Review Services

\gs via: email
C: CO Clerk, File



City of Albuquerque

Planning Department
Development & Building Services Division

DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 09/2015)

Project Title: Broadstone Promenade, Tract 1, Santa Monica Place Building Permit #: _____ City Drainage #: D18-D054A

DRB#: _____ EPC#: _____ Work Order#: 689984

Legal Description: Tract 1, Santa Monica Place

City Address: _____

Engineering Firm: Isaacson & Arfman, P.A. Contact: Asa Nilsson-Weber

Address: 128 Monroe Street NE - Albuquerque, NM 87108

Phone#: (505) 268-8828 Fax#: _____ E-mail: asaw@iacivil.com

Owner: DRI TSM, LLC - c/o Titan Development Company Contact: Kurt Browning

Address: 6300 Riverside Plaza Lane NW, Suite 200 - Albuquerque, NM 87120

Phone#: (505) 998-0163 Fax#: _____ E-mail: _____

Architect: _____ Contact: _____

Address: _____

Phone#: _____ Fax#: _____ E-mail: _____

Other Contact: Surv-Tek, Inc. Contact: Russ P. Hugg

Address: 9384 Valley View Drive - Albuquerque, NM 87114

Phone#: (505) 897-3366 Fax#: _____ E-mail: _____

Check all that Apply:

DEPARTMENT:

- ☐ HYDROLOGY/ DRAINAGE
☒ TRAFFIC/ TRANSPORTATION
☐ MS4/ EROSION & SEDIMENT CONTROL

CHECK TYPE OF APPROVAL/ACCEPTANCE SOUGHT:

- ☐ BUILDING PERMIT APPROVAL
☒ CERTIFICATE OF OCCUPANCY

TYPE OF SUBMITTAL:

☒ ENGINEER ARCHITECT CERTIFICATION **PHASE 17, BLDG 14**

- ☐ CONCEPTUAL G & D PLAN
☐ GRADING PLAN
☐ DRAINAGE MASTER PLAN
☐ DRAINAGE REPORT
☐ CLOMR/LOMR

- ☒ TRAFFIC CIRCULATION LAYOUT (TCL)
☐ TRAFFIC IMPACT STUDY (TIS)
☐ EROSION & SEDIMENT CONTROL PLAN (ESC)

☐ OTHER (SPECIFY) _____

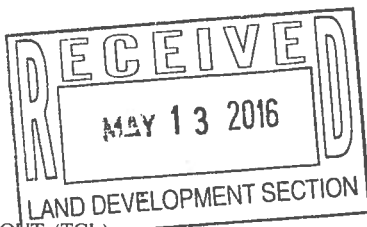
- ☐ PRELIMINARY PLAT APPROVAL
☐ SITE PLAN FOR SUB'D APPROVAL
☐ SITE PLAN FOR BLDG. PERMIT APPROVAL
☐ FINAL PLAT APPROVAL
☐ SIA/ RELEASE OF FINANCIAL GUARANTEE
☐ FOUNDATION PERMIT APPROVAL
☐ GRADING PERMIT APPROVAL
☐ SO-19 APPROVAL
☐ PAVING PERMIT APPROVAL
☐ GRADING/ PAD CERTIFICATION
☐ WORK ORDER APPROVAL
☐ CLOMR/LOMR

- ☐ PRE-DESIGN MEETING
☐ OTHER (SPECIFY) _____

IS THIS A RESUBMITTAL?: ☐ Yes ☒ No

DATE SUBMITTED: May 12, 2016 By: Asa Nilsson-Weber

COA STAFF: _____ ELECTRONIC SUBMITTAL RECEIVED: _____





Isaacson & Arfman, P.A.

Consulting Engineering Associates

*Thomas O. Isaacson, PE (Ret.) & LS (Ret.) * Fred C. Arfman, PE * Åsa Nilsson-Weber, PE*

Date: May 12, 2016

Project: Broadstone Promenade
7400 San Pedro Blvd, NE

Subject: Rolling TCL Certification; Phase 17, Building No: 14

TRAFFIC CERTIFICATION

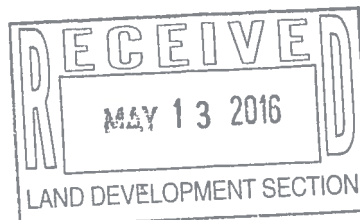
I, Åsa Nilsson-Weber, PE, NMPE No. 17631, of the firm Isaacson & Arfman, PA, hereby certify that the portion of the site as identified on the attached TCL plan is in substantial compliance with and in accordance with the design intent of the TCL Plan dated June 19, 2014. The record information edited onto the original design document has been obtained by Russ Hugg of the firm of Surv-Tek, Inc. I further certify that I or someone under my direct supervision have personally visited the project on May 9, 2016 and have determined by visual observations that the survey data provided is representative of actual site conditions and is true and correct to the best of my knowledge and belief.

Area of modification:

1. The median at parking north of Building 14 was shifted east.

This certification is submitted in support of a request for a Permanent Certificate of Occupancy.

The record information presented hereon is not necessarily complete and intended only to verify substantial compliance of the traffic aspects of this project. Those relying on the record document are advised to obtain independent verification of its accuracy before using it for any other purpose.



Åsa Nilsson-Weber, PE No. 17631

FILE: M:\PROJECTS\2000-2099\2025\DWG\BUILDING PERMIT\2025 CP 1.1.dwg USER: Aso DATE: Jun, 19 2014 TIME: 11:56 am

SAN PEDRO BLVD

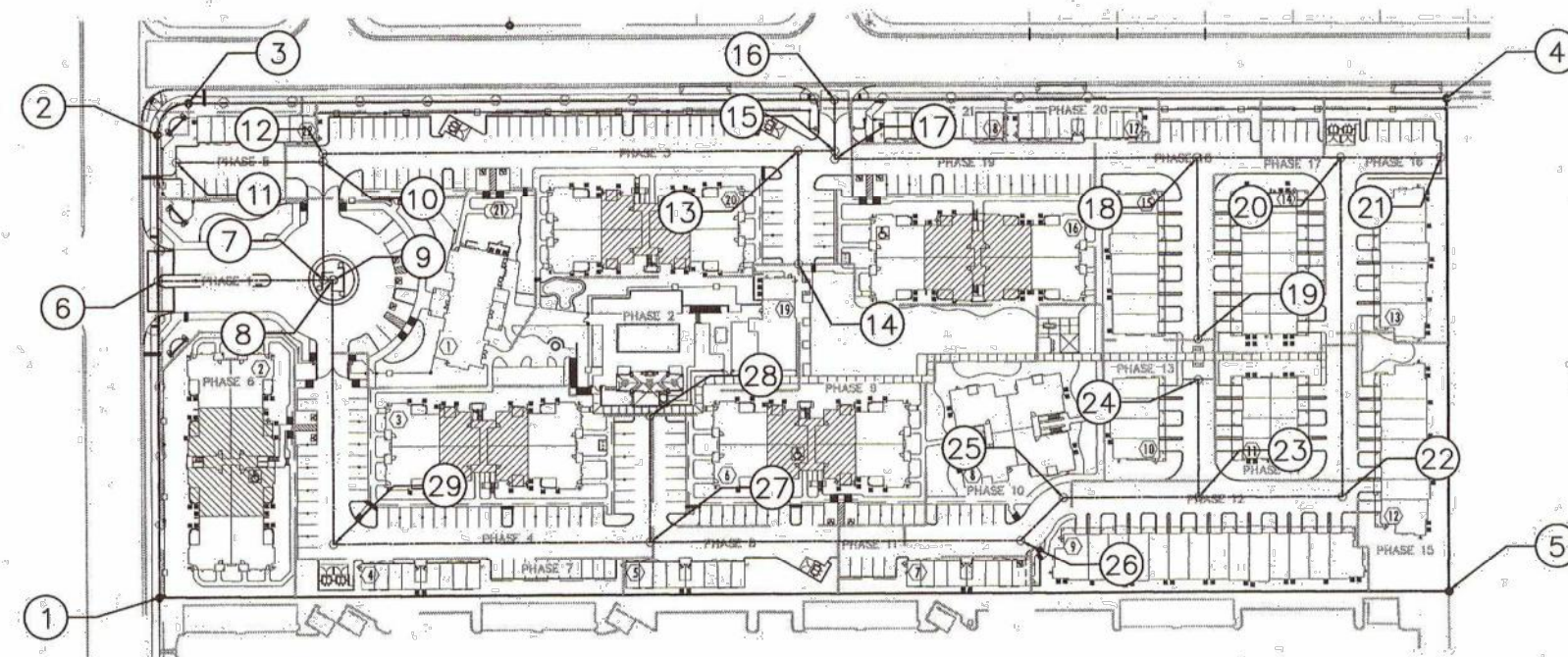
DERICKSON AVE

PHASE A

PHASE B

Point	X	Y	Point	X	Y
1	1543067.9333	1515291.0778	16	1543609.5935	1515680.7531
2	1543069.4858	1515658.8550	17	1543609.3200	1515633.7538
3	1543094.6310	1515683.7491	18	1543897.8801	1515632.0750
4	1544097.5754	1515677.9141	19	1543897.2700	1515487.5503
5	1544095.9172	1515285.0971	20	1544012.3783	1515631.4089
6	1543068.9932	1515542.1587	21	1544092.3180	1515630.9438
7	1543200.6586	1515541.6030	22	1544011.2373	1515361.0911
8	1543207.6586	1515541.5734	23	1543896.6392	1515361.7578
9	1543208.8252	1515541.5685	24	1543897.1819	1515455.0504
10	1543201.0576	1515636.1291	25	1543788.9432	1515362.3843
11	1543084.4120	1515636.8077	26	1543754.7137	1515329.0829
12	1543201.0851	1515642.6291	27	1543459.7070	1515330.7992
13	1543580.1306	1515640.4238	28	1543460.2858	1515430.2975
14	1543579.6041	1515549.9254	29	1543206.7751	1515332.2707
15	1543609.3578	1515640.2538			

NMSP COORDINATE TABLE



NMSP COORDINATE PAIR/BEARING-DISTANCE EXHIBIT
SCALE: 1"=150'

COORDINATE/CONTROL POINTS:

POINTS ARE SITUATED AT THE ACCESS ISLE CENTERLINE INTERSECTION, CENTERLINE INTERSECTION OR THE ENTRIES WITH THE PROJECT BOUNDARY AND AT THE DEAD-END ACCESS ISLE CENTERLINE INTERSECTION WITH THE FACE OF CURB AT THE ISLE TERMINUS

KEYED NOTES

- ENTRANCE VALLEY GUTTER & PAVING BY PUBLIC WORK ORDER.
- PCC PINNED CURB, SEE DETAIL SHEET CP5.1.
- 6" COLORED PCC SIDEWALK, COLOR PER ARCHITECTURAL PLANS.
- HEADER CURB (COLORED), SEE DETAIL SHEET CP5.1.
- REFUSE ENCLOSURE W/ SET PAVING PER ARCHITECTURAL.
- 6" CONCRETE SIDEWALK, SEE DETAIL SHEET CP5.1.
- 6" CONCRETE SIDEWALK W/ TURN-DOWN EDGE, PER ARCHITECTURAL.
- ADA RAMP, SEE ARCHITECTURAL PLANS.

PHASING NOTES

PHASE A PAVING, SIDEWALKS, RAMPS, DUMPSTERS SHALL BE CONSTRUCTED PRIOR TO CERTIFICATION FOR THE FOLLOWING BUILDINGS (PHASES):
BUILDING NOS. 1-5, 19-22 (PHASES 1-8)
PHASE B PAVING, SIDEWALKS, RAMPS, DUMPSTERS SHALL BE CONSTRUCTED PRIOR TO CERTIFICATION FOR THE FOLLOWING BUILDINGS (PHASES):
BUILDING NOS. 6-18 (PHASES 9-21)

TRAFFIC CIRCULATION LAYOUT
APPROVED

CLB 20 Jun 14
Signed Date

ALL WHEELCHAIR RAMPS LOCATED
WITHIN THE PUBLIC RIGHT OF WAY
MUST HAVE TRUNCATED DOMES.

Public Infrastructure shown
on these plans for information
only and not part of approval.
Separate DRC/Permit approval
and Work Order required.

LANDSCAPING/CLEAR SIGHT NOTE:

LANDSCAPING AND SIGNING WILL NOT
INTERFERE WITH CLEAR SIGHT
REQUIREMENTS. THEREFORE, SIGNS,
WALLS, TREES, AND SHRUBBERY BETWEEN
3 AND 8 FEET TALL (AS MEASURED
FROM THE GUTTER PAN) WILL NOT BE
ACCEPTABLE IN THIS AREA.



0 20 40 80 120
SCALE 1"=40'

LEGEND

- CONCRETE PAVERS, SEE CONCRETE PAVERS SECTION SHEET CP5.1.
- LIGHT DUTY ASPHALT, SEE PAVEMENT SECTION SHEET CP5.1.
- HEAVY DUTY ASPHALT, SEE PAVEMENT SECTION SHEET CP5.1.
- CONCRETE PAVING, SEE PCC PAVEMENT SECTION SHEET CP5.1.
- BUILDING PHASE LINE
- PAVING CONSTRUCTION PHASE LINE

GENERAL NOTES

- CONTRACTOR SHALL COORDINATE WITH OWNER FOR ALLOWABLE STAGING AREAS DURING CONSTRUCTION.
- FOR DRAINAGE DETAILS, SEE CIVIL DRAWINGS AND SPECIFICATIONS.
- FOR IRRIGATION DETAILS, SEE IRRIGATION DRAWINGS AND SPECIFICATIONS.
- FOR ADDITIONAL INFORMATION ON SITE FEATURES SEE ARCHITECTURAL SITE PLAN.

PARKING SPACE TABLE

	STANDARD	ACCESSIBLE
TOTAL PARKING SPACES REQUIRED:	331	
TOTAL PARKING SPACES PROVIDED:	357	
TOTAL BICYCLE SPACES REQUIRED:	90	
TOTAL BICYCLE SPACES PROVIDED:	120	

**BROADSTONE
PROMENADE**
6400 SAN PEDRO DRIVE NE
Albuquerque, New Mexico

Office of Rich Barber
ORB
Architecture, LLC
WorldHQ@ORBArch.com

ASA M. WILSSON-WEBER
NEW MEXICO
17631
Professional Engineer
6-19-14

TITAN
DEVELOPMENT

ALLIANCE
RESIDENTIAL COMPANY

**ISAACSON &
ARFMAN, P.A.**
Consulting Engineering Associates
Ph. 505-268-8828 www.isaacson.com
2025 CP 1.1.dwg Jun 19, 2014

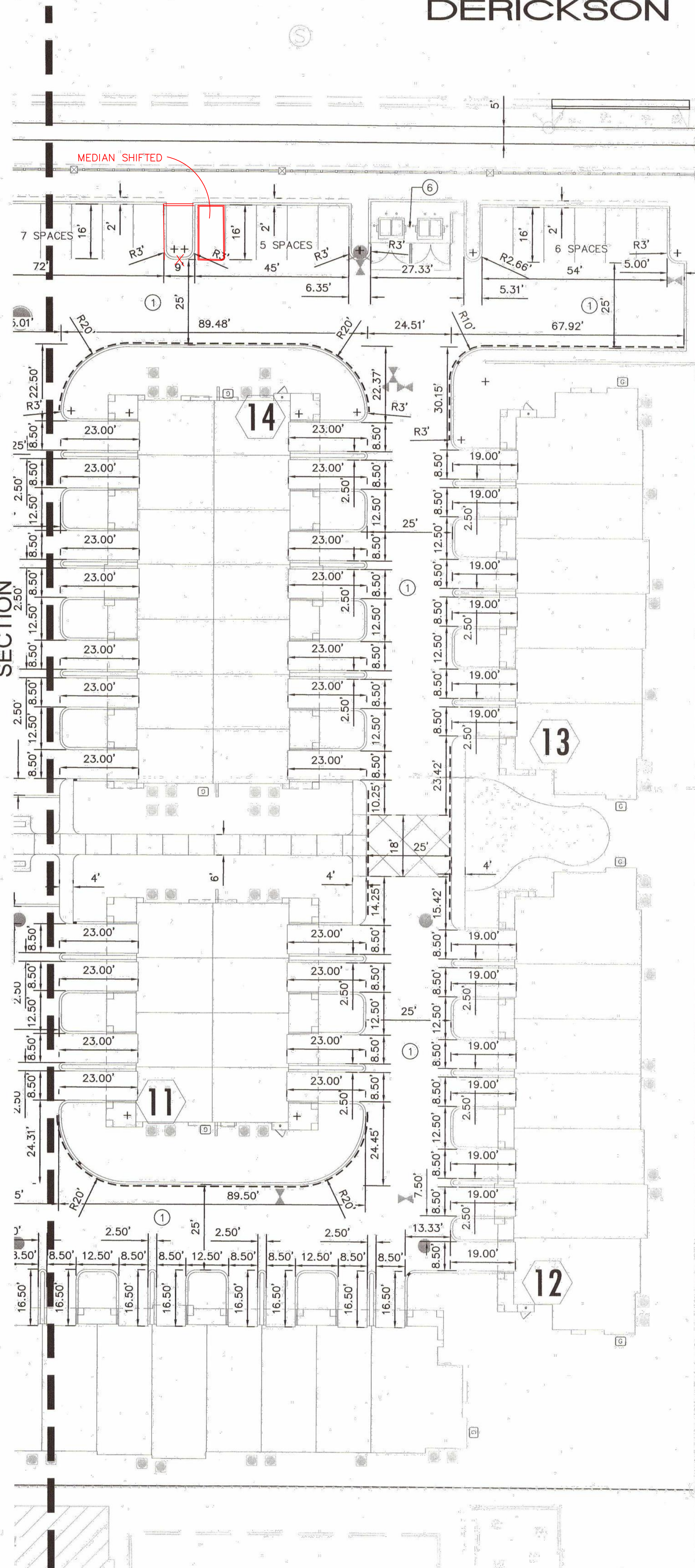
Contractor must verify all dimensions at
project before proceeding with this work.
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DATE: JUNE, 2014 ORB # 13-220

CP1.1
JUN 19 2014
SITE PAVING PLAN
LAND DEVELOPMENT SECTION





- A. BASIS OF BEARINGS: PLAT OF LOTS 1-4, SANTA MONICA PLACE; (BK. 2012C, PG. 0002; REC: 01-06-2012).
- B. COORDINATE PAIRS ARE NEW MEXICO STATE PLANE COORDINATES, CENTRAL ZONE.
- C. SEE ARCHITECTURAL DETAIL SHEET, A1.20 FOR SITE STRIPING AND ACCESSIBILITY DETAILS.
- D. ALL STANDARD PARKING SPACES ARE 9'-0" WIDE BY 16'-0" DEEP UNLESS OTHERWISE NOTED.
- E. ALL ACCESSIBLE PARKING STALLS ARE 11'-0" WIDE BY 18'-0" DEEP.
- F. ALL DIMENSIONING IS TO FACE OF CURB/EDGE OF TURNDOWN SIDEWALKS/ENTRANCE OF PARKING STALLS.
- G. REMOVE ALL UNUSED CURB CUTS WITHIN THE PUBLIC ROW AND REPLACE WITH C&A STD CURB & GUTTER AND PCC SIDEWALKS AS SHOWN ON THE PUBLIC WORK ORDER DRAWINGS.
- H. ALL IMPROVEMENTS SHOWN IN THE PUBLIC ROW TO BE CONSTRUCTED PER PUBLIC WORK ORDER DRAWINGS.
- I. ALL ONSITE SIDEWALKS ARE 4' IN WIDTH UNLESS OTHERWISE NOTED/DIMENSIONED.

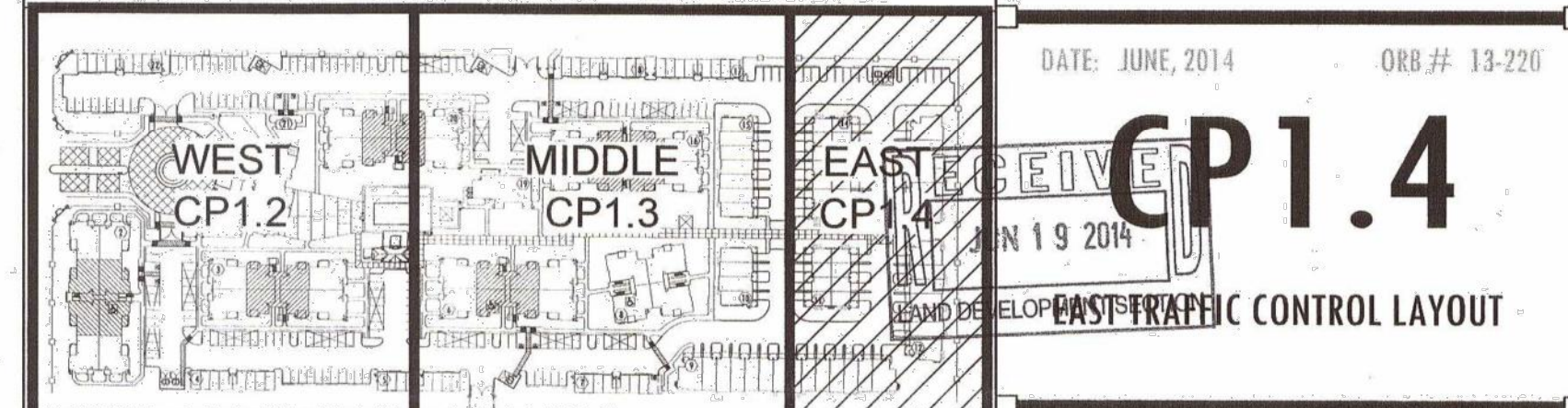
○ KEYED NOTES

1. PAVED AREAS, SEE PAVING PLAN FOR LIMITS AND TYPE OF PAVING.
2. ADA RAMP, SEE DETAIL ON ARCHITECTURAL PLANS.
3. ACCESSIBLE PARKING SIGN, SEE DETAIL ON ARCHITECTURAL PLANS.
4. ACCESSIBLE PARKING STALL, SEE DETAILS ON ARCHITECTURAL PLANS.
5. SINGLE TRASH ENCLOSURE.
6. DOUBLE TRASH ENCLOSURE.
7. ADA ACCESSIBLE RAMP WITH TRUNCATED DOME MATS, SEE PUBLIC WORK ORDER DRAWINGS.

LEGEND

-  4" WIDE PAINTED (BLUE) PARKING STRIPES,
3' ON CENTER.
-  PAINT CURB RED & STENCIL WHITE TEXT
"FIRE LANE NO PARKING", 4" HIGH LETTERS,
1/2" STROKE.

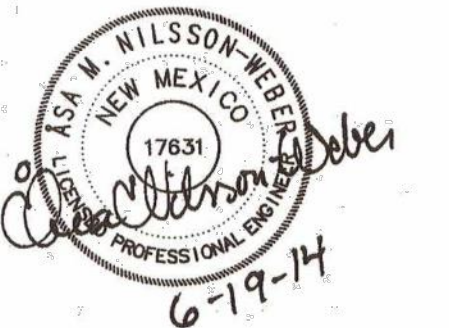
TRAFFIC CONTROL SHEET KEY



BROADSTONE PROMENADE



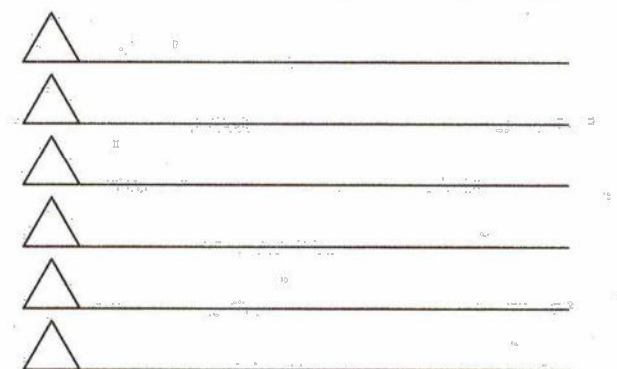
WorldHQ@ORBArch.com



Contractor must verify all dimensions at project before proceeding with this work.

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DATE: JUNE, 2014 ORB # 13-220

CP 1.4

EAST TRAFFIC CONTROL LAYOUT