
Traffic Impact Study

I-25 & San Mateo Commercial Development

Albuquerque, NM

May 2024

Prepared for:

JAMA Holdings Inc.
P.O. Box 122808
Ft. Worth, TX 76121-2808

Prepared by:



Civil Transformations Inc.
2929 Coors Blvd. NW, Suite 309
Albuquerque, NM 87120

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The technical material and data contained in this document were prepared by the undersigned, whose seal as a Professional Engineer, licensed to practice in the State of New Mexico, is affixed below.

A handwritten signature in blue ink that reads "Timothy D. Simmons".

Timothy D. Simmons, PE, PTOE
05/17/2024



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1.0 EXECUTIVE SUMMARY

1.1 Purpose of Study

This report documents the results of a Traffic Impact Study (TIS) for a proposed commercial land development located in the northwest quadrant of the San Mateo Blvd. and Pan American NE intersection in Albuquerque, NM. The purpose of this TIS is to assess traffic operations associated with traffic generated by this proposed project on the adjacent transportation network, specifically Osuna Rd./San Mateo Blvd.

This study evaluated both the existing and proposed conditions of Osuna/San Mateo from the I-25 Southbound Frontage Road (SBFR) through Pan American NE and two adjacent shopping center site entrances, and was conducted in accordance with the City of Albuquerque Development Process Manual (COA DPM) Article 7-5, Traffic Studies. Study requirements were established through a traffic scoping meeting held with the COA Traffic Engineer and New Mexico Department of Transportation (NMDOT) District 3 Traffic Engineering staff (see scoping meeting minutes, Appendix A).

1.2 Principal Findings

Key findings of this analysis are summarized as follows:

1. The Osuna/San Mateo corridor at I-25 acts serves commuter access to I-25 and local access to adjacent commercial and residential areas.
2. For existing (baseline) conditions, Levels of service (LOS) at traffic signalized intersections along Osuna/San Mateo are within the acceptable range.
3. Minor geometric modifications at the San Mateo/Pan American NE intersection will be needed to accommodate the proposed development.
4. Sustained traffic growth in the region may intensify heavy directional movements and necessitate improvements by Year 2035 including:
 - a. Adjust traffic signal timings to improve cross-street operations while maintaining throughput and traffic progression.
 - b. Intersection improvements consisting of lane designations at the I-25 SBFR to add turn lane capacity and/or extend queue storage.
5. Site-generated traffic is not expected to cause significant, adverse impacts to the adjacent roadway network, including the traffic signalized intersections along Osuna/San Mateo, but would incrementally increase delay.

1.3 Recommendations

This analysis has demonstrated that significant detrimental traffic impacts associated with the proposed commercial development project are not expected to occur on the adjacent transportation system. The following recommendations are offered regarding on- and off-site access modifications.

1.3.1 On-Site Access

1. Two access driveways are proposed to provide ample circulation and queueing space on site for the drive-through businesses and retail center as illustrated in Figure 2 and detailed in Section 7.1 of this report.
2. These driveways will be designed in accordance with City of Albuquerque Development Process Manual (COA DPM) requirements.

1.3.2 Off-Site Roadway Improvements**San Mateo/Pan American NE Intersection.**

1. Rehabilitate or reconstruct pavement and provide 100' left-turn plus transition with pavement markings.
2. Verify southbound right turns (SBR) into the site can accommodate turning trucks or enlarge return radius if necessary.
3. Reconstruct southeast quadrant to COA standards with curb & gutter, sidewalk with access ramps, and traffic signal and lighting modifications.
4. Designate pedestrian access route (PAR) for crossing the south San Mateo approach and prohibit pedestrian traffic westward along the south side of San Mateo toward the I-25 northbound frontage road (NBFR).
5. Provide pedestrian access to the site from the south side of Pan American NE to San Mateo Blvd. as depicted in Figure 2.
6. Submit waiver request for SBR lane on San Mateo at Pan American NE.

Osuna/I-25 Southbound Frontage Road (SBFR) Intersection.

1. Implement future mitigation measures as recommended in §7.4.2 of this report:
 - a. Optimize signal timings to delay and improve level of service (LOS).
 - b. Evaluate the need for conversion of the outer southbound through lane into a shared through and right-turn lane as future traffic operations may dictate.

See Section 7 of this report for further details regarding these recommendations.

2.0 INTRODUCTION**2.1 Description of Proposed Project**

The project consists of a proposed multi-tenant commercial development. Construction is projected to commence in year 2024 and is slated for opening by 2025.

2.2 Project Location

The project is located in north Albuquerque at the southeast quadrant of the Interstate 25 (I-25) interchange as depicted in Figure 1.

2.3 Study Area

The study area encompasses the site and adjacent roadways, Osuna Rd./San Mateo Blvd. from north of Academy Rd. to the I-25 Southbound Frontage Rd. (SBFR). Included in the analysis are the traffic signalized intersections of San Mateo/I-25 West Ramp, San Mateo/I-25 East Ramp, and San Mateo/Pan American as well as the unsignalized intersections of Pan American/Harper Dr. and two commercial sites.

2.4 Scope of Analysis

Primary tasks incorporated into this analysis include:

- A. **Data Collection** – including traffic volume counts, other roadway network parameters, and regional data for the traffic analysis.
- B. **Traffic Operations Analysis** – utilizing the collected data, computerized models were developed in *Synchro 12* software for analysis utilizing *Highway Capacity Manual (HCM)* procedures.
- C. **Geometric Evaluation** – consideration of safe access measures such as auxiliary lanes and/or access geometry to mitigate traffic impacts, if necessary.

Figure 1: Vicinity Map



2.5 Planned Developments or Projects in the Vicinity

No imminent development projects were identified for reference in this study.

3.0 STUDY AREA CONDITIONS

3.1 Existing Land Use & Zoning

The site is currently vacant, and zoning is not designated as the property lies within the New Mexico Department of Transportation (NMDOT)-owned right-of-way (ROW) for the Interstate 25 (I-25) corridor. Adjacent land uses are developed with mixed commercial uses including retail strip malls, restaurants, and a hotel. Residential neighborhoods are located north of the site.

3.2 Other Known Projects

No pending development projects in the area were identified during the project scoping review. Recent traffic signal modifications at the I-25/San Mateo interchange were made that have been incorporated into this analysis. These consist of the addition of flashing yellow arrow (FYA) operations for the dual left-turn lanes between interchange ramps and the addition of a lead pedestrian interval (LPI) at the east frontage road.

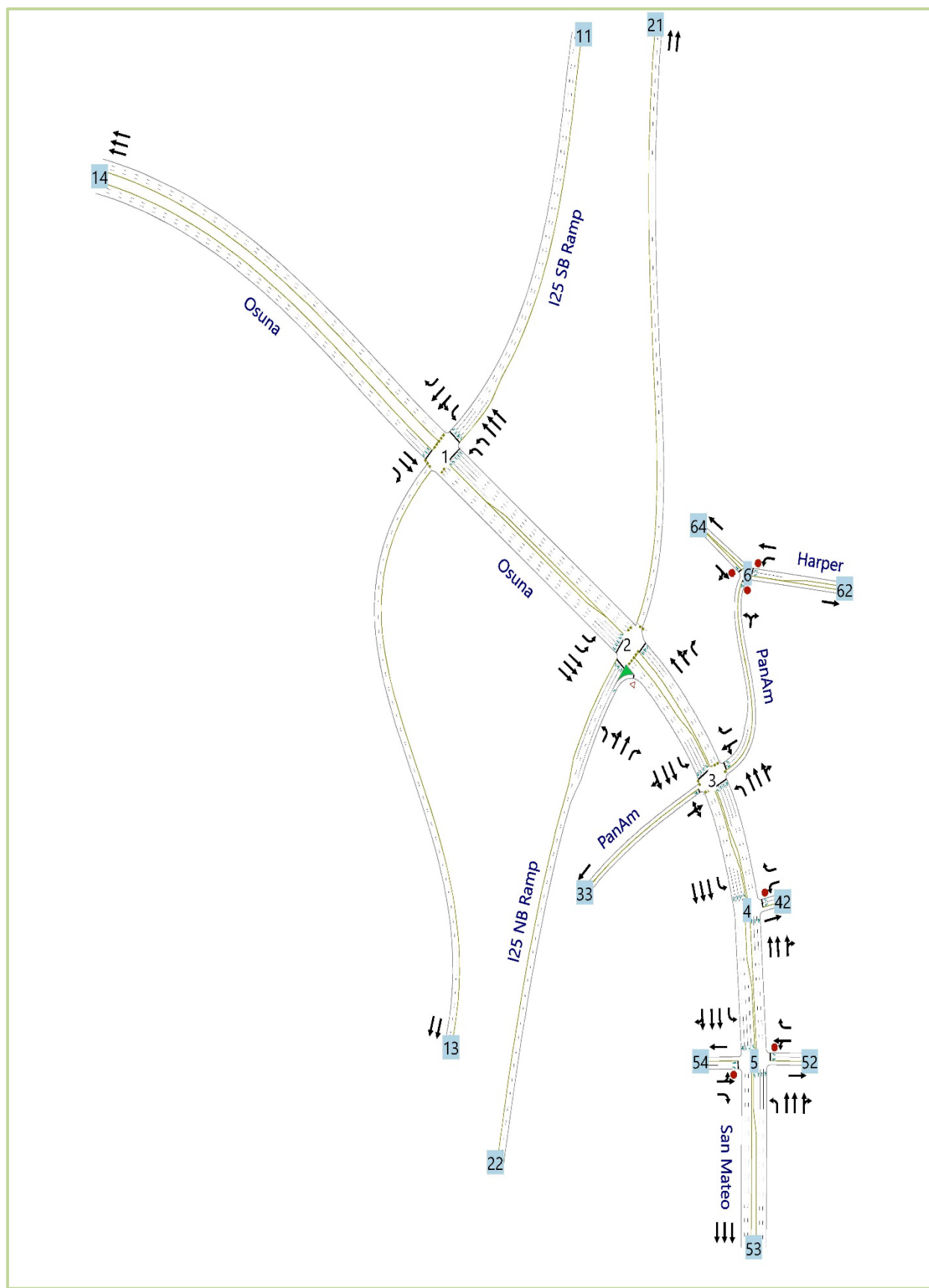
3.3 Existing Roadway System

The existing study street network is shown in Figure 1 and described below. These routes are within the jurisdiction of the City of Albuquerque (COA) and with designations as shown on the Functional Classification in the Albuquerque Metropolitan Planning Area by the Mid-Region Council of Governments (MRCOG). The study network is illustrated graphically in Figure 2.

3.3.1 Osuna Rd.

Designated as a Principal Arterial, the route has a posted speed of 45 mph and consists of six travel lanes, a raised median, curb & gutter, and concrete sidewalks along the north side. Intersection approach or spot lighting exists at the signalized intersections. Average Weekday Traffic (AWDT) on Osuna Rd. west of I-25 was 22,708 in year 2022.

Figure 2: Existing Roadway Network



3.3.2 San Mateo Blvd.

A Principal Arterial roadway with AWD_{T2022} of 48,026 east of I-25, the roadway consists of 6 travel lanes, raised median, curb & gutter, and concrete sidewalks with a posted speed of 40 mph.

3.3.3 Pan American NE

A 2-lane Major Collector with curb & gutter and sidewalks at developed parcels, the AWD_T in 2022 was 6,649. This formerly served as the I-25 northbound (NB) off-ramp, and the right-of-way (ROW) west of San Mateo is presently owned by NMDOT. It was treated as an east-west route in this analysis.

3.3.4 Harper Dr.

A 2-lane Major Collector with a posted speed of 30mph, curb & gutter and sidewalks, and a continuous two-way left-turn lane (TWLTL). The AWD_T in 2022 was 3,145.

3.3.6 Interstate 25

I-25 has 8 general purpose travel lanes plus auxiliary lanes approaching the interchange with a posted at 65 mph and AWD_{T2022} of 67,000 southbound and 65,995 northbound. The adjacent urban interstate frontage roads provide access from the interstate ramps to adjacent properties/roadways and consist of:

1. **Pan American Fwy West (I-25 Southbound Frontage Rd., SBFR):** Four travel lanes, shoulders, posted speed 45 mph, and AWD_{T2022} of 8,180. The southbound (SB) approach to Osuna Rd. consists of the following lanes: Right, Thru, Thru/Left, and Left.
2. **Pan American Fwy West (I-25 Northbound Frontage Rd., NBFR):** Four travel lanes, shoulders, posted speed 45 mph, and AWD_{T2022} of 15,283. The northbound (NB) approach to San Mateo Blvd. consists of the following lanes: Right, Thru, Thru/Left, and Left.

3.5 Transit Service

Commuter route 93 traverses the interchange along with fixed routes 140 and 141 which run along San Mateo and Osuna. Bus stops exist on both sides of San Mateo south of Pan American NE.

3.6 Bicycle & Pedestrian Facilities

No bike lanes exist along San Mateo. Sidewalks are present as described in Section 3.3.

4.0 ANALYSIS OF EXISTING CONDITIONS**4.1 Baseline Traffic Data**

Historic traffic volumes were obtained from the NMDOT Transportation Data Management System via the MS2 web host (accessed via <https://nmdot.public.ms2soft.com/tcds/tsearch.asp?loc=nmdot>) and are summarized in Appendix B. In addition, manual turning movement counts (TMCs) were also conducted for use in analyzing traffic operations and intersection capacity for the study intersections. These were collected on Wednesday, August 16, 2023 during the AM peak period (0700-0900) and PM peak period (1600-1800) correlating with trip generation estimates. Because the signalized intersections along Osuna/San Mateo comprise a coordinated network, combined peak hour periods for the corridor were established as beginning at 0715 and 1630 hours (4:30 p.m.), respectively.

Traffic data available from MRCOG showed that these signalized intersections were not saturated. Also, for each 15-minute interval recorded at the intersections as part of this study, traffic volumes were serviced by the signals and thus the recorded volumes represent demand volumes. Furthermore, residual queues were typically cleared in subsequent signal cycles and thus a multi-period analysis was not required for this study.

Existing peak hour traffic volumes used for this analysis are presented in Figures 3 and 4, representing baseline conditions. Detailed reports of the TMC data and supporting volume counts are contained in Appendix B. Graphic exhibits depicting the roadway network and TMCs are provided in Appendix E.

4.2 Existing Roadway Intersection Capacity

An analysis of the study intersections was conducted for the existing baseline conditions as described in Section 6.1 of this report. These were analyzed with the existing traffic signal timing plans as provided in Appendix B. The major intersections operate at satisfactory levels of service (LOS) B or above as summarized in Table 5 (see Appendix E for LOS worksheets).

4.3 Crash Analysis

Crash records within the study area were requested from the NMDOT Traffic Safety Division's database for the most-recent 5-year period available at the time of publication to evaluate potential safety issues at the four study intersections (year 2022 data were not available at the time of the request). The focus for this study was on a more generalized review of recurring crash patterns that could pose safety concerns relative to the proposed project and that could be addressed in the design stage of project development.

The raw crash data spreadsheets are contained in Appendix C with the most relevant columns displayed for clarity. These were tabulated in separate summary sheets by intersection to better evaluate crash classifications and contributing causes with the total for all signalized intersections summarized in Table 1 (it is noted that probable cause factors were discontinued from the records after 2019).

Overall, there has been a decrease in the total number of crashes through the evaluation period as illustrated in Figure 5. Following is a summary of the three signalized study intersections:

1. Top three crash classifications overall included:
 - a. Sideswipe – Same, Opposite Direction combined (32%)
 - b. Right Angle (30%)
 - c. Left Turn (16%)
2. The top three contributing factors* overall included:
 - a. Improper Driving (23%)
 - b. Driver Inattention (21%)
 - c. Failure to yield right-of-way (21%)
3. *Probable cause factors were discontinued from the crash records after 2019.
4. The vast majority of the crashes occurred during daylight hours, and during clear or dry conditions 91% of the time.
5. Only one pedestrian and one bicycle injury crash were recorded in this data set, amounting to less than 1% of the total.
6. No fatalities were identified in this evaluation.

Crash summaries for the individual study intersections are tabulated in Appendix C, with specific issues identified as outlined following Figure 5.

The diagram illustrates a complex highway interchange with the following components:

- I-25 SB Ramp:** Located at the top, with lane numbers 11, 21, and 22. Lane counts include 13895, 6035, 220, 338, 254, 189, and 266.
- I-25 NB Ramp:** Located on the left, with lane numbers 13, 22, and 33. Lane counts include 264, 1420, 446, 213, 249, 1316, 839, 162, 200, 157, 150, 220, 1557, 42, 30, 2332, 1483, 9, 52, 0, 1, 4, 54, 52, 13, 222, 81, 290, 13, and 53.
- Osuna:** A major road running diagonally across the center, with lane numbers 14, 13, and 21. Lane counts include 13895, 6035, 220, 338, 254, 189, and 266.
- PanAm:** A road running horizontally across the middle, with lane numbers 33, 3, 4, and 5. Lane counts include 446, 213, 249, 1316, 839, 162, 200, 157, 150, 220, 1557, 42, 30, 2332, 1483, 9, 52, 0, 1, 4, 54, 52, 13, 222, 81, 290, 13, and 53.
- Harper:** A road running horizontally at the bottom, with lane numbers 64, 42, 233, 62, and 6. Lane counts include 100, 40, 24, 861, 1866, 162, 200, 157, 150, 220, 1557, 42, 30, 2332, 1483, 9, 52, 0, 1, 4, 54, 52, 13, 222, 81, 290, 13, and 53.
- San Mateo:** A road running vertically on the right, with lane numbers 54, 52, 13, 222, 81, 290, 13, and 53. Lane counts include 1483, 9, 52, 0, 1, 4, 54, 52, 13, 222, 81, 290, 13, and 53.

Figure 4: PM Peak Hour Volumes (Baseline)

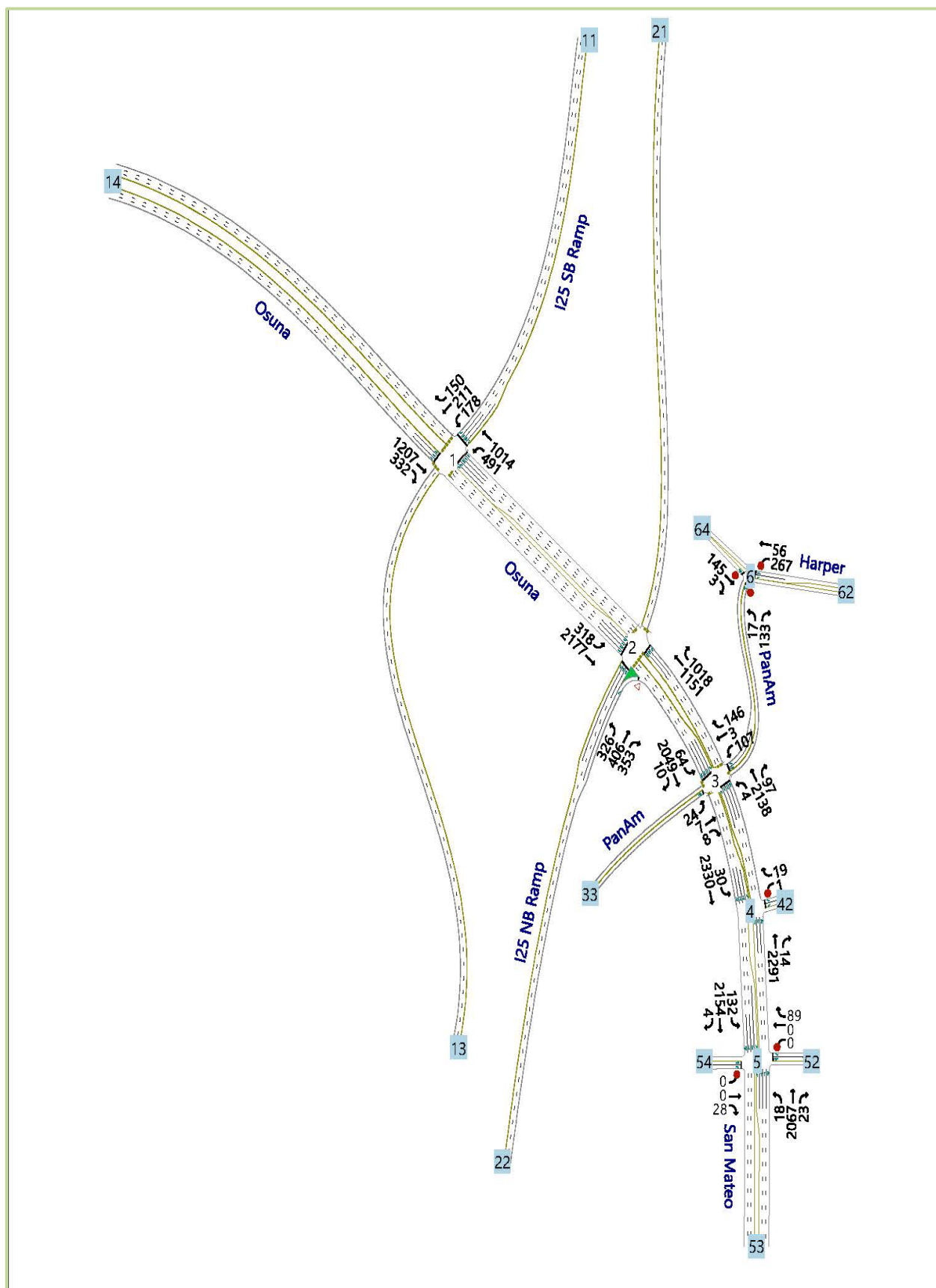
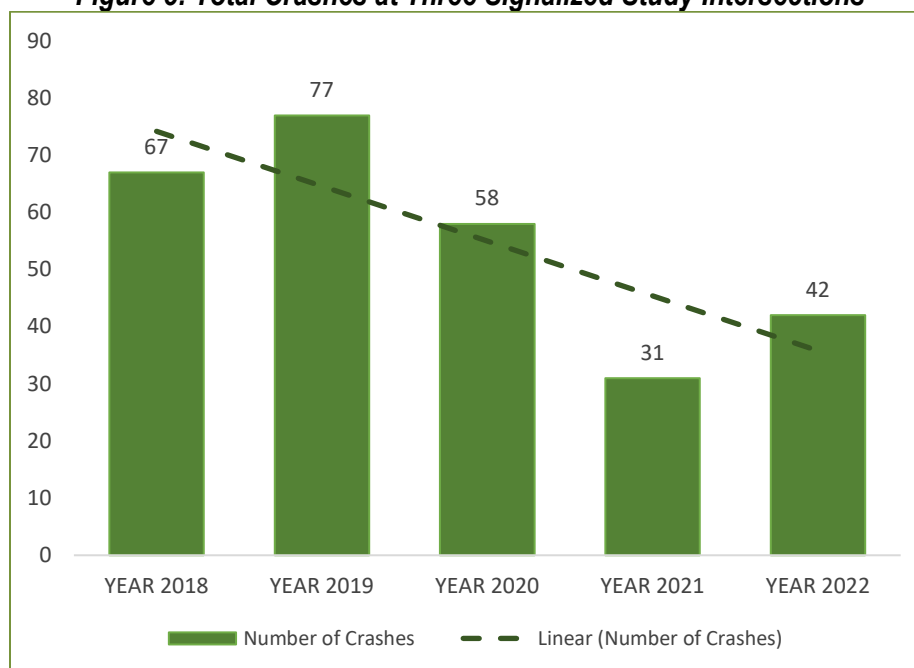


Table 1: Summary of Crashes at Three Signalized Study Intersections

ROUTE Osuna/San Mateo MP # TO MP #	YEAR 2018		YEAR 2019		YEAR 2020		YEAR 2021		YEAR 2022		TOTAL	
	No. 67	24%	No. 77	28%	No. 58	21%	No. 31	11%	No. 42	15%	No. 275	% 100
CRASH SEVERITY												
Property Damage Only (PDO)	60	71	83	78	48	69	33	70	38	73	262	73
Injury/Non-Fatal	25	29	24	22	22	31	14	30	14	27	99	27
Fatal	0	0	0	0	0	0	0	0	0	0	0	0
CRASH CLASSIFICATION (ACCIDENT TYPE)												
Fixed Object	1	1	4	5	3	5	2	6	0	0	10	4
Right Angle	18	27	18	23	21	36	10	32	15	36	82	30
Rear End	12	18	14	18	2	3	0	0	7	17	35	13
Backing	0	0	0	0	0	0	0	0	0	0	0	0
Sideswipe: Same Direction	9	13	10	13	5	9	4	13	17	40	45	16
Sideswipe: Opposite Direction	6	9	12	16	14	24	12	39	1	2	45	16
Head On	1	1	1	1	1	2	0	0	2	5	5	2
Left Turn	18	27	14	18	11	19	0	0	0	0	43	16
Parked Vehicle/Parking Maneuver	0	0	2	3	0	0	0	0	0	0	2	1
Overturn	0	0	1	1	0	0	1	3	0	0	2	1
Driveway/Driveway Maneuver	0	0	0	0	0	0	0	0	0	0	0	0
Pedestrian/Bicyclist	1	1	1	1	0	0	0	0	0	0	2	1
Other	1	1	0	0	1	2	2	6	0	0	4	1
PROBABLE CAUSE ("HIGHEST CONTRIBUTING FACTOR")*												
Following Too Close	5	6	11	12							16	9
Driver Inattention	17	22	19	20							36	21
Excess Speed/Too Fast For Conditions	7	9	2	2							9	5
Avoid Other Vehicle	4	5	1	1							5	3
Improper Driving	13	17	26	28							39	23
Failure to use Turn Signal	0	0	0	0							0	0
Failure to Yield R.O.W.	19	24	17	18							36	21
Disregard Traffic Control Device	10	13	11	12							21	12
Under Influence Alcohol	0	0	2	2							2	1
Mechanical Defect	3	4	3	3							6	4
Pedestrian Error	0	0	0	0							0	0
Road Defect/Construction Activity	0	0	0	0							0	0
Other	0	0	1	1							1	1
ROAD CONDITIONS												
Dry/Clear	75	95	93	91	62	93	34	87	37	82	301	91
Wet	4	5	6	6	3	4	2	5	2	4	17	5
Snowy/Icy	0	0	2	2	2	3	0	0	1	2	5	2
Other	0	0	1	1	0	0	3	8	5	11	9	3
LIGHTING												
Daylight	64	78	81	79	51	77	35	88	37	74	268	79
Darkness	14	17	18	17	10	15	5	13	10	20	57	17
Dawn or Dusk	4	5	4	4	5	8	0	0	3	6	16	5
SOBRIETY												
Sobriety Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Had Been Drinking/Drug	0	0	2	2	3	4	0	0	3	6	8	2
Had Not Been Drinking/Drug	83	100	104	98	67	96	45	100	49	94	348	98

*Probable cause information excluded from post-2019 records.

Figure 5: Total Crashes at Three Signalized Study Intersections



1. **Node 1 – Osuna / I25 SBFR**

- Total crashes: 2018 = 18, 2019 = 29, 2020 = 22, 2021 = 13, 2022 = 10.
- Top three crash classifications: Right Angle 34%, Sideswipe – Opposite Direction 19%, Left Turn 18%.
- The number of angle and left-turn collisions may be indicative of lengthy delays associated with turning vehicles waiting for adequate gaps in through traffic to make turns. Yellow and All-Red clearance intervals were updated in 2013 to current standard, so the long cycle length and/or lengthy green times for through traffic might lead to this condition.

2. **Node 2 – San Mateo / I25 NBFR**

- Total crashes: 2018 = 61, 2019 = 71, 2020 = 37, 2021 = 13, 2022 = 22.
- Top three crash classifications: Right Angle 26%, Rear End 19%, Sideswipe (Same Dir.) 16%.
- This intersection exhibits similar issues to Node 1 although higher incidences of rear-end collisions likely reflect the heavy turning volumes for the NBR and EBR movements.

3. **Node 3 – San Mateo / Pan American NE**

- Total crashes: 2018 = 6, 2019 = 7, 2020 = 11, 2021 = 21, 2022 = 20.
- Top three crash classifications: Right Angle 35%, Sideswipe – Opposite Direction 25%, Sideswipe – Same Direction 23%.
- Many of the crash analysis fields were left blank, and it is unclear what may have led to a spike in crashes in 2021 and 2022. Contributing factors could include the heavy turning traffic from the “east” approach coupled with imbalanced geometry on the “west” approach of Pan American.

It should be noted that the crash record database typically lags 1-2 years, and data evaluated for this study is approaching 6 years ago for 2018. Nevertheless, this summary and potential mitigating measures may be useful to the NMDOT D3 and COA staff in ongoing planning and engineering activities to improve safety at these study intersections.

5.0 FUTURE TRAFFIC CONDITIONS

5.1 Background Traffic Projection

This project was initiated in 2023 with construction and project opening projected to be completed by year 2025, which was established as the Implementation Year. A Horizon Year analysis typically incorporates the Implementation Year plus 10 years, thus traffic projections up to year 2035 were required.

Forecast traffic volumes for the year 2040 were obtained from the MRCOG travel demand forecast model (TDFM). Figure 6 depicts the applicable region of the TDFM and affected roadway links between nodes representing intersections in the study area. TDFM output for these links by direction are presented in Appendix D (note that several columns were hidden for clarity). Annual growth rates were then calculated between the baseline (2016) and forecast (2040) TDFM scenarios and then applied to the TMCs collected for this study in order to derive the Implementation (2025) and Horizon (2035) Year forecast traffic volumes. Some links showed extraordinary growth rates while others showed negative growth or decline based on the TDFM development trends and resulting travel patterns.

The calculated growth factors were input into the traffic models and applied to the traffic movements by approach to expand background traffic volumes for analysis of the forecast traffic scenarios. It should also be noted that the calculated growth rate was not indicative of statewide economic and related traffic growth but reflected local and regional activity in recent years. The recession associated with COVID-19 pandemic closures could be expected to negatively impact economic growth and therefore these growth rates may not be expected to continue unaltered but may result in a somewhat conservative traffic forecast estimate.

The resulting background traffic volumes used in this analysis are provided graphically in Appendix D in advance of the LOS worksheets and are also tabulated in the LOS summary tables in Section 6.1.

5.2 Proposed Site Development Characteristics

The project is comprised of commercial uses including fast-food restaurants with drive-through windows and a strip retail center as depicted in Figure 7. Lengthy, dual drive-through lanes will provide ample queuing capacity and abundant on-site parking will be provided adjacent to the three buildings. Construction is projected to commence in 2024 and full buildout is assumed to be completed by 2025.

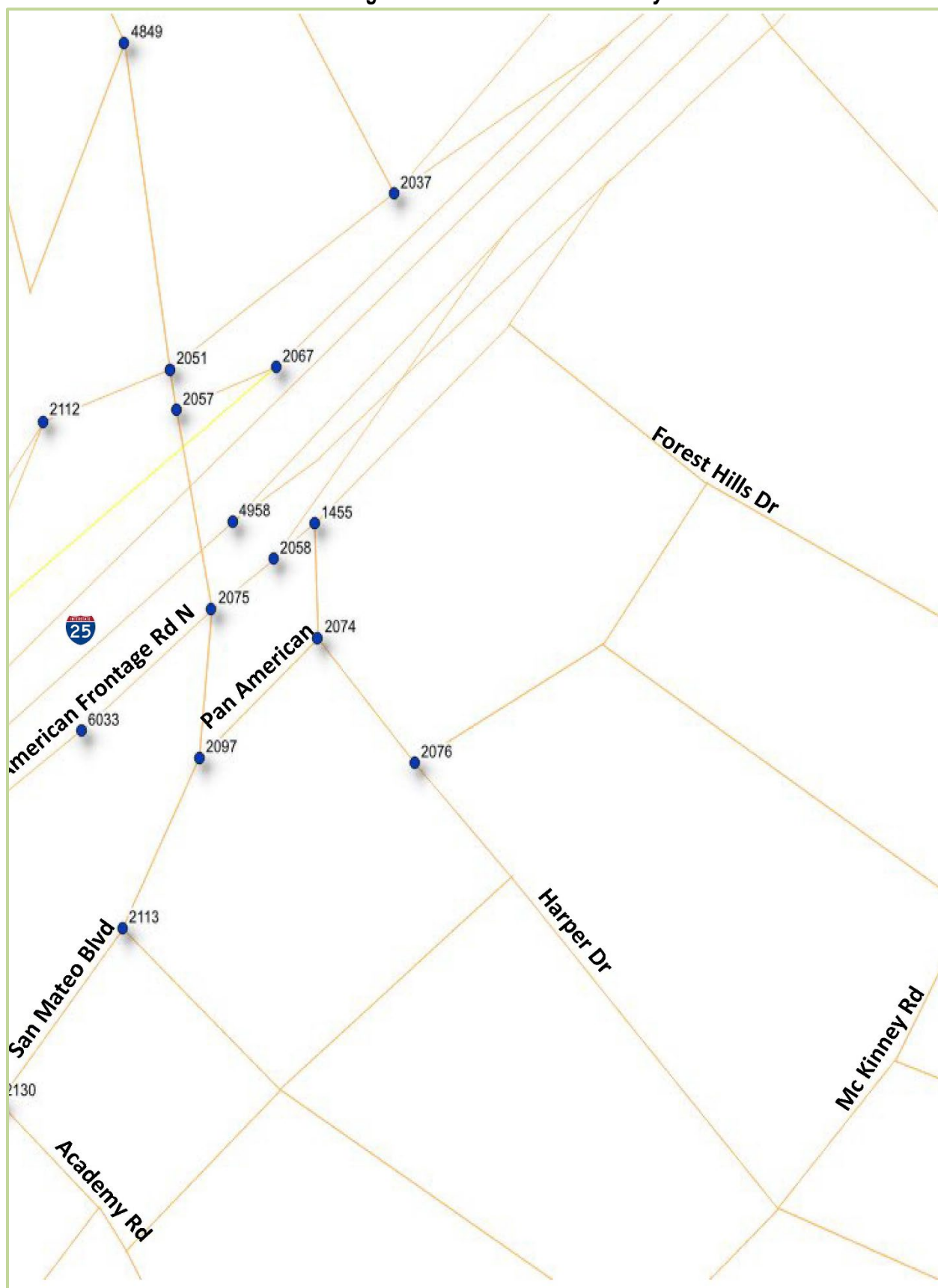
5.3 Site Access and Circulation

As illustrated on Figure 7, access is proposed via two site driveways on Pan American NE west of San Mateo.

1. **Site Drive 1** – full access on Pan American NE across from a Speedway gas station back entrance. This driveway will have over 200' of separation from San Mateo to accommodate queuing and turning movements.
2. **Site Drive 2** – full access on Pan American NE at the south end of the site adjacent to the commercial center on the west side.

These driveways will be considered internal on private property and therefore were not analyzed individually. Drive-through lanes will be physically separated and flow in a counterclockwise direction for driver-side access. Sidewalk access will be provided to San Mateo along the east side of Pan American NE. A sidewalk across the San Mateo frontage is not feasible as no sidewalk exists under the I-25 bridge due to the southbound-to-eastbound (SB-EB) loop ramp.

Figure 6: MRCOG TDFM Roadway Network



Pan American NE provides a back service access to an auto dealership that is used for vehicle service and parking in the public ROW, which will be purchased and incorporated into the project and therefore continued access and/or parking privileges would be allowed through grant of easement.

5.4 Trip Generation

Institute of Transportation Engineers Trip Generation, 11th Edition (ITE, 2021) was used to estimate traffic generated by the proposed development. The peak of the adjacent street was selected for the calculations indicative of urban/suburban traffic patterns for typical AM and PM peak periods (i.e., 7-9 a.m., 4-6 p.m., respectively).

The following steps and assumptions were applied to the data:

1. The “peak hour of the adjacent street traffic” was the criterion selected for the calculations as this would most likely be impacted by site traffic during typical AM and PM peak periods given the characteristics of the proposed land uses. In the vicinity of the site, San Mateo is urbanized and thus “general urban/suburban” data were selected;
2. Gross floor area (GFA) was used for the independent variable;
3. Fitted equations were applied if available, otherwise average rates were used;
4. Internal capture trips (ICT) were calculated using the National Cooperative Highway Research Program (NCHRP) Report 684;
5. Pass-by trips were calculated by ITE methodology and deducted from the existing through traffic;
6. Transit trip reductions were not accounted for as ridership data were not readily available.

The calculated site traffic volume estimates are contained in Appendix D and summarized in Table 2.

Table 2: Estimated Site Trip Generation

Description	ITE Code	Quant.	Units	Daily Total	AM			PM		
					Enter	Exit	Total	Enter	Exit	Total
Bldg. 1: Fast Food w/Drive-Thru	934	2.47	GFA ¹	1155	56	54	110	43	39	82
Bldg. 2: Fast Food w/Drive-Thru	934	2.47	GFA ¹	1155	56	54	110	43	39	82
Bldg. 3: Strip Retail Plaza (<40k)	822	5.69	GFA ¹	310	12	8	20	26	26	52
Total (unadjusted)				2620	124	116	240	112	104	216
-Internal Capture Trips					-2	-2	-4	-21	-21	-42
-Pass-By Trips ³					-54	-54	-108	-41	-41	-82
Volume Added to Adjacent Street					68	60	128	50	42	92

¹Gross Floor Area, per 1,000 square feet.
²Per 1,000 daily vehicles of adjacent street traffic.
³Included with driveway volumes but deducted from through traffic on adjacent street.

5.5 Site Traffic Distribution and Assignment

Socioeconomic data developed as part of the regional travel demand forecast model (TDFM) maintained by the Mid Region Council of Governments (MRCOG) were utilized to distribute site trips in proportion to the population (productions) within a 2-mile radius of the commercial property (attractions) according to the following formula:

$$Ts = (Tt) (Sp) / (Sp)$$

Ts = Development to Individual Subarea Trips
Tt = Total Trips
Sp = Subarea Population


Figure 7: Proposed Site Development Plan

I-25 + SAN MATEO CONCEPTUAL SITEPLAN

ALBUQUERQUE, NEW MEXICO



BUILDING	SQ. FT.	USE/AREA	FACTOR	PARKING REQUIRED	PARKING PROVIDED	INCLUDES HC (REQ) PARKING	PLUS MC (REQ) PARKING	BICYCLE PARKING (REQ)
BLDG 1	2,472 SF	DRIVE THRU	STACKING	12	29 + STACKING	2 (1)	2 (1)	3 (3)
BLDG. 2	2,472 SF	DRIVE THRU	STACKING	12	14 + STACKING	2 (1)	2 (1)	3 (3)
BLDG 3	5,688 SF	RETAIL	41000	23	50	3 (1)	2 (1)	3 (3)
				41	93 + STACKING	1 (3)	6 (3)	9 (9)



MARTIN FM GRUMMER
ARCHITECT
301 WILSON BLVD NE
ALBUQUERQUE, NEW MEXICO 87114
(505) 265-2567

DATE:
4 DEC 2023

DRAWN BY:
MFMG

CHECKED BY:

VERIFIED BY:

REVISIONS

SHEET NO:
A1.1

ALL DIMENSIONS ARE TO BE FIELD VERIFIED. IF THERE ARE DISCREPANCIES, PLEASE NOTIFY THE ARCHITECT. DRAWING ARE NOT TO BE SCALED. USE DIMENSIONS FOR ACCURACY.

The calculations entailed the TDFM baseline (2016) and forecast (2040) populations interpolated to the implementation year (2025). The area distribution map and associated calculations are contained in Appendix D. The resulting trip distribution percentages are tabulated in Table 3.

Table 3: Trip Distribution Summary

Route and Traffic Movement	%
% from/to Pan American FWY north	17%
% from/to Pan American NE northeast	14%
% from/to San Mateo south	45%
% from/to Pan American FWY south	15%
% from/to Osuna west	9%
Total	100%

These distribution percentages were applied to the estimated site trips in Table 2, and proposed trips were then assigned to San Mateo/Osuna via Pan American NE. The distributed trips are shown in **red** text in Figure 8 as well as in the map views and on the “Future Volume” rows in the level of service worksheets contained in Appendix E.

6.0 TRANSPORTATION ANALYSIS

6.1 Traffic Operations Analysis

Intersection “nodes” constrain the capacity of a roadway segment, and therefore the baseline capacity of the study intersections were analyzed using the *Highway Capacity Manual 7th Edition (HCM)* methodology. The Level of Service (LOS) for an intersection is determined by the computed or measured delay and is defined for each minor movement at signalized, unsignalized, and roundabout intersections. LOS is assigned a letter grade from A (best) through F (worst), as summarized in Table 4 for signalized and unsignalized intersections. Signalized intersections have higher levels of delays due to higher volumes and driver expectation of greater delays. LOS D is generally considered acceptable in urban areas with right-of-way constraints; however, COA DPM Table 7.5.88 provides additional LOS criteria by location and corridor type. San Mateo Blvd. lies along a Premium Transit Corridor and the site borders the Far North Activity Center; thus, LOS E is the threshold.

Table 4: Intersection Level of Service (LOS) Criteria (HCM 6th Ed.)

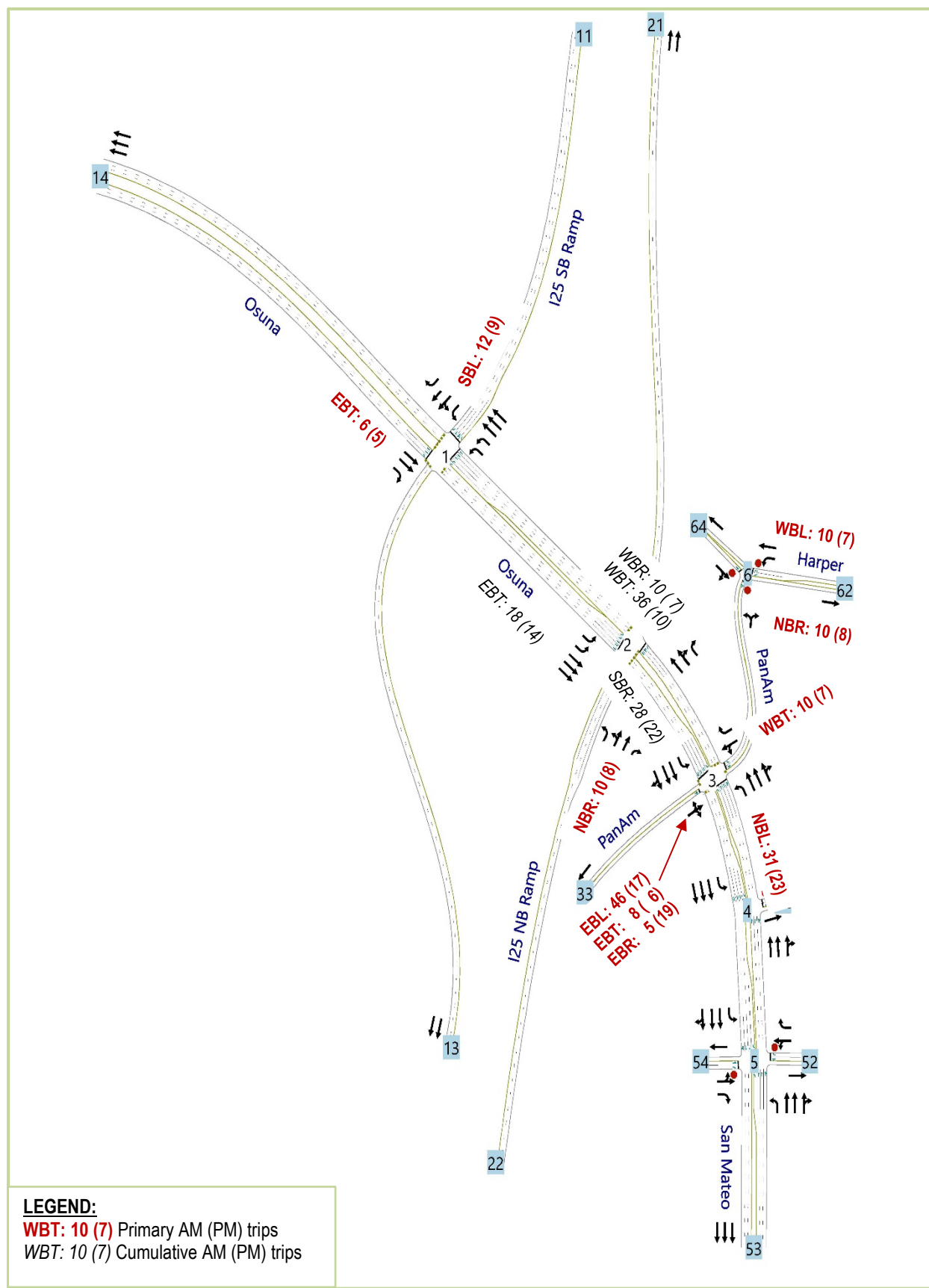
LOS ¹	Signalized Control Delay (sec/veh)	Unsignalized Control Delay (sec/veh)
A	0 – 10	0 – 10
B	10 – 20	10 – 15
C	20 – 35	15 – 25
D	35 – 55	25 – 35
E	55 – 80	35 – 50
F	> 80	> 50

¹For Volume-to-Capacity Ratio (V/C) ≤ 1.0; LOS = F for V/C > 1.

6.1.1 Software

Synchro 12 software package by *Trafficware Ltd.* was utilized to establish the Osuna/San Mateo network and compute the results in HCM format. Synchro provides better functionality for urban traffic network analysis

Figure 8: Site Trip Distribution



and was used to report unsignalized intersection LOS as well as to distribute site trips via the TIA module. Signalized intersections were then exported to *Highway Capacity Software, HCS2024*, which was used to compute LOS for the signalized intersections as dictated by NMDOT District 3 Traffic Engineering staff.

6.1.2 Model Inputs

Following is a summary of the key settings applied for this analysis:

1. Default saturated flow of 1900 pc/h/ln was utilized for the Albuquerque metro area.
2. Peak hour factors (PHF) for each intersection were used rather than a common peak hour for the corridor to replicate demand and provide a slightly conservative analysis.
3. For forecast scenarios, the HCM de facto PHF of 0.92 was applied at a minimum; however, the existing PHF was maintained if greater than 0.92 as these intersections were projected to increase.
4. Traffic signal timing settings for the signalized intersections were provided by COA Traffic Engineering staff and input into the traffic models for analysis.
 - a. These intersections are currently coordinated and therefore the appropriate time-of-day plans were used for the peak analyses and set to Actuated/Coordinated operation.
 - b. Nodes 1 & 2 were treated as east-west intersections with Phase 2 = EB. However, Node 3 (Pan American NE) is a north-south arterial intersection but Phase 2 is NB. While this can be coordinated in *Synchro*, *HCS* is unable to coordinate non-linear arterials and therefore Node 3 had to be analyzed separately.
 - c. Maximum splits were set to Max1 unless otherwise noted.
 - d. Arrival types for Nodes 1 and 2 (I-25 frontage roads) were set to 5 per HCS Exhibit 19-14 for coordinated signal spacing <1,600'. This could also apply to node 3 at Pan American NE, but it was treated as random arrival type 3 due to the number of intervening driveways and for a more conservative analysis.
 - e. Pedestrian timings were input but set to actuated mode rather than recall mode as observed. A lead pedestrian interval (LPI) exists at Node 2, but *HCS* has no settings for this mode.
 - f. Flashing yellow arrow (FYA) operations were input as "Dallas phasing."
5. Dimensions were measured with *Google Earth*®; dual left-turn lanes at Nodes 1 & 2 were averaged.
6. Shared lane volumes were calculated from the TMCs.
7. Free right-turn lane volumes were recorded for exclusion from the signalized intersections.
8. Driveway nodes 4 and 5 were analyzed with two-stage left turn maneuvers.

6.1.2 Operational Analysis Results

Capacity analyses were computed using the same, systematic method so results could be compared for these alternative scenarios:

1. **Scenario 1: Baseline (2023)** – represents existing conditions prior to development of the site utilizing traffic data collected in August 2023.
2. **Scenario 2: Implementation Year NO-Build (2025)** – baseline conditions plus background traffic growth without development, representing the implementation year operating conditions.
3. **Scenario 3: Implementation Year BUILD (2025)** – existing traffic plus background traffic growth and complete development traffic.
4. **Scenario 4: Horizon NO-Build (2035)** – existing traffic conditions plus background traffic growth without development, representing the horizon year operating conditions.
5. **Scenario 6: Horizon BUILD (2035)** – forecast conditions including background traffic growth and site traffic, to assess forecast traffic operations with developed conditions.

Computed results are contained in Appendix E and summarized in Tables 5 through 9.

Table 5: LOS Summary for Baseline Conditions

Node	Lane Group	AM Peak								PM Peak							
		EB		WB		NB		SB		EB		WB		NB		SB	
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR
1: Osuna / I-25 SB Ramp	Volume		789	605	1385			220	358		1207	491	1014			178	211
	V/C ¹		0.36	0.63	0.43			0.60	0.82		0.46	0.58	0.26			0.53	0.79
	Queue ²		8	168	93			190	253		10	130	115			133	200
	Delay (s) ³		0.4	11.9	3.8			44.0	46.0		0.6	8.2	4.8			51.5	53.6
	LOS	-	A	B	A	-	-	D	D	-	A	A	A	-	-	D	D
	Intersection Delay, LOS: 11.7 B									9.8 A							
2: I-25 NB Ramp Osuna / San Mateo	Volume	264	1420		1666	446	249			318	2177		1151	326	406		
	V/C ¹	0.42	0.28		0.86	0.06	0.87			0.23	0.37		0.70	0.38	0.87		
	Queue ²	98	85		318	23	363			40	143		45	178	365		
	Delay (s) ³	40.2	5.6		13.7	33.2	43.5			8.6	6.6		3.6	38.8	50.1		
	LOS	D	A		B	C	D			A	A		A	D	D		
	Intersection Delay, LOS: 18.3 B									13.4 B							
3: San Mateo / Pan American	Volume	2	4	91	200	1	2046	83	1329	24	8	107	146	4	2138	64	2049
	V/C ¹		0.03		0.76	0.00	0.65	0.47	0.36		0.15		0.50	0.03	0.62	0.35	0.51
	Queue ²		8		205		385	65	125		45			3	385	30	223
	Delay (s) ³		41.30		51.40	5.70	12.60	16.50	4.70		46.10		50.70	8.50	11.80	11.80	6.10
	LOS		D		D	A	B	B	A		D		D	A	B	B	A
	Intersection Delay, LOS: 11.6 B									10.7 B							
4: San Mateo / Jackelope	Volume			2	14		2332	15	1557			1	19		2291	30	2330
	V/C ¹			0.16	0.11			0.26				0.97	0.20			0.64	
	Queue ²			10	10			23				30	18			70	
	Delay (s) ³			308.5	35.4			79.7				1492.9	37.0			123.9	
	LOS			F	E			F				F	E			F	
	Intersection Delay, LOS: 0.6 --									2.4 --							
5: San Mateo / Garcia Far North	Volume		4	2	12	18	2260	52	1483		28		89	18	2067	132	2154
	V/C ¹		0.02		2.05	0.11		0.86			0.25		0.57	0.34		1.93	
	Queue ²		3		25	10		103			25		75	33		358	
	Delay (s) ³		18.0		5340.5	26.7		174.7			35.2		53.2	69.3		541.9	
	LOS		C		F	D		F			A		F	F		F	
	Intersection Delay, LOS: 5.4 --									19.5 --							
6: Pan American / Harper	Volume		100	233	42	24					145	267	56	17	133		
	V/C ¹		0.16	0.40	0.07	0.23					0.25	0.54	0.13	0.23			
	Queue ²		13	48	5	23					25	80	10	23			
	Delay (s) ³		8.8	12.0	8.7	8.9					9.7	14.8	8.6	9.4			
	LOS		A	B	A	A					A	B	A	A			
	Intersection Delay, LOS: 10.2 B									11.8 B							

1. v/c = volume-to-capacity ratio for traffic movement

2. 95th percentile queue in feet

3. Control Delay measured in seconds per vehicle (only critical movement LOS at unsignalized intersections is tabulated).

Table 6: LOS Summary for Implementation NO-Build (2025)

Node	Lane Group	AM Peak								PM Peak							
		EB		WB		NB		SB		EB		WB		NB		SB	
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR
1: San Mateo / I-25 SB Ramp	Volume		797	611	1399			242	394		1222	496	1024			192	228
	V/C ¹		0.37	0.62	0.42			0.61	0.83		0.49	0.56	0.30			0.57	0.84
	Queue ²		8	168	33			205	273		10	75	158			145	233
	Delay (s) ³		0.5	12.1	1.3			43.0	45.2		0.7	4.5	5.9			51.6	63.2
	LOS		A	B	A			D	D		A	A	A			D	E
	Intersection Delay, LOS: 11.4 B									11.0 B							
2: San Mateo / I-25 NB Ramp	Volume	272	1463		1699	459	256			328	2242		1163	336	418		
	V/C ¹	0.37	0.29		0.89	0.06	0.87			0.24	0.38		0.71	0.38	0.88		
	Queue ²	73	98		118	25	373			40	188		58	183	380		
	Delay (s) ³	21.6	6.3		6.7	32.7	43.6			9.1	9.0		4.2	38.2	50.5		
	LOS	C	A		A	C	D			A	A		A	D	D		
	Intersection Delay, LOS: 14.1 B									14.4 B							
3: San Mateo / Pan American	Volume	2	4	93	204	1	2087	85	1356	24	8	111	152	4	2159	65	2090
	V/C ¹		0.02		0.77	0.00	0.66	0.50	0.37		0.18		0.67	0.03	0.60	0.35	0.50
	Queue ²		8	130			403	80	130		48		170	3	348	28	188
	Delay (s) ³		41.00		51.30	5.90	13.20	18.40	4.90		49.20		56.20	6.70	10.00	10.20	5.00
	LOS		D		D	A	B	B	A		D		E	A	A	B	A
	Intersection Delay, LOS: 12.0 B									9.6 A							
4: San Mateo / Jackalope	Volume			2	14		2279	15	1588			1	19		2340	30	2377
	V/C ¹			0.18	0.12			0.27				0.16	0.15			0.47	
	Queue ²			13	10			25				10	13		0	48	0
	Delay (s) ³			350.5	36.9			86.4				648.2	35.3			97.4	
	LOS			F	E			F				F	E			F	
	Intersection Delay, LOS: 0.6 --									0.9 --							
5: San Mateo / Garcia Far North	Volume		4	2	12	18	2305	52	1498		28		89	18	2088	132	2176
	V/C ¹		0.02		3.81	0.11		0.92			0.20		0.59	0.24		1.63	
	Queue ²		3	25	8	10		108		0	18	0	78	23	0	290	0
	Delay (s) ³		15.8	133.1	35.3	27.2		198.3			33.9		53.9	63.5		409.7	
	LOS		C	F	E	D		F			D	F		F		F	
	Intersection Delay, LOS: 8.1 --									13.4 --							
6: Pan American / Harper	Volume		99	238	43	24					138	278	58	17	134		
	V/C ¹		0.15	0.41	0.07	0.23					0.21	0.47	0.09	0.22			
	Queue ²		13	50	5	23				0	20	63	8	20	0	0	0
	Delay (s) ³		8.8	12.1	8.2	8.9					9.2	13.2	8.3	9.1			
	LOS		A	B	A	A					A	B	A	A			
	Intersection Delay, LOS: 10.2 B									10.9 B							

1. v/c = volume-to-capacity ratio for traffic movement

2. 95th percentile queue in feet

3. Control Delay measured in seconds per vehicle (only critical movement LOS at unsignalized intersections is tabulated).

Table 7: LOS Summary for Implementation BUILD (2025)

Node	Lane Group	AM Peak								PM Peak							
		EB		WB		NB		SB		EB		WB		NB		SB	
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR
1: San Mateo / I-25 SB Ramp	Volume		803	620	1429			254	394		1227	502	1028			201	228
	V/C ¹		0.37	0.63	0.43			0.63	0.83		0.49	0.56	0.31			0.59	0.84
	Queue ²		8	178	30			213	273		10	75	133			153	235
	Delay (s) ³		0.5	13.0	1.1			43.2	45.1		0.7	4.6	4.8			52.3	63.6
	LOS		A	B	A			D	D		A	A	A			D	E
Intersection Delay, LOS:		11.5 B								10.8 B							
2: San Mateo / I-25 NB Ramp	Volume	272	1481		1735	459	256			328	2256		1173	336	418		
	V/C ¹	0.39	0.29		0.91	0.06	0.87			0.27	0.40		0.76	0.34	0.90		
	Queue ²	80	93		133	25	373			45	190		105	175	403		
	Delay (s) ³	24.0	5.9		7.7	32.7	43.6			11.2	9.7		6.7	35.5	54.2		
	LOS	C	A		A	C	D			B	A		A	D	D		
Intersection Delay, LOS:		14.4 B								15.5 B							
3: San Mateo / Pan American	Volume	80	27	93	204	64	2055	85	1334	61	47	111	152	47	2139	65	2070
	V/C ¹		0.49		0.80	0.25	0.64	0.46	0.37		0.52		0.61	0.31	0.61	0.35	0.52
	Queue ²		158		208	35	378	53	133		165		180	38	368	30	215
	Delay (s) ³		45.40		52.60	9.80	12.80	15.00	5.10		50.6		54.4	15.8	11.1	11.2	5.8
	LOS		D		D	A	B	B	A		D		D	B	B	B	A
Intersection Delay, LOS:		12.8 B								11.2 B							
4: San Mateo / Jackalope	Volume			2	14		2410	15	1593			1	19		2363	30	2396
	V/C ¹			0.19	0.12			0.28				0.18	0.15			0.48	
	Queue ²			13	10			25				10	13			48	
	Delay (s) ³			376.5	37.9			90.8				700.3	36.0			101.8	
	LOS			F	E			F				F	E			F	
Intersection Delay, LOS:		0.7 --								0.9 --							
5: San Mateo / Garcia Far North	Volume		4	2	12	18	2336	52	1503		28		89	18	2111	132	2195
	V/C ¹		0.02		7.60	0.11		0.96			0.20		0.60	0.25		1.68	
	Queue ²		3		25	10		113			18		80	23		295	
	Delay (s) ³				36.2	27.4		215.1			34.5		55.9	65.4		433.0	
	LOS		C		F	D		F			D		F	F		F	
Intersection Delay, LOS:		13.5 --								14.0 --							
6: Pan American / Harper	Volume		99	248	43	24	147				139	285	58	17	140		
	V/C ¹		0.16	0.43	0.07	0.24					0.22	0.49	0.09	0.23			
	Queue ²		13	53	5	23					20	68	8	23			
	Delay (s) ³		8.8	12.5	8.2	9.0					9.3	13.5	8.3	9.2			
	LOS		A	B	A	A					A	B	A	A			
Intersection Delay, LOS:		10.5 B								11.0 B							

1. v/c = volume-to-capacity ratio for traffic movement

2. 95th percentile queue in feet

3. Control Delay measured in seconds per vehicle (only critical movement LOS at unsignalized intersections is tabulated).

Table 8: LOS Summary for Horizon NO-Build (2035)

Node	Lane Group	AM Peak								PM Peak							
		EB		WB		NB		SB		EB		WB		NB		SB	
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR
1: Osuna / I-25 SB Ramp	Volume		1002	593	1357			387	630		1291	501	1034			278	329
	V/C ¹		0.55	0.81	0.47			0.66	0.87		0.53	0.59	0.30			0.72	1.06
	Queue ²	0	90	293	48	0	0	288	413		13	83	130			223	403
	Delay (s) ³		4.9	32.2	2.1			38.1	47.8		0.8	5.2	5.0			60.1	111.5
	LOS		A	C	A			D	D		A	A	A			E	E
	Intersection Delay, LOS: 18.9 B									20.7 C							
2: I-25 NB Ramp Osuna / San Mateo	Volume	312	1676		1849	544	304			372	2547		1427	398	495		
	V/C ¹	0.76	0.39		0.99	0.06	0.88			0.45	0.43		0.94	0.39	0.89		
	Queue ²	138	93		295	28	420			53	218		230	203	460		
	Delay (s) ³	53.7	4.9		21.1	30.3	43.7			20.3	11.4		16.6	34.7	52.9		
	LOS	D	A		C	C	D			C	B		B	C	D		
	Intersection Delay, LOS: 21.4 C									20.7 C							
3: San Mateo / Pan American	Volume	2	4	103	226	1	2251	94	1502	24	8	133	181	4	2330	75	2315
	V/C ¹		0.02		0.78	0.00	0.68	0.53	0.39		0.16		0.74	0.03	0.67	0.45	0.57
	Queue ²		8		225		433	83	150		48		205	3	428	70	245
	Delay (s) ³		40.1		50.9	6.6	14.3	20.8	5.4		47.3		58.8	9.4	12.4	19.4	6.4
	LOS		D		D	A	B	C	A		D		E	A	B	B	A
	Intersection Delay, LOS: 12.8 B									11.6 B							
4: San Mateo / Jackalope	Volume			2	14		2565	15	1728			1	19		2520	30	2540
	V/C ¹			0.28	0.14			0.35				0.30	0.17			0.59	
	Queue ²			15	13			30				13	15			58	
	Delay (s) ³			594.1	43.3			118.3				1271.0	41.2			140.3	
	LOS			F	E			F				F	E			F	
	Intersection Delay, LOS: 0.8 --									1.2 --							
5: San Mateo / Garcia Far North	Volume		4		12	18	2554	52	1676		28		89	18	2212	132	2434
	V/C ¹		0.02	0.56	0.13	0.14		1.28			0.25		0.65	0.34		1.91	
	Queue ²		3	20	10	13		138			23		90	30		320	
	Delay (s) ³		21.0	1308.5	43.7	33.9		371.6			43.2		66.0	96.8		545.4	
	LOS		C	F	E	D		F			E		F	F		F	
	Intersection Delay, LOS: 5.3 --									16.4 --							
6: Pan American / Harper	Volume		95	259	47	26	150				113	331	69	21	165		
	V/C ¹		0.15	0.45	0.07	0.25					0.18	0.57	0.11	0.28			
	Queue ²		13	58	5	25					15	90	10	28			
	Delay (s) ³		8.8	12.8	8.2	9.2					9.3	15.7	8.5	9.7			
	LOS		A	B	A	A					A	C	A	A			
	Intersection Delay, LOS: 10.7 B									12.4 B							

1. v/c = volume-to-capacity ratio for traffic movement

2. 95th percentile queue in feet

3. Control Delay measured in seconds per vehicle (only critical movement LOS at unsignalized intersections is tabulated).

Table 9: LOS Summary for Horizon BUILD (2035)

Node	Lane Group	AM Peak								PM Peak							
		EB		WB		NB		SB		EB		WB		NB		SB	
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR
1: Osuna / I-25 SB Ramp	Volume		1008	602	1384			399	630		1296	507	1038			287	329
	V/C ¹		0.59	0.81	0.49			0.69	0.89		0.53	0.60	0.32			0.75	1.07
	Queue ²		123	303	58			303	428		13	83	138			233	410
	Delay (s) ³		6.8	35.2	2.5			40.2	51.8		0.8	5.2	5.2			62.2	114.4
	LOS		A	D	A			D	D		A	A	A			E	F
	Intersection Delay, LOS: 20.7 C									21.3 C							
2: I-25 NB Ramp Osuna / San Mateo	Volume	312	1694		1885	544	314			372	2547		1427	398	495		
	V/C ¹	0.82	0.40		1.01	0.06	0.88			0.61	0.45		0.98	0.35	0.91		
	Queue ²	143	95		350	28	420			68	215		328	195	478		
	Delay (s) ³	57.9	5.0		42.2	29.6	43.7			29.9	11.9		25.2	32.3	56.8		
	LOS	E	A		F	C	D			C	B		C	C	E		
	Intersection Delay, LOS: 30.5 C									25.7 C							
3: San Mateo / Pan American	Volume	80	27	103	226	64	2219	94	1480	61	47	133	181	47	2310	75	2295
	V/C ¹		0.47		0.87	0.26	0.65	0.53	0.39		0.45		0.62	0.38	0.72	0.45	0.61
	Queue ²		160		290	38	415	98	155		153		193	55	463	65	285
	Delay (s) ³		48.7		75.1	10.6	12.9	20.6	5.2		44.0		47.5	27.6	15.4	20.5	8.2
	LOS		D		E	B	B	C	A		D		D	C	B	C	A
	Intersection Delay, LOS: 14.2 B									13.7 B							
4: San Mateo / Jackalope	Volume			2	14		2596	15	1733			1	19		2543	30	2559
	V/C ¹			0.30	0.14			0.36				0.33	0.17			0.61	
	Queue ²			15	13			33				13	15			60	
	Delay (s) ³			644.7	44.5			124.9				1400.1	42.0			147.3	
	LOS			F	E			F				F	E			F	
	Intersection Delay, LOS: 0.9 --									1.3 --							
5: San Mateo / Garcia Far North	Volume		4	2	12	18	2585	52	1681		28		89	18	2235	132	2453
	V/C ¹		0.02	0.60	0.13	0.14		1.34			0.25		0.66	0.35		1.97	
	Queue ²		3	20	10	13		140			23		93	33		33	
	Delay (s) ³		21.0	1396.7	44.8	34.1		399.7			44.0		68.6	100.1		573.5	
	LOS		C	F	E	D		F			E		F	F		F	
	Intersection Delay, LOS: 5.7 --									17.0 --							
6: Pan American / Harper	Volume		95	269	47	26	158				113	338	69	21	171		
	V/C ¹		0.15	0.47	0.07	0.26					0.18	0.58	0.11	0.29			
	Queue ²		13	63	5	28					15	95	10	30			
	Delay (s) ³		8.9	13.2	8.3	9.3					9.3	16.2	8.6	9.8			
	LOS		A	B	A	A					A	C	A	A			
	Intersection Delay, LOS: 10.9 B									12.6 B							

1. v/c = volume-to-capacity ratio for traffic movement

2. 95th percentile queue in feet

3. Control Delay measured in seconds per vehicle (only critical movement LOS at unsignalized intersections is tabulated).

6.2 Queueing Summary

Queue length measurements are useful in evaluating traffic operations and for determination of turn lane storage requirements. As shown in the *HCM Guide*, “The 95th-percentile queue is defined to be the queue length (in vehicles) that has only a 5-percent probability of being exceeded during the analysis time period.” A procedure for calculating queues is provided in the HCM and computations are included in the LOS worksheets in Appendix E. A summary of available turn lane storage lengths for traffic queues is provided in Table 10 for relevant turning lane groups that could be impacted with site traffic in the implementation year. Based on the calculated results listed in Tables 5 through 9, queues that may require mitigation were identified at the locations listed in Table 10.

Table 10: Available Queueing Capacity

Intersection	Movement	Available Storage ¹	Remarks ²
Node 1 (SBFR)	SBL	220' / 9	Scenario 2, 3, 4, 5
Node 1 (SBFR)	SBR	225' / 9	Scenario 4, 5
Node 1 (SBFR)	WBL	325' / 13 ³	
Node 2 (NBFR)	EBL	250' / 10	
Node 2 (NBFR)	NBR	350' / 14	Scenario 1, 2, 3, 4, 5
Node 2 (NBFR)	WBR	400' / 16	
Node 3 (Pan Am)	WBR	150' / 6	Scenario 2, 3, 4, 5
Node 3 (Pan Am)	NBL	120' / 5	

¹Storage length in feet or per number of vehicles (at an average of 25' per vehicle).

²Queue Storage Ratio (RQ) exceeds 1.0 for the Scenarios listed.

³EBL and WBL dual left-turn lanes are averaged.

6.3 Assessment of Impacts

Conclusions drawn from the results of these capacity analyses include:

1. All intersections operated at acceptable levels of service for all scenarios.
2. Several lane groups experience high delays with poor LOS and/or exceeding v/c ratio of 1.0; most notably, Osuna/San Mateo maintained high LOS while the frontage roads experienced high delays.
3. Unsignalized access LOS were high for the two shopping center driveways, but turning movements experienced very high delays due to large volumes of through traffic. Low incidences of crashes were recorded at these locations, and alternative access is available as well as U-turn opportunities at adjacent signalized intersections.
4. Site access at Pan American showed satisfactory LOS without and with development trips added.
5. Current traffic signal operations provide extensive green time for the arterial in order to enhance progression. However, side-street phases experience increasing delays as a result beginning with the implementation year 2025 (I25 SBFR, see Table 7) and getting progressively worse through the horizon year.
6. The existing dedicated left-turn lane on the I-25 SBFR measures nominally 220'. By comparison, the NBL on the I-25 NBFR is about 350', albeit with nearly double the volume of left-turns. Queues are projected to exceed available storage beginning with Scenario 2 and progressively increase through the horizon period for no-build and build scenarios.

Because intersection LOS fell below acceptable levels for some scenarios, mitigation measures were evaluated to resolve operational deficiencies as described in Section 6.4.

6.4 Mitigation Analysis

Based upon the results summarized in Tables 5 through 9, additional scenarios were analyzed to evaluate mitigation measures that would address operational deficiencies identified in the Horizon Year. These scenarios are described below and summarized in Table 11 (see Appendix F for LOS worksheets).

1. Mitigation Scenario 1 (MIT1) – Signal Timing Optimization Plus. Signalized intersections at Nodes 1 through 3 are coordinated, though coordinated phase numbers differ between the City and NMDOT intersections. Signal timing and offset optimization was run for this scenario to evaluate whether the corridor operations could be improved. In addition, the Pan American NE west approach had been analyzed as a single lane approach, which was modified to add a dedicated left-turn lane matching the east approach. Following is a summary of this scenario.
 - a. Optimized network cycle lengths & splits:
 - i. *AM Peak Period*: Network cycle lengths increased from C = 110s to C = 120s and cross-street green times increased.
 - ii. *PM Peak Period*: Network cycle lengths maintained at C = 120s but increased cross-street green times.
 - b. Node 3 – maintained permissive left-turns for east-west approaches.
 - c. Signal operations improved under this scenario as summarized in Table 11.
 - i. All lane groups improved to LOS D or above.
 - ii. Queue Storage Ratios (RQ) improved for all scenarios; however, the following lane group queues may continue to have issues:
 - a) Node 3 WBR: RQ > 1 as forecast queue exceeded storage length by 2+ vehicles. However, the adjacent Left-Thru (LT) lane queue does not block WBR access, so the extra 2-3 vehicles can be stack behind LT and shift in the transition area & shoulder.
 - b) Node 1 SBL: RQ > 1 as forecast queue exceeded storage length by 5 vehicles. Additional cross-street green time wouldn't improve RQ but would degrade LOS for the arterial. Heavy SBT traffic would cause queue-blocking and prevent some of the left-turners from entering the dedicated left turn lane; however, the inside through lane is a shared through-left, so left-turners could use this lane and thereby increase the proportion of shared lane users to improve shared lane efficiency. Furthermore, no residual queue would be expected, and thus the left-turners would be typically serviced on a per cycle basis.
 - c) Node 1 SBR: RQ > 1 as forecast queue exceeded storage length by 2 vehicles. Additional cross-street green time wouldn't improve RQ but would degrade LOS for the arterial. Heavy SBT traffic would cause queue-blocking and prevent some of the right-turners from entering the dedicated right-turn lane; however, the adjacent through lane could be converted to a shared through-right, so right-turners could use this lane and thereby increase the proportion of shared lane users to improve shared lane efficiency. Furthermore, no residual queue would be expected, and thus the right-turners should be typically serviced on a per cycle basis.

Table 11: LOS Summary for Mitigation Scenarios

Node	Lane Group	AM Peak								PM Peak								
		EB		WB		NB		SB		EB		WB		NB		SB		
		L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	L	TR	
3-San Mateo/Pan Am.	Mitigation 1 (2035)																	
	Volume		1008	602	1384			399	630		1296	507	1038			287	329	
	V/C ¹	0.55	0.36	0.92	0.51			0.55	0.86		0.64	0.77	0.36			0.45	0.64	
	Queue ²	93	54	296	70			273	450		85	158	182			196	276	
	Delay (s) ³	4.7	4.6	33.6	3.4			38.0	42.7		3.9	12.9	8.3			41.8	44.8	
	LOS	A	A	C	A			D	D		A	B	A			D	D	
	Intersection Delay, LOS:								18.4	B							13.7	B
	Volume	312	1694		1885	544	314			372	2547		1427	398	495			
	V/C ¹	0.48	0.37		0.92	0.36	0.87			0.67	0.47		0.99	0.35	0.64			
	Queue ²	114	129		118	167	401			73	228		358	198	359			
	Delay (s) ³	20.4	5.9		7.6	38.8	46.5			31.0	12.0		27.2	32.0	35.8			
	LOS	C	A		A	D	D			C	B		C	C	D			
	Intersection Delay, LOS:								15.0	B							26.5	C
	Volume	61	47	133	181	47	2310	75	2295	80	12	103	226	64	2219	94	1480	
	V/C ¹	0.36	0.17		0.47	0.46	0.78	0.52	0.67	0.26	0.08		0.30	0.38	0.80	0.63	0.47	
	Queue ²	91	72		197	83	616	65	436	103	38		140	68	633	80	289	
	Delay (s) ³	53.5	37.1		44.4	48.6	20.8	26.6	12.6	42.1	32.3		36.2	25.3	24.6	30.1	11.9	
	LOS	D	D		D	D	C	C	B	D	C		D	C	C	C	B	
	Intersection Delay, LOS:								19.8	B							22.2	C
	1-Osuna/SBFR	Mitigation 2, 3 (2035)																
Volume			1008	602	1384			399	630									
V/C ¹			0.64	0.96	0.54			0.48	0.89									
Queue ²			201	286	74			259	503									
Delay (s) ³			11.5	41.7	3.9			34.1	40.6									
LOS			B	D	A			C	C									
Intersection Delay, LOS:								20.6	C									
Volume			1008	602	1384			399	630									
V/C ¹			0.90	0.95	.58			0.35	1.17									
Queue ²			490	296	130			220	1412									
1-Osuna/SBFR	Delay (s) ³		39.2	41.1	7.6			30.7	135									
	LOS		D	D	A			C	F									
	Intersection Delay, LOS:								42.4	D								

2. Mitigation Scenario 2 (MIT2) – Convert SBT to Shared Lane SBTR. The adjacent dedicated through lane was converted to a shared through-right turn lane (TR):
 - a. Assumed 30%-40% or right-turn traffic would use the shared through/right-turn (TR) lane.
 - b. Optimized phase splits.
 - c. LOS C for intersection, LOS D for SB approach.
 - d. QR fell below 1.0 at 0.90.
3. Mitigation Scenario 3 (MIT3) – Convert SBTL to Dedicated Left-Turn Lane. The inside through/left (TL) lane was converted from a shared through-left turn lane to a dedicated left, providing dual left-turn lanes:
 - a. Optimized phase splits.
 - b. LOS D for intersection, LOS F for SB approach.
 - c. QR fell to 1.10.
 - d. Traffic patterns in the AM peak favor the shared TL for this second lane as through traffic utilizes the lane to access the SB I-25 On ramp south of the intersection, and this option may not be desirable.

7.0 ACCESS DESIGN RECOMMENDATIONS

As illustrated in the Conceptual Site Plan (Figure 7), two access points are proposed to provide ample circulation and queueing space on site for the drive-through restaurant and retail businesses. It is proposed that ownership of Pan American NE right-of-way east of San Mateo be transferred from NMDOT to private ownership, and thus the site driveways will be considered as internal and subject to COA development guidelines. Easement(s) may be needed to maintain access for the car dealership, gas station, and access by NMDOT to drainage infrastructure at the southern boundary of the site for maintenance purposes.

7.1 Site Drive 1

1. A 3-lane, 48' wide commercial access with 2 ingress and 1 egress lane:
 - a. Outer ingress lane will be dedicated to Building 2. Site signing and pavement markings should be provided to direct traffic accordingly.
 - b. Second ingress lane would serve Buildings 1 and 3.
2. Drive 1 is perpendicular to Pan American NE but slightly off-alignment from the existing driveway at the Speedway gas station to the southeast, an acceptable condition as the predominant movement would be toward San Mateo and little cross-traffic would be anticipated.

7.2 Site Drive 2

1. A 28' 2-lane full commercial access predominantly serving Building 3.
2. Additional parking spaces are proposed along the southeast side of Pan American NE for overflow and to accommodate existing parking demand associated with the car dealership service operations.

7.3 General Driveway Requirements

1. Curb returns shall be provided as per the COA Development Process Manual (DPM) §7-4(B)(5)(i).
2. Dimensional criteria are as per DPM §7-4(B)(5)(ii) and §7-4(B)(5)(iii).

7.4 Off-Site Improvements

7.4.1 San Mateo/Pan American NE Intersection (Node 3).

1. The existing Pan American NE pavement west of San Mateo has not been properly maintained and is poor condition. In addition, the roadway is superelevated as it formerly served as the I-25 NB exit ramp. Pavement rehabilitation and/or full reconstruction should be considered based on geotechnical evaluation and in consideration of site grading requirements.
2. Two loop detectors exist on the west approach indicating 2 departure lanes are available. New pavement markings should be provided to designate a 100' dedicated left-turn lane and through/right-turn (TL) lane striping.
3. The existing northwest quadrant return radius is large but verification should be made to accommodate right-turning trucks turning from San Mateo.
4. The southwest quadrant radius return should be reconstructed with concrete curb & gutter, sidewalk, and access ramps. This may necessitate the relocation or replacement of the existing traffic signal mastarm, meter pedestal, and overhead-fed light standard.
5. Existing traffic signal indications for the west approach comply with 4D.05 of the MUTCD and left-turn phasing was not necessary for suitable operations; therefore, no additional signal modifications are hereby recommended.
6. A pedestrian access route (PAR) should be designated as follows:
 - a. Cross the south approach of San Mateo.
 - b. Cross the east approach of Pan American NE.
 - c. Continue westward on the north side of San Mateo/Osuna across the frontage road.
 - d. Eradicate crosswalk pavement markings on the north approach of San Mateo:
 - i. This will eliminate conflicts with SBR traffic.
 - ii. No curb cut or access ramp exists on the northwest corner, and there are no pedestrian measures along the south side of San Mateo/Osuna across the I-25 interchange; therefore this frontage along the north boundary of the site should be treated in such a ways as to discourage or prohibit pedestrians from traversing it.
 - iii. Remove the pedestrian signals across the north approach of San Mateo and install MUTCD sign code **R9-3a**.
 - e. Provide supplemental signing on the southwest quadrant advising pedestrians to cross the south approach of San Mateo using MUTCD sign code **R9- 3bP**.



R9-3a



R9-3bP

7. Provide pedestrian access to the site from the south side of Pan American NE to San Mateo Blvd. as depicted in Figure 2.
8. Traffic signal timing adjustments were shown to mitigate poor LOS for the cross-streets from the implementation year and beyond. It is recommended that the affected intersections be monitored

on a periodic basis to determine when adjustments would be needed based on observed queuing and delays on the cross streets.

9. Southbound right-turn (SBR) volumes amounted to 68 AM and 53 PM in the horizon year, slightly exceeding the limit of 50 shown in *DPM Table 7.4.67* and necessitating a right-turn lane length of 350' per *DPM Table 7.4.68*. The length between returns at the I-25 NBFR and Pan American NE measure approximately 295' and thus the full requirement could not be implemented. Furthermore, the presence of three southbound travel lanes and LOS A for both peak periods suggest that a SBR lane is not imperative. It is recommended that a request to waive this requirement be submitted.
10. The northbound left-turn lane (NBL) had ample capacity through the horizon year, operating at LOS B and C in permissive mode for the AM and PM peak periods, respectively. In addition, the queue storage ratios measured less than half. Thus, there is adequate storage available and no additional measures are recommended.

7.4.2 Osuna/I-25 SBFR Intersection (Node 1).

1. Implement Mitigation Scenario 2 (MIT2):
 - a. Optimize signal timings to reduce delay and improve LOS for the SBFR.
 - b. Convert the outer through lane to a through-right (TR) lane.
 - c. Extension of the southbound left-turn lane (SBL) to accommodate left-turn storage could benefit queue storage for the SBL movement but may not be necessary as shown in §6.4.1.c.
2. Timing of these modifications would be beyond the implementation year and toward the horizon year.
3. A tabulation of intersection traffic volumes showed the proposed site development would only contribute 1% to 2% to this intersection. Therefore, no physical modifications are recommended as part of this project but should be noted for ongoing monitoring and future implementation through the normal course of traffic operations and maintenance activities.

8.0 SUMMARY OF FINDINGS

Key findings of this analysis are summarized as follows:

1. The Osuna/San Mateo corridor at I-25 acts serves commuter access to I-25 and local access to adjacent commercial and residential areas.
2. For existing (baseline) conditions, Levels of service (LOS) at traffic signalized intersections along Osuna/San Mateo are within the acceptable range.
3. Minor geometric modifications at the San Mateo/Pan American NE intersection will be needed to accommodate the proposed development.
4. Sustained traffic growth in the region may intensify heavy directional movements and necessitate improvements by Year 2035 including:
 - a. Adjust traffic signal timings to improve cross-street operations while maintaining throughput and traffic progression.
 - b. Intersection improvements consisting of lane designations at the I-25 SBFR to add turn lane capacity and/or extend queue storage.

5. Site-generated traffic is not expected to cause significant, adverse impacts to the adjacent roadway network, including the traffic signalized intersections along Osuna/San Mateo, but would incrementally increase delay.

9.0 RECOMMENDATIONS & MITIGATION MEASURES

This analysis has demonstrated that significant detrimental traffic impacts associated with the proposed commercial development project are not expected to occur on the adjacent transportation system. The following recommendations are offered regarding on- and off-site access modifications.

9.1 On-Site Access

1. Two access driveways are proposed to provide ample circulation and queueing space on site for the drive-through businesses and retail center as illustrated in Figure 2 and detailed in Section 7.1 of this report.
2. These driveways will be designed in accordance with City of Albuquerque Development Process Manual (COA DPM) requirements.

9.2 Off-Site Roadway Improvements

9.2.1 San Mateo/Pan American NE Intersection (Node 3).

1. Rehabilitate or reconstruct pavement and provide 100' left-turn plus transition with pavement markings.
2. Verify southbound right turns (SBR) into the site can accommodate turning trucks or enlarge return radius if necessary.
3. Reconstruct southeast quadrant to COA standards with curb & gutter, sidewalk with access ramps, and traffic signal and lighting modifications.
4. Designate pedestrian access route (PAR) for crossing the south San Mateo approach and prohibit pedestrian traffic westward along the south side of San Mateo toward the I-25 northbound frontage road (NBFR).
5. Provide pedestrian access to the site from the south side of Pan American NE to San Mateo Blvd. as depicted in Figure 2.
6. Submit waiver request for SBR lane on San Mateo at Pan American NE.

9.2.2 Osuna/I-25 Southbound Frontage Road (SBFR) Intersection (Node 3).

1. Implement future mitigation measures as recommended in §7.4.2 of this report:
 - a. Optimize signal timings to delay and improve level of service (LOS).
 - b. Evaluate the need for conversion of the outer southbound through lane into a shared through and right-turn lane as future traffic operations may dictate.

APPENDIX A

Traffic Scoping Requirements



Minutes

To: Distribution
From: Timothy D. Simmons, PE, PTOE
Date: July 14, 2023
Re: Scoping Meeting – Traffic Impact Study for I25 & San Mateo SE Commercial Development

A meeting was held on July 14, 2023 to discuss the traffic study requirements for the proposed project located at I25 and Pan American NE in Albuquerque, NM. This scoping meeting was held in accordance with the NMDOT *State Access Management Manual (SAMM)* and City of Albuquerque (COA) *Development Process Manual (DPM)* requirements. The following individuals participated:

- Margaret Haynes, PE – NMDOT District 3 Assistant Traffic Engineer
- Matthew Grush, PE – City of Albuquerque Traffic Engineer
- Randi Estrada – NMDOT ROW Bureau
- Will Browning – Owner's Representative, Browning Commercial Real Estate
- Mark Edwards – Edwards Commercial Realty
- Justin Simenson – Isaacson & Arfman
- Fred Arfman, PE – Isaacson & Arfman
- Tim Simmons, PE, PTOE – Transportation and Traffic Engineer

- **Background:**

- Tim previously emailed NMDOT's Site Threshold Analysis (STH) and COA's Traffic Scoping forms along with an overall project location map and conceptual site plan.
- This project meets the threshold for a traffic impact study (TIS).
- Tim described the proposed land uses consisting of commercial & restaurant uses.

- **San Mateo Place/Pan American:**

- Access via San Mateo Pl.
 - Existing access via signal
 - No direct access will be allowed from San Mateo or Pan American Fwy
- Study limits will include:
 - 3 signalized intersections at I25 west, I25 east, Pan American Fwy; not Academy
 - Also include Harper/Pan American
 - Evaluate 2 unsignalized driveways at shopping centers
- Recent traffic signal modifications:
 - Permissive/protected operation
 - Northbound ramp study – signal timing changed
 - Demand volumes required if saturated
 - 5 years crash data
- No other imminent projects in the area

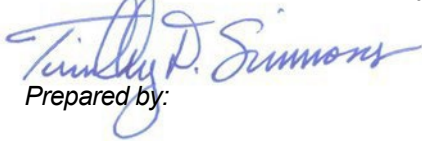
- **Property Ownership/Right-of-Way:**

- San Mateo Pl.:
 - ~75' ROW existing
 - Users include Garcias (shop), Speedway

- Need maintenance access to culverts
 - 3 options for ROW:
 - NMDOT maintained; don't want to continue maintaining it
 - COA / DHC process if improved
 - Private ownership – could construct 26' wide w/C&G on west side
 - NMDOT Legal would meet with team to review ROW transfer, need to prepare a ROW exhibit and roadway section exhibit
 - One-street parking vs. removal of parking
 - Need to review status for adjacent properties if precedent for access/parking
 - May need to provide a turnaround
 - NMDOT would grant a deed, COA would review site & easements if required
 - Randi & Margaret – will set up a meeting next week w/ROW Bureau
- **Other project-related items:**
 - Sidewalk – not along property frontage because there is none under bridge
 - Possibly fence this area to prohibit pedestrians

END OF MEETING MINUTES

These meeting minutes represent a summary of the items discussed. Any corrections or revisions should be directed to the author within 5 business days, after which time they will be considered as final.


Prepared by:

Date: _____

Distribution:



SITE THRESHOLD ASSESSMENT (STH)

A Site Threshold Assessment (STH) is required of all developing or redeveloping properties that directly or indirectly access a state highway.

District No.: _____

Project No.: _____

Date: _____

Applicant Name: _____

Business Name: _____

Address: _____

SITE DESCRIPTION

<input type="checkbox"/> Residential	Building Size (SF) _____	Dwelling Unit _____
<input type="checkbox"/> Retail	Parcel Size (ac) _____	Rooms _____
<input type="checkbox"/> Office	Roadway Frontage (ft) _____	Beds _____
<input type="checkbox"/> Industrial	Parking Spaces _____	Students _____
<input type="checkbox"/> Institutional	Employees _____	Seats _____
<input type="checkbox"/> Lodging	Other: _____	Fuel Pumps _____
<input type="checkbox"/> Restaurant		Courts _____
<input type="checkbox"/> Convenience/Gas		Storage Units _____
<input type="checkbox"/> Other: _____		

The STH examines existing roadway volumes and anticipated site trip generation for the purpose of determining if additional analyses are required. If the site characteristics and the trip generation estimate for a proposed development do not satisfy the requirements for a STA or a TIA as determined by the District Traffic Engineer, the STH should be approved and the traffic study requirement for the proposed development will be complete. If additional analysis is required based on the results of the STH, the District Traffic Engineer should indicate to the applicant the level of analysis that is required.

TRIP GENERATION

Option A (Commercial Access)

ITE Trip Generation Land Use Category: _____

AM Peak Hour Trips Entering: _____

Exiting: _____

PM Peak Hour Trips Entering: _____

Exiting: _____

Option B (Residential Access)

Daily Trips Entering: _____

Exiting: _____

FOR OFFICIAL USE ONLY

EXISTING ROADWAY DATA

Highway No.: _____

Site Mile Post: _____

Highway ADT: _____

Count Year: _____

Number of Lanes (two way): _____

Function Class: _____

EXCEEDS THRESHOLD ☐ Yes ☐ No ☐ STA Required ☐ TIA Required

Thresholds:

STA: 25 to 99 Peak Hour Total Trips AND more than 1,000 Vehicles per Lane per Day on adjacent Highway

TIA: 100 or more Peak Hour Total Trips

OTHER REQUIREMENT BASIS/DTE COMMENTS: _____



City of Albuquerque

Planning Department
Development Review Services Division

Traffic Scoping Form (REV 12/2020)

Project Title: I-25 & San Mateo SE Corner Building Permit #: _____ Hydrology File #: _____

Zone Atlas Page: _____ DRB#: _____ EPC#: _____ Work Order#: _____

Legal Description: _____

City Address: _____

Applicant: Jama Holdings Inc Contact: Will Browning

Address: PO BOX 122808, Ft. Worth, TX 76121-2808

Phone#: 505-883-8012 Fax#: _____ E-mail: willb@swcp.com

Development Information

Build out/Implementation Year: _____ Current/Proposed Zoning: t.b.d.

Project Type: New: ☒ Change of Use: () Same Use/Unchanged: () Same Use/Increased Activity: ()

Proposed Use (mark all that apply): Residential: () Office: () Retail: ☒ Mixed-Use: ☒

Describe development and Uses:

Retail space and restaurants

Days and Hours of Operation (if known): _____

Facility

Building Size (sq. ft.): 5,002; 2,472, 2,472

Number of Residential Units: _____

Number of Commercial Units: _____

Traffic Considerations

Expected Number of Daily Visitors/Patrons (if known):* _____

Expected Number of Employees (if known):* _____

Expected Number of Delivery Trucks/Buses per Day (if known):* _____

Trip Generations during PM/AM Peak Hour (if known):* 222,238

Driveway(s) Located on: Street Name San Mateo Place

Adjacent Roadway(s) Posted Speed: Street Name San Mateo Posted Speed 45

Street Name Posted Speed _____

Roadway Information (adjacent to site)

Comprehensive Plan Corridor Designation/Functional Classification: Community Principal Arterial
(arterial, collector, local, main street)

Comprehensive Plan Center Designation: Far North
(urban center, employment center, activity center)

Jurisdiction of roadway (NMDOT, City, County): San Mateo Pl. = NMDOT, San Mateo = COA

Adjacent Roadway(s) Traffic Volume: 49,392 (2020) Volume-to-Capacity Ratio: _____
(if applicable)

Adjacent Transit Service(s): 93, 140, 141 Nearest Transit Stop(s): San Mateo S of Pan American

Is site within 660 feet of Premium Transit?: No

Current/Proposed Bicycle Infrastructure: None
(bike lanes, trails)

Current/Proposed Sidewalk Infrastructure: Existing on San Mateo, n.a. across site

Relevant Web-sites for Filling out Roadway Information:

City GIS Information: <http://www.cabq.gov/gis/advanced-map-viewer>

Comprehensive Plan Corridor/Designation: <https://abc-zone.com/document/abc-comp-plan-chapter-5-land-use> (map after Page 5-5)

Road Corridor Classification: <https://www.mrcog-nm.gov/DocumentCenter/View/1920/Long-Range-Roadway-System-LRRS-PDF?bidId=>

Traffic Volume and V/C Ratio: <https://www.mrcog-nm.gov/285/Traffic-Counts> and <https://public.mrcog-nm.gov/taqa/>

Bikeways: http://documents.cabq.gov/planning/adopted-longrange-plans/BTFP/Final/BTFP%20FINAL_Jun25.pdf (Map Pages 75 to 81)

TIS Determination

Note: Changes made to development proposals / assumptions, from the information provided above, will result in a new TIS determination.

Traffic Impact Study (TIS) Required: Yes [☐] No [☐] Borderline [☐]

Thresholds Met? Yes [☐] No [☐]

Mitigating Reasons for Not Requiring TIS: _____ Previously Studied: [☐]

Notes:

TRAFFIC ENGINEER

DATE

Submittal

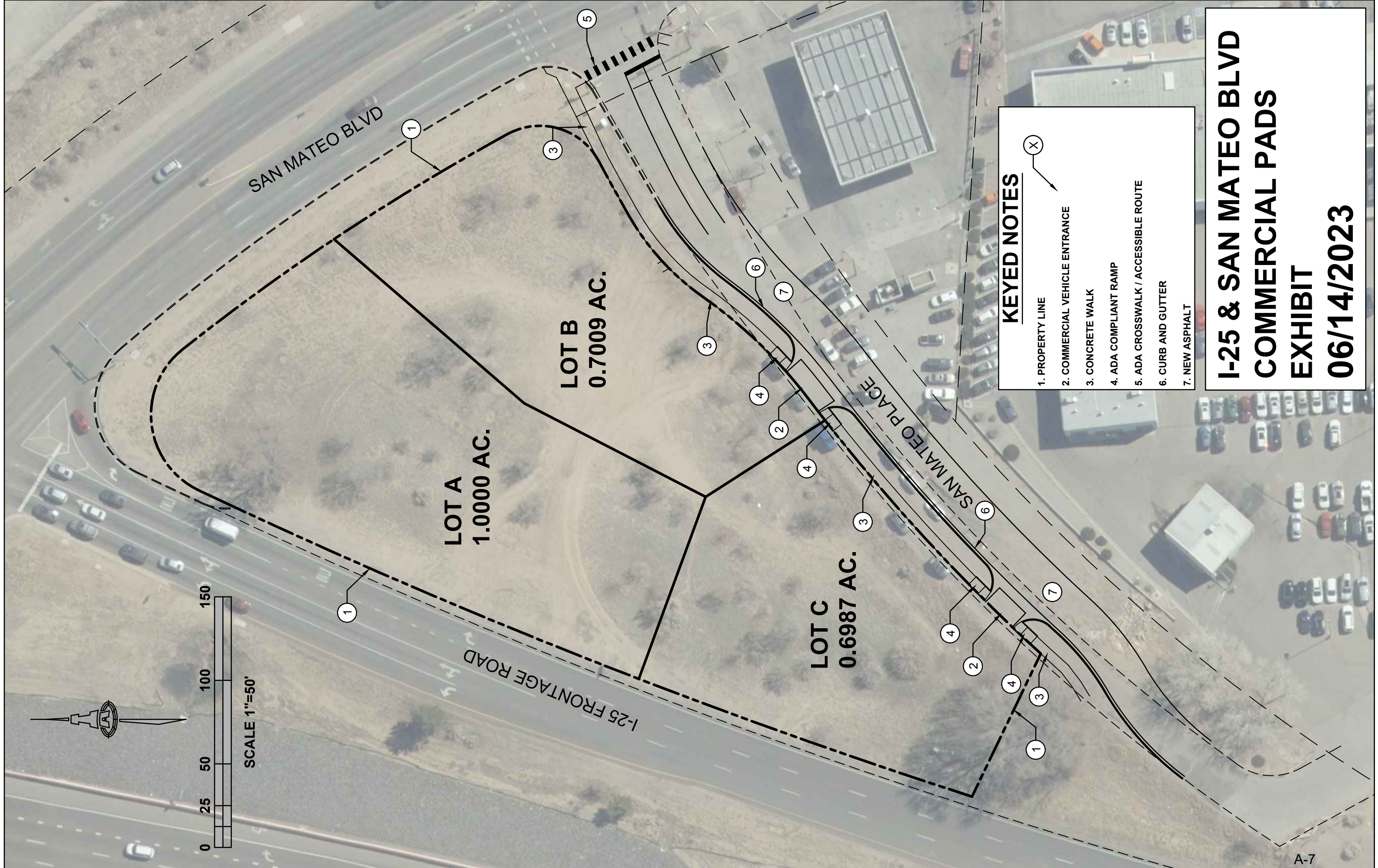
The Scoping Form must be submitted as part of any building permit application, DRB application, or EPC application. See the Development Process Manual Chapter 7.4 for additional information.

Submit by email to the City Traffic Engineer mgrush@cabq.gov . Call 924-3362 for information.

Site Plan/Traffic Scoping Checklist

Site plan, building size in sq. ft. (show new, existing, remodel), to include the following items as applicable:

1. Access -- location and width of driveways
2. Sidewalks (Check DPM and IDO for sidewalk requirements. Also, Centers have wider sidewalk requirements.)
3. Bike Lanes (check for designated bike routes, long range bikeway system) ([*check MRCOG Bikeways and Trails in the 2040 MTP map*](#))
4. Location of nearby multi-use trails, if applicable ([*check MRCOG Bikeways and Trails in the 2040 MTP map*](#))
5. Location of nearby transit stops, transit stop amenities (eg. bench, shelter). Note if site is within 660 feet of premium transit.
6. Adjacent roadway(s) configuration (number of lanes, lane widths, turn bays, medians, etc.)
7. Distance from access point(s) to nearest adjacent driveways/intersections.
8. Note if site is within a Center and more specifically if it is within an Urban Center.
9. Note if site is adjacent to a Main Street.
10. Identify traffic volumes on adjacent roadway per MRCOG information. If site generates more than 100 vehicles per hour, identify v/c ratio on this form.



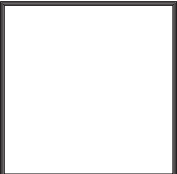
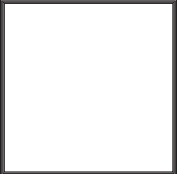
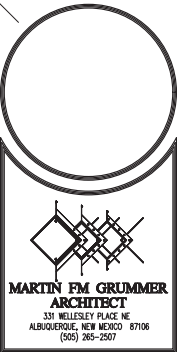
I-25 + SAN MATEO

CONCEPTUAL SITEPLAN

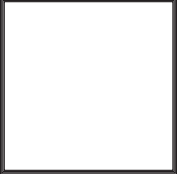
ALBUQUERQUE, NEW MEXICO



BUILDING	SQ. FT.	USE/AREA	FACTOR	PARKING REQUIRED	PARKING PROVIDED	INCLUDES HC (REQ) PARKING	PLUS MC (REQ) PARKING	BICYCLE PARKING (REQ)
BLDG 1	2,472 SF	DRIVE THRU	STACKING	12	28 + STACKING	2 (1)	2 (1)	3 (3)
BLDG 2	2,472 SF	DRIVE THRU	STACKING	12	16 + STACKING	2 (1)	2 (1)	3 (3)
BLDG 3	5,002 SF	RETAIL	41000	21	28 + STACKING	2 (1)	2 (1)	3 (3)
				45	73 + STACKING	6 (3)	6 (3)	9 (9)



I-25 & SAN MATEO BLVD NE
SE CORNER
ALBUQUERQUE, NM 87114
SITE DEVELOPMENT PLAN



DATE: 8 FEB 2023
DRAWN BY: MFMG
CHECKED BY:
VERIFIED BY:

REVISIONS

SHEET NO:
A1.1
A-8

ALL DIMENSIONS ARE TO BE FIELD VERIFIED. IF THERE ARE DISCREPANCIES, PLEASE NOTIFY THE ARCHITECT. DRAWING ARE NOT TO BE SCALED. USE DIMENSIONS FOR ACCURACY.

APPENDIX B

Existing Traffic Data

Route_Name	Location	Count_Date	Total_Vol	Dir1_D_V ol	Dir1_Vol	Dir2_D_ Vol	Dir2	AMPH _Begin	AMPH_V ol	AMPH _Perc	AMPH _Split	AMPH _PkDir	PMPH _Begin	PMPH_V ol	PMPH _Perc	PMPH _Split	PMPH _PkDir
PAN AM. EAST	NORTH OF SAN MATEO	04/09/2019	17,184	17,184 N				730	1,164	6.77		1 N	1630	1,585	9.22		1 N
PAN AM. EAST	SOUTH OF HARPER	01/24/2012	17,460	17,460 N				715	1,171	6.71		1 N	1645	1,869	10.7		1 N
PAN AM. EAST	NORTH OF HARPER	04/09/2019	5,582	5,582 N				730	364	6.52		1 N	1645	605	10.84		1 N
OSUNA	EAST OF PAN AM. WEST	01/14/2019	41,165	19,177 E	21,988 W			715	3,638	8.84	0.61 W		1630	3,282	7.97	0.56 E	
SAN MATEO	WEST OF PAN AM. EAST	11/01/1994	42,816	19,695 W	23,121 E			715	3,732	8.72	0.59 W		1630	3,800	8.88	0.64 E	
SAN MATEO	I-25 UNDERPASS	09/10/2019	46,208	22,864 E	23,344 W			715	3,982	8.62	0.56 W		1630	4,081	8.83	0.59 E	
SAN MATEO INTCH.	I-25 SBD OFF RAMP TO EBD SAN MATEO	11/27/2018	6,493	6,493 S				800	580	8.93		1 S	1645	623	9.59		1 S
HARPER DR.	EAST OF PAN AM. EAST	02/14/2017	3,583	2,468 E	1,115 W			715	390	10.88	0.71 E		1645	340	9.49	0.67 E	
HARPER DR.	EAST OF PAN AMERICAN RD.	05/02/2017	3,300	2,230 E	1,070 W			715	304	9.21	0.62 E		1700	295	8.94	0.74 E	
PAN AM. EAST	SOUTH OF SAN MATEO	11/06/2017	13,352	13,352 N				715	1,069	8.01		1 N	1615	1,080	8.09		1 N
PAN AMERICAN	SOUTH OF HARPER	01/23/2018	6,602	2,514 N	4,088 S			730	609	9.22	0.7 S		1530	516	7.82	0.69 S	
SAN MATEO	NORTHWEST OF PAN AM. ROAD	02/01/1992	37,833	17,388 E	20,445 W			700	2,912	7.7	0.63 W		1645	3,213	8.49	0.55 E	
SAN MATEO	EAST OF PAN AM. EAST	01/14/2020	49,392	22,118 E	27,274 W			715	3,854	7.8	0.61 W		1645	4,260	8.62	0.51 E	
SAN MATEO	NORTH OF ACADEMY	03/31/2020	24,534	12,990 N	11,544 S			715	1,480	6.03	0.62 N		1630	2,104	8.58	0.54 S	
SAN MATEO	SOUTHEAST OF PAN AM. ROAD	02/01/1992	36,059	20,074 N	15,985 S			700	2,681	7.44	0.63 N		1645	3,139	8.71	0.54 S	
PAN AM. WEST	SOUTH OF OSUNA	12/16/2019	15,562	15,562 S				715	1,266	8.14		1 S	1630	1,335	8.58		1 S
PAN MATEO INTCH.	I-25 NBD OFF RAMP	11/27/2018	10,875	10,875 N				715	969	8.91		1 N	1700	885	8.14		1 N
I-25 NBD	SAN MATEO OFF RAMP - SAN ANTONIO OFF RAMP	10/16/2018	61,662	61,662 N				715	4,295	6.97		1 N	1530	5,304	8.6		1 N
I-25 SBD	SAN MATEO ON RAMP - SAN MATEO OFF RAMP	06/05/2012	65,667	65,667 S				700	5,655	8.61		1 S	1615	5,060	7.71		1 S
SAN MATEO INTCH.	I-25 SBD ON RAMP	11/27/2018	11,243	11,243 S				715	883	7.85		1 S	1515	811	7.21		1 S
ACADEMY	EAST OF SAN MATEO	09/11/2017	30,556	14,302 E	16,254 W			715	2,453	8.03	0.7 W		1700	2,591	8.48	0.57 E	
PAN AM. WEST	SOUTH OF SAN MATEO INTCH ON RAMP	08/28/2018	3,910	3,910 S				745	534	13.66		1 S	1530	468	11.97		1 S
PAN AM. EAST	NORTH OF OSUNA	01/23/2018	4,184	4,184 N				730	250	5.98		1 N	1700	530	12.67		1 N

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : I25 West.San Mateo West

Site Code : 00000001

Start Date : 8/16/2023

Page No : 1

Groups Printed- Cars - Trucks

	I25 AM Southbound					SAN MATEO AM Westbound					I25 AM Northbound					SAN MATEO AM Eastbound						
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total	
07:00	17	20	7	1	45	125	249	0	0	374	0	0	0	0	0	0	119	30	0	149	568	
07:15	22	28	10	0	60	161	292	0	1	454	0	0	0	0	0	0	179	48	0	227	741	
07:30	27	31	25	0	83	180	363	0	1	544	0	0	0	0	0	0	200	47	0	247	874	
07:45	71	133	47	0	251	149	399	0	1	549	0	0	0	0	0	0	210	57	0	267	1067	
Total	137	212	89	1	439	615	1303	0	3	1921	0	0	0	0	0	0	708	182	0	890	3250	
08:00	100	166	24	0	290	115	331	0	0	446	0	0	0	0	0	0	199	48	0	247	983	
08:15	92	159	23	0	274	131	284	13	0	428	0	0	0	0	0	0	206	49	0	255	957	
08:30	98	129	25	0	252	112	273	18	3	406	0	0	0	0	0	0	191	54	0	245	903	
08:45	21	20	5	0	46	129	251	0	1	381	0	0	0	0	0	0	178	36	0	214	641	
Total	311	474	77	0	862	487	1139	31	4	1661	0	0	0	0	0	0	774	187	0	961	3484	
*** BREAK ***																						
16:00	28	55	8	0	91	145	236	0	0	381	0	0	0	0	0	0	251	66	0	317	789	
16:15	29	37	5	0	71	114	268	0	1	383	0	0	0	0	0	0	285	67	0	352	806	
16:30	53	63	11	0	127	142	258	0	0	400	0	0	0	0	0	0	269	57	0	326	853	
16:45	39	46	21	0	106	98	266	0	1	365	0	0	0	0	0	0	290	66	0	356	827	
Total	149	201	45	0	395	499	1028	0	2	1529	0	0	0	0	0	0	1095	256	0	1351	3275	
17:00	53	52	18	0	123	139	228	0	1	368	0	0	0	0	0	0	326	87	0	413	904	
17:15	33	50	13	0	96	112	262	0	1	375	0	0	0	0	0	0	322	62	0	384	855	
17:30	29	43	6	0	78	138	248	0	1	387	0	0	0	0	0	0	229	44	0	273	738	
17:45	19	30	5	0	54	117	203	0	0	320	0	0	0	0	0	0	3	217	51	0	271	645
Total	134	175	42	0	351	506	941	0	3	1450	0	0	0	0	0	0	3	1094	244	0	1341	3142
Grand Total	731	1062	253	1	2047	2107	4411	31	12	6561	0	0	0	0	0	0	3	3671	869	0	4543	13151
Apprch %	35.7	51.9	12.4	0		32.1	67.2	0.5	0.2		0	0	0	0		0.1	80.8	19.1	0			
Total %	5.6	8.1	1.9	0	15.6	16	33.5	0.2	0.1	49.9	0	0	0	0	0	0	27.9	6.6	0	34.5		
Cars	724	1050	243	1	2018	2091	4338	29	12	6470	0	0	0	0	0	0	3	3629	857	0	4489	12977
% Cars	99	98.9	96	100	98.6	99.2	98.3	93.5	100	98.6	0	0	0	0	0	0	100	98.9	98.6	0	98.8	98.7
Trucks	7	12	10	0	29	16	73	2	0	91	0	0	0	0	0	0	0	42	12	0	54	174
% Trucks	1	1.1	4	0	1.4	0.8	1.7	6.5	0	1.4	0	0	0	0	0	0	0	1.1	1.4	0	1.2	1.3

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : I25 West.San Mateo West

Site Code : 00000001

Start Date : 8/16/2023

Page No : 2

	I25 AM Southbound					SAN MATEO AM Westbound					I25 AM Northbound					SAN MATEO AM Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total

Peak Hour Analysis From 07:00 to 11:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 07:45

07:45	71	133	47	0	251	149	399	0	1	549	0	0	0	0	0	0	210	57	0	267	1067
08:00	100	166	24	0	290	115	331	0	0	446	0	0	0	0	0	0	199	48	0	247	983
08:15	92	159	23	0	274	131	284	13	0	428	0	0	0	0	0	0	206	49	0	255	957
08:30	98	129	25	0	252	112	273	18	3	406	0	0	0	0	0	0	191	54	0	245	903
Total Volume	361	587	119	0	1067	507	1287	31	4	1829	0	0	0	0	0	0	806	208	0	1014	3910
% App. Total	33.8	55	11.2	0		27.7	70.4	1.7	0.2		0	0	0	0		0	79.5	20.5	0		
PHF	.903	.884	.633	.000	.920	.851	.806	.431	.333	.833	.000	.000	.000	.000	.000	.000	.960	.912	.000	.949	.916
Cars	357	577	116	0	1050	501	1283	29	4	1817	0	0	0	0	0	0	795	207	0	1002	3869
% Cars	98.9	98.3	97.5	0	98.4	98.8	99.7	93.5	100	99.3	0	0	0	0	0	0	98.6	99.5	0	98.8	99.0
Trucks	4	10	3	0	17	6	4	2	0	12	0	0	0	0	0	0	11	1	0	12	41
% Trucks	1.1	1.7	2.5	0	1.6	1.2	0.3	6.5	0	0.7	0	0	0	0	0	0	1.4	0.5	0	1.2	1.0

Peak Hour Analysis From 12:00 to 17:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 16:30

16:30	53	63	11	0	127	142	258	0	0	400	0	0	0	0	0	0	269	57	0	326	853
16:45	39	46	21	0	106	98	266	0	1	365	0	0	0	0	0	0	290	66	0	356	827
17:00	53	52	18	0	123	139	228	0	1	368	0	0	0	0	0	0	326	87	0	413	904
17:15	33	50	13	0	96	112	262	0	1	375	0	0	0	0	0	0	322	62	0	384	855
Total Volume	178	211	63	0	452	491	1014	0	3	1508	0	0	0	0	0	0	1207	272	0	1479	3439
% App. Total	39.4	46.7	13.9	0		32.6	67.2	0	0.2		0	0	0	0		0	81.6	18.4	0		
PHF	.840	.837	.750	.000	.890	.864	.953	.000	.750	.943	.000	.000	.000	.000	.000	.000	.926	.782	.000	.895	.951
Cars	177	211	58	0	446	484	976	0	3	1463	0	0	0	0	0	0	1202	270	0	1472	3381
% Cars	99.4	100	92.1	0	98.7	98.6	96.3	0	100	97.0	0	0	0	0	0	0	99.6	99.3	0	99.5	98.3
Trucks	1	0	5	0	6	7	38	0	0	45	0	0	0	0	0	0	5	2	0	7	58
% Trucks	0.6	0	7.9	0	1.3	1.4	3.7	0	0	3.0	0	0	0	0	0	0	0.4	0.7	0	0.5	1.7

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2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : 2_San Mateo-I-25 East

Site Code : 00000002

Start Date : 8/15/2023

Page No : 1

Groups Printed- Cars - Trucks

	I25 EAST Southbound						SAN MATEO Westbound					I25 EAST Northbound						SAN MATEO Eastbound						
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total			
07:00	0	1	0	0	1	0	266	99	0	365	97	41	21	0	159	24	185	0	0	209	734			
07:15	0	0	0	0	0	0	369	126	0	495	102	46	35	0	183	68	278	0	0	346	1024			
07:30	0	0	0	0	0	0	479	144	1	624	112	53	26	0	191	68	339	1	0	408	1223			
07:45	0	0	0	0	0	0	491	149	0	640	122	57	47	0	226	73	387	1	0	461	1327			
Total	0	1	0	0	1	0	1605	518	1	2124	433	197	129	0	759	233	1189	2	0	1424	4308			
08:00	0	1	0	0	1	0	327	138	1	466	110	57	55	0	222	55	416	0	1	472	1161			
08:15	0	0	0	0	0	0	345	122	0	467	79	28	28	0	135	40	326	0	0	366	968			
08:30	0	0	0	0	0	0	277	154	0	431	86	36	44	0	166	47	422	0	0	469	1066			
08:45	0	0	0	0	0	0	280	144	0	424	83	65	58	0	206	55	341	9	0	405	1035			
Total	0	1	0	0	1	0	1229	558	1	1788	358	186	185	0	729	197	1505	9	1	1712	4230			
*** BREAK ***																								
16:00	0	1	0	0	1	0	291	213	0	504	88	83	66	0	237	61	402	0	1	464	1206			
16:15	0	0	0	0	0	0	306	229	0	535	92	95	52	0	239	63	476	0	1	540	1314			
16:30	0	0	0	0	0	0	286	214	0	500	97	94	83	0	274	95	530	0	0	625	1399			
16:45	0	0	0	0	0	0	302	267	0	569	78	89	57	0	224	82	478	0	0	560	1353			
Total	0	1	0	0	1	0	1185	923	0	2108	355	361	258	0	974	301	1886	0	2	2189	5272			
17:00	0	1	0	0	1	0	281	200	0	481	75	113	66	0	254	79	593	0	0	672	1408			
17:15	0	0	0	0	0	0	282	253	0	535	76	110	100	0	286	62	576	0	0	638	1459			
17:30	0	0	0	0	0	0	269	207	0	476	91	76	62	0	229	63	452	0	0	515	1220			
17:45	0	0	0	0	0	0	260	234	0	494	70	66	50	0	186	45	394	0	0	439	1119			
Total	0	1	0	0	1	0	1092	894	0	1986	312	365	278	0	955	249	2015	0	0	2264	5206			
Grand Total	0	4	0	0	4	0	5111	2893	2	8006	1458	1109	850	0	3417	980	6595	11	3	7589	19016			
Apprch %	0	100	0	0		0	63.8	36.1	0		42.7	32.5	24.9	0		12.9	86.9	0.1	0					
Total %	0	0	0	0	0	0	26.9	15.2	0	42.1	7.7	5.8	4.5	0	18	5.2	34.7	0.1	0	39.9				
Cars	0	4	0	0	4	0	4983	2865	0	7848	1391	1091	821	0	3303	945	6505	11	3	7464	18619			
% Cars	0	100	0	0	100	0	97.5	99	0	98	95.4	98.4	96.6	0	96.7	96.4	98.6	100	100	98.4	97.9			
Trucks	0	0	0	0	0	0	128	28	2	158	67	18	29	0	114	35	90	0	0	125	397			
% Trucks	0	0	0	0	0	0	2.5	1	100	2	4.6	1.6	3.4	0	3.3	3.6	1.4	0	0	1.6	2.1			

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : 2_San Mateo-I-25 East

Site Code : 00000002

Start Date : 8/15/2023

Page No : 2

	I25 EAST Southbound					SAN MATEO Westbound					I25 EAST Northbound					SAN MATEO Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour Analysis From 07:00 to 11:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15																					
07:15	0	0	0	0	0	0	369	126	0	495	102	46	35	0	183	68	278	0	0	346	1024
07:30	0	0	0	0	0	0	479	144	1	624	112	53	26	0	191	68	339	1	0	408	1223
07:45	0	0	0	0	0	0	491	149	0	640	122	57	47	0	226	73	387	1	0	461	1327
08:00	0	1	0	0	1	0	327	138	1	466	110	57	55	0	222	55	416	0	1	472	1161
Total Volume	0	1	0	0	1	0	1666	557	2	2225	446	213	163	0	822	264	1420	2	1	1687	4735
% App. Total	0	100	0	0		0	74.9	25	0.1		54.3	25.9	19.8	0		15.6	84.2	0.1	0.1		
PHF	.000	.250	.000	.000	.250	.000	.848	.935	.500	.869	.914	.934	.741	.000	.909	.904	.853	.500	.250	.894	.892
Cars	0	1	0	0	1	0	1650	549	0	2199	432	204	150	0	786	246	1385	2	1	1634	4620
% Cars	0	100	0	0	100	0	99.0	98.6	0	98.8	96.9	95.8	92.0	0	95.6	93.2	97.5	100	100	96.9	97.6
Trucks	0	0	0	0	0	0	16	8	2	26	14	9	13	0	36	18	35	0	0	53	115
% Trucks	0	0	0	0	0	0	1.0	1.4	100	1.2	3.1	4.2	8.0	0	4.4	6.8	2.5	0	0	3.1	2.4
Peak Hour Analysis From 12:00 to 17:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 16:30																					
16:30	0	0	0	0	0	0	286	214	0	500	97	94	83	0	274	95	530	0	0	625	1399
16:45	0	0	0	0	0	0	302	267	0	569	78	89	57	0	224	82	478	0	0	560	1353
17:00	0	1	0	0	1	0	281	200	0	481	75	113	66	0	254	79	593	0	0	672	1408
17:15	0	0	0	0	0	0	282	253	0	535	76	110	100	0	286	62	576	0	0	638	1459
Total Volume	0	1	0	0	1	0	1151	934	0	2085	326	406	306	0	1038	318	2177	0	0	2495	5619
% App. Total	0	100	0	0		0	55.2	44.8	0		31.4	39.1	29.5	0		12.7	87.3	0	0		
PHF	.000	.250	.000	.000	.250	.000	.953	.875	.000	.916	.840	.898	.765	.000	.907	.837	.918	.000	.000	.928	.963
Cars	0	1	0	0	1	0	1082	928	0	2010	302	402	303	0	1007	317	2156	0	0	2473	5491
% Cars	0	100	0	0	100	0	94.0	99.4	0	96.4	92.6	99.0	99.0	0	97.0	99.7	99.0	0	0	99.1	97.7
Trucks	0	0	0	0	0	0	69	6	0	75	24	4	3	0	31	1	21	0	0	22	128
% Trucks	0	0	0	0	0	0	6.0	0.6	0	3.6	7.4	1.0	1.0	0	3.0	0.3	1.0	0	0	0.9	2.3

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : 3_San Mateo-Pan American

Site Code : 00000003

Start Date : 8/16/2023

Page No : 1

Groups Printed- Cars - Trucks

	SAN MATEO Southbound					PAN AMERICAN Westbound					SAN MATEO Northbound					PAN AMERICAN Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
07:00	21	204	4	0	229	5	1	12	0	18	1	318	20	0	339	1	1	0	0	2	588
07:15	18	326	1	0	345	26	1	28	0	55	1	474	11	0	486	0	1	0	0	1	887
07:30	13	358	1	0	372	15	0	39	0	54	0	497	8	0	505	1	3	0	2	6	937
07:45	29	357	4	0	390	28	1	48	0	77	0	604	22	0	626	0	0	0	0	0	1093
Total	81	1245	10	0	1336	74	3	127	0	204	2	1893	61	0	1956	2	5	0	2	9	3505
08:00	23	288	4	0	315	22	1	23	0	46	0	471	32	0	503	1	0	0	1	2	866
08:15	14	286	4	0	304	19	2	39	0	60	1	465	22	0	488	1	0	0	1	2	854
08:30	17	330	5	0	352	17	0	29	0	46	3	467	19	0	489	1	2	0	2	5	892
08:45	27	393	4	0	424	29	0	17	0	46	1	439	21	0	461	2	3	0	3	8	939
Total	81	1297	17	0	1395	87	3	108	0	198	5	1842	94	0	1941	5	5	0	7	17	3551

*** BREAK ***

16:00	8	468	5	0	481	36	0	37	0	73	1	465	29	0	495	6	0	0	0	6	1055
16:15	22	441	5	0	468	31	2	16	1	50	7	495	25	0	527	5	0	0	0	5	1050
16:30	20	472	1	0	493	35	0	21	0	56	1	562	31	1	595	2	2	2	2	8	1152
16:45	17	508	3	0	528	20	2	28	1	51	1	551	22	0	574	11	2	1	4	18	1171
Total	67	1889	14	0	1970	122	4	102	2	230	10	2073	107	1	2191	24	4	3	6	37	4428
17:00	10	593	3	0	606	30	1	30	0	61	1	545	22	1	569	8	1	1	3	13	1249
17:15	17	476	0	0	493	22	0	20	2	44	1	479	18	0	498	3	2	0	0	5	1040
17:30	13	368	1	0	382	9	0	31	1	41	0	496	25	0	521	0	1	1	2	4	948
17:45	12	346	1	0	359	19	0	15	0	34	0	429	19	0	448	1	0	0	2	3	844
Total	52	1783	5	0	1840	80	1	96	3	180	2	1949	84	1	2036	12	4	2	7	25	4081

*** BREAK ***

Grand Total	281	6214	46	0	6541	363	11	433	5	812	19	7757	346	2	8124	43	18	5	22	88	15565
Apprch %	4.3	95	0.7	0		44.7	1.4	53.3	0.6		0.2	95.5	4.3	0		48.9	20.5	5.7	25		
Total %	1.8	39.9	0.3	0	42	2.3	0.1	2.8	0	5.2	0.1	49.8	2.2	0	52.2	0.3	0.1	0	0.1	0.6	
Cars	279	6107	43	0	6429	356	11	427	4	798	17	7602	341	2	7962	43	17	5	22	87	15276
% Cars	99.3	98.3	93.5	0	98.3	98.1	100	98.6	80	98.3	89.5	98	98.6	100	98	100	94.4	100	100	98.9	98.1
Trucks	2	107	3	0	112	7	0	6	1	14	2	155	5	0	162	0	1	0	0	1	289
% Trucks	0.7	1.7	6.5	0	1.7	1.9	0	1.4	20	1.7	10.5	2	1.4	0	2	0	5.6	0	0	1.1	1.9

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : 3_San Mateo-Pan American

Site Code : 00000003

Start Date : 8/16/2023

Page No : 2

	SAN MATEO Southbound					PAN AMERICAN Westbound					SAN MATEO Northbound					PAN AMERICAN Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour Analysis From 07:00 to 11:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15																					
07:15	18	326	1	0	345	26	1	28	0	55	1	474	11	0	486	0	1	0	0	1	887
07:30	13	358	1	0	372	15	0	39	0	54	0	497	8	0	505	1	3	0	2	6	937
07:45	29	357	4	0	390	28	1	48	0	77	0	604	22	0	626	0	0	0	0	0	1093
08:00	23	288	4	0	315	22	1	23	0	46	0	471	32	0	503	1	0	0	1	2	866
Total Volume	83	1329	10	0	1422	91	3	138	0	232	1	2046	73	0	2120	2	4	0	3	9	3783
% App. Total	5.8	93.5	0.7	0		39.2	1.3	59.5	0		0	96.5	3.4	0		22.2	44.4	0	33.3		
PHF	.716	.928	.625	.000	.912	.813	.750	.719	.000	.753	.250	.847	.570	.000	.847	.500	.333	.000	.375	.375	.865
Cars	82	1293	10	0	1385	88	3	138	0	229	1	2018	73	0	2092	2	4	0	3	9	3715
% Cars	98.8	97.3	100	0	97.4	96.7	100	100	0	98.7	100	98.6	100	0	98.7	100	100	0	100	100	98.2
Trucks	1	36	0	0	37	3	0	0	0	3	0	28	0	0	28	0	0	0	0	0	68
% Trucks	1.2	2.7	0	0	2.6	3.3	0	0	0	1.3	0	1.4	0	0	1.3	0	0	0	0	0	1.8
Peak Hour Analysis From 12:00 to 18:00 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 16:15																					
16:15	22	441	5	0	468	31	2	16	1	50	7	495	25	0	527	5	0	0	0	5	1050
16:30	20	472	1	0	493	35	0	21	0	56	1	562	31	1	595	2	2	2	2	8	1152
16:45	17	508	3	0	528	20	2	28	1	51	1	551	22	0	574	11	2	1	4	18	1171
17:00	10	593	3	0	606	30	1	30	0	61	1	545	22	1	569	8	1	1	3	13	1249
Total Volume	69	2014	12	0	2095	116	5	95	2	218	10	2153	100	2	2265	26	5	4	9	44	4622
% App. Total	3.3	96.1	0.6	0		53.2	2.3	43.6	0.9		0.4	95.1	4.4	0.1		59.1	11.4	9.1	20.5		
PHF	.784	.849	.600	.000	.864	.829	.625	.792	.500	.893	.357	.958	.806	.500	.952	.591	.625	.500	.563	.611	.925
Cars	69	2001	12	0	2082	114	5	93	1	213	10	2082	97	2	2191	26	5	4	9	44	4530
% Cars	100	99.4	100	0	99.4	98.3	100	97.9	50.0	97.7	100	96.7	97.0	100	96.7	100	100	100	100	100	98.0
Trucks	0	13	0	0	13	2	0	2	1	5	0	71	3	0	74	0	0	0	0	0	92
% Trucks	0	0.6	0	0	0.6	1.7	0	2.1	50.0	2.3	0	3.3	3.0	0	3.3	0	0	0	0	0	2.0

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : 4_SAN MATEO-JACKALOPE

Site Code : 00000004

Start Date : 8/16/2023

Page No : 1

Groups Printed- Cars - Trucks

	SAN MATEO Southbound					JACKALOPE Westbound					SAN MATEO Northbound					JACKALOPE Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
07:00	2	224	0	0	226	1	0	7	0	8	0	338	2	0	340	0	0	0	0	0	574
07:15	1	362	0	0	363	1	0	4	0	5	0	531	3	0	534	0	0	0	0	0	902
07:30	5	442	0	0	447	0	0	1	0	1	0	606	10	0	616	0	0	0	0	0	1064
07:45	5	390	0	0	395	0	0	5	0	5	0	666	7	1	674	0	0	0	0	0	1074
Total	13	1418	0	0	1431	2	0	17	0	19	0	2141	22	1	2164	0	0	0	0	0	3614
08:00	4	363	0	0	367	1	0	4	0	5	0	529	10	1	540	0	0	0	0	0	912
08:15	3	367	0	0	370	3	0	5	0	8	0	513	4	1	518	0	0	0	0	0	896
08:30	5	392	0	0	397	1	0	1	0	2	0	451	9	3	463	0	0	0	1	1	863
08:45	12	487	0	0	499	2	0	3	1	6	0	463	1	0	464	0	0	0	0	0	969
Total	24	1609	0	0	1633	7	0	13	1	21	0	1956	24	5	1985	0	0	0	1	1	3640
*** BREAK ***																					
16:00	7	591	0	0	598	0	0	3	0	3	0	531	0	1	532	0	0	0	0	0	1133
16:15	5	527	0	0	532	1	0	4	0	5	0	521	2	0	523	0	0	0	0	0	1060
16:30	6	562	0	0	568	0	0	2	0	2	0	555	3	0	558	0	0	0	1	1	1129
16:45	4	597	0	0	601	0	0	5	0	5	0	621	4	1	626	0	0	0	0	0	1232
Total	22	2277	0	0	2299	1	0	14	0	15	0	2228	9	2	2239	0	0	0	1	1	4554
17:00	11	646	0	0	657	0	0	5	0	5	0	582	3	3	588	0	0	0	0	0	1250
17:15	9	525	0	0	534	1	0	7	0	8	0	533	4	0	537	0	0	0	0	0	1079
17:30	3	494	0	0	497	0	0	3	0	3	0	551	7	0	558	0	0	0	1	1	1059
17:45	6	430	0	0	436	0	0	7	0	7	0	496	6	1	503	0	0	0	2	2	948
Total	29	2095	0	0	2124	1	0	22	0	23	0	2162	20	4	2186	0	0	0	3	3	4336
Grand Total	88	7399	0	0	7487	11	0	66	1	78	0	8487	75	12	8574	0	0	0	5	5	16144
Apprch %	1.2	98.8	0	0		14.1	0	84.6	1.3		0	99	0.9	0.1		0	0	0	100		
Total %	0.5	45.8	0	0	46.4	0.1	0	0.4	0	0.5	0	52.6	0.5	0.1	53.1	0	0	0	0	0	
Cars	86	7269	0	0	7355	9	0	65	1	75	0	8329	72	12	8413	0	0	0	5	5	15848
% Cars	97.7	98.2	0	0	98.2	81.8	0	98.5	100	96.2	0	98.1	96	100	98.1	0	0	0	100	100	98.2
Trucks	2	130	0	0	132	2	0	1	0	3	0	158	3	0	161	0	0	0	0	0	296
% Trucks	2.3	1.8	0	0	1.8	18.2	0	1.5	0	3.8	0	1.9	4	0	1.9	0	0	0	0	0	1.8

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : 4_SAN MATEO-JACKALOPE

Site Code : 00000004

Start Date : 8/16/2023

Page No : 2

	SAN MATEO Southbound					JACKALOPE Westbound					SAN MATEO Northbound					JACKALOPE Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour Analysis From 07:00 to 12:30 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15																					
07:15	1	362	0	0	363	1	0	4	0	5	0	531	3	0	534	0	0	0	0	0	902
07:30	5	442	0	0	447	0	0	1	0	1	0	606	10	0	616	0	0	0	0	0	1064
07:45	5	390	0	0	395	0	0	5	0	5	0	666	7	1	674	0	0	0	0	0	1074
08:00	4	363	0	0	367	1	0	4	0	5	0	529	10	1	540	0	0	0	0	0	912
Total Volume	15	1557	0	0	1572	2	0	14	0	16	0	2332	30	2	2364	0	0	0	0	0	3952
% App. Total	1	99	0	0		12.5	0	87.5	0		0	98.6	1.3	0.1		0	0	0	0		
PHF	.750	.881	.000	.000	.879	.500	.000	.700	.000	.800	.000	.875	.750	.500	.877	.000	.000	.000	.000	.000	.920
Cars	15	1509	0	0	1524	2	0	14	0	16	0	2303	29	2	2334	0	0	0	0	0	3874
% Cars	100	96.9	0	0	96.9	100	0	100	0	100	0	98.8	96.7	100	98.7	0	0	0	0	0	98.0
Trucks	0	48	0	0	48	0	0	0	0	0	0	29	1	0	30	0	0	0	0	0	78
% Trucks	0	3.1	0	0	3.1	0	0	0	0	0	0	1.2	3.3	0	1.3	0	0	0	0	0	2.0
Peak Hour Analysis From 12:45 to 17:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 16:30																					
16:30	6	562	0	0	568	0	0	2	0	2	0	555	3	0	558	0	0	0	1	1	1129
16:45	4	597	0	0	601	0	0	5	0	5	0	621	4	1	626	0	0	0	0	0	1232
17:00	11	646	0	0	657	0	0	5	0	5	0	582	3	3	588	0	0	0	0	0	1250
17:15	9	525	0	0	534	1	0	7	0	8	0	533	4	0	537	0	0	0	0	0	1079
Total Volume	30	2330	0	0	2360	1	0	19	0	20	0	2291	14	4	2309	0	0	0	1	1	4690
% App. Total	1.3	98.7	0	0		5	0	95	0		0	99.2	0.6	0.2		0	0	0	100		
PHF	.682	.902	.000	.000	.898	.250	.000	.679	.000	.625	.000	.922	.875	.333	.922	.000	.000	.000	.250	.250	.938
Cars	30	2315	0	0	2345	1	0	18	0	19	0	2219	13	4	2236	0	0	0	1	1	4601
% Cars	100	99.4	0	0	99.4	100	0	94.7	0	95.0	0	96.9	92.9	100	96.8	0	0	0	100	100	98.1
Trucks	0	15	0	0	15	0	0	1	0	1	0	72	1	0	73	0	0	0	0	0	89
% Trucks	0	0.6	0	0	0.6	0	0	5.3	0	5.0	0	3.1	7.1	0	3.2	0	0	0	0	0	1.9

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : 5_SAN MATEO_FAR NORTH SC

Site Code : 00000005

Start Date : 8/16/2023

Page No : 1

Groups Printed- Cars - Trucks

	SAN MATEO Southbound					FAR NORTH SC Westbound					SAN MATEO Northbound					FAR NORTH SC Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
07:00	11	213	0	0	224	0	0	1	0	1	0	368	0	0	368	1	0	0	0	1	594
07:15	16	344	2	0	362	0	0	1	0	1	4	514	1	1	520	0	0	1	0	1	884
07:30	12	409	3	2	426	1	0	1	0	2	4	572	2	0	578	0	0	3	0	3	1009
07:45	14	381	2	0	397	0	0	4	0	4	3	660	3	1	667	0	0	0	0	0	1068
Total	53	1347	7	2	1409	1	0	7	0	8	11	2114	6	2	2133	1	0	4	0	5	3555
08:00	10	349	2	0	361	1	0	6	0	7	7	514	7	0	528	0	0	0	0	0	896
08:15	15	345	3	0	363	1	0	3	0	4	4	485	4	2	495	0	0	4	0	4	866
08:30	21	382	2	0	405	0	0	8	0	8	6	454	1	1	462	0	0	2	0	2	877
08:45	18	481	4	1	504	0	0	4	0	4	9	425	3	0	437	0	0	2	0	2	947
Total	64	1557	11	1	1633	2	0	21	0	23	26	1878	15	3	1922	0	0	8	0	8	3586
*** BREAK ***																					
16:00	27	502	1	0	530	0	0	17	0	17	12	470	3	0	485	0	0	6	0	6	1038
16:15	21	511	1	0	533	0	0	13	0	13	6	467	11	1	485	0	0	8	0	8	1039
16:30	25	511	1	0	537	0	0	23	0	23	1	493	5	1	500	0	0	6	1	7	1067
16:45	26	587	2	1	616	0	0	23	0	23	6	574	7	0	587	0	0	3	0	3	1229
Total	99	2111	5	1	2216	0	0	76	0	76	25	2004	26	2	2057	0	0	23	1	24	4373
17:00	41	544	1	0	586	0	0	22	0	22	7	483	4	0	494	0	0	9	1	10	1112
17:15	40	512	0	1	553	0	0	21	0	21	4	517	7	0	528	0	0	10	0	10	1112
17:30	42	451	2	2	497	0	0	21	0	21	1	498	10	0	509	0	0	7	0	7	1034
17:45	23	392	0	0	415	0	0	19	0	19	1	514	2	2	519	0	0	6	0	6	959
Total	146	1899	3	3	2051	0	0	83	0	83	13	2012	23	2	2050	0	0	32	1	33	4217
Grand Total	362	6914	26	7	7309	3	0	187	0	190	75	8008	70	9	8162	1	0	67	2	70	15731
Apprch %	5	94.6	0.4	0.1		1.6	0	98.4	0		0.9	98.1	0.9	0.1		1.4	0	95.7	2.9		
Total %	2.3	44	0.2	0	46.5	0	0	1.2	0	1.2	0.5	50.9	0.4	0.1	51.9	0	0	0.4	0	0.4	
Cars	362	6781	26	7	7176	3	0	187	0	190	75	7860	70	9	8014	1	0	67	2	70	15450
% Cars	100	98.1	100	100	98.2	100	0	100	0	100	100	98.2	100	100	98.2	100	0	100	100	100	98.2
Trucks	0	133	0	0	133	0	0	0	0	0	0	148	0	0	148	0	0	0	0	0	281
% Trucks	0	1.9	0	0	1.8	0	0	0	0	0	0	1.8	0	0	1.8	0	0	0	0	0	1.8

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : 5_SAN MATEO_FAR NORTH SC

Site Code : 00000005

Start Date : 8/16/2023

Page No : 2

	SAN MATEO Southbound					FAR NORTH SC Westbound					SAN MATEO Northbound					FAR NORTH SC Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour Analysis From 07:00 to 11:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15																					
07:15	16	344	2	0	362	0	0	1	0	1	4	514	1	1	520	0	0	1	0	1	884
07:30	12	409	3	2	426	1	0	1	0	2	4	572	2	0	578	0	0	3	0	3	1009
07:45	14	381	2	0	397	0	0	4	0	4	3	660	3	1	667	0	0	0	0	0	1068
08:00	10	349	2	0	361	1	0	6	0	7	7	514	7	0	528	0	0	0	0	0	896
Total Volume	52	1483	9	2	1546	2	0	12	0	14	18	2260	13	2	2293	0	0	4	0	4	3857
% App. Total	3.4	95.9	0.6	0.1		14.3	0	85.7	0		0.8	98.6	0.6	0.1		0	0	100	0		
PHF	.813	.906	.750	.250	.907	.500	.000	.500	.000	.500	.643	.856	.464	.500	.859	.000	.000	.333	.000	.333	.903
Cars	52	1439	9	2	1502	2	0	12	0	14	18	2234	13	2	2267	0	0	4	0	4	3787
% Cars	100	97.0	100	100	97.2	100	0	100	0	100	100	98.8	100	100	98.9	0	0	100	0	100	98.2
Trucks	0	44	0	0	44	0	0	0	0	0	0	26	0	0	26	0	0	0	0	0	70
% Trucks	0	3.0	0	0	2.8	0	0	0	0	0	0	1.2	0	0	1.1	0	0	0	0	0	1.8
Peak Hour Analysis From 12:00 to 17:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 16:30																					
16:30	25	511	1	0	537	0	0	23	0	23	1	493	5	1	500	0	0	6	1	7	1067
16:45	26	587	2	1	616	0	0	23	0	23	6	574	7	0	587	0	0	3	0	3	1229
17:00	41	544	1	0	586	0	0	22	0	22	7	483	4	0	494	0	0	9	1	10	1112
17:15	40	512	0	1	553	0	0	21	0	21	4	517	7	0	528	0	0	10	0	10	1112
Total Volume	132	2154	4	2	2292	0	0	89	0	89	18	2067	23	1	2109	0	0	28	2	30	4520
% App. Total	5.8	94	0.2	0.1		0	0	100	0		0.9	98	1.1	0		0	0	93.3	6.7		
PHF	.805	.917	.500	.500	.930	.000	.000	.967	.000	.967	.643	.900	.821	.250	.898	.000	.000	.700	.500	.750	.919
Cars	132	2138	4	2	2276	0	0	89	0	89	18	2000	23	1	2042	0	0	28	2	30	4437
% Cars	100	99.3	100	100	99.3	0	0	100	0	100	100	96.8	100	100	96.8	0	0	100	100	100	98.2
Trucks	0	16	0	0	16	0	0	0	0	0	0	67	0	0	67	0	0	0	0	0	83
% Trucks	0	0.7	0	0	0.7	0	0	0	0	0	0	3.2	0	0	3.2	0	0	0	0	0	1.8

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

Turning Movement Counts
San Mateo_I-25

File Name : 6_PAN AMERICAN-HARPER DR

Site Code : 00000006

Start Date : 8/16/2023

Page No : 1

Groups Printed- Cars - Trucks

	PAN AMERICAN Southbound						HARPER DR Westbound					PAN AMERICAN Northbound					HARPER DR Eastbound						
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total		
07:00	0	0	0	0	0	51	8	0	0	59	2	0	29	0	31	0	26	1	0	27	117		
07:15	0	0	0	0	0	63	6	0	0	69	5	0	27	0	32	0	29	0	0	29	130		
07:30	0	0	0	0	0	58	17	0	0	75	3	0	23	0	26	0	23	3	0	26	127		
07:45	0	0	0	0	0	67	13	0	0	80	7	0	38	0	45	0	25	1	0	26	151		
Total	0	0	0	0	0	239	44	0	0	283	17	0	117	0	134	0	103	5	0	108	525		
08:00	0	0	0	0	0	45	6	0	0	51	9	0	50	0	59	0	23	0	0	23	133		
08:15	0	0	0	0	0	55	6	0	0	61	4	0	31	0	35	0	21	1	0	22	118		
08:30	0	0	0	0	0	58	4	0	0	62	4	0	38	2	44	0	30	0	0	30	136		
08:45	0	0	0	0	0	65	9	0	1	75	5	0	44	1	50	0	27	2	0	29	154		
Total	0	0	0	0	0	223	25	0	1	249	22	0	163	3	188	0	101	3	0	104	541		
*** BREAK ***																							
16:00	0	0	0	0	0	77	18	0	0	95	4	0	46	0	50	0	32	0	0	32	177		
16:15	0	0	0	0	0	56	11	0	0	67	4	0	44	1	49	0	26	0	1	27	143		
16:30	0	0	0	0	0	75	13	0	0	88	6	0	35	1	42	0	44	1	0	45	175		
16:45	0	0	0	0	0	50	22	0	0	72	4	0	36	0	40	0	39	1	0	40	152		
Total	0	0	0	0	0	258	64	0	0	322	18	0	161	2	181	0	141	2	1	144	647		
17:00	0	0	0	0	0	87	11	0	0	98	3	0	31	1	35	0	37	0	0	37	170		
17:15	0	0	0	0	0	55	10	0	0	65	4	0	31	0	35	0	25	1	0	26	126		
17:30	0	0	0	0	0	47	9	0	0	56	1	0	35	1	37	0	37	0	0	37	130		
17:45	0	0	0	0	0	44	2	0	0	46	3	0	22	1	26	0	20	1	0	21	93		
Total	0	0	0	0	0	233	32	0	0	265	11	0	119	3	133	0	119	2	0	121	519		
Grand Total	0	0	0	0	0	953	165	0	1	1119	68	0	560	8	636	0	464	12	1	477	2232		
Apprch %	0	0	0	0		85.2	14.7	0	0.1		10.7	0	88.1	1.3		0	97.3	2.5	0.2				
Total %	0	0	0	0	0	42.7	7.4	0	0	50.1	3	0	25.1	0.4	28.5	0	20.8	0.5	0	21.4			
Cars	0	0	0	0	0	939	164	0	1	1104	67	0	552	8	627	0	454	12	1	467	2198		
% Cars	0	0	0	0	0	98.5	99.4	0	100	98.7	98.5	0	98.6	100	98.6	0	97.8	100	100	97.9	98.5		
Trucks	0	0	0	0	0	14	1	0	0	15	1	0	8	0	9	0	10	0	0	10	34		
% Trucks	0	0	0	0	0	1.5	0.6	0	0	1.3	1.5	0	1.4	0	1.4	0	2.2	0	0	2.1	1.5		

Civil Transformations Inc.

2929 Coors Blvd. NW, Suite 309

Albuquerque, NM 87120

Transforming Infrastructure Needs into Sustainable Solutions

File Name : 6_PAN AMERICAN-HARPER DR

Site Code : 00000006

Start Date : 8/16/2023

Page No : 2

	PAN AMERICAN Southbound					HARPER DR Westbound					PAN AMERICAN Northbound					HARPER DR Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour Analysis From 07:00 to 11:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15																					
07:15	0	0	0	0	0	63	6	0	0	69	5	0	27	0	32	0	29	0	0	29	130
07:30	0	0	0	0	0	58	17	0	0	75	3	0	23	0	26	0	23	3	0	26	127
07:45	0	0	0	0	0	67	13	0	0	80	7	0	38	0	45	0	25	1	0	26	151
08:00	0	0	0	0	0	45	6	0	0	51	9	0	50	0	59	0	23	0	0	23	133
Total Volume	0	0	0	0	0	233	42	0	0	275	24	0	138	0	162	0	100	4	0	104	541
% App. Total	0	0	0	0	0	84.7	15.3	0	0	0	14.8	0	85.2	0	0	0	96.2	3.8	0	0	0
PHF	.000	.000	.000	.000	.000	.869	.618	.000	.000	.859	.667	.000	.690	.000	.686	.000	.862	.333	.000	.897	.896
Cars	0	0	0	0	0	232	42	0	0	274	23	0	138	0	161	0	96	4	0	100	535
% Cars	0	0	0	0	0	99.6	100	0	0	99.6	95.8	0	100	0	99.4	0	96.0	100	0	96.2	98.9
Trucks	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	0	4	0	0	4	6
% Trucks	0	0	0	0	0	0.4	0	0	0	0.4	4.2	0	0	0	0.6	0	4.0	0	0	3.8	1.1
Peak Hour Analysis From 12:00 to 17:45 - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 16:00																					
16:00	0	0	0	0	0	77	18	0	0	95	4	0	46	0	50	0	32	0	0	32	177
16:15	0	0	0	0	0	56	11	0	0	67	4	0	44	1	49	0	26	0	1	27	143
16:30	0	0	0	0	0	75	13	0	0	88	6	0	35	1	42	0	44	1	0	45	175
16:45	0	0	0	0	0	50	22	0	0	72	4	0	36	0	40	0	39	1	0	40	152
Total Volume	0	0	0	0	0	258	64	0	0	322	18	0	161	2	181	0	141	2	1	144	647
% App. Total	0	0	0	0	0	80.1	19.9	0	0	0	9.9	0	89	1.1	0	0	97.9	1.4	0.7	0	0
PHF	.000	.000	.000	.000	.000	.838	.727	.000	.000	.847	.750	.000	.875	.500	.905	.000	.801	.500	.250	.800	.914
Cars	0	0	0	0	0	253	64	0	0	317	18	0	160	2	180	0	140	2	1	143	640
% Cars	0	0	0	0	0	98.1	100	0	0	98.4	100	0	99.4	100	99.4	0	99.3	100	100	99.3	98.9
Trucks	0	0	0	0	0	5	0	0	0	5	0	0	1	0	1	0	1	0	0	1	7
% Trucks	0	0	0	0	0	1.9	0	0	0	1.6	0	0	0.6	0	0.6	0	0.7	0	0	0.7	1.1

Intersection No.: 17

CENTRAC
1

Intersection Name: OSUNA & I-25 SB FRONTAGE

Revision Date 07/07/2020

Timing Data

Phase I.D.:	1	2	3	4	5	6	7	8
Phase Dir.:	W-S	EB				WB		SB
Min Grn	3	16				16		8
Walk:	0	7				7		7
Ped Clr:	0	10				14		28
Veh Ext:	1.5	4.0				4.0		2.0
Veh Ext2:	1.5	4.0				4.0		2.0
Max 1:	20	32				32		18
Max 2:	20	32				32		18
Max 3:								
Yellow:	4.0	4.0				4.0		4.5
Red Clr	1.0	1.0				1.0		1.0

Recall Data

Locking Memory:							
Vehicle Recall:							
Ped Recall:							
Recall To Max:		X				X	

Flash Mode: ALL RED

Start Up Mode: ALL RED

Time: 8 SEC.

First Phases: 2 & 6

Start In: GREEN

Overlap Phases: NONE

Overlap	Par Ph	Grn	Yel	Red
A				
B				
C				
D				

NOTES:	1. State Highway project - new geometrics - controller changeout from Multi 911 to Multi 820, 10/92. Timings adjusted, 11/18/92.
	2. Placed phase 4 on Max recall due to bad SB loop. Reduced max time 24 sec. to 18 sec. Added ped times for SB., 8/11/99.
	3. Updated file, 8/4/00.
	4. Timing sheet updated, 5/19/03.
	5. W-S and SB taken off of min recall by Bobby from Econolite at 3:00pm on 12/11/06.
	6. Timing sheet updated to reflect I2 changed address and controller type, 12/15/08.
	7. Phasing changed due to installation of new cabinet, 10/4/11.
	8. Controller changed out to an ASC-3, 12/5/12.
	9. Clearance intervals updated to NMDOT standard by BB, 12/18/13.
	10. Timing sheet updated to current timing sheet, 11-1-16.
	11. Rephase to city standard for Flashing Yellow arrows done by NMDOT. A.F. 7/7/20

ASC3 COORDINATION PLAN DATA

17 - Osuna & I-25 SB Frontage

COORDINATOR OPTIONS (MM 3-1)

MANUAL PATTERN	AUTO	ECPI COORD	YES
SYSTEM SOURCE	SYS	SYSTEM FORMAT	PTN
SPLITS IN	PERCENT	OFFSET IN	PERCENT
TRANSITION	SMOOTH	MAX SELECT	MAXINH
DWELL/ADD TIME	0	ENABLE MAN SYNC	NO
DLY COORD WK-LZ	NO	FORCE OFF	FLOAT
OFFSET REF	LEAD	CAL USE PED TM	NO
PED RECALL	NO	PED RESERVE	YES
LOCAL ZERO OVRD	NO	FO ADD INI GRN	NO
RE-SYNC COUNT	0	MULTISYNC	NO

COORDINATION PATTERN 11 (MM 3-2)

USE SPLIT PATTERN	11	SPLIT SUM	100%
TS2 (PAT-OFF)	0-1		
CYCLE	110s	STD (COS)	111
OFFSET VAL	51%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	W-S	EB				WB		SB
SPLITS	32	43				75		25
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

COORDINATION PATTERN 13

USE SPLIT PATTERN	13	SPLIT SUM	100%
TS2 (PAT-OFF)	0-3		
CYCLE	110s	STD (COS)	131
OFFSET VAL	51%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	W-S	EB				WB		SB
SPLITS	23	42				65		35
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

ASC3 COORDINATION PLAN DATA

COORDINATION PATTERN 15

USE SPLIT PATTERN	15	SPLIT SUM	100%
TS2 (PAT-OFF)	0-5		
CYCLE	120s	STD (COS)	151
OFFSET VAL	51%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	W-S	EB				WB		SB
SPLITS	26	52				78		22

PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

CLOCK / CALENDAR DATA (MM 5-1)

CURRENT DATE	CURRENT DOW	CURRENT TOD
ENA ACTION PLAN	0	
SYNC REF TIME	00:00	SYNC REF REF TIME
TIME FROM GMT	+00	DAY LIGHT SAVE NO
TIME RESET INPUT SET TIME		3:30:00

ACTION PLAN 11 (MM 5-2)

PATTERN	11	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 13

PATTERN	13	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 15

PATTERN	15	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ASC3 COORDINATION PLAN DATA

ACTION PLAN 100

PATTERN	254	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

DAY PLAN/EVENT 1 (MM 5-3)

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

DAY PLAN/EVENT 2

EVENT	ACTION PLAN	START TIME
1	11	6:30
2	13	9:00
3	15	15:00
4	13	18:30
5	100	22:00
6	0	00:00
7	0	00:00

DAY PLAN/EVENT 3

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

SCHEDULE NUMBER 1 (MM 5-4)

SCHEDULE NUMBER	1											
DAY PLAN NO	1	CLEAR ALL FIELDS										
SELECT ALL MONTHS												
DOW												
DOM												
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	X	.	.	.	.	.	.
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

ASC3 COORDINATION PLAN DATA

SCHEDULE NUMBER 2												
SCHEDULE NUMBER	2											
DAY PLAN NO	2		CLEAR ALL FIELDS									
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	X	X	X	X	X	.					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

SCHEDULE NUMBER 3												
SCHEDULE NUMBER	3											
DAY PLAN NO	3		CLEAR ALL FIELDS									
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	.	.	.	.	.	X					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

NOTES:

1. 6/18/08 - Adjusted the Offsets to allow better progression.
2. 12/5/12 - Coordination Sheet updated to reflect installation of ASC-3 controller
3. 9/22/16 - Coordination sheet corrected back to original offsets.
4. 9/27/16 - Sequences placed back to normal operation A.F.
5. 7-9-20 Rephased intersection for flashing yellow arrows NMDOT project. TBD A.F.
6. 8-30-22 - Changed offsets and sequence for Ptns 1, 3, & 5. Not yet installed - TB
7. Test Ptns 11,13 & 15. Impletd and tested 12-15-22 to 12-30-22 MA
8. 2-6-23 - Implement final Ptns 11, 13, & 15. MA

Intersection No.: 217

System: Centrac
Address: 1

Intersection Name: SAN MATEO & I-25 NB FRONTAGE

Revision Date 07/14/2020

Timing Data

Phase I.D.:	1	2	3	4	5	6	7	8
Phase Dir.:		EB		NB	E-N	WB		
Min Grn		16		8	3	16		
Walk:		7		7	0	7		
Ped Clr:		15		26	0	15		
Veh Ext:		2.0		2.0	1.5	2.0		
Veh Ext2:								
Max 1:		36		24	16	36		
Max 2:								
Max 3:								
Yellow:		4.0		4.5	4.0	4.0		
Red Clr		1.0		1.0	1.0	1.0		
Delay Grn						8.0		

Recall Data

Locking Memory:							
Vehicle Recall:							
Ped Recall:							
Recall To Max:		X				X	

Flash Mode: ALL RED

Start Up Mode: ALL RED

Time: 8 SEC.

First Phases: 2 & 6

Start In: GREEN

Overlap Phases: NONE

Overlap	Par Ph	Grn	Yel	Red
A				
B				
C				
D				

EB

NOTES:

1. Intersection upgraded to 3 phase operation with addition of E-N turn arrow.
2. Interchange has been under construction - Intersection running off line, 11/10/92.
3. Back on line, new geometrics and timing features.
4. Intersection number and address changed, was ICU 28, Add 4, 11/28/94.
5. Red clearance time change in database, 7/795.
6. Updated file, 12/1/00.
7. Timing sheet updated, 5/21/03.
8. Pattern 2 offset was 40% to 35%, 4/25/05.
9. E-N and NB taken off of min recall by Bobby A. of Econolite, at 2:30pm on 12/11/06.
10. Timing sheet updated to reflect I2 changed address and controller type, 10/16/08.
11. Clearance intervals updated to NMDOT standard by BB, 12/18/13.
12. E-N arrow clearance changed to match thru movement due to lagging arrow, 8/18/15
13. Created timing sheet due to NMDOT flashing yellow project. A.F. 7-14-20
14. Programmed a Ped Jump to West bound. 8/17/2022 MA

ASC3 COORDINATION PLAN DATA

217 - San Mateo & I-25 E. Side

COORDINATOR OPTIONS (MM 3-1)

MANUAL PATTERN	AUTO	ECPI COORD	YES
SYSTEM SOURCE	SYS	SYSTEM FORMAT	PTN
SPLITS IN	PERCENT	OFFSET IN	PERCENT
TRANSITION	SMOOTH	MAX SELECT	MAXINH
DWELL/ADD TIME	0	ENABLE MAN SYNC	NO
DLY COORD WK-LZ	NO	FORCE OFF	FLOAT
OFFSET REF	LEAD	CAL USE PED TM	NO
PED RECALL	NO	PED RESERVE	YES
LOCAL ZERO OVRD	NO	FO ADD INI GRN	NO
RE-SYNC COUNT	0	MULTISYNC	NO

COORDINATION PATTERN 11 (MM 3-2)

USE SPLIT PATTERN	11	SPLIT SUM	100%
TS2 (PAT-OFF)	0-1		
CYCLE	110s	STD (COS)	111
OFFSET VAL	46%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION		EB		NB	E-N	WB		
SPLITS		54		46	16	38		
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

COORDINATION PATTERN 13

USE SPLIT PATTERN	13	SPLIT SUM	100%
TS2 (PAT-OFF)	0-3		
CYCLE	110s	STD (COS)	131
OFFSET VAL	46%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION		EB		NB	E-N	WB		
SPLITS		54		46	16	38		
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

ASC3 COORDINATION PLAN DATA

COORDINATION PATTERN 15

USE SPLIT PATTERN	15	SPLIT SUM	100%
TS2 (PAT-OFF)	0-5		
CYCLE	120s	STD (COS)	151
OFFSET VAL	46%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	6
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION		EB		NB	E-N	WB		
SPLITS		54		46	16	38		

PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X				X		
VEH RECALL								
MAX RECALL		X				X		

CLOCK / CALENDAR DATA (MM 5-1)

CURRENT DATE	CURRENT DOW	CURRENT TOD
ENA ACTION PLAN	0	
SYNC REF TIME	00:00	SYNC REF REF TIME
TIME FROM GMT	+00	DAY LIGHT SAVE NO
TIME RESET INPUT SET TIME		3:30:00

ACTION PLAN 11 (MM 5-2)

PATTERN	11	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 13

PATTERN	13	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 15

PATTERN	15	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ASC3 COORDINATION PLAN DATA

ACTION PLAN 100

PATTERN	254	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

DAY PLAN/EVENT 1 (MM 5-3)

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

DAY PLAN/EVENT 2

EVENT	ACTION PLAN	START TIME
1	11	6:30
2	13	9:00
3	15	15:00
4	13	18:30
5	100	22:00
6	0	00:00
7	0	00:00

DAY PLAN/EVENT 3

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

SCHEDULE NUMBER 1 (MM 5-4)

SCHEDULE NUMBER	1											
DAY PLAN NO	1	CLEAR ALL FIELDS										
SELECT ALL MONTHS												
DOW												
DOM												
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	X	.	.	.	.	.	.
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

ASC3 COORDINATION PLAN DATA

SCHEDULE NUMBER 2												
SCHEDULE NUMBER	2											
DAY PLAN NO	2		CLEAR ALL FIELDS									
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	X	X	X	X	X	.					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

SCHEDULE NUMBER 3												
SCHEDULE NUMBER	3											
DAY PLAN NO	3		CLEAR ALL FIELDS									
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	.	.	.	.	.	X					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

NOTES:

1. 6/18/08 - Adjusted the Offsets to allow better progression.
2. 9/22/16 - Coordination sheet corrected to correct splits.
3. 7/14/16 Created time sheet for NMDOT Flashing yellow arrow. A.F.
4. 8/30/22 - Changed offsets and sequence for Ptns 1, 3, & 5. Not yet installed - TB
5. Test Ptns 11,13 & 15. Impleted and tested 12-15-22 to 12-30-22 MA
6. 2-6-23 - Implement final Ptns 11, 13, & 15. MA

Intersection No.: 18

System: Centrac
Address: 1

Intersection Name: PAN AMERICAN & SAN MATEO

Revision Date 12/18/2013

Timing Data

Phase I.D.:	1	2	3	4	5	6	7	8
Phase Dir.:	S-E	NB		E/W				
Min Grn	3	12		8				
Walk:	0	7		7				
Ped Clr:	0	15		26				
Veh Ext:	1.5	4.0		3.0				
Veh Ext2:								
Max 1:	16	38		24				
Max 2:								
Max 3:								
Yellow:	3.0	4.0		4.5				
Red Clr	0.5	1.0		1.0				

Recall Data

Locking Memory:							
Vehicle Recall:							
Ped Recall:							
Recall To Max:		X					

Flash Mode: ALL RED

Start Up Mode: ALL RED

Time: 8 SEC.

First Phases: 2

Start In: GREEN

Overlap Phases: NONE

Overlap	Par Ph	Grn	Yel	Red	
A					
B	1 & 2		4.0	1.0	SB
C					
D					

NOTES:

1. Ped heads & buttons installed E/W south approach, 9/25/86.
2. Controller to be program with Anti-Backup feature. Clearance times adjusted at this time also.
3. Timing sheet updated, 10/5/05.
4. Timing sheet updated, 9/5/12 to accommodate for ped times.
5. Timing sheet updated, 9/11/12 to reflect field values
6. Clearance intervals updated to NMDOT standard by BB, 12/18/13.

ASC3 COORDINATION PLAN DATA

018 - Pan American & San Mateo

COORDINATOR OPTIONS (MM 3-1)

MANUAL PATTERN	AUTO	ECPI COORD	YES
SYSTEM SOURCE	TBC	SYSTEM FORMAT	PTN
SPLITS IN	PERCENT	OFFSET IN	PERCENT
TRANSITION	SMOOTH	MAX SELECT	MAXINH
DWELL/ADD TIME	0	ENABLE MAN SYNC	NO
DLY COORD WK-LZ	NO	FORCE OFF	FIXED
OFFSET REF	LEAD	CAL USE PED TM	NO
PED RECALL	NO	PED RESERVE	YES
LOCAL ZERO OVRD	NO	FO ADD INI GRN	NO
RE-SYNC COUNT	0	MULTISYNC	NO

COORDINATION PATTERN 11 (MM 3-2)

USE SPLIT PATTERN	11	SPLIT SUM	100%
TS2 (PAT-OFF)	6,3		
CYCLE	110s	STD (COS)	111
OFFSET VAL	43%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	0
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	S-E	NB		E/W				
SPLITS	19	56		44				
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X						
VEH RECALL								
MAX RECALL		X						

COORDINATION PATTERN 13

USE SPLIT PATTERN	13	SPLIT SUM	100%
TS2 (PAT-OFF)	7,2		
CYCLE	110s	STD (COS)	131
OFFSET VAL	50%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	0
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	S-E	NB		E/W				
SPLITS	11	60		29				
PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X						
VEH RECALL								
MAX RECALL		X						

ASC3 COORDINATION PLAN DATA

COORDINATION PATTERN 15

USE SPLIT PATTERN	15	SPLIT SUM	100%
TS2 (PAT-OFF)	8,1		
CYCLE	120s	STD (COS)	251
OFFSET VAL	48%		
ACTUATED COORD	YES	TIMING PLAN	0
ACT WALK REST	NO	SEQUENCE	0
PHASE RESRVCE	NO	ACTION PLAN	0

PHASE	1	2	3	4	5	6	7	8
DIRECTION	S-E	NB		E/W				
SPLITS	20	55		25				

PHASE	1	2	3	4	5	6	7	8
COORD PHASE		X						
VEH RECALL								
MAX RECALL		X						

CLOCK / CALENDAR DATA (MM 5-1)

CURRENT DATE	CURRENT DOW	CURRENT TOD
ENA ACTION PLAN	0	
SYNC REF TIME	00:00	SYNC REF REF TIME
TIME FROM GMT	+00	DAY LIGHT SAVE NO
TIME RESET INPUT SET TIME		3:30:00

ACTION PLAN 11 (MM 5-2)

PATTERN	11	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 13

PATTERN	13	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ACTION PLAN 15

PATTERN	15	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

ASC3 COORDINATION PLAN DATA

ACTION PLAN 100

PATTERN	254	SYS OVERRIDE	NO
TIMING PLAN	0	SEQUENCE	0
VEHICLE DETECTOR PLAN	0.00	DET LOG	NONE
FLASH	--	RED REST	NO
VEH DET DIAG PLN	0	PED DET DIAG PLN	0
DIMMING ENABLE	NO		

DAY PLAN/EVENT 1 (MM 5-3)

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

DAY PLAN/EVENT 2

EVENT	ACTION PLAN	START TIME
1	11	6:30
2	13	9:00
3	15	15:00
4	13	18:30
5	100	22:00
6	0	00:00
7	0	00:00

DAY PLAN/EVENT 3

EVENT	ACTION PLAN	START TIME
1	13	7:00
2	100	22:00
3	0	00:00

SCHEDULE NUMBER 1 (MM 5-4)

SCHEDULE NUMBER	1											
DAY PLAN NO	1	CLEAR ALL FIELDS										
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	X	.	.	.	.	.	.
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

SCHEDULE NUMBER 2

SCHEDULE NUMBER	2											
DAY PLAN NO	2	CLEAR ALL FIELDS										
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D

ASC3 COORDINATION PLAN DATA

	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	X	X	X	X	X	.					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

SCHEDULE NUMBER 3												
SCHEDULE NUMBER	3											
DAY PLAN NO	3 CLEAR ALL FIELDS											
SELECT ALL MONTHS				DOW				DOM				
MONTH	J	F	M	A	M	J	J	A	S	O	N	D
	X	X	X	X	X	X	X	X	X	X	X	X
DAY(DOW)	SUN	MON	TUE	WED	THU	FRI	SAT					
	.	.	.	.	.	.	X					
DAY(DOM)	1	2	3	4	5	6	7	8	9	10	11	
	X	X	X	X	X	X	X	X	X	X	X	
	12	13	14	15	16	17	18	19	20	21	22	
	X	X	X	X	X	X	X	X	X	X	X	
	23	24	25	26	27	28	29	30	31			
	X	X	X	X	X	X	X	X	X			

- NOTES:**
1. Adjusted the offsets to allow better progression, 6/18/08.
 2. ASC 3 coord sheet created, 1/31/17.
 3. 8/30/22 - Changed offsets for Ptns 1, 3, & 5. Not yet installed - TB
 4. Test Ptns 11, 13 & 15. Implemented and tested 12-15-22 to 12-30-22 MA
 5. 2-6-23 - Implement final Ptns 11, 13, & 15. MA

APPENDIX C

Crash Data

CRASH RECORDS
2018 - 2019

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH DIRECTION	CRASH SEVERITY	CRASH CLASSIFICATION	CRASH ANALYSIS	HIGHEST CONTRIBUTING FACTOR TO CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
1/20/2018	15:44	OSUNA RD NE	PAN AMERICAN WEST HY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Avoid No Contact - Vehicle	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/20/2018	13:10	OSUNA RD NE	SAN MATEO BLVD NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/22/2018	13:18	SAN MATEO BLVD NE	I 25 NORTH-BD FW	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/6/2018	19:00	SAN MATEO BLVD NE	I 25 FRONTAGE RD	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Other Mechanical Defect	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/6/2018	19:47	I 25 FRONTAGE RD	OSUNA RD NE	N	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Inadequate Brakes	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/8/2018	10:00	OSUNA RD NE	SAN MATEO BLVD NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/14/2018	16:28	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Drove Left Of Center	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/16/2018	14:35	SAN MATEO BLVD NE	PAN AMERICAN EAST FWY		Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/22/2018	13:57	SAN MATEO BLVD NE	PAN AMERICAN FY NE	S	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/23/2018	19:54	SAN MATEO BLVD NE	I 25 FRONTAGE RD	NW	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Other Improper Driving	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/26/2018	11:25	OSUNA RD NE	FRONTAGE RD	SE	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/3/2018	7:20	SAN MATEO BLVD NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/5/2018	18:06	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/13/2018	11:30	PAN AMERICAN WEST HY NE	SAN MATEO BLVD NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/13/2018	11:15	SAN MATEO BLVD NE	PAN AMERICAN WEST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/17/2018	10:36	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/17/2018	13:48	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE RD	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/24/2018	15:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/2/2018	16:09	OSUNA RD NE	PAN AMERICAN EAST HY NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/11/2018	17:30	OSUNA RD NE	SAN MATEO BLVD NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/14/2018	19:00	SAN MATEO BLVD NE	I 25 FRONTAGE RD	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/15/2018	16:15	SAN MATEO	I-25		Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
5/15/2018	16:33	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/15/2018	6:30	SAN MATEO BLVD NE	I 25 FRONTAGE RD	W	Injury Crash	Other Vehicle	Other Vehicle - All Others/Entering At Angle	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/24/2018	15:41	FRONTAGE RD NE	HARPER DR NE	N	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/5/2018	19:14	OSUNA RD NE	PAN AMERICAN WEST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/6/2018	17:32	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/15/2018	22:00	OSUNA RD NE	I 25 SOUTH-BD FW	SE	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction	None	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
6/18/2018	14:00	EAST FRONTAGE ROAD @ I-25 & SAN MATEO	I-25 & SAN MATEO		Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/25/2018	11:48	SAN MATEO BLVD NE	PAN AMERICAN	NW	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Stopped/Entering At Angle	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/27/2018	19:30	OSUNA RD NE	I 25 FRONTAGE RD	E	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/28/2018	19:40	SAN MATEO NE	PAN AMERICAN FWY	N	Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
7/2/2018	7:29	PAN AMERICAN	HARPER	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Avoid No Contact - Other	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/3/2018	7:47	PAN AMERICAN EAST HY NE	HARPER DR NE	NE	Injury Crash	Other Vehicle	Other Vehicle - All Others/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/4/2018	12:57	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/5/2018	17:48	SAN MATEO BLVD NE	FRONTAGE RD NE	W	Injury Crash	Pedalcyclist	Pedalcyclist Struck Vehicle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Involved
7/7/2018	12:10	FRONTAGE RD NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/7/2018	16:10	OSUNA RD NE	PAN AMERICAN EAST HY NE	S	Property Damage Only Crash	Other (Non-Collision)	Non-Collision - All Other/Not Stated	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/15/2018	Left Blank	OSUNA BLVD. NE	SAN MATEO BLVD NE	E	Property Damage Only Crash	Vehicle on Other Road	Left Blank	Disregarded Traffic Signal	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
7/20/2018	17:31	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/21/2018	23:00	I-25 E FRONTAGE RD/OSUNA	SAN MATEO BLVD NE		Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Left Blank	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/23/2018	20:55	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Disregarded Traffic Signal	Raining	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/23/2018	12:57	SAN MATEO BLVD NE	OSUNA RD NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/26/2018	14:12	OSUNA RD NE	N/B PAN AMERICAN FWY NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/One Stopped	Avoid No Contact - Vehicle	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/27/2018	13:00	SAN MATEO BLVD NE	HARPER		Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/31/2018	16:48	SAN MATEO BLVD NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/2/2018	11:50	SAN MATEO BLVD NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Other Improper Driving	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/9/2018	14:00	SAN MATEO BLVD NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/14/2018	15:22	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/17/2018	15:50	SAN MATEO /OSUNA	PAN AMERICAN	E	Property Damage Only Crash	Other Vehicle	Left Blank	Disregarded Traffic Signal	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/19/2018	18:44	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Sideswipe Collision	Other Improper Driving	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
8/22/2018	6:59	OSUNA RD NE	PAN AMERICAN EAST HY NE	SW	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/23/2018	15:07	OSUNA RD NE	PAN AMERICAN FY SB	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/25/2018	1:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Fixed Object	ed Object - Embankment Man-made (Concrete, Wire Mesh)	Excessive Speed	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
8/25/2018	20:06	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Disregarded Traffic Signal	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
8/28/2018	16:15	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	N	Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/29/2018	18:23	OSUNA RD NE	PAN AMERICAN WEST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/29/2018	13:06	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/30/2018	10:37	SAN MATEO BLVD NE	FRONTAGE RD NB	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/5/2018	15:15	OSUNA	SAN MATEO	W	Property Damage Only Crash	Left Blank	Left Blank	Speed Too Fast for Conditions	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/11/2018	7:34	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Inadequate Brakes	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/20/2018	16:20	SAN MATEO BLVD NE	I-25	S	Property Damage Only Crash	Other Vehicle	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/20/2018	11:30	SAN MATEO	PAN AMERICAN		Property Damage Only Crash	Other Vehicle	Left Blank	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/24/2018	7:45	SAN MATEO BLVD	FRONTAGE	S	Property Damage Only Crash	Left Blank	Left Blank	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/4/2018	20:00	OSUNA RD NE	PAN AMERICAN FY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved
10/5/2018	16:08	PAN AMERICAN WEST HY NE	OSUNA RD NE	S	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/6/2018	9:15	SAN MATEO BLVD NE	PAN AMERICAN FWY NE		Property Damage Only Crash	Other Vehicle	Left Blank	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/6/2018	19:50	SAN MATEO BLVD NE	OSUNA RD NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Right Turn	Avoid No Contact - Vehicle	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
10/17/2018	15:27	SAN MATEO BLVD NE	PAN AMERICAN /N FRONTAGE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/17/2018	18:21	OSUNA RD NE	PAN AMERICAN /S FRONTAGE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved

CRASH RECORDS
2018 - 2019

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH DIRECTION	CRASH SEVERITY	CRASH CLASSIFICATION	CRASH ANALYSIS	HIGHEST CONTRIBUTING FACTOR TO CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
10/23/2018	18:17	SAN MATEO BLVD NE	PAN AMERICAN NE	S	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/24/2018	16:25	PAN AMERICAN EAST HY NE	OSUNA RD NE	N	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/7/2018	17:35	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Right Turn/Entering At Angle	Other Improper Driving	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
11/8/2018	14:39	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/9/2018	16:00	OSUNA	PAN AMERICAN FWY NE		Injury Crash	Other Vehicle	Left Blank	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/9/2018	16:04	SAN MATEO BLVD NE	PAN AMERICAN NB	N	Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
11/9/2018	20:55	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/One Stopped	Disregarded Traffic Signal	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
11/10/2018	19:16	PAN AMERICAN WEST HY NE	OSUNA RD NW	SW	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/13/2018	15:46	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/14/2018	17:15	OSUNA RD NE	I-25		Property Damage Only Crash	Other Vehicle	Left Blank	Disregarded Traffic Signal	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
11/15/2018	13:45	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	N	Property Damage Only Crash	Left Blank	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
11/21/2018	15:15	SAN MATEO / OSUNA TURNING TO FRONTAGE	NORTH TO FRONTAGE	N	Property Damage Only Crash	Other Vehicle	Left Blank	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/22/2018	10:47	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/25/2018	19:00	PAN AMERICAN EAST HY NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Left/Entering At Angle	Disregarded Traffic Signal	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
11/27/2018	13:00	SAN MATEO	I-25		Property Damage Only Crash	Other Vehicle	Left Blank	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/27/2018	16:47	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/1/2018	18:26	OSUNA RD NE	I 25 FRONTAGE RD	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Avoid No Contact - Vehicle	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/5/2018	19:30	PAN AMERICAN FWY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Following Too Closely	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/6/2018	11:40	SAN MATEO NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Left Blank	Excessive Speed	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/6/2018	11:08	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	NW	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/13/2018	14:45	SAN MATEO BLVD NE	I-25		Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Failed to Yield Right of Way	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
12/15/2018	18:42	SAN MATEO BLVD NE	I 25 FRONTAGE RD	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/25/2018	19:42	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Following Too Closely	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/28/2018	19:00	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Right Turn/Entering At Angle	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved

CRASH RECORDS
2018 - 2019

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH DIRECTION	CRASH SEVERITY	CRASH CLASSIFICATION	CRASH ANALYSIS	HIGHEST CONTRIBUTING FACTOR TO CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
1/1/2019	10:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Other Improper Driving	Snowing	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/3/2019	15:45	SAN MATEO NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Right Turn/Entering At Angle	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/4/2019	15:44	SAN MATEO BLVD NE	PAN AMERICAN FREEWAY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Turn Right	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/9/2019	18:20	PAN AMERICAN FRONTAGE RD	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Lane Change	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/10/2019	21:00	OSUNA	PAN AMERICAN FRONTAGE RD	S	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Overtaking	Raining	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/14/2019	8:47	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/14/2019	10:14	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/20/2019	11:24	SAN MATEO BLVD NE	PAN AMERICAN Fwy NE	S	Injury Crash	Pedestrian	Pedestrian Collision - Vehicle Turning Right	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Involved	Not Involved
2/1/2019	8:53	PAN AMERI N/ SAN MATEO	OSUNA	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/4/2019	11:26	OSUNA RD NE	PAN AMERICAN WEST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/8/2019	13:13	SAN MATEO BLVD NE	PAN AMERICAN PL NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Alcohol/Drug Involved	Clear	Daylight	Involved	Not Involved	Not Involved	Not Involved
2/12/2019	12:08	I 25 FRONTAGE RD	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Right Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/16/2019	15:07	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Alcohol/Drug Involved	Clear	Daylight	Involved	Not Involved	Not Involved	Not Involved
2/21/2019	17:15	PAN AMERICAN FREEWAY/ N FRONTAGE RD	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/22/2019	19:30	OSUNA NE	SAN MATEO BLVD NE	E	Property Damage Only Crash	Other Vehicle	Left Blank	Other - No Driver Error	Snowing	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved
2/22/2019	13:40	I 25 FRONTAGE RD	OSUNA RD NE	S	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/23/2019	10:10	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/26/2019	17:51	SAN MATEO BLVD NE	PAN AMERICAN EAST Fwy NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/2/2019	14:45	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/5/2019	18:33	OSUNA RD NE	PAN AMERICAN Fwy NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Failed to Yield Right of Way	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/6/2019	17:46	SAN MATEO BLVD NE	PAN AMERICAN NE		Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/9/2019	19:56	SAN MATEO BLVD NE	PAN AMERICAN FY NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Improper Lane Change	Clear	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/11/2019	13:20	SAN MATEO / PAN AMERICAN	PAN AMERICAN / OFF RAMP	E	Property Damage Only Crash	Other Vehicle	Left Blank	Defective Steering	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/11/2019	13:45	SAN MATEO NE	PAN AMERICAN Fwy NE	S	Property Damage Only Crash	Other Vehicle	Left Blank	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/12/2019	16:23	OSUNA RD NE	PAN AMERICAN FREEWAY	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/19/2019	10:15	SAN MATEO BLVD NE	PAN AMERICAN Fwy NE	N	Property Damage Only Crash	Other Vehicle	Left Blank	Drove Left Of Center	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/23/2019	20:38	OSUNA RD NE	I 25 SOUTH-BD FW	W	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Failed to Yield Right of Way	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/23/2019	10:51	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	S	Property Damage Only Crash	Fixed Object	Fixed Object - Median Raised Or Curb	Other Mechanical Defect	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/27/2019	8:21	PAN AMERICAN Fwy NE	SAN MATEO BLVD NE		Property Damage Only Crash	Parked Vehicle	Left Blank	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/29/2019	16:45	SAN MATEO BLVD NE	PAN AMERICAN Fwy NE	NE	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Overtaking	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/31/2019	0:50	SAN MATEO	OSUNA	S	Property Damage Only Crash	Other Vehicle	Left Blank	Excessive Speed	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/31/2019	3:00	OSUNA RD NW	PAN AMERICAN WEST HY NE	E	Property Damage Only Crash	Fixed Object	Fixed Object - Median Raised Or Curb	Avoid No Contact - Vehicle	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
4/2/2019	13:25	OSUNA RD	PAN AMERICAN FY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/5/2019	13:43	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/7/2019	16:15	EXIT ON SAN MATEO/OSUNA (NORTHBOUND)	SAN MATEO/OSUNA		Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/9/2019	Left Blank	SAN MATEO BLVD NE	SAN MATEO AND PAN AMERICAN	E	Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
4/14/2019	15:50	SAN MATEO NE	PAN AMERICAN PL NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/17/2019	17:30	SAN MATEO BLVD NE	OSUNA	NW	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Lane Change	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/19/2019	9:45	PAN AMERICAN Fwy	SAN MATEO	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Turn Right	Drove Left Of Center	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/19/2019	13:30	OSUNA RD NE	I-25	W	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/25/2019	18:00	SAN MATEO	OSUNA	S	Property Damage Only Crash	Other Vehicle	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/25/2019	13:30	SAN MATEO	OSUNA	S	Property Damage Only Crash	Left Blank	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/25/2019	12:19	OSUNA RD NE	PAN AMERICAN FY NE	W	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/26/2019	14:09	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/30/2019	10:55	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Property Damage Only Crash	Parked Vehicle	Vehicle Parked in Proper Location	Other Improper Driving	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/2/2019	15:17	SAN MATEO BLVD NE	6500 SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/3/2019	17:30	PAN AMERICAN NE	SAN MATEO I-25	N	Property Damage Only Crash	Other Vehicle	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/4/2019	13:05	SAN MATEO BLVD NE	I-25 E FRONTAGE RD	SW	Property Damage Only Crash	Other Vehicle	Left Blank	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/6/2019	7:45	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Driver Inattention	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/10/2019	18:30	OSUNA	PAN AMERICAN	W	Property Damage Only Crash	Other Vehicle	Left Blank	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/10/2019	7:50	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE ROA	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - All Others/Entering At Angle	Failed to Yield Right of Way	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/17/2019	17:32	SAN MATEO BLVD NE	PAN AMERICAN FREEWAY	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/23/2019	11:50	SAN MATEO BLVD NE	PAN AMERICAN Fwy NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/24/2019	11:29	OSUNA RD NE	PAN AMERICAN FRONTAGE STH	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/28/2019	9:17	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE N	SE	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/14/2019	12:34	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Other Improper Driving	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/14/2019	18:55	PAN AMERICAN FREEWAY	SAN MATEO BLVD NE	N	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Other Mechanical Defect	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/16/2019	12:30	SOUTHBOUND PAN AMERICAN FREEWAY	OSUNA	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/17/2019	17:15	SAN MATEO	OSUNA	N	Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
6/19/2019	16:30	PAN AMERICAN	SAN MATEO	N	Property Damage Only Crash	Left Blank	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/19/2019	16:05	SAN MATEO BLVD NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/20/2019	8:00	OSUNA RD	PAN AMERICAN N FRONT	W	Property Damage Only Crash	Other Vehicle	Left Blank	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/25/2019	12:19	SAN MATEO BLVD NE	6400 SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/27/2019	7:26	SAN MATEO BLVD NE	PAN AMERICAN PL NE	S	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/29/2019	17:11	PAN AMERICAN Fwy NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/29/2019	22:50	SAN MATEO BLVD NE	I 25 NORTH-BD FW	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/3/2019	12:00	SAN MATEO 6300 NE	ROAD OUT OF SHOPPING	W	Property Damage Only Crash	Other Vehicle	Left Blank	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/3/2019	10:15	I 25 ON RAMP	SAN MATEO		Property Damage Only Crash	Other Vehicle	Left Blank	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/12/2019	13:50	SAN MATEO	PAN AMERICAN	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Right Turn	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/16/2019	13:08	OSUNA RD NE	NORTH FRONTAG RD	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Improper Overtaking	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH DIRECTION	CRASH SEVERITY	CRASH CLASSIFICATION	CRASH ANALYSIS	HIGHEST CONTRIBUTING FACTOR TO CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
7/18/2019	15:36	SAN MATEO BLVD NE	OSUNA RD NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/19/2019	16:50	SAN MATEO	PAN AMERICAN	N	Property Damage Only Crash	Other Vehicle	Left Blank	Drove Left Of Center	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/19/2019	22:43	PAN AMERICAN FY NE	OSUNA RD NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/20/2019	17:11	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/27/2019	13:53	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	S	Injury Crash	Overturn/Rollover	Overturn/Rollover - Right Side of Road	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/2/2019	16:49	SAN MATEO BLVD NE	I 25 NORTH-BD FW	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/4/2019	14:27	SAN MATEO BLVD NE	I 25 FRONTAGE RD	N	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/5/2019	18:03	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Following Too Closely	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
8/13/2019	13:00	PAN AMERICAN	SAN MATEO	W	Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/14/2019	21:46	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
8/25/2019	4:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	N	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	Failed to Yield Right of Way	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
9/3/2019	22:48	SAN MATEO BLVD NE	OSUNA RD NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
9/13/2019	11:12	OSUNA RD NE	FRONTAGE RD NE	E	Injury Crash	Other Vehicle	Other Vehicle - Both Going Straight/Entering At Angle	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/24/2019	7:55	SAN MATEO BLVD NE	PAN AMERICAN FWY NE		Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Following Too Closely	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/25/2019	18:10	PAN AMERICAN EAST HY NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Going Straight	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
9/25/2019	7:47	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction/One Left Turn	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/26/2019	12:38	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/30/2019	20:49	PAN AMERICAN FY NE	SAN MATEO BLVD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Disregarded Traffic Signal	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
10/3/2019	17:35	SAN MATEO BLVD NE	NORTHBOUND RAMP TO I-25		Property Damage Only Crash	Other Vehicle	Left Blank	Missing Data	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/4/2019	15:00	SAN MATEO NE	PAN AMERICAN	N	Property Damage Only Crash	Other Vehicle	Left Blank	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/7/2019	13:00	OSUNA	SAN MATEO	W	Property Damage Only Crash	Left Blank	Left Blank	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
10/15/2019	2:40	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE		Property Damage Only Crash	Fixed Object	Fixed Object - Traffic Signal Standard	Other Improper Driving	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
10/15/2019	16:30	OSUNA RD NE	PAN AMERICAN SB	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/17/2019	15:14	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE RD	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Improper Lane Change	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/18/2019	9:08	SAN MATEO	PAN AMERICAN		Property Damage Only Crash	Vehicle on Other Road	Left Blank	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/19/2019	7:20	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	None	Clear	Dawn	Not Involved	Not Involved	Not Involved	Not Involved
10/22/2019	11:40	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/24/2019	8:59	SAN MATEO BLVD NE	I 25 FRONTAGE RD	W	Injury Crash	Other Vehicle	Other Vehicle - From Same Direction/One Right Turn	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/25/2019	16:35	SAN MATEO BLVD NE	I25 ON RAMP	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Sideswipe Collision	Driver Inattention	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/1/2019	21:46	OSUNA	AN AMERICAN / SOUTH FRONTAGE	W	Injury Crash	Other Vehicle	Other Vehicle - From Opposite Direction/Both Going Straight	Driver Inattention	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
11/6/2019	12:18	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	W	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Rear End Collision	Driver Inattention	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/9/2019	17:13	PAN AMERICAN	OSUNA RD NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
11/22/2019	6:00	PAN AMERICAN EAST HY NE	OSUNA RD NE	W	Injury Crash	Fixed Object	Fixed Object - Ditch	None	Fog	Dawn	Not Involved	Not Involved	Not Involved	Not Involved
11/22/2019	17:42	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	W	Injury Crash	Other Vehicle	Other Vehicle - One Right Turn/Entering At Angle	Improper Lane Change	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
11/23/2019	15:00	SAN MATEO BLVD NE	HARPER DR NE	SW	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/27/2019	10:05	SAN MATEO BLVD NE	FRONTAGE RD NE	NW	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Same Direction/Both Turn Right	None	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/2/2019	13:45	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	NE	Property Damage Only Crash	Left Blank	Invalid Code	Missing Data	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
12/5/2019	10:20	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE RD	W	Property Damage Only Crash	Other Vehicle	Left Blank	Drove Left Of Center	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/6/2019	10:15	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Excessive Speed	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/13/2019	7:55	OSUNA RD NE	PAN AMERICAN EAST HY NE	S	Property Damage Only Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Failed to Yield Right of Way	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/20/2019	6:07	OSUNA RD NE	PAN AMERICAN EAST HY NE	E	Injury Crash	Other Vehicle	Other Vehicle - One Left Turn/Entering At Angle	Disregarded Traffic Signal	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/27/2019	11:20	OSUNA RD NE	PAN AMERICAN FWY NE	E	Property Damage Only Crash	Other Vehicle	Other Vehicle - From Opposite Direction	Improper Overtaking	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/31/2019	15:30	PAN AMERICAN EAST HY NE	OSUNA RD NE	N	Property Damage Only Crash	Other Vehicle	Other Vehicle - Both Turn Right/Entering At Angle	Made Improper Turn	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved

Key:

- Osuna/Pan American Fwy West
- Non-Intersection (between West/East frontage)
- San Mateo/Pan American Fwy East
- San Mateo/Pan American NE
- Pan American NE/Harper Dr.
- Shopping Center Access

CRASH RECORDS
2020 - 2021

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH SEVERITY	NUMBER OF PEOPLE KILLED IN CRASH	NUMBER OF PEOPLE INJURED (CLASS A+B+C) IN CRASH	CRASH ANALYSIS	FIRST HARMFUL EVENT	FIRST HARMFUL EVENT – MANNER OF CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
1/1/2020	19:00	OSUNA RD NE	I 25 FRONTAGE RD	Injury Crash	0	1	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/3/2020	14:02	SAN MATEO BLVD NE		Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/4/2020	15:47	SAN MATEO BLVD NE	PAN AMERICAN PL NE	Injury Crash	0	1	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/6/2020	7:50	6500 SAN MATEO BLVD NE		Property Damage Only Crash	0	0	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/7/2020	12:45	PAN AMERICAN FWY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
1/12/2020	1:42	PAN AMERICAN FWY NE - SAN MATEO BLVD NE		Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
1/15/2020	6:03	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE RD	Injury Crash	0	1	Other Vehicle - From Same Direction/Both Going Straight	Collision with Motor Vehicle	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/16/2020	11:47	OSUNA RD NE	FR 2537	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/16/2020	9:13	OSUNA RD NW	PAN AMERICAN S NW	Injury Crash	0	1	Other Vehicle - From Opposite Direction/One Left Turn	Collision with Motor Vehicle	Left Blank	Blowing Snow	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/17/2020	17:27	PAN AMERICAN EAST HY NE	OSUNA RD NE	Property Damage Only Crash	0	0	Other Vehicle - One Right Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
1/21/2020	17:42	PAN AMERICAN FY NE	SAN MATEO BLVD NE	Injury Crash	0	1	Other Vehicle - From Same Direction/Rear End Collision	Collision with Motor Vehicle	From Same Direction	Raining	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/23/2020	10:59	PAN AMERICAN PL NE	OSUNA RD NE	Injury Crash	0	1	Fixed Object - Unknown/Not Stated	Collision with Fixed Object	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/27/2020	6:20	SAN MATEO	PAN AMERICAN FWY S	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
2/3/2020	10:00	SAN MATEO BLVD NE	FRONTAGE ROAD I25	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/4/2020	14:18	SAN MATEO BLVD NE	6828 SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - One Right Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/5/2020	12:19	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/6/2020	7:29	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/13/2020	4:15		OSUNA & FRONTAGE RD	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/15/2020	11:00	SAN MATEO	OSUNA	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/21/2020	17:58	SAN MATEO BLVD NE	PAN AMERICAN PL NE	Property Damage Only Crash	0	0	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
2/21/2020	14:03	PAN AMERICAN FWY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/22/2020	16:17	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Fixed Object - Median Raised Or Curb	Collision with Fixed Object	Left Blank	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/24/2020	16:55	I-25 N	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/26/2020	15:41	OSUNA RD NE	PAN AMERICAN WEST HY NE	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/27/2020	16:50	SAN MATEO BLVD NE	OSUNA RD NE	Property Damage Only Crash	0	0	Other Vehicle - From Same Direction/Both Going Straight	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Involved	Not Involved	Not Involved	Not Involved
2/28/2020	14:36	OSUNA RD NE	I 25 FRONTAGE RD	Property Damage Only Crash	0	0	Other Vehicle - All Others/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/28/2020	14:44	OSUNA RD NE	I 25 FRONTAGE RD	Property Damage Only Crash	0	0	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/9/2020	13:46	PAN AMERICAN WEST HY NE	OSUNA RD NE	Injury Crash	0	1	Other Vehicle - From Opposite Direction/Both Going Straight	Collision with Motor Vehicle	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/11/2020	13:00	OSUNA RD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Other Vehicle - All Others/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/6/2020	7:04	OSUNA RD NE	I 25 FRONTAGE RD	Injury Crash	0	1	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/10/2020	12:55	FR 2523	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/11/2020	16:45	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/18/2020	13:45	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Other Vehicle - Both Turn Right/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/18/2020	14:15	PAN AMERICAN FY NE	SAN MATEO BLVD NE	Injury Crash	0	2	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/28/2020	14:10	OSUNA E	PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/30/2020	9:16	PAN AMERICAN EAST HY NE	OSUNA RD NE	Injury Crash	0	1	Other Vehicle - From Opposite Direction/One Left Turn	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/3/2020	20:37	PAN AMERICAN EAST HY NE	OSUNA RD NE	Injury Crash	0	2	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
6/16/2020	15:12	PAN AMERICAN EAST HY NE	OSUNA RD NE	Injury Crash	0	1	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/17/2020	18:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Other Vehicle - One Left Turn/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/7/2020	21:45	SAN MATEO BLVD NE	I25 NB ON RAMP	Injury Crash	0	2	Other Vehicle - From Same Direction/Rear End Collision	Collision with Motor Vehicle	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/17/2020	12:00	SAN MATEO + PAN AMERICAN	SAN MATEO + PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/23/2020	20:22	OSUNA RD NE	PAN AMERICAN FWY NE	Injury Crash	0	1	Other Vehicle - From Same Direction/Both Going Straight	Collision with Motor Vehicle	From Same Direction	Raining	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
7/24/2020	12:05	NB PAN AMERICAN FRONTAGE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - Both Turn Right/Entering At Angle	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/31/2020	9:42	PAN AMERICAN NB FRONTAGE	SAN MATEO BLVD NE	Injury Crash	0	1	Other Vehicle - From Opposite Direction/One Left Turn	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/2/2020	17:20	SAN MATEO BLVD NORTH BOUND	FRONTAGE RD	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/4/2020	ft Blank	SAN MATEO/FREEWAY	PAN AMERICAN NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/12/2020	9:23	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/23/2020	18:50	OSUNA & PAN AMERICAN FWY NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/26/2020	8:50	SAN MATEO BLVD ILLEGIBLE	FRONTAGE RD. GOING ON FREEWAY	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/31/2020	ft Blank	PAN AMERICAN FWY NE	OSUNA	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/4/2020	18:45	PAN AMERICAN FRONTAGE RD	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
9/4/2020	18:47	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
9/9/2020	8:30	SAN MATEO + I-25		Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/14/2020	20:23	SAN MATEO	PAN AMERICAN SOUTH	Property Damage Only Crash	0	0	Other Vehicle - Both Going Straight/Entering At Angle	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
9/17/2020	16:00	OSUNA RD	PAN AMERICAN FWY	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/17/2020	16:00	OSUNA	PAN AMERICAN NE	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
9/18/2020	14:18	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/21/2020	9:37	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/22/2020	17:25	OSUNA RD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Fixed Object	Left Blank	Clear	Dusk	Involved	Not Involved	Not Involved	Not Involved
9/23/2020	9:53	OSUNA RD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	2	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/2/2020	17:00	SAN MATEO	I-25/PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/2/2020	13:20	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE N	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/9/2020	10:39	OSUNA RD NE		Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/16/2020	14:50	SAN MATEO BLVD NE	NE + PAN AMERICAN FRONTAGE RD N	Property Damage Only Crash	0	0	Invalid Code	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/19/2020	5:50	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Dawn	Not Involved	Not Involved	Not Involved	Not Involved
10/26/2020	14:10	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	with Other Non-Fixed Object	Left Blank	Snowing	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/4/2020	7:30	SAN MATEO AND PAN AMERICAN	PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/8/2020	11:19	OSUNA RD NE	PAN AMERICAN EAST HY SW	Injury Crash	0	2	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/14/2020	8:37	OSUNA RD NE	PAN AMERICAN WEST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/14/2020	9:30	OSUNA RD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/5/2020	17:25	SAN MATEO BLVD NE	PAN AMERICAN FY NE	Injury Crash	0	2	Left Blank	Collision with Motor Vehicle	From Opposite Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
12/9/2020	12:07	PAN AMERICAN EAST HY NE	OSUNA RD NW	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved

CRASH RECORDS
2020 - 2021

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH SEVERITY	NUMBER OF PEOPLE KILLED IN CRASH	NUMBER OF PEOPLE INJURED (CLASS A+B+C) IN CRASH	CRASH ANALYSIS	FIRST HARMFUL EVENT	FIRST HARMFUL EVENT – MANNER OF CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	PEDALCYCLE INVOLVEMENT
12/17/2020	8:46	SAN MATEO BLVD NE	NORTH BOUND FRONTAGE RD	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/22/2020	16:00	SAN MATEO BLVD NE		Injury Crash	0	1	Left Blank	Collision with Fixed Object	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/28/2020	22:16	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/10/2021	21:40	CORNER OF SAN MATEO	I-25	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
1/17/2021	11:50	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
1/29/2021	13:45	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
2/11/2021	22:09	PAN AMERICAN WEST HY NE	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	with Other Non-Fixed Object	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
2/23/2021	21:35	OSUNA RD NE	I 25 NORTH-BD FW	Injury Crash	0	1	Left Blank	Other	Left Blank	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
2/25/2021	18:55	OSUNA & PAN AMERICAN	PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved
3/18/2021	12:28	OSUNA RD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
3/24/2021	18:56	HARPER DR NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Cloudy	Dusk	Not Involved	Not Involved	Not Involved	Not Involved
3/25/2021	11:01	SAN MATEO BLVD NE	PAN AMERICAN	Property Damage Only Crash	0	0	Left Blank	with Other Non-Fixed Object	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/1/2021	16:14	PAN AMERICAN FWY NE AND OSUNA RD NE	SOUTH BOUND	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Involved	Not Involved	Not Involved
4/2/2021	11:04	OSUNA RD NE	PAN AMERICAN WEST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/12/2021	16:51	I 25 FRONTAGE RD	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/16/2021	14:45	SAN MATEO	PAN AMERICAN FWY	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/18/2021	18:18	PAN AMERICAN NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/19/2021	13:20	OSUNA	PAN AMERICAN FWY	Property Damage Only Crash	0	0	Invalid Code	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/26/2021	17:03	PAN AMERICAN EAST HY NE	OSUNA RD NE	Injury Crash	0	1	Left Blank	Collision with Fixed Object	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/28/2021	8:30	PAN AMERICAN WEST HY NE	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
4/28/2021	13:58	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	2	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/18/2021	10:25	OSUNA PL NW	FRONTAGE ROAD NORTH	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/19/2021	12:15	HARPER DR NE	PAN AMERICAN NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/21/2021	ft Blank	SAN MATEO NE	I-25 SAN MATEO	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/22/2021	17:30	PAN AMERICAN FWY	OSUNA	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
5/24/2021	8:46	SAN MATEO BLVD NE	PAN AMERICAN WEST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
6/1/2021	16:00	I-25 & OSUNA ON RAMP	2ND TO RIGHT HAND LN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/6/2021	12:22	SAN MATEO RD	PAN AMERICAN PL NE	Injury Crash	0	3	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/9/2021	10:07	SAN MATEO BLVD NE	HARPER DR NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/16/2021	15:11	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/16/2021	15:18	HARPER DR NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
7/25/2021	22:57	I25 NB ONRP	SAN MATEO BL NE	Property Damage Only Crash	0	0	Left Blank	Collision with Fixed Object	Left Blank	Raining	Dark-Lighted	Involved	Not Involved	Not Involved	Not Involved
7/29/2021	8:00	SAN MATEO BLVD	SAN MATEO AND PAN AMERICAN	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
7/30/2021	9:54	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Involved	Not Involved	Not Involved
8/5/2021	9:56	SAN MATEO NE	OSUNA NE	Injury Crash	0	3	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
8/22/2021	16:00	I-25 FRONTAGE NB	SAN MATEO BLVD	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
8/25/2021	14:29	PAN AMERICAN FY NE	SAN MATEO BL NE	Property Damage Only Crash	0	0	Left Blank	Non-Collision	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/11/2021	19:39	6400 SAN MATEO BL NE		Property Damage Only Crash	0	0	Left Blank	Collision with Fixed Object	Left Blank	Clear	Dusk	Involved	Not Involved	Not Involved	Not Involved
9/14/2021	7:43	SAN MATEO BLVD NE	PAN AMERICAN NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/15/2021	8:28	PAN AMERICAN FW NB	SAN MATEO BLVD NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/16/2021	9:26	OSUNA RD	PAN AMERICAN/FRONTAGE	Property Damage Only Crash	0	0	Invalid Code	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/17/2021	14:00	HARPER DR NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/17/2021	15:00	OSUNA RD NE	I 25 SOUTH-BD FW	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
9/28/2021	17:53		SAN MATEO & I-25 FRONTAGE	Property Damage Only Crash	0	0	Invalid Code	Collision with Motor Vehicle	From Same Direction	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/5/2021	17:40	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/6/2021	8:30	HARPER DR NE	PAN AMERICAN FWY NE	Injury Crash	0	1	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/9/2021	17:40	OSUNA RD NE	SAN MATEO RD	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/11/2021	9:50	6300 BLOCK OF SAN MATEO		Property Damage Only Crash	0	0	Vehicle On Other Roadway - Not Stated	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/12/2021	15:15	OSUNA RD NE		Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Wind	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/15/2021	11:49	SAN MATEO BLVD NE	PAN AMERICAN FY NE	Injury Crash	0	3	Left Blank	Collision with Motor Vehicle	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/19/2021	7:50	HARPER DR NE	PAN AMERICAN FWY	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
10/19/2021	12:45	INTERSTATE 25 NB FRONTAGE	SAN MATEO BLVD NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/1/2021	6:26	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved
11/5/2021	10:21	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/6/2021	15:02	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
11/18/2021	9:05	INTERSECTION OF SAN MATEO AND PAN AM	PAN AMERICAN	Property Damage Only Crash	0	0	Overturn/Rollover - All Other/Not Stated	Non-Collision	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
11/19/2021	15:45	SAN MATEO / PAN AMERICAN		Property Damage Only Crash	0	0	Invalid Code	Collision with Motor Vehicle	From Same Direction	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
11/20/2021	10:30	SAN MATEO BLVD NE	PAN AMERICAN PL NE	Property Damage Only Crash	0	0	Left Blank	Collision with Motor Vehicle	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/6/2021	16:30	OSUNA WB	PAN AMERICAN	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved
12/7/2021	7:50	SAN MATEO BLVD NE		Injury Crash	0	1	Left Blank	Collision with Motor Vehicle	Intersecting Path (T-bone)	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/8/2021	16:30	SAN MATEO	I-25 OVERPASS	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved
12/11/2021	14:17	PAN AMERICAN FWY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Other Vehicle - From Opposite Direction	Collision with Motor Vehicle	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved

CRASH DATE	TIME OF CRASH	PRIMARY STREET	SECONDARY STREET	CRASH SEVERITY	NUMBER OF PEOPLE KILLED IN CRASH	NUMBER OF PEOPLE INJURED (CLASS A+B+C) IN CRASH	CRASH ANALYSIS	FIRST HARMFUL EVENT – MANNER OF IMPACT	FIRST HARMFUL EVENT – MANNER OF CRASH	WEATHER	LIGHTING	ALCOHOL INVOLVEMENT	DRUG INVOLVEMENT	PEDESTRIAN INVOLVEMENT	MOTORCYCLE INVOLVEMENT	PEDALCYCLE INVOLVEMENT
1/2/2022	9:24	PAN AMERICAN N FRNTGE RD	OSUNA RD NE	Injury Crash	0	1	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/8/2022	18:25	SAN MATEO BLVD NE	I-25 NORTH-BD FW	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Dark-Lighted	Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/21/2022	16:00	SAN MATEO & OSUNA TO I-25	FRONTAGE RD / I-25	Property Damage Only Crash	0	0	Vehicle On Other Roadway - Not Stated	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/13/2022	18:25	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0		Sideswipe	From Same Direction	Clear	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/17/2022	9:45	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/23/2022	14:02	SAN MATEO	I-25	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
2/10/2022	19:00	SAN MATEO BLVD NE	I-25 NORTH-BD FW	Injury Crash	0	3	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
3/2/2022	12:42	SAN MATEO BLVD NE	6500 SAN MATEO BLVD NE	Injury Crash	0	3	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
6/5/2022	14:21	SAN MATEO BLVD NE	I-25	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
2/15/2022	8:45	OSUNA NE	SAN MATEO NE	Injury Crash	0	1	Other Vehicle - From Opposite Direction	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
2/16/2022	13:07	I-25 SB OFF RAMP	OSUNA RD NE	Injury Crash	0	1		Left Blank	Other	Cloudy	Daylight	Not Involved	Not Involved	Involved	Not Involved	Not Involved
6/8/2022	16:56	SAN MATEO BLVD NE		Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
3/4/2022	15:45	I-25 E FRONTAGE RD	SAN MATEO BLVD NE	Injury Crash	0	1	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
3/10/2022	0:29	PAN AMERICAN EAST HY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
8/23/2022	10:00	SAN MATEO BLVD NE	OSUNA CT NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/7/2022	16:55	SAN MATEO BLVD NE		Property Damage Only Crash	0	0	Left Blank	Front-to-Front	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/23/2022	10:48	SAN MATEO BLVD NE	OSUNA RD NE	Injury Crash	0	1	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/1/2022	15:15	SAN MATEO BLVD NE	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
1/3/2022	17:02	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
4/20/2022	16:01	OSUNA RD NE	PAN AMERICAN PL NE	Injury Crash	0	2	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
2/2/2022	17:37	SAN MATEO	PAN AMERICAN NE	Property Damage Only Crash	0	0	Invalid Code	Sideswipe	Intersecting Path (T-bone)	Snowing	Dusk	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
2/4/2022	17:09	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
3/17/2022	18:16	SAN MATEO BLVD NE	PAN AMERICAN PL NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
3/21/2022	22:22	SAN MATEO BLVD NE	PAN AMERICAN FRNT RD N	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Raining	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
5/3/2022	23:00	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
6/17/2022	17:32	OSUNA RD NE	S PAN AMERICAN FY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
5/29/2022	15:00	SAN MATEO BLVD NE	PAN AMERICAN FY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Front	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
5/31/2022	11:00	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/15/2022	20:57	OSUNA RD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
6/6/2022	17:45	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/19/2022	15:57	PAN AMERICAN FY NE	OSUNA RD NE	Injury Crash	0	1	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/22/2022	6:58	PAN AMERICAN FY NE	SAN MATEO BL NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
6/24/2022	12:45	SAN MATEO BL NE	PAN AMERICAN FY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
6/29/2022	11:25	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
8/19/2022	17:26	OSUNA RD NW	I-10 FRONTAGE RD	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/21/2022	10:49	SAN MATEO BLVD NE	PAN AMERICAN FRONTAGE RD	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/23/2022	19:45	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Dusk	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
8/31/2022	17:05	OSUNA RD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
7/28/2022	13:01	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Left Blank	Left Blank	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
8/4/2022	16:40	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	From Same Direction	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/15/2022	12:35	OSUNA RD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/21/2022	21:58	OSUNA RD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	From Same Direction	Clear	Dark-Lighted	Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/22/2022	12:30	OSUNA RD NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/22/2022	16:43	OSUNA RD NE	I-25	Injury Crash	0	2	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Raining	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
10/4/2022	4:30	I-25 FRONTAGE RD	OSUNA RD NE	Injury Crash	0	1	Left Blank	Left Blank	Left Blank	Left Blank	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
10/15/2022	1:03	PAN AMERICAN EAST HY NE	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Left Blank	Dark-Not Lighted	Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/6/2022	Id Code	OSUNA / FRONTAGE ROAD	FRONTAGE RD	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
9/7/2022	15:38	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
10/4/2022	12:30	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Injury Crash	0	1	Left Blank	Front-to-Side	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/8/2022	8:12	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Injury Crash	0	4	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/11/2022	15:44	SAN MATEO BLVD NE	PAN AMERICAN PL NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	From Same Direction	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/30/2022	15:14	OSUNA RD NE	PAN AMERICAN FWY NE	Injury Crash	0	1	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/2/2022	22:58	PAN AMERICAN FWY NE	OSUNA RD NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/3/2022	22:00	OSUNA RD NE	I-25 SB OFF RAMP	Injury Crash	0	1	Left Blank	Left Blank	Left Blank	Clear	-Unknown Lighting	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/14/2022	12:38	PAN AMERICAN NE	SAN MATEO BLVD NE	Injury Crash	0	2	Left Blank	Front-to-Front	From Opposite Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/18/2022	15:04	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Cloudy	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/23/2022	13:15	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Left Blank	Left Blank	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/25/2022	18:40	SAN MATEO BLVD NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Dark-Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/26/2022	11:40	PAN AMERICAN FWY NE	SAN MATEO BLVD NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Rear	From Same Direction	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
12/27/2022	14:24	SAN MATEO BLVD NE	PAN AMERICAN FWY NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Left Blank	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/11/2022	23:12	PAN AMERICAN EAST HY NE	HARPER DR NE	Property Damage Only Crash	0	0	Left Blank	Sideswipe	From Same Direction	Clear	Dark-Not Lighted	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved
11/18/2022	16:01	HARPER DR NE	PAN AMERICAN EAST HY NE	Property Damage Only Crash	0	0	Left Blank	Front-to-Side	Intersecting Path (T-bone)	Clear	Daylight	Not Involved	Not Involved	Not Involved	Not Involved	Not Involved

Key:
Osuna/Pan American Fwy West
Non-Intersection (between West/East frontage)
San Mateo/Pan American Fwy East
San Mateo/Pan American NE
Pan American NE/Harper Dr.
Shopping Center Access

ACCIDENT SUMMARY SHEET

INTERSECTION/SEGMENT: Osuna / Pan American Fwy West (Southbound Frontage Road, SBFR)

ROUTE MP TO MP	YEAR 2018		YEAR 2019		YEAR 2020		YEAR 2021		YEAR 2022		TOTAL	
	No. 18	20%	No. 29	32%	No. 22	24%	No. 13	14%	No. 10	11%	No. 92	% 100
CRASH SEVERITY												
Property Damage Only (PDO)	8	44	21	72	11	50	9	69	4	40	53	58
Injury/Non-Fatal	10	56	8	28	11	50	4	31	6	60	39	42
Fatal											0	0
CRASH CLASSIFICATION (ACCIDENT TYPE)												
Fixed Object			1	5	2	9	1	8			4	5
Right Angle	3	21	6	27	11	50	2	15	5	63	27	34
Rear End			2	9					2	25	4	5
Backing											0	0
Sideswipe: Same Direction	2	14	2	9	2	9	4	31	1	13	11	14
Sideswipe: Opposite Direction	1	7	6	27	3	14	5	38			15	19
Head On	1	7	1	5	1	5					3	4
Left Turn	7	50	4	18	3	14					14	18
Parked Vehicle/Parking Maneuver											0	0
Overturn											0	0
Driveway/Driveway Maneuver											0	0
Pedestrian/Bicyclist											0	0
Other							1	8			1	1
PROBABLE CAUSE ("HIGHEST CONTRIBUTING FACTOR")												
Following Too Close			2	7							2	5
Driver Inattention	3	20	6	22							9	21
Excess Speed/Too Fast For Conditions	1	7									1	2
Avoid Other Vehicle	1	7	1	4							2	5
Improper Driving	3	20	6	22							9	21
Failure to use Turn Signal											0	0
Failure to Yield R.O.W.	7	47	7	26							14	33
Disregard Traffic Control Device			5	19							5	12
Under Influence Alcohol/Drugs											0	0
Mechanical Defect											0	0
Pedestrian Error											0	0
Road Defect/Construction Activity											0	0
Other											0	0
ROAD CONDITIONS												
Dry/Clear	14	88	27	93	21	95	12	100	4	50	78	90
Wet	2	13	2	7					1	13	5	6
Snowy/Icy					1	5					1	1
Other									3	38	3	3
LIGHTING												
Daylight	14	88	24	83	18	82	10	77	8	80	74	82
Darkness	2	13	4	14	3	14	3	23	2	20	14	16
Dawn or Dusk			1	3	1	5					2	2
SOBRIETY												
Sobriety Unknown											0	0
Had Been Drinking/Drug					2	9			2	20	4	4
Had Not Been Drinking/Drug	17	100	29	100	20	91	13	100	8	80	87	96

*Probable cause information excluded from post-2019 records.

ACCIDENT SUMMARY SHEET

INTERSECTION/SEGMENT: San Mateo / Pan American Fwy East (Northbound Frontage Road, NBFR)

ROUTE MP TO MP	YEAR 2018		YEAR 2019		YEAR 2020		YEAR 2021		YEAR 2022		TOTAL	
	No. 61	30%	No. 71	35%	No. 37	18%	No. 13	6%	No. 22	11%	No. 204	% 100
CRASH SEVERITY												
Property Damage Only (PDO)	48	79	56	79	26	70	9	69	17	77	156	76
Injury/Non-Fatal	13	21	15	21	11	30	4	31	5	23	48	24
Fatal											0	0
CRASH CLASSIFICATION (ACCIDENT TYPE)												
Fixed Object	1	2	3	6	1	3	1	17			6	4
Right Angle	13	27	12	24	8	27	2	33	6	30	41	26
Rear End	11	22	12	24	2	7			4	20	29	19
Backing											0	0
Sideswipe: Same Direction	7	14	6	12	3	10			9	45	25	16
Sideswipe: Opposite Direction	5	10	5	10	7	23	3	50			20	13
Head On									1	5	1	1
Left Turn	10	20	9	18	8	27					27	17
Parked Vehicle/Parking Maneuver			2	4							2	1
Overturn			1	2							1	1
Driveway/Driveway Maneuver											0	0
Pedestrian/Bicyclist	1	2	1	2							2	1
Other	1	2			1	3					2	1
PROBABLE CAUSE ("HIGHEST CONTRIBUTING FACTOR")												
Following Too Close	4	7	8	13							12	10
Driver Inattention	14	25	13	22							27	23
Excess Speed/Too Fast For Conditions	6	11	2	3							8	7
Avoid Other Vehicle	3	5									3	3
Improper Driving	8	14	17	28							25	21
Failure to use Turn Signal											0	0
Failure to Yield R.O.W.	9	16	10	17							19	16
Disregard Traffic Control Device	10	18	5	8							15	13
Under Influence Alcohol/Drugs			1	2							1	1
Mechanical Defect	3	5	3	5							6	5
Pedestrian Error											0	0
Road Defect/Construction Activity											0	0
Other			1	2							1	1
ROAD CONDITIONS												
Dry/Clear	55	96	60	90	31	89	6	60	18	86	170	89
Wet	2	4	4	6	3	9	1	10	1	5	11	6
Snowy/Icy			2	3	1	3					3	2
Other			1	1			3	30	2	10	6	3
LIGHTING												
Daylight	45	75	52	76	27	77	9	90	15	68	148	76
Darkness	12	20	13	19	5	14	1	10	7	32	38	19
Dawn or Dusk	3	5	3	4	3	9					9	5
SOBRIETY												
Sobriety Unknown											0	0
Had Been Drinking/Drug			1	1	1	3			1	5	3	2
Had Not Been Drinking/Drug	60	100	69	99	36	97	11	100	21	95	197	99

*Probable cause information excluded from post-2019 records.

ACCIDENT SUMMARY SHEET

INTERSECTION/SEGMENT: San Mateo/Pan American NE

ROUTE MP TO MP	YEAR 2018		YEAR 2019		YEAR 2020		YEAR 2021		YEAR 2022		TOTAL	
	No. 6	9%	No. 7	11%	No. 11	17%	No. 21	32%	No. 20	31%	No. 65	% 100
CRASH SEVERITY												
Property Damage Only (PDO)	4	67	6	86	11	100	15	71	17	85	53	82
Injury/Non-Fatal	2	33	1	14			6	29	3	15	12	18
Fatal											0	0
CRASH CLASSIFICATION (ACCIDENT TYPE)												
Fixed Object											0	0
Right Angle	2	50			2	33	6	50	4	29	14	35
Rear End	1	25							1	7	2	5
Backing											0	0
Sideswipe: Same Direction			2	50					7	50	9	23
Sideswipe: Opposite Direction			1	25	4	67	4	33	1	7	10	25
Head On									1	7	1	3
Left Turn	1	25	1	25							2	5
Parked Vehicle/Parking Maneuver											0	0
Overturn							1	8			1	3
Driveway/Driveway Maneuver											0	0
Pedestrian/Bicyclist											0	0
Other							1	8			1	3
PROBABLE CAUSE ("HIGHEST CONTRIBUTING FACTOR")												
Following Too Close	1	17	1	17							2	17
Driver Inattention											0	0
Excess Speed/Too Fast For Conditions											0	0
Avoid Other Vehicle											0	0
Improper Driving	2	33	3	50							5	42
Failure to use Turn Signal											0	0
Failure to Yield R.O.W.	3	50									3	25
Disregard Traffic Control Device			1	17							1	8
Under Influence Alcohol/Drugs			1	17							1	8
Mechanical Defect											0	0
Pedestrian Error											0	0
Road Defect/Construction Activity											0	0
Other											0	0
ROAD CONDITIONS												
Dry/Clear	6	100	6	100	10	100	16	94	15	94	53	96
Wet							1	6			1	2
Snowy/Icy									1	6	1	2
Other											0	0
LIGHTING												
Daylight	5	83	5	83	6	67	16	94	14	78	46	82
Darkness			1	17	2	22	1	6	1	6	5	9
Dawn or Dusk	1	17			1	11			3	17	5	9
SOBRIETY												
Sobriety Unknown											0	0
Had Been Drinking/Drug			1	14							1	2
Had Not Been Drinking/Drug	6	100	6	86	11	100	21	100	20	100	64	98

*Probable cause information excluded from post-2019 records.

ACCIDENT SUMMARY SHEET

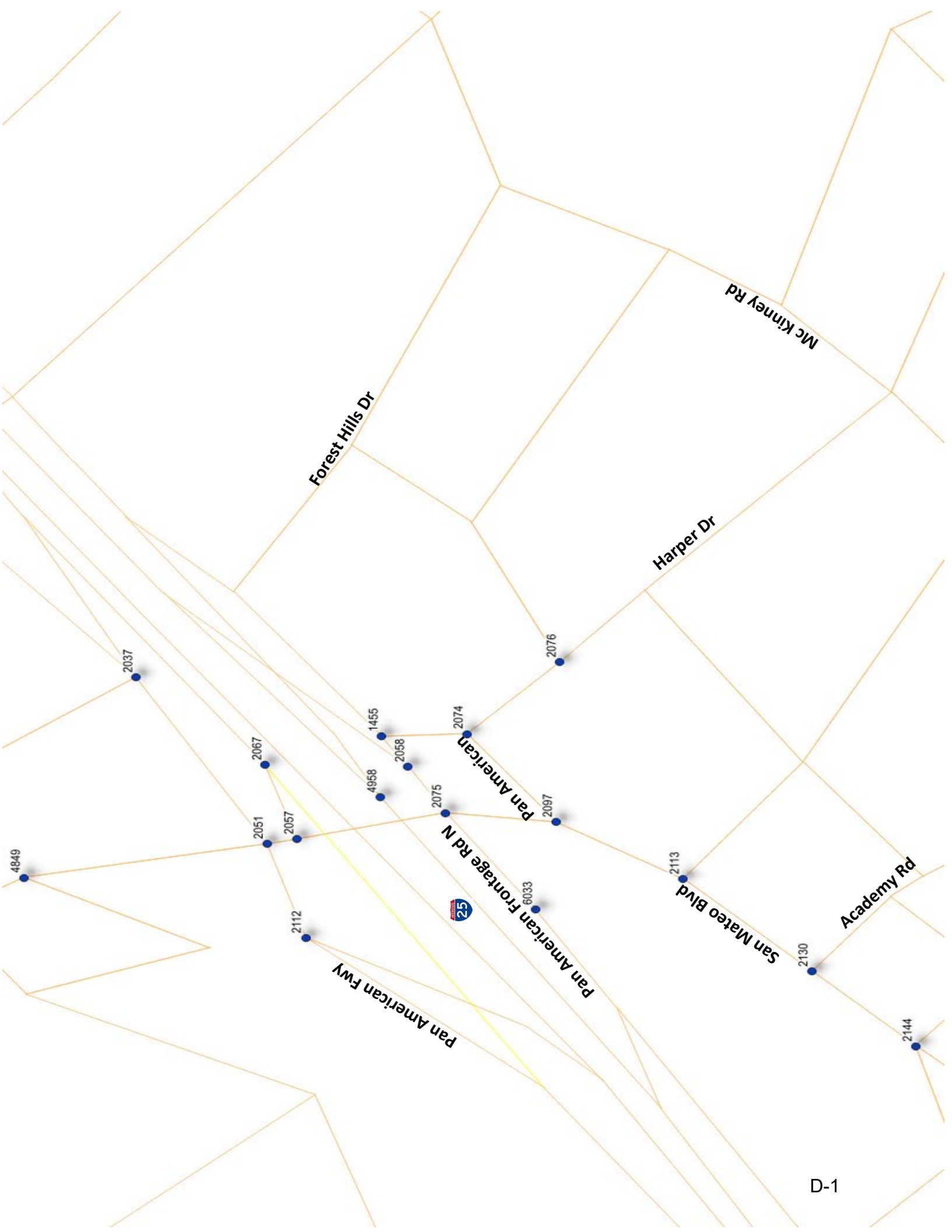
INTERSECTION/SEGMENT: Osuna/I25SBFR, San Mateo/I25NBFR, San Mateo/Pan American NE

ROUTE Osuna/San Mateo MP # TO MP #	YEAR 2018		YEAR 2019		YEAR 2020		YEAR 2021		YEAR 2022		TOTAL	
	No. 67	24%	No. 77	28%	No. 58	21%	No. 31	11%	No. 42	15%	No. 275	% 100
CRASH SEVERITY												
Property Damage Only (PDO)	60	71	83	78	48	69	33	70	38	73	262	73
Injury/Non-Fatal	25	29	24	22	22	31	14	30	14	27	99	27
Fatal	0	0	0	0	0	0	0	0	0	0	0	0
CRASH CLASSIFICATION (ACCIDENT TYPE)												
Fixed Object	1	1	4	5	3	5	2	6	0	0	10	4
Right Angle	18	27	18	23	21	36	10	32	15	36	82	30
Rear End	12	18	14	18	2	3	0	0	7	17	35	13
Backing	0	0	0	0	0	0	0	0	0	0	0	0
Sideswipe: Same Direction	9	13	10	13	5	9	4	13	17	40	45	16
Sideswipe: Opposite Direction	6	9	12	16	14	24	12	39	1	2	45	16
Head On	1	1	1	1	1	2	0	0	2	5	5	2
Left Turn	18	27	14	18	11	19	0	0	0	0	43	16
Parked Vehicle/Parking Maneuver	0	0	2	3	0	0	0	0	0	0	2	1
Overturn	0	0	1	1	0	0	1	3	0	0	2	1
Driveway/Driveway Maneuver	0	0	0	0	0	0	0	0	0	0	0	0
Pedestrian/Bicyclist	1	1	1	1	0	0	0	0	0	0	2	1
Other	1	1	0	0	1	2	2	6	0	0	4	1
PROBABLE CAUSE ("HIGHEST CONTRIBUTING FACTOR")*												
Following Too Close	5	6	11	12							16	9
Driver Inattention	17	22	19	20							36	21
Excess Speed/Too Fast For Conditions	7	9	2	2							9	5
Avoid Other Vehicle	4	5	1	1							5	3
Improper Driving	13	17	26	28							39	23
Failure to use Turn Signal	0	0	0	0							0	0
Failure to Yield R.O.W.	19	24	17	18							36	21
Disregard Traffic Control Device	10	13	11	12							21	12
Under Influence Alcohol	0	0	2	2							2	1
Mechanical Defect	3	4	3	3							6	4
Pedestrian Error	0	0	0	0							0	0
Road Defect/Construction Activity	0	0	0	0							0	0
Other	0	0	1	1							1	1
ROAD CONDITIONS												
Dry/Clear	75	95	93	91	62	93	34	87	37	82	301	91
Wet	4	5	6	6	3	4	2	5	2	4	17	5
Snowy/Icy	0	0	2	2	2	3	0	0	1	2	5	2
Other	0	0	1	1	0	0	3	8	5	11	9	3
LIGHTING												
Daylight	64	78	81	79	51	77	35	88	37	74	268	79
Darkness	14	17	18	17	10	15	5	13	10	20	57	17
Dawn or Dusk	4	5	4	4	5	8	0	0	3	6	16	5
SOBRIETY												
Sobriety Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Had Been Drinking/Drug	0	0	2	2	3	4	0	0	3	6	8	2
Had Not Been Drinking/Drug	83	100	104	98	67	96	45	100	49	94	348	98

*Probable cause information excluded from post-2019 records.

APPENDIX D

Forecast Traffic Data



San Mateo/I-25 SE
Traffic Impact Study

Forecast Link Traffic Volumes¹

STREET	A	B	APPROACH	CONTROL	NLANES	CAPACITY	FFS	VC_PM	DayVol_16	AMPH_16	PMPH_16	DayVol1640	AMPH1640	PMPH1640	2040 AMPH	Growth Rate ²	2040 PMPH	Growth Rate ²
Harper	1455	2074		STOP	1	700	24	0.61912	6040	415	703	-1131	-35	-270	380	-0.4%	433	-2.0%
I25 SB Frontage	2037	2051	MAJOR	SIGNAL	4	1000	33.6	0.4837	7857	544	783	11194	1131	1152	1675	4.8%	1935	3.8%
Osuna	2051	2057	MAJOR	SIGNAL	3	850	29.225	0.98978	12297	690	1422	9006	882	1102	1572	3.5%	2524	2.4%
I25 SB Frontage	2051	2112			3	1000	33.6	0.4308	12848	1206	1222	1021	13	70	1219	0.0%	1292	0.2%
Osuna	2051	4849			3	850	29.225	0.5456	17027	1657	1206	2173	196	186	1853	0.5%	1392	0.6%
Osuna	2057	2051	MAJOR	SIGNAL	3	850	29.225	0.52629	22779	2417	1583	-3762	-402	-241	2015	-0.8%	1342	-0.7%
Osuna	2057	2075	MAJOR	SIGNAL	4	850	29.225	0.95111	23058	1533	2379	7805	589	855	2122	1.4%	3234	1.3%
I25 NB Frontage	2058	1455		UNCONTROLLED	2	1000	33.6	0.50629	9211	565	1321	-733	-52	-308	513	-0.4%	1073	-1.1%
I25 SB-EB Ramp	2067	2057	MAJOR	SIGNAL	1	1000	36.75	0.70983	14664	1167	1248	-5105	-618	-538	549	-3.1%	710	-2.3%
Harper	2074	1455		STOP	1	700	24	0.30087	57	20	0	500	-18	210	2	-8.9%	210	29.4%
Harper	2074	2076			1	700	24	1.04459	6447	447	744	-348	-28	-13	419	-0.3%	731	-0.1%
PanAmerican	2074	2097	MINOR	SIGNAL	1	700	28	0.80565	4402	581	368	2551	157	196	738	1.0%	564	1.8%
Osuna	2075	2057	MINOR	SIGNAL	3	850	29.225	0.52629	18875	2093	1292	142	-78	50	2015	-0.2%	1342	0.2%
I25 NB Frontage	2075	2058			3	1000	33.6	1.10612	18190	1252	2323	14688	1299	995	2551	3.0%	3318	1.5%
San Mateo	2075	2097	MAJOR	SIGNAL	4	850	29.225	0.69916	21741	1469	1855	4555	385	523	1854	1.0%	2378	1.0%
Harper	2076	2074		STOP	1	700	24	0.81072	4452	598	368	2510	139	199	737	0.9%	567	1.8%
PanAmerican	2097	2074		STOP	1	700	28	0.72126	413	34	41	1325	6	464	40	0.7%	505	11.0%
San Mateo	2097	2075	MAJOR	SIGNAL	4	850	29.225	0.70654	24677	2330	2163	7277	558	240	2888	0.9%	2403	0.4%
San Mateo	2097	2113			4	850	29.225	0.66585	22526	1580	1912	4330	386	352	1966	0.9%	2264	0.7%
San Mateo	2113	2097	MAJOR	SIGNAL	3	850	29.225	0.87448	21474	1894	1893	5826	409	337	2303	0.8%	2230	0.7%
San Mateo	2113	2130	MAJOR	SIGNAL	4	850	29.225	0.62373	19640	1473	1671	5069	390	450	1863	1.0%	2121	1.0%
San Mateo	2130	2113			3	850	29.225	0.81745	18780	1694	1715	5626	365	370	2059	0.8%	2085	0.8%
San Mateo	2130	2144			3	750	29.225	0.92902	20656	1693	1857	3153	225	233	1918	0.5%	2090	0.5%
San Mateo	2144	2130	MINOR	SIGNAL	3	750	29.225	0.94697	17977	1491	1830	5304	384	301	1875	1.0%	2131	0.6%
Osuna	4849	2051	MINOR	SIGNAL	3	850	29.225	0.75716	11536	592	1484	4768	362	447	954	2.0%	1931	1.1%
I25 NB Frontage	6033	2075	MINOR	SIGNAL	5	1100	35.88	0.25482	11071	951	928	4302	460	474	1411	1.7%	1402	1.7%
																		2.2%

¹ Source: Mid Region Council of Governments (MRCOG); highlighted calculations derived by Civil Transformations Inc.

² Annual growth rate from 2016 to horizon year

Detailed Land Use Data

For 5.69 1000 Sq. Ft. GLA of LU 822 Strip Retail Plaza (<40k) - Building 3
(820) Shopping Center

Open Date: 07/01/2025
Analysis Date: 08/16/2023

Project: SanMateo-I25 SE

Day / Period	Total Trips	Pass-By Trips	Avg Rate	Min Rate	Max Rate	Std Dev	Avg Size	% Enter	% Exit	Use Eq.	Equation	R ²
Weekday Average Daily Trips Source : ITE11 - Custom	310	0	54.45	47.86	65.07	7.81	19	50	50	False	$T = 42.2(X) + 229.68$	0.96
Weekday AM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	20	0	2.36	1.6	3.73	0.94	18	60	40	True	$\ln(T) = 0.66 \ln(X) + 1.84$	0.57
Weekday PM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	52	18	6.59	2.81	15.2	2.94	21	50	50	True	$\ln(T) = 0.71 \ln(X) + 2.72$	0.56

Detailed Land Use Data

For 2.47 1000 Sq. Ft. GFA of FASTFOOD-DRIVE 1 - Building 1
(934) Fast-Food Restaurant with Drive-Through Window

Open Date: 07/01/2025
Analysis Date: 08/16/2023

Project: SanMateo-I25 SE

Day / Period	Total Trips	Pass-By Trips	Avg Rate	Min Rate	Max Rate	Std Dev	Avg Size	% Enter	% Exit	Use Eq.	Equation	R2
Weekday Average Daily Trips Source : ITE11 - Custom	1155	0	467.48	98.89	1137.66	238.62	3	50	50	False		0
Weekday AM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	110	54	44.61	1.05	164.25	27.14	4	51	49	False		0
Weekday PM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	82	41	33.03	8.77	117.22	17.59	3	52	48	False		0

Detailed Land Use Data

For 2.47 1000 Sq. Ft. GFA of FASTFOOD-DRIVE 1 - Building 2
(934) Fast-Food Restaurant with Drive-Through Window

Project: SanMateo-I25 SE Open Date: 07/01/2025
Analysis Date: 08/16/2023

Day / Period	Total Trips	Pass-By Trips	Avg Rate	Min Rate	Max Rate	Std Dev	Avg Size	% Enter	% Exit	Use Eq.	Equation	R2
Weekday Average Daily Trips Source : ITE11 - Custom	1155	0	467.48	98.89	1137.66	238.62	3	50	50	False		0
Weekday AM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	110	54	44.61	1.05	164.25	27.14	4	51	49	False		0
Weekday PM Peak Hour of Adjacent Street Traffic Source : ITE11 - Custom	82	41	33.03	8.77	117.22	17.59	3	52	48	False		0

Internal Capture Report - Alternative 1

AM & PM Peak Hour

Project: SanMateo-I25 SE

Open Date: 07/01/2025

Analysis Date: 08/16/2023

AM	Entering Trips				Exiting Trips			
Category	Internal	External	Total	Percent	Internal	External	Total	Percent
Cinema	0	0	0	0%	0	0	0	0%
Hotel	0	0	0	0%	0	0	0	0%
Office	0	0	0	0%	0	0	0	0%
Residential	0	0	0	0%	0	0	0	0%
Restaurant	1	111	112	1%	1	107	108	1%
Retail	1	11	12	8%	1	7	8	12%
All Other Land Uses	0	0	0	0%	0	0	0	0%
Totals	2	122	124	2%	2	114	116	2%

PM	Entering Trips				Exiting Trips			
Category	Internal	External	Total	Percent	Internal	External	Total	Percent
Cinema	0	0	0	0%	0	0	0	0%
Hotel	0	0	0	0%	0	0	0	0%
Office	0	0	0	0%	0	0	0	0%
Residential	0	0	0	0%	0	0	0	0%
Restaurant	8	78	86	9%	13	65	78	17%
Retail	13	13	26	50%	8	18	26	31%
All Other Land Uses	0	0	0	0%	0	0	0	0%
Totals	21	91	112	19%	21	83	104	20%

Trip Generation Summary

Alternative: Alternative 1

Phase:

Open Date: 07/01/2025

Project: SanMateo-125 SE

Analysis Date: 08/16/2023

ITE	Land Use	Weekday Average Daily Trips				Weekday AM Peak Hour of Adjacent Street Traffic				Weekday PM Peak Hour of Adjacent Street Traffic			
		★	Enter	Exit	Total	★	Enter	Exit	Total	★	Enter	Exit	Total
820	LU 822 Strip Retail Plaza (<40k) - Building 3 5.69 1000 Sq. Ft. GLA	✓	155	155	310	✓	12	8	20	✓	26	26	52
934	FASTFOOD-DRIVE 1 - Building 2 2.47 1000 Sq. Ft. GFA	✓	578	577	1155	✓	56	54	110	✓	43	39	82
934	FASTFOOD-DRIVE 1 - Building 1 2.47 1000 Sq. Ft. GFA	✓	578	577	1155	✓	56	54	110	✓	43	39	82
Unadjusted Volume			1311	1309	2620		124	116	240		112	104	216
Internal Capture Trips			0	0	0		2	2	4		21	21	42
Pass-By Trips			0	0	0		54	54	108		41	41	82
Volume Added to Adjacent Streets			1311	1309	2620		68	60	128		50	42	92

Total Weekday Average Daily Trips Internal Capture = 0 Percent

Total Weekday AM Peak Hour of Adjacent Street Traffic Internal Capture = 2 Percent

Total Weekday PM Peak Hour of Adjacent Street Traffic Internal Capture = 19 Percent

D-7 * - Custom rate used for selected time period.

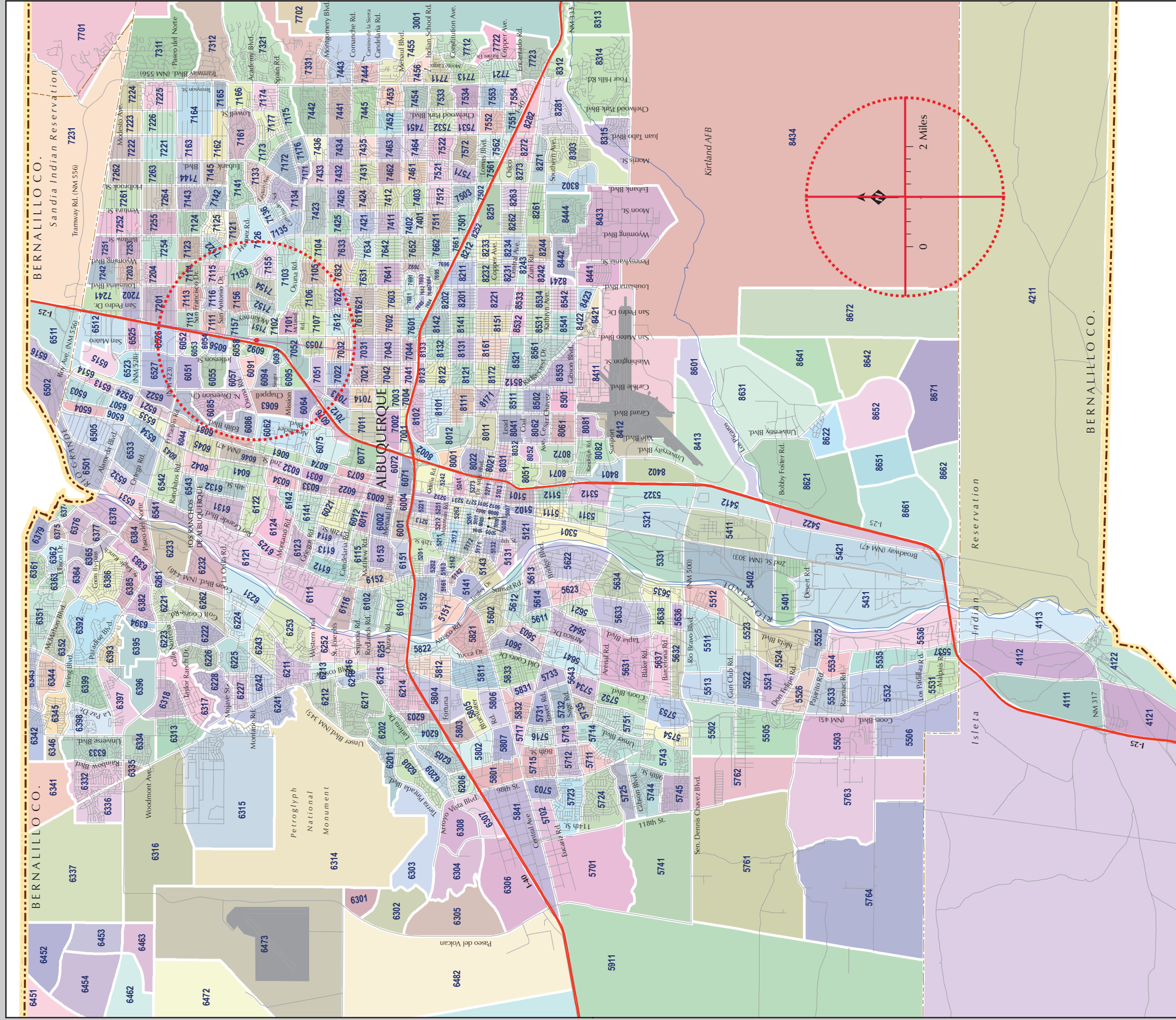
Source: Institute of Transportation Engineers, Trip Generation Manual 10th Edition
TRIP GENERATION 10, TRAFFICWARE, LLC

Driveway Report : Alternative 1





Project : SanMateo-I25 SE
Alternative : Alternative 1

Open Date : 2025-07-01
Analysis Date : 2023-08-16

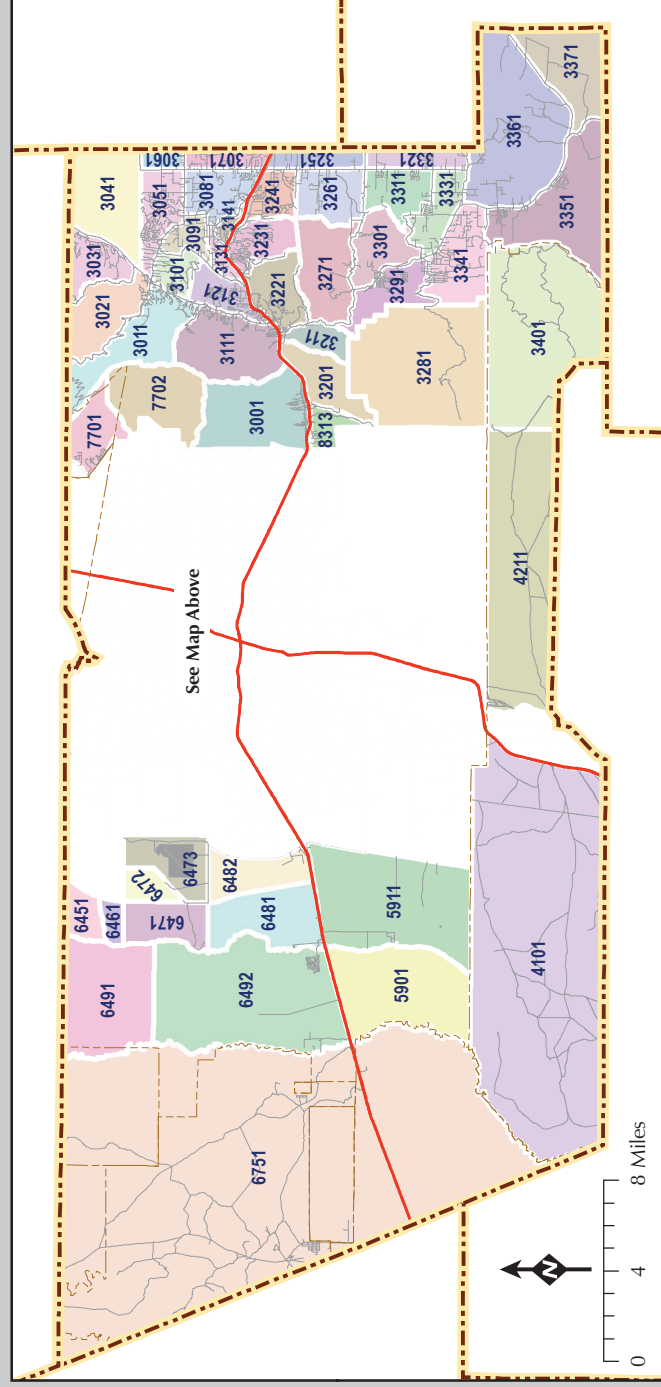
	<u>% In</u>	<u>% Out</u>	<u>Trips In</u>	<u>Trips Out</u>
Name : North				
Description : Access to FF w/DTW				
Weekday Average Daily Trips	80	80	0	0
Weekday AM Peak Hour of Adjacent Street Traffic	80	80	54	48
Weekday PM Peak Hour of Adjacent Street Traffic	80	80	40	33
Name : South				
Description : Access to Retail Site				
Weekday Average Daily Trips	20	20	0	0
Weekday AM Peak Hour of Adjacent Street Traffic	20	20	13	12
Weekday PM Peak Hour of Adjacent Street Traffic	20	20	10	8



2010 Data Analysis Subzones (DASZ) Bernalillo County, NM

-  DASZ and DASZ Number
 Roadways
 Indian Reservation Boundary
 County Boundary

Source: MRCOG 2011
Printed: March 2014



Trip Distribution Table

Trip Type: Commercial

Project:
I-25 + San Mateo Commercial

Prepared By:
Civil Transformations Inc.

Sub Area Population Data							NORTH - PanAm FWY			NORTHEAST - Pan American			SOUTHEAST - San Mateo			SOUTH - PanAm FWY			WEST - Osuna		
DASZ NUMBER ¹	% DASZ ¹ in Study	2016 Population ²	2040 Population ²	Interpolated Population for the Year	Population in Study Area	% Population	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion
		2016		2025																	
6051	100%	0	0	0	0	0.00%	25%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	75%	0.00%	0
6052	100%	0	408	-2	-2	0.00%	75%	0.00%	-1	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
6053	100%	0	0	0	0	0.00%	75%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
6054	100%	0	0	0	0	0.00%	75%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
6055	100%	57	184	56	56	0.12%	50%	0.06%	28	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	50%	0.06%	28
6056	100%	0	62	0	0	0.00%	75%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
6057	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	100%	0.00%	0
6058	100%	50	70	50	50	0.10%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	100%	0.10%	50
6062	60%	1,334	1,543	1,333	800	1.65%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.41%	200	75%	1.24%	600
6063	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0	75%	0.00%	0
6064	100%	130	325	129	129	0.27%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	70%	0.19%	90	30%	0.08%	39
6076	50%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	100%	0.00%	0	0%	0.00%	0
6081	10%	351	536	350	35	0.07%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	100%	0.07%	35
6085	100%	3,882	3,911	3,882	3,882	8.01%	25%	2.00%	970	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	75%	6.01%	2,911
6086	80%	1,051	1,350	1,050	840	1.73%	50%	0.87%	420	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	50%	0.87%	420
6091	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	100%	0.00%	0
6092	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	50%	0.00%	0	50%	0.00%	0
6093	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	50%	0.00%	0	50%	0.00%	0
6094	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	50%	0.00%	0	50%	0.00%	0
6095	100%	0	0	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	75%	0.00%	0	25%	0.00%	0
6522	5%	0	0	0	0	0.00%	75%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
6526	50%	1,005	940	1,005	503	1.04%	75%	0.78%	377	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.26%	126
6527	40%	0	0	0	0	0.00%	75%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	25%	0.00%	0
7012	20%	568	1,239	565	113	0.23%	0%	0.00%	0	0%	0.00%	0	25%	0.06%	28	75%	0.17%	85	0%	0.00%	0
7013	40%	1,213	1,217	1,213	485	1.00%	0%	0.00%	0	0%	0.00%	0	25%	0.25%	121	75%	0.75%	364	0%	0.00%	0
7022	90%	1,598	1,715	1,597	1,438	2.97%	0%	0.00%	0	0%	0.00%	0	25%	0.74%	359	75%	2.23%	1,078	0%	0.00%	0
7031	5%	1,926	2,435	1,924	96	0.20%	0%	0.00%	0	0%	0.00%	0	50%	0.10%	48	50%	0.10%	48	0%	0.00%	0
7032	100%	1,572	1,785	1,571	1,571	3.24%	0%	0.00%	0	0%	0.00%	0	50%	1.62%	786	50%	1.62%	786	0%	0.00%	0
7051	100%	3,353	2,953	3,355	3,355	6.92%	0%	0.00%	0	0%	0.00%	0	25%	1.73%	839	75%	5.19%	2,516	0%	0.00%	0
7052	100%	0	43	0	0	0.00%	0%	0.00%	0	0%	0.00%	0	50%	0.00%	0	50%	0.00%	0	0%	0.00%	0
7053	100%	187	291	187	187	0.38%	0%	0.00%	0	0%	0.00%	0	50%	0.19%	93	50%	0.19%	93	0%	0.00%	0
7101	100%	2,265	2,586	2,264	2,264	4.67%	0%	0.00%	0	0%	0.00%	0	50%	2.34%	1,132	50%	2.34%	1,132	0%	0.00%	0
7102	100%	756	967	755	755	1.56%	0%	0.00%	0	0%	0.00%	0	100%	1.56%	755	0%	0.00%	0	0%	0.00%	0
7103	80%	977	1,161	976	781	1.61%	0%	0.00%	0	0%	0.00%	0	100%	1.61%	781	0%	0.00%	0	0%	0.00%	0
7104	5%	1,185	1,270	1,185	59	0.12%	0%	0.00%	0	0%	0.00%	0	100%	0.12%	59	0%	0.00%	0	0%	0.00%	0
7105	90%	1,906	2,369	1,904	1,714	3.54%	0%	0.00%	0	0%	0.00%	0	100%	3.54%	1,714	0%	0.00%	0	0%	0.00%	0
7106	100%	2,006	2,259	2,005	2,005	4.14%	0%	0.00%	0	0%	0.00%	0	100%	4.14%	2,005	0%	0.00%	0	0%	0.00%	0
7107	100%	2,607	3,570	2,603	2,603	5.37%	0%	0.00%	0	0%	0.00%	0	75%	4.03%	1,952	25%	1.34%	651	0%	0.00%	0
7111	100%	1,292	1,446	1,291	1,291	2.66%	100%	2.66%	1,291	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7112	100%	0	0	0	0	0.00%	100%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0

Trip Distribution Table

Trip Type: Commercial

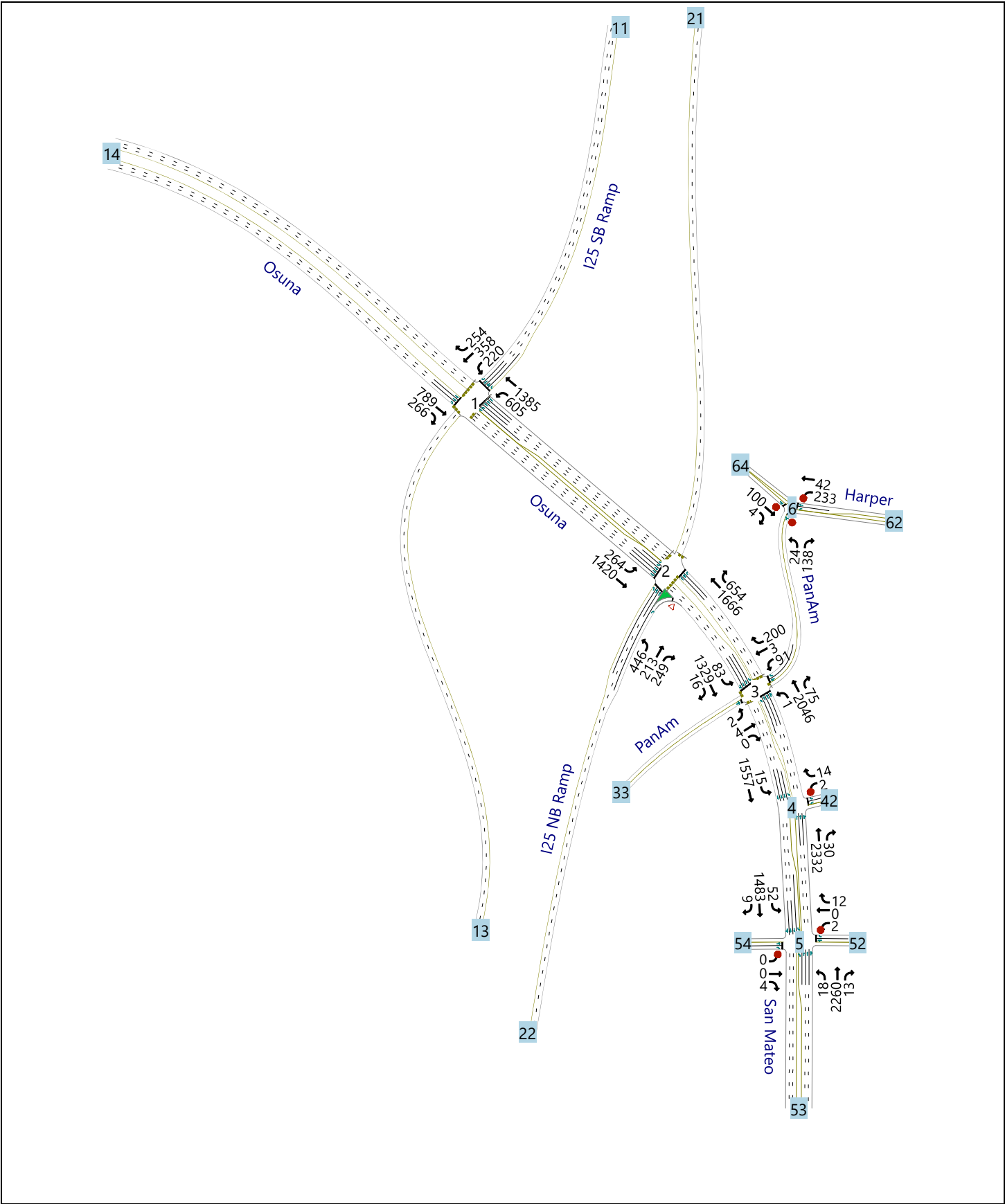
Project:
I-25 + San Mateo Commercial

Prepared By:
Civil Transformations Inc.

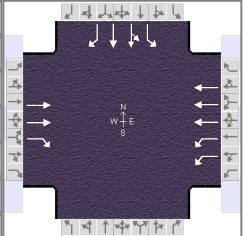
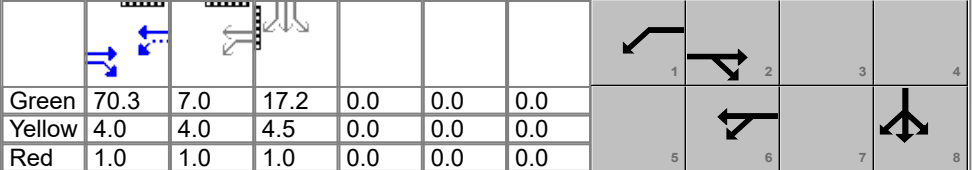
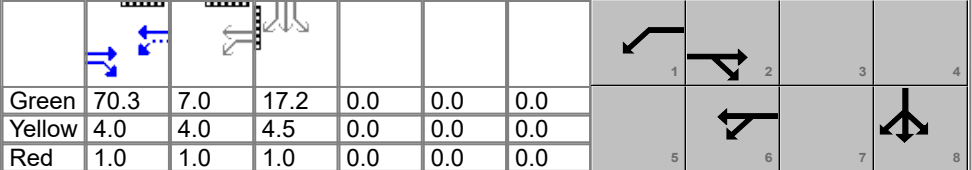
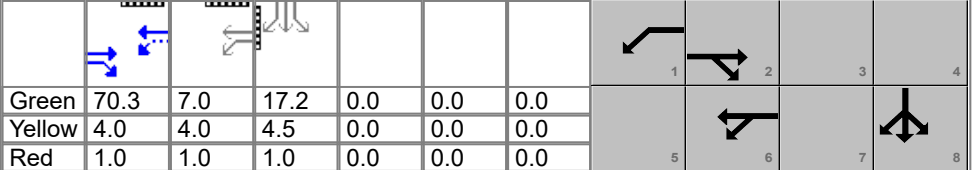
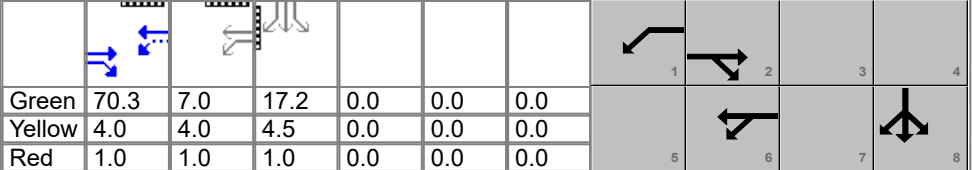
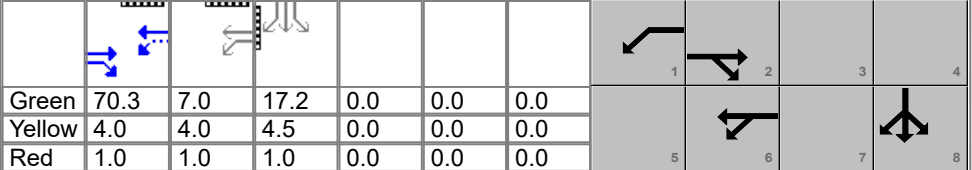
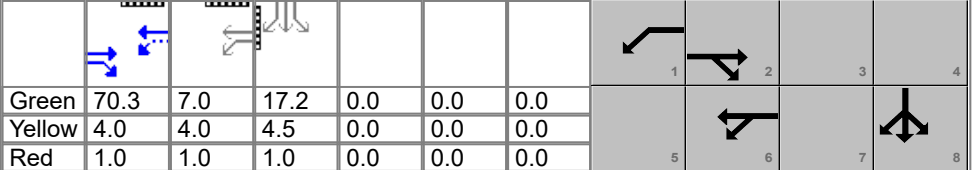
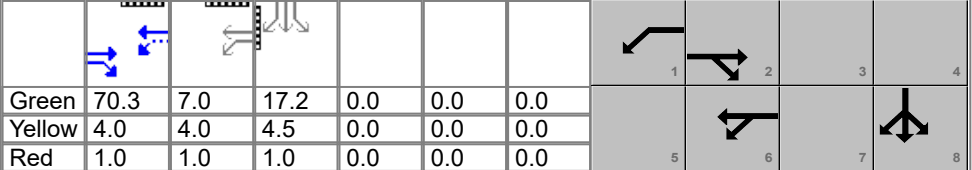
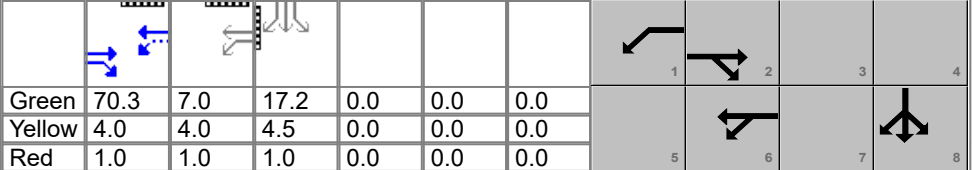
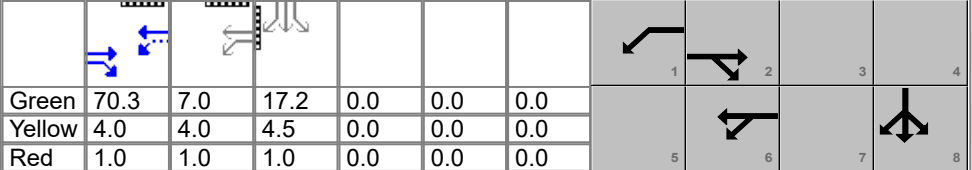
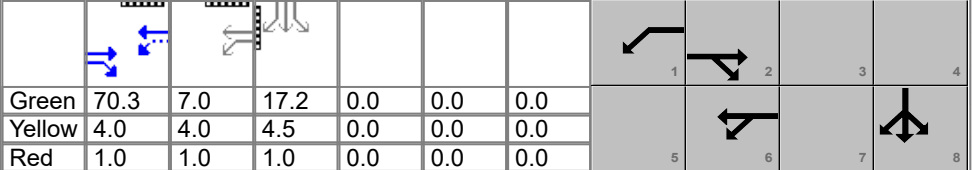
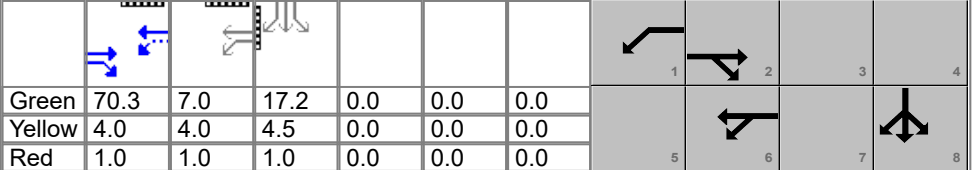
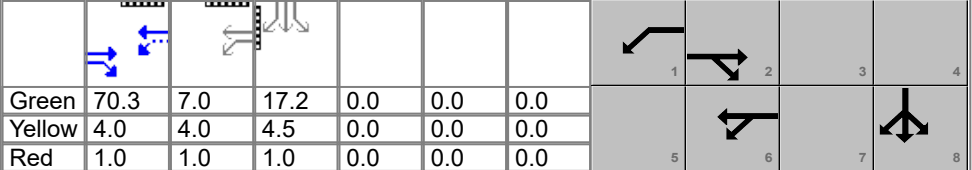
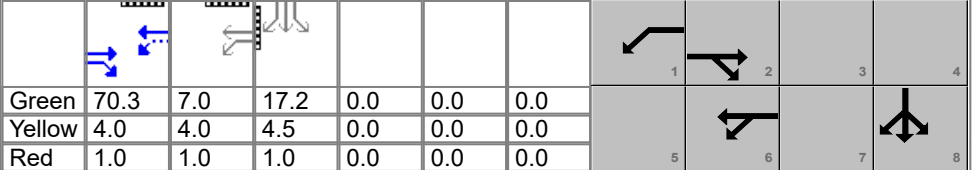
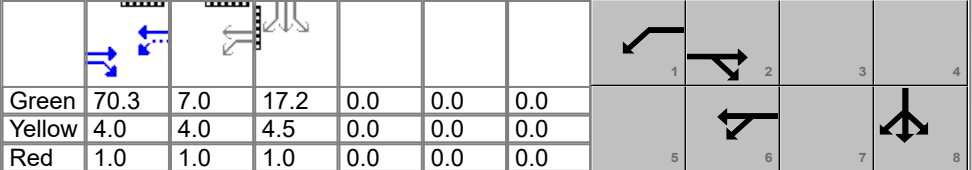
Sub Area Population Data							NORTH - PanAm FWY			NORTHEAST - Pan American			SOUTHEAST - San Mateo			SOUTH - PanAm FWY			WEST - Osuna		
DASZ NUMBER ¹	% DASZ ¹ in Study	2016 Population ²	2040 Population ²	Interpolated Population for the Year	Population in Study Area	% Population	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion	% Utilizing	% Population Utilizing	Popula- tion
		2016		2025																	
7113	100%	924	1,214	923	923	1.90%	75%	1.43%	692	25%	0.48%	231	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7114	60%	1,397	1,645	1,396	838	1.73%	75%	1.30%	628	25%	0.43%	209	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7115	100%	1,446	1,534	1,446	1,446	2.98%	75%	2.24%	1,084	25%	0.75%	361	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7116	100%	2,282	2,829	2,280	2,280	4.70%	50%	2.35%	1,140	50%	2.35%	1,140	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7121	10%	875	866	875	88	0.18%	75%	0.14%	66	25%	0.05%	22	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7122	30%	1,196	1,225	1,196	359	0.74%	75%	0.56%	269	25%	0.19%	90	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7123	5%	1,287	1,649	1,285	64	0.13%	75%	0.10%	48	25%	0.03%	16	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7126	30%	0	117	-1	0	0.00%	0%	0.00%	0	50%	0.00%	0	50%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7151	100%	959	1,447	957	957	1.97%	0%	0.00%	0	25%	0.49%	239	75%	1.48%	718	0%	0.00%	0	0%	0.00%	0
7152	100%	1,426	1,445	1,426	1,426	2.94%	0%	0.00%	0	50%	1.47%	713	50%	1.47%	713	0%	0.00%	0	0%	0.00%	0
7153	100%	1,227	1,474	1,226	1,226	2.53%	0%	0.00%	0	100%	2.53%	1,226	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7154	100%	1,103	1,254	1,102	1,102	2.27%	0%	0.00%	0	50%	1.14%	551	50%	1.14%	551	0%	0.00%	0	0%	0.00%	0
7155	100%	891	1,102	890	890	1.84%	0%	0.00%	0	50%	0.92%	445	50%	0.92%	445	0%	0.00%	0	0%	0.00%	0
7156	100%	1,360	1,581	1,359	1,359	2.80%	0%	0.00%	0	100%	2.80%	1,359	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7157	100%	737	964	736	736	1.52%	50%	0.76%	368	50%	0.76%	368	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7201	30%	2,090	2,249	2,089	627	1.29%	100%	1.29%	627	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0	0%	0.00%	0
7611	3%	1,739	1,834	1,739	52	0.11%	0%	0.00%	0	0%	0.00%	0	100%	0.11%	52	0%	0.00%	0	0%	0.00%	0
7612	100%	926	1,140	925	925	1.91%	0%	0.00%	0	0%	0.00%	0	100%	1.91%	925	0%	0.00%	0	0%	0.00%	0
7622	80%	9,921	1,302	9,959	7,968	16.44%	0%	0.00%	0	0%	0.00%	0	100%	16.44%	7,968	0%	0.00%	0	0%	0.00%	0
7632	20%	956	1,197	955	191	0.39%	0%	0.00%	0	0%	0.00%	0	100%	0.39%	191	0%	0.00%	0	0%	0.00%	0
		64,013	64,703	64,010	48,462	100%		17%			14%			46%			15%			9%	

¹DASZ = Data Analysis SubZone
²Source: Mid-Region Council of Governments (MRCOG) "2040 Socioeconomic Forecasts by Data Analysis Subzones for the Mid-Region of New Mexico "

APPENDIX E
LOS Worksheets
Baseline Scenario (2023)
Implementation Year (2025)
Horizon Year (2035)



HCS Signalized Intersection Results Summary

General Information						Intersection Information											
Agency	Civil Transformations Inc.					Duration, h	0.250										
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other										
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.92										
Urban Street	San Mateo		Analysis Year	2023		Analysis Period	1> 7:15										
Intersection	I25 SB Ramp		File Name	SM-I25_2023AMX.xus													
Project Description	Baseline Scenario																
Demand Information						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h							789	266	605	1385					220	358	254
Signal Information																	
Cycle, s	110.0	Reference Phase	2														
Offset, s	56	Reference Point	Begin														
Uncoordinated	No	Simult. Gap E/W	On														
Force Mode	Fixed	Simult. Gap N/S	On														
				Green	70.3	7.0	17.2	0.0	0.0	0.0							
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0							
				Red	1.0	1.0	1.0	0.0	0.0	0.0							
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT						
Assigned Phase					2	1	6				8						
Case Number					7.4	1.0	4.0				9.0						
Phase Duration, s					75.3	12.0	87.3				22.7						
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5						
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1						
Queue Clearance Time (g s), s						2.0					15.6						
Green Extension Time (g e), s					0.0	5.0	0.0				1.6						
Phase Call Probability						1.00					1.00						
Max Out Probability						0.02					0.00						
Movement Group Results				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Assigned Movement					2	12	1	6					3	8	18		
Adjusted Flow Rate (v), veh/h					858	217	721	1652					167	461	115		
Adjusted Saturation Flow Rate (s), veh/h/ln					1845	1527	1497	1718					1781	1802	1505		
Queue Service Time (g s), s					0.0	0.0	0.0	10.5					9.6	13.6	7.7		
Cycle Queue Clearance Time (g c), s					0.0	0.0	0.0	10.5					9.6	13.6	7.7		
Green Ratio (g/C)					0.64	0.64	0.70	0.75					0.16	0.16	0.16		
Capacity (c), veh/h					2357	976	1145	3857					279	563	235		
Volume-to-Capacity Ratio (X)					0.364	0.223	0.630	0.428					0.601	0.818	0.489		
Back of Queue (Q), ft/ln (95 th percentile)					6	6	171	94					194	257	131		
Back of Queue (Q), veh/ln (95 th percentile)					0.3	0.3	6.7	3.7					7.6	10.1	5.2		
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.53	0.00					0.97	0.00	0.58		
Uniform Delay (d 1), s/veh					0.0	0.0	11.9	3.7					43.2	44.9	42.4		
Incremental Delay (d 2), s/veh					0.4	0.5	0.1	0.1					0.8	1.1	0.6		
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0		
Control Delay (d), s/veh					0.4	0.5	11.9	3.8					44.0	46.0	43.0		
Level of Service (LOS)					A	A	B	A					D	D	D		
Approach Delay, s/veh / LOS				0.5	A		6.3	A		0.0			45.1	D			
Intersection Delay, s/veh / LOS				11.7						B							
Multimodal Results				EB			WB			NB			SB				
Pedestrian LOS Score / LOS																	
Bicycle LOS Score / LOS																	

HCS Signalized Intersection Results Summary

General Information						Intersection Information							
Agency		Civil Transformations Inc.				Duration, h		0.250					
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other			
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.89			
Urban Street		San Mateo		Analysis Year		2023		Analysis Period		1> 7:15			
Intersection		I25 NB Ramp		File Name		SM-I25_2023AMX.xus							
Project Description		Baseline Scenario											

Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				264	1420			1666	654	446	213	249			

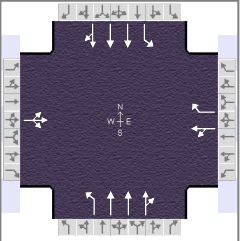
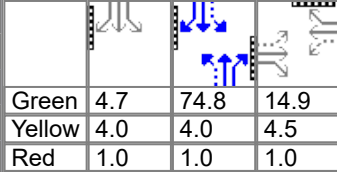
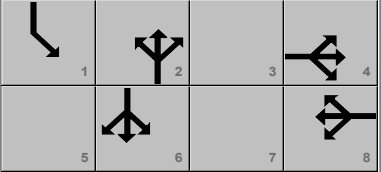
Signal Information															
Cycle, s	110.0	Reference Phase	2	Green	64.4	5.5	25.1	0.0	0.0	0.0					
Offset, s	51	Reference Point	Begin	Yellow	4.0	3.5	4.5	0.0	0.0	0.0					
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	1.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On												

Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				5	2		6		4		
Case Number				1.0	4.0		7.4		9.0		
Phase Duration, s				10.0	79.4		69.4		30.6		
Change Period, (Y+R c), s				5.0	5.0		5.0		5.5		
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1		
Queue Clearance Time (g s), s				2.0					23.0		
Green Extension Time (g e), s				1.5	0.0		0.0		2.1		
Phase Call Probability				0.99					1.00		
Max Out Probability				1.00					0.01		

Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				172	925			2003	494	25	715	183			
Adjusted Saturation Flow Rate (s), veh/h/ln				1730	1658			1981	1589	1781	1801	1520			
Queue Service Time (g s), s				0.0	6.6			37.5	12.8	1.2	21.0	11.6			
Cycle Queue Clearance Time (g c), s				0.0	6.6			37.5	12.8	1.2	21.0	11.6			
Green Ratio (g/C)				0.63	0.68			0.59	0.59	0.23	0.23	0.23			
Capacity (c), veh/h				407	3363			2319	930	407	823	347			
Volume-to-Capacity Ratio (X)				0.423	0.275			0.864	0.532	0.062	0.870	0.528			
Back of Queue (Q), ft/ln (95 th percentile)				100	86			322	156	24	368	196			
Back of Queue (Q), veh/ln (95 th percentile)				3.9	3.4			12.7	6.1	0.9	14.5	7.7			
Queue Storage Ratio (RQ) (95 th percentile)				0.40	0.00			0.00	0.00	0.08	0.00	0.56			
Uniform Delay (d 1), s/veh				39.6	5.5			9.1	6.4	33.2	40.9	37.2			
Incremental Delay (d 2), s/veh				0.6	0.2			4.6	2.2	0.0	2.7	0.5			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				40.2	5.6			13.7	8.6	33.2	43.5	37.7			
Level of Service (LOS)				D	A			B	A	C	D	D			
Approach Delay, s/veh / LOS				11.1	B		12.7	B		42.1	D		0.0		
Intersection Delay, s/veh / LOS				18.3						B					










Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.87									
Urban Street		San Mateo		Analysis Year		2023		Analysis Period		1> 7:15									
Intersection		PanAm		File Name		SM-I25_2023AMX-Node3.xus													
Project Description		Baseline Scenario																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				2	4	0	91	3	200	1	2046	75	83	1329	16				
Signal Information																			
Cycle, s	110.0	Reference Phase	2																
Offset, s	47	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	4.7	74.8	14.9	0.0	0.0	0.0									
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						4				8				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						20.4				20.4				79.8		9.7		89.6	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.4				4.4				0.0		2.5		0.0	
Queue Clearance Time (g s), s						2.4				13.0						3.6			
Green Extension Time (g e), s						1.1				1.1				0.0		0.1		0.0	
Phase Call Probability						0.19				1.00						0.95			
Max Out Probability						0.00				0.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				7	4	14	3	8	18	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					7			108	164	1	1631	807	95	1028	512				
Adjusted Saturation Flow Rate (s), veh/h/ln					1735			1414	1585	337	1870	1835	1781	1870	1862				
Queue Service Time (g s), s					0.0			7.5	11.0	0.1	27.2	27.6	1.6	9.6	9.6				
Cycle Queue Clearance Time (g c), s					0.4			7.9	11.0	0.1	27.2	27.6	1.6	9.6	9.6				
Green Ratio (g/C)					0.14			0.14	0.14	0.68	0.68	0.68	0.74	0.77	0.77				
Capacity (c), veh/h					279			256	215	294	2545	1249	202	2876	1432				
Volume-to-Capacity Ratio (X)					0.025			0.422	0.764	0.004	0.641	0.646	0.472	0.358	0.358				
Back of Queue (Q), ft/ln (95 th percentile)					8			130	209	0	378	391	66	120	126				
Back of Queue (Q), veh/ln (95 th percentile)					0.3			5.1	8.2	0.0	14.9	15.4	2.6	4.7	5.0				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	0.70	0.00	0.00	0.00	0.66	0.00	0.00				
Uniform Delay (d 1), s/veh					41.2			44.5	45.8	5.6	10.0	10.0	15.9	4.1	4.1				
Incremental Delay (d 2), s/veh					0.0			1.1	5.6	0.0	1.3	2.6	0.6	0.3	0.7				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					41.3			45.6	51.4	5.7	11.2	12.6	16.5	4.4	4.7				
Level of Service (LOS)					D			D	D	A	B	B	B	A	A				
Approach Delay, s/veh / LOS				41.3		D		49.1		D		11.7		B		5.2		A	
Intersection Delay, s/veh / LOS				11.6										B					
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, AM Peak
2023 NO-Build, 11

Intersection						
Int Delay, s/veh	0.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			  			  
Traffic Vol, veh/h	2	14	2332	30	15	1557
Future Vol, veh/h	2	14	2332	30	15	1557
Conflicting Peds, #/hr	0	0	0	2	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	15	2535	33	16	1692
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	3263	1286	0	0	2569	0
Stage 1	2553	-	-	-	-	-
Stage 2	710	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*23	133	-	-	64	-
Stage 1	*25	-	-	-	-	-
Stage 2	*720	-	-	-	-	-
Platoon blocked, %	0		-	-		-
Mov Cap-1 Maneuver	*17	133	-	-	64	-
Mov Cap-2 Maneuver	*17	-	-	-	-	-
Stage 1	*25	-	-	-	-	-
Stage 2	*535	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s/v	61.09	0		0.76		
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	17	133	64	-
HCM Lane V/C Ratio	-	-	0.125	0.114	0.256	-
HCM Control Delay (s/veh)	-	-	240	35.5	80.1	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.4	0.4	0.9	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

Intersection												
Int Delay, s/veh	4.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	4	2	0	12	18	2260	13	52	1483	9
Future Vol, veh/h	0	0	4	2	0	12	18	2260	13	52	1483	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	2	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	4	2	0	13	20	2511	14	58	1648	10





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2815	4338	831	3335	4336	1265	1660	0	0	2528	0	0
Stage 1	1770	1770	-	2560	2560	-	-	-	-	-	-	-
Stage 2	1044	2568	-	775	1775	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	33	1	*690	*12	1	138	401	-	-	67	-	-
Stage 1	185	277	-	*15	53	-	-	-	-	-	-	-
Stage 2	221	52	-	*708	275	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	-	-	-
Mov Cap-1 Maneuver	4	0	*688	*~ 2	0	137	401	-	-	67	-	-
Mov Cap-2 Maneuver	4	0	-	*~ 2	0	-	-	-	-	-	-	-
Stage 1	25	38	-	*14	50	-	-	-	-	-	-	-
Stage 2	189	50	-	*96	37	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.26		\$ 562.76	0.11	5.93
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	401	-	-	-	688	2	137	67	-	-
HCM Lane V/C Ratio	0.05	-	-	-	0.006	1.464	0.097	0.864	-	-
HCM Control Delay (s/veh)	14.5	-	-	0	10.3	3735.4	34	176.1	-	-
HCM Lane LOS	B	-	-	A	B	F	D	F	-	-
HCM 95th %tile Q(veh)	0.2	-	-	-	0	1	0.3	4.1	-	-

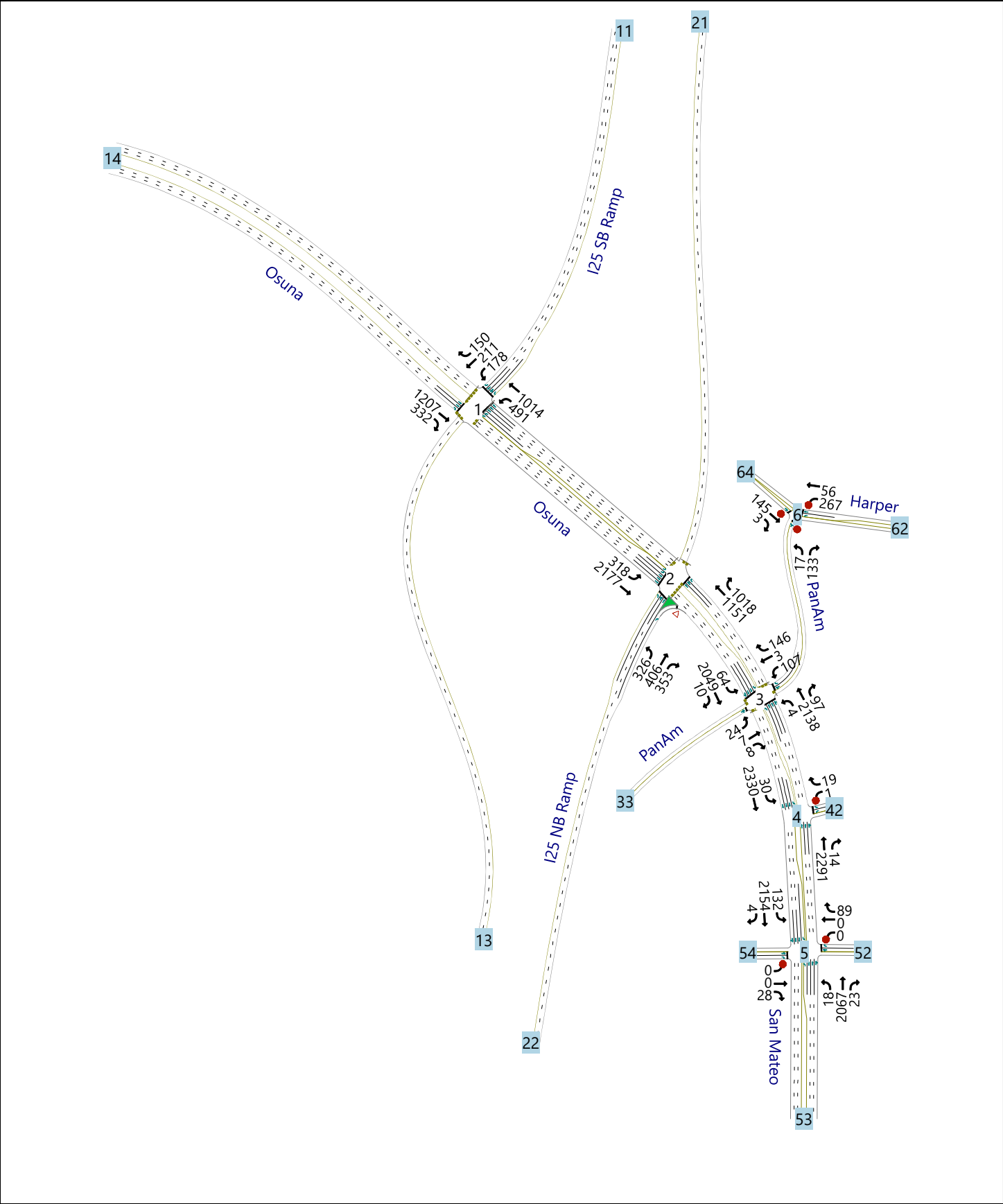
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined						*: All major volume in platoon				

Intersection	
Intersection Delay, s/veh	10.2
Intersection LOS	B

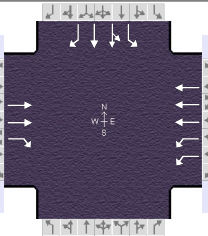
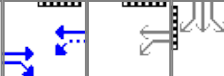











Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	100	4	233	42	24	138
Future Vol, veh/h	100	4	233	42	24	138
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	111	4	259	47	27	153
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	8.8	11.4	8.9
HCM LOS	A	B	A

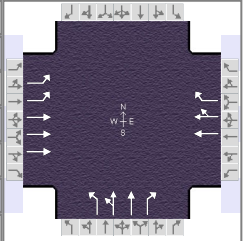
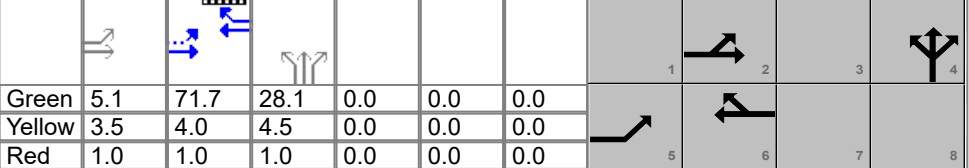
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	15%	0%	100%	0%
Vol Thru, %	0%	96%	0%	100%
Vol Right, %	85%	4%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	162	104	233	42
LT Vol	24	0	233	0
Through Vol	0	100	0	42
RT Vol	138	4	0	0
Lane Flow Rate	180	116	259	47
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.226	0.155	0.399	0.065
Departure Headway (Hd)	4.519	4.814	5.548	5.045
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	794	742	647	708
Service Time	2.55	2.864	3.294	2.791
HCM Lane V/C Ratio	0.227	0.156	0.4	0.066
HCM Control Delay, s/veh	8.9	8.8	12	8.1
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.9	0.5	1.9	0.2



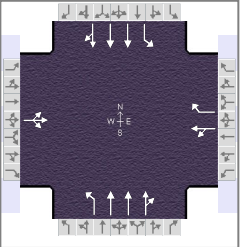
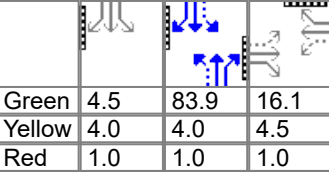
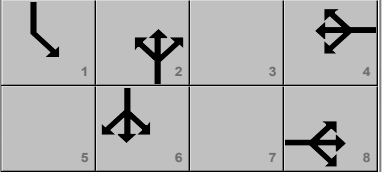
HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95					
Urban Street		Osuna		Analysis Year		2023		Analysis Period		1> 16:30					
Intersection		I25 SB Ramp		File Name		SM-I25_2023PMX.xus									
Project Description		Baseline Scenario													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1207	332	491	1014					178	211	150
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	61	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green	86.5	5.0	13.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	4.0	4.0	4.5	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	1.0	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				8				
Case Number					7.4	1.0	4.0				9.0				
Phase Duration, s					91.5	10.0	101.5				18.5				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1				
Queue Clearance Time (g s), s						2.0					12.0				
Green Extension Time (g e), s					0.0	3.0	0.0				1.0				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.00					0.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					3	8	18
Adjusted Flow Rate (v), veh/h					1271	286	502	1037					103	306	66
Adjusted Saturation Flow Rate (s), veh/h/ln					1901	1548	1367	1675					1781	1787	1496
Queue Service Time (g s), s					0.0	0.0	0.0	8.8					6.6	10.0	5.0
Cycle Queue Clearance Time (g c), s					0.0	0.0	0.0	8.8					6.6	10.0	5.0
Green Ratio (g/C)					0.72	0.72	0.76	0.80					0.11	0.11	0.11
Capacity (c), veh/h					2741	1116	862	4040					193	388	162
Volume-to-Capacity Ratio (X)					0.464	0.257	0.582	0.257					0.534	0.791	0.409
Back of Queue (Q), ft/ln (95 th percentile)					10	8	132	118					135	204	86
Back of Queue (Q), veh/ln (95 th percentile)					0.4	0.3	5.2	4.6					5.3	8.0	3.4
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.41	0.00					0.68	0.00	0.38
Uniform Delay (d 1), s/veh					0.0	0.0	8.0	4.7					50.6	52.2	49.9
Incremental Delay (d 2), s/veh					0.6	0.6	0.2	0.1					0.9	1.4	0.6
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					0.6	0.6	8.2	4.8					51.5	53.6	50.5
Level of Service (LOS)					A	A	A	A					D	D	D
Approach Delay, s/veh / LOS				0.6	A		5.9	A		0.0			52.7	D	
Intersection Delay, s/veh / LOS				9.8						A					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.96									
Urban Street		Osuna		Analysis Year		2023		Analysis Period		1> 16:30									
Intersection		I25 NB Ramp		File Name		SM-I25_2023PMX.xus													
Project Description		Baseline Scenario																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				318	2177			1151	1018	326	406	353							
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	55	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	5.1	71.7	28.1	0.0	0.0	0.0									
				Yellow	3.5	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase				5		2				6				4					
Case Number				1.0		4.0				7.3				9.0					
Phase Duration, s				9.6		86.4				76.7				33.6					
Change Period, (Y+R c), s				4.5		5.0				5.0				5.5					
Max Allow Headway (MAH), s				4.0		0.0				0.0				3.1					
Queue Clearance Time (g s), s				4.5										25.7					
Green Extension Time (g e), s				0.6		0.0				0.0				2.5					
Phase Call Probability				1.00										1.00					
Max Out Probability				0.00										0.01					
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				5	2			6	16	7	4	14							
Adjusted Flow Rate (v), veh/h				186	1272			1520	652	160	603	319							
Adjusted Saturation Flow Rate (s), veh/h/ln				1666	1696			1809	1644	1781	1817	1556							
Queue Service Time (g s), s				2.5	10.7			0.6	0.5	9.0	18.3	23.7							
Cycle Queue Clearance Time (g c), s				2.5	10.7			0.6	0.5	9.0	18.3	23.7							
Green Ratio (g/C)				0.64	0.68			0.60	0.60	0.23	0.23	0.23							
Capacity (c), veh/h				816	3451			2162	982	417	851	365							
Volume-to-Capacity Ratio (X)				0.228	0.369			0.703	0.664	0.382	0.708	0.874							
Back of Queue (Q), ft/ln (95 th percentile)				40	144			30	47	181	326	372							
Back of Queue (Q), veh/ln (95 th percentile)				1.6	5.7			1.2	1.8	7.1	12.8	14.6							
Queue Storage Ratio (RQ) (95 th percentile)				0.16	0.00			0.00	0.00	0.60	0.00	1.06							
Uniform Delay (d 1), s/veh				8.5	6.4			0.1	0.1	38.6	42.2	44.2							
Incremental Delay (d 2), s/veh				0.1	0.3			1.9	3.5	0.2	0.4	5.8							
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0							
Control Delay (d), s/veh				8.6	6.6			2.0	3.6	38.8	42.6	50.1							
Level of Service (LOS)				A	A			A	A	D	D	D							
Approach Delay, s/veh / LOS				6.9		A		2.5		A		44.2		D		0.0			
Intersection Delay, s/veh / LOS				13.4						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency	Civil Transformations Inc.					Duration, h	0.250												
Analyst	CTI	Analysis Date	Aug 16, 2023			Area Type	Other												
Jurisdiction	NMDOT & COA		Time Period	PM Peak		PHF	0.93												
Urban Street	San Mateo		Analysis Year	2023		Analysis Period	1> 16:30												
Intersection	PanAm		File Name	SM-I25_2023PMX-Node3.xus															
Project Description	2023 Baseline																		
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				24	7	8	107	3	146	4	2138	97	64	2049	10				
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	58	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On			Green	4.5	83.9	16.1	0.0	0.0	0.0							
Force Mode	Fixed	Simult. Gap N/S	On			Yellow	4.0	4.0	4.5	0.0	0.0	0.0							
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						21.6				21.6				88.9		9.5		98.4	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.2				4.2				0.0		4.0		0.0	
Queue Clearance Time (g s), s						4.4				11.7						3.2			
Green Extension Time (g e), s						0.8				0.5				0.0		0.1		0.0	
Phase Call Probability						0.71				1.00						0.90			
Max Out Probability						0.00				0.16						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					38			118	106	4	1606	793	69	1477	737				
Adjusted Saturation Flow Rate (s), veh/h/ln					1529			1383	1574	174	1870	1828	1781	1870	1865				
Queue Service Time (g s), s					0.0			7.3	7.5	1.1	27.2	27.6	1.2	17.3	17.4				
Cycle Queue Clearance Time (g c), s					2.4			9.7	7.5	9.0	27.2	27.6	1.2	17.3	17.4				
Green Ratio (g/C)					0.13			0.13	0.13	0.70	0.70	0.70	0.75	0.78	0.78				
Capacity (c), veh/h					256			245	211	171	2616	1279	195	2912	1452				
Volume-to-Capacity Ratio (X)					0.147			0.484	0.504	0.025	0.614	0.620	0.353	0.507	0.508				
Back of Queue (Q), ft/ln (95 th percentile)					46			160	142	2	380	391	31	218	226				
Back of Queue (Q), veh/ln (95 th percentile)					1.8			6.3	5.6	0.1	14.9	15.4	1.2	8.6	8.9				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	0.94	0.02	0.00	0.00	0.31	0.00	0.00				
Uniform Delay (d 1), s/veh					46.0			49.2	48.3	8.3	9.5	9.6	10.7	4.9	4.9				
Incremental Delay (d 2), s/veh					0.1			1.5	1.9	0.3	1.1	2.3	1.1	0.6	1.3				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					46.1			50.7	50.1	8.5	10.6	11.8	11.8	5.5	6.1				
Level of Service (LOS)					D			D	D	A	B	B	B	A	A				
Approach Delay, s/veh / LOS				46.1	D		50.4	D		11.0	B		5.9	A					
Intersection Delay, s/veh / LOS				10.7									B						
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, PM Peak
2023 NO-Build, Timing Plan: 15

Intersection						
Int Delay, s/veh	2.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↰	↰ ↰ ↰ ↰	↰ ↰ ↰ ↰		↰ ↰ ↰ ↰	↰ ↰ ↰ ↰
Traffic Vol, veh/h	1	19	2291	14	30	2330
Future Vol, veh/h	1	19	2291	14	30	2330
Conflicting Peds, #/hr	0	1	0	4	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	25	68	92	88	68	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	28	2490	16	44	2589
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	3626	1258	0	0	2510	0
Stage 1	2502	-	-	-	-	-
Stage 2	1124	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*14	139	-	-	68	-
Stage 1	*27	-	-	-	-	-
Stage 2	*604	-	-	-	-	-
Platoon blocked, %	0	-	-	-	-	-
Mov Cap-1 Maneuver	*5	139	-	-	68	-
Mov Cap-2 Maneuver	*5	-	-	-	-	-
Stage 1	*27	-	-	-	-	-
Stage 2	*213	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s/veh	84.34	0	2.11			
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	5	139	68	-
HCM Lane V/C Ratio	-	-	0.811	0.202	0.647	-
HCM Control Delay (s/veh)	-	\$	1210.5	37.4	125.7	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	1.2	0.7	2.8	-
Notes						
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined		*: All major volume in platoon		

Intersection												
Int Delay, s/veh	19.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	28	0	0	89	18	2067	23	132	2154	4
Future Vol, veh/h	0	0	28	0	0	89	18	2067	23	132	2154	4
Conflicting Peds, #/hr	0	0	2	0	0	0	0	0	1	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	70	100	100	97	64	90	82	80	92	50
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	40	0	0	92	28	2297	28	165	2341	8





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3652	5059	1179	3636	5049	1163	2351	0	0	2326	0	0
Stage 1	2677	2677	-	2368	2368	-	-	-	-	-	-	-
Stage 2	975	2382	-	1269	2681	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	7	0	*576	*8	0	161	226	-	-	~ 85	-	-
Stage 1	48	103	-	*21	67	-	-	-	-	-	-	-
Stage 2	244	66	-	*591	103	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	3	0	*574	*6	0	161	226	-	-	~ 85	-	-
Mov Cap-2 Maneuver	3	0	-	*6	0	-	-	-	-	-	-	-
Stage 1	47	0	-	*18	58	-	-	-	-	-	-	-
Stage 2	92	57	-	*550	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.74		53.43	0.28	35.69
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	226	-	-	-	574	-	161	~ 85	-	-
HCM Lane V/C Ratio	0.125	-	-	-	0.07	-	0.57	1.938	-	-
HCM Control Delay (s/veh)	23.2	-	-	0	11.7	0	53.4	543.9	-	-
HCM Lane LOS	C	-	-	A	B	A	F	F	-	-
HCM 95th %tile Q(veh)	0.4	-	-	-	0.2	-	3	14.3	-	-

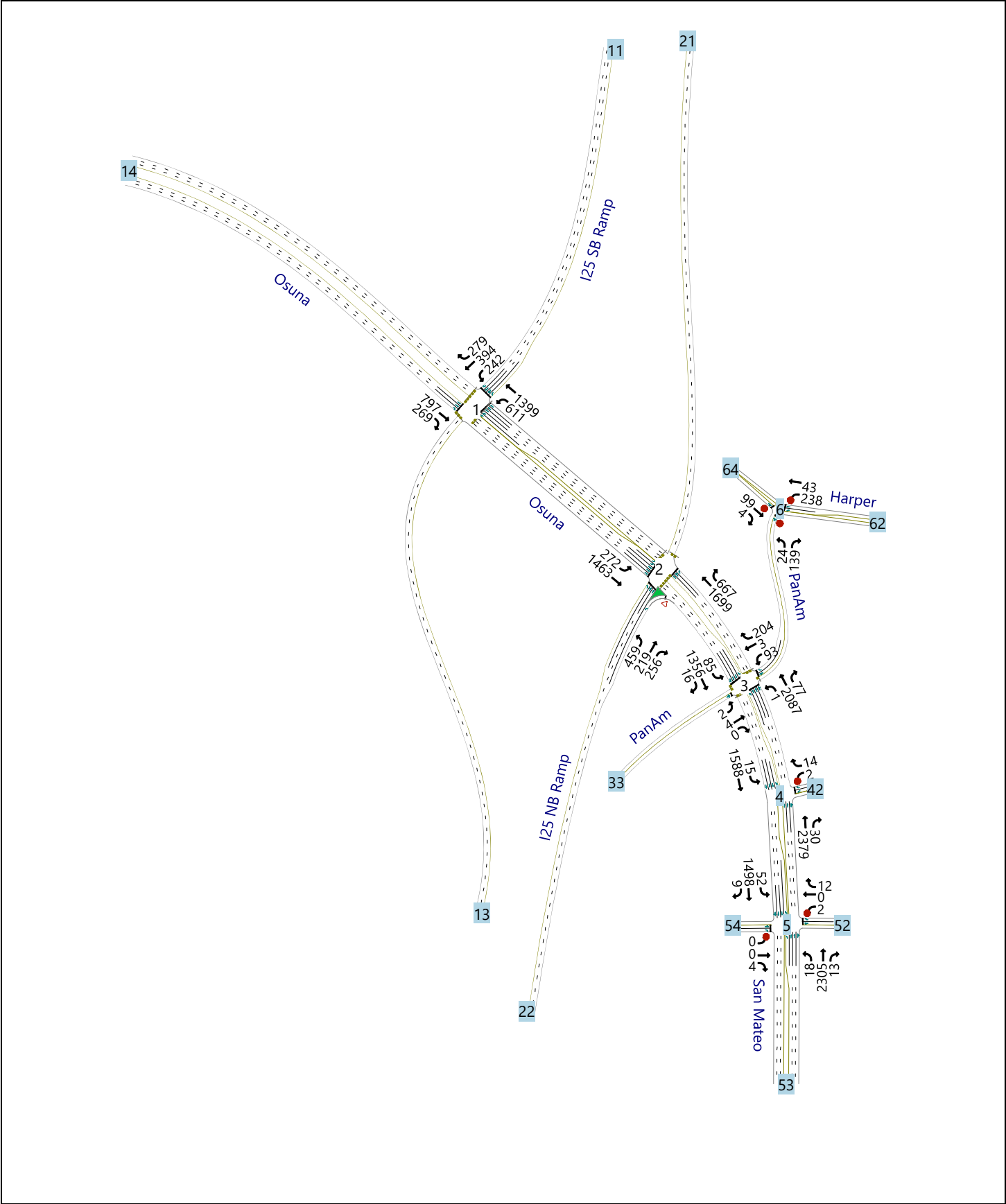
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon							

Intersection	
Intersection Delay, s/veh	11.8
Intersection LOS	B

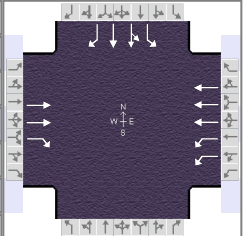
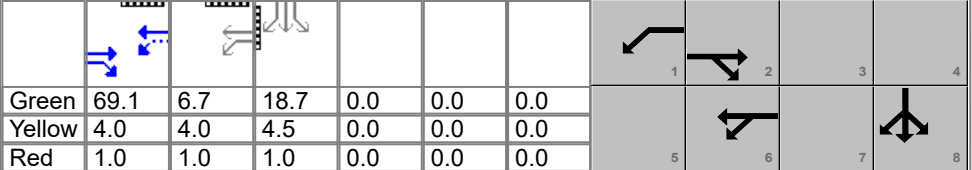
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	145	3	267	56	17	133
Future Vol, veh/h	145	3	267	56	17	133
Peak Hour Factor	0.82	0.75	0.77	0.64	0.71	0.92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	177	4	347	88	24	145
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	9.7	13.6	9.4
HCM LOS	A	B	A

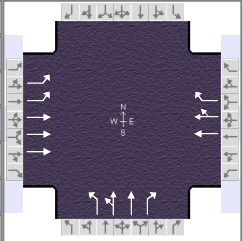
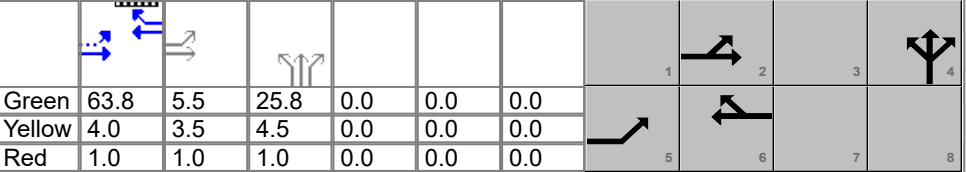
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	11%	0%	100%	0%
Vol Thru, %	0%	98%	0%	100%
Vol Right, %	89%	2%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	150	148	267	56
LT Vol	17	0	267	0
Through Vol	0	145	0	56
RT Vol	133	3	0	0
Lane Flow Rate	169	181	347	88
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.23	0.249	0.54	0.124
Departure Headway (Hd)	4.904	4.962	5.606	5.103
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	728	718	641	698
Service Time	2.957	3.03	3.37	2.866
HCM Lane V/C Ratio	0.232	0.252	0.541	0.126
HCM Control Delay, s/veh	9.4	9.7	14.8	8.6
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.9	1	3.2	0.4



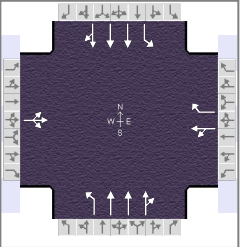
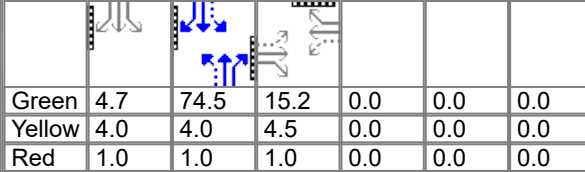
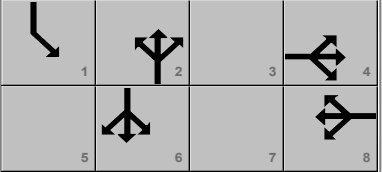
HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92					
Urban Street		San Mateo		Analysis Year		2025		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2025AMX.xus									
Project Description		Implementation NO-Build													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					797	269	611	1399					242	394	279
Signal Information															
Cycle, s	110.0	Reference Phase	2												
Offset, s	56	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	69.1	6.7	18.7	0.0	0.0	0.0					
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				8				
Case Number					7.4	1.0	4.0				9.0				
Phase Duration, s					74.1	11.7	85.8				24.2				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1				
Queue Clearance Time (g s), s						2.0					16.9				
Green Extension Time (g e), s					0.0	4.7	0.0				1.8				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.01					0.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					3	8	18
Adjusted Flow Rate (v), veh/h					866	221	693	1586					184	507	142
Adjusted Saturation Flow Rate (s), veh/h/ln					1847	1528	1512	1713					1781	1807	1511
Queue Service Time (g s), s					0.0	0.0	0.0	4.0					10.5	14.9	9.5
Cycle Queue Clearance Time (g c), s					0.0	0.0	0.0	4.0					10.5	14.9	9.5
Green Ratio (g/C)					0.63	0.63	0.69	0.73					0.17	0.17	0.17
Capacity (c), veh/h					2320	959	1118	3774					303	614	257
Volume-to-Capacity Ratio (X)					0.373	0.230	0.620	0.420					0.608	0.825	0.554
Back of Queue (Q), ft/ln (95 th percentile)					7	7	170	34					207	276	162
Back of Queue (Q), veh/ln (95 th percentile)					0.3	0.3	6.7	1.3					8.2	10.9	6.4
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.52	0.00					1.04	0.00	0.72
Uniform Delay (d 1), s/veh					0.0	0.0	12.0	1.2					42.3	44.1	41.8
Incremental Delay (d 2), s/veh					0.5	0.6	0.1	0.1					0.7	1.1	0.7
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					0.5	0.6	12.1	1.3					43.0	45.2	42.5
Level of Service (LOS)					A	A	B	A					D	D	D
Approach Delay, s/veh / LOS				0.5	A	4.6	A	0.0				44.2	D		
Intersection Delay, s/veh / LOS				11.4						B					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary









General Information					Intersection Information														
Agency		Civil Transformations Inc.					Duration, h		0.250										
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type						Other					
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF						0.89					
Urban Street		San Mateo		Analysis Year		2025		Analysis Period						1> 7:15					
Intersection		I25 NB Ramp		File Name		SM-I25_2025AMX.xus													
Project Description		Implementation NO-Build																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				272	1463			1699	667	459	219	256							
Signal Information																			
Cycle, s	110.0	Reference Phase	2																
Offset, s	51	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	63.8	5.5	25.8	0.0	0.0	0.0									
				Yellow	4.0	3.5	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase				5		2				6				4					
Case Number				1.0		4.0				7.4				9.0					
Phase Duration, s				10.0		78.7				68.8				31.3					
Change Period, (Y+R c), s				5.0		5.0				5.0				5.5					
Max Allow Headway (MAH), s				4.0		0.0				0.0				3.1					
Queue Clearance Time (g s), s				2.0										23.6					
Green Extension Time (g e), s				1.6		0.0				0.0				2.1					
Phase Call Probability				1.00										1.00					
Max Out Probability				1.00										0.01					
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				5	2			6	16	7	4	14							
Adjusted Flow Rate (v), veh/h				177	952			2043	506	26	736	191							
Adjusted Saturation Flow Rate (s), veh/h/ln				1468	1660			1982	1592	1781	1802	1522							
Queue Service Time (g s), s				0.0	7.5			13.7	2.5	1.2	21.6	12.1							
Cycle Queue Clearance Time (g c), s				0.0	7.5			13.7	2.5	1.2	21.6	12.1							
Green Ratio (g/C)				0.62	0.67			0.58	0.58	0.23	0.23	0.23							
Capacity (c), veh/h				478	3339			2297	922	417	844	356							
Volume-to-Capacity Ratio (X)				0.371	0.285			0.890	0.548	0.062	0.872	0.536							
Back of Queue (Q), ft/ln (95 th percentile)				74	99			119	45	24	377	202							
Back of Queue (Q), veh/ln (95 th percentile)				2.9	3.9			4.7	1.8	1.0	14.9	7.9							
Queue Storage Ratio (RQ) (95 th percentile)				0.30	0.00			0.00	0.00	0.08	0.00	0.58							
Uniform Delay (d 1), s/veh				21.2	6.2			1.0	0.8	32.7	40.5	36.9							
Incremental Delay (d 2), s/veh				0.4	0.2			5.7	2.3	0.0	3.0	0.5							
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0							
Control Delay (d), s/veh				21.6	6.3			6.7	3.2	32.7	43.6	37.3							
Level of Service (LOS)				C	A			A	A	C	D	D							
Approach Delay, s/veh / LOS				8.7		A		6.0		A		42.0		D		0.0			
Intersection Delay, s/veh / LOS				14.1								B							
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.87									
Urban Street		San Mateo		Analysis Year		2025		Analysis Period		1> 7:15									
Intersection		PanAm		File Name		SM-I25_2025AMX-Node3.xus													
Project Description		Implementation NO-Build																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				2	4	0	93	3	204	1	2087	77	85	1356	16				
Signal Information																			
Cycle, s	110.0	Reference Phase	2																
Offset, s	47	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
Green	4.7	74.5	15.2	0.0	0.0	0.0													
Yellow	4.0	4.0	4.5	0.0	0.0	0.0													
Red	1.0	1.0	1.0	0.0	0.0	0.0													
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						4				8				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						20.7				20.7				79.5		9.7		89.3	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.4				4.4				0.0		2.5		0.0	
Queue Clearance Time (g s), s						2.4				13.3						3.7			
Green Extension Time (g e), s						1.2				1.1				0.0		0.1		0.0	
Phase Call Probability						0.19				1.00						0.95			
Max Out Probability						0.00				0.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				7	4	14	3	8	18	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					7			110	169	1	1664	824	98	1049	522				
Adjusted Saturation Flow Rate (s), veh/h/ln					1734			1414	1585	327	1870	1835	1781	1870	1862				
Queue Service Time (g s), s					0.0			7.7	11.3	0.1	28.4	28.9	1.7	10.0	10.0				
Cycle Queue Clearance Time (g c), s					0.4			8.0	11.3	0.4	28.4	28.9	1.7	10.0	10.0				
Green Ratio (g/C)					0.14			0.14	0.14	0.68	0.68	0.68	0.74	0.77	0.77				
Capacity (c), veh/h					284			260	220	286	2534	1243	197	2865	1426				
Volume-to-Capacity Ratio (X)					0.024			0.424	0.769	0.004	0.657	0.663	0.495	0.366	0.366				
Back of Queue (Q), ft/ln (95 th percentile)					8			133	214	0	394	408	81	126	133				
Back of Queue (Q), veh/ln (95 th percentile)					0.3			5.2	8.4	0.0	15.5	16.1	3.2	5.0	5.2				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	0.71	0.00	0.00	0.00	0.81	0.00	0.00				
Uniform Delay (d 1), s/veh					41.0			44.3	45.7	5.8	10.3	10.4	17.7	4.2	4.2				
Incremental Delay (d 2), s/veh					0.0			1.1	5.6	0.0	1.3	2.8	0.7	0.4	0.7				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					41.0			45.4	51.3	5.9	11.7	13.2	18.4	4.5	4.9				
Level of Service (LOS)					D			D	D	A	B	B	B	A	A				
Approach Delay, s/veh / LOS				41.0		D		48.9		D		12.2		B		5.5		A	
Intersection Delay, s/veh / LOS				12.0						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, AM Peak
2025 Implementation NO-Build, Timing Plan: 11

Intersection						
Int Delay, s/veh	0.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 			  
Traffic Vol, veh/h	2	14	2379	30	15	1588
Future Vol, veh/h	2	14	2379	30	15	1588
Conflicting Peds, #/hr	0	0	0	2	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	15	2586	33	16	1726
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	3327	1311	0	0	2620	0
Stage 1	2604	-	-	-	-	-
Stage 2	723	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*21	128	-	-	60	-
Stage 1	*23	-	-	-	-	-
Stage 2	*720	-	-	-	-	-
Platoon blocked, %	0		-	-		-
Mov Cap-1 Maneuver	*15	128	-	-	60	-
Mov Cap-2 Maneuver	*15	-	-	-	-	-
Stage 1	*23	-	-	-	-	-
Stage 2	*524	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s/v67.03		0		0.81		
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	15	128	60	-
HCM Lane V/C Ratio	-	-	0.143	0.119	0.273	-
HCM Control Delay (s/veh)	-	-	277.7	36.9	86.4	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.4	0.4	1	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

HCM 7th TWSC
5: San Mateo & Garcia/FarNorth

SanMateo_I25 SE, AM Peak
2025 Implementation NO-Build, Timing Plan: 11

Intersection												
Int Delay, s/veh	6.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	4	2	0	12	18	2305	13	52	1498	9
Future Vol, veh/h	0	0	4	2	0	12	18	2305	13	52	1498	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	2	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	4	2	0	13	20	2561	14	58	1664	10





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2851	4405	839	3392	4402	1290	1676	0	0	2578	0	0
Stage 1	1787	1787	-	2610	2610	-	-	-	-	-	-	-
Stage 2	1064	2618	-	781	1792	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	31	1	*690	*11	1	132	392	-	-	63	-	-
Stage 1	179	271	-	*14	50	-	-	-	-	-	-	-
Stage 2	214	49	-	*708	269	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	-	-	-
Mov Cap-1 Maneuver	2	0	*688	*~ 1	0	132	391	-	-	63	-	-
Mov Cap-2 Maneuver	2	0	-	*~ 1	0	-	-	-	-	-	-	-
Stage 1	15	22	-	*13	47	-	-	-	-	-	-	-
Stage 2	183	47	-	*58	22	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.26		\$ 1045.21	0.11	6.62
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	391	-	-	-	688	1	132	63	-	-
HCM Lane V/C Ratio	0.051	-	-	-	0.006	2.702	0.101	0.917	-	-
HCM Control Delay (s/veh)	14.7	-	-	0	10.5	7104.9	35.3	198.3	-	-
HCM Lane LOS	B	-	-	A	B	F	E	F	-	-
HCM 95th %tile Q(veh)	0.2	-	-	-	0	1	0.3	4.3	-	-

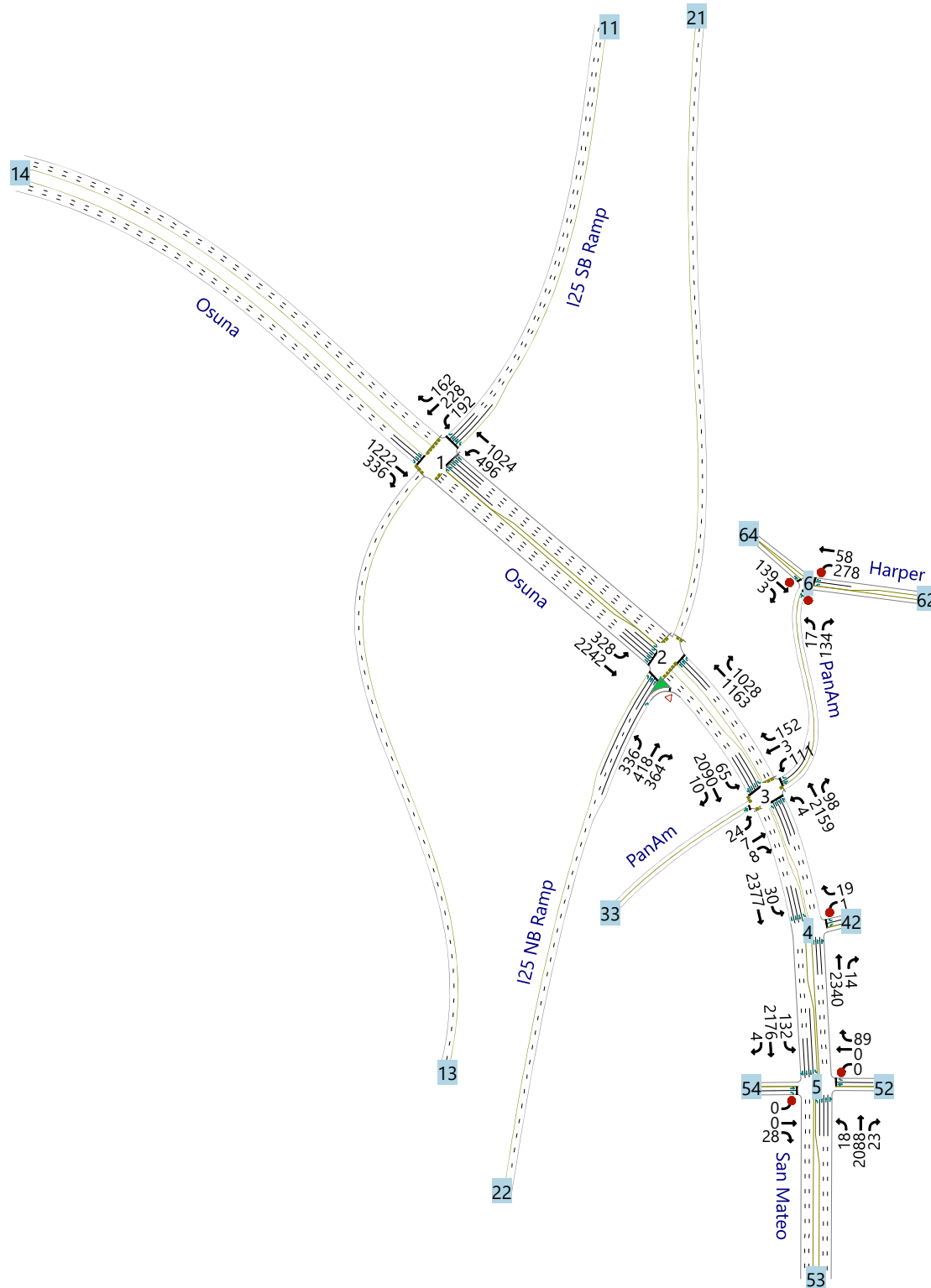
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined						*: All major volume in platoon				

Intersection	
Intersection Delay, s/veh	10.2
Intersection LOS	B

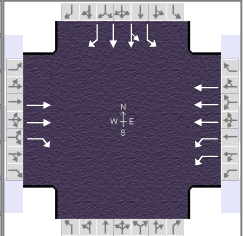
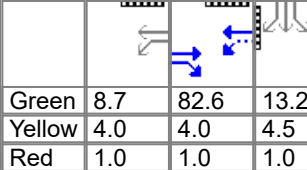
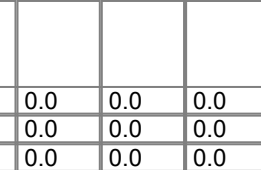
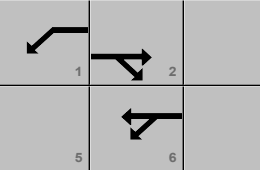

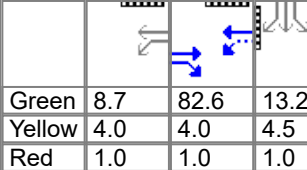
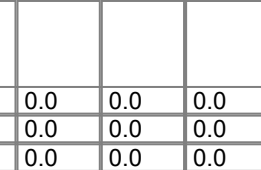
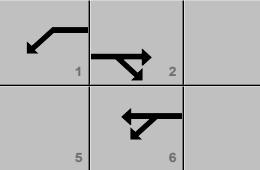

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	99	4	238	43	24	139
Future Vol, veh/h	99	4	238	43	24	139
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	110	4	264	48	27	154
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	8.8	11.5	8.9
HCM LOS	A	B	A

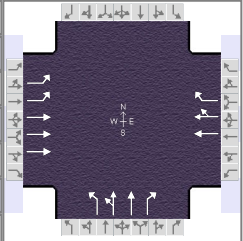
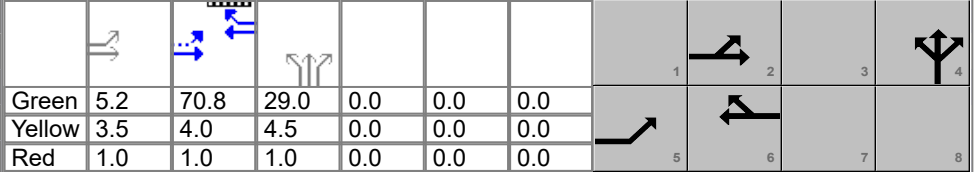
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	15%	0%	100%	0%
Vol Thru, %	0%	96%	0%	100%
Vol Right, %	85%	4%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	163	103	238	43
LT Vol	24	0	238	0
Through Vol	0	99	0	43
RT Vol	139	4	0	0
Lane Flow Rate	181	114	264	48
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.228	0.153	0.408	0.067
Departure Headway (Hd)	4.531	4.823	5.549	5.047
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	791	739	648	708
Service Time	2.563	2.877	3.297	2.794
HCM Lane V/C Ratio	0.229	0.154	0.407	0.068
HCM Control Delay, s/veh	8.9	8.8	12.1	8.2
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.9	0.5	2	0.2



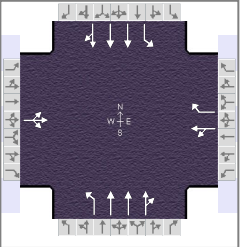
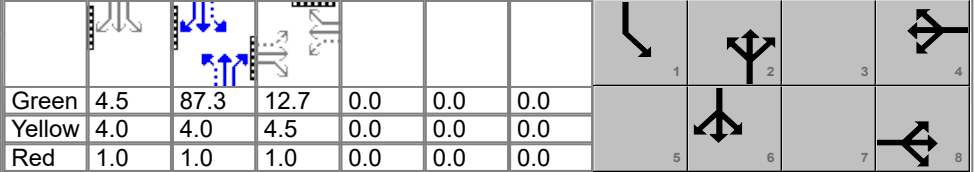
HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95					
Urban Street		Osuna		Analysis Year		2025		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2025PMX.xus									
Project Description		Implementation NO-Build													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1222	336	496	1024					192	228	162
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	61	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green	8.7	82.6	13.2	0.0	0.0	0.0									
Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				4				
Case Number					7.3	1.0	4.0				9.0				
Phase Duration, s					87.6	13.7	101.3				18.7				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1				
Queue Clearance Time (g s), s						7.9					12.9				
Green Extension Time (g e), s					0.0	0.8	0.0				0.3				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.00					1.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					7	4	14
Adjusted Flow Rate (v), veh/h					1286	291	510	1052					111	331	79
Adjusted Saturation Flow Rate (s), veh/h/ln					1906	1550	1381	1676					1781	1790	1499
Queue Service Time (g s), s					0.0	0.0	5.9	10.5					7.1	10.9	5.9
Cycle Queue Clearance Time (g c), s					0.0	0.0	5.9	10.5					7.1	10.9	5.9
Green Ratio (g/C)					0.69	0.69	0.76	0.70					0.11	0.11	0.11
Capacity (c), veh/h					2625	1067	911	3499					196	394	165
Volume-to-Capacity Ratio (X)					0.490	0.272	0.559	0.301					0.567	0.840	0.479
Back of Queue (Q), ft/ln (95 th percentile)					11	8	75	160					146	235	103
Back of Queue (Q), veh/ln (95 th percentile)					0.4	0.3	3.0	6.3					5.8	9.3	4.0
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.47	0.00					0.97	0.00	0.46
Uniform Delay (d 1), s/veh					0.0	0.0	4.4	5.7					50.7	52.4	50.2
Incremental Delay (d 2), s/veh					0.7	0.6	0.2	0.2					1.0	10.9	0.8
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					0.7	0.6	4.5	5.9					51.6	63.2	51.0
Level of Service (LOS)					A	A	A	A					D	E	D
Approach Delay, s/veh / LOS				0.7	A		5.4	A		0.0			58.9	E	
Intersection Delay, s/veh / LOS				11.0						B					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary










General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.96					
Urban Street		Osuna		Analysis Year		2025		Analysis Period		1> 7:15					
Intersection		I25 NB Ramp		File Name		SM-I25_2025PMX.xus									
Project Description		Implementation NO-Build													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				328	2242			1163	1028	336	418	364			
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	55	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	5.2	70.8	29.0	0.0	0.0	0.0					
				Yellow	3.5	4.0	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase				5	2		6		4						
Case Number				1.0	4.0		7.3		9.0						
Phase Duration, s				9.7	85.5		75.8		34.5						
Change Period, (Y+R c), s				4.5	5.0		5.0		5.5						
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1						
Queue Clearance Time (g s), s				4.5					26.5						
Green Extension Time (g e), s				0.6	0.0		0.0		2.5						
Phase Call Probability				1.00					1.00						
Max Out Probability				0.00					0.01						
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				189	1293			1536	659	165	621	330			
Adjusted Saturation Flow Rate (s), veh/h/ln				1718	1698			1821	1645	1781	1818	1559			
Queue Service Time (g s), s				2.5	13.5			2.7	2.3	9.3	18.7	24.5			
Cycle Queue Clearance Time (g c), s				2.5	13.5			2.7	2.3	9.3	18.7	24.5			
Green Ratio (g/C)				0.63	0.67			0.59	0.59	0.24	0.24	0.24			
Capacity (c), veh/h				791	3418			2150	971	430	879	377			
Volume-to-Capacity Ratio (X)				0.239	0.378			0.714	0.678	0.382	0.707	0.877			
Back of Queue (Q), ft/ln (95 th percentile)				42	190			42	59	185	333	386			
Back of Queue (Q), veh/ln (95 th percentile)				1.6	7.5			1.7	2.3	7.3	13.1	15.2			
Queue Storage Ratio (RQ) (95 th percentile)				0.56	0.00			0.00	0.00	0.62	0.00	1.10			
Uniform Delay (d 1), s/veh				8.9	8.8			0.4	0.4	38.0	41.6	43.8			
Incremental Delay (d 2), s/veh				0.1	0.3			2.1	3.8	0.2	0.4	6.7			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				9.1	9.0			2.5	4.2	38.2	42.0	50.5			
Level of Service (LOS)				A	A			A	A	D	D	D			
Approach Delay, s/veh / LOS				9.0	A		3.0	A	44.0	D		0.0			
Intersection Delay, s/veh / LOS				14.4						B					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.93									
Urban Street		San Mateo		Analysis Year		2025		Analysis Period		1> 7:00									
Intersection		PanAm		File Name		SM-I25_2025PMX-Node3.xus													
Project Description		Implementation NO-Build																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				24	7	8	111	3	152	4	2159	98	65	2090	10				
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	58	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
Green	4.5	87.3	12.7	0.0	0.0	0.0													
Yellow	4.0	4.0	4.5	0.0	0.0	0.0													
Red	1.0	1.0	1.0	0.0	0.0	0.0													
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						18.2				18.2				92.3		9.5		101.8	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.2				4.2				0.0		4.0		0.0	
Queue Clearance Time (g s), s						4.5				12.2						3.1			
Green Extension Time (g e), s						0.8				0.5				0.0		0.1		0.0	
Phase Call Probability						0.71				1.00						0.90			
Max Out Probability						0.00				0.22						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					38			123	113	4	1622	801	70	1506	752				
Adjusted Saturation Flow Rate (s), veh/h/ln					1548			1412	1585	167	1870	1829	1781	1870	1866				
Queue Service Time (g s), s					0.0			7.7	8.2	1.0	25.1	25.5	1.1	15.7	15.7				
Cycle Queue Clearance Time (g c), s					2.5			10.2	8.2	7.2	25.1	25.5	1.1	15.7	15.7				
Green Ratio (g/C)					0.11			0.11	0.11	0.73	0.73	0.73	0.78	0.81	0.81				
Capacity (c), veh/h					215			209	168	173	2720	1330	200	3017	1505				
Volume-to-Capacity Ratio (X)					0.175			0.587	0.672	0.025	0.596	0.602	0.350	0.499	0.500				
Back of Queue (Q), ft/ln (95 th percentile)					48			173	160	2	341	352	28	179	190				
Back of Queue (Q), veh/ln (95 th percentile)					1.9			6.8	6.3	0.1	13.4	13.9	1.1	7.0	7.5				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	1.07	0.02	0.00	0.00	0.28	0.00	0.00				
Uniform Delay (d 1), s/veh					49.0			52.5	51.6	6.5	7.9	7.9	9.2	3.8	3.8				
Incremental Delay (d 2), s/veh					0.1			2.6	4.6	0.3	1.0	2.0	1.0	0.6	1.2				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					49.2			55.1	56.2	6.7	8.9	10.0	10.2	4.4	5.0				
Level of Service (LOS)					D			E	E	A	A	A	B	A	A				
Approach Delay, s/veh / LOS				49.2	D		55.6	E		9.2	A		4.7		A				
Intersection Delay, s/veh / LOS				9.6						A									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, PM Peak
2025 Implementation NO-Build, Timing Plan: 15

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			  			  
Traffic Vol, veh/h	1	19	2340	14	30	2377
Future Vol, veh/h	1	19	2340	14	30	2377
Conflicting Peds, #/hr	0	1	0	4	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	20	2489	15	32	2529
Major/Minor	Minor1		Major1		Major2	
Conflicting Flow All	3576	1257	0	0	2508	0
Stage 1	2501	-	-	-	-	-
Stage 2	1075	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*16	139	-	-	69	-
Stage 1	*27	-	-	-	-	-
Stage 2	*604	-	-	-	-	-
Platoon blocked, %	0		-	-	-	-
Mov Cap-1 Maneuver	*8	139	-	-	68	-
Mov Cap-2 Maneuver	*8	-	-	-	-	-
Stage 1	*27	-	-	-	-	-
Stage 2	*322	-	-	-	-	-
Approach	WB		NB		SB	
HCM Control Delay, s/v58.45			0		1.21	
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	8	139	68	-
HCM Lane V/C Ratio	-	-	0.129	0.146	0.467	-
HCM Control Delay (s/veh)	-	-	\$ 497.9	35.3	97.4	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.3	0.5	1.9	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

HCM 7th TWSC
5: San Mateo & Garcia/FarNorth

SanMateo_I25 SE, PM Peak
2025 Implementation NO-Build, Timing Plan: 15

Intersection												
Int Delay, s/veh	13.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	28	0	0	89	18	2088	23	132	2176	4
Future Vol, veh/h	0	0	28	0	0	89	18	2088	23	132	2176	4
Conflicting Peds, #/hr	0	0	2	0	0	0	0	0	1	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	30	0	0	97	20	2270	25	143	2365	4





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3603	4991	1189	3557	4981	1148	2372	0	0	2296	0	0
Stage 1	2656	2656	-	2322	2322	-	-	-	-	-	-	-
Stage 2	947	2335	-	1235	2659	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	8	0	*576	*9	0	165	218	-	-	~ 88	-	-
Stage 1	50	107	-	*22	70	-	-	-	-	-	-	-
Stage 2	254	69	-	*591	107	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	3	0	*574	*8	0	165	218	-	-	~ 88	-	-
Mov Cap-2 Maneuver	3	0	-	*8	0	-	-	-	-	-	-	-
Stage 1	50	0	-	*20	64	-	-	-	-	-	-	-
Stage 2	95	63	-	*560	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v11.62		53.9	0.2	23.39
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	218	-	-	-	574	-	165	~ 88	-	-
HCM Lane V/C Ratio	0.09	-	-	-	0.053	-	0.587	1.626	-	-
HCM Control Delay (s/veh)	23.2	-	-	0	11.6	0	53.9	409.7	-	-
HCM Lane LOS	C	-	-	A	B	A	F	F	-	-
HCM 95th %tile Q(veh)	0.3	-	-	-	0.2	-	3.1	11.6	-	-

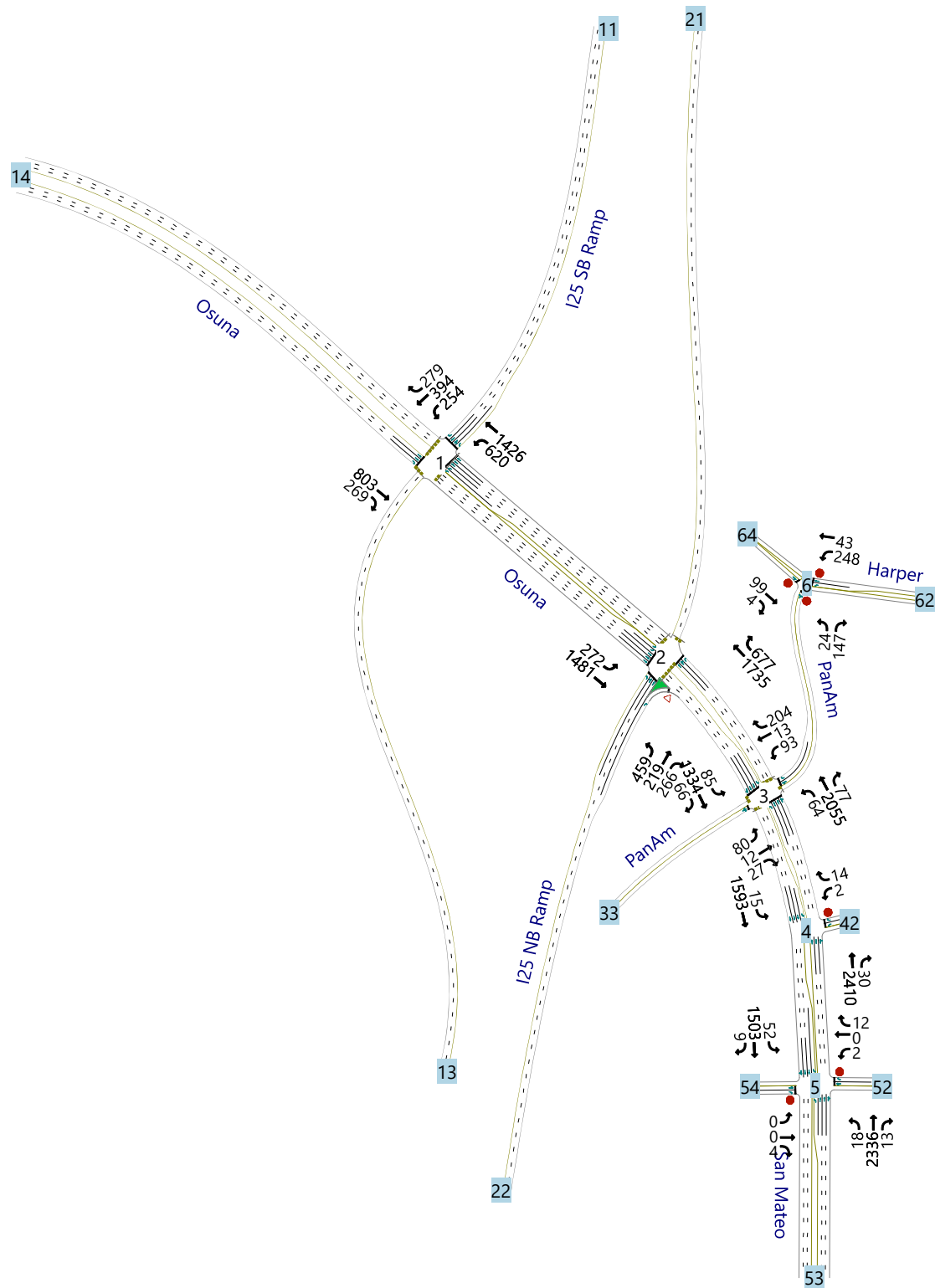
Notes												
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon						

Intersection	
Intersection Delay, s/veh	10.9
Intersection LOS	B

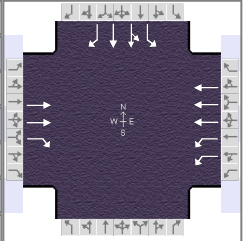
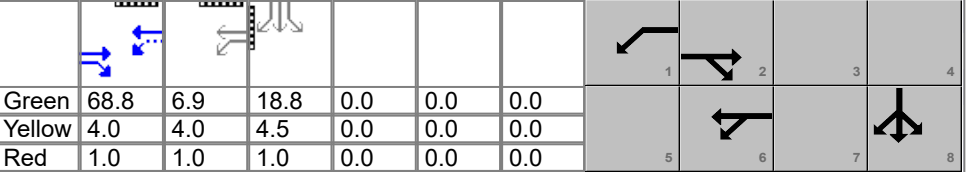
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	139	3	278	58	17	134
Future Vol, veh/h	139	3	278	58	17	134
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	153	3	305	64	19	147
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	9.2	12.4	9.1
HCM LOS	A	B	A

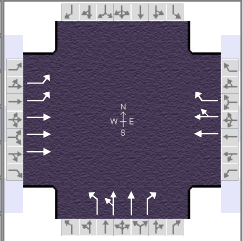
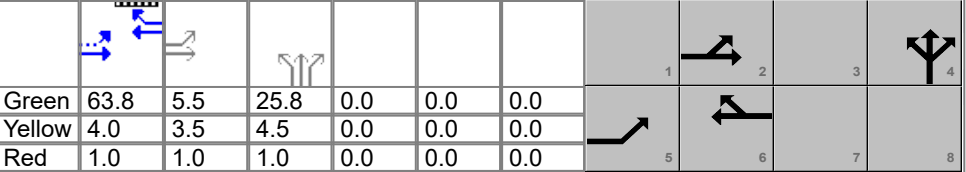
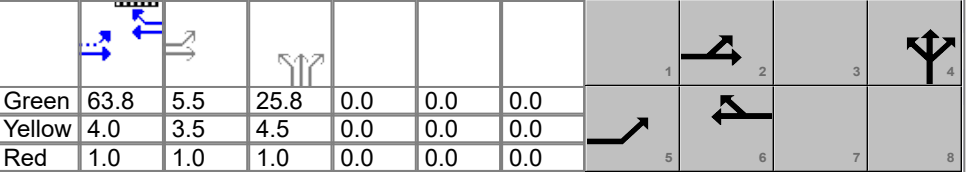
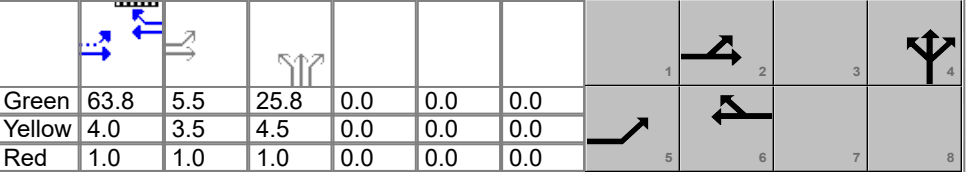
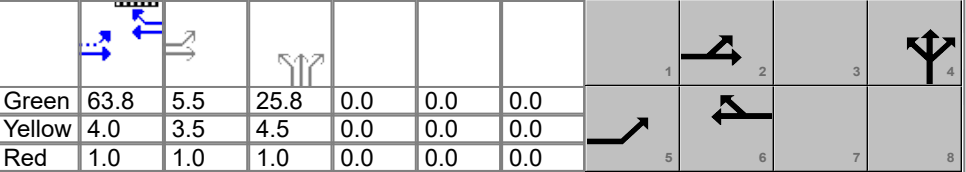
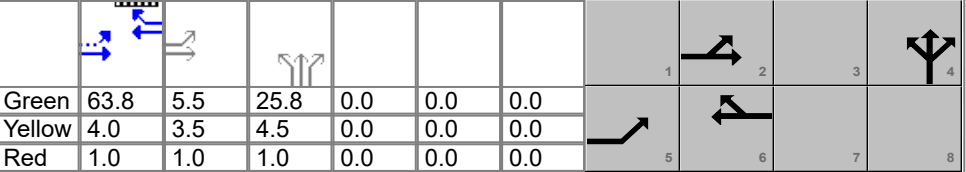
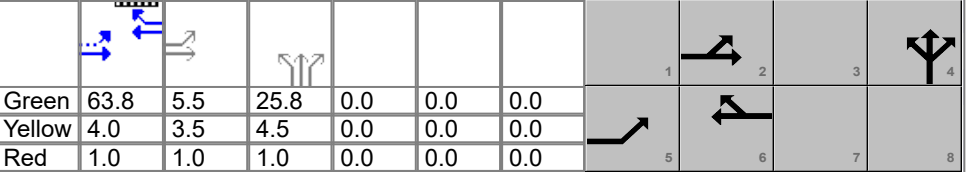
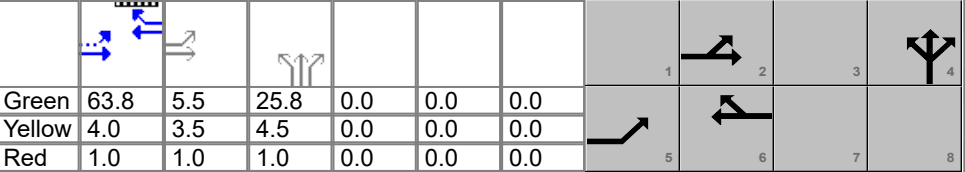
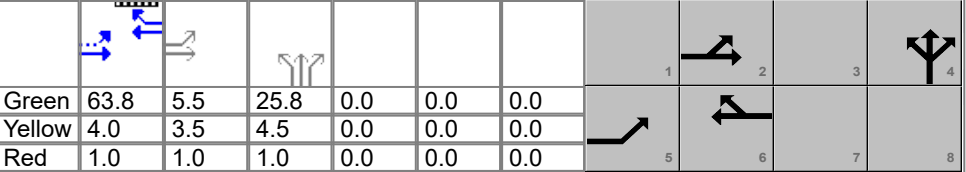
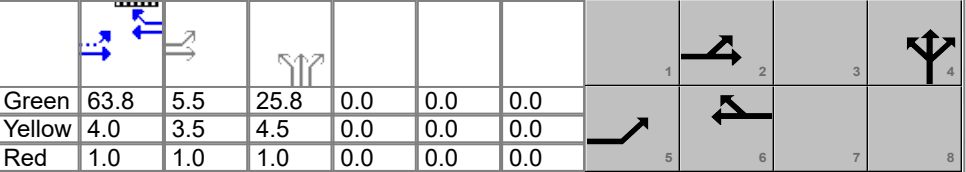
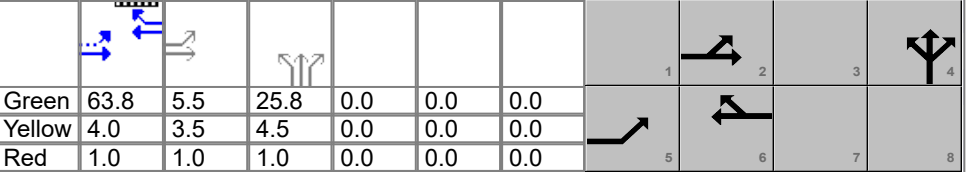
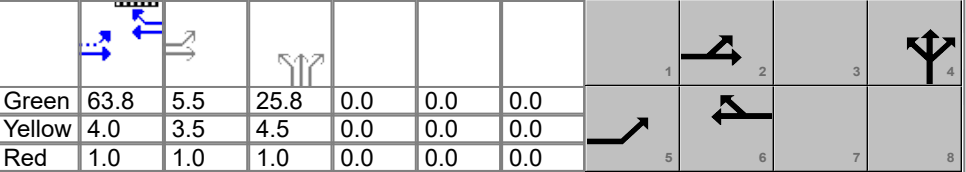
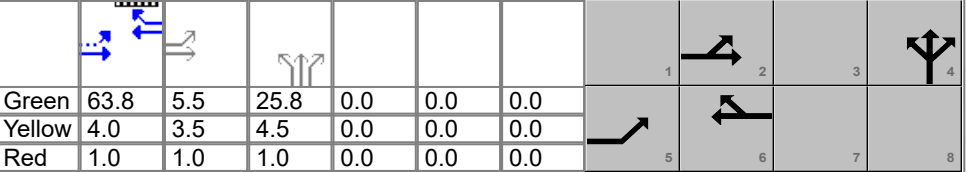
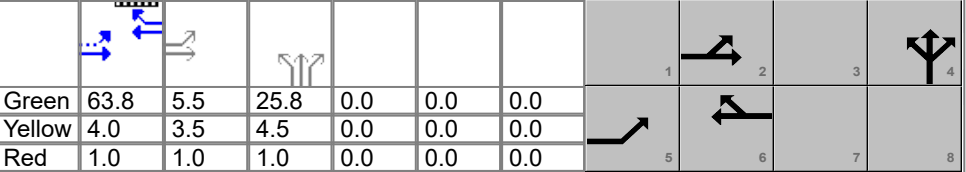
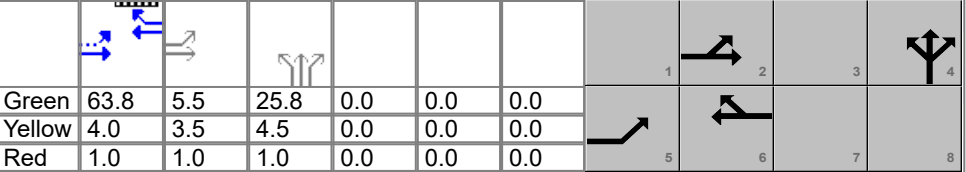
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	11%	0%	100%	0%
Vol Thru, %	0%	98%	0%	100%
Vol Right, %	89%	2%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	151	142	278	58
LT Vol	17	0	278	0
Through Vol	0	139	0	58
RT Vol	134	3	0	0
Lane Flow Rate	166	156	305	64
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.217	0.211	0.472	0.09
Departure Headway (Hd)	4.717	4.873	5.563	5.061
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	758	733	645	705
Service Time	2.759	2.929	3.315	2.812
HCM Lane V/C Ratio	0.219	0.213	0.473	0.091
HCM Control Delay, s/veh	9.1	9.2	13.2	8.3
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.8	0.8	2.5	0.3



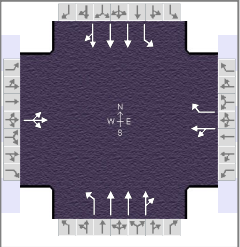
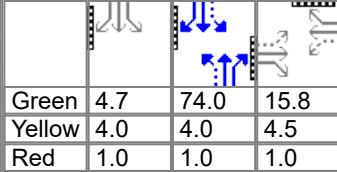
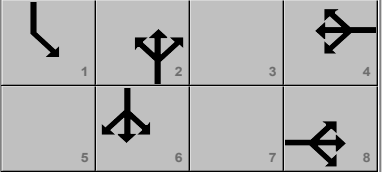
HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92					
Urban Street		San Mateo		Analysis Year		2025		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2025AMB.xus									
Project Description		Implementation BUILD													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					803	269	620	1429					254	394	279
Signal Information															
Cycle, s	110.0	Reference Phase	2												
Offset, s	56	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	68.8	6.9	18.8	0.0	0.0	0.0					
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				8				
Case Number					7.4	1.0	4.0				9.0				
Phase Duration, s					73.8	11.9	85.7				24.3				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1				
Queue Clearance Time (g s), s						2.0					17.0				
Green Extension Time (g e), s					0.0	4.9	0.0				1.8				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.02					0.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					3	8	18
Adjusted Flow Rate (v), veh/h					873	221	704	1622					193	511	142
Adjusted Saturation Flow Rate (s), veh/h/ln					1850	1528	1534	1716					1781	1807	1511
Queue Service Time (g s), s					0.0	0.0	0.0	3.6					11.1	15.0	9.5
Cycle Queue Clearance Time (g c), s					0.0	0.0	0.0	3.6					11.1	15.0	9.5
Green Ratio (g/C)					0.63	0.63	0.69	0.73					0.17	0.17	0.17
Capacity (c), veh/h					2314	956	1117	3775					305	619	259
Volume-to-Capacity Ratio (X)					0.377	0.231	0.630	0.430					0.634	0.826	0.550
Back of Queue (Q), ft/ln (95 th percentile)					7	7	180	30					216	278	162
Back of Queue (Q), veh/ln (95 th percentile)					0.3	0.3	7.1	1.2					8.5	10.9	6.4
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.55	0.00					1.08	0.00	0.72
Uniform Delay (d 1), s/veh					0.0	0.0	13.0	1.0					42.4	44.0	41.7
Incremental Delay (d 2), s/veh					0.5	0.6	0.1	0.1					0.8	1.1	0.7
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					0.5	0.6	13.0	1.1					43.2	45.1	42.4
Level of Service (LOS)					A	A	B	A					D	D	D
Approach Delay, s/veh / LOS				0.5	A		4.7	A		0.0			44.2	D	
Intersection Delay, s/veh / LOS				11.5						B					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary








General Information						Intersection Information															
Agency	Civil Transformations Inc.					Duration, h	0.250														
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other														
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.89														
Urban Street	San Mateo		Analysis Year	2025		Analysis Period	1> 7:15														
Intersection	I25 NB Ramp		File Name	SM-I25_2025AMB.xus																	
Project Description	Implementation BUILD																				
Demand Information						EB			WB			NB			SB						
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h						272	1481			1735	677	459	219	256							
Signal Information																					
Cycle, s	110.0	Reference Phase	2																		
Offset, s	51	Reference Point	Begin																		
Uncoordinated	No	Simult. Gap E/W	On																		
Force Mode	Fixed	Simult. Gap N/S	On																		
				Green	63.8	5.5	25.8	0.0	0.0	0.0											
				Yellow	4.0	3.5	4.5	0.0	0.0	0.0											
				Red	1.0	1.0	1.0	0.0	0.0	0.0											
Timer Results						EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						5		2				6				4					
Case Number						1.0		4.0				7.4				9.0					
Phase Duration, s						10.0		78.7				68.8				31.3					
Change Period, (Y+R c), s						5.0		5.0				5.0				5.5					
Max Allow Headway (MAH), s						4.0		0.0				0.0				3.1					
Queue Clearance Time (g s), s						2.0										23.6					
Green Extension Time (g e), s						1.6		0.0				0.0				2.1					
Phase Call Probability						1.00										1.00					
Max Out Probability						1.00										0.01					
Movement Group Results						EB			WB			NB			SB						
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement						5	2			6	16	7	4	14							
Adjusted Flow Rate (v), veh/h						178	971			2086	515	26	736	191							
Adjusted Saturation Flow Rate (s), veh/h/ln						1468	1662			1984	1594	1781	1802	1522							
Queue Service Time (g s), s						0.0	7.2			15.9	2.6	1.2	21.6	12.1							
Cycle Queue Clearance Time (g c), s						0.0	7.2			15.9	2.6	1.2	21.6	12.1							
Green Ratio (g/C)						0.62	0.67			0.58	0.58	0.23	0.23	0.23							
Capacity (c), veh/h						460	3342			2299	924	417	844	356							
Volume-to-Capacity Ratio (X)						0.387	0.290			0.907	0.557	0.062	0.872	0.536							
Back of Queue (Q), ft/ln (95 th percentile)						81	94			134	47	24	377	202							
Back of Queue (Q), veh/ln (95 th percentile)						3.2	3.7			5.3	1.8	1.0	14.9	7.9							
Queue Storage Ratio (RQ) (95 th percentile)						0.32	0.00			0.00	0.00	0.08	0.00	0.58							
Uniform Delay (d 1), s/veh						23.6	5.7			1.1	0.8	32.7	40.5	36.9							
Incremental Delay (d 2), s/veh						0.5	0.2			6.6	2.4	0.0	3.0	0.5							
Initial Queue Delay (d 3), s/veh						0.0	0.0			0.0	0.0	0.0	0.0	0.0							
Control Delay (d), s/veh						24.0	5.9			7.7	3.3	32.7	43.6	37.3							
Level of Service (LOS)						C	A			A	A	C	D	D							
Approach Delay, s/veh / LOS						8.7		A		6.8		A		42.0		D		0.0			
Intersection Delay, s/veh / LOS						14.4						B									
Multimodal Results						EB			WB			NB			SB						
Pedestrian LOS Score / LOS																					
Bicycle LOS Score / LOS																					

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.90									
Urban Street		San Mateo		Analysis Year		2025		Analysis Period		1> 7:00									
Intersection		PanAm		File Name		SM-I25_2025AMB-Node3.xus													
Project Description		Implementation BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				80	12	27	93	13	204	64	2055	77	85	1334	66				
Signal Information																			
Cycle, s	110.0	Reference Phase	2																
Offset, s	47	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
Green	4.7	74.0	15.8	0.0	0.0	0.0													
Yellow	4.0	4.0	4.5	0.0	0.0	0.0													
Red	1.0	1.0	1.0	0.0	0.0	0.0													
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						21.3				21.3				79.0		9.7		88.7	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.3				4.3				0.0		2.5		0.0	
Queue Clearance Time (g s), s						10.8				14.2						3.6			
Green Extension Time (g e), s						1.6				1.6				0.0		0.1		0.0	
Phase Call Probability						0.98				1.00						0.94			
Max Out Probability						0.00				0.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					132			118	163	71	1586	783	94	1046	510				
Adjusted Saturation Flow Rate (s), veh/h/ln					1520			1406	1427	332	1870	1834	1781	1870	1823				
Queue Service Time (g s), s					0.1			0.0	12.2	10.0	26.5	26.8	1.6	10.2	10.2				
Cycle Queue Clearance Time (g c), s					8.8			8.6	12.2	10.4	26.5	26.8	1.6	10.2	10.2				
Green Ratio (g/C)					0.14			0.14	0.14	0.67	0.67	0.67	0.73	0.76	0.76				
Capacity (c), veh/h					273			263	205	287	2517	1234	206	2847	1388				
Volume-to-Capacity Ratio (X)					0.485			0.448	0.798	0.248	0.630	0.635	0.458	0.367	0.367				
Back of Queue (Q), ft/ln (95 th percentile)					160			142	211	36	372	384	54	130	134				
Back of Queue (Q), veh/ln (95 th percentile)					6.3			5.6	8.3	1.4	14.7	15.1	2.1	5.1	5.3				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	1.41	0.30	0.00	0.00	0.54	0.00	0.00				
Uniform Delay (d 1), s/veh					44.0			44.0	45.6	7.7	10.2	10.3	14.4	4.4	4.4				
Incremental Delay (d 2), s/veh					1.3			1.2	7.0	2.1	1.2	2.5	0.6	0.4	0.8				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					45.4			45.2	52.6	9.8	11.4	12.8	15.0	4.7	5.1				
Level of Service (LOS)					D			D	D	A	B	B	B	A	A				
Approach Delay, s/veh / LOS				45.4	D		49.5	D		11.8	B		5.4		A				
Intersection Delay, s/veh / LOS				12.8						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, AM Peak
2025 Implementation BUILD, 11

Intersection						
Int Delay, s/veh	0.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 			 
Traffic Vol, veh/h	2	14	2379	30	15	1588
Future Vol, veh/h	2	14	2410	30	15	1593
Conflicting Peds, #/hr	0	0	0	2	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	15	2620	33	16	1732
Major/Minor	Minor1		Major1		Major2	
Conflicting Flow All	3363	1328	0	0	2654	0
Stage 1	2638	-	-	-	-	-
Stage 2	725	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*20	125	-	-	58	-
Stage 1	*22	-	-	-	-	-
Stage 2	*720	-	-	-	-	-
Platoon blocked, %	0		-	-	-	-
Mov Cap-1 Maneuver	*14	124	-	-	57	-
Mov Cap-2 Maneuver	*14	-	-	-	-	-
Stage 1	*22	-	-	-	-	-
Stage 2	*515	-	-	-	-	-
Approach	WB		NB		SB	
HCM Control Delay, s/v70.98			0		0.85	
HCM LOS	F					
Minor Lane/Major Mvmt		NBT	NBRWBLn1	WBLn2	SBL	SBT
Capacity (veh/h)		-	-	14 124	57	-
HCM Lane V/C Ratio		-	-	0.154 0.122	0.284	-
HCM Control Delay (s/veh)		-	-\$ 302.5	37.9	90.8	-
HCM Lane LOS		-	-	F E	F	-
HCM 95th %tile Q(veh)		-	-	0.4 0.4	1	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

Intersection												
Int Delay, s/veh	10.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	4	2	0	12	18	2305	13	52	1498	9
Future Vol, veh/h	0	0	4	2	0	12	18	2336	13	52	1503	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	2	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	4	2	0	13	20	2596	14	58	1670	10





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2871	4445	842	3428	4442	1307	1682	0	0	2612	0	0
Stage 1	1793	1793	-	2645	2645	-	-	-	-	-	-	-
Stage 2	1078	2652	-	784	1798	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	30	1	*690	*10	1	129	388	-	-	61	-	-
Stage 1	177	268	-	*13	48	-	-	-	-	-	-	-
Stage 2	210	47	-	*708	266	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	-	-	-
Mov Cap-1 Maneuver	1	0	*688	*~ 0	0	129	388	-	-	60	-	-
Mov Cap-2 Maneuver	1	0	-	*~ 0	0	-	-	-	-	-	-	-
Stage 1	8	12	-	*12	45	-	-	-	-	-	-	-
Stage 2	179	45	-	*31	12	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.26		\$ 2127.66	0.11	7.15
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	388	-	-	-	688	-	129	60	-	-
HCM Lane V/C Ratio	0.052	-	-	-	0.006	5.48	0.104	0.956	-	-
HCM Control Delay (s/veh)	14.8	-	-	0	10.3	1467.5	36.2	215.1	-	-
HCM Lane LOS	B	-	-	A	B	F	E	F	-	-
HCM 95th %tile Q(veh)	0.2	-	-	-	0	1	0.3	4.5	-	-

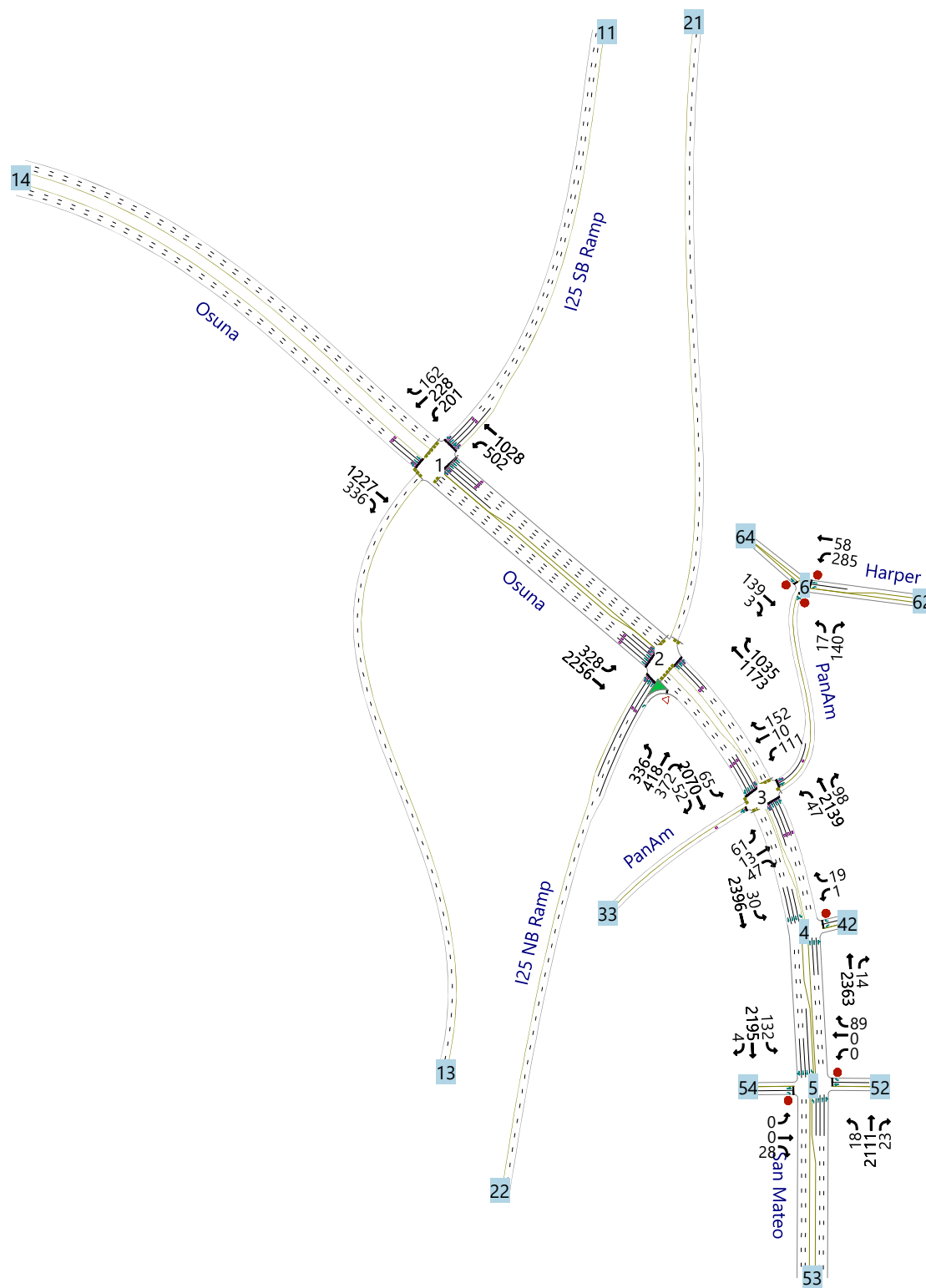
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined							*: All major volume in platoon			

Intersection	
Intersection Delay, s/veh	10.5
Intersection LOS	B

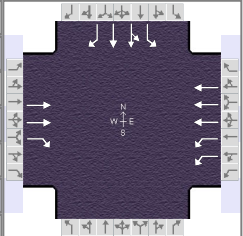
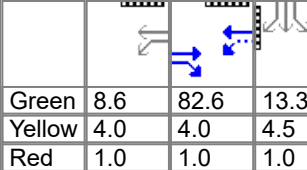
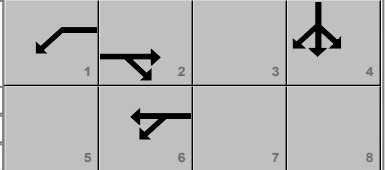
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	99	4	238	43	24	139
Future Vol, veh/h	99	4	248	43	24	147
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	110	4	276	48	27	163
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	8.8	11.9	9
HCM LOS	A	B	A

Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	14%	0%	100%	0%
Vol Thru, %	0%	96%	0%	100%
Vol Right, %	86%	4%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	171	103	248	43
LT Vol	24	0	248	0
Through Vol	0	99	0	43
RT Vol	147	4	0	0
Lane Flow Rate	190	114	276	48
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.241	0.155	0.427	0.067
Departure Headway (Hd)	4.559	4.863	5.573	5.07
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	787	734	645	703
Service Time	2.594	2.922	3.326	2.823
HCM Lane V/C Ratio	0.241	0.155	0.428	0.068
HCM Control Delay, s/veh	9	8.8	12.5	8.2
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.9	0.5	2.1	0.2

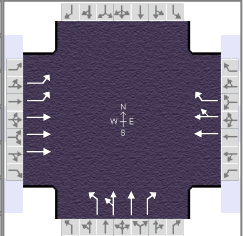


HCS Signalized Intersection Results Summary








General Information						Intersection Information											
Agency		Civil Transformations Inc.				Duration, h		0.250									
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other							
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95							
Urban Street		Osuna		Analysis Year		2025		Analysis Period		1> 7:15							
Intersection		I25 SB Ramp		File Name		SM-I25_2025PMB.xus											
Project Description		Implementation BUILD															
Demand Information				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Demand (v), veh/h					1227	336	502	1028					201	228	162		
Signal Information																	
Cycle, s	120.0	Reference Phase	2														
Offset, s	61	Reference Point	Begin														
Uncoordinated	No	Simult. Gap E/W	On														
Force Mode	Fixed	Simult. Gap N/S	On														
Green	8.6	82.6	13.3	0.0	0.0	0.0											
Yellow	4.0	4.0	4.5	0.0	0.0	0.0											
Red	1.0	1.0	1.0	0.0	0.0	0.0											
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT						
Assigned Phase					2	1	6				4						
Case Number					7.3	1.0	4.0				9.0						
Phase Duration, s					87.6	13.6	101.2				18.8						
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5						
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1						
Queue Clearance Time (g s), s						7.8					13.0						
Green Extension Time (g e), s					0.0	0.8	0.0				0.3						
Phase Call Probability						1.00					1.00						
Max Out Probability						0.00					1.00						
Movement Group Results				EB			WB			NB			SB				
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R		
Assigned Movement					2	12	1	6					7	4	14		
Adjusted Flow Rate (v), veh/h					1292	291	512	1049					116	335	79		
Adjusted Saturation Flow Rate (s), veh/h/ln					1907	1550	1397	1676					1781	1790	1499		
Queue Service Time (g s), s					0.0	0.0	5.8	9.2					7.5	11.0	5.9		
Cycle Queue Clearance Time (g c), s					0.0	0.0	5.8	9.2					7.5	11.0	5.9		
Green Ratio (g/C)					0.69	0.69	0.76	0.67					0.11	0.11	0.11		
Capacity (c), veh/h					2625	1066	908	3375					198	398	167		
Volume-to-Capacity Ratio (X)					0.492	0.272	0.564	0.311					0.588	0.843	0.474		
Back of Queue (Q), ft/ln (95 th percentile)					11	8	77	135					155	238	103		
Back of Queue (Q), veh/ln (95 th percentile)					0.4	0.3	3.0	5.3					6.1	9.4	4.0		
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.48	0.00					1.03	0.00	0.46		
Uniform Delay (d 1), s/veh					0.0	0.0	4.4	4.6					50.7	52.3	50.0		
Incremental Delay (d 2), s/veh					0.7	0.6	0.2	0.2					1.6	11.3	0.8		
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0		
Control Delay (d), s/veh					0.7	0.6	4.6	4.8					52.3	63.6	50.8		
Level of Service (LOS)					A	A	A	A					D	E	D		
Approach Delay, s/veh / LOS				0.7	A		4.7	A		0.0			59.2	E			
Intersection Delay, s/veh / LOS				10.8						B							
Multimodal Results				EB			WB			NB			SB				
Pedestrian LOS Score / LOS																	
Bicycle LOS Score / LOS																	

HCS Signalized Intersection Results Summary

General Information						Intersection Information							
Agency		Civil Transformations Inc.				Duration, h		0.250					
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other			
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.96			
Urban Street		Osuna		Analysis Year		2025		Analysis Period		1> 7:15			
Intersection		I25 NB Ramp		File Name		SM-I25_2025PMB.xus							
Project Description		Implementation BUILD											



Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				328	2256			1173	1035	336	418	372			

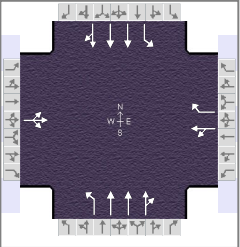
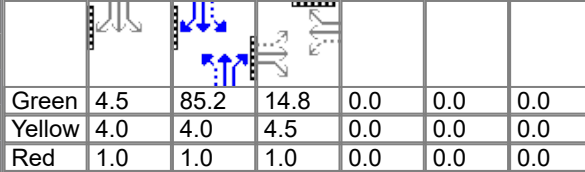
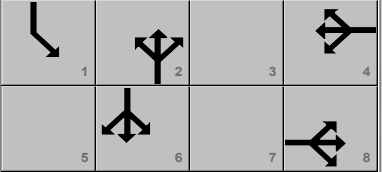
Signal Information														
Cycle, s	120.0	Reference Phase	2	Green	5.3	67.4	32.3	0.0	0.0	0.0	1	2	3	4
Offset, s	55	Reference Point	Begin	Yellow	3.5	4.0	4.5	0.0	0.0	0.0				
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	1.0	0.0	0.0	0.0	5	6	7	8
Force Mode	Fixed	Simult. Gap N/S	On											

Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				5	2		6		4		
Case Number				1.0	4.0		7.3		9.0		
Phase Duration, s				9.8	82.2		72.4		37.8		
Change Period, (Y+R c), s				4.5	5.0		5.0		5.5		
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1		
Queue Clearance Time (g s), s				4.7					29.8		
Green Extension Time (g e), s				0.6	0.0		0.0		2.4		
Phase Call Probability				1.00					1.00		
Max Out Probability				0.00					0.03		

Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				189	1300			1549	664	165	621	339			
Adjusted Saturation Flow Rate (s), veh/h/ln				1718	1699			1823	1647	1781	1818	1405			
Queue Service Time (g s), s				2.7	13.9			11.1	9.4	8.9	18.1	27.8			
Cycle Queue Clearance Time (g c), s				2.7	13.9			11.1	9.4	8.9	18.1	27.8			
Green Ratio (g/C)				0.61	0.64			0.56	0.56	0.27	0.27	0.27			
Capacity (c), veh/h				700	3279			2048	925	479	978	378			
Volume-to-Capacity Ratio (X)				0.270	0.397			0.756	0.718	0.343	0.635	0.895			
Back of Queue (Q), ft/ln (95 th percentile)				46	193			95	108	177	321	410			
Back of Queue (Q), veh/ln (95 th percentile)				1.8	7.6			3.7	4.2	7.0	12.6	16.1			
Queue Storage Ratio (RQ) (95 th percentile)				0.62	0.00			0.00	0.00	0.59	0.00	1.17			
Uniform Delay (d 1), s/veh				11.0	9.4			2.0	2.0	35.3	38.7	42.2			
Incremental Delay (d 2), s/veh				0.2	0.3			2.7	4.8	0.2	0.3	12.0			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				11.2	9.7			4.7	6.7	35.5	38.9	54.2			
Level of Service (LOS)				B	A			A	A	D	D	D			
Approach Delay, s/veh / LOS				9.9	A		5.3	A		43.0	D		0.0		
Intersection Delay, s/veh / LOS				15.5					B						










Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency	Civil Transformations Inc.					Duration, h	0.250								
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other								
Jurisdiction	NMDOT & COA		Time Period	PM Peak		PHF	0.93								
Urban Street	San Mateo		Analysis Year	2025		Analysis Period	1> 7:15								
Intersection	PanAm		File Name	SM-I25_2025PMB-Node3.xus											
Project Description	Implementation BUILD														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				61	13	47	111	10	152	47	2139	98	65	2070	52
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	58	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green	4.5	85.2	14.8	0.0	0.0	0.0									
Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					8		4		2	1	6				
Case Number					8.0		7.0		6.3	1.0	4.0				
Phase Duration, s					20.3		20.3		90.2	9.5	99.7				
Change Period, (Y+R c), s					5.5		5.5		5.0	5.0	5.0				
Max Allow Headway (MAH), s					4.0		4.0		0.0	4.0	0.0				
Queue Clearance Time (g s), s					10.8		14.2			3.2					
Green Extension Time (g e), s					0.8		0.6		0.0	0.1	0.0				
Phase Call Probability					0.98		1.00			0.90					
Max Out Probability					0.12		0.67			0.00					
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16
Adjusted Flow Rate (v), veh/h					126			130	113	51	1608	793	70	1526	756
Adjusted Saturation Flow Rate (s), veh/h/ln					1597			1272	1585	163	1870	1828	1781	1870	1846
Queue Service Time (g s), s					0.0			3.4	8.1	19.2	26.2	26.7	1.2	17.4	17.5
Cycle Queue Clearance Time (g c), s					8.8			12.2	8.1	27.2	26.2	26.7	1.2	17.4	17.5
Green Ratio (g/C)					0.12			0.12	0.12	0.71	0.71	0.71	0.76	0.79	0.79
Capacity (c), veh/h					242			214	195	165	2656	1298	198	2953	1457
Volume-to-Capacity Ratio (X)					0.519			0.608	0.578	0.306	0.605	0.611	0.354	0.517	0.519
Back of Queue (Q), ft/ln (95 th percentile)					168			184	154	39	362	374	30	210	219
Back of Queue (Q), veh/ln (95 th percentile)					6.6			7.2	6.1	1.5	14.3	14.7	1.2	8.3	8.6
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	1.03	0.32	0.00	0.00	0.30	0.00	0.00
Uniform Delay (d 1), s/veh					49.9			51.7	49.7	11.1	8.8	8.9	10.1	4.5	4.5
Incremental Delay (d 2), s/veh					0.6			2.8	2.7	4.8	1.0	2.2	1.1	0.6	1.3
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh					50.6			54.4	52.4	15.8	9.9	11.1	11.2	5.1	5.8
Level of Service (LOS)					D			D	D	B	A	B	B	A	A
Approach Delay, s/veh / LOS				50.6	D	53.5	D	10.4	B	5.5	A				
Intersection Delay, s/veh / LOS				11.2					B						
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															





HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, PM Peak
2025 Implementation BUILD, Timing Plan: 15

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			  			  
Traffic Vol, veh/h	1	19	2340	14	30	2377
Future Vol, veh/h	1	19	2363	14	30	2396
Conflicting Peds, #/hr	0	1	0	4	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	20	2514	15	32	2549
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	3609	1269	0	0	2533	0
Stage 1	2525	-	-	-	-	-
Stage 2	1083	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*15	137	-	-	67	-
Stage 1	*26	-	-	-	-	-
Stage 2	*604	-	-	-	-	-
Platoon blocked, %	0		-	-		-
Mov Cap-1 Maneuver	*8	136	-	-	66	-
Mov Cap-2 Maneuver	*8	-	-	-	-	-
Stage 1	*26	-	-	-	-	-
Stage 2	*314	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s/v	61.77	0		1.26		
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	8	136	66	-
HCM Lane V/C Ratio	-	-	0.142	0.149	0.481	-
HCM Control Delay (s/veh)	-	-	\$ 551.2	36	101.8	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.3	0.5	1.9	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

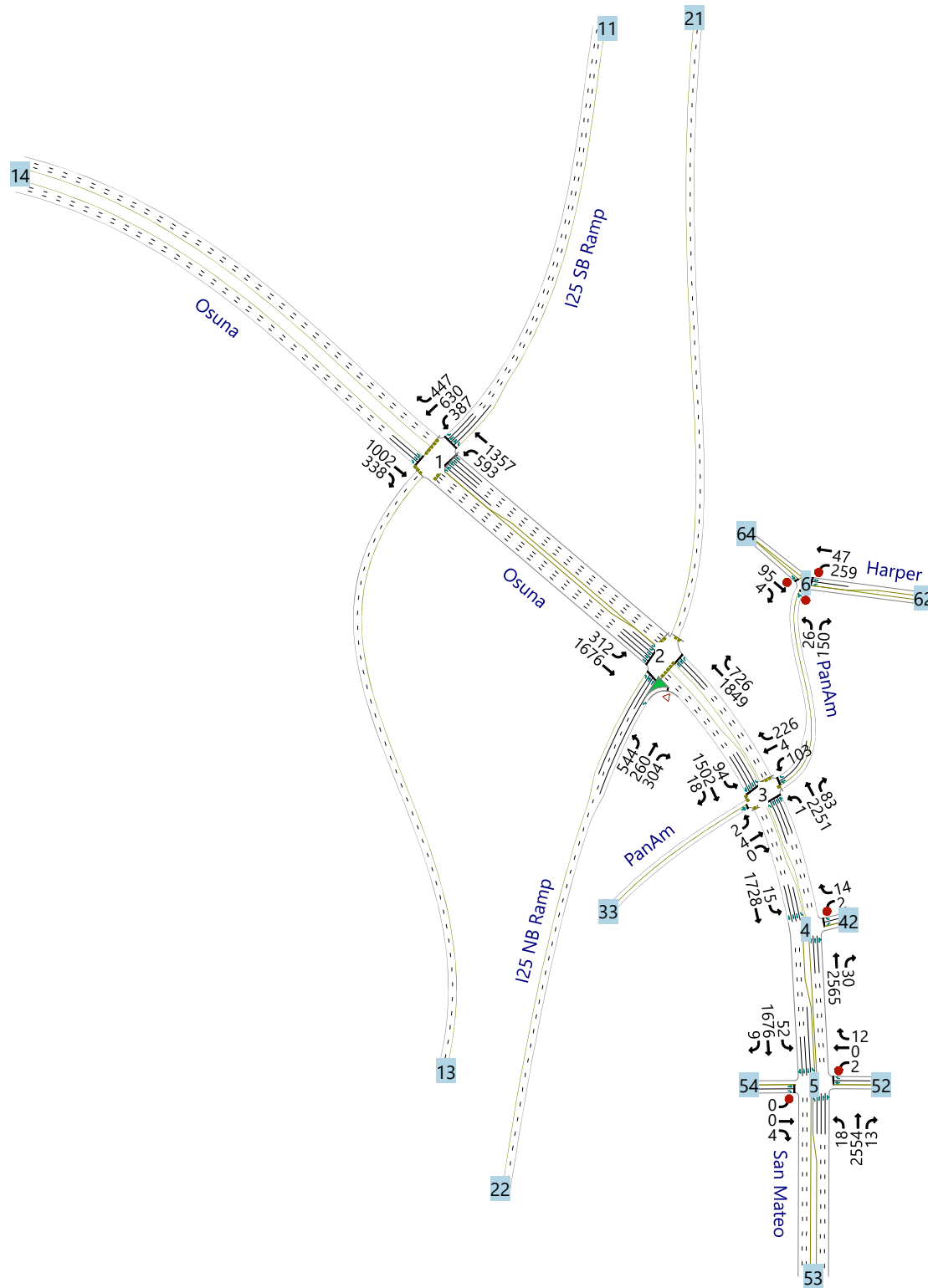
Intersection												
Int Delay, s/veh	13.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕	↗	↖	↑↑↑	↖	↖	↑↑↑	
Traffic Vol, veh/h	0	0	28	0	0	89	18	2088	23	132	2176	4
Future Vol, veh/h	0	0	28	0	0	89	18	2111	23	132	2195	4
Conflicting Peds, #/hr	0	0	2	0	0	0	0	0	1	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	30	0	0	97	20	2295	25	143	2386	4
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3634	5037	1199	3590	5026	1161	2392	0	0	2321	0	0
Stage 1	2677	2677	-	2347	2347	-	-	-	-	-	-	-
Stage 2	957	2360	-	1243	2679	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	8	0	*576	*9	0	162	210	-	-	~ 86	-	-
Stage 1	48	104	-	*21	68	-	-	-	-	-	-	-
Stage 2	250	67	-	*591	103	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	3	0	*574	*7	0	162	210	-	-	~ 86	-	-
Mov Cap-2 Maneuver	3	0	-	*7	0	-	-	-	-	-	-	-
Stage 1	47	0	-	*19	62	-	-	-	-	-	-	-
Stage 2	91	61	-	*560	0	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s/v11.62			55.88		0.2		24.52					
HCM LOS	B		F									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR		
Capacity (veh/h)	210	-	-	-	574	-	162	~ 86	-	-		
HCM Lane V/C Ratio	0.093	-	-	-	0.053	-	0.598	1.675	-	-		
HCM Control Delay (s/veh)	23.9	-	-	0	11.6	0	55.9	\$ 433	-	-		
HCM Lane LOS	C	-	-	A	B	A	F	F	-	-		
HCM 95th %tile Q(veh)	0.3	-	-	-	0.2	-	3.2	11.8	-	-		
Notes												
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined				*: All major volume in platoon				

Intersection	
Intersection Delay, s/veh	11
Intersection LOS	B

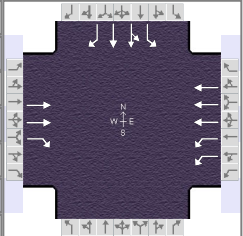
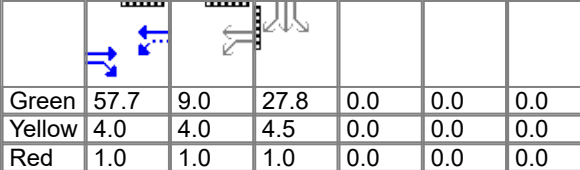
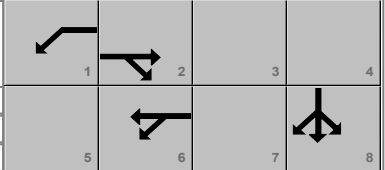
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	139	3	278	58	17	134
Future Vol, veh/h	139	3	285	58	17	140
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	153	3	313	64	19	154
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	9.3	12.6	9.2
HCM LOS	A	B	A

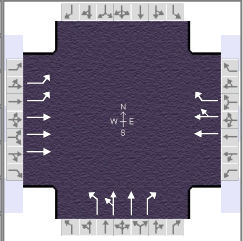
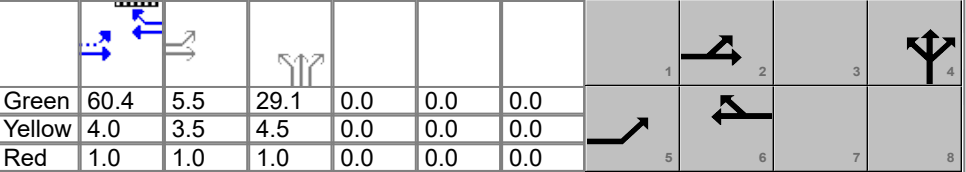
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	11%	0%	100%	0%
Vol Thru, %	0%	98%	0%	100%
Vol Right, %	89%	2%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	157	142	285	58
LT Vol	17	0	285	0
Through Vol	0	139	0	58
RT Vol	140	3	0	0
Lane Flow Rate	173	156	313	64
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.227	0.212	0.486	0.09
Departure Headway (Hd)	4.735	4.901	5.581	5.078
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	756	727	642	702
Service Time	2.78	2.963	3.337	2.834
HCM Lane V/C Ratio	0.229	0.215	0.488	0.091
HCM Control Delay, s/veh	9.2	9.3	13.5	8.3
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	0.9	0.8	2.7	0.3



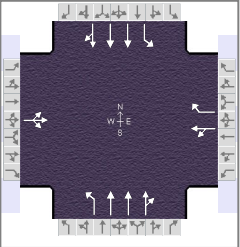
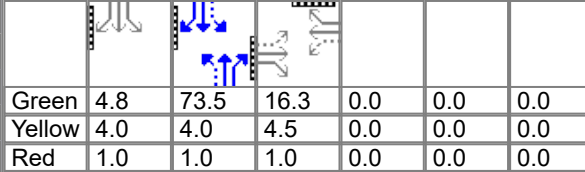
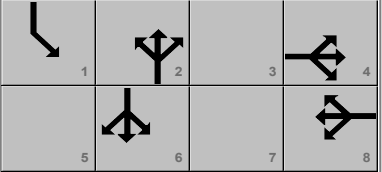
HCS Signalized Intersection Results Summary

General Information						Intersection Information										
Agency		Civil Transformations Inc.				Duration, h		0.250								
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other						
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92						
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15						
Intersection		I25 SB Ramp		File Name		SM-I25_2035AMX.xus										
Project Description		Horizon NO-Build														
Demand Information				EB			WB			NB			SB			
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h					1002	338	593	1357					387	630	447	
Signal Information																
Cycle, s	110.0	Reference Phase	2													
Offset, s	56	Reference Point	Begin													
Uncoordinated	No	Simult. Gap E/W	On			Green	57.7	9.0	27.8	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On			Yellow	4.0	4.0	4.5	0.0	0.0	0.0				
				Red	1.0	1.0	1.0	0.0	0.0	0.0						
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Assigned Phase					2	1	6				8					
Case Number					7.4	1.0	4.0				9.0					
Phase Duration, s					62.7	14.0	76.7				33.3					
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5					
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1					
Queue Clearance Time (g s), s						4.5					25.2					
Green Extension Time (g e), s					0.0	4.6	0.0				2.5					
Phase Call Probability						1.00					1.00					
Max Out Probability						0.02					0.33					
Movement Group Results				EB			WB			NB			SB			
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R	
Assigned Movement					2	12	1	6					3	8	18	
Adjusted Flow Rate (v), veh/h					1089	296	685	1567					294	811	325	
Adjusted Saturation Flow Rate (s), veh/h/ln					1875	1545	1467	1711					1781	1841	1551	
Queue Service Time (g s), s					7.8	3.9	2.5	5.1					16.3	23.2	21.8	
Cycle Queue Clearance Time (g c), s					7.8	3.9	2.5	5.1					16.3	23.2	21.8	
Green Ratio (g/C)					0.52	0.52	0.61	0.65					0.25	0.25	0.25	
Capacity (c), veh/h					1968	811	843	3348					449	929	391	
Volume-to-Capacity Ratio (X)					0.553	0.365	0.813	0.468					0.655	0.873	0.830	
Back of Queue (Q), ft/ln (95 th percentile)					92	54	298	48					292	419	356	
Back of Queue (Q), veh/ln (95 th percentile)					3.6	2.1	11.7	1.9					11.5	16.5	14.0	
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.92	0.00					1.46	0.00	1.58	
Uniform Delay (d 1), s/veh					3.8	3.5	31.7	1.9					36.8	39.4	38.9	
Incremental Delay (d 2), s/veh					1.1	1.3	0.5	0.2					1.3	6.1	8.9	
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0	
Control Delay (d), s/veh					4.9	4.8	32.2	2.1					38.1	45.6	47.8	
Level of Service (LOS)					A	A	C	A					D	D	D	
Approach Delay, s/veh / LOS				4.9	A		11.2	B		0.0			44.5	D		
Intersection Delay, s/veh / LOS				18.9									B			
Multimodal Results				EB			WB			NB			SB			
Pedestrian LOS Score / LOS																
Bicycle LOS Score / LOS																

HCS Signalized Intersection Results Summary








General Information						Intersection Information									
Agency	Civil Transformations Inc.					Duration, h	0.250								
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other								
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.92								
Urban Street	San Mateo		Analysis Year	2035		Analysis Period	1> 7:15								
Intersection	I25 NB Ramp		File Name	SM-I25_2035AMX.xus											
Project Description	Horizon NO-Build														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				312	1676			1849	726	544	260	304			
Signal Information															
Cycle, s	110.0	Reference Phase	2		Green	60.4	5.5	29.1	0.0	0.0	0.0				
Offset, s	51	Reference Point	Begin		Yellow	4.0	3.5	4.5	0.0	0.0	0.0				
Uncoordinated	No	Simult. Gap E/W	On		Red	1.0	1.0	1.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On												
Timer Results				EBL	EBT	WBL		WBT	NBL		NBT	SBL		SBT	
Assigned Phase				5	2			6			4				
Case Number				1.0	4.0			7.4			9.0				
Phase Duration, s				10.0	75.4			65.4			34.6				
Change Period, (Y+R c), s				5.0	5.0			5.0			5.5				
Max Allow Headway (MAH), s				4.0	0.0			0.0			3.1				
Queue Clearance Time (g s), s				4.1							26.7				
Green Extension Time (g e), s				0.6	0.0			0.0			2.4				
Phase Call Probability				1.00							1.00				
Max Out Probability				1.00							0.05				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				237	1273			2153	540	30	844	237			
Adjusted Saturation Flow Rate (s), veh/h/ln				1684	1686			1984	1600	1781	1807	1532			
Queue Service Time (g s), s				2.1	8.0			53.0	7.2	1.4	24.7	14.8			
Cycle Queue Clearance Time (g c), s				2.1	8.0			53.0	7.2	1.4	24.7	14.8			
Green Ratio (g/C)				0.59	0.64			0.55	0.55	0.26	0.26	0.26			
Capacity (c), veh/h				312	3238			2179	878	471	956	405			
Volume-to-Capacity Ratio (X)				0.760	0.393			0.988	0.615	0.063	0.884	0.585			
Back of Queue (Q), ft/ln (95 th percentile)				140	94			300	86	27	428	237			
Back of Queue (Q), veh/ln (95 th percentile)				5.5	3.7			11.8	3.4	1.1	16.8	9.3			
Queue Storage Ratio (RQ) (95 th percentile)				0.56	0.00			0.00	0.00	0.09	0.00	0.68			
Uniform Delay (d 1), s/veh				46.9	4.7			4.4	2.4	30.3	38.8	35.2			
Incremental Delay (d 2), s/veh				6.9	0.2			16.7	3.2	0.0	4.9	0.5			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				53.7	4.9			21.1	5.6	30.3	43.7	35.7			
Level of Service (LOS)				D	A			C	A	C	D	D			
Approach Delay, s/veh / LOS				12.6	B		18.0	B		41.6	D		0.0		
Intersection Delay, s/veh / LOS				21.4						C					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information																					
Agency		Civil Transformations Inc.				Duration, h		0.250																			
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other																	
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92																	
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15																	
Intersection		PanAm		File Name		SM-I25_2035AMX-Node3.xus																					
Project Description		Horizon NO-Build																									
Demand Information				EB			WB			NB			SB														
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R												
Demand (v), veh/h				2	4	0	103	4	226	1	2251	83	94	1502	18												
Signal Information																											
Cycle, s	110.0	Reference Phase	2																								
Offset, s	47	Reference Point	Begin																								
Uncoordinated	No	Simult. Gap E/W	On																								
Force Mode	Fixed	Simult. Gap N/S	On																								
Green	4.8	73.5	16.3	0.0	0.0	0.0																					
Yellow	4.0	4.0	4.5	0.0	0.0	0.0																					
Red	1.0	1.0	1.0	0.0	0.0	0.0																					
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT									
Assigned Phase						4				8				2		1		6									
Case Number						8.0				7.0				6.3		1.0		4.0									
Phase Duration, s						21.8				21.8				78.5		9.8		88.2									
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0									
Max Allow Headway (MAH), s						4.4				4.4				0.0		2.5		0.0									
Queue Clearance Time (g s), s						2.3				14.3						3.8											
Green Extension Time (g e), s						1.2				1.2				0.0		0.1		0.0									
Phase Call Probability						0.18				1.00						0.96											
Max Out Probability						0.00				0.00						0.00											
Movement Group Results				EB			WB			NB			SB														
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R												
Assigned Movement				7	4	14	3	8	18	5	2	12	1	6	16												
Adjusted Flow Rate (v), veh/h					7			116	184	1	1696	841	102	1099	547												
Adjusted Saturation Flow Rate (s), veh/h/ln					1731			1417	1585	304	1870	1835	1781	1870	1862												
Queue Service Time (g s), s					0.0			8.1	12.3	0.1	30.3	30.9	1.8	11.1	11.1												
Cycle Queue Clearance Time (g c), s					0.3			8.4	12.3	1.5	30.3	30.9	1.8	11.1	11.1												
Green Ratio (g/C)					0.15			0.15	0.15	0.67	0.67	0.67	0.73	0.76	0.76												
Capacity (c), veh/h					299			274	234	265	2498	1225	192	2831	1409												
Volume-to-Capacity Ratio (X)					0.022			0.425	0.784	0.004	0.679	0.686	0.533	0.388	0.388												
Back of Queue (Q), ft/ln (95 th percentile)					7			139	228	0	422	439	83	145	152												
Back of Queue (Q), veh/ln (95 th percentile)					0.3			5.5	9.0	0.0	16.6	17.3	3.3	5.7	6.0												
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	0.76	0.00	0.00	0.00	0.83	0.00	0.00												
Uniform Delay (d 1), s/veh					40.1			43.5	45.2	6.6	11.1	11.2	20.0	4.6	4.6												
Incremental Delay (d 2), s/veh					0.0			1.0	5.7	0.0	1.5	3.1	0.9	0.4	0.8												
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0												
Control Delay (d), s/veh					40.1			44.6	50.9	6.6	12.6	14.3	20.8	5.0	5.4												
Level of Service (LOS)					D			D	D	A	B	B	C	A	A												
Approach Delay, s/veh / LOS				40.1		D	48.4		D	13.2		B	6.1		A												
Intersection Delay, s/veh / LOS				12.8						B																	
Multimodal Results				EB			WB			NB			SB														
Pedestrian LOS Score / LOS																											
Bicycle LOS Score / LOS																											

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, AM Peak
2035 Horizon NO-Build, Timing Plan: 11

Intersection							
Int Delay, s/veh	0.8						
Movement	WBL	WBR	NBT	NBR	SBL	SBT	
Lane Configurations			 			 	
Traffic Vol, veh/h	2	14	2565	30	15	1728	
Future Vol, veh/h	2	14	2565	30	15	1728	
Conflicting Peds, #/hr	0	0	0	2	0	0	
Sign Control	Stop	Stop	Free	Free	Free	Free	
RT Channelized	-	None	-	None	-	None	
Storage Length	0	0	-	-	90	-	
Veh in Median Storage, #	0	-	0	-	-	0	
Grade, %	0	-	0	-	-	0	
Peak Hour Factor	92	92	92	92	92	92	
Heavy Vehicles, %	2	2	2	2	2	2	
Mvmt Flow	2	15	2788	33	16	1878	
Major/Minor	Minor1		Major1		Major2		
Conflicting Flow All	3590	1412	0	0	2823	0	
Stage 1	2806	-	-	-	-	-	
Stage 2	784	-	-	-	-	-	
Critical Hdwy	5.74	7.14	-	-	5.34	-	
Critical Hdwy Stg 1	6.64	-	-	-	-	-	
Critical Hdwy Stg 2	6.04	-	-	-	-	-	
Follow-up Hdwy	3.82	3.92	-	-	3.12	-	
Pot Cap-1 Maneuver	*13	109	-	-	47	-	
Stage 1	*17	-	-	-	-	-	
Stage 2	*705	-	-	-	-	-	
Platoon blocked, %	0		-	-		-	
Mov Cap-1 Maneuver	*9	109	-	-	47	-	
Mov Cap-2 Maneuver	*9	-	-	-	-	-	
Stage 1	*17	-	-	-	-	-	
Stage 2	*460	-	-	-	-	-	
Approach	WB		NB		SB		
HCM Control Delay, s/veh	0.69		0		1.02		
HCM LOS	F						
Minor Lane/Major Mvmt		NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)		-	-	9	109	47	-
HCM Lane V/C Ratio		-	-	0.248	0.14	0.348	-
HCM Control Delay (s/veh)		-	-	\$ 518.5	43.3	118.3	-
HCM Lane LOS		-	-	F	E	F	-
HCM 95th %tile Q(veh)		-	-	0.6	0.5	1.2	-
Notes							
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon	

Intersection												
Int Delay, s/veh	5.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	4	2	0	12	18	2554	13	52	1676	9
Future Vol, veh/h	0	0	4	2	0	12	18	2554	13	52	1676	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	2	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	4	2	0	13	20	2838	14	58	1862	10





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3160	4879	938	3747	4877	1428	1874	0	0	2854	0	0
Stage 1	1985	1985	-	2887	2887	-	-	-	-	-	-	-
Stage 2	1175	2894	-	860	1990	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	18	0	*658	*5	0	107	337	-	-	~ 45	-	-
Stage 1	146	232	-	*8	35	-	-	-	-	-	-	-
Stage 2	183	35	-	*675	230	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	15	0	*656	*5	0	106	337	-	-	~ 45	-	-
Mov Cap-2 Maneuver	15	0	-	*5	0	-	-	-	-	-	-	-
Stage 1	145	0	-	*8	33	-	-	-	-	-	-	-
Stage 2	150	33	-	*670	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.52		182.02	0.11	11.12
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	337	-	-	-	656	5	106	~ 45	-	-
HCM Lane V/C Ratio	0.059	-	-	-	0.007	0.449	0.125	1.281	-	-
HCM Control Delay (s/veh)	16.4	-	-	0	10.5	1012.4	43.6	371.6	-	-
HCM Lane LOS	C	-	-	A	B	F	E	F	-	-
HCM 95th %tile Q(veh)	0.2	-	-	-	0	0.8	0.4	5.5	-	-

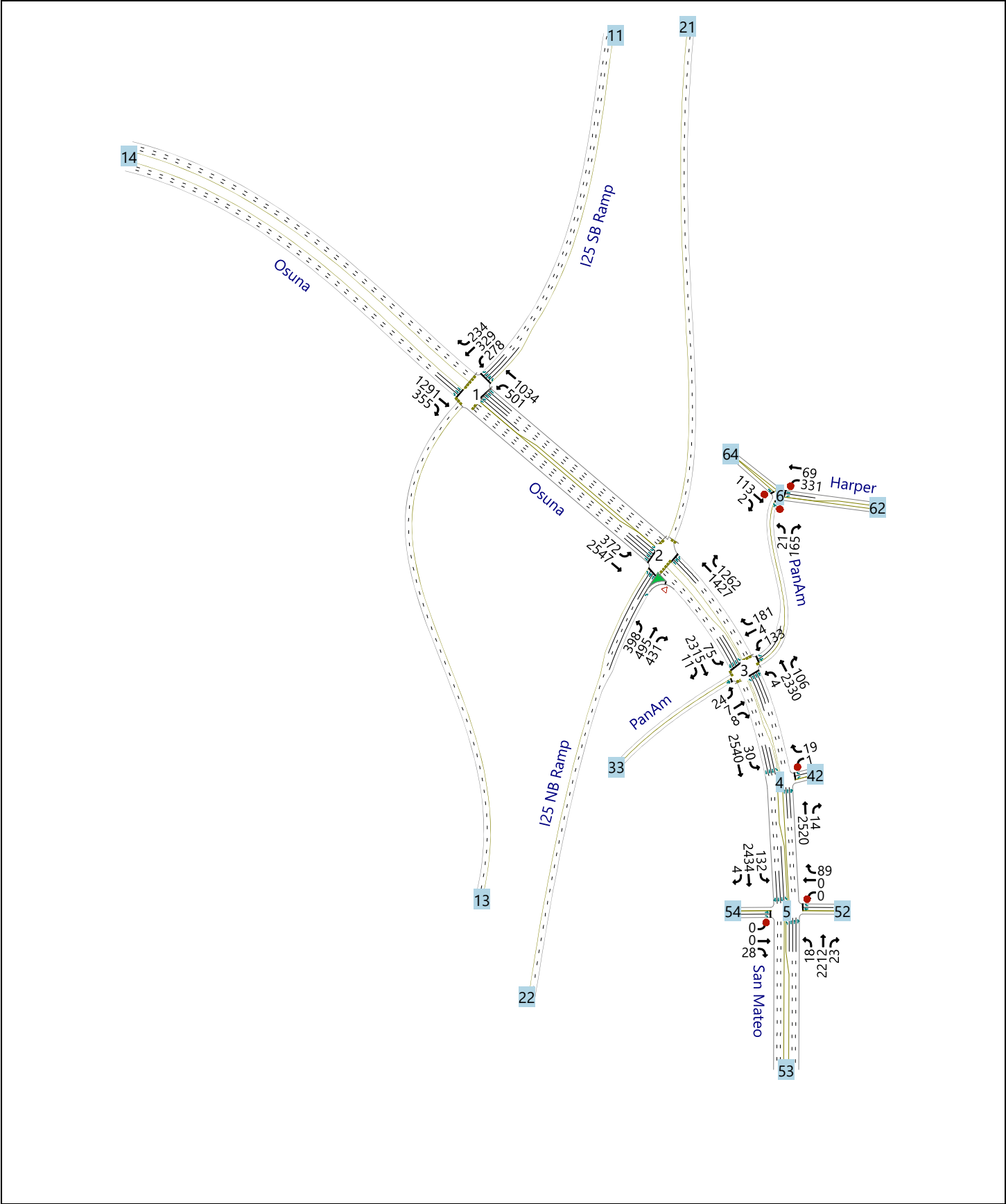
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined						*: All major volume in platoon				

Intersection	
Intersection Delay, s/veh	10.7
Intersection LOS	B

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	95	4	259	47	26	150
Future Vol, veh/h	95	4	259	47	26	150
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	106	4	288	52	29	167
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	8.8	12.1	9.2
HCM LOS	A	B	A

Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	15%	0%	100%	0%
Vol Thru, %	0%	96%	0%	100%
Vol Right, %	85%	4%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	176	99	259	47
LT Vol	26	0	259	0
Through Vol	0	95	0	47
RT Vol	150	4	0	0
Lane Flow Rate	196	110	288	52
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.25	0.15	0.447	0.074
Departure Headway (Hd)	4.596	4.899	5.586	5.084
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	780	727	642	701
Service Time	2.633	2.961	3.341	2.838
HCM Lane V/C Ratio	0.251	0.151	0.449	0.074
HCM Control Delay, s/veh	9.2	8.8	12.8	8.2
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	1	0.5	2.3	0.2

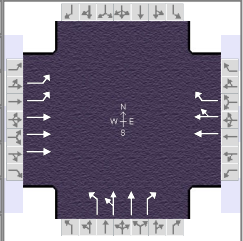


HCS Signalized Intersection Results Summary





General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95					
Urban Street		Osuna		Analysis Year		2035		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2035PMX.xus									
Project Description		Horizon NO-Build													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1291	355	501	1034					278	329	234
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	61	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	8.9	80.6	15.0	0.0	0.0	0.0					
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				4				
Case Number					7.3	1.0	4.0				9.0				
Phase Duration, s					85.6	13.9	99.5				20.5				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1				
Queue Clearance Time (g s), s						8.1					17.0				
Green Extension Time (g e), s					0.0	0.8	0.0				0.0				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.00					1.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					7	4	14
Adjusted Flow Rate (v), veh/h					1359	311	510	1052					161	478	155
Adjusted Saturation Flow Rate (s), veh/h/ln					1917	1554	1394	1676					1781	1805	1517
Queue Service Time (g s), s					0.0	0.0	6.1	9.0					10.4	15.0	11.9
Cycle Queue Clearance Time (g c), s					0.0	0.0	6.1	9.0					10.4	15.0	11.9
Green Ratio (g/C)					0.67	0.67	0.75	0.69					0.12	0.12	0.12
Capacity (c), veh/h					2576	1044	864	3483					223	451	190
Volume-to-Capacity Ratio (X)					0.528	0.297	0.590	0.302					0.723	1.059	0.816
Back of Queue (Q), ft/ln (95 th percentile)					13	10	84	131					227	409	242
Back of Queue (Q), veh/ln (95 th percentile)					0.5	0.4	3.3	5.2					8.9	16.1	9.5
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.52	0.00					1.51	0.00	1.08
Uniform Delay (d 1), s/veh					0.0	0.0	5.0	4.8					50.5	52.5	51.2
Incremental Delay (d 2), s/veh					0.8	0.7	0.2	0.2					9.6	59.0	22.0
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					0.8	0.7	5.2	5.0					60.1	111.5	73.2
Level of Service (LOS)					A	A	A	A					E	F	E
Approach Delay, s/veh / LOS				0.8	A		5.0	A		0.0			93.6	F	
Intersection Delay, s/veh / LOS				20.7						C					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information							
Agency		Civil Transformations Inc.				Duration, h		0.250					
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other			
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.96			
Urban Street		Osuna		Analysis Year		2035		Analysis Period		1> 7:15			
Intersection		I25 NB Ramp		File Name		SM-I25_2035PMX.xus							
Project Description		Horizon NO-Build											



Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				372	2547			1427	1262	398	495	431			

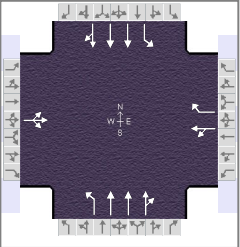
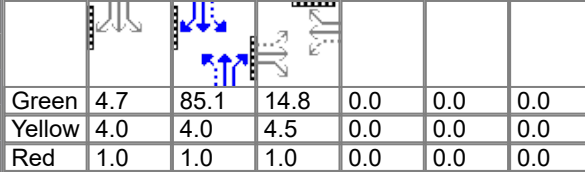
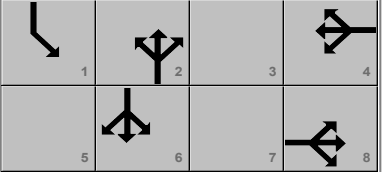
Signal Information															
Cycle, s	120.0	Reference Phase	2	Green	5.5	65.4	34.1	0.0	0.0	0.0		1	2	3	4
Offset, s	55	Reference Point	Begin	Yellow	3.5	4.0	4.5	0.0	0.0	0.0		5	6	7	8
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	1.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On												

Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				5	2		6		4		
Case Number				1.0	4.0		7.3		9.0		
Phase Duration, s				10.0	80.4		70.4		39.6		
Change Period, (Y+R c), s				4.5	5.0		5.0		5.5		
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1		
Queue Clearance Time (g s), s				4.9					31.2		
Green Extension Time (g e), s				0.6	0.0		0.0		2.9		
Phase Call Probability				1.00					1.00		
Max Out Probability				0.00					0.08		

Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				202	1380			1891	822	195	735	400			
Adjusted Saturation Flow Rate (s), veh/h/ln				1781	1706			1857	1692	1781	1830	1577			
Queue Service Time (g s), s				2.9	16.2			37.2	28.2	10.6	21.6	29.2			
Cycle Queue Clearance Time (g c), s				2.9	16.2			37.2	28.2	10.6	21.6	29.2			
Green Ratio (g/C)				0.59	0.63			0.54	0.54	0.28	0.28	0.28			
Capacity (c), veh/h				450	3216			2023	922	506	1040	448			
Volume-to-Capacity Ratio (X)				0.448	0.429			0.935	0.892	0.385	0.707	0.893			
Back of Queue (Q), ft/ln (95 th percentile)				53	220			226	234	205	375	467			
Back of Queue (Q), veh/ln (95 th percentile)				2.1	8.7			8.9	9.2	8.1	14.8	18.4			
Queue Storage Ratio (RQ) (95 th percentile)				0.70	0.00			0.00	0.00	0.68	0.00	1.34			
Uniform Delay (d 1), s/veh				19.8	11.1			4.2	3.8	34.5	38.5	41.2			
Incremental Delay (d 2), s/veh				0.5	0.3			9.6	12.8	0.2	0.7	11.7			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				20.3	11.4			13.8	16.6	34.7	39.2	52.9			
Level of Service (LOS)				C	B			B	B	C	D	D			
Approach Delay, s/veh / LOS				12.5	B		14.7	B		42.6	D		0.0		
Intersection Delay, s/veh / LOS				20.7					C						






Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information																	
Agency	Civil Transformations Inc.					Duration, h	0.250																
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other																
Jurisdiction	NMDOT & COA		Time Period	PM Peak		PHF	0.93																
Urban Street	San Mateo		Analysis Year	2035		Analysis Period	1> 7:00																
Intersection	PanAm		File Name	SM-I25_2035PMX-Node3.xus																			
Project Description	Horizon NO-Build																						
Demand Information																							
				EB			WB			NB			SB										
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R								
Demand (v), veh/h				24	7	8	133	4	181	4	2330	106	75	2315	11								
Signal Information																							
Cycle, s	120.0	Reference Phase	2																				
Offset, s	58	Reference Point	Begin																				
Uncoordinated	No	Simult. Gap E/W	On																				
Force Mode	Fixed	Simult. Gap N/S	On																				
Green	4.7	85.1	14.8	0.0	0.0	0.0																	
Yellow	4.0	4.0	4.5	0.0	0.0	0.0																	
Red	1.0	1.0	1.0	0.0	0.0	0.0																	
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT												
Assigned Phase					8		4		2	1	6												
Case Number					8.0		7.0		6.3	1.0	4.0												
Phase Duration, s					20.3		20.3		90.1	9.7	99.7												
Change Period, (Y+R c), s					5.5		5.5		5.0	5.0	5.0												
Max Allow Headway (MAH), s					4.2		4.2		0.0	4.0	0.0												
Queue Clearance Time (g s), s					4.5		14.2			3.3													
Green Extension Time (g e), s					1.0		0.5		0.0	0.2	0.0												
Phase Call Probability					0.71		1.00			0.93													
Max Out Probability					0.00		0.76			0.00													
Movement Group Results				EB			WB			NB			SB										
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R								
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16								
Adjusted Flow Rate (v), veh/h					38			147	144	4	1748	867	81	1668	833								
Adjusted Saturation Flow Rate (s), veh/h/ln					1544			1413	1585	131	1870	1829	1781	1870	1866								
Queue Service Time (g s), s					0.0			9.8	10.5	1.5	30.7	31.5	1.3	20.3	20.4								
Cycle Queue Clearance Time (g c), s					2.5			12.2	10.5	12.3	30.7	31.5	1.3	20.3	20.4								
Green Ratio (g/C)					0.12			0.12	0.12	0.71	0.71	0.71	0.76	0.79	0.79								
Capacity (c), veh/h					241			233	195	141	2652	1296	181	2953	1473								
Volume-to-Capacity Ratio (X)					0.156			0.632	0.738	0.030	0.659	0.669	0.445	0.565	0.566								
Back of Queue (Q), ft/ln (95 th percentile)					47			205	208	3	415	434	72	239	249								
Back of Queue (Q), veh/ln (95 th percentile)					1.9			8.1	8.2	0.1	16.3	17.1	2.8	9.4	9.8								
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	1.39	0.02	0.00	0.00	0.72	0.00	0.00								
Uniform Delay (d 1), s/veh					47.2			51.5	50.8	9.0	9.5	9.7	17.7	4.8	4.8								
Incremental Delay (d 2), s/veh					0.1			3.2	8.1	0.4	1.3	2.8	1.7	0.8	1.6								
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0								
Control Delay (d), s/veh					47.3			54.7	58.8	9.4	10.8	12.4	19.4	5.6	6.4								
Level of Service (LOS)					D			D	E	A	B	B	B	A	A								
Approach Delay, s/veh / LOS				47.3	D	56.8	E	11.4	B	6.3	A												
Intersection Delay, s/veh / LOS				11.6					B														
Multimodal Results				EB			WB			NB			SB										
Pedestrian LOS Score / LOS																							
Bicycle LOS Score / LOS																							

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, PM Peak
2035 Horizon NO-Build, Timing Plan: 15

Intersection						
Int Delay, s/veh	1.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	19	2520	14	30	2540
Future Vol, veh/h	1	19	2520	14	30	2540
Conflicting Peds, #/hr	0	1	0	4	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	20	2681	15	32	2702
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	3837	1353	0	0	2700	0
Stage 1	2692	-	-	-	-	-
Stage 2	1145	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*9	120	-	-	54	-
Stage 1	*20	-	-	-	-	-
Stage 2	*578	-	-	-	-	-
Platoon blocked, %	0	-	-	-	-	-
Mov Cap-1 Maneuver	*4	119	-	-	54	-
Mov Cap-2 Maneuver	*4	-	-	-	-	-
Stage 1	*20	-	-	-	-	-
Stage 2	*238	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s/v	97.8	0	1.64			
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBRWBLn1WBLn2	SBL	SBT		
Capacity (veh/h)	-	-	4	119	54	-
HCM Lane V/C Ratio	-	-	0.28	0.169	0.588	-
HCM Control Delay (s/veh)	-	\$	1173.4	41.2	140.3	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.5	0.6	2.3	-
Notes						
~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon						

Intersection												
Int Delay, s/veh	15.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	28	0	0	89	18	2212	23	132	2434	4
Future Vol, veh/h	0	0	28	0	0	89	18	2212	23	132	2434	4
Conflicting Peds, #/hr	0	0	2	0	0	0	0	0	1	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	30	0	0	97	20	2404	25	143	2646	4





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3938	5406	1329	3804	5396	1216	2652	0	0	2430	0	0
Stage 1	2937	2937	-	2457	2457	-	-	-	-	-	-	-
Stage 2	1001	2469	-	1347	2939	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	4	0	*532	*5	0	149	160	-	-	~ 75	-	-
Stage 1	31	75	-	*18	60	-	-	-	-	-	-	-
Stage 2	235	59	-	*546	75	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	0	-	-	-	-	-
Mov Cap-1 Maneuver	1	0	*530	*4	0	148	160	-	-	~ 75	-	-
Mov Cap-2 Maneuver	1	0	-	*4	0	-	-	-	-	-	-	-
Stage 1	31	0	-	*16	53	-	-	-	-	-	-	-
Stage 2	72	52	-	*514	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v12.21		65.96	0.24	28.01
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	160	-	-	-	530	-	148	~ 75	-	-
HCM Lane V/C Ratio	0.122	-	-	-	0.057	-	0.651	1.909	-	-
HCM Control Delay (s/veh)	30.6	-	-	0	12.2	0	66	545.4	-	-
HCM Lane LOS	D	-	-	A	B	A	F	F	-	-
HCM 95th %tile Q(veh)	0.4	-	-	-	0.2	-	3.6	12.8	-	-

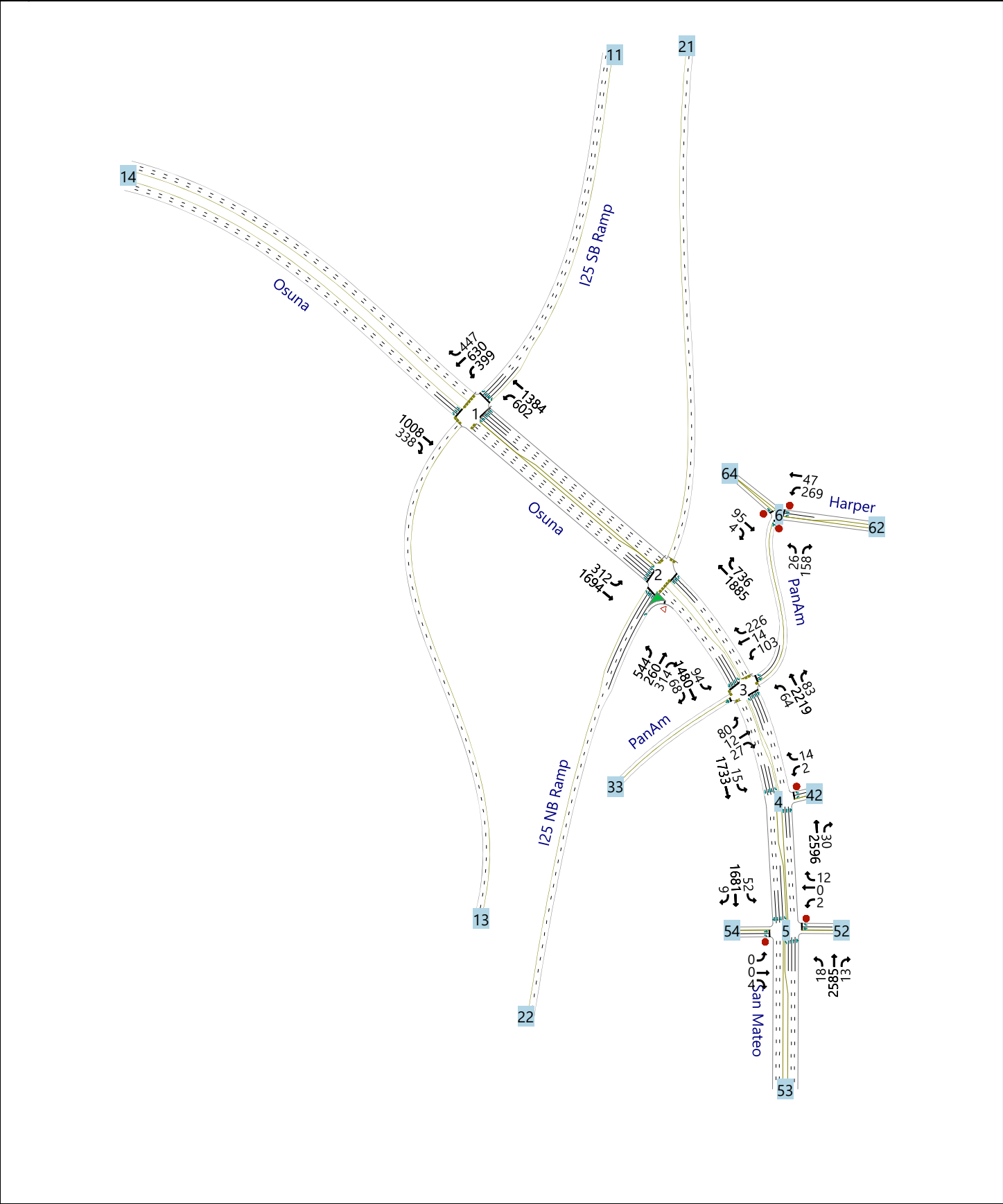
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon							

Intersection	
Intersection Delay, s/veh	12.4
Intersection LOS	B

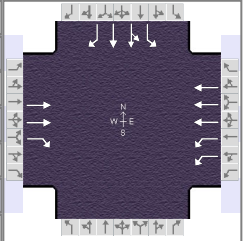
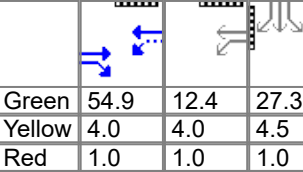
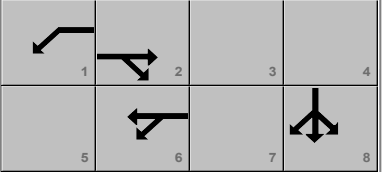
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	113	2	331	69	21	165
Future Vol, veh/h	113	2	331	69	21	165
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	124	2	364	76	23	181
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	9.3	14.5	9.7
HCM LOS	A	B	A

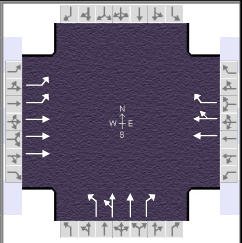
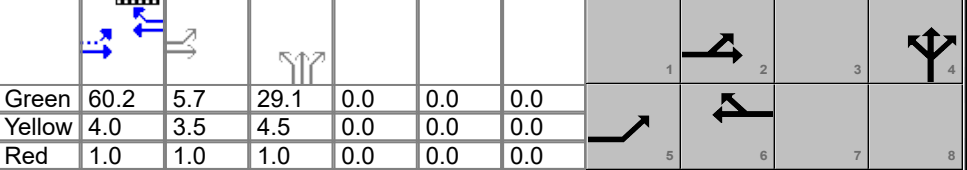
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	11%	0%	100%	0%
Vol Thru, %	0%	98%	0%	100%
Vol Right, %	89%	2%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	186	115	331	69
LT Vol	21	0	331	0
Through Vol	0	113	0	69
RT Vol	165	2	0	0
Lane Flow Rate	204	126	364	76
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.274	0.178	0.57	0.108
Departure Headway (Hd)	4.834	5.068	5.642	5.139
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	740	701	637	692
Service Time	2.882	3.15	3.413	2.909
HCM Lane V/C Ratio	0.276	0.18	0.571	0.11
HCM Control Delay, s/veh	9.7	9.3	15.7	8.5
HCM Lane LOS	A	A	C	A
HCM 95th-tile Q	1.1	0.6	3.6	0.4



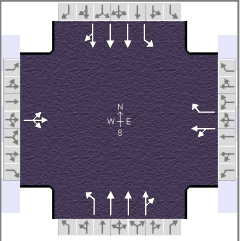
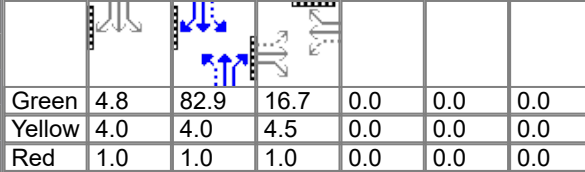
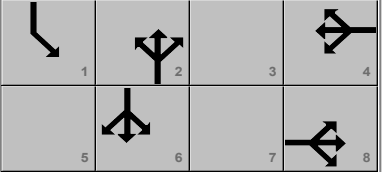
HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.						Duration, h		0.250									
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92									
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15									
Intersection		I25 SB Ramp		File Name		SM-I25_2035AMB.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h					1008	338	602	1384					399	630	447				
Signal Information																			
Cycle, s	110.0	Reference Phase	2																
Offset, s	56	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	54.9	12.4	27.3	0.0	0.0	0.0									
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						2		1		6								8	
Case Number						7.4		1.0		4.0								9.0	
Phase Duration, s						59.9		17.4		77.2								32.8	
Change Period, (Y+R c), s						5.0		5.0		5.0								5.5	
Max Allow Headway (MAH), s						0.0		2.5		0.0								3.1	
Queue Clearance Time (g s), s								7.7										25.5	
Green Extension Time (g e), s						0.0		4.8		0.0								1.7	
Phase Call Probability								1.00										1.00	
Max Out Probability								0.05										0.77	
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement					2	12	1	6					3	8	18				
Adjusted Flow Rate (v), veh/h					1096	296	717	1649					304	815	325				
Adjusted Saturation Flow Rate (s), veh/h/ln					1876	1545	1489	1718					1781	1841	1551				
Queue Service Time (g s), s					10.5	5.2	5.7	6.6					17.0	23.5	21.9				
Cycle Queue Clearance Time (g c), s					10.5	5.2	5.7	6.6					17.0	23.5	21.9				
Green Ratio (g/C)					0.50	0.50	0.61	0.66					0.25	0.25	0.25				
Capacity (c), veh/h					1874	771	882	3383					442	914	385				
Volume-to-Capacity Ratio (X)					0.585	0.383	0.814	0.487					0.687	0.891	0.844				
Back of Queue (Q), ft/ln (95 th percentile)					125	71	306	59					308	434	370				
Back of Queue (Q), veh/ln (95 th percentile)					4.9	2.8	12.1	2.3					12.1	17.1	14.5				
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.94	0.00					1.54	0.00	1.64				
Uniform Delay (d 1), s/veh					5.5	5.0	34.7	2.4					37.5	39.9	39.3				
Incremental Delay (d 2), s/veh					1.3	1.4	0.5	0.1					2.8	9.0	12.5				
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0				
Control Delay (d), s/veh					6.8	6.5	35.2	2.5					40.2	48.9	51.8				
Level of Service (LOS)					A	A	D	A					D	D	D				
Approach Delay, s/veh / LOS				6.8		A		12.4		B		0.0				47.7		D	
Intersection Delay, s/veh / LOS				20.7												C			
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCS Signalized Intersection Results Summary










General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92					
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15					
Intersection		I25 NB Ramp		File Name		SM-I25_2035AMB.xus									
Project Description		Horizon BUILD													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				312	1694			1885	736	544	260	314			
Signal Information															
Cycle, s	110.0	Reference Phase	2												
Offset, s	51	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	60.2	5.7	29.1	0.0	0.0	0.0					
				Yellow	4.0	3.5	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase				5	2		6		4						
Case Number				1.0	4.0		7.4		9.0						
Phase Duration, s				10.2	75.4		65.2		34.6						
Change Period, (Y+R c), s				5.0	5.0		5.0		5.5						
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1						
Queue Clearance Time (g s), s				5.1					26.7						
Green Extension Time (g e), s				0.0	0.0		0.0		2.4						
Phase Call Probability				1.00					1.00						
Max Out Probability				1.00					0.05						
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				237	1285			2195	549	30	844	248			
Adjusted Saturation Flow Rate (s), veh/h/ln				1684	1687			1985	1602	1781	1807	1534			
Queue Service Time (g s), s				3.1	8.1			60.2	7.7	1.4	24.7	15.6			
Cycle Queue Clearance Time (g c), s				3.1	8.1			60.2	7.7	1.4	24.7	15.6			
Green Ratio (g/C)				0.59	0.64			0.55	0.55	0.26	0.26	0.26			
Capacity (c), veh/h				289	3239			2173	877	471	956	406			
Volume-to-Capacity Ratio (X)				0.818	0.397			1.010	0.626	0.063	0.883	0.610			
Back of Queue (Q), ft/ln (95 th percentile)				144	95.9			355.1	90.8	26.7	427.2	247.1			
Back of Queue (Q), veh/ln (95 th percentile)				5.7	3.8			14.0	3.6	1.1	16.8	9.7			
Queue Storage Ratio (RQ) (95 th percentile)				0.58	0.00			0.00	0.00	0.09	0.00	0.71			
Uniform Delay (d 1), s/veh				47.4	4.7			4.8	2.5	30.2	38.8	35.5			
Incremental Delay (d 2), s/veh				10.8	0.2			21.7	3.4	0.0	4.8	0.6			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				58.2	4.9			26.6	5.9	30.3	43.7	36.0			
Level of Service (LOS)				E	A			F	A	C	D	D			
Approach Delay, s/veh / LOS				13.2	B		22.4	C		41.6	D		0.0		
Intersection Delay, s/veh / LOS				23.8						C					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.93									
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15									
Intersection		PanAm		File Name		SM-I25_2035PMB-Node3.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				80	12	27	103	14	226	64	2219	83	94	1480	68				
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	58	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
Green	4.8	82.9	16.7	0.0	0.0	0.0													
Yellow	4.0	4.0	4.5	0.0	0.0	0.0													
Red	1.0	1.0	1.0	0.0	0.0	0.0													
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						8.0				7.0				6.3		1.0		4.0	
Phase Duration, s						22.2				22.2				87.9		9.8		97.8	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.1				4.1				0.0		4.0		0.0	
Queue Clearance Time (g s), s						11.0				16.3						3.8			
Green Extension Time (g e), s						1.0				0.5				0.0		0.2		0.0	
Phase Call Probability						0.98				1.00						0.97			
Max Out Probability						0.18				1.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h					124			126	192	69	1653	818	101	1118	546				
Adjusted Saturation Flow Rate (s), veh/h/ln					1513			1415	1585	299	1870	1836	1781	1870	1826				
Queue Service Time (g s), s					0.0			1.1	14.3	11.6	29.3	29.8	1.8	11.6	11.6				
Cycle Queue Clearance Time (g c), s					9.0			10.1	14.3	13.4	29.3	29.8	1.8	11.6	11.6				
Green Ratio (g/C)					0.14			0.14	0.14	0.69	0.69	0.69	0.75	0.77	0.77				
Capacity (c), veh/h					262			254	221	262	2585	1269	191	2892	1412				
Volume-to-Capacity Ratio (X)					0.472			0.496	0.871	0.263	0.639	0.645	0.528	0.387	0.387				
Back of Queue (Q), ft/ln (95 th percentile)					162			169	295	38	409	423	98	153	157				
Back of Queue (Q), veh/ln (95 th percentile)					6.4			6.7	11.6	1.5	16.1	16.6	3.9	6.0	6.2				
Queue Storage Ratio (RQ) (95 th percentile)					0.00			0.00	1.96	0.32	0.00	0.00	0.98	0.00	0.00				
Uniform Delay (d 1), s/veh					48.2			48.8	50.6	8.2	10.3	10.3	18.4	4.4	4.4				
Incremental Delay (d 2), s/veh					0.5			1.5	24.5	2.4	1.2	2.5	2.2	0.4	0.8				
Initial Queue Delay (d 3), s/veh					0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh					48.7			50.3	75.1	10.6	11.5	12.9	20.6	4.8	5.2				
Level of Service (LOS)					D			D	E	B	B	B	C	A	A				
Approach Delay, s/veh / LOS				48.7	D		65.3	E		11.9	B		5.8		A				
Intersection Delay, s/veh / LOS				14.2						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, AM Peak
2035 Horizon BUILD, Timing Plan: 11

Intersection						
Int Delay, s/veh	0.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			  			  
Traffic Vol, veh/h	2	14	2565	30	15	1728
Future Vol, veh/h	2	14	2596	30	15	1733
Conflicting Peds, #/hr	0	0	0	2	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	15	2822	33	16	1884
Major/Minor	Minor1		Major1		Major2	
Conflicting Flow All	3626	1429	0	0	2856	0
Stage 1	2840	-	-	-	-	-
Stage 2	786	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*13	106	-	-	45	-
Stage 1	*16	-	-	-	-	-
Stage 2	*705	-	-	-	-	-
Platoon blocked, %	0		-	-	-	-
Mov Cap-1 Maneuver	*8	106	-	-	45	-
Mov Cap-2 Maneuver	*8	-	-	-	-	-
Stage 1	*16	-	-	-	-	-
Stage 2	*450	-	-	-	-	-
Approach	WB		NB		SB	
HCM Control Delay, s/veh	10.51		0		1.07	
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	8	106	45	-
HCM Lane V/C Ratio	-	-	0.27	0.143	0.362	-
HCM Control Delay (s/veh)	-	-	\$ 572.7	44.5	124.9	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.6	0.5	1.3	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

Intersection												
Int Delay, s/veh	5.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	4	2	0	12	18	2554	13	52	1676	9
Future Vol, veh/h	0	0	4	2	0	12	18	2585	13	52	1681	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	2	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	4	2	0	13	20	2872	14	58	1868	10





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3179	4919	941	3784	4917	1445	1880	0	0	2889	0	0
Stage 1	1990	1990	-	2921	2921	-	-	-	-	-	-	-
Stage 2	1189	2929	-	863	1995	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	17	0	*658	*5	0	104	334	-	-	~ 43	-	-
Stage 1	144	230	-	*8	34	-	-	-	-	-	-	-
Stage 2	179	34	-	*675	228	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	14	0	*656	*5	0	104	334	-	-	~ 43	-	-
Mov Cap-2 Maneuver	14	0	-	*5	0	-	-	-	-	-	-	-
Stage 1	143	0	-	*7	32	-	-	-	-	-	-	-
Stage 2	147	32	-	*670	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v10.52		196.74	0.11	11.93
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	334	-	-	-	656	5	104	~ 43	-	-
HCM Lane V/C Ratio	0.06	-	-	-	0.007	0.486	0.129	1.336	-	-
HCM Control Delay (s/veh)	16.5	-	-	0	10.5	1108.2	44.8	399.7	-	-
HCM Lane LOS	C	-	-	A	B	F	E	F	-	-
HCM 95th %tile Q(veh)	0.2	-	-	-	0	0.8	0.4	5.6	-	-

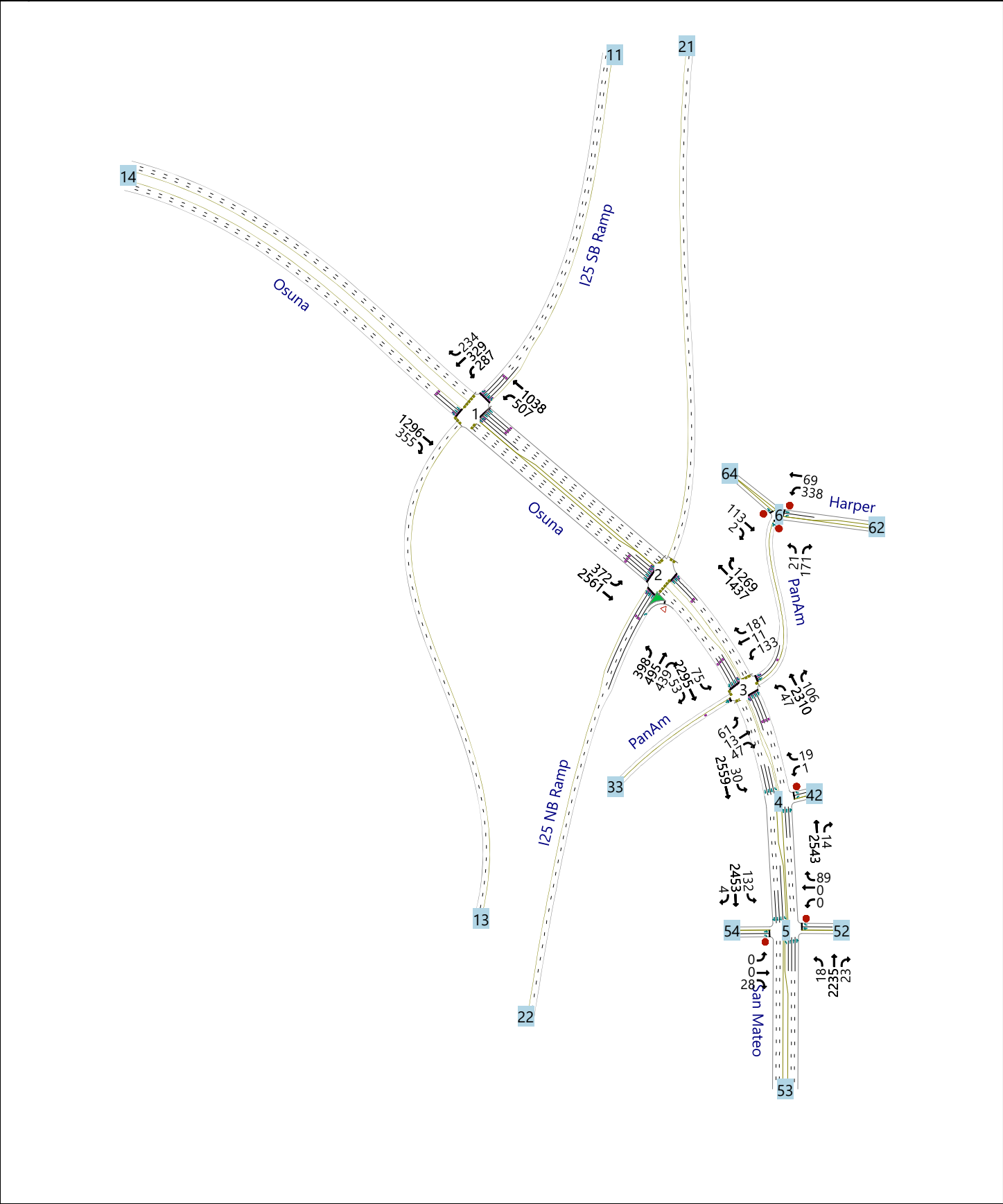
Notes												
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined						*: All major volume in platoon				

Intersection	
Intersection Delay, s/veh	10.9
Intersection LOS	B

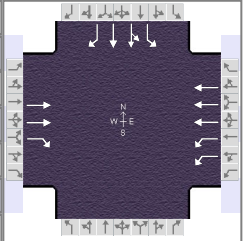
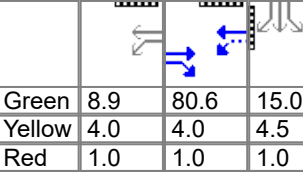
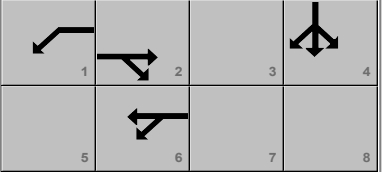
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	95	4	259	47	26	150
Future Vol, veh/h	95	4	269	47	26	158
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	106	4	299	52	29	176
Number of Lanes	1	0	1	1	1	0

Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	8.9	12.5	9.3
HCM LOS	A	B	A

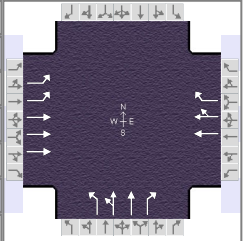
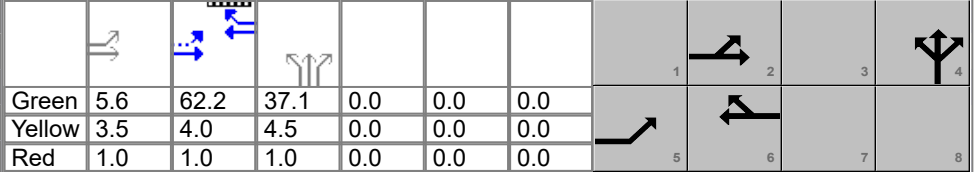
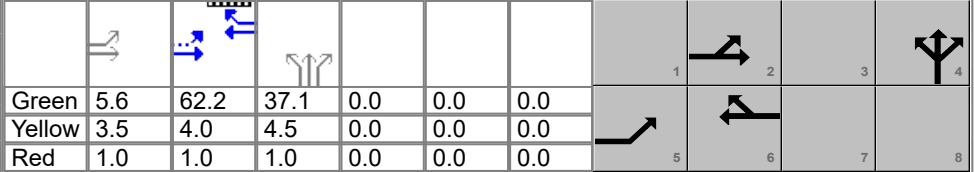
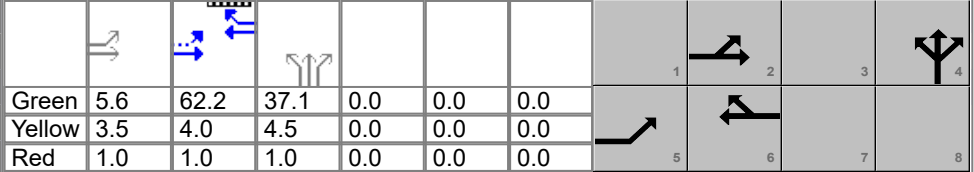
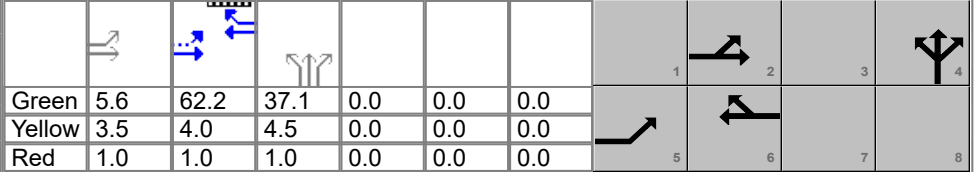
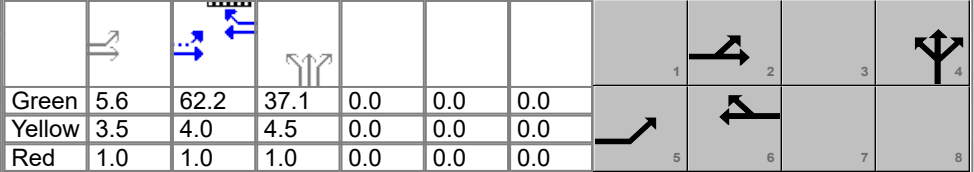
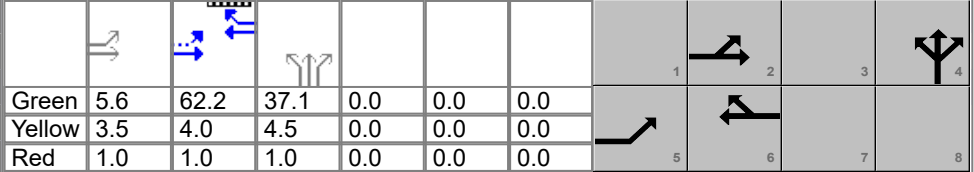
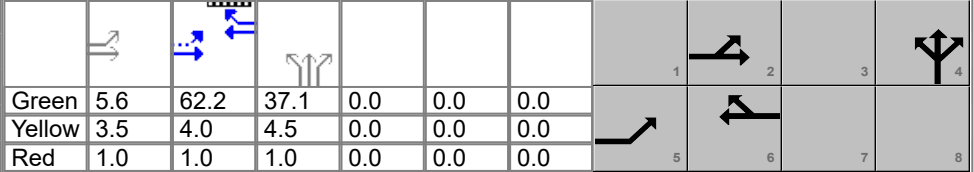
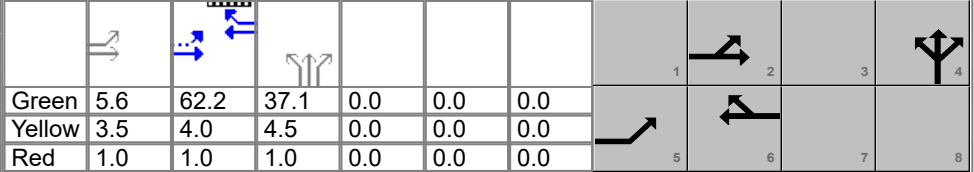
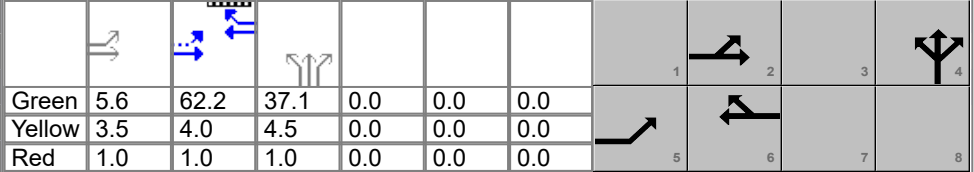
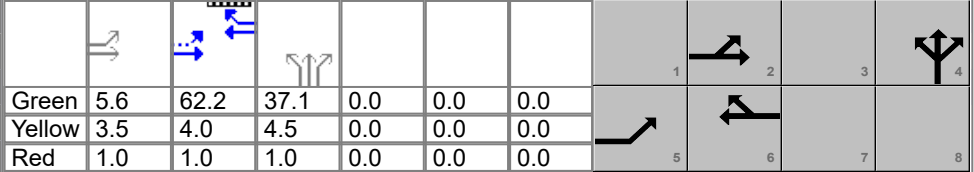
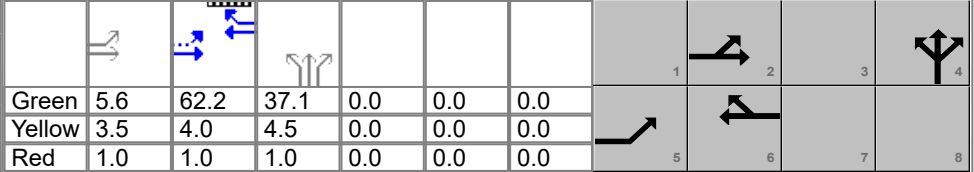
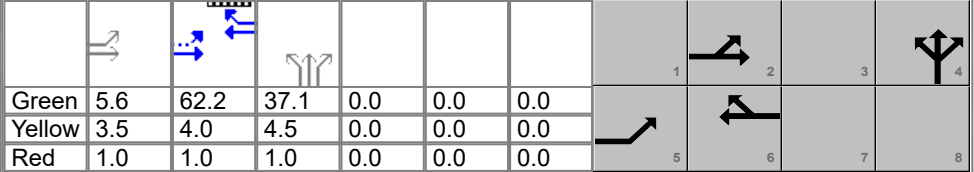
Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	14%	0%	100%	0%
Vol Thru, %	0%	96%	0%	100%
Vol Right, %	86%	4%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	184	99	269	47
LT Vol	26	0	269	0
Through Vol	0	95	0	47
RT Vol	158	4	0	0
Lane Flow Rate	204	110	299	52
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.263	0.151	0.466	0.074
Departure Headway (Hd)	4.624	4.94	5.61	5.107
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	775	721	640	698
Service Time	2.661	3.007	3.369	2.866
HCM Lane V/C Ratio	0.263	0.153	0.467	0.074
HCM Control Delay, s/veh	9.3	8.9	13.2	8.3
HCM Lane LOS	A	A	B	A
HCM 95th-tile Q	1.1	0.5	2.5	0.2



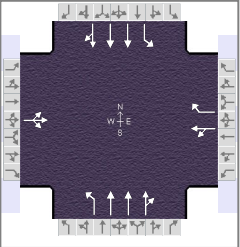
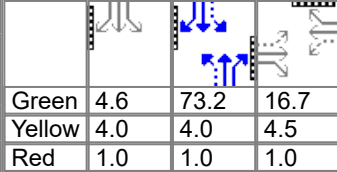
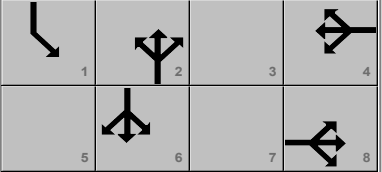
HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95					
Urban Street		Osuna		Analysis Year		2035		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2035PMB.xus									
Project Description		Horizon BUILD													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1296	355	507	1038					287	329	234
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	61	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green	8.9	80.6	15.0	0.0	0.0	0.0									
Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				4				
Case Number					7.3	1.0	4.0				9.0				
Phase Duration, s					85.6	13.9	99.5				20.5				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1				
Queue Clearance Time (g s), s						8.1					17.0				
Green Extension Time (g e), s					0.0	0.8	0.0				0.0				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.00					1.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6				7	4	14	
Adjusted Flow Rate (v), veh/h					1364	311	516	1056				166	482	155	
Adjusted Saturation Flow Rate (s), veh/h/ln					1918	1554	1411	1677				1781	1805	1517	
Queue Service Time (g s), s					0.0	0.0	6.1	9.4				10.8	15.0	11.9	
Cycle Queue Clearance Time (g c), s					0.0	0.0	6.1	9.4				10.8	15.0	11.9	
Green Ratio (g/C)					0.67	0.67	0.75	0.66				0.12	0.12	0.12	
Capacity (c), veh/h					2577	1044	864	3339				223	451	190	
Volume-to-Capacity Ratio (X)					0.529	0.297	0.597	0.316				0.746	1.069	0.816	
Back of Queue (Q), ft/ln (95 th percentile)					13	10	85	140				237	417	242	
Back of Queue (Q), veh/ln (95 th percentile)					0.5	0.4	3.3	5.5				9.3	16.4	9.5	
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.53	0.00				1.58	0.00	1.08	
Uniform Delay (d 1), s/veh					0.0	0.0	5.0	5.0				50.7	52.5	51.2	
Incremental Delay (d 2), s/veh					0.8	0.7	0.2	0.2				11.5	61.9	22.0	
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0				0.0	0.0	0.0	
Control Delay (d), s/veh					0.8	0.7	5.2	5.2				62.2	114.4	73.2	
Level of Service (LOS)					A	A	A	A				E	F	E	
Approach Delay, s/veh / LOS				0.8	A	5.2	A	0.0			95.7	F			
Intersection Delay, s/veh / LOS				21.3						C					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary










General Information						Intersection Information									
Agency	Civil Transformations Inc.					Duration, h	0.250								
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other								
Jurisdiction	NMDOT & COA		Time Period	PM Peak		PHF	0.96								
Urban Street	Osuna		Analysis Year	2035		Analysis Period	1> 7:15								
Intersection	I25 NB Ramp		File Name	SM-I25_2035PMB.xus											
Project Description	Horizon BUILD														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				372	2547			1427	1262	398	495	431			
Signal Information															
Cycle, s	120.0	Reference Phase	2												
Offset, s	55	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
				Green	5.6	62.2	37.1	0.0	0.0	0.0					
				Yellow	3.5	4.0	4.5	0.0	0.0	0.0					
				Red	1.0	1.0	1.0	0.0	0.0	0.0					
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase				5	2		6		4						
Case Number				1.0	4.0		7.3		9.0						
Phase Duration, s				10.1	77.4		67.2		42.6						
Change Period, (Y+R c), s				4.5	5.0		5.0		5.5						
Max Allow Headway (MAH), s				4.0	0.0		0.0		3.1						
Queue Clearance Time (g s), s				5.1					34.5						
Green Extension Time (g e), s				0.5	0.0		0.0		2.6						
Phase Call Probability				1.00					1.00						
Max Out Probability				0.00					0.19						
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				5	2			6	16	7	4	14			
Adjusted Flow Rate (v), veh/h				202	1385			1891	822	195	735	400			
Adjusted Saturation Flow Rate (s), veh/h/ln				1781	1706			1857	1692	1781	1830	1419			
Queue Service Time (g s), s				3.1	16.4			54.8	41.6	10.2	20.8	32.5			
Cycle Queue Clearance Time (g c), s				3.1	16.4			54.8	41.6	10.2	20.8	32.5			
Green Ratio (g/C)				0.57	0.60			0.52	0.52	0.31	0.31	0.31			
Capacity (c), veh/h				331	3088			1926	877	551	1132	439			
Volume-to-Capacity Ratio (X)				0.611	0.448			0.982	0.937	0.354	0.649	0.911			
Back of Queue (Q), ft/ln (95 th percentile)				69	218			332	316	198	361	485			
Back of Queue (Q), veh/ln (95 th percentile)				2.7	8.6			13.1	12.4	7.8	14.2	19.1			
Queue Storage Ratio (RQ) (95 th percentile)				0.92	0.00			0.00	0.00	0.66	0.00	1.39			
Uniform Delay (d 1), s/veh				28.7	11.6			7.6	6.7	32.1	35.8	39.9			
Incremental Delay (d 2), s/veh				1.2	0.3			16.7	18.5	0.1	0.5	17.0			
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0			
Control Delay (d), s/veh				29.9	11.9			24.3	25.2	32.3	36.3	56.8			
Level of Service (LOS)				C	B			C	C	C	D	E			
Approach Delay, s/veh / LOS				14.2	B		24.6	C		41.9	D		0.0		
Intersection Delay, s/veh / LOS				25.7					C						
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information											
Agency	Civil Transformations Inc.					Duration, h	0.250										
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other										
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.92										
Urban Street	San Mateo		Analysis Year	2035		Analysis Period	1> 7:00										
Intersection	PanAm		File Name	SM-I25_2035AMB-Node3.xus													
Project Description	Horizon BUILD																
Demand Information						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h						61	13	47	133	11	181	47	2310	106	75	2295	53
Signal Information																	
Cycle, s	110.0	Reference Phase	2														
Offset, s	47	Reference Point	Begin														
Uncoordinated	No	Simult. Gap E/W	On														
Force Mode	Fixed	Simult. Gap N/S	On														
Green	4.6	73.2	16.7	0.0	0.0	0.0											
Yellow	4.0	4.0	4.5	0.0	0.0	0.0											
Red	1.0	1.0	1.0	0.0	0.0	0.0											
Timer Results						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase							8		4		2	1	6				
Case Number							8.0		7.0		6.3	1.0	4.0				
Phase Duration, s							22.2		22.2		78.2	9.6	87.8				
Change Period, (Y+R c), s							5.5		5.5		5.0	5.0	5.0				
Max Allow Headway (MAH), s							4.4		4.4		0.0	2.5	0.0				
Queue Clearance Time (g s), s							10.2		15.1			3.4					
Green Extension Time (g e), s							1.7		1.6		0.0	0.1	0.0				
Phase Call Probability							0.98		1.00			0.92					
Max Out Probability							0.00		0.00			0.00					
Movement Group Results						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement						3	8	18	7	4	14	5	2	12	1	6	16
Adjusted Flow Rate (v), veh/h							132			157	135	51	1756	870	82	1704	848
Adjusted Saturation Flow Rate (s), veh/h/ln							1599			1294	1427	125	1870	1827	1781	1870	1848
Queue Service Time (g s), s							0.0			4.9	9.7	34.9	32.5	33.5	1.4	22.8	23.1
Cycle Queue Clearance Time (g c), s							8.2			13.1	9.7	48.4	32.5	33.5	1.4	22.8	23.1
Green Ratio (g/C)							0.15			0.15	0.15	0.67	0.67	0.67	0.73	0.75	0.75
Capacity (c), veh/h							292			260	217	133	2489	1216	182	2815	1391
Volume-to-Capacity Ratio (X)							0.450			0.603	0.622	0.384	0.705	0.716	0.449	0.605	0.610
Back of Queue (Q), ft/ln (95 th percentile)							156			196	166	56	448	470	66	277	290
Back of Queue (Q), veh/ln (95 th percentile)							6.1			7.7	6.5	2.2	17.7	18.5	2.6	10.9	11.4
Queue Storage Ratio (RQ) (95 th percentile)							0.00			0.00	1.11	0.47	0.00	0.00	0.66	0.00	0.00
Uniform Delay (d 1), s/veh							43.0			45.3	43.7	19.5	11.6	11.8	19.9	6.2	6.2
Incremental Delay (d 2), s/veh							1.1			2.2	2.9	8.2	1.7	3.6	0.6	1.0	2.0
Initial Queue Delay (d 3), s/veh							0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh							44.0			47.5	46.6	27.6	13.3	15.4	20.5	7.2	8.2
Level of Service (LOS)							D			D	D	C	B	B	C	A	A
Approach Delay, s/veh / LOS						44.0	D	47.1	D	14.3	B	7.9	A				
Intersection Delay, s/veh / LOS						13.7					B						
Multimodal Results						EB			WB			NB			SB		
Pedestrian LOS Score / LOS																	
Bicycle LOS Score / LOS																	

HCM 7th TWSC
4: San Mateo & Jackalope

SanMateo_I25 SE, PM Peak
2035 Horizon BUILD, Timing Plan: 15

Intersection						
Int Delay, s/veh	1.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			  			  
Traffic Vol, veh/h	1	19	2520	14	30	2540
Future Vol, veh/h	1	19	2543	14	30	2559
Conflicting Peds, #/hr	0	1	0	4	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	90	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	20	2705	15	32	2722
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	3870	1365	0	0	2724	0
Stage 1	2717	-	-	-	-	-
Stage 2	1153	-	-	-	-	-
Critical Hdwy	5.74	7.14	-	-	5.34	-
Critical Hdwy Stg 1	6.64	-	-	-	-	-
Critical Hdwy Stg 2	6.04	-	-	-	-	-
Follow-up Hdwy	3.82	3.92	-	-	3.12	-
Pot Cap-1 Maneuver	*9	118	-	-	53	-
Stage 1	*19	-	-	-	-	-
Stage 2	*578	-	-	-	-	-
Platoon blocked, %	0		-	-		-
Mov Cap-1 Maneuver	*3	117	-	-	53	-
Mov Cap-2 Maneuver	*3	-	-	-	-	-
Stage 1	*19	-	-	-	-	-
Stage 2	*228	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s/v106.5		0		1.71		
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	3	117	53	-
HCM Lane V/C Ratio	-	-	0.314	0.173	0.606	-
HCM Control Delay (s/veh)	-	\$	1331.5	42	147.3	-
HCM Lane LOS	-	-	F	E	F	-
HCM 95th %tile Q(veh)	-	-	0.5	0.6	2.4	-
Notes						
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon

Intersection												
Int Delay, s/veh	16.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔	↔	↔	↔		↔	↔	
Traffic Vol, veh/h	0	0	28	0	0	89	18	2212	23	132	2434	4
Future Vol, veh/h	0	0	28	0	0	89	18	2235	23	132	2453	4
Conflicting Peds, #/hr	0	0	2	0	0	0	0	0	1	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	100	-	-	180	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	30	0	0	97	20	2429	25	143	2666	4





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3968	5452	1339	3837	5442	1228	2673	0	0	2455	0	0
Stage 1	2957	2957	-	2482	2482	-	-	-	-	-	-	-
Stage 2	1011	2494	-	1355	2960	-	-	-	-	-	-	-
Critical Hdwy	6.44	6.54	7.14	6.44	6.54	7.14	5.34	-	-	5.34	-	-
Critical Hdwy Stg 1	7.34	5.54	-	7.34	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.74	5.54	-	6.74	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.82	4.02	3.92	3.82	4.02	3.92	3.12	-	-	3.12	-	-
Pot Cap-1 Maneuver	3	0	*532	*5	0	146	154	-	-	~ 73	-	-
Stage 1	29	72	-	*17	58	-	-	-	-	-	-	-
Stage 2	232	57	-	*546	72	-	-	-	-	-	-	-
Platoon blocked, %	0	0	0	0	0	0	-	-	-	-	-	-
Mov Cap-1 Maneuver	1	0	*530	*4	0	146	154	-	-	~ 73	-	-
Mov Cap-2 Maneuver	1	0	-	*4	0	-	-	-	-	-	-	-
Stage 1	29	0	-	*15	51	-	-	-	-	-	-	-
Stage 2	68	50	-	*514	0	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v12.21		68.62	0.25	29.24
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	154	-	-	-	530	-	146	~ 73	-	-
HCM Lane V/C Ratio	0.127	-	-	-	0.057	-	0.664	1.967	-	-
HCM Control Delay (s/veh)	31.8	-	-	0	12.2	0	68.6	573.5	-	-
HCM Lane LOS	D	-	-	A	B	A	F	F	-	-
HCM 95th %tile Q(veh)	0.4	-	-	-	0.2	-	3.7	13	-	-

Notes												
~: Volume exceeds capacity		\$: Delay exceeds 300s		+: Computation Not Defined		*: All major volume in platoon						

Intersection	
Intersection Delay, s/veh	12.6
Intersection LOS	B

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	113	2	331	69	21	165
Future Vol, veh/h	113	2	338	69	21	171
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	124	2	371	76	23	188
Number of Lanes	1	0	1	1	1	0

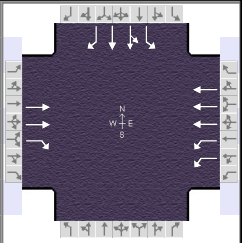
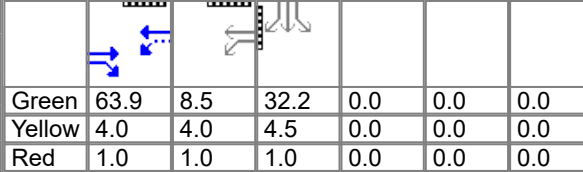
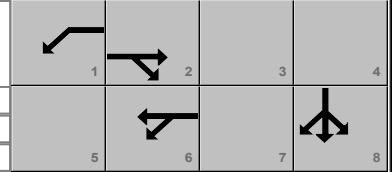
Approach	EB	WB	NB
Opposing Approach	WB	EB	
Opposing Lanes	2	1	0
Conflicting Approach Left		NB	EB
Conflicting Lanes Left	0	1	1
Conflicting Approach Right	NB		WB
Conflicting Lanes Right	1	0	2
HCM Control Delay, s/veh	9.3	14.9	9.8
HCM LOS	A	B	A

Lane	NBLn1	EBLn1	WBLn1	WBLn2
Vol Left, %	11%	0%	100%	0%
Vol Thru, %	0%	98%	0%	100%
Vol Right, %	89%	2%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	192	115	338	69
LT Vol	21	0	338	0
Through Vol	0	113	0	69
RT Vol	171	2	0	0
Lane Flow Rate	211	126	371	76
Geometry Grp	2	4a	5	5
Degree of Util (X)	0.284	0.179	0.584	0.109
Departure Headway (Hd)	4.852	5.1	5.661	5.158
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	736	696	635	689
Service Time	2.904	3.184	3.435	2.931
HCM Lane V/C Ratio	0.287	0.181	0.584	0.11
HCM Control Delay, s/veh	9.8	9.3	16.2	8.6
HCM Lane LOS	A	A	C	A
HCM 95th-tile Q	1.2	0.6	3.8	0.4

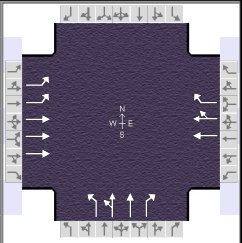
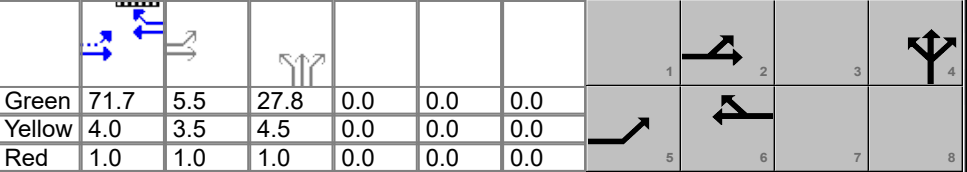
APPENDIX F

Mitigation Analysis LOS Worksheets

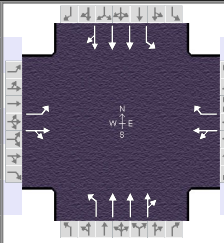
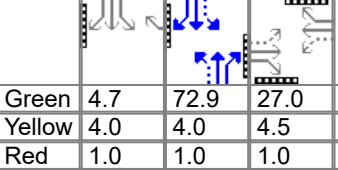
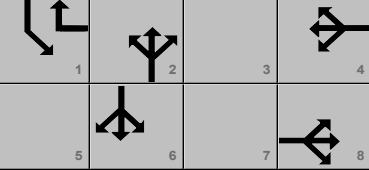
HCS Signalized Intersection Results Summary

General Information						Intersection Information											
Agency	Civil Transformations Inc.					Duration, h	0.250										
Analyst	CTI		Analysis Date	Aug 16, 2023		Area Type	Other										
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.92										
Urban Street	San Mateo		Analysis Year	2035		Analysis Period	1> 7:15										
Intersection	I25 SB Ramp		File Name	SM-I25_2035AMB_Mit1.xus													
Project Description	Horizon BUILD																
Demand Information						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h							1008	338	602	1384					399	630	447
Signal Information																	
Cycle, s	120.0	Reference Phase	2														
Offset, s	56	Reference Point	Begin														
Uncoordinated	No	Simult. Gap E/W	On														
Force Mode	Fixed	Simult. Gap N/S	On	Green	63.9	8.5	32.2	0.0	0.0	0.0							
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0							
				Red	1.0	1.0	1.0	0.0	0.0	0.0							
Timer Results						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase							2	1	6				8				
Case Number							7.4	1.0	4.0				9.0				
Phase Duration, s							68.9	13.5	82.3				37.7				
Change Period, (Y+R c), s							5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s							0.0	2.5	0.0				3.1				
Queue Clearance Time (g s), s								6.1					28.5				
Green Extension Time (g e), s							0.0	2.3	0.0				3.7				
Phase Call Probability								1.00					1.00				
Max Out Probability								0.78					0.00				
Movement Group Results						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement							2	12	1	6				3	8	18	
Adjusted Flow Rate (v), veh/h							1096	296	746	1715				260	858	325	
Adjusted Saturation Flow Rate (s), veh/h/ln							1890	1551	1489	1738				1781	1852	1558	
Queue Service Time (g s), s							7.7	3.8	4.1	9.2				15.0	26.5	23.1	
Cycle Queue Clearance Time (g c), s							7.7	3.8	4.1	9.2				15.0	26.5	23.1	
Green Ratio (g/C)							0.53	0.53	0.60	0.64				0.27	0.27	0.27	
Capacity (c), veh/h							2009	824	810	3358				478	993	418	
Volume-to-Capacity Ratio (X)							0.545	0.359	0.921	0.511				0.545	0.864	0.778	
Back of Queue (Q), ft/ln (95 th percentile)							92.9	54.4	295.9	69.7				272.6	449.8	349.9	
Back of Queue (Q), veh/ln (95 th percentile)							3.7	2.1	11.7	2.7				10.7	17.7	13.8	
Queue Storage Ratio (RQ) (95 th percentile)							0.00	0.00	0.91	0.00				1.36	0.00	1.56	
Uniform Delay (d 1), s/veh							3.7	3.4	31.4	3.3				37.6	41.8	40.6	
Incremental Delay (d 2), s/veh							1.1	1.2	2.1	0.1				0.4	0.9	1.2	
Initial Queue Delay (d 3), s/veh							0.0	0.0	0.0	0.0				0.0	0.0	0.0	
Control Delay (d), s/veh							4.7	4.6	33.6	3.4				38.0	42.7	41.8	
Level of Service (LOS)							A	A	C	A				D	D	D	
Approach Delay, s/veh / LOS						4.7	A	12.5	B	0.0			41.7	D			
Intersection Delay, s/veh / LOS						18.4					B						
Multimodal Results						EB			WB			NB			SB		
Pedestrian LOS Score / LOS																	
Bicycle LOS Score / LOS																	

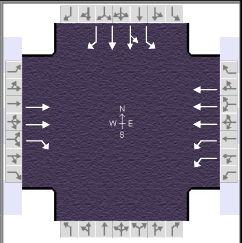
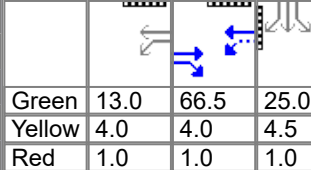
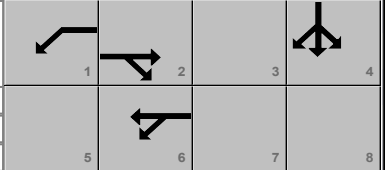
HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92									
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15									
Intersection		I25 NB Ramp		File Name		SM-I25_2035AMB_Mit1.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				312	1694			1885	736	544	260	314							
Signal Information																			
Cycle, s	120.0	Reference Phase	2		Green	71.7	5.5	27.8	0.0	0.0	0.0	1	2	3	4				
Offset, s	26	Reference Point	Begin		Yellow	4.0	3.5	4.5	0.0	0.0	0.0	5	6	7	8				
Uncoordinated	No	Simult. Gap E/W	On		Red	1.0	1.0	1.0	0.0	0.0	0.0								
Force Mode	Fixed	Simult. Gap N/S	On																
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase				5		2				6				4					
Case Number				1.0		4.0				7.4				9.0					
Phase Duration, s				10.0		86.7				76.7				33.3					
Change Period, (Y+R c), s				5.0		5.0				5.0				5.5					
Max Allow Headway (MAH), s				4.0		0.0				0.0				3.1					
Queue Clearance Time (g s), s				2.0										25.2					
Green Extension Time (g e), s				2.0		0.0				0.0				2.6					
Phase Call Probability				1.00										1.00					
Max Out Probability				1.00										0.00					
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				5	2			6	16	7	4	14							
Adjusted Flow Rate (v), veh/h				238	1291			2195	549	148	726	248							
Adjusted Saturation Flow Rate (s), veh/h/ln				1684	1698			1989	1614	1781	1808	1539							
Queue Service Time (g s), s				0.0	10.0			3.2	0.4	8.3	23.2	17.7							
Cycle Queue Clearance Time (g c), s				0.0	10.0			3.2	0.4	8.3	23.2	17.7							
Green Ratio (g/C)				0.64	0.68			0.60	0.60	0.23	0.23	0.23							
Capacity (c), veh/h				494	3469			2377	965	412	837	356							
Volume-to-Capacity Ratio (X)				0.481	0.372			0.923	0.569	0.358	0.867	0.696							
Back of Queue (Q), ft/ln (95 th percentile)				113.9	129.3			117.7	32.2	167	400.8	279.4							
Back of Queue (Q), veh/ln (95 th percentile)				4.5	5.1			4.6	1.3	6.6	15.8	11.0							
Queue Storage Ratio (RQ) (95 th percentile)				0.46	0.00			0.00	0.00	0.56	0.00	0.80							
Uniform Delay (d 1), s/veh				19.9	5.7			0.1	0.1	38.6	44.3	42.2							
Incremental Delay (d 2), s/veh				0.5	0.2			7.5	2.4	0.2	2.1	0.9							
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0							
Control Delay (d), s/veh				20.4	5.9			7.6	2.5	38.8	46.5	43.2							
Level of Service (LOS)				C	A			A	A	D	D	D							
Approach Delay, s/veh / LOS				8.2		A		6.6		A		44.7		D		0.0			
Intersection Delay, s/veh / LOS				15.0						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

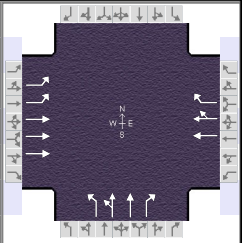
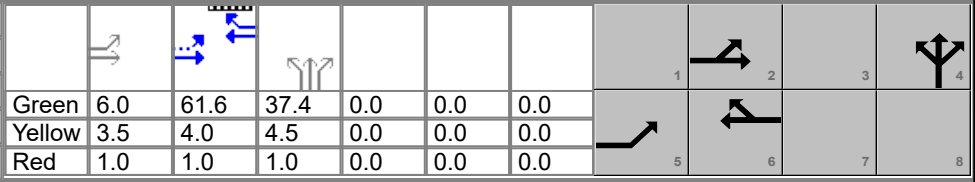
HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type					Other						
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF					0.92						
Urban Street		San Mateo		Analysis Year		2035		Analysis Period					1> 7:00						
Intersection		PanAm		File Name		SM-I25_2035AMB-Node3_Mit1.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				61	13	47	133	11	181	47	2310	106	75	2295	53				
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	47	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	4.7	72.9	27.0	0.0	0.0	0.0									
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						6.0				7.0				6.3		1.0		4.0	
Phase Duration, s						32.5				32.5				77.9		9.7		87.5	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.4				4.4				0.0		2.5		0.0	
Queue Clearance Time (g s), s						24.6				18.2						4.0			
Green Extension Time (g e), s						1.6				1.7				0.0		0.1		0.0	
Phase Call Probability						1.00				1.00						0.93			
Max Out Probability						0.00				0.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h				66	65			157	135	51	1756	870	82	1704	848				
Adjusted Saturation Flow Rate (s), veh/h/ln				1402	1639			1181	1427	125	1870	1827	1781	1870	1848				
Queue Service Time (g s), s				5.4	3.8			12.4	9.1	49.2	42.4	43.6	2.0	32.0	32.4				
Cycle Queue Clearance Time (g c), s				22.6	3.8			16.2	9.1	72.1	42.4	43.6	2.0	32.0	32.4				
Green Ratio (g/C)				0.22	0.22			0.22	0.26	0.61	0.61	0.61	0.66	0.69	0.69				
Capacity (c), veh/h				184	379			331	385	111	2247	1097	157	2548	1259				
Volume-to-Capacity Ratio (X)				0.360	0.172			0.473	0.350	0.460	0.781	0.793	0.521	0.669	0.673				
Back of Queue (Q), ft/ln (95 th percentile)				90.9	72.3			196.9	150	83.2	615.8	646.4	65.3	435.9	452.3				
Back of Queue (Q), veh/ln (95 th percentile)				3.6	2.8			7.8	5.9	3.3	24.2	25.5	2.6	17.2	17.8				
Queue Storage Ratio (RQ) (95 th percentile)				0.00	0.00			0.00	1.00	0.69	0.00	0.00	0.65	0.00	0.00				
Uniform Delay (d 1), s/veh				52.3	36.9			43.4	35.3	35.5	18.0	18.3	25.6	11.2	11.3				
Incremental Delay (d 2), s/veh				1.2	0.2			1.0	0.5	13.1	2.8	5.9	1.0	1.4	2.9				
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh				53.5	37.1			44.4	35.8	48.6	20.8	24.2	26.6	12.6	14.2				
Level of Service (LOS)				D	D			D	D	D	C	C	C	B	B				
Approach Delay, s/veh / LOS				45.4		D		40.4		D		22.4		C		13.5		B	
Intersection Delay, s/veh / LOS				19.8						B									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

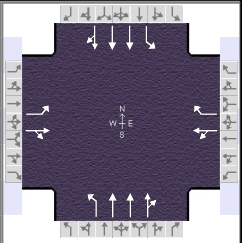
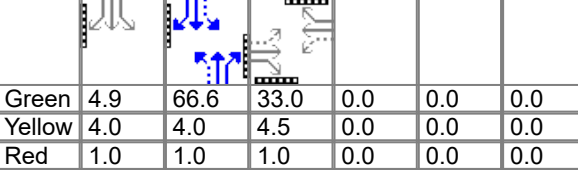
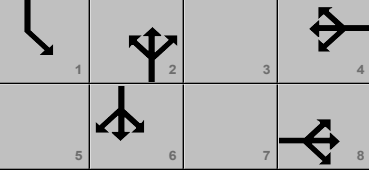
HCS Signalized Intersection Results Summary

General Information						Intersection Information															
Agency		Civil Transformations Inc.						Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other											
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.95											
Urban Street		Osuna		Analysis Year		2035		Analysis Period		1> 7:15											
Intersection		I25 SB Ramp		File Name		SM-I25_2035PMB_Mit1.xus															
Project Description		Horizon BUILD																			
Demand Information				EB			WB			NB			SB								
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R						
Demand (v), veh/h					1296	355	507	1038					287	329	234						
Signal Information																					
Cycle, s	120.0	Reference Phase	2																		
Offset, s	61	Reference Point	Begin																		
Uncoordinated	No	Simult. Gap E/W	On																		
Force Mode	Fixed	Simult. Gap N/S	On																		
Green	13.0	66.5	25.0	0.0	0.0	0.0															
Yellow	4.0	4.0	4.5	0.0	0.0	0.0															
Red	1.0	1.0	1.0	0.0	0.0	0.0															
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT										
Assigned Phase					2	1	6				4										
Case Number					7.3	1.0	4.0				9.0										
Phase Duration, s					71.5	18.0	89.5				30.5										
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5										
Max Allow Headway (MAH), s					0.0	2.6	0.0				3.1										
Queue Clearance Time (g s), s						12.1					16.6										
Green Extension Time (g e), s					0.0	0.9	0.0				1.4										
Phase Call Probability						1.00					1.00										
Max Out Probability						0.00					0.09										
Movement Group Results				EB			WB			NB			SB								
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R						
Assigned Movement					2	12	1	6				7	4	14							
Adjusted Flow Rate (v), veh/h					1364	311	624	1277				166	482	155							
Adjusted Saturation Flow Rate (s), veh/h/ln					1922	1554	1411	1697				1781	1805	1517							
Queue Service Time (g s), s					8.0	2.8	10.1	13.2				9.8	14.6	10.8							
Cycle Queue Clearance Time (g c), s					8.0	2.8	10.1	13.2				9.8	14.6	10.8							
Green Ratio (g/C)					0.55	0.55	0.66	0.70				0.21	0.21	0.21							
Capacity (c), veh/h					2129	861	815	3584				371	752	316							
Volume-to-Capacity Ratio (X)					0.641	0.361	0.766	0.356				0.448	0.641	0.490							
Back of Queue (Q), ft/ln (95 th percentile)					84.9	41.5	158.2	181.5				195.5	275.6	185.1							
Back of Queue (Q), veh/ln (95 th percentile)					3.3	1.6	6.2	7.1				7.7	10.8	7.3							
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.99	0.00				1.30	0.00	0.82							
Uniform Delay (d 1), s/veh					2.4	2.2	11.8	8.1				41.5	43.4	41.9							
Incremental Delay (d 2), s/veh					1.5	1.2	1.1	0.2				0.3	1.4	0.4							
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0				0.0	0.0	0.0							
Control Delay (d), s/veh					3.9	3.3	12.9	8.3				41.8	44.8	42.3							
Level of Service (LOS)					A	A	B	A				D	D	D							
Approach Delay, s/veh / LOS				3.8	A		9.8	A		0.0			43.7	D							
Intersection Delay, s/veh / LOS				13.7						B											
Multimodal Results				EB			WB			NB			SB								
Pedestrian LOS Score / LOS																					
Bicycle LOS Score / LOS																					

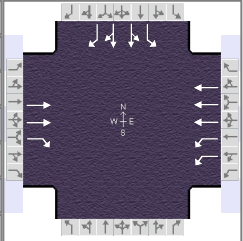
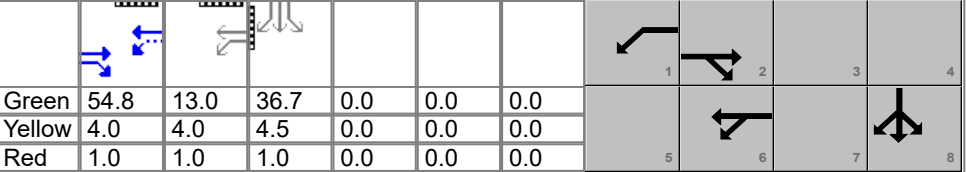
HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other									
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF		0.96									
Urban Street		Osuna		Analysis Year		2035		Analysis Period		1> 7:15									
Intersection		I25 NB Ramp		File Name		SM-I25_2035PMB_Mit1.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				372	2547			1427	1262	398	495	431							
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	55	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	6.0	61.6	37.4	0.0	0.0	0.0									
				Yellow	3.5	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase				5		2				6				4					
Case Number				1.0		4.0				7.3				9.0					
Phase Duration, s				10.5		77.1				66.6				42.9					
Change Period, (Y+R c), s				4.5		5.0				5.0				5.5					
Max Allow Headway (MAH), s				4.0		0.0				0.0				3.1					
Queue Clearance Time (g s), s				5.3										34.4					
Green Extension Time (g e), s				0.7		0.0				0.0				3.0					
Phase Call Probability				1.00										1.00					
Max Out Probability				0.00										0.05					
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				5	2			6	16	7	4	14							
Adjusted Flow Rate (v), veh/h				212	1454			1891	822	195	735	400							
Adjusted Saturation Flow Rate (s), veh/h/ln				1781	1713			1857	1692	1781	1830	1419							
Queue Service Time (g s), s				3.3	17.4			58.3	44.2	10.1	20.8	32.4							
Cycle Queue Clearance Time (g c), s				3.3	17.4			58.3	44.2	10.1	20.8	32.4							
Green Ratio (g/C)				0.56	0.60			0.51	0.51	0.31	0.31	0.31							
Capacity (c), veh/h				316	3087			1907	869	555	1141	443							
Volume-to-Capacity Ratio (X)				0.671	0.471			0.992	0.946	0.351	0.644	0.904							
Back of Queue (Q), ft/ln (95 th percentile)				72.7	228.2			358.3	334.7	197.6	358.6	468.1							
Back of Queue (Q), veh/ln (95 th percentile)				2.9	9.0			14.1	13.2	7.8	14.1	18.4							
Queue Storage Ratio (RQ) (95 th percentile)				0.97	0.00			0.00	0.00	0.66	0.00	1.34							
Uniform Delay (d 1), s/veh				29.3	11.6			8.4	7.4	31.9	35.6	39.6							
Incremental Delay (d 2), s/veh				1.7	0.4			18.8	20.0	0.1	0.2	12.7							
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0							
Control Delay (d), s/veh				31.0	12.0			27.2	27.4	32.0	35.8	52.3							
Level of Service (LOS)				C	B			C	C	C	D	D							
Approach Delay, s/veh / LOS				14.4		B		27.3		C		40.2		D		0.0			
Intersection Delay, s/veh / LOS				26.5						C									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

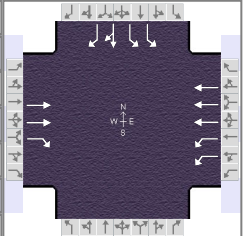
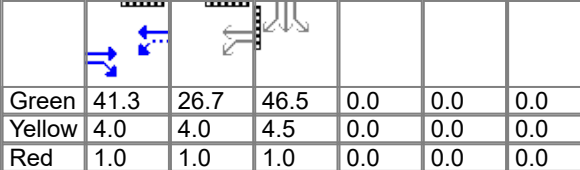
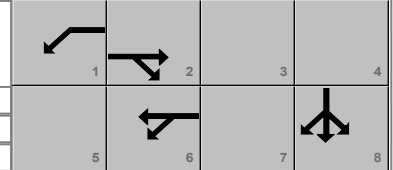
HCS Signalized Intersection Results Summary

General Information						Intersection Information													
Agency		Civil Transformations Inc.				Duration, h		0.250											
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type					Other						
Jurisdiction		NMDOT & COA		Time Period		PM Peak		PHF					0.93						
Urban Street		San Mateo		Analysis Year		2035		Analysis Period					1> 16:30						
Intersection		PanAm		File Name		SM-I25_2035PMB-Node3_Mit1.xus													
Project Description		Horizon BUILD																	
Demand Information				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Demand (v), veh/h				80	12	27	103	14	226	64	2219	83	94	1480	68				
Signal Information																			
Cycle, s	120.0	Reference Phase	2																
Offset, s	58	Reference Point	Begin																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
				Green	4.9	66.6	33.0	0.0	0.0	0.0									
				Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
				Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL		EBT		WBL		WBT		NBL		NBT		SBL		SBT	
Assigned Phase						8				4				2		1		6	
Case Number						6.0				7.0				6.3		1.0		4.0	
Phase Duration, s						38.5				38.5				71.6		9.9		81.5	
Change Period, (Y+R c), s						5.5				5.5				5.0		5.0		5.0	
Max Allow Headway (MAH), s						4.1				4.1				0.0		4.0		0.0	
Queue Clearance Time (g s), s						18.5				14.0						4.8			
Green Extension Time (g e), s						1.6				1.6				0.0		0.3		0.0	
Phase Call Probability						1.00				1.00						0.97			
Max Out Probability						0.00				0.00						0.00			
Movement Group Results				EB			WB			NB			SB						
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R				
Assigned Movement				3	8	18	7	4	14	5	2	12	1	6	16				
Adjusted Flow Rate (v), veh/h				86	38			126	192	69	1653	818	101	1118	546				
Adjusted Saturation Flow Rate (s), veh/h/ln				1398	1673			1346	1585	299	1870	1836	1781	1870	1826				
Queue Service Time (g s), s				6.4	2.0			8.3	12.0	18.6	42.3	43.0	2.8	18.6	18.6				
Cycle Queue Clearance Time (g c), s				16.5	2.0			10.3	12.0	27.2	42.3	43.0	2.8	18.6	18.6				
Green Ratio (g/C)				0.28	0.28			0.28	0.28	0.55	0.55	0.55	0.61	0.64	0.64				
Capacity (c), veh/h				326	460			427	436	204	2074	1018	160	2385	1164				
Volume-to-Capacity Ratio (X)				0.264	0.082			0.295	0.442	0.337	0.797	0.804	0.632	0.469	0.469				
Back of Queue (Q), ft/ln (95 th percentile)				102.6	38			140.3	212.8	68.3	633.2	661.3	79.8	288.8	291.4				
Back of Queue (Q), veh/ln (95 th percentile)				4.0	1.5			5.5	8.4	2.7	24.9	26.0	3.1	11.4	11.5				
Queue Storage Ratio (RQ) (95 th percentile)				0.00	0.00			0.00	1.42	0.57	0.00	0.00	0.80	0.00	0.00				
Uniform Delay (d 1), s/veh				41.9	32.3			35.9	35.9	20.9	21.3	21.5	26.1	11.2	11.3				
Incremental Delay (d 2), s/veh				0.2	0.0			0.4	0.7	4.4	3.3	6.7	4.1	0.7	1.4				
Initial Queue Delay (d 3), s/veh				0.0	0.0			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Control Delay (d), s/veh				42.1	32.3			36.2	36.6	25.3	24.6	28.2	30.1	11.9	12.6				
Level of Service (LOS)				D	C			D	D	C	C	C	C	B	B				
Approach Delay, s/veh / LOS				39.1		D		36.5		D		25.8		C		13.2		B	
Intersection Delay, s/veh / LOS				22.2						C									
Multimodal Results				EB			WB			NB			SB						
Pedestrian LOS Score / LOS																			
Bicycle LOS Score / LOS																			

HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency	Civil Transformations Inc.					Duration, h	0.250								
Analyst	CTI	Analysis Date	Aug 16, 2023			Area Type	Other								
Jurisdiction	NMDOT & COA		Time Period	AM Peak		PHF	0.92								
Urban Street	San Mateo		Analysis Year	2035		Analysis Period	1> 7:15								
Intersection	I25 SB Ramp		File Name	SM-I25_2035AMB_Mit2.xus											
Project Description	Horizon BUILD - Mitigation 2														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1008	338	602	1384					399	630	447
Signal Information															
Cycle, s	120.0	Reference Phase	2		Green	54.8	13.0	36.7	0.0	0.0	0.0				
Offset, s	56	Reference Point	Begin		Yellow	4.0	4.0	4.5	0.0	0.0	0.0				
Uncoordinated	No	Simult. Gap E/W	On		Red	1.0	1.0	1.0	0.0	0.0	0.0				
Force Mode	Float	Simult. Gap N/S	On												
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				8				
Case Number					7.4	1.0	4.0				9.0				
Phase Duration, s					59.8	18.0	77.8				42.2				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1				
Queue Clearance Time (g s), s						13.1					33.0				
Green Extension Time (g e), s					0.0	0.0	0.0				3.7				
Phase Call Probability						1.00					1.00				
Max Out Probability						1.00					0.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					3	8	18
Adjusted Flow Rate (v), veh/h					1096	296	746	1715					260	988	195
Adjusted Saturation Flow Rate (s), veh/h/ln					1890	1551	1489	1738					1781	1823	1526
Queue Service Time (g s), s					16.1	8.0	11.1	9.9					14.2	31.0	12.2
Cycle Queue Clearance Time (g c), s					16.1	8.0	11.1	9.9					14.2	31.0	12.2
Green Ratio (g/C)					0.46	0.46	0.56	0.61					0.31	0.31	0.31
Capacity (c), veh/h					1726	708	775	3162					545	1115	467
Volume-to-Capacity Ratio (X)					0.635	0.418	0.963	0.543					0.477	0.886	0.417
Back of Queue (Q), ft/ln (95 th percentile)					201	111	286	74					259	503	203
Back of Queue (Q), veh/ln (95 th percentile)					7.9	4.4	11.3	2.9					10.2	19.8	8.0
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.88	0.00					1.30	0.00	0.90
Uniform Delay (d 1), s/veh					9.7	8.8	36.9	3.8					33.8	39.6	33.1
Incremental Delay (d 2), s/veh					1.8	1.8	4.8	0.1					0.2	1.0	0.2
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					11.5	10.6	41.7	3.9					34.1	40.6	33.4
Level of Service (LOS)					B	B	D	A					C	D	C
Approach Delay, s/veh / LOS				11.3	B	15.4	B	0.0			38.5	D			
Intersection Delay, s/veh / LOS				20.6					C						
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

HCS Signalized Intersection Results Summary

General Information						Intersection Information									
Agency		Civil Transformations Inc.				Duration, h		0.250							
Analyst		CTI		Analysis Date		Aug 16, 2023		Area Type		Other					
Jurisdiction		NMDOT & COA		Time Period		AM Peak		PHF		0.92					
Urban Street		San Mateo		Analysis Year		2035		Analysis Period		1> 7:15					
Intersection		I25 SB Ramp		File Name		SM-I25_2035AMB_Mit3.xus									
Project Description		Horizon BUILD - Mitigation 2													
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h					1008	338	602	1384					399	630	447
Signal Information															
Cycle, s	130.0	Reference Phase	2												
Offset, s	56	Reference Point	Begin												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Float	Simult. Gap N/S	On												
Green	41.3	26.7	46.5	0.0	0.0	0.0									
Yellow	4.0	4.0	4.5	0.0	0.0	0.0									
Red	1.0	1.0	1.0	0.0	0.0	0.0									
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					2	1	6				8				
Case Number					7.4	1.0	4.0				9.0				
Phase Duration, s					46.3	31.7	78.0				52.0				
Change Period, (Y+R c), s					5.0	5.0	5.0				5.5				
Max Allow Headway (MAH), s					0.0	2.5	0.0				3.1				
Queue Clearance Time (g s), s						25.4					48.5				
Green Extension Time (g e), s					0.0	1.6	0.0				0.0				
Phase Call Probability						1.00					1.00				
Max Out Probability						0.02					1.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement					2	12	1	6					3	8	18
Adjusted Flow Rate (v), veh/h					1096	296	745	1713					434	815	195
Adjusted Saturation Flow Rate (s), veh/h/ln					1905	1557	1489	1751					1730	1940	1530
Queue Service Time (g s), s					33.6	16.9	23.4	16.1					12.0	46.5	12.2
Cycle Queue Clearance Time (g c), s					33.6	16.9	23.4	16.1					12.0	46.5	12.2
Green Ratio (g/C)					0.32	0.32	0.53	0.56					0.36	0.36	0.36
Capacity (c), veh/h					1218	498	788	2950					1237	694	547
Volume-to-Capacity Ratio (X)					0.900	0.594	0.946	0.581					0.351	1.174	0.356
Back of Queue (Q), ft/ln (95 th percentile)					490	241	296	130					220	1412	203
Back of Queue (Q), veh/ln (95 th percentile)					19.3	9.5	11.6	5.1					8.6	55.6	8.0
Queue Storage Ratio (RQ) (95 th percentile)					0.00	0.00	0.91	0.00					1.10	0.00	0.90
Uniform Delay (d 1), s/veh					28.5	24.6	40.0	7.5					30.7	41.8	30.7
Incremental Delay (d 2), s/veh					10.7	5.2	1.1	0.1					0.1	93.1	0.1
Initial Queue Delay (d 3), s/veh					0.0	0.0	0.0	0.0					0.0	0.0	0.0
Control Delay (d), s/veh					39.2	29.8	41.1	7.6					30.7	134.9	30.9
Level of Service (LOS)					D	C	D	A					C	F	C
Approach Delay, s/veh / LOS				37.2	D		17.7	B		0.0			89.5	F	
Intersection Delay, s/veh / LOS				42.4						D					
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS															
Bicycle LOS Score / LOS															

APPENDIX G

Site Traffic Proportional Calculations

I-25 & San Mateo SE Commercial Development

Projected share of traffic at the Osuna/I-25 SBFR Intersection

Source: Traffic operations analyses for Scenario 5 (Horizon Year Build).

EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	TOTAL	Remarks
AM PEAK													
	1002	338	593	1357					387	630	447	4754	2035 AM Peak (w/growth)
	6		9	27					12			54	Site Traffic
0	1008	338	602	1384	0	0	0	0	399	630	447	4808	Total
												1%	AM Site traffic proportion
PM PEAK													
	1291	355	501	1034					278	329	234	4022	2035 PM Peak (w/growth)
	5		6	4			63		9			87	2040 Site Traffic
0	1296	355	507	1038	0	0	63	0	287	329	234	4109	Total
												2%	PM Site traffic proportion
												2% Average	