

CITY OF ALBUQUERQUE

Planning Department
Alan Varela, Director



Mayor Timothy M. Keller

April 4, 2025

Kenneth Myers, R.A
REMBE Design
1716 Central Ave. SW
Albuquerque, NM 87104

**Re: Griegos Farm/ 4521 Guadalupe Church Rd. NW PHASE 3
Request for Certificate of Occupancy
Transportation Development Final Inspection**
Architect's Stamp dated 06-14-22 (F13D028A)
Certification dated 03-31-25

Dear Mr. Myers,

Based upon the information provided in your submittal received 04-02-25, Transportation Development has no objection to the issuance of a Permanent Certificate of Occupancy. This letter serves as a "green tag" from Transportation Development for a Permanent Certificate of Occupancy to be issued by the Building and Safety Division. **THE CONSTRUCTION OF THE MISSING PORTION OF THE SIDEWALK AT THE SOUTHEAST SIDE OF THE SITE IS A CONDITION FOR RELEASING THE FINAL CO FOR THE FINAL PHASE.**

PO Box 1293

Albuquerque

If you have any questions, please contact Ernest Armijo at (505) 924-3991 or at earmijo@cabq.gov

NM 87103

Sincerely,

www.cabq.gov

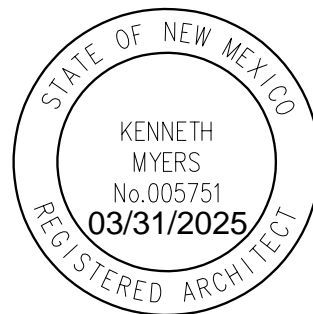
Ernest Armijo, P.E.
Principal Engineer, Planning Dept.
Development Review Services

\xxx via: email
C: CO Clerk, File

TRAFFIC CERTIFICATION

I, KENNETH MYERS, ARCHITECT FOR GRIEGOS FARMS, LLC, HEREBY CERTIFY THAT TO THE BEST OF MY KNOWLEDGE, AND BELIEF THIS PROJECT IS IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH THE DESIGN INTENT OF THE APPROVED PLAN DATED 07/22/2022 JULY TWENTY SECOND TWO THOUSAND TWENTY TWO. AS-BUILT CONDITIONS AND A SUBSTANTIAL COMPLETION BOUNDARY HAVE BEEN NOTED ON THE APPROVED TCL. I, KENNETH MYERS, HAVE VISITED THE PROJECT SITE ON 03/31/2025 AND HAVE DETERMINED BY VISUAL INSPECTION THAT THE RECORD INFORMATION IS REPRESENTATIVE OF THE ACTUAL SITE CONDITIONS WITHIN THE REVISED PHASING BOUNDARY AS NOTED ON THE REDLINED TCL PLAN. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR CERTIFICATION OF OCCUPANCY FOR THAT PORTION OF THE PROJECT DEFINED BY THE PHASE ONE BOUNDARY LINE INCLUDED ON THE REDLINED COPY OF THE APPROVED TCL.

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE TRAFFIC ASPECTS OF THIS PROJECT. THOSE RELYING ON THE RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.



SIGNATURE OF ENGINEER OR ARCHITECT

ENGINEERS OR ARCHITECTS STAMP

SITE INFORMATION

ADDRESS: PROPERTY NORTH AND WEST OF 1860 GRIEGOS, ALBUQUERQUE, NM 87107
 ZONE: ATLAS MAP; F-13-Z
 ZONING: R-A
 DEVELOPMENT TYPE: COTTAGE COURT, IDO 4-3(B)(3)
 PROJECT SIZE: 10 ACRE +/- PARCEL SUBDIVIDED AS 8 LOTS EACH BETWEEN 1 AND 2 ACRES.
 SETBACKS:
 FRONT: 20'-0"
 REAR: 25'-0"
 SIDE: 10'-0"

PROJECT DESCRIPTION

LANDSCAPE FORWARD COTTAGE COURT DEVELOPMENT W/ 90 UNITS RANGING IN SIZE FROM 655 SF. TO 1070 SF. DISTRIBUTED BETWEEN 8 LOTS (SEE UNIT COUNT NOTES THIS SHEET). PROJECT WILL INCLUDE A COMMUNITY BUILDING TO BE DETERMINED.

PARKING REQUIREMENTS

DWELLING, COTTAGE DEVELOPMENT- IDO TABLE 5-5-1:
 1 SPACE PER UNIT + 2 VISITORS= 90 + 2 SPACES PER 8 LOTS= 106 SPACES
 RESIDENTIAL COMMUNITY AMENITY, IDO TABLE 5-5-1:
 3 SPACES/1,000 GFA= 15 SPACES REQUIRED (BASED ON CURRENT COMMUNITY BLD. DESIGN=5,000 SF)
 TOTAL OFF STREET PARKING:
 REQUIRED: 121 SPACES
 PROVIDED: 139 SPACES
 HC PARKING, IBC TABLE 1106.1:
 101-150 PROVIDED SPACES=5 SPACES REQUIRED (INCLUDED IN OVERALL REQUIRED CALCULATIONS)
 PROVIDED:
 MOTORCYCLE PARKING, IDO TABLE 5-5-4:
 101-150 REQUIRED SPACES= 4 MOTORCYCLE SPACES PROVIDED=4 SPACES
 BICYCLE PARKING, IDO TABLE 5-5-5:
 RESIDENTIAL USE (MORE THAN 5 UNITS)= 3 SPACES OR 10% REQUIRED OFF-STREET PARKING
 121 REQUIRED OFF STREET PARKING=13 BIKE SPACES REQUIRED PROVIDED 33 SPACES

KEYED NOTES-SITE PLAN

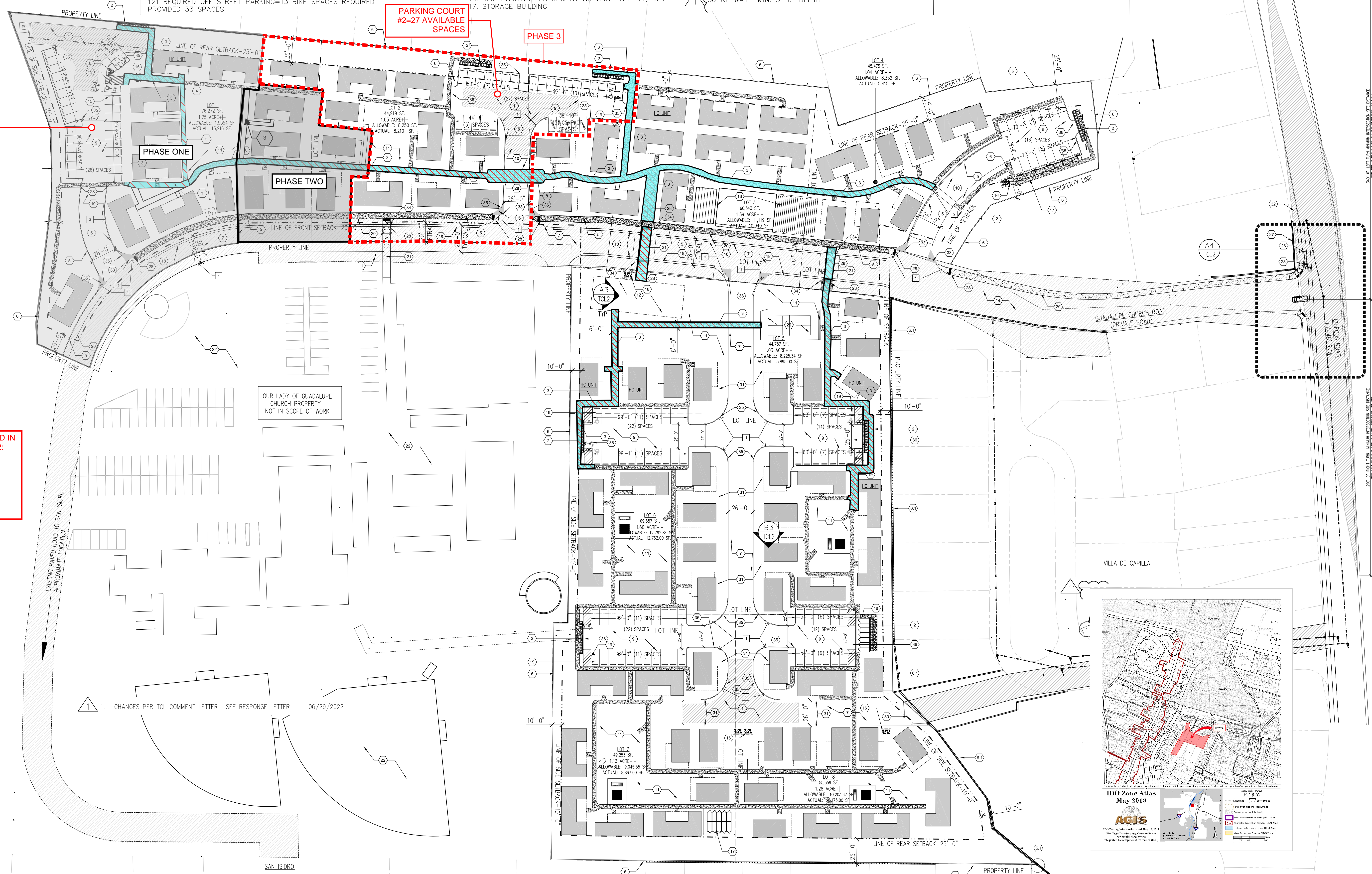
- FIRE ACCESS ALTERNATIVE HAMMERHEAD
- LOCKABLE TRASH CONTAINERS
- CONCRETE PEDESTRIAN WALKWAY UNLESS OTHERWISE NOTED
- FLUSH PAVEMENT TRANSITION
- SITE WALL- MAXIMUM HEIGHT ABOVE FINISH ROAD 3'-0"
- +/- 6'-0" TALL ADOBE WALL/ FENCE
- EXISTING 6'-0" ADOBE WALL TO REMAIN
- ASPHALT ROAD WAY PER COA STD. DTL.#2405A W/ ESTATE STYLE CURB COA STD. DTL.#2406 WHERE APPLICABLE
- WATER FEATURE
- PARKING LOT- COMPACTED AND STABILIZED GRAVEL- SEE DETAIL D2/TCL2 FOR DETAILS- NOTE ALL PARKING SPACES TO INCLUDE PARKING BLOCKS
- PARKING LOT DRIVEWAYS- COMPACTED AND STABILIZED GRAVEL DRIVE
- LANDSCAPED COURTYARD- FINAL LANDSCAPING TBD
- APPROXIMATE LOCATION OF AMENITIES BUILDING- TBD
- VINEYARD AND LAWN AREA- FINAL LAYOUT TBD
- EXISTING PRIVATE ROADWAY AT ACCESS EASEMENT
- MOTORCYCLE SPACE PER DPM STANDARDS-SEE D1/TCL2
- BIKE PARKING PER DPM STANDARDS- SEE D4/TCL2
- STORAGE BUILDING
- FIRE EXTINGUISHER PER COA STANDARD DRAWING #2406
- HC PARKING SPACE PER DPM STANDARDS SEE D1/TCL2
- 6'-0" SIDEWALK STANDARD DWG. 2430- CONSTRUCT PER DPM REQ'D
- RAISED PEDESTRIAN CROSSING/ SPEED TABLE- SEE DETAIL A1/TCL2
- EXISTING CHURCH CAMPUS AND PARKING TO REMAIN-NOT IN SCOPE OF WORK
- EXISTING BIKE LANE TO REMAIN
- NOT USED
- NOT USED
- NEW CONCRETE CURB EXTENSION
- EXISTING WALL- NOT IN SCOPE
- DETECTABLE WARNING SURFACE- TRUNCATED DOME OR OTHER APPROVED SURFACE- INSTALL PER COA STANDARD DRAWING #2446
- PICKLE BALL COURT- TBD
- 26'-0" OPERABLE GATE- FOR FIRE ACCESS ONLY
- NOT USED
- EXISTING SIDEWALK TO REMAIN
- MINI SITE TRIANGLE PER DPM 7-4(i)(5)(V)
- PEDESTRIAN CROSSING SIGN- SEE DETAIL D3/TCL2
- CURB STOP PER DETAIL C4/TCL2
- KEYWAY- MIN. 5'-0" DEPTH

RADIUS LEGEND

- 28'-0" RADIUS
 - 178'-0" RADIUS
 - 182'-0" RADIUS
 - 176'-0" RADIUS
- LEGEND**
- EXISTING EASEMENTS TO REMAIN
 - CONCRETE WALKWAYS- SEE KEYED NOTES AND PLAN
 - GRAVEL FINISH- SEE KEYED NOTES
 - INTERSECTION SITE TRIANGLE
 - ADA ACCESSIBLE PATHWAY (6'-0" WIDE)

TCL GENERAL NOTES

- PARKING AREA TO SLOPE A MIN. OF 1% AND A MAXIMUM OF 8% PER COA DPM
- ALL NEW PUBLIC SIDEWALK PER COA STANDARD DRAWING 2430
- ALL BROKEN OR CRACKED SIDEWALK MUST BE REPLACED WITH SIDEWALK AND CURB & GUTTER. - SEE NOTE 2
- LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
- NOTE NO ACCESSIBLE PATHWAY IS TO EXCEED 1:20 SLOPE
- ALL IMPROVEMENTS LOCATED IN THE RIGHT OF WAY MUST BE INCLUDED ON THE WORK ORDER
- ALL RAMP, CURBS, AND ROAD WORK ALONG GUADALUPE CHURCH ROAD AND AT GRIEGOS GUADALUPE CHURCH ROAD TO BE DONE THROUGH A WORK ORDER WITH THE CITY OF ALBUQUERQUE



PARKING COURT #1=26 AVAILABLE SPACES

PARKING COURT #2=27 AVAILABLE SPACES

TRAFFIC CIRCULATION LAYOUT APPROVED
 Ernest Arriaga 7/22/2022
 Signed Date

NOTE: PARKING FOR PHASE 3 IS PROVIDED IN PARKING COURT 1, AND PARKING COURT 2.
 PHASE 1= 9 UNITS-REQUIRED 9 SPACES
 PHASE 2= 6 UNITS- REQUIRED 6 SPACES
 PHASE 3= 9 UNITS- REQUIRED 9 SPACES

TOTAL REQUIRED 18 SPACES
 TOTAL PROVIDED 53 SPACES

1. CHANGES PER TCL COMMENT LETTER- SEE RESPONSE LETTER 06/29/2022

A1 SITE PLAN
 1"=50'-0"



A4 VICINITY MAP-ZONE ATLAS MAP
 1"=50'-0"

1860 GRIEGOS RD. NW
 ALBUQUERQUE, NM 87107

GRIEGOS FARMS

Architect Stamp:
 STATE OF NEW MEXICO
 KENNETH MYERS
 No.005751
 REGISTERED ARCHITECT
 06/14/2022

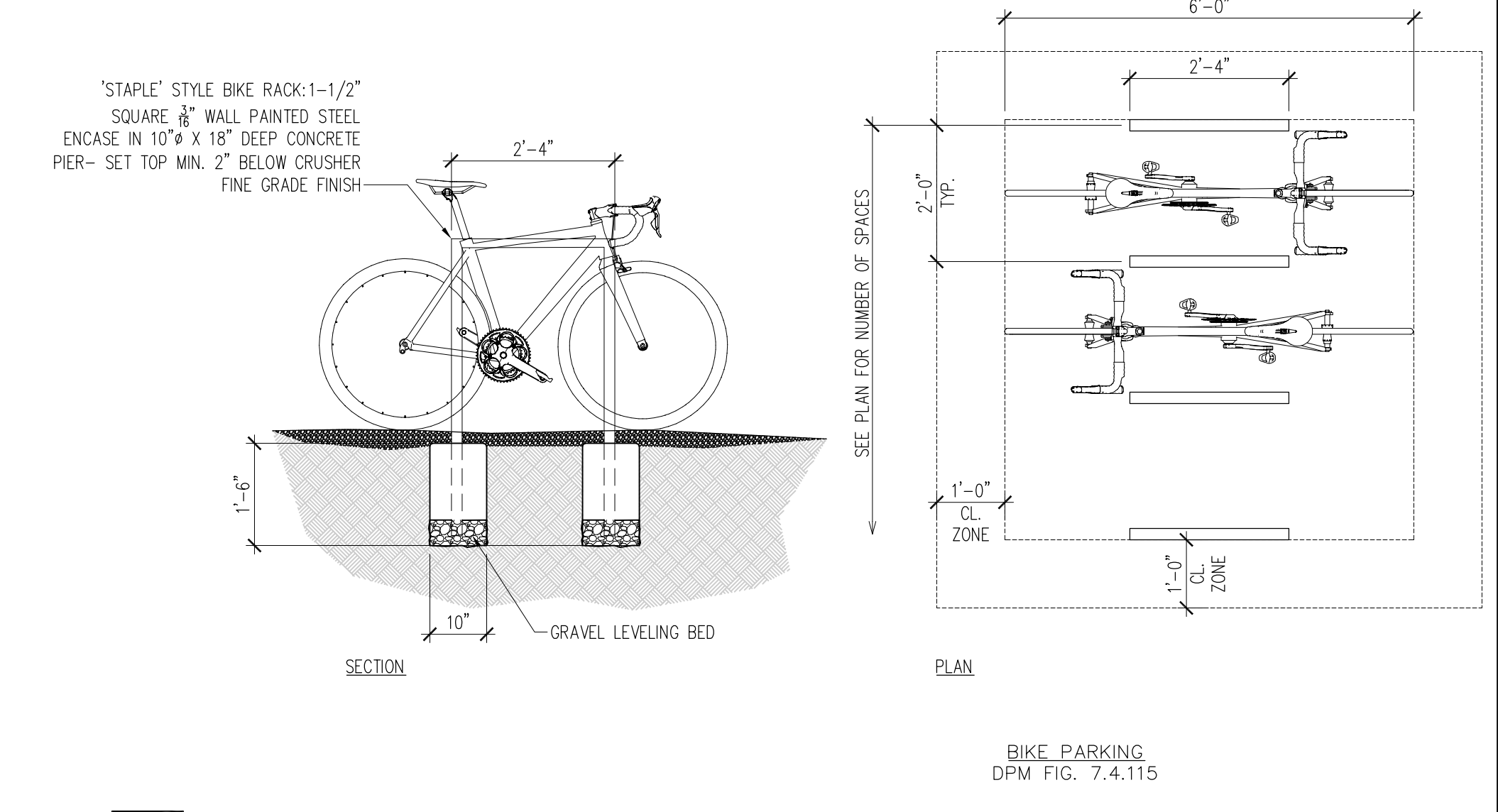
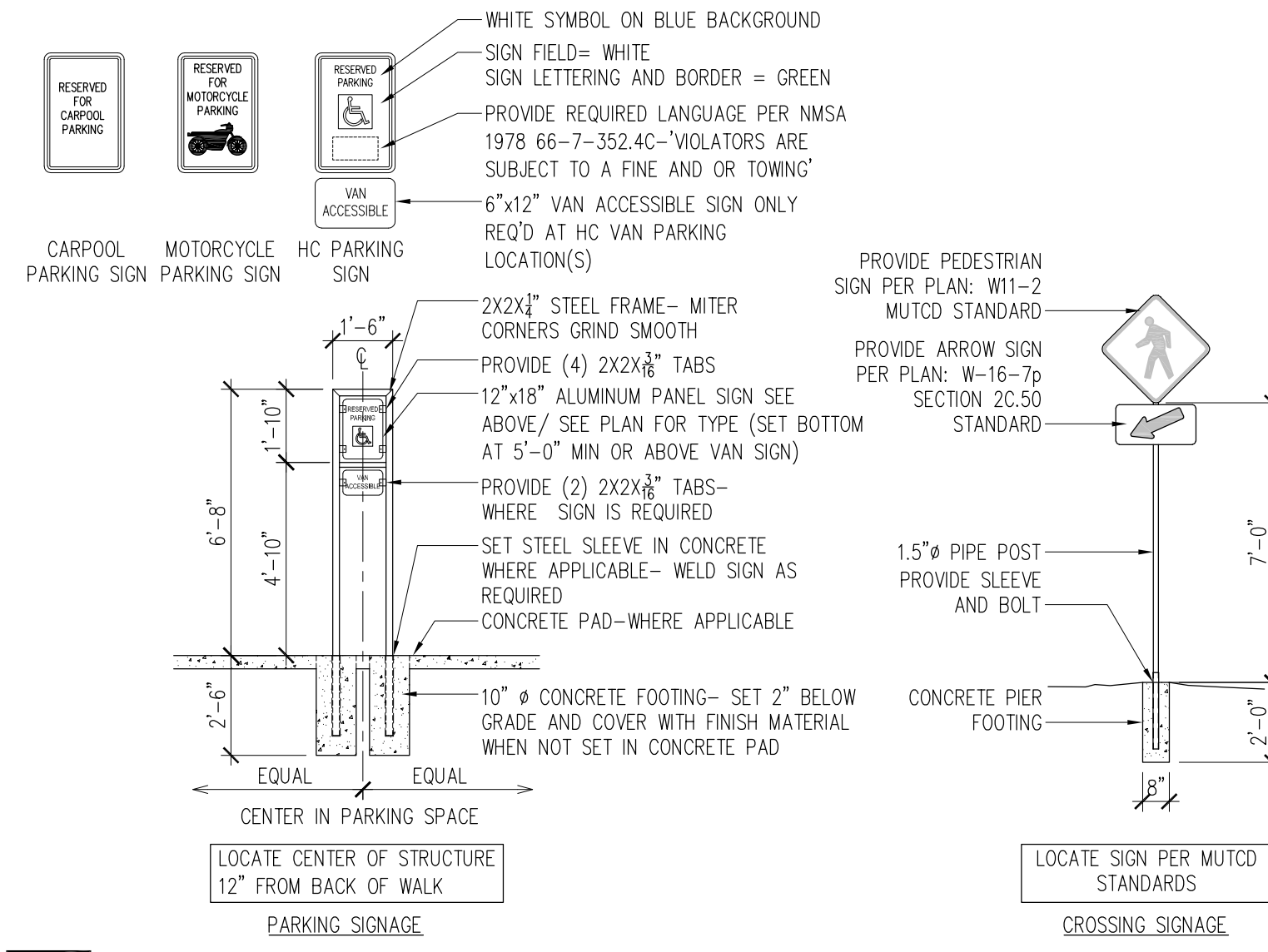
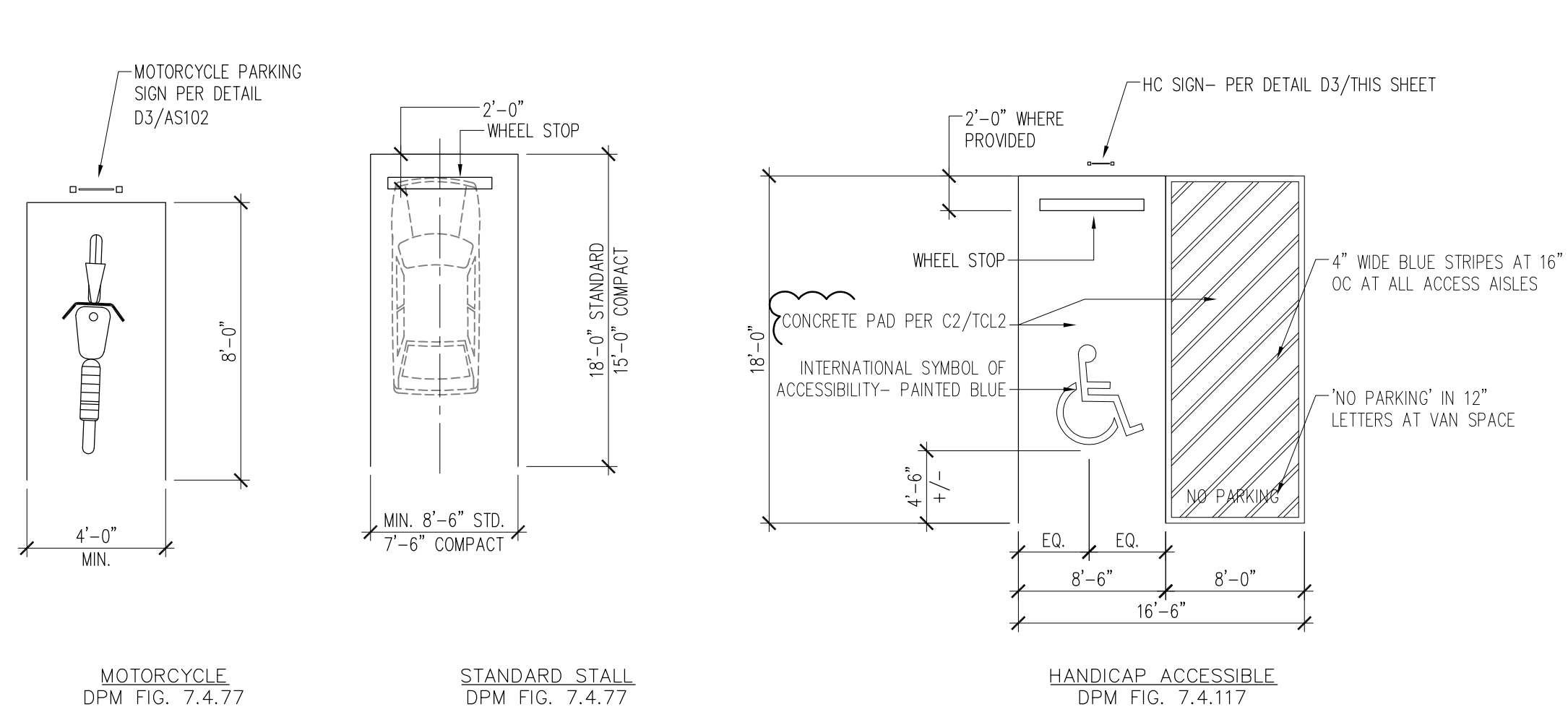
REMBE
 urban design + development
 1716 Central SW, Suite A
 Albuquerque NM 87104
 t. 505.243.0186

Project: **GRIEGOS FARMS**
 Drawn By: **KMA** Checked By: **KMA**
 Phase: **SD**
 Date: ---

Drawing Title:
TCL- TRAFFIC CONTROL LAYOUT SITE PLAN

Project Number: **2102**

Sheet Number:
TCL1



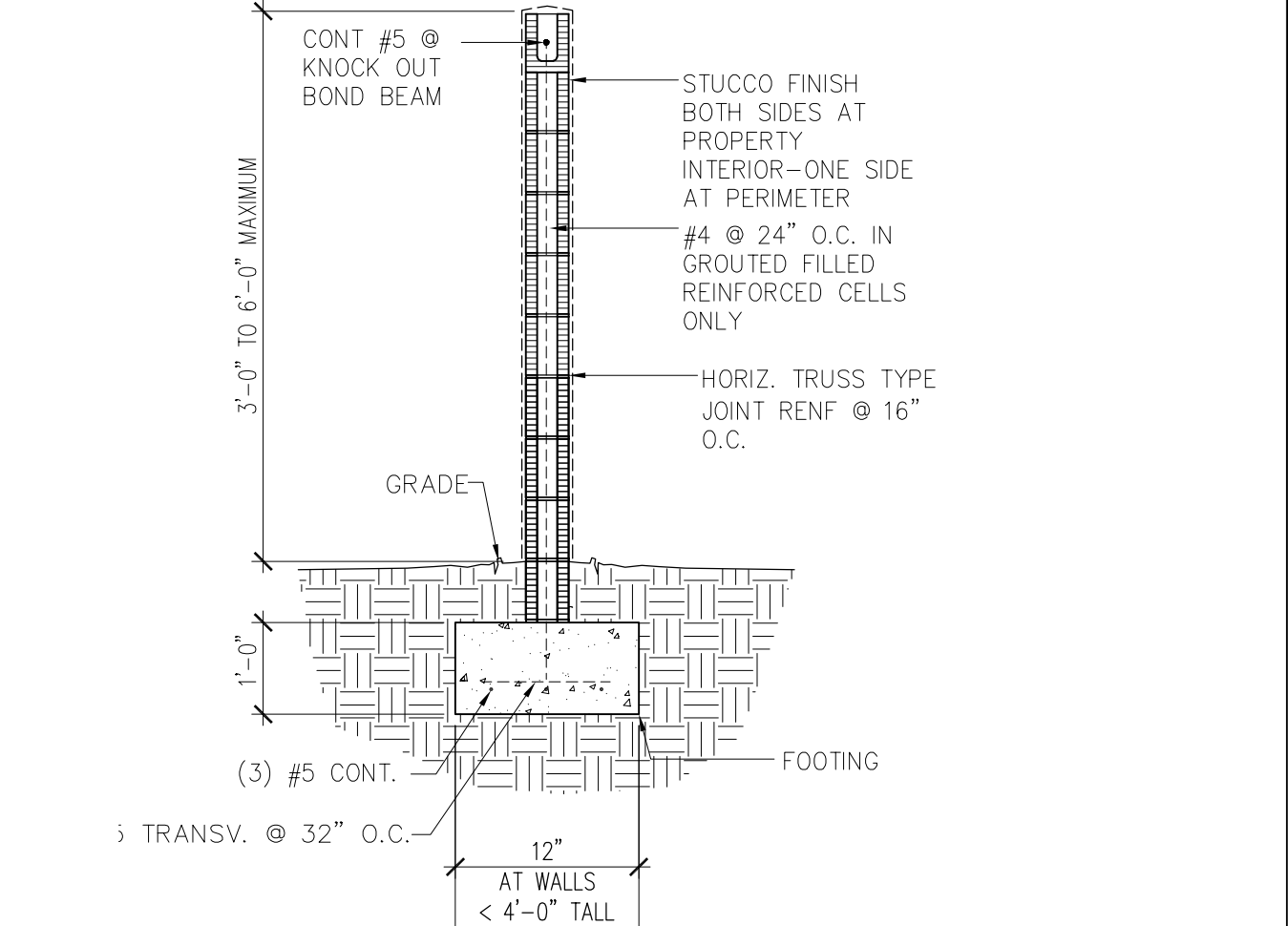
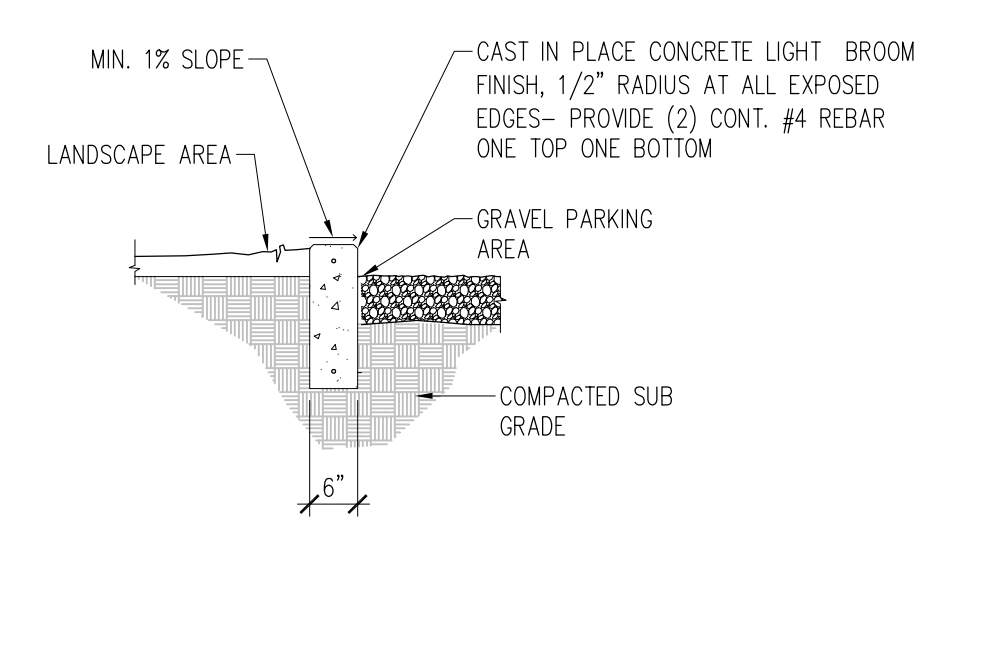
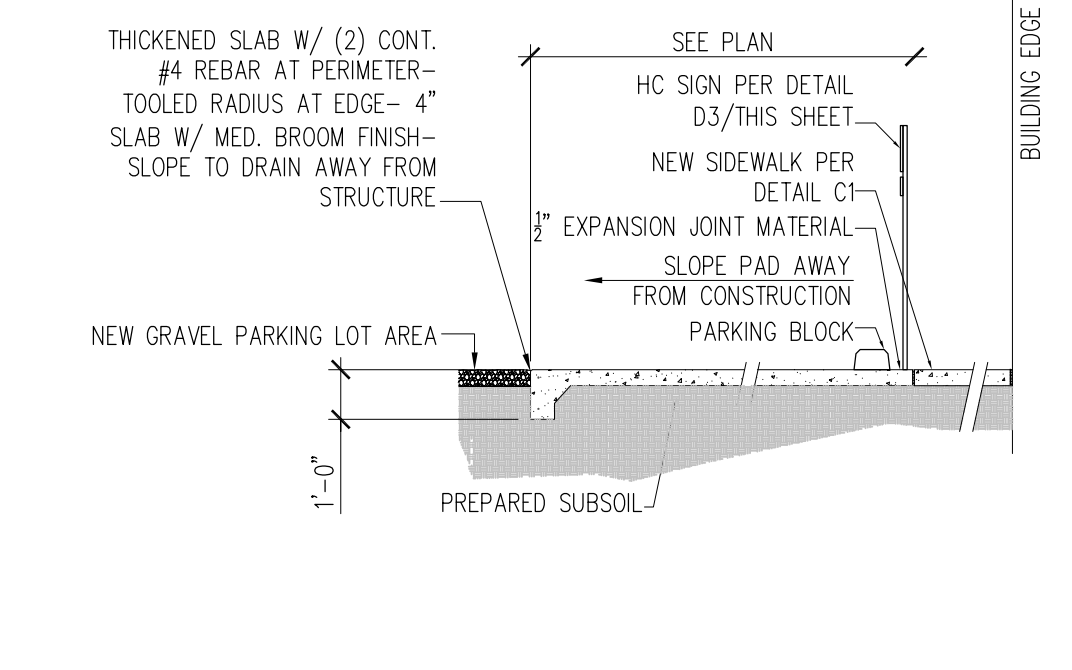
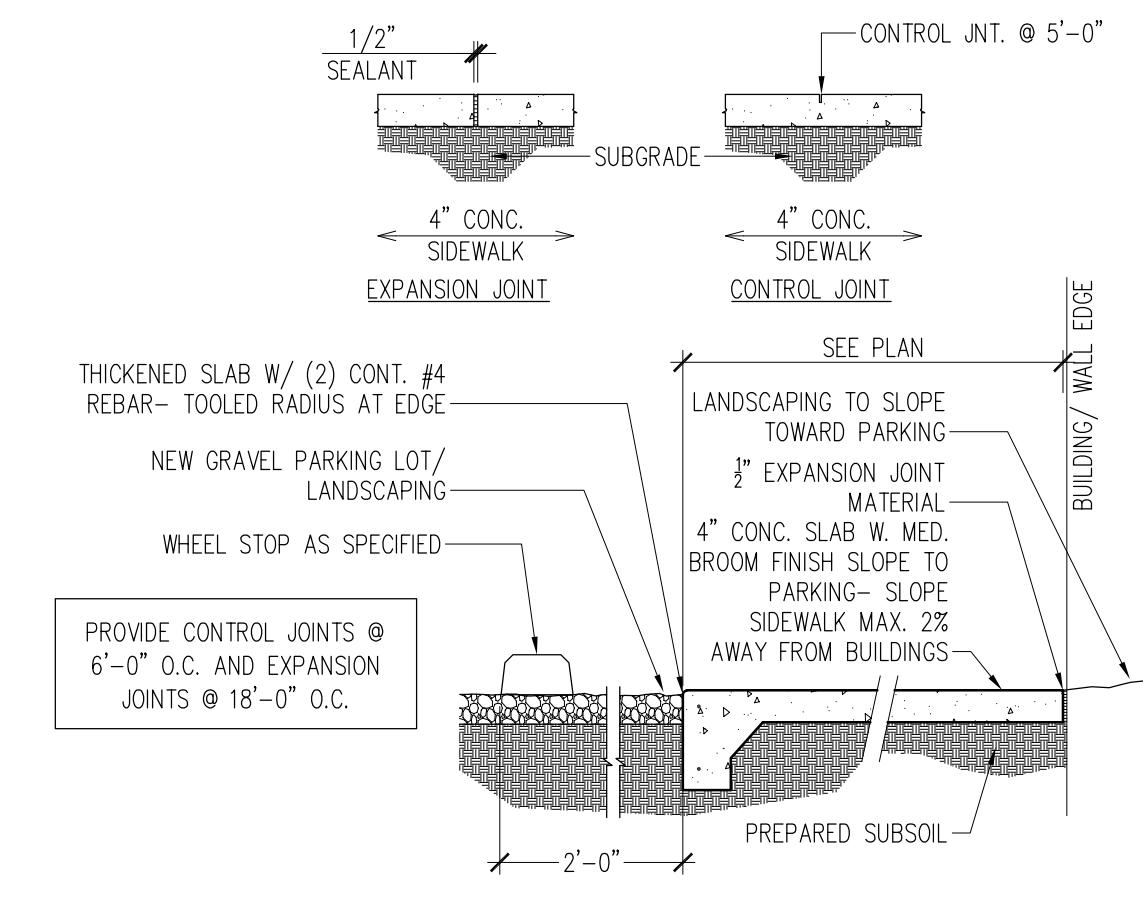
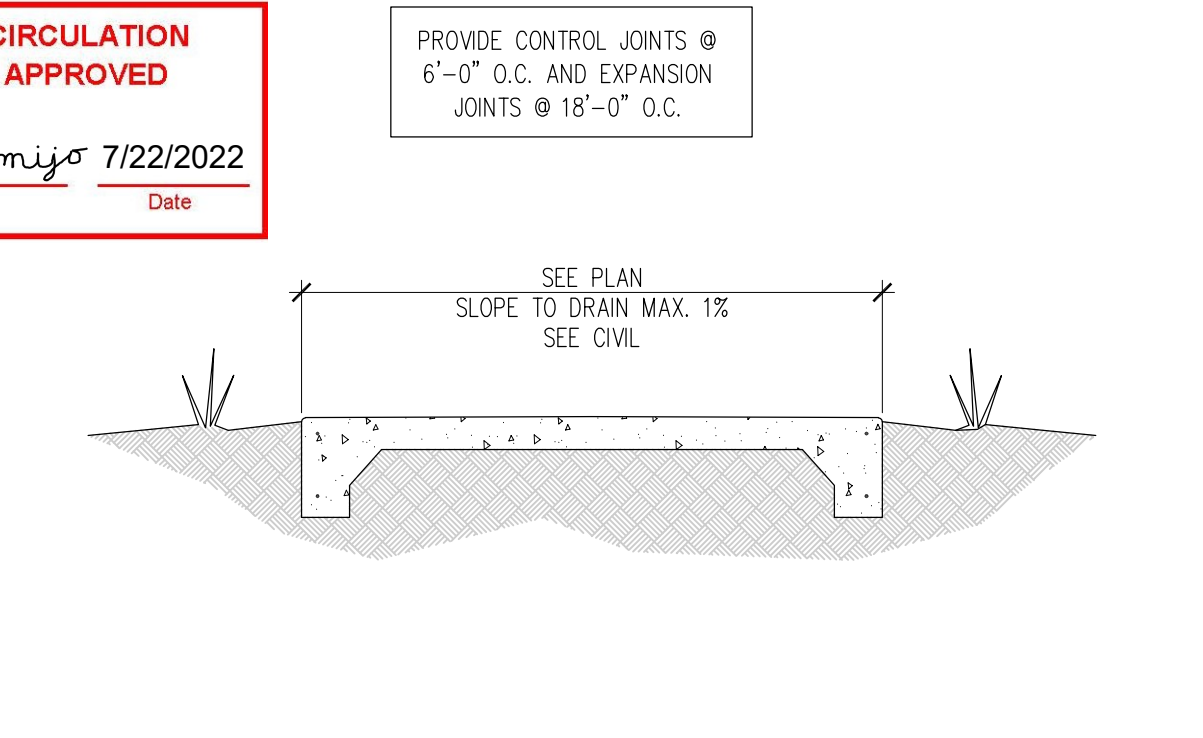
D1 PARKING STALL DIMENSIONS AND DETAILS
1/4"=1'-0"

D3 POST SIGN DETAIL
1/4"=1'-0"

D4 BIKE RACK DETAIL
1/4"=1'-0"

1. CHANGES PER TCL COMMENT LETTER- SEE RESPONSE LETTER 06/29/2022

TRAFFIC CIRCULATION LAYOUT APPROVED
Ernest Armijo 7/22/2022
Signed Date



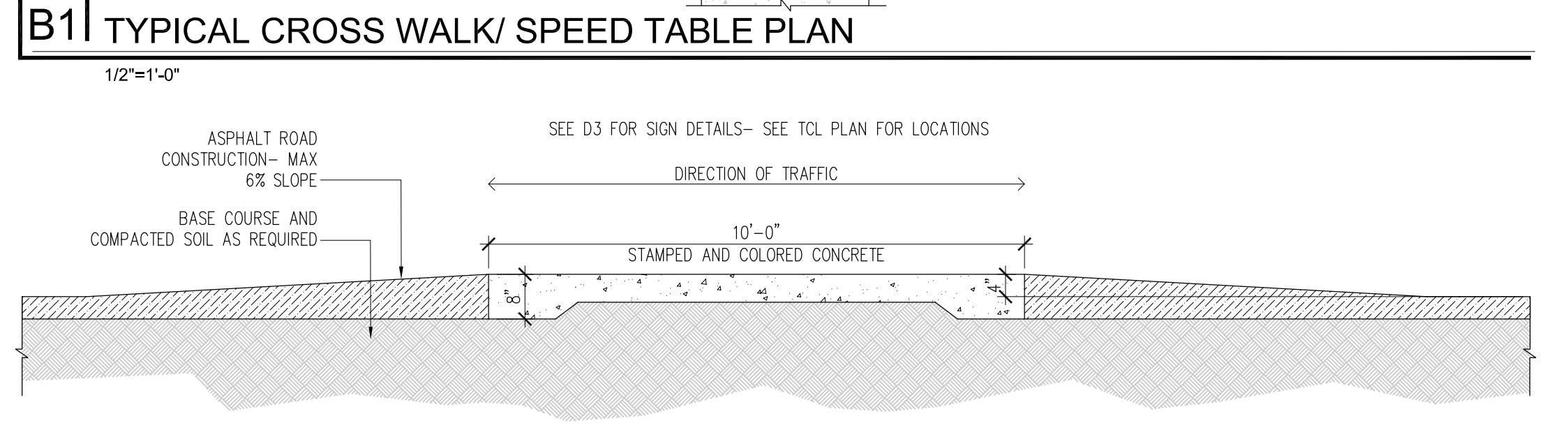
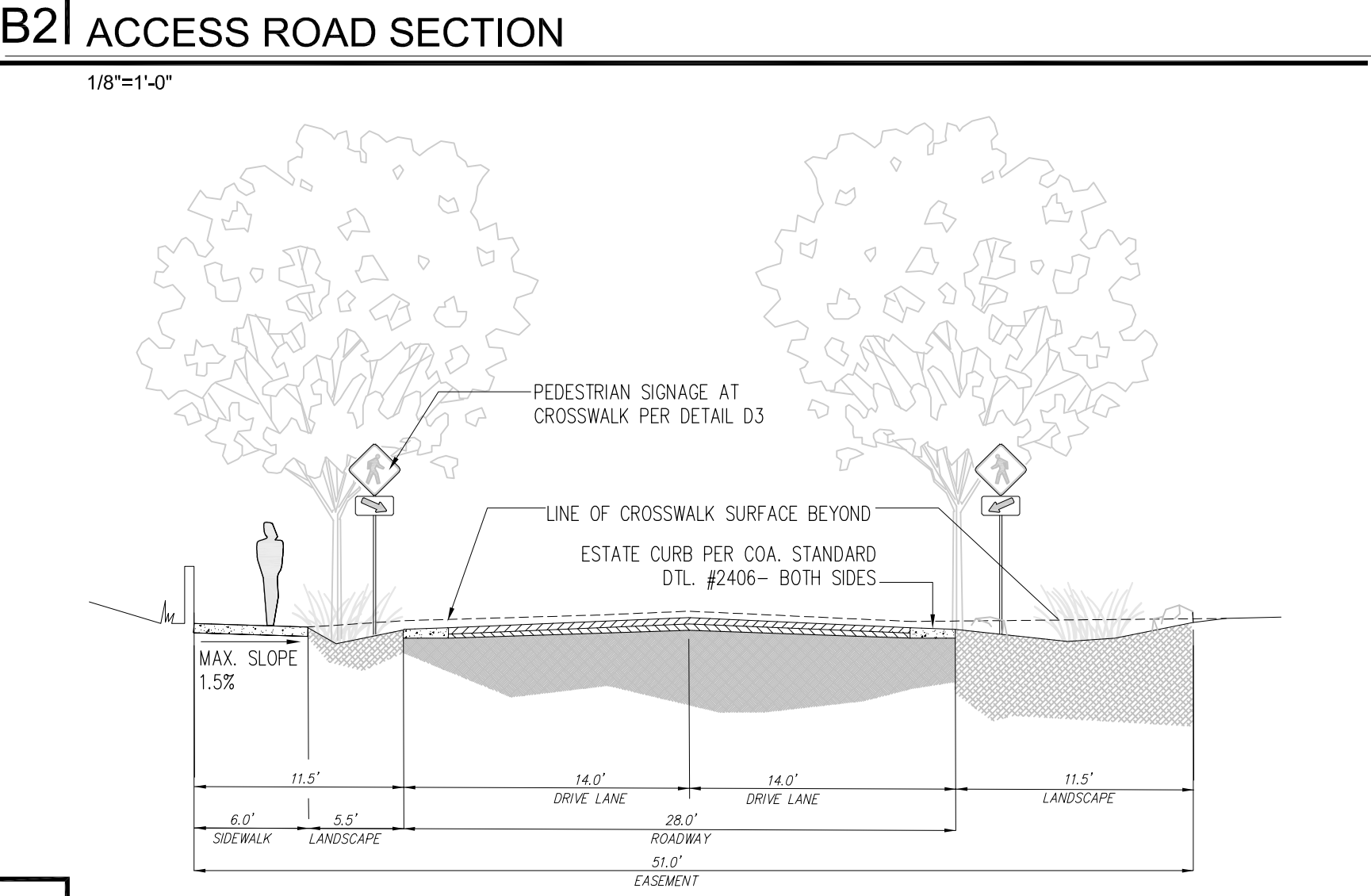
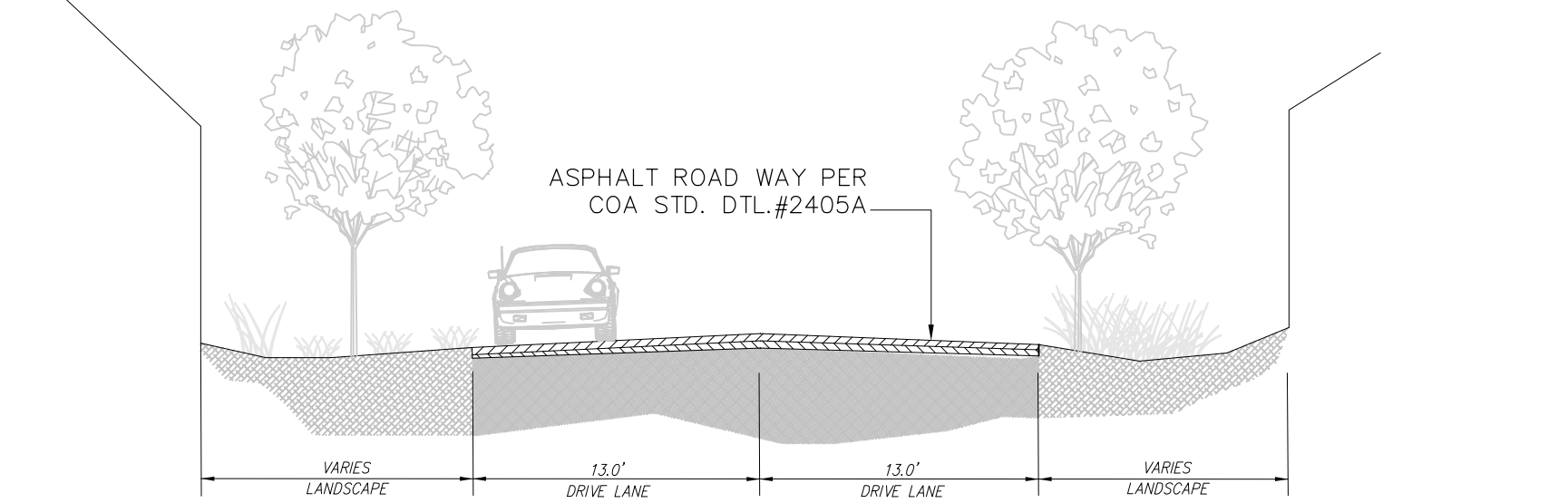
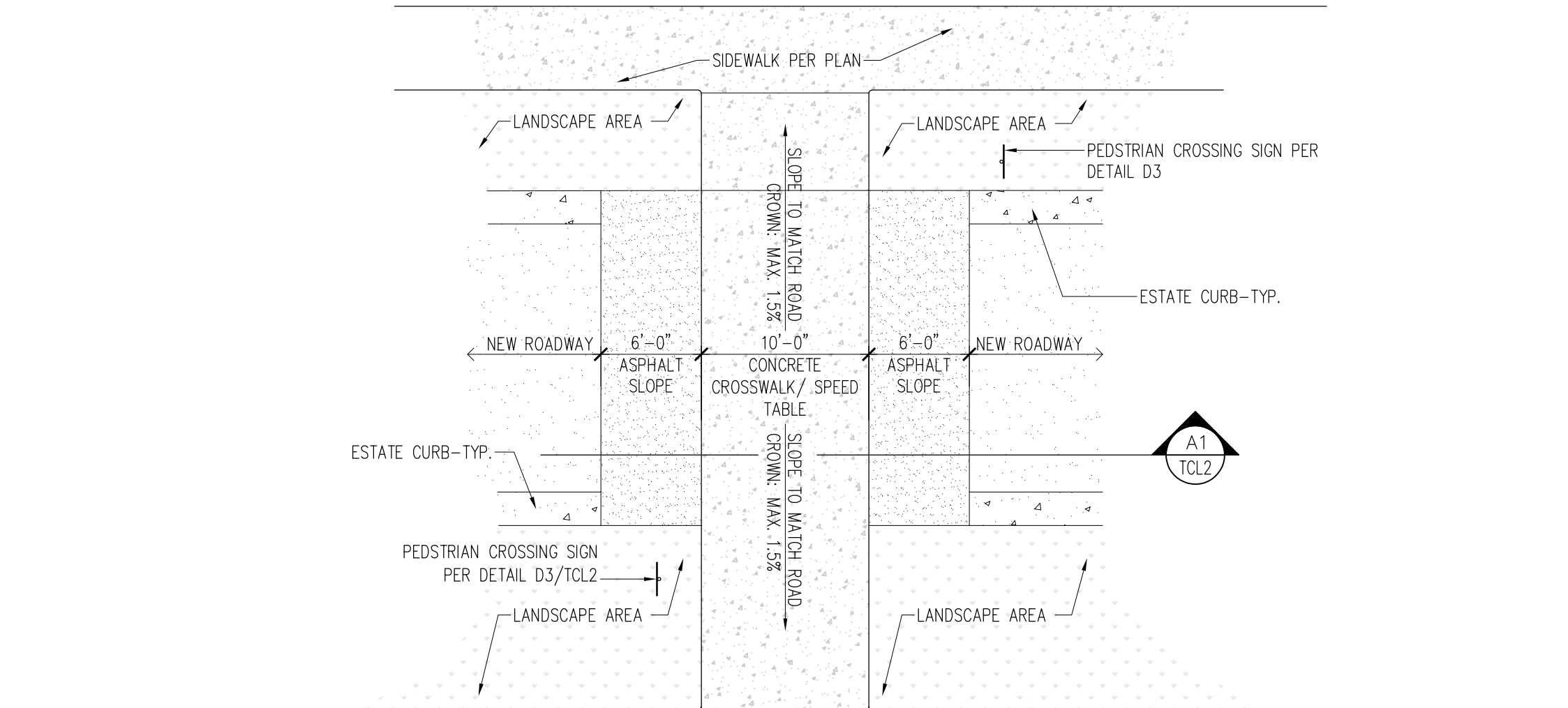
C1 PEDESTRIAN WALKWAY SECTION
1/2"=1'-0"

C2 SIDEWALK SECTION
1/2"=1'-0"

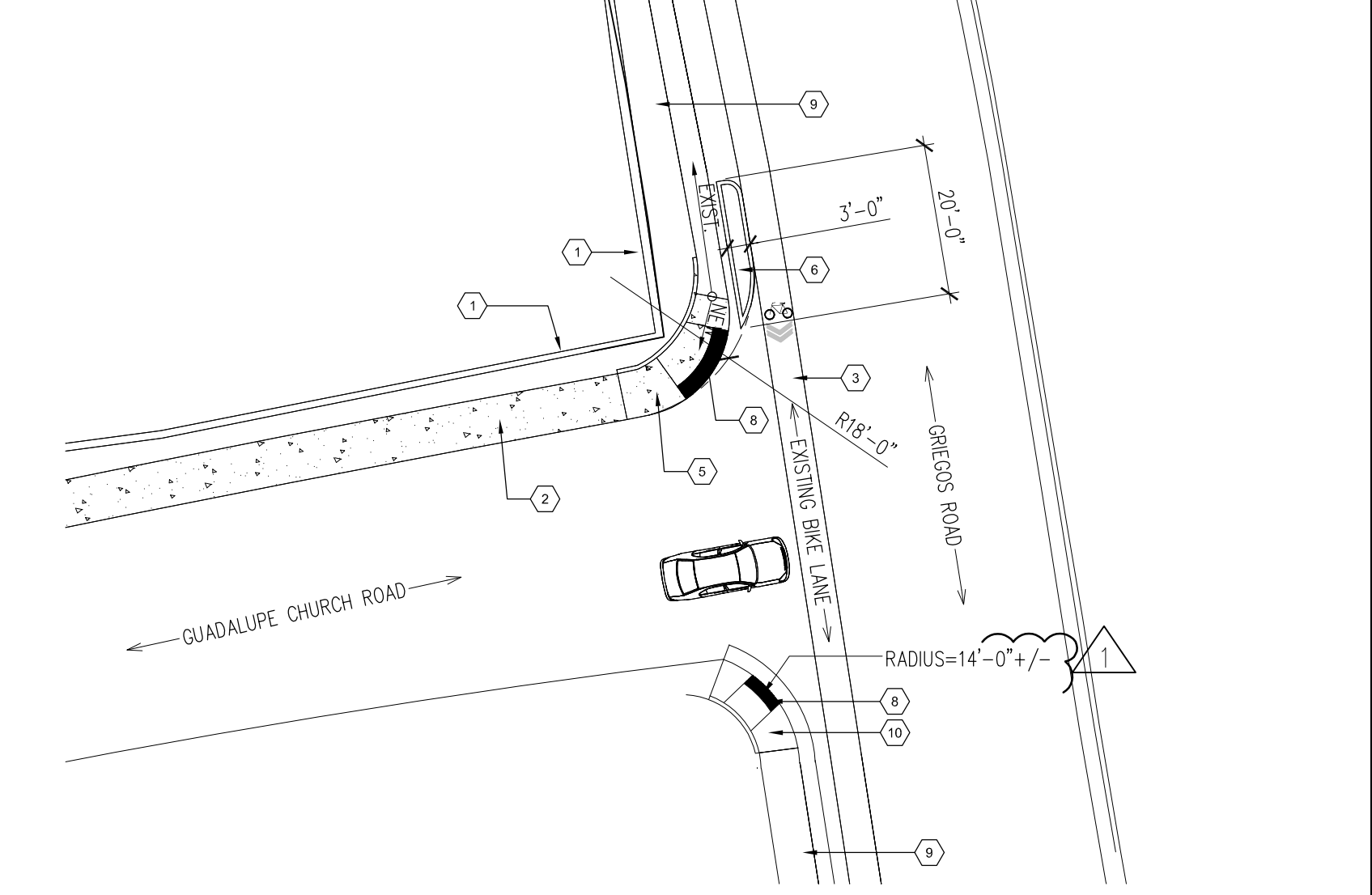
C3 HC PARKING SPACE SECTION
1/4"=1'-0"

C4 MOW CURB/ CURB STOP DETAIL
1/2"=1'-0"

C5 SITE WALL DETAIL
1/2"=1'-0"



- KEYED NOTES**
- EXISTING ADOBE WALL +/-42" ABOVE SIDEWALK ASPHALT ROAD WAY
 - NEW 6'-0" WIDE SIDEWALK PER DPM STANDARDS
 - EXISTING BIKE LANE TO REMAIN
 - NOT USED
 - NEW RAMP COA STANDARD DRAWING #2443
 - NEW CONCRETE CURB EXTENSION- PROVIDE MIN. 18" WIDE DRAINAGE WAY AT CURB
 - EXISTING WALL- NOT IN SCOPE
 - DETECTABLE WARNING SURFACE- TRUNCATED DOME OR OTHER APPROVED SURFACE- INSTALL PER COA
 - EXISTING SIDEWALK TO REMAIN
 - EXISTING CORNER ACCESS RAMP SIM. COA STD. #2441



A1 CROSS WALK/ SPEED TABLE DETAIL
1/2"=1'-0"

A3 GUADALUPE CHURCH ROAD SECTION-TYPICAL
1/8"=1'-0"

A4 GUADALUPE CHURCH ROAD/ GRIEGOS INTERSECTION DETAIL
1"=20'-0"

1860 GRIEGOS RD. NW
ALBUQUERQUE, NM 87107

GRIEGOS FARMS

Architect Stamp:
STATE OF NEW MEXICO
KENNETH MYERS
No.005751
REGISTERED ARCHITECT

06/14/2022
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1716 Central SW Suite A
Albuquerque, NM 87104
T. 505-243-0188

Project: **GRIEGOS FARMS**
Drawn By: **KMA** Checked By: **KMA**
Phase: **SD**
Date: ---
Drawing Title:

TCL- TRAFFIC CONTROL LAYOUT - SITE DETAILS

Project Number: **2102**
Sheet Number:

TCL2