CITY OF ALBUQUERQUE

Planning Department Alan Varela, Director



Mayor Timothy M. Keller

May 25, 2022

	Verlyn Miller, P.E. Miller Engineering Consultants, Inc 3500 Comanche NE Bldg. F Albuquerque, NM 87107				
	RE: Enterprise Rent-A-Car 5700 San Mateo NE Grading & Drainage Plans Engineer's Stamp Date: 05/04/22 Hydrology File: F18D052				
	Dear Mr. Miller:				
PO Box 1293	Based upon the information provided in your submittal received 05/04/2022, the Grading & Drainage Plans are not approved for Building Permit. The following comments need to be addressed for approval of the above referenced project:				
Albuquerque	Sheet C-101				
NM 87103	 Stormwater Quality Ponds cannot be built within the City's R.O.W. Either relocate the ponds on-site or use the Waiver of Management Onsite form and request a Payment in Lieu for the required Stormwater Quality Volume. 				
www.cabq.gov	<u>Sheet C-102</u>				
	2. Please provide the Benchmark information (location, description and elevation) for the survey contour information provided.				
	3. Overlaying the latest aerial with the site plan and there are several discrepancies such as the existing landscape areas along the north of the property, and the relocation of the existing ADA spaces. Please verify that an actual topographic survey was done on this site.				

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PO Box 1293

Albuquerque

NM 87103

www.cabq.gov

Question – Is the southern property also owned by the property owner of 5700 San Mateo? If so, then are they planning to do an updated plat to remove the property line? If not, them Hydrology will need a written permission to grade and do work on that Tract.

As a reminder, if the project total area of disturbance (including the staging area and any work within the adjacent Right-of-Way) is 1 acre or more, then an Erosion and Sediment Control (ESC) Plan and Owner's certified Notice of Intent (NOI) is required to be submitted to the Stormwater Quality Engineer (Doug Hughes, PE, <u>jhughes@cabq.gov</u>, 924-3420) 14 days prior to any earth disturbance.

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If you have any questions, please contact me at 924-3995 or <u>rbrissette@cabq.gov</u>.

Sincerely,

Renée C. Brissette

Renée C. Brissette, P.E. CFM Senior Engineer, Hydrology Planning Department

PO Box 1293

Albuquerque

NM 87103

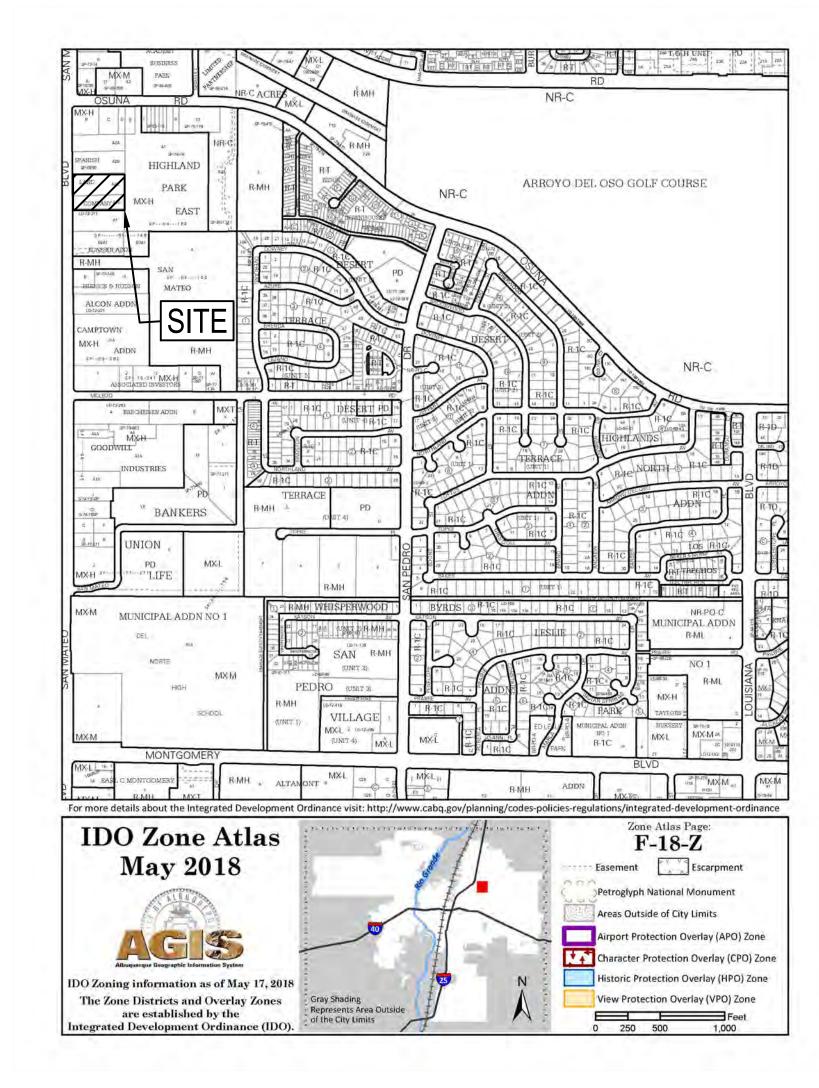
www.cabq.gov



City of Albuquerque

Planning Department Development & Building Services Division DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 6/2018)

Project Title: <u>Enterprise Rent-A-Car</u>		
DRB#:	EPC#:	Work Order#:
		ANISH LAND CO SUBD CONT 1.3011 AC ML
City Address: 5700 San Mateo NE, Albuquerqu	e, NM 8/109	
Applicant: Scott C Anderson, AIA And A	ssociates LLC	Contact: Scott Anderson
Address: 7604 Rio Penasco Court NW, A	Albuquerque, NM 87120	
Phone#: 505-401-7575	Fax#:	E-mail: <u>scott@scaarchitect</u> s.com
Other Contact: Miller Engineering Consu	ltants, Inc.	Contact: Verlyn Miller
Address: 3500 Comanche NE, Bldg. F		
Phone#: 505-888-7500	Fax#:	E-mail: <u>vmiller@mecnm.com</u>
TYPE OF DEVELOPMENT: PLAT (#	# of lots) RESIDENCE	DRB SITE X ADMIN SITE
IS THIS A RESUBMITTAL? Yes	X No	
DEPARTMENT TRANSPORTATION	X HYDROLOGY/DRAINAGE	
Check all that Apply: TYPE OF SUBMITTAL: ENGINEER/ARCHITECT CERTIFICATION PAD CERTIFICATION PAD CERTIFICATION CONCEPTUAL G & D PLAN ORADING PLAN DRAINAGE REPORT DRAINAGE MASTER PLAN DRAINAGE MASTER PLAN FLOODPLAIN DEVELOPMENT PERMIT A ELEVATION CERTIFICATE CLOMR/LOMR TRAFFIC CIRCULATION LAYOUT (TCL) TRAFFIC IMPACT STUDY (TIS) STREET LIGHT LAYOUT OTHER (SPECIFY) PRE-DESIGN MEETING?	PRELIMINARY PRELIMINARY SITE PLAN FO SITE PLAN FO FINAL PLAT A PPLIC SIA/ RELEASE FOUNDATION X GRADING PER SO-19 APPROV PAVING PERM GRADING/ PAI WORK ORDER A CLOMR/LOMR FLOODPLAIN I OTHER (SPECI	OF OCCUPANCY TPLAT APPROVAL R SUB'D APPROVAL R BLDG. PERMIT APPROVAL PPROVAL OF FINANCIAL GUARANTEE PERMIT APPROVAL MIT APPROVAL VAL IIT APPROVAL O CERTIFICATION APPROVAL
DATE SUBMITTED: <u>5-4-2022</u>	By: Verlyn Miller	
COA STAFF:	ELECTRONIC SUBMITTAL RECEIVED:	



VICINITY MAP (B1 SCALE: NOT TO SCALE

National Flood Hazard Layer FIRMette

FLOOD ZONE MAP

FLOOD ZONE MAP: 35001C01390

EE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOU Without Base Flood Elevation (BFE) With BFE or Depth Zone AE, AO, AH, VE, AR SPECIAL FLOOD HAZARD AREAS **Regulatory Floodway** 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone Future Conditions 1% Annual Chance Flood Hazard Zone Area with Reduced Flood Risk due to Levee. See Notes. Zone X THER AREAS OF FLOOD HAZARD /// Area with Flood Risk due to Levee Zone NO SCREEN Area of Minimal Flood Hazard Zone Effective LOMRs THER AREAS Area of Undetermined Flood Hazard Zone L GENERAL - - - Channel, Culvert, or Storm Sewer TRUCTURES IIIIII Levee, Dike, or Floodwall B 20.2 Cross Sections with 1% Annual Chance 17.5 Water Surface Elevation AREA OFMINIMAL FLOOD HAZARD 8 - - - Coastal Transect www.513 www. Base Flood Elevation Line (BFE) Limit of Study Jurisdiction Bounda ---- Coastal Transect Baselin OTHER - Profile Baseline EATURES Hydrographic Feature **Digital Data Available** No Digital Data Available MAP PANELS Unmapped The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location. his map complies with FEMA's standards for the use of ligital flood maps if it is not void as described below. he basemap shown complies with FEMA's basema The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 4/27/2022 at 12:46 PM and does not flect changes or amendments subsequent to this date and ime. The NFHL and effective information may change or ecome superseded by new data over time. This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, egend, scale bar, map creation date, community identifiers, IRM panel number, and FIRM effective date. Map images for 1:6,000 unmapped and unmodernized areas cannot be used for 2,000 Basemap: USGS National Map: Orthoimagery: Data refreshed October, 2020 0 250 500 1,000 1,500

😵 FEMA

Legend

HYDROLOGY REPORT

SITE LOCATION

The existing site is an approximate 2.5-acre site located 5700 San Mateo Boulevard NE in Albuquerque. The site is located on the east side of San Mateo north of Montgomery Boulevard and can be accessed via San Mateo and south of Central Avenue (see vicinity map this sheet).

EXISTING CONDITIONS

The existing site is estimated at 2.5 acres and is mostly developed with a building and asphalt paving. There is a portion of land on the east side of the site that is currently base course. The site currently slopes from the east to west at a mild slope. The site does not lie within a 100-year FEMA floodplain (see FEMA panel on this sheet).

PROPOSED CONDITIONS

The proposed project will consist of a new wash bay facility to be located on the site and new asphalt paving of the eastern base course area of the site. The existing building will remain for the new facility. Storm water quality pond areas will be provided along the western portion of the site. The drainage calculations for proposed conditions are indicated on this sheet.

CONCLUSIONS

When fully developed as indicated on the grading and drainage plan, the increased runoff from the site is estimated at 1.37 cfs and 0.114 acre-feet during the 100-year, 24-hour event. Storm water runoff from the site will discharge into the proposed storm water quality pond. The storm water quality volume for this stie is estimated at 3385 cubic feet. The storm water quality pond has a volume of 3547 cubic feet which is greater than the storm water quality volume of 3385 cubic feet. Overflow from the pond will spill to its historical location in San Mateo Boulevard.

DPM HYDROLOGY CALCULATIONS

			HYDRO	LOGY					
Precipitation	Zone 3 - 100-	year Storm		P(360) =	2.6	in in	P(1440)	= 3.1	in
	Basin	L	Land Treatment Factors		rs				
Basin	Area	A	В	С	D	Ew	V(100-6)	V(100-24)	Q(100)
	(Ac)		(Acres)		(in)	(af)	(af)	(cfs)
Existing Cond	ditions								
Site	2.50	0.00	0.00	1.15	1.35	1.87	0.389	0.445	10.74
Total	2.50								10.74
Proposed Co	nditions		al an Arrigan Ar		1	and the state	in dia ka		
Site	2.50	0.00	0.00	0.28	2.22	2.24	0.467	0.559	12.11
Total	2.50	1.			(12.11

SWQV CALCULATIONS

FIRST FLUSH =(0.42/12" * 96,703 SF) = 3385 CF > 3547

GENERAL NOTES:

- 2. ACS STA "12-F18"
- CONSTRUCTION.
- UTILITIES.
- PROPERTY SERVED.

POND RATING TABLE

	Pond R	ating Table	
V	ATER HAP	RVEST ARE	A #1
Pond Rating	Table	Spillway	Crest = 5216
Side Slope	2:1		
Depth	Area	Volume	Cum Volume
(ft)	(sq ft)	(cubic fet)	(cubic feet)
5213.5	224	0	0
5214.5	575	400	400
5215.5	983	779	1179
Pond Invert		5213.5	
Spillway Cres		5215.5	
WSE (First Fl WSE (100-ye	a the second of the second		
WSE (100-ye	ai)		
		ating Table	
V	VATER HAP	RVEST ARE	A #2
Pond Rating	Table	Spillway	Crest = 5216
Side Slope	2:1	: · · · · · · · · · · · · · · · · · · ·	
Depth	Area	Volume	Cum Volume
(ft)	(sq ft)	(cubic fet)	(cubic feet)
5213.5	162	0	0
5214.5	417	290	290
5215.5	734	576	865
Pond Invert		5213.5	
Spillway Cres		5215.5	
WSE (First Fl			
WSE (100-ye	ar)		
	Pond R	ating Table	
	Po	ond #3	
Pond Rating Table		Spillway	Crest = 5216
Side Slope	2:1		
Depth	Area	Volume	Cum Volume
(ft)	(sq ft)	(cubic fet)	(cubic feet)
5213.5	300	0	0
5214.5	737	519	519
5215.5	1232	985	1503
Pond Invert		5213.5	
Spillway Cres	7	5215.5	
WSE (First Fl			
	100 T 200		

1. EXISTING TOPOGRAPHIC DATA SHOWN ON THESE PLANS WAS PROVIDED BY HARRIS SURVEYING, INC., CORRALES, NEW MEXICO. MILLER ENGINEERING CONSULTANTS HAS UNDERTAKEN NO FIELD VERIFICATION OF THIS INFORMATION.

THE BENCHMARK IS LOCATED 5.1 MILES NORTHEAST OF INTERSECTION OF SAN MATEO BLVD. AND MCLEOD RD. NE. THE BENCHMARK IS A 1 3/4" METALLIC DISK EPOXIED TO THE TOP OF CONCRETE STORM DRAIN INLET LOCATED IN THE SOUTHEAST QUADRANT OF THE INTERSECTION, STAMPED "ACS BM 12-F18" THE GROUND ELEV. 5215.969 (NAVD 1988)

3. THE CONTRACTOR IS RESPONSIBLE FOR ALL TEMPORARY SEDIMENT AND EROSION CONTROL DEVICES DURING THE CONSTRUCTION PHASE.

4. CONTRACTOR SHALL OBTAIN A GRADING PERMIT FROM THE CITY OF ALBUQUERQUE, PRIOR TO ANY GRADING OR

5. TWO WORKING DAYS PRIOR TO ANY EXCAVATION CONTRACTOR MUST CONTACT LINE LOCATING SERVICE 260-1990 FOR LOCATION OF EXISTING

6. ALL EMBANKMENTS SHALL BE PLACED AND COMPACTED IN LIFTS OF MAXIMUM OF 8". THE EMBANKMENTS SHALL BE WETTED AND COMPACTED TO 95% OPTIMUM DENSITY PER ASTM D1557 AND 95% UNDER ALL STRUCTURES INCLUDING DRIVEWAYS AND PARKING LOTS.

7. MAINTENANCE OF THESE FACILITIES SHALL BE THE RESPONSIBILITY OF THE OWNER(S) OF THE

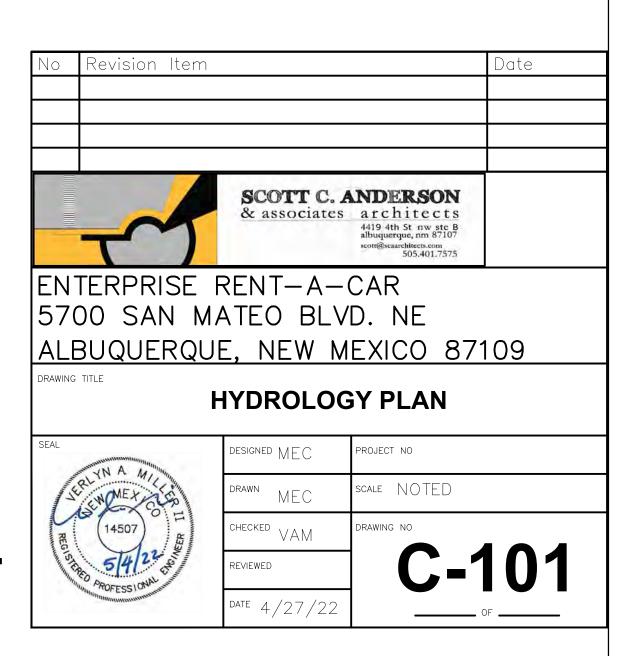
8. THE CONTRACTOR SHALL FIELD VERIFY LOCATION AND SIZE OF ALL UTILITIES PRIOR TO CONSTRUCTION.

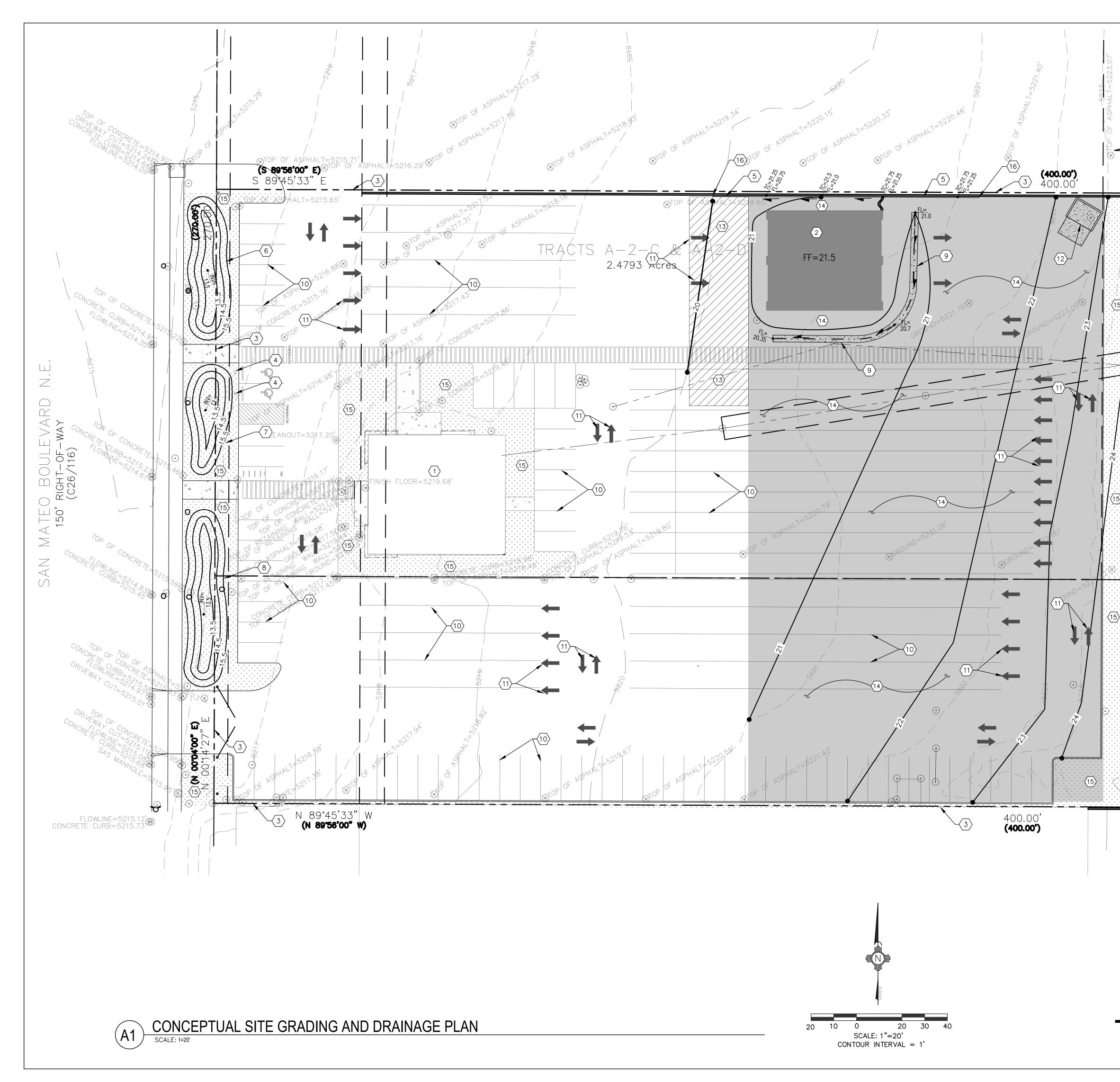
9. THE SUBJECT PROPERTY IS LOCATED WITHIN ZONE X (500 YEAR) DESIGNATING AREAS DETERMINED TO BE OUTSIDE THE 100-YEAR FLOOD PLANE ACCORDING TO THE FLOOD INSURANCE RATE MAP, ALBUQUERQUE, NEW MEXICO AND UNINCORPORATED AREAS PER MAP NO 35001C 0139G.

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3500 COMANCHE, NE BUILDING F ALBUQUERQUE, NM 87107 (505)888-7500 (505)888-3800 (FAX)

- 10. ALL WORK PERFORMED SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY OF ALBUQUERQUE STORM DRAINAGE REGULATIONS. ALL WORK PERFORMED SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY OF ALBUQUERQUE "GRADING AND DRAINAGE DESIGN REQUIREMENTS AND POLICIES FOR LAND DEVELOPMENT."
- DOWNTOWN ALBUQUERQUE. THE BENCHMARK IS LOCATED AT THE 11. THE OWNER, CONTRACTOR AND/OR BUILDER SHALL COMPLY WITH ALL APPROPRIATE LOCAL, STATE AND FEDERAL REGULATIONS AND REQUIREMENTS.
 - 12. THE CONTRACTOR SHALL TAKE ALL APPROPRIATE AND REASONABLE MEASURES TO PREVENT SEDIMENT OR POLLUTANT LADEN STORM WATER FROM EXITING THE SITE DURING CONSTRUCTION. STORMWATER MAY BE DISCHARGED IN A MANNER, WHICH COMPLIES WITH THE APPROVED GRADING AND DRAINAGE PLAN.
 - 13. THE CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO PREVENT THE MOVEMENT OF CONSTRUCTION RELATED SEDIMENT, DUST, MUD, POLLUTANTS, DEBRIS, WASTE, ETC FROM THE SITE BY WIND, STORM FLOW OR ANY OTHER METHOD EXCLUDING THE INTENTIONAL, LEGAL TRANSPORTATION OF SAME IN A MANNER ACCEPTABLE BY THE CITY.
 - 14. THE CONTRACTOR SHALL NOT DISTURB AREAS OUTSIDE THE AREAS SHOWN AS "SLOPE LIMITS" ON THE GRADING AND DRAINAGE PLAN.
 - 15. SEE ARCHITECTURAL DRAWINGS FOR SIDEWALK AND HANDICAPPED RAMPS, DETAILS AROUND THE BUILDING.
 - 16. THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER FOR CLARIFICATION IF THERE ARE ANY SPOT ELEVATIONS ON THE GRADING AND DRAINAGE PLAN WHICH APPEAR TO BE AMBIGUOUS OR DO NOT MEET THE INTENT OF THE GRADING AND DRAINAGE PLAN.
 - 17. THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER FOR CLARIFICATION IF THERE ARE SIDEWALKS OR CONCRETE FLATWORK WHICH DOES NOT MEET ADA ACCESSIBILITY REQUIREMENTS. ALL SIDEWALKS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.0%, ALL SIDEWALKS SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5.0%, AND ALL RAMPS SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 15:1.
 - 18. ALL SIDEWALKS AND CONCRETE FLATWORK SHALL HAVE A MINIMUM OF 0.5% SLOPE. CONTRACTOR SHALL CONTACT PROJECT ENGINEER IF THERE ARE SIDEWALKS OR CONCRETE FLATWORK WHICH DO NOT MEET THIS REQUIREMENT.
 - 19. THE CONTRACTOR SHALL SUBMIT MATERIAL SUBMITTALS, CUT SHEETS AND SHOP DRAWINGS FOR ALL CIVIL RELATED ITEMS FOR REVIEW PRIOR TO CONSTRUCTION.
 - 20. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS (UPDATE 8, AMENDMENT 1)
 - 21. ALL EXISTING MANHOLES, VALVES AND METERS SHALL BE ADJUSTED TO NEW FINISH GRADE.





LEGEND: •<u>38.0</u>0 FG PROPOSED SPOT ELEVATIONS (FINISHED GRADE) •<u>MATC</u>H (95.19) MATCH EXISTING ELEVATIONS TCON TOP OF CONCRETE FL FLOW LINE, CURB INVERT INV FG FINISH GRADE TBC TOP OF BASE COURSE TC TOP OF CURB TOP OF GRATE ΤG TOP OF ASPHALT ΤA FLOW ARROW GRADE BREAK-HIGH POINT SWALE STORM DRAIN LINE EXISTING STORM DRAIN LINE PROPOSED MAJOR CONTOUR PROPOSED MINOR CONTOUR EXISTING MAJOR CONTOUR — — 5895 — — EXISTING MINOR CONTOUR NEW WATER LINE NEW SANITARY SEWER LINE EXISTING WATER SERVICE LINE EXISTING SANITARY SEWER LINE — — — S NEW DOUBLE CLEANOUT NEW WATER METER NEW FIRE HYDRANT NEW POST INDICATOR VALVE \cap

KEYED NOTES:

 $\left< \underline{1} \right>$ EXISTING BUILDING TO REMAIN.

 $\langle 2 \rangle$ NEW BUILDING, SEE ARCHITECTURAL PLANS FOR DETAILS.

 $\langle 3 \rangle$ property line.

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 $\langle 4 \rangle$ proposed ada signage.

(5) proposed header curb. See sheet C-501 for details.

6 PROPOSED WATER HARVEST PONDING AREA #1 TOP=5216.00, BOTTOM=5213.5. SEE DETAIL SHEET C-501.

 $\langle 7 \rangle$ proposed water harvest pounding area #2 top=5216.00, BOTTOM=5213.5. SEE DETAIL SHEET C-501.

 $\langle 8 \rangle$ proposed water harvest pounding area #3 top=5216.00, Bottom=5213.5. See detail sheet C-501.

9 proposed 36" wide concrete valley gutter. See detail sheet C-501.

(10) New parking lot striping (typical), see architectural plans.

(11) NEW DIRECTIONAL ARROWS, SEE ARCHITECTURAL PLANS.

 $\langle 12 \rangle$ proposed trash enclosure. See architectural plans for details.

(13) remove and replace existing paving as required to accommodate new wash building.

(14) New hot mix asphalt in this area, match existing elevations around perimeter.

 $\langle 15 \rangle$ Landscape area see architectural plans for details.

 $\langle 16 \rangle$ END CURB WITH 3' CURB TAPER AND MATCH EXISTING ELEVATIONS.

No	Revision Item			Date
		SCOTT C & associates	ANDERSON architects 4419 4th 5t nw ste B albuquerque, nm 87107 scott@scaarchitects.com 505.401.7575	
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DRAWING			DRAINAGE	
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