

# CITY OF ALBUQUERQUE

*Planning Department*  
Brennon Williams, Director



*Mayor Timothy M. Keller*

April 20, 2020

Asa Nilsson-Weber, P.E  
Isaacson & Arfman, Inc.  
128 Monroe St. NE  
Albuquerque, NM 87108

RE: **Campbell Compound**  
**Request for Release of Financial Guarantee**  
**Hydrology Final Inspection – Accepted**  
• **CPN 708882;**  
**Grading Plan Stamp Date: 2/6/2018**  
**Certification Dated: 4/8/2020**  
**Hydrology File: G13D032**

PO Box 1293

Dear Ms. Nilsson-Weber:

Albuquerque

Based on the certification received on 4/8/20, this certification is accepted for Release of Financial Guarantee by Hydrology.

If you have any questions, please contact me at 924-3986 or [earmijo@cabq.gov](mailto:earmijo@cabq.gov).

NM 87103

Sincerely,

[www.cabq.gov](http://www.cabq.gov)

Ernest Armijo, P.E.  
Principal Engineer, Planning Dept.  
Development Review Services



# City of Albuquerque

Planning Department

Development & Building Services Division

## DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 11/2018)

**Project Title:** Campbell Compound **Building Permit #:** \_\_\_\_\_ **Hydrology File #:** G13D032  
**DRB#:** 1011223 **EPC#:** \_\_\_\_\_ **Work Order#:** 708882  
**Legal Description:** Campbell Compound  
**City Address:** \_\_\_\_\_

**Applicant:** Isaacson & Arfman, Inc. **Contact:** Åsa Nilsson-Weber  
**Address:** 128 Monroe Street NE - Albuquerque, NM 87108  
**Phone#:** (505) 268-8828 **Fax#:** \_\_\_\_\_ **E-mail:** asaw@iacivil.com  
**Owner:** Las Ventanas NM, Inc. **Contact:** Scott Ashcraft  
**Address:** 8330-A Washington Place NE - Albuquerque, NM 87113  
**Phone#:** (505) 362-6824 **Fax#:** \_\_\_\_\_ **E-mail:** \_\_\_\_\_

**TYPE OF SUBMITTAL:** ☒ PLAT (10# OF LOTS) ☐ RESIDENCE ☐ DRB SITE ☐ ADMIN SITE

IS THIS A RESUBMITTAL?: \_\_\_\_\_ Yes ☒ No

**DEPARTMENT:** ☐ TRAFFIC/ TRANSPORTATION ☒ HYDROLOGY/ DRAINAGE

Check all that Apply:

**TYPE OF SUBMITTAL:**

☒ ENGINEER ARCHITECT CERTIFICATION  
☒ PAD CERTIFICATION  
☐ CONCEPTUAL G & D PLAN  
☐ GRADING PLAN  
☐ DRAINAGE MASTER PLAN  
☐ DRAINAGE REPORT  
☐ FLOODPLAIN DEVELOPMENT PERMIT APPLIC  
☐ ELEVATION CERTIFICATE  
☐ CLOMR/LOMR  
☐ TRAFFIC CIRCULATION LAYOUT (TCL)  
☐ TRAFFIC IMPACT STUDY (TIS)  
☐ OTHER (SPECIFY) \_\_\_\_\_  
☐ PRE-DESIGN MEETING?

**TYPE OF APPROVAL/ACCEPTANCE SOUGHT:**

☒ BUILDING PERMIT APPROVAL  
☐ CERTIFICATE OF OCCUPANCY  
☐ PRELIMINARY PLAT APPROVAL  
☐ SITE PLAN FOR SUB'D APPROVAL  
☐ SITE PLAN FOR BLDG. PERMIT APPROVAL  
☐ FINAL PLAT APPROVAL  
☒ SIA/ RELEASE OF FINANCIAL GUARANTEE  
☐ FOUNDATION PERMIT APPROVAL  
☐ GRADING PERMIT APPROVAL  
☐ SO-19 APPROVAL  
☐ PAVING PERMIT APPROVAL  
☐ GRADING/ PAD CERTIFICATION  
☐ WORK ORDER APPROVAL  
☐ CLOMR/LOMR  
☐ FLOODPLAIN DEVELOPMENT PERMIT  
☐ OTHER (SPECIFY) \_\_\_\_\_

**DATE SUBMITTED:** April 8, 2020 **By:** Åsa Nilsson-Weber

COA STAFF:

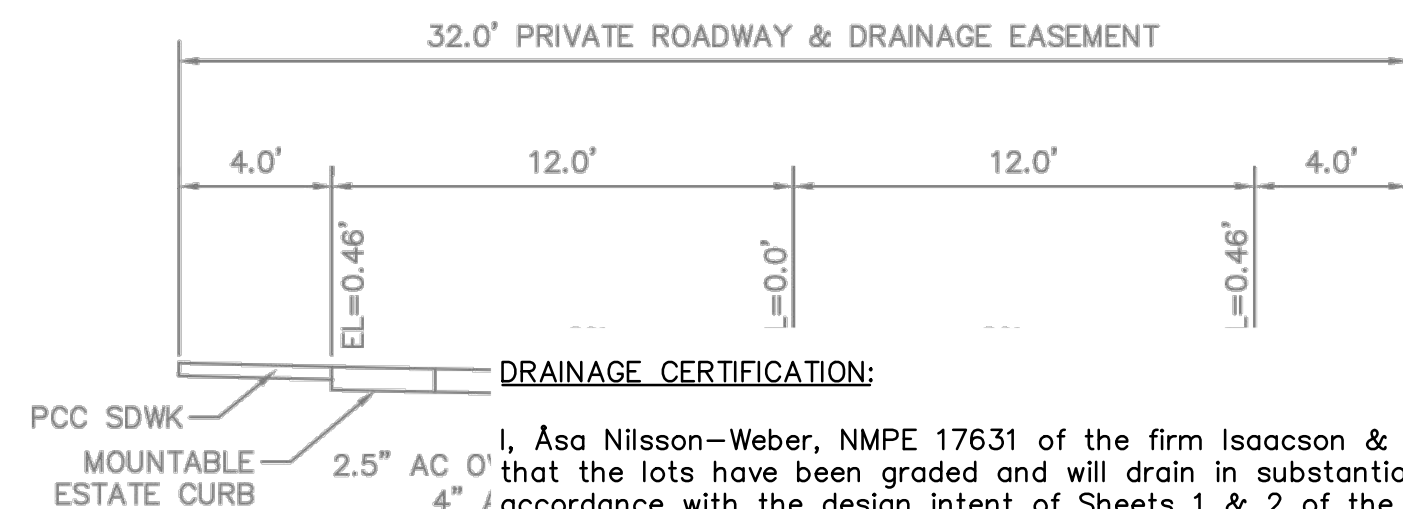
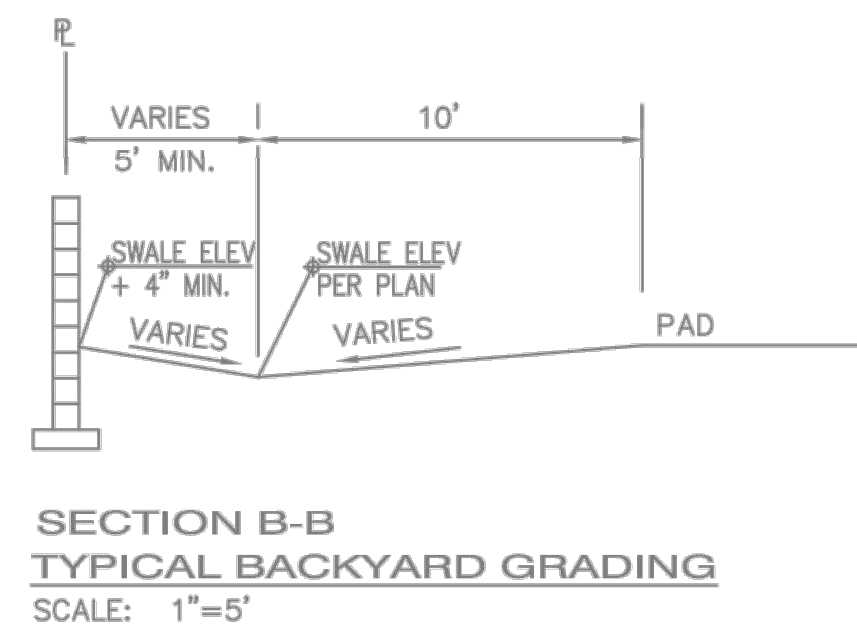
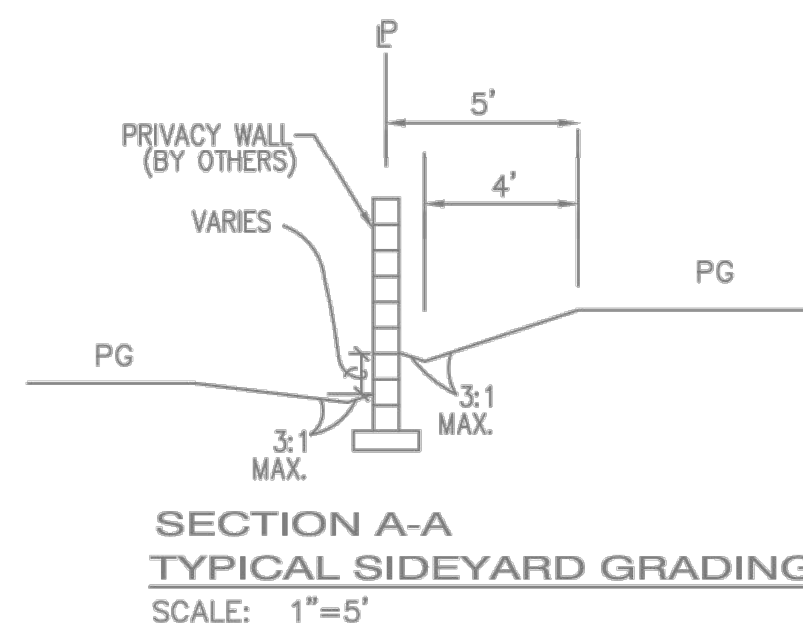
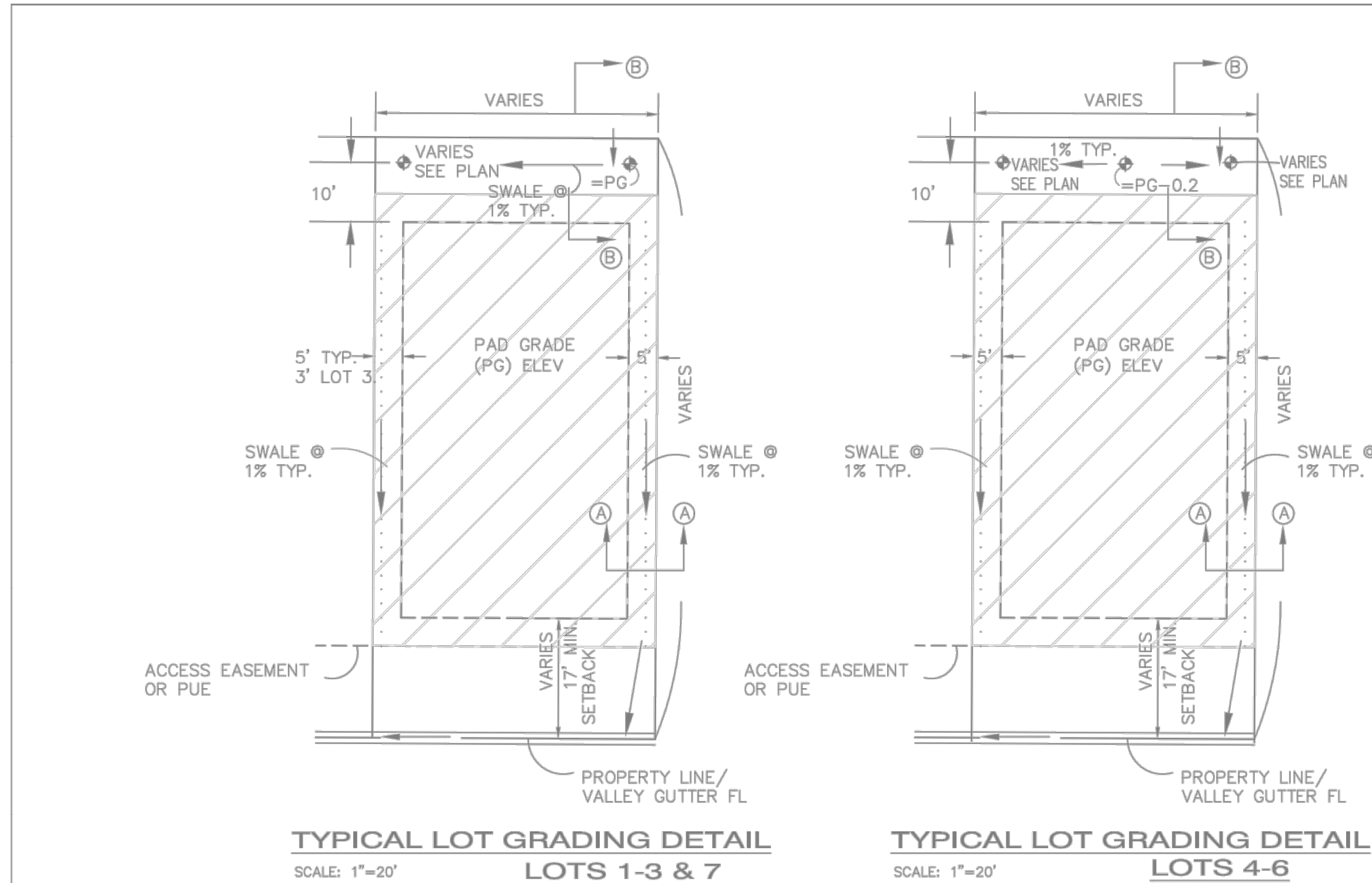
ELECTRONIC SUBMITTAL RECEIVED: \_\_\_\_\_

FEE PAID: \_\_\_\_\_









I, Asa Nilsson-Weber, NMPE 17631 of the firm Isaacson & Arfman, P.A., hereby certify that the lots have been graded and will drain in substantial compliance with and in accordance with the design intent of Sheets 1 & 2 of the approved Grading & Drainage Plan dated March 6, 2018. I also certify that retaining walls have been constructed in substantial compliance with said approved Grading & Drainage Plan, except as noted below.

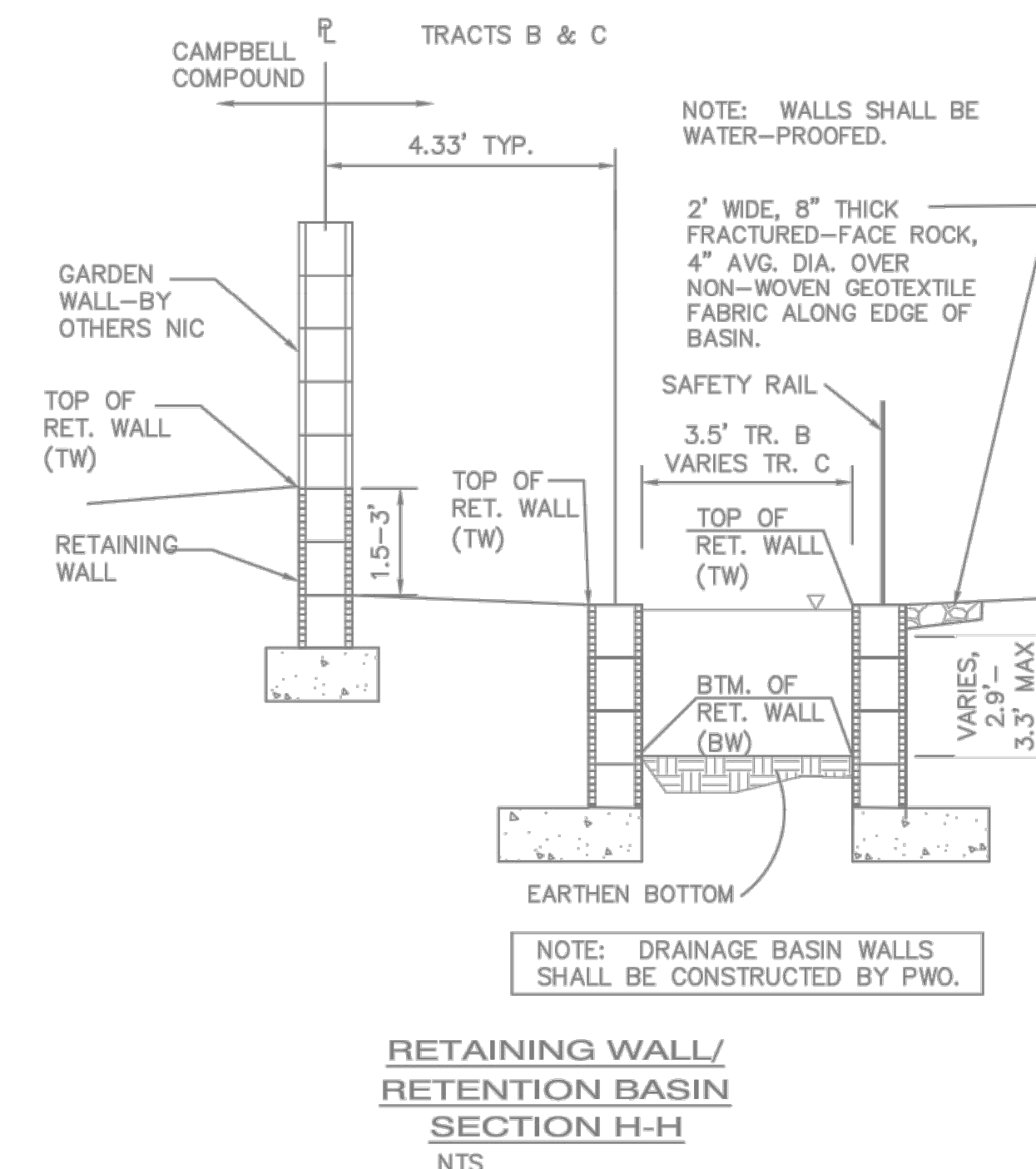
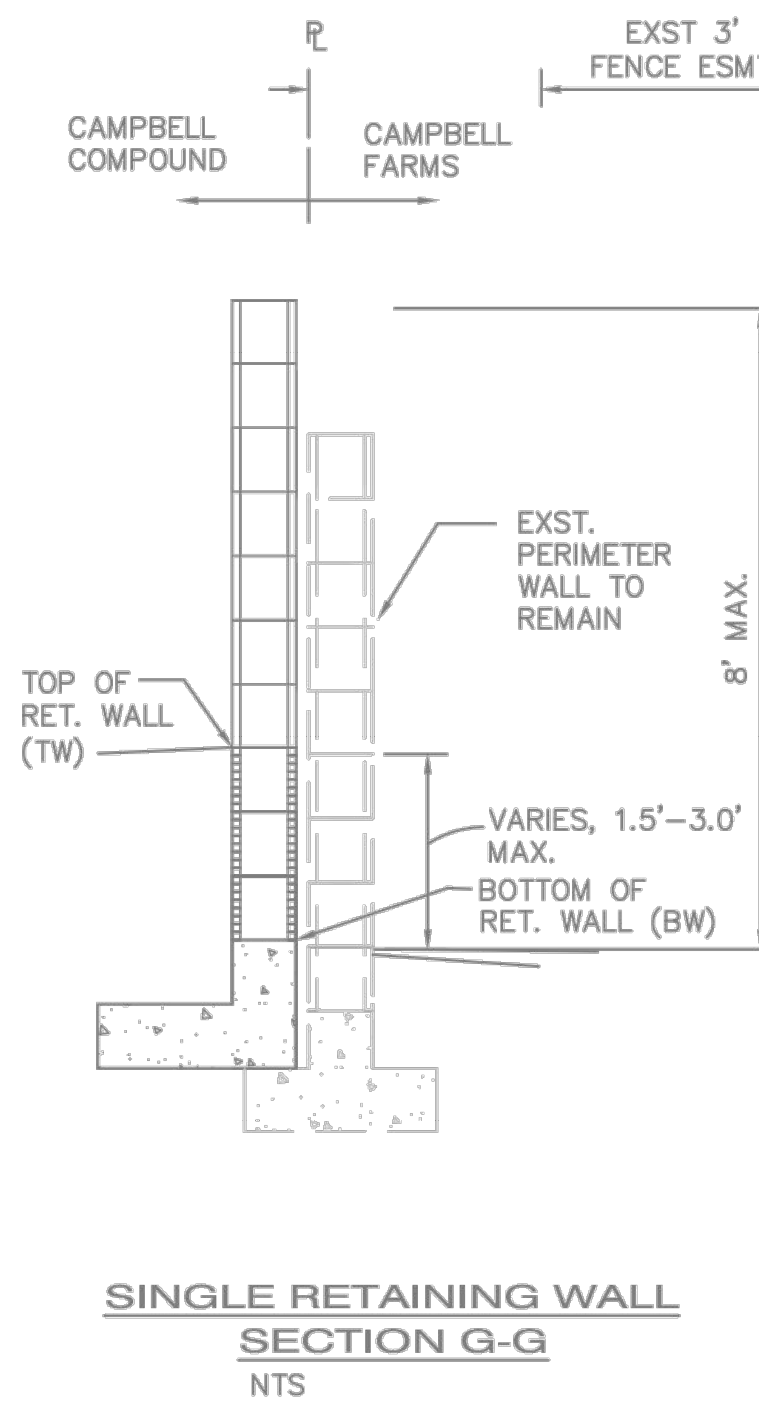
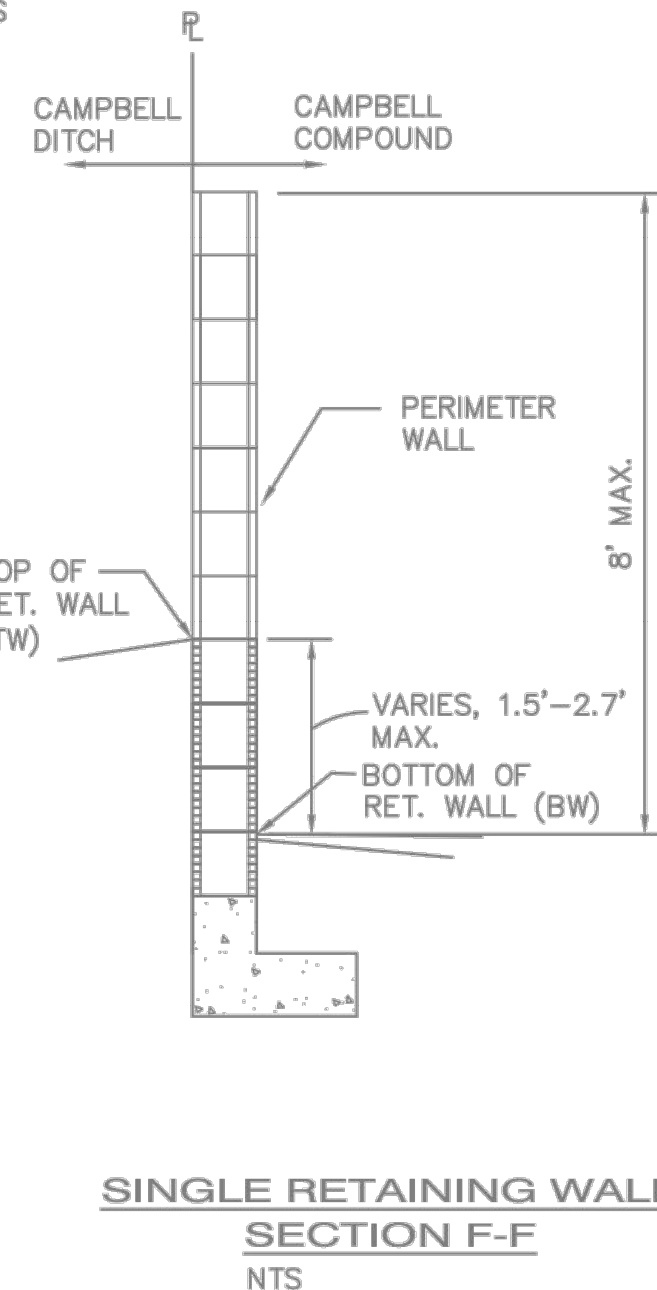
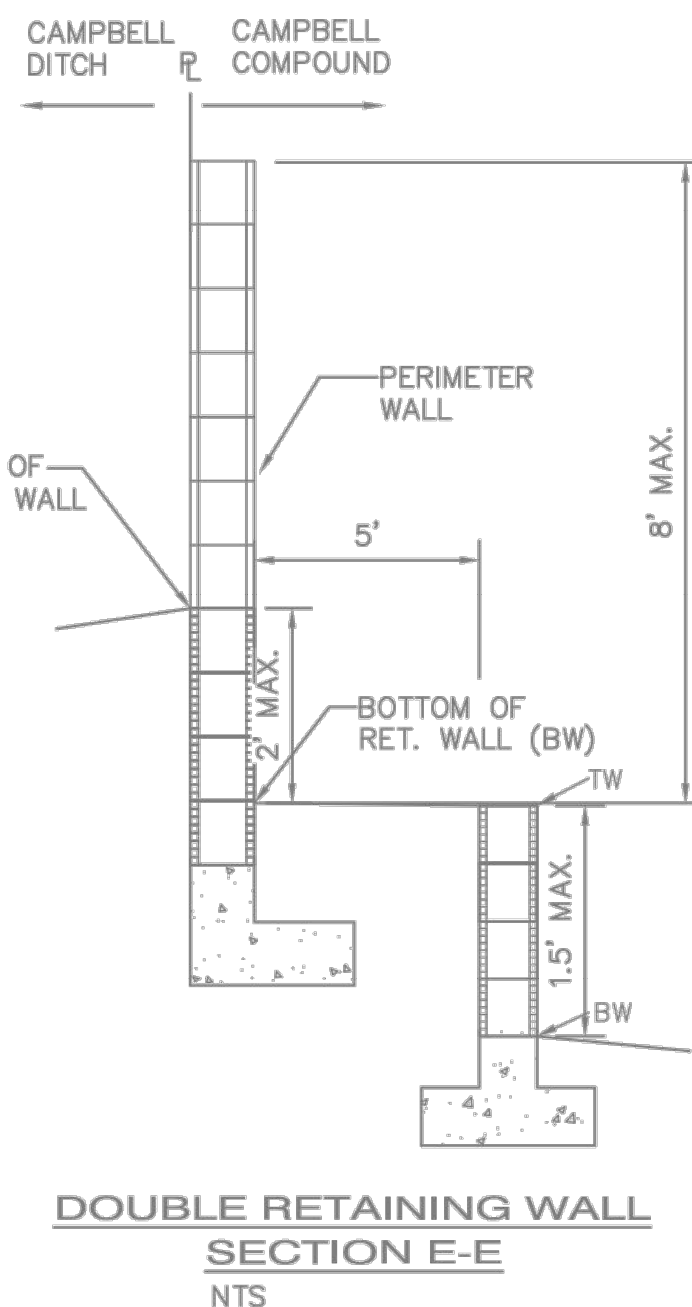
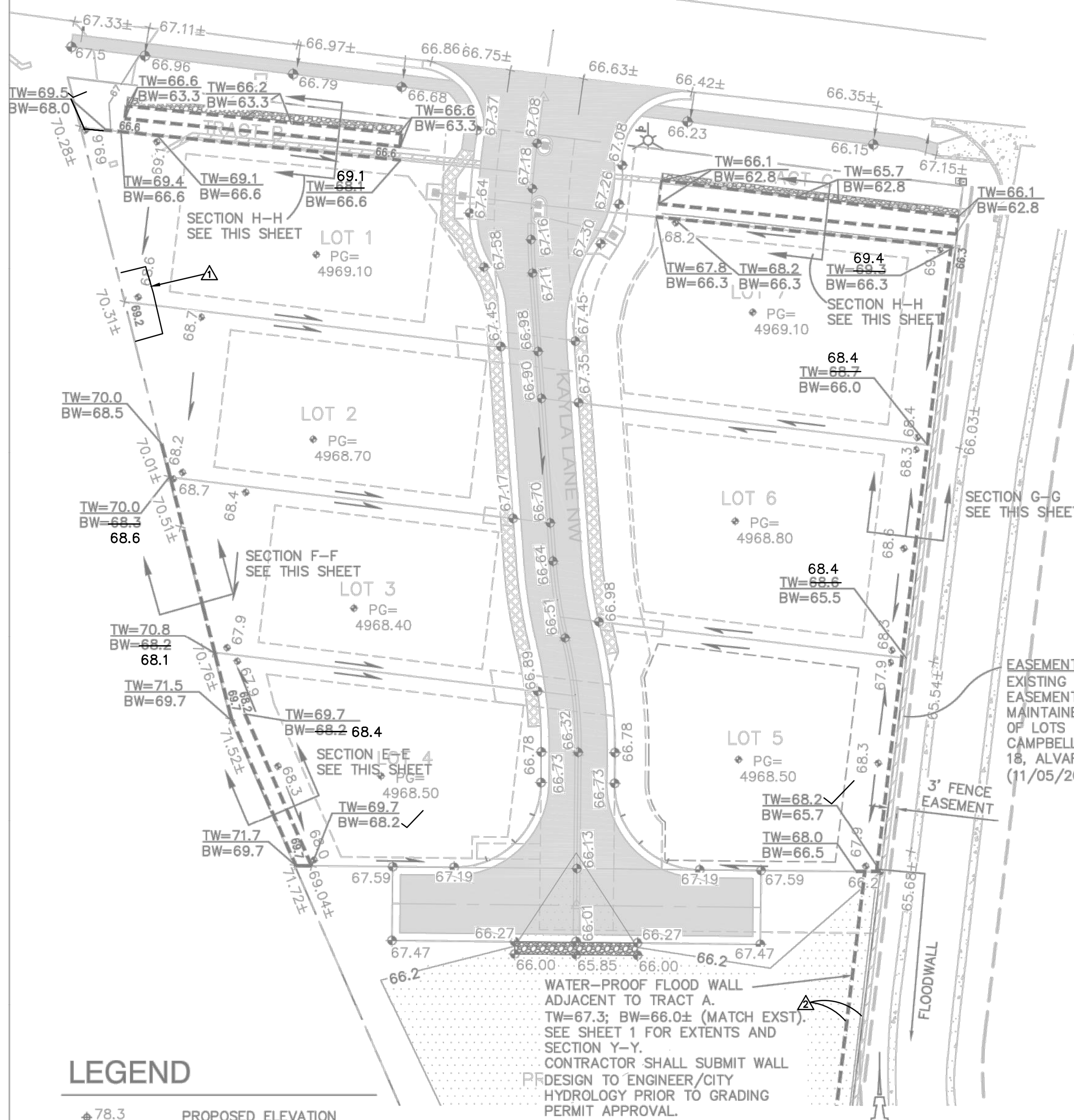
A retaining wall was constructed around an existing tree.

No offset flood wall was constructed. The existing perimeter wall was extended and water proofed with permission from adjacent lot owners. The flood wall has been inspected and certified by Jeremy F. Randall, P.E., S.E. with a separate letter.

The record information edited onto the original design document has been obtained by Brian Martinez, NMRPS 18374, of the firm Cartesian Surveys, Inc. I further certify that I visited the project site on October 24, 2019, and have determined by visual inspection that the survey data provided is representative of actual site conditions and is true and correct to the best of my knowledge and belief. This certification is submitted in support of a request for Release of Financial Guarantee.

The record information presented hereon is not necessarily complete and intended only to verify substantial compliance of the grading and drainage aspects of this project. Those relying on this record document are advised to obtain independent verification of its accuracy before using it for any other purpose.

Asa Nilsson-Weber, NMPE 17631 Date 04/08/20



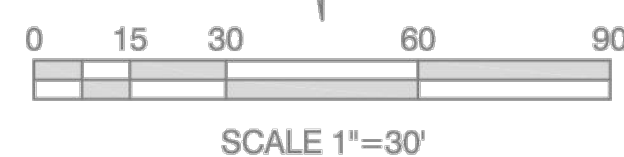
## CIVIL GENERAL NOTES

- THE CONTRACTOR SHALL ABIDE BY ALL STATE, LOCAL, AND FEDERAL LAWS, CODES, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS, INCLUDING EPA AND ADA REQUIREMENTS.
- ALL SITE PREPARATION, GRADING OPERATIONS, FOUNDATION CONSTRUCTION, AND PAVEMENT INSTALLATION WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT, WHICH WILL BE PROVIDED BY THE OWNER OR ARCHITECT. ALL OTHER WORK SHALL, UNLESS OTHERWISE NOTED IN THE PLANS, BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
- NO WORK SHALL BE PERFORMED WITHOUT THE APPROPRIATE PERMITS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS FOR THE PROJECT PRIOR TO COMMENCING CONSTRUCTION, OR PRIOR TO OCCUPANCY, AS APPROPRIATE. IF PERMITS ARE DELAYED OR ISSUED WITH CONDITIONS, THE CONTRACTOR SHALL NOTIFY THE OWNER AND ARCHITECT IMMEDIATELY.
- COORDINATE WORK WITH SITE PLAN, UTILITY PLAN, DEMOLITION PLAN, AND LANDSCAPE PLAN.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING OBSTRUCTIONS, AND CONDITION OF ALL EXISTING INFRASTRUCTURE PRIOR TO CONSTRUCTION. REPORT ALL DISCREPANCIES TO THE ENGINEER AND VERIFY THE ENGINEER'S INTENT BEFORE PROCEEDING.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE SAFETY.
- THE CONTRACTOR SHALL MAINTAIN RECORD DRAWINGS ON SITE AT ALL TIMES. THE CONTRACTOR SHALL NOT SCALE DRAWINGS. ONLY WRITTEN DIMENSIONS OR KEYED NOTES SHALL BE USED.
- CONSTRUCTION ACTIVITY SHALL BE LIMITED TO THE PROPERTY AND/OR PROJECT LIMITS. ANY DAMAGE TO ADJACENT STRUCTURES RESULTING FROM THE CONSTRUCTION PROCESS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL BE RESPONSIBLE FOR DOCUMENTING EXISTING CONDITIONS PRIOR TO CONSTRUCTION.
- CONSTRUCTION EQUIPMENT SHALL NOT OBSTRUCT DRIVEWAYS. EQUIPMENT SHALL ONLY OBSTRUCT DESIGNATED TRAFFIC LANES IF APPROPRIATE BARRICADING PERMITS HAVE BEEN OBTAINED. THE CONTRACTOR SHALL NOT STORE ANY EQUIPMENT OR MATERIAL IN THE RIGHT-OF-WAY.
- THE CONTRACTOR SHALL PROVIDE A CONSTRUCTION TRAFFIC CONTROL AND SIGNING PLAN THAT CONFORMS TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND LOCAL REQUIREMENTS. THE CONTRACTOR SHALL OBTAIN BARRICADING PERMITS FROM THE APPROPRIATE AUTHORITIES PRIOR TO ANY CONSTRUCTION WORK ON OR ADJACENT TO EXISTING STREETS.
- THE CONTRACTOR SHALL MAINTAIN ALL BARRICADING AND CONSTRUCTION SIGNING AT ALL TIMES. THE CONTRACTOR SHALL VERIFY THE PROPER LOCATION OF ALL BARRICADING AT THE END AND BEGINNING OF EACH DAY.
- EXISTING UTILITY LINES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND MAY BE INCOMPLETE OR OBSOLETE. SUCH LINES MAY OR MAY NOT EXIST WHERE SHOWN OR NOT SHOWN. CONTRACTOR SHALL CONTACT NM-811 FOR UTILITY LINE SPOTS FIVE WORKING DAYS PRIOR TO CONDUCTING SITE FIELD WORK. CONTRACTOR SHALL FIELD VERIFY AND LOCATE ALL UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES, AND UNDERGROUND UTILITY LINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF NECESSARY DRY UTILITY ADJUSTMENTS.
- ALL TRASH, DEBRIS, & SURFACE VEGETATION SHALL BE CLEARED AND LEGALLY DISPOSED OF OFFSITE.
- VIBRATORY COMPACTION SHALL NOT BE USED OVER IN-PLACE UTILITIES.
- SOIL TESTING AND INSPECTION SERVICES DURING SITE OPERATIONS ARE REQUIRED. CONTRACTOR SHALL ALLOW TESTING LABS TO INSPECT AND APPROVE COMPACTED SUBGRADES, BACKFILL, AND FILL LAYERS BEFORE FURTHER CONSTRUCTION WORK IS DONE. SHOULD COMPACTION TESTS INDICATE INADEQUATE DENSITY, CONTRACTOR SHALL PROVIDE ADDITIONAL COMPACTION AND TESTING AT THE CONTRACTOR'S SOLE EXPENSE.
- CONTRACTOR SHALL LOCATE AND PRESERVE ALL BOUNDARY CORNERS AND REPLACE ANY LOST OR DISTURBED CORNERS AT CONTRACTOR'S SOLE EXPENSE. PROPERTY CORNERS SHALL ONLY BE RESET BY A REGISTERED LAND SURVEYOR.
- CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING TO FACILITATE ACCURACY IN CONSTRUCTION STAKING. UPON WRITTEN REQUEST FROM THE CONTRACTOR, A FILE CONTAINING THE ELECTRONIC DATA COMPRISING THE SITE CIVIL DRAWINGS WILL BE FORWARDED TO THE LICENSED LAND SURVEYOR TO PERFORM CONSTRUCTION STAKING. ALL SITE CONSTRUCTION LAYOUT MUST BE PERFORMED BY A LICENSED SURVEYOR USING ELECTRONIC DATA PROVIDED IN AUTOCAD \*.DWG (CURRENT VERSION) BY ISAACSON & ARFMAN, P.A. CONTACT PROJECT CIVIL ENGINEER, ASA NILSSON-WEBER, PE AT (505)-266-1688.
- ADJUST ANY RIMS OF EXISTING UTILITY FEATURES AS NECESSARY TO MATCH NEW GRADES. UTILITIES IN PAVED AREAS SHALL BE HS-25 TRAFFIC RATED.
- CONTRACTOR SHALL COMPLY WITH LOCAL REGULATIONS FOR RESEEDING OF DISTURBED AREAS.

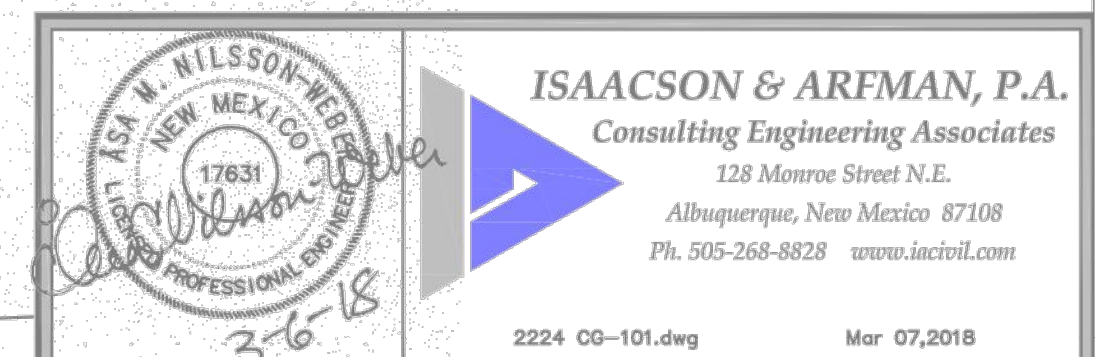
## LEGEND

- 78.3 PROPOSED ELEVATION
- PG=4968.5 PAD GRADE ELEVATION
- PROPOSED RETAINING WALL (DESIGN BY OTHERS)
- PROPOSED GARDEN RETAINING WALL
- TW=70.5 BW=68.3 TOP OF RETAINING (SEE DETAIL THIS SHEET)
- BOTTOM OF RETAINING
- FLOW ARROW

## RETAINING WALL PLAN



NOTE: ALL RETAINING WALLS/FLOODWALLS SHALL BE DESIGNED BY OTHERS. THE DETAILS/SECTIONS SHOWN ON THIS SHEET ONLY DEPICT TOP AND BOTTOM AND MAX. HEIGHTS OF RETAINING WALLS.



ISAACSON & ARFMAN, P.A.  
Consulting Engineering Associates  
128 Monroe Street N.E.  
Albuquerque, New Mexico 87108  
Ph. 505-268-8828 www.iactoll.com

2224 CG-101.dwg Mar 07,2018

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## CAMPBELL COMPOUND

LAS VENTANAS NM, INC.

## DETAILS & RETAINING WALL PLAN

Date:	No. Revision:	Date:	Job No.
Sept 2017			2224
Drawn By:			G-13
ANW			
Chk By:			SH.2 OF 2