

CITY OF ALBUQUERQUE

Planning Department
Brennon Williams, Director



Mayor Timothy M. Keller

August 30, 2021

Carlos Iglesias
Cumulus Design
2080 N. Highway 360, Ste. 240
Grand Prairie, TX 75050

**Re: Chase Bank ATM
11004 Montgomery Blvd NE
Traffic Circulation Layout
Engineer's Stamp 08-27-2021 (G21-D018A(A))**

Dear Mr. Carlos Iglesias,

The TCL submittal received 08-30-2021 is approved for Building Permit with. A copy of the stamped and signed plan will be needed for each of the building permit plans. Please keep the original to be used for certification of the site for final C.O. for Transportation.

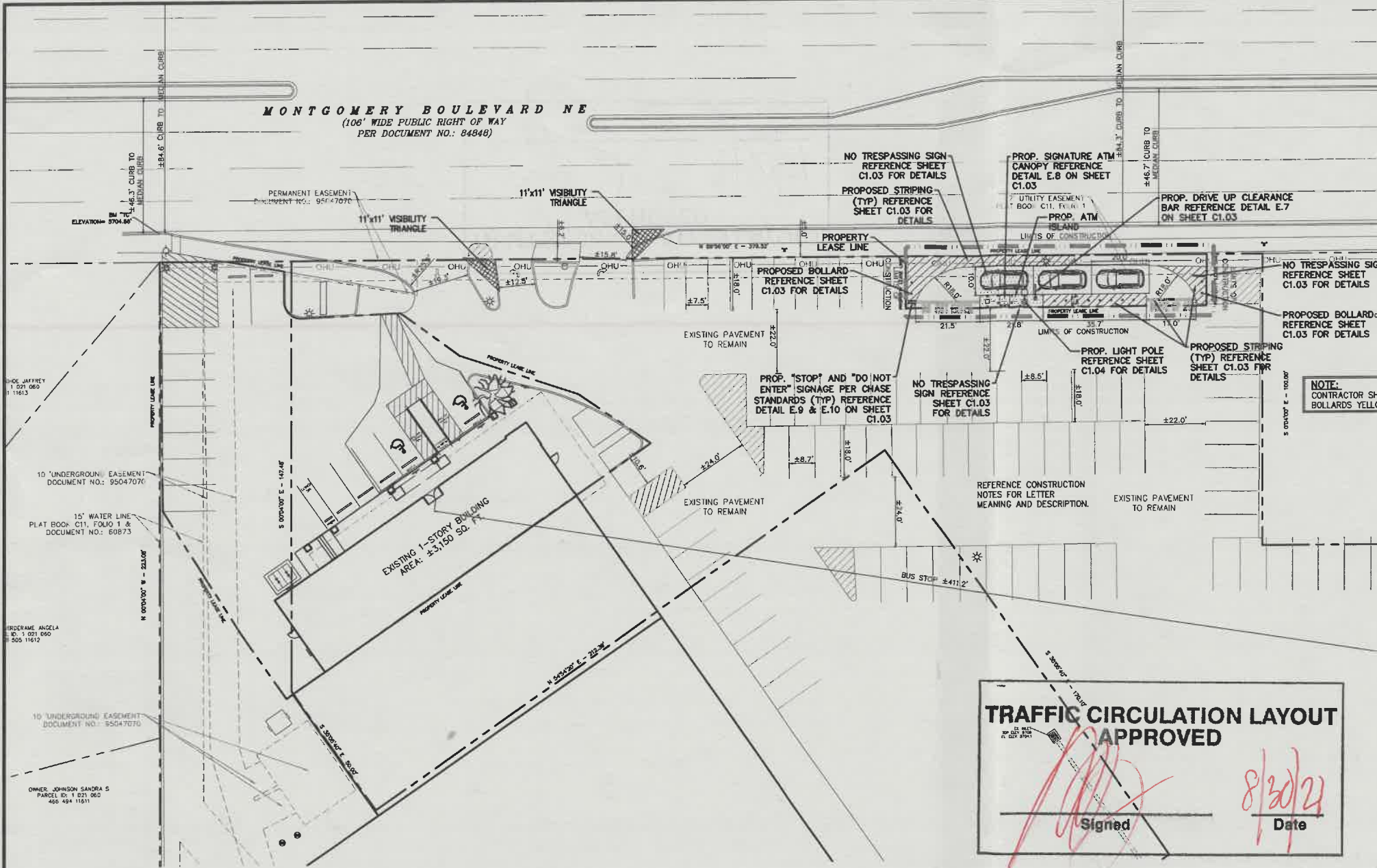
When the site construction is completed and an inspection for Certificate of Occupancy (C.O.) is requested, use the original City stamped approved TCL for certification. Redline any minor changes and adjustments that were made in the field. A NM registered architect or engineer must stamp, sign, and date the certification TCL along with indicating that the development was built in "substantial compliance" with the TCL. Submit this certification, the TCL, and a completed Drainage and Transportation Information Sheet to front counter personnel for log in and evaluation by Transportation.

Once verification of certification is completed and approved, notification will be made to Building Safety to issue Final C.O. To confirm that a final C.O. has been issued, call Building Safety at 924-3690.

Sincerely,


Nilo Salgado-Fernandez, P.E.
Senior Traffic Engineer, Planning Dept.
Development Review Services

C: CO Clerk, File



LEGEND

- EXISTING CURB
- PROPOSED CURB
- PROPOSED PAVEMENT
- PROPOSED SIDEWALK
- PARKING COUNT
- PROPOSED LANDSCAPE AREA

10
LS

NOTE:
CONTRACTOR SHALL BOLLARDS YELLOW

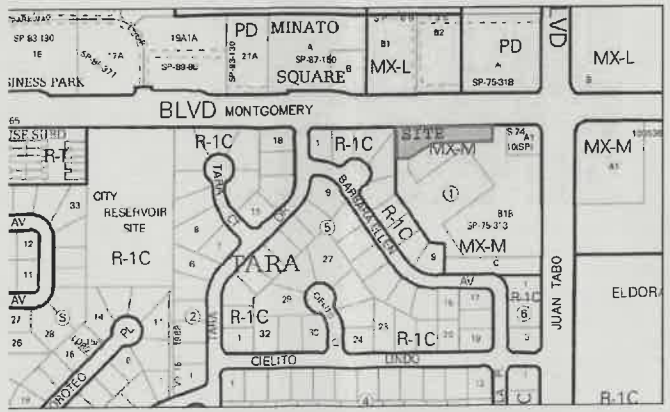
SITE PLAN	
EXISTING ZONING: MX-M/MIXED USE	
LAND USE:	FINANCIAL INSTITUTION
LEASE ACREAGE:	0.356 ACRES (15,512 SQ. FT.)
BUILDING FOOT PRINT (TOTAL):	±3,150 SQUARE FEET
OPEN SPACE PROVIDED:	±418 SQUARE FEET (2.7%)
BUILDING COVERAGE:	3,150/15,512: 20.3%
APPROX EX. IMPERVIOUS AREA:	±15,194 SQUARE FEET (98.0%)
APPROX PROPOSED IMPERVIOUS AREA:	±15,194 SQUARE FEET (98.0%)
PARKING REQUIRED (3 PER 1000 SF OF PROPOSED BUILDING):	10 SPACES
PARKING PROVIDED:	14 SPACES
ACCESSIBLE PARKING REQUIRED (1 ACCESSIBLE SPACE/25 STANDARD SPACE):	1 SPACES
PARKING PROVIDED (ACCESSIBLE):	2 SPACES
BICYCLE SPACE: REQUIRED / PROVIDED	3/4 SPACES
MOTORCYCLE SPACES: REQUIRED / PROVIDED	1/1 SPACES
REQUIRED STACKING SPACES:	4 SPACES PER ATM
PROVIDED STACKING SPACES:	3 SPACES PER ATM

- GENERAL NOTES**
1. DEVIATIONS FROM THESE STANDARDS SHALL BE SUBMITTED TO THE CITY ENGINEER AND/OR CITY TRAFFIC ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
 2. SUBGRADE UNDER SIDEWALKS AND DRIVEPADS SHALL BE COMPACTED IN ACCORDANCE WITH SECTION 301.
 3. FOR SIDEWALKS LESS THAN 60' WIDE ON ACCESSIBLE ROUTE, PASSING SPACE AT LEAST 60' X 60' SHALL BE PROVIDED AT LEAST EVERY 200 FT.
 4. GRATINGS LOCATED IN WALKING SURFACE SHALL HAVE SPACES NO GREATER THAN 1/2" WIDE IN DIRECTION OF TRAVEL. IF OPENINGS ARE ELONGATED, LONG DIMENSION SHALL BE PLACED PERPENDICULAR TO DIRECTION OF TRAVEL.
 5. ALONG THE ACCESSIBLE ROUTE, CHANGES IN LEVEL BETWEEN 1/4" AND 1/2" SHALL BE BEVELED WITH A SLOPE NO GREATER THAN 2H:1V. CHANGES IN LEVEL GREATER THAN 1/2" REQUIRE A RAMP.
 6. PROVIDE A MINIMUM SIDEWALK WIDTH OF 4' AROUND OBSTACLES FOR ADA ACCESS.
 7. SEE COA STD DWG 2425A AND 2425B FOR DRIVEPAD DETAILS.
- CONSTRUCTION NOTES**
- A. SLOPE 2% MAX. 1.5% PREFERRED SLOPE.
 - B. 5' MIN. SIDEWALK WIDTH. SIDEWALK WIDTH SHALL BE IN ACCORDANCE WITH CHAPTER 23 OF DEVELOPMENT PROCESS MANUAL.
 - C. SETBACK TO BE DETERMINED BY AVAILABLE RIGHT-OF-WAY. SEE CHAPTER 7 OF DEVELOPMENT PROCESS MANUAL. ALSO SEE COA STD. DWG. 2414 FOR LANDSCAPE BUFFER.
 - D. SEE DRIVEPAD DETAILS, DWG. 2425A AND 2425B.
 - E. WALKWAY VARIABLE (4' MINIMUM).
 - F. PROPERTY LINE.
 - G. 1/2" EXPANSION JOINTS WHERE SIDEWALK OR DRIVEPAD ABUTS BUILDINGS, FENCES, WALLS OR OTHER IMMOVABLE OBJECTS.
 - H. HEADER CURB OR INTEGRAL CURB AS REQUIRED TO MEET GRADE AT BACK OF SIDEWALK. SEE STD. DWG. 2415.
 - I. CONTRACTION JOINTS.
 - J. FOR CURB ACCESS RAMPS, SEE DWGS. 2440 THROUGH 2445.
 - K. CHECK DIMENSION FROM BOTH PROPERTY LINE AND FLOW LINE. USE IN AREAS WHERE DRIVEPAD IS FARTHEST FROM INTERSECTION.
 - L. RAMP AS REQUIRED TO MEET DRIVEPAD GRADE. 8.3% MAX. SLOPE, 7% PREFERRED SLOPE.
 - M. ADA ACCESSIBLE PATHWAY. 2% MAX. CROSS-SLOPE. 1.5% PREFERRED CROSS-SLOPE.

- NOTE:**
ALL DIMENSIONS ARE FROM FACE OF CURB OR EDGE OF PAVEMENT
- NOTE:**
LIGHT POLES SHOULD NOT BE INSTALLED IN PARKING SPACES AND DRIVEWAYS. CONTRACTOR MUST MAKE SURE THE LIGHT POLES ARE INSTALLED AWAY FROM EXISTING AND PROPOSED UNDERGROUND UTILITIES.
- NOTE:**
LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
- NOTE:**
ALL BROKEN OR CRACKED SIDEWALK MUST BE REPLACED WITH SIDEWALK AND CURB & GUTTER.

EXISTING CONDITIONS PARKING ANALYSIS	
EXISTING ZONING: MX-M/MIXED USE	
TENANT ESTABLISHMENTS	
WECK'S	8 PARKING PER 1,000 SQ. FT. GFA
CLUB PILATES	2.5 PARKING PER 1,000 SQ. FT. GFA
VERIZON	4 PARKING PER 1,000 SQ. FT. GFA
WALMART	4 PARKING PER 1,000 SQ. FT. GFA
UPS STORE	4 PARKING PER 1,000 SQ. FT. GFA
DOMINO'S	4 PARKING PER 1,000 SQ. FT. GFA
T-MOBILE	4 PARKING PER 1,000 SQ. FT. GFA
H&R BLOCK	4 PARKING PER 1,000 SQ. FT. GFA
NAIL SALON	4 PARKING PER 1,000 SQ. FT. GFA
GREAT CLIPS	4 PARKING PER 1,000 SQ. FT. GFA
BASKIN ROBINS	4 PARKING PER 1,000 SQ. FT. GFA
REQUIRED PARKING	204 PARKING SPACES
PROVIDED PARKING	257 PARKING SPACES

PROPOSED CONDITIONS PARKING ANALYSIS	
EXISTING ZONING: MX-M/MIXED USE	
TENANT ESTABLISHMENTS	
CHASE BANK	3 PARKING PER 1,000 SQ. FT. GFA
CLUB PILATES	2.5 PARKING PER 1,000 SQ. FT. GFA
VERIZON	4 PARKING PER 1,000 SQ. FT. GFA
WALMART	4 PARKING PER 1,000 SQ. FT. GFA
UPS STORE	4 PARKING PER 1,000 SQ. FT. GFA
DOMINO'S	4 PARKING PER 1,000 SQ. FT. GFA
T-MOBILE	4 PARKING PER 1,000 SQ. FT. GFA
H&R BLOCK	4 PARKING PER 1,000 SQ. FT. GFA
NAIL SALON	4 PARKING PER 1,000 SQ. FT. GFA
GREAT CLIPS	4 PARKING PER 1,000 SQ. FT. GFA
BASKIN ROBINS	4 PARKING PER 1,000 SQ. FT. GFA
REQUIRED PARKING	188 PARKING SPACES
PROVIDED PARKING	254 PARKING SPACES



VICINITY MAP
ZONE ATLAS MAP G-21 NTS



CALL NM ONE-CALL
SYSTEM SEVEN (7) DAYS
PRIOR TO ANY EXCAVATION

III CAUTION III
UNDERGROUND UTILITIES

EXISTING UTILITIES AND UNDERGROUND FACILITIES INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY VARIOUS PARTIES. THE ENGINEER DOES NOT ASSUME THE RESPONSIBILITY FOR THE UTILITY LOCATIONS SHOWN. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR(S) TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL UTILITIES AND UNDERGROUND FACILITIES PRIOR TO CONSTRUCTION, TO TAKE PRECAUTIONS IN ORDER TO PROTECT ALL FACILITIES ENCOUNTERED AND NOTIFY THE ENGINEER OF ALL CONFLICTS OF THE WORK WITH EXISTING FACILITIES. THE CONTRACTOR SHALL PROTECT AND MAINTAIN ALL UTILITIES FROM DAMAGE DURING CONSTRUCTION. ANY DAMAGE BY THE CONTRACTOR TO UTILITIES SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THEIR OWN EXPENSE.

CALL: NEW MEXICO ONE CALL @ NM 811 AT LEAST 7 DAYS PRIOR TO CONSTRUCTION.

Cumulus Design
2080 N. Highway 360, Suite 240
Grand Prairie, Texas 75050
Tel: 214.235.0367



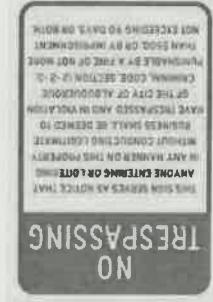
TRAFFIC CIRCULATION PLAN
11004 MONTGOMERY BOULEVARD NE
CITY OF ALBUQUERQUE, NEW MEXICO
BERNALILLO COUNTY

CHASEO
PLOT DATE
08/27/21
DRAWING SCALE
1" = 20'
PROJECT NUMBER
CDC21003
SHEET NUMBER
C1.02

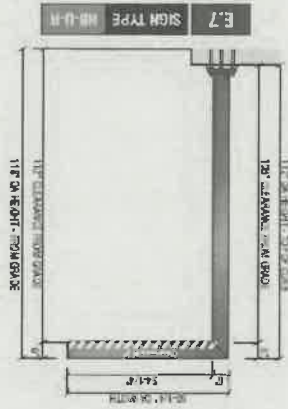
G21-DO18A(A)

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NO TRESPASSING SIGN

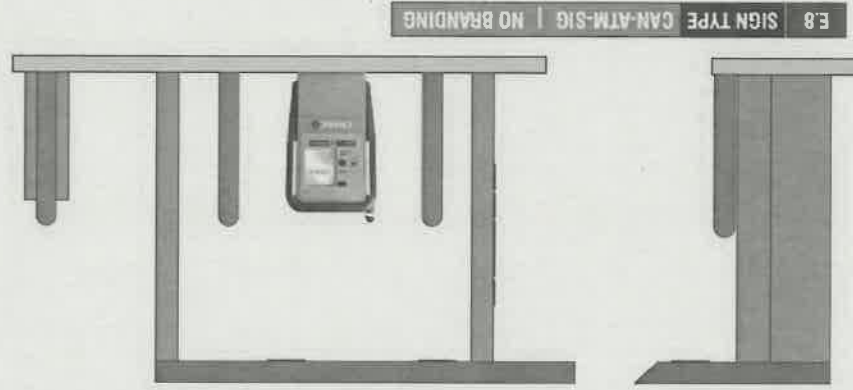


NON-ILLUMINATED DRIVE UP CLEARANCE BAR



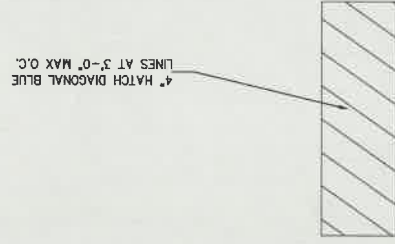
E.7

ILLUMINATED SIGNATURE ATM CANOPY WITH OCTAGON



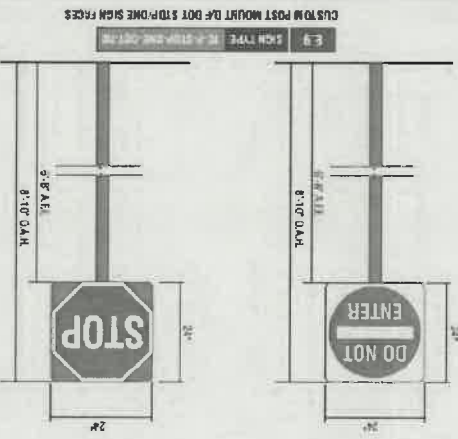
E.8

NO PARKING STRIPING DETAIL



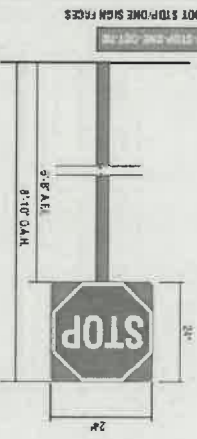
TRAFFIC CIRCULATION LAYOUT
APPROVED
Signed _____
Date 8/30/21

DO NOT ENTER SIGN R5-1 N.T.S.

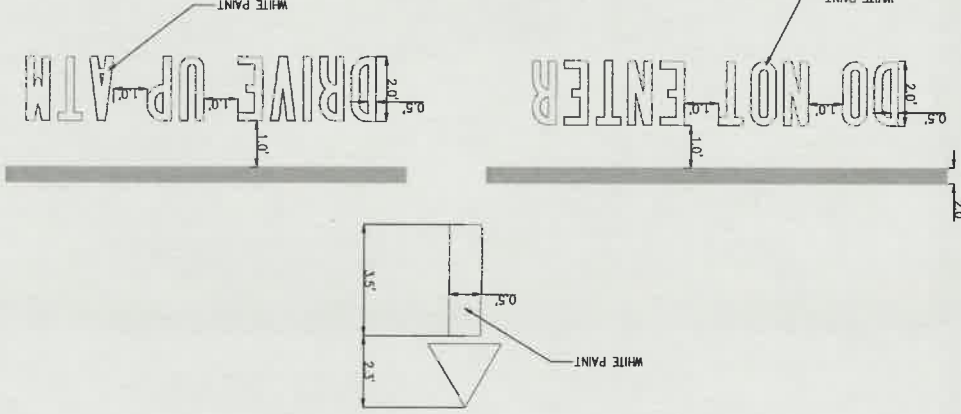


E.9 E.10

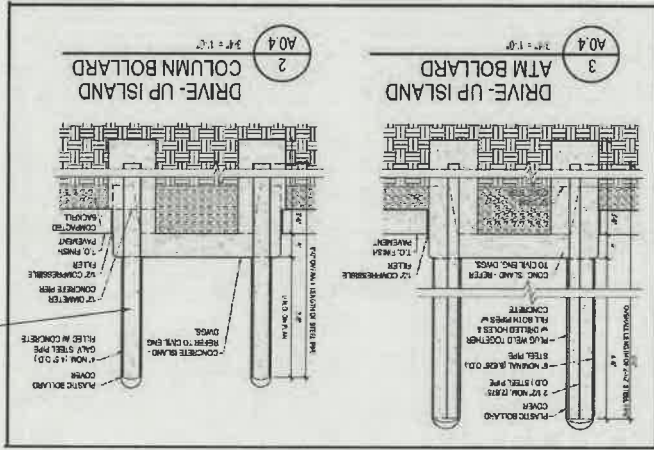
STOP SIGN R1-1 N.T.S.



TEXT DETAILS



BOLLARD DETAIL



BOLLARD TO BE PAINTED YELLOW

C1.03

SHEET NUMBER

CDCC21003

PROJECT NUMBER

DRAWING SCALE

08/27/21

PLOT DATE

CHASEO

TRAFFIC CIRCULATION PLAN

11004 MONTGOMERY BOULEVARD NE
CITY OF ALBUQUERQUE, NEW MEXICO
BERNALILLO COUNTY



Cumulus Design
2080 N. Highway 360, Suite 240
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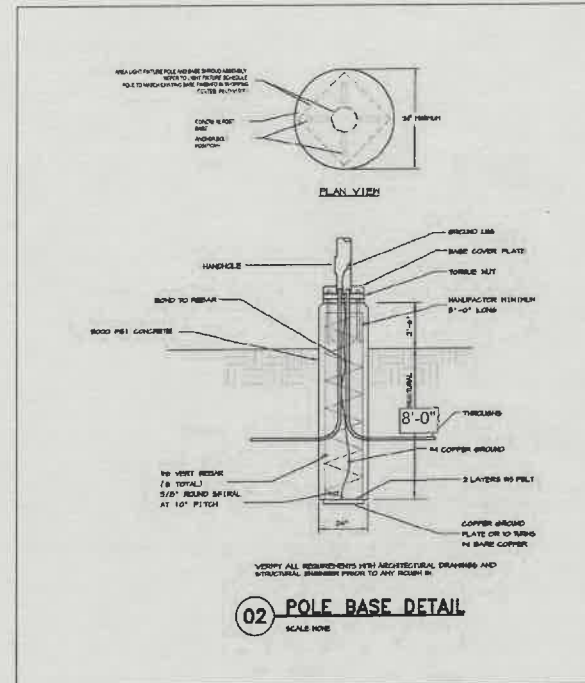
G21-D018A(A)

TRAFFIC CIRCULATION LAYOUT
APPROVED

Signed

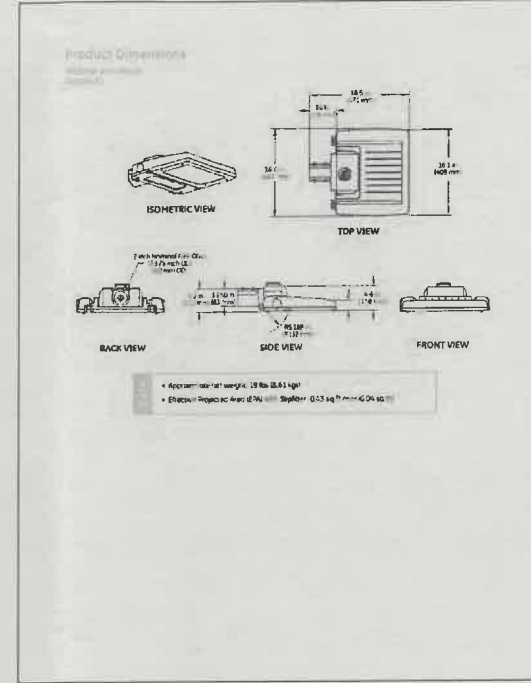
Date

8/30/21



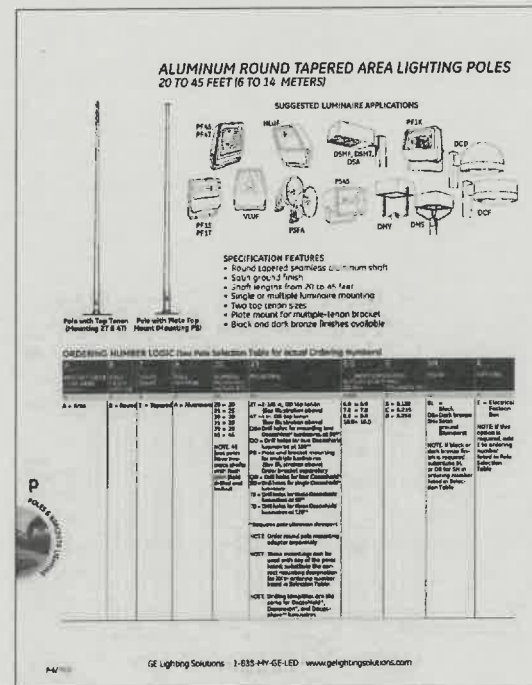
LIGHT POLE BASE DETAIL

N.T.S.



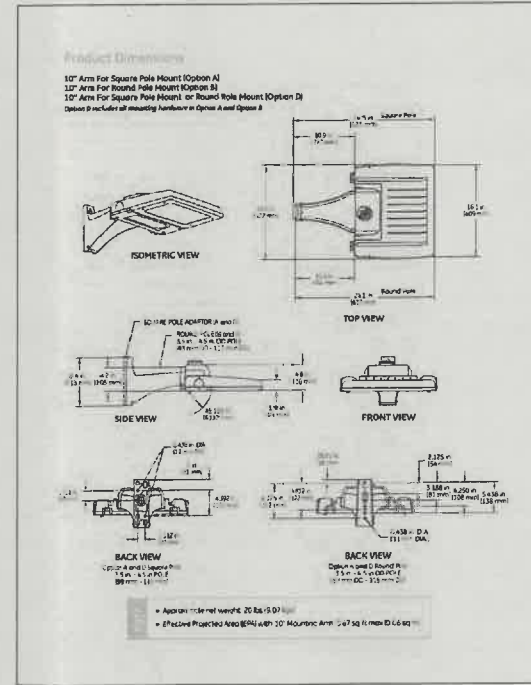
LIGHT FIXTURE *MANUFACTURER SPECS

N.T.S.



LIGHT FIXTURE *MANUFACTURER SPECS

N.T.S.



LIGHT POLE *MANUFACTURER SPECS

N.T.S.

LIGHT POLE DETAILS

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11004 MONTGOMERY BOULEVARD NE
CITY OF ALBUQUERQUE, NEW MEXICO
BERNALILLO COUNTY

CHASE

PLOT DATE
08/27/21
DRAWING SCALE

PROJECT NUMBER
CDC21003

SHEET NUMBER

C1.04

921-0018A(A)