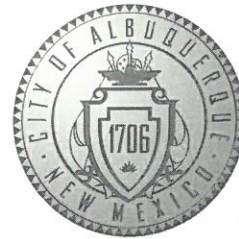


CITY OF ALBUQUERQUE



November 22, 2017

Mullen Heller Architecture P.C.
Doug Heller
1718 Central Ave. SW
Albuquerque, NM 87104

Re: Shell Building @ Los Lomas
2101 Vista Oeste NW
Traffic Circulation Layout
Architect's Stamp 11-10-2017 (H10-D031)

Dear Mr. Heller,

The TCL submittal received 11-13-17 is approved for Building Permit. A copy of the stamped and signed plan will be needed for each of the building permit plans. Please keep the original to be used for certification of the site for final C.O. for Transportation.

When the site construction is completed and a Certificate of Occupancy (C.O.) is requested, use the original City stamped approved TCL for certification. Redline any minor changes and adjustments that were made in the field. A NM registered architect or engineer must stamp, sign, and date the certification TCL along with indicating that the development was built in "substantial compliance" with the TCL. Submit this certification TCL with a completed Drainage and Transportation Information Sheet to front counter personnel for log in and evaluation by Transportation.

Once verification of certification is completed and approved, notification will be made to Building Safety to issue Final C.O. To confirm that a final C.O. has been issued, call Building Safety at 924-3690.

Sincerely,

Logan Patz
Senior Engineer, Planning Department
Development Review Services

LWP via: email
C: File

DRAINAGE AND TRANSPORTATION INFORMATION SHEET

(REV. 1/28/2003rd)

PROJECT TITLE: Shell Building @ Los Lomas

ZONE MAP/DRG. FILE #: H-9-Z H10 D031

DRB #: N/A

EPC#: N/A

WORK ORDER#: N/A

LEGAL DESCRIPTION: Track 15 & 16 of Ladera Business Park Unit 1

CITY ADDRESS: 2101 Vista Oeste Building 1 Albuquerque, New Mexico 87120

ENGINEERING FIRM: Mark Goodwin & Associates

ADDRESS: PO Box 90606

CITY, STATE: Albuquerque, NM

CONTACT: Mark Goodwin

PHONE: 505-828-2200

ZIP CODE: 87199

OWNER: Sky City Business Enterprises

ADDRESS: x

CITY, STATE: Albuquerque, NM.

CONTACT: John Mohr

PHONE: x

ZIP CODE: x

ARCHITECT: Mullen Heller Architecture P.C.

ADDRESS: 1718 Central Avenue SW, Suite D

CITY, STATE: Albuquerque, NM

CONTACT: Doug Heller

PHONE: 505-268-4144

ZIP CODE: 87102

SURVEYOR: The Survey Office LLC

ADDRESS: 333 Lomas Boulevard NE

CITY, STATE: Albuquerque, NM

CONTACT: Anthony Harris

PHONE: 505-998-0303

ZIP CODE: 87102

CONTRACTOR: TBD

ADDRESS: _____

CITY, STATE: _____

CONTACT: _____

PHONE: _____

ZIP CODE: _____

CHECK TYPE OF SUBMITTAL:

- ☐ DRAINAGE REPORT
- ☐ DRAINAGE PLAN 1ST SUBMITTAL, *REQUIRES TCL or equal*
- ☐ DRAINAGE PLAN RESUBMITTAL
- ☐ CONCEPTUAL GRADING & DRAINAGE PLAN
- ☐ GRADING PLAN
- ☐ EROSION CONTROL PLAN
- ☐ ENGINEER'S CERTIFICATION (HYDROLOGY)
- ☐ CLOMR/LOMR
- ☒ TRAFFIC CIRCULATION LAYOUT (TCL)
- ☐ ENGINEERS CERTIFICATION (TCL)
- ☐ ENGINEERS CERTIFICATION (DRB APPR. SITE PLAN)
- ☐ OTHER

CHECK TYPE OF APPROVAL SOUGHT:

- ☐ SIA / FINANCIAL GUARANTEE RELEASE
- ☐ PRELIMINARY PLAT APPROVAL
- ☐ S. DEV. PLAN FOR SUB'D. APPROVAL
- ☐ S. DEV. PLAN FOR BLDG. PERMIT APPROVAL
- ☐ SECTOR PLAN APPROVAL
- ☐ FINAL PLAT APPROVAL
- ☐ FOUNDATION PERMIT APPROVAL
- ☒ BUILDING PERMIT APPROVAL
- ☐ CERTIFICATE OF OCCUPANCY (PERM.)
- ☐ CERTIFICATE OF OCCUPANCY (TEMP.)
- ☐ GRADING PERMIT APPROVAL
- ☐ PAVING PERMIT APPROVAL
- ☐ WORK ORDER APPROVAL
- ☐ OTHER (SPECIFY)

WAS A PRE-DESIGN CONFERENCE ATTENDED:

- ☐ YES
- ☒ NO
- ☐ COPY PROVIDED

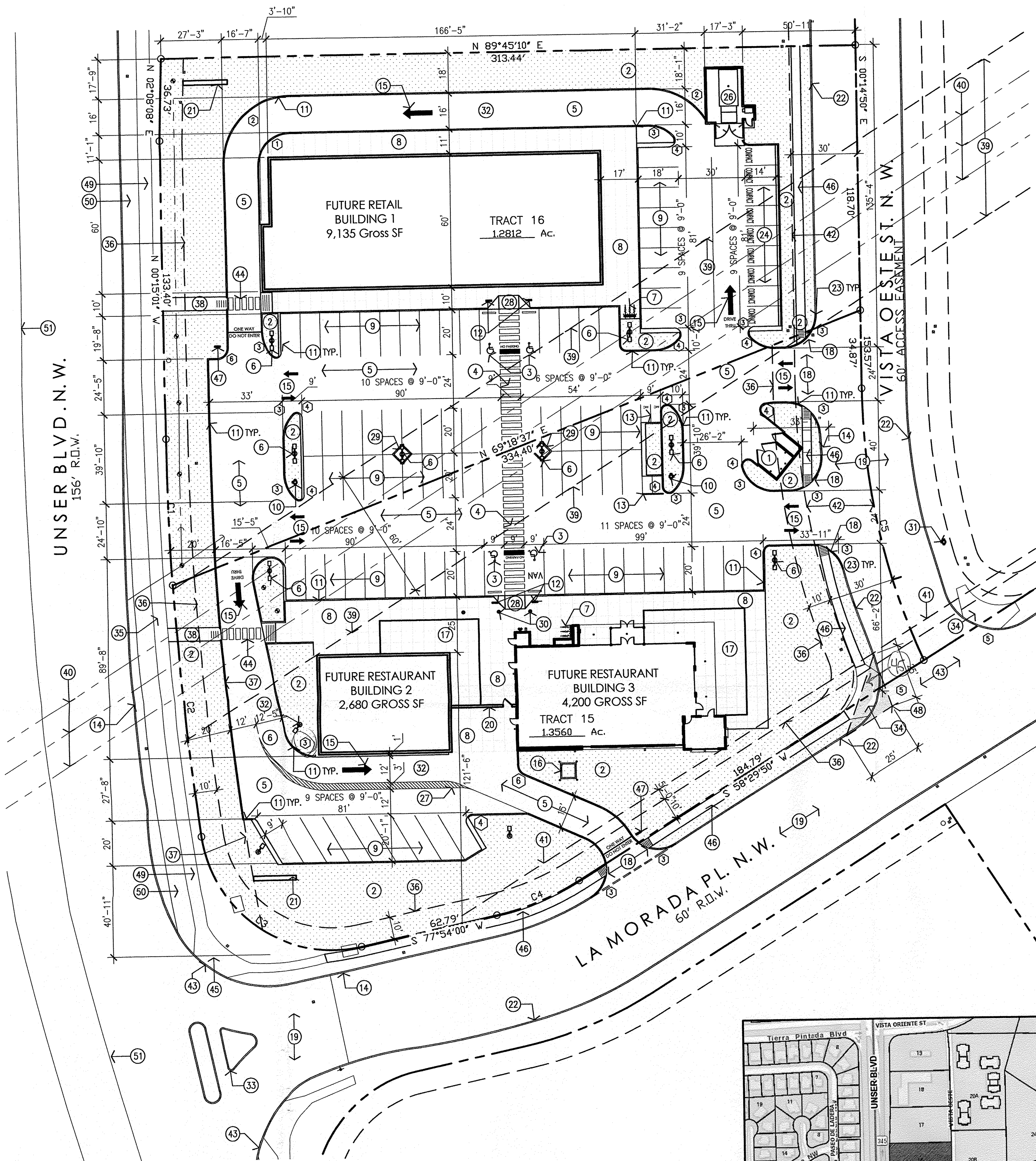
DATE SUBMITTED: November 13, 2017

BY: Doug Heller

Requests for approvals of Site Development Plans and/or Subdivision Plats shall be accompanied by a drainage submittal. The particular nature, location and scope of the proposed development defines the degree of drainage detail. One or more of the following levels of submittal may be required based on the following:

1. **Conceptual Grading and Drainage Plan:** Required for approval of Site Development Plans greater than five (5) acres and Sector Plans.
2. **Drainage Plans:** Required for building permits, grading permits, paving permits and site plans less than five (5) acres.
3. **Drainage Report:** Required for subdivisions containing more than ten (10) lots or constituting five (5) acres or more.





1 Architectural Site Plan
Scale: 1"=30'-0"



VICINITY MAP:

GENERAL NOTES:

- [A] THE PARKING LOT DIMENSIONS ARE FROM FACE OF CURB TO FACE OF CURB.
- [B] LANDSCAPING AND SIGNING WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THIS AREA.
- [C] ANY ENCROACHMENT OR OVERHANG INTO THE ROW FROM SIGNS, FENCES, CANOPY, ROOF LINES, ETC. WILL REQUIRE AN ANNUAL REVOCABLE ENCROACHMENT PERMIT.
- [D] ALL CURBS SURROUNDING LANDSCAPING, SHALL PROVIDE WATER RUNOFF OPENINGS TO ALLOW SURFACE DRAINAGE INTO THE LANDSCAPED AREAS (WATER HARVESTING).
- [E] ALL SIGNAGE AND FENCES WILL BE DONE UNDER SEPARATE PERMIT.
- [F] ALL IMPROVEMENTS LOCATED IN THE RIGHT OF WAY MUST BE INCLUDED ON A PUBLIC WORK ORDER.

KEYED NOTES:

- [1] PROPOSED REFUSE ENCLOSURE.
- [2] PROPOSED LANDSCAPING AREA. REFER TO LANDSCAPING PLAN.
- [3] PROPOSED HANDICAP PAVEMENT SIGN.
- [4] PROPOSED STRIPED HANDICAP AISLE WITH 12" HIGH PAINTED LETTERING "NO PARKING" PER 66-1-4.1.5 NMSA 1978.
- [5] PROPOSED ASPHALT PAVING.
- [6] PROPOSED SITE LIGHTING. HEIGHT NOT TO EXCEED 20' TALL.
- [7] PROPOSED BIKE RACK WITH 3 SPACES.
- [8] PROPOSED CONCRETE SIDEWALK WITH CONTROL JOINTS EVERY 5'-0" AND EXPANSION JOINTS EVERY 20'-0".
- [9] PROPOSED PAINTED PARKING STRIPING.
- [10] PROPOSED FIRE HYDRANT LOCATION. REFER TO UTILITY PLAN.
- [11] PROPOSED CONCRETE CURB. SEE GRADING & DRAINAGE PLAN.
- [12] PROPOSED HANDICAP PARKING SIGN. "VAN" WHERE NOTED. SIGN TO BE NOTED PER 66-7-352.4C NMSA 1978. "VIOLATORS ARE SUBJECT TO A FINE AND/OR TOWING".
- [13] PROPOSED 4'-6" WIDE x 8'-0" LONG MOTORCYCLE SPACES WITH SIGNAGE AT EACH SPACE.
- [14] EXISTING CITY CURB AND GUTTER TO REMAIN.
- [15] PROPOSED PAINTED DIRECTION ARROW ON PAVEMENT.
- [16] PROPOSED ELECTRICAL TRANSFORMER LOCATION.
- [17] PROPOSED PATIO.
- [18] THE NEW DRIVEWAY CURB INTO EXISTING CURB AND GUTTER SYSTEM. REFER TO GRADING AND DRAINAGE PLAN.
- [19] EXISTING ASPHALT ROAD TO REMAIN.
- [20] PROPOSED 6'-0" TALL PATIO FENCE.
- [21] PROPOSED 75 SF MONUMENT SIGN.
- [22] EXISTING CONCRETE CURB TO REMAIN.
- [23] CLEAR SIGHT TRIANGLE: LANDSCAPING, FENCING, AND SIGNING WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
- [24] COMPACT PARKING SPACES 9'-0"x15'-0" WITH PAINTED "COMPACT CAR" PAVEMENT DESIGNATION.
- [25] FUTURE CONCRETE CURB. SEE GRADING & DRAINAGE PLAN AND COA STD. DWG. 2415A.
- [26] FUTURE TRASH COMPACTOR.
- [27] PROPOSED STRIPING.
- [28] PROPOSED HC RAMP.
- [29] LANDSCAPE ISLAND.
- [30] BOLLARD LIGHT FIXTURE. SEE ELECTRICAL.
- [31] EXISTING FIRE HYDRANT TO REMAIN.
- [32] PROPOSED DRIVE THRU LANE.
- [33] EXISTING TRAFFIC MEDIAN TO REMAIN.
- [34] EXISTING CITY STANDARD HC RAMP.
- [35] EXISTING CITY STANDARD SIDEWALK.
- [36] 10' PUBLIC UTILITY EASEMENT.
- [37] 20' PUBLIC WATERLINE EASEMENT.
- [38] PROPOSED 6' SIDEWALK CONNECTION TO EXISTING PUBLIC SIDEWALK.
- [39] EXISTING 60' CONTINENTAL DIVIDE ELECTRIC COOPERATIVE EASEMENT FOR OVERHEAD POWER LINES.
- [40] EXISTING OVERHEAD POWER LINES.
- [41] EXISTING PNM EASEMENT.
- [42] 30' ACCESS EASEMENT.
- [43] EXISTING CURB CUT TO REMAIN.
- [44] PROPOSED PAINTED ASPHALT PEDESTRIAN PATHWAY.
- [45] EXISTING ADA PUBLIC SIDEWALK. INSTALL TRUNCATED DOMES TO MEET CITY STANDARDS.
- [46] PROPOSED 4' WIDE CONCRETE SIDEWALK.
- [47] PROPOSED STANDARD SIGNAGE TO READ "DO NOT ENTER".
- [48] CLEAR SIGHT TRIANGLE: LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
- [49] EXISTING PUBLIC SIDEWALK.
- [50] EXISTING PUBLIC ASPHALT TRAIL.
- [51] EXISTING MEDIAN.

SITE LIGHTING NOTES:

- [A] THE LOCATION OF THE LIGHTING FIXTURES TOGETHER WITH ITS CUT-OFF ANGLE SHALL BE SUCH THAT IT DOES NOT DIRECTLY SHINE ON ANY PUBLIC RIGHT-OF-WAY OR ADJACENT RESIDENTIAL PROPERTY.
- [B] THE MAXIMUM HEIGHT OF A LIGHT POLE MEASURED FROM THE FINISH GRADE TO THE TOP OF THE POLE SHALL BE 20'-0" MAXIMUM.
- [C] LIGHTING TO BE PROVIDED ON COMMERCIAL BUILDING FACADES SHALL BE MOUNTED AT 15 FEET MAXIMUM.
- [D] SITE LIGHTING SHALL BE FULLY SHIELDED, AND HIGH-PRESSURE SODIUM LIGHTING AND UN-SHIELDED UPLIGHTING SHALL BE PROHIBITED AT 20 FEET MAXIMUM.

PARKING CALCULATIONS: (NET LEASABLE AREAS)

OVERALL SF 16,032 SF.

BUILDING 1- 9135 SF 45 SPACES
BUILDING 2- 2680 SF 10 SPACES
BUILDING 3- 4200 SF 35 SPACES
TOTAL REQUIRED SPACES 90 SPACES

TRANSIT REDUCTIONS:

95 SPACES REQ'D x 10% REDUCTION = 9.5 (PROXIMITY TO BUS ROUTE 94)
95 x 10% = 9.5 TOTAL SPACES REQUIRED

TOTAL PARKING SPACES REQUIRED: = 84 SPACES

TOTAL PARKING SPACES PROVIDED: = 100 SPACES

87 REGULAR SPACES
9 COMPACT SPACES
4 HANDICAP PARKING SPACES

COMPACT PARKING CALCULATION:

95 REQ'D SPACES x 33% = 31 SPACES ALLOWED
TOTAL COMPACT SPACES = 9 SPACES PROVIDED

DISABLED PARKING REQUIREMENTS:

TOTAL DISABLED SPACES REQUIRED = 4 SPACES
TOTAL DISABLED SPACES PROVIDED = 4 SPACES

MOTORCYCLE REQUIREMENTS:

TOTAL MOTORCYCLE SPACES = 2 SPACE
REQUIRED
TOTAL MOTORCYCLE SPACES = 4 SPACE
PROVIDED

COMMERCIAL BICYCLE REQUIREMENTS:

1 BICYCLE SPACE FOR EACH 20 PARKING SPACES =
95 PARKING SPACES/20 = 5 TOTAL SPACE
REQUIRED

TOTAL BICYCLE SPACES REQUIRED: = 5 SPACES
TOTAL BICYCLE SPACES PROVIDED: = 6 SPACES

RADIUS INFORMATION:

- 1 = 14'-6"
- 2 = 30'-0"
- 3 = 15'-0"
- 4 = 3'-0"
- 5 = 21'-0"
- 6 = 5'-0"

TRAFFIC CIRCULATION LAYOUT APPROVED

[Signature] 11-22-17
Signed Date

ALL WHEELCHAIR RAMPS LOCATED WITHIN THE PUBLIC RIGHT OF WAY MUST HAVE TRUNCATED DOMES.

NUMBER	DELTA ANGLE	CHORD DIRECTION	RADIUS	ARC LENGTH	CHORD LENGTH
C1	02°50'08"	S 01°39'57" E	1322.00	65.42	65.42
C2	04°55'19"	S 05°32'40" E	1322.00	113.56	113.53
C3	94°02'28"	S 55°00'53" E	60.00	98.48	87.79
C4	19°22'48"	N 68°14'53" E	120.00	40.59	40.40
C5	23°59'56"	S 12°12'40" E	300.00	125.66	124.74

DESCRIPTION

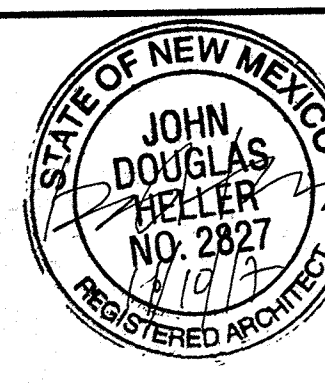
BY

DATE

REV

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mullen heller architecture



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F | 505.268.4244
www.mullenheller.com

JOB NUMBER 16-53

DRAWN BY AV

PROJECT MGR JDH

DATE 11-10-2017

PHASE CD

PROJECT
Shell Building at Los Lomas Plaza
2101 Vista Oeste Building 1
Albuquerque, NM 87120

TITLE

Traffic Control Layout

SHEET

TCL