

INTRODUCTION

THE PURPOSE OF THIS SITE DEVELOPMENT PLAN FOR BUILDING PERMIT IS TO ENSURE THAT THE SOUTHEAST CORNER OF BROADWAY AND INDIAN SCHOOL NE IS COMPREHENSIVELY DEVELOPED WITH RESPECT TO SITE LAYOUT FOR BUILDINGS, PARKING, INGRESS/EGRESS POINTS, PEDESTRIAN CIRCULATION, AND LINKAGES TO ADJACENT USES AS WELL AS ARCHITECTURAL AND NEIGHBORHOOD DESIGN.

THE PROPOSED USE OF THE SITE IS TO DEVELOP 68 UNITS OF MULTI-FAMILY HOUSING AND ASSOCIATED COMMUNITY SUPPORT FUNCTIONS, INCLUDING A LEASING OFFICE, COMMUNITY SUPPORT BUILDING, AND PARK FACILITY ACCESSIBLE TO THE PUBLIC.

BUILDING MASSING IS ARRANGED RECTILINEARLY TO MELD THE DEVELOPMENT WITH THE EXISTING FABRIC OF SURROUNDING CITY BLOCKS. BUILDINGS ARE PLACED WITH THEIR FRONT FACADES FACING STREETS AND ADJACENT TO SIDEWALKS, WITH FRONT SETBACKS VARYING FROM A MAXIMUM OF 22' FROM THE PROPERTY LINE ALONG BROADWAY TO A MINIMUM ZERO SETBACK AT THE COMMUNITY BUILDINGS ALONG INDIAN SCHOOL. HEIGHTS RANGE FROM ONE TO THREE STORIES, WITH A MAXIMUM HEIGHT OF 48' AT THE THREE STORY UNITS. THE MAXIMUM FLOOR AREA RATIO FOR THE DEVELOPMENT IS .3.

OFF-STREET PARKING IS LOCATED AT THE CENTER OF EACH RESIDENTIAL BLOCK, WITH CONTROLLED VEHICULAR INGRESS AND EGRESS TO THE OFF-STREET PARKING AREAS FROM INDIAN SCHOOL NE. A PROPOSED STREET BISECTS THE CENTER OF THE SITE IN A NORTH-SOUTH DIRECTION, AND IS MASTER PLANNED TO CONNECT TO A POSSIBLE EASTWEST STREET TO THE SOUTH OF THIS DEVELOPMENT IN A FUTURE PHASE. NEW ON-STREET PARKING IS PROPOSED TO BE CONSTRUCTED AS A PART OF THIS DEVELOPMENT, 100% OF WHICH SERVES THE REQUIREMENTS OF THE DEVELOPMENT.

PEDESTRIAN INGRESS AND EGRESS IS THROUGH AN EXTENSION OF THE EXISTING NEIGHBORHOOD'S SIDEWALK SYSTEM ALONG BROADWAY AND INDIAN SCHOOL. CONTROLLED GATED PEDESTRIAN ACCESS POINTS CONNECT THE PERIMETER SIDEWALK SYSTEM TO THE INTERNAL CIRCULATION SYSTEM OF SIDEWALKS WITHIN THE PERIMETER SECURITY WALLS.

LANDSCAPING EMPLOYS A LARGE NUMBER OF TREES FOR SHADING AND TO TIE THE PROPOSED PARK AREA VISUALLY TO THE PROPOSED OFF-STREET PARKING AREAS. PERMEABLE PAVING IS USED IN THE OFF-STREET PARKING AREAS TO ENCOURAGE THEIR USE AS MULTI-FUNCTIONING SPACES.

LEGEND:

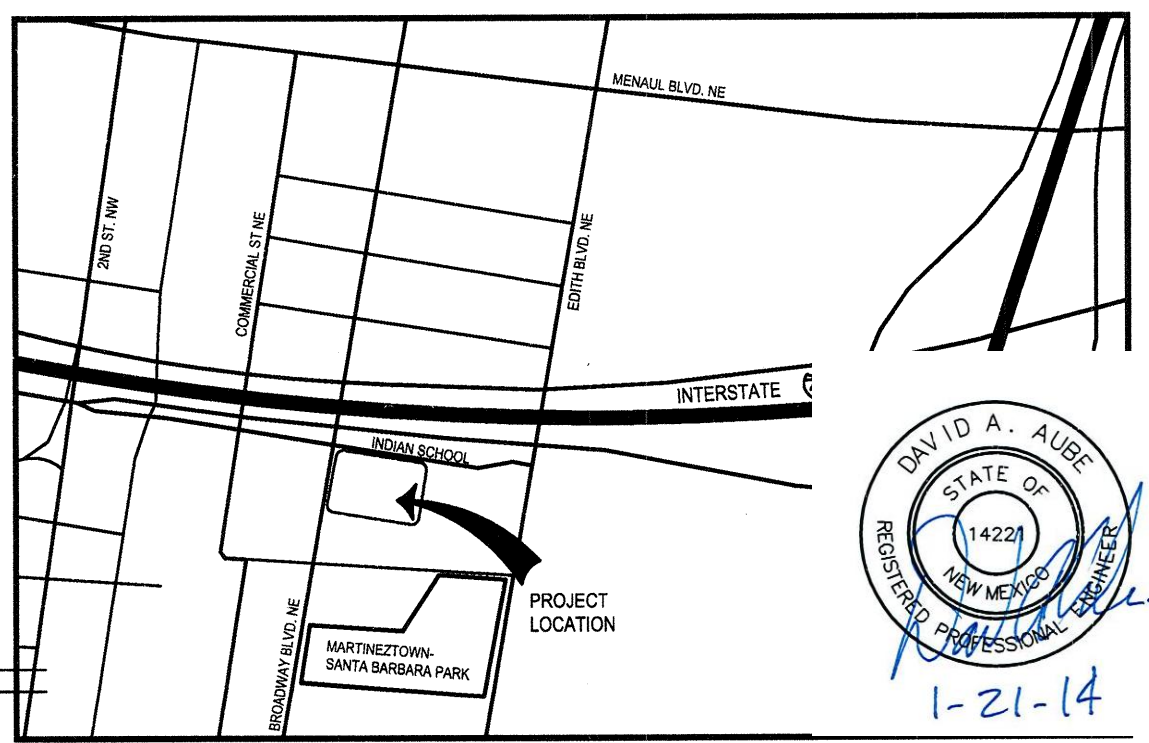
- PEDESTRIAN ACCESS (CONCRETE SIDEWALK)
- PERMEABLE PAVEMENT (6" THICK 3/4" GRAVEL OVER 95% COMPACTED NATIVE SUBGRADE)
- PATTERNED CONCRETE CROSSWALK
- 5'-4" HIGH SECURITY WALL - GRAY-BROWN CONCRETE BLOCK, 8X16 STACKED BOND, WITH DECORATIVE METAL GATES WHERE SHOWN.
- 6'-0" HIGH SECURITY WALL - DECORATIVE METAL WITH DECORATIVE METAL GATES WHERE SHOWN.
- LOW RETAINING WALL - GRAY-BROWN CONCRETE BLOCK, 8X16 STACKED BOND, 2'-0" APPROXIMATE HEIGHT.

- 6'-0" HIGH SECURITY WALL - DECORATIVE METAL "COYOTE FENCE"
- SITE LIGHTING, METAL HALIDE, 15' ALUM. POLE
- SITE LIGHTING, METAL HALIDE, 15' ALUM. POLE, DOUBLE LAMP
- SITE LIGHTING, METAL HALIDE, 12' ALUM. POLE
- PICNIC TABLE
- TRASH CAN
- BENCH
- BOULDERS
- BIKE RACKS

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NOTE: PARK AREA IS PRIVATELY OWNED AND PRIVATELY MAINTAINED BY OWNER.



VICINITY MAP

SCALE: 1" = 100'

SDP for BP Partial Certification

I, David A. Aube, NMPE 14221, of the firm The Hartman + Majewski Design Group, Inc., hereby certify that this project has been constructed in accordance with the design intent of the SDP for BP previously approved and dated 1-31-12.

The record information that has been edited onto the original design documents was obtained by Pavilion Construction. I further certify that I have personally visited the project site on January 14, 2014 and have determined by visual inspection that the actual site conditions shown on this plan to be true and correct to the best of my knowledge and belief. This certification is submitted in support of a request for Temporary Certificate of Occupancy for Buildings A and B. A Permanent CO has already been issued for Buildings E, F and G. Buildings C, and D are still under construction and should be completed within 2 months. The parking area on the inside of the site behind Buildings A, and B will provide parking for Buildings C and D that are still under construction.

The record information presented herein is not necessarily complete and intended only to verify substantial compliance of the drainage aspects of this project. Those relying on the record documents are advised to obtain independent verification of its accuracy before using it for any other purpose.

ADMINISTRATIVE AMENDMENT
FILE #1444/1000 PROJECT #102649
Changes to sidewalk along Indian School, internal crosswalks, various facade changes.
APPROVED BY DATE

PROJECT LOCATION: SOUTHEAST CORNER OF INDIAN SCHOOL AND BROADWAY NE

LEGAL DESCRIPTION: LOTS 1 THROUGH 3, BLOCK 9, FRANCISCAN ACRES

ZONE ATLAS PAGE: H-14

TOTAL ACREAGE: 2.9945 ACRES

PROPOSED SUBDIVISION PARCELS: PARCEL A 2.9945 ACRES

EXISTING ZONING: SU-2/SU-1 FOR PRD

PROPOSED USES: RESIDENTIAL DWELLINGS

LIST OF APPLICABLE PLANS:
RANK 1 PLANS - ALBUQUERQUE/BERNALILLO COUNTY COMPREHENSIVE PLAN
RANK 2 PLANS - NONE
RANK 3 PLANS - MARTINEZTOWN / SANTA BARBARA SECTOR DEVELOPMENT PLAN

MAXIMUM BUILDING HEIGHT: 48 FEET

MINIMUM BUILDING SETBACK: ZERO (INDIAN SCHOOL ONLY)

MAXIMUM TOTAL DWELLING UNITS:

68 DWELLING UNITS
NET: 28.84 DU/SI ACRE
GROSS: 22.87 DU/SI ACRE

PROPOSED TOTAL AREA FOR RESIDENTIAL UNITS: 56,481 SF (LEASING OFFICE AND COMMUNITY BUILDING NOT INCL.)

FAR SUMMARY:
MAXIMUM FAR FOR NONRESIDENTIAL UNITS.....0.3
PROVIDED FAR FOR NONRESIDENTIAL UNITS.....0.05
PROVIDED FAR FOR ALL DEVELOPMENT (RES AND NON-RES).....0.53

REQUIRED PARKING:

PER DRB APPROVED SDP FOR SUBDIVISION 8-8-12:
TOTAL REQUIRED PARKING.....115 SPACES

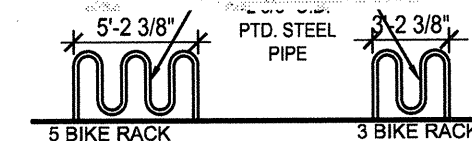
PROPOSED PARKING:

OFF-STREET PARKING.....87 SPACES
ON-STREET PARKING.....28 SPACES
TOTAL PROPOSED PARKING.....115 SPACES

HANDICAPPED, MOTORCYCLE, AND BICYCLE PARKING IS PROVIDED TO MEET THE REQUIREMENTS OF THE APPROVED SITE DEVELOPMENT PLAN FOR SUBDIVISION DATED 8-8-2012.

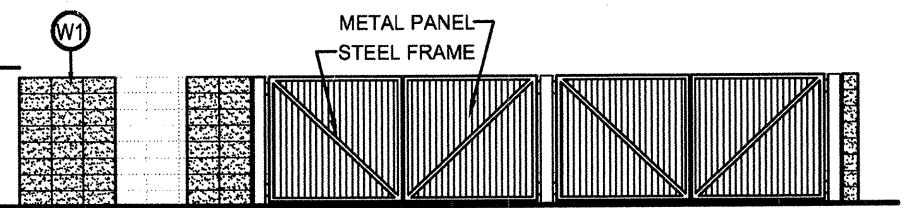
SIGNATURE BLOCK

PROJECT NUMBER 1007649
APPLICATION NUMBER 12 DRB 70255
Is an infrastructure list required? (If yes, then a set of approved DRG plans with a work order is required for any construction within public right-of-way or for construction of public improvements.)
TRAFFIC ENGINEERING, TRANSPORTATION DIVISION DATE: 09-12-12
UTILITIES DEVELOPMENT DATE: 09/12/12
PARKS AND RECREATION DATE: 9/12/12
CITY ENGINEER DATE: 1-31-13
ENVIRONMENTAL HEALTH DEPT. (if required) DATE:
SOLID WASTE MANAGEMENT DATE: 1-31-12
DRB CHAIRPERSON, PLANNING DEPARTMENT DATE:



BIKE RACKS

SCALE: 1/8" = 1'-0"



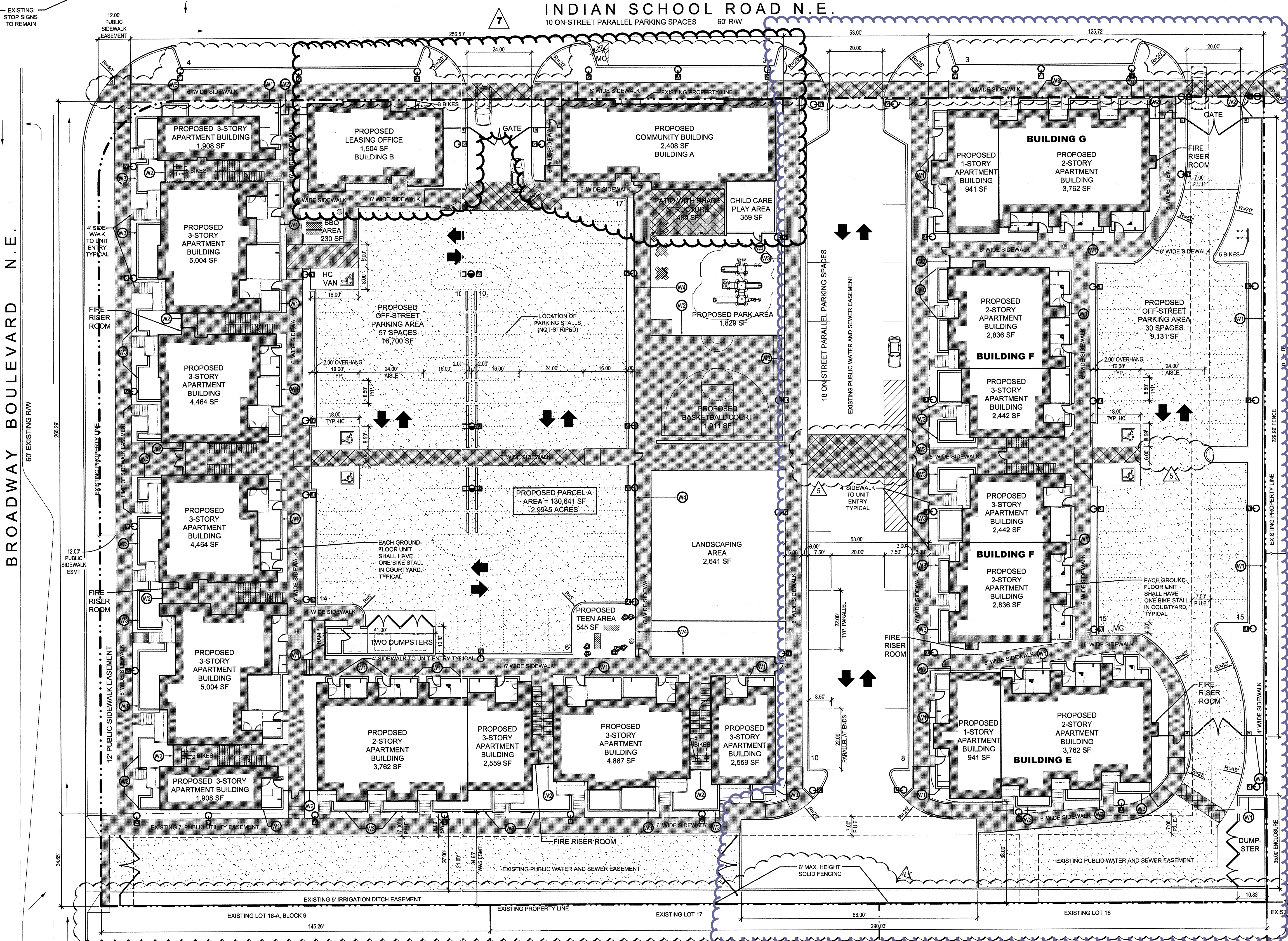
DUMPSTER ENCLOSURE

SCALE: 1/8" = 1'-0"

BROADWAY BOULEVARD N.E.

INDIAN SCHOOL ROAD N.E.

10 ON-STREET PARALLEL PARKING SPACES 60' R/W



SITE DEVELOPMENT PLAN FOR BUILDING PERMIT

SCALE: 1" = 20'-0"

NOTE: PLAT COMBINING THREE LOTS INTO SINGLE LOT AND CREATING EASEMENTS WAS RECORDED ON 8-30-2012 IN PLAT BOOK 2012C, PAGE 101

PROJECT NAME

GREATER ALBUQUERQUE
HOUSING PARTNERSHIP

FRANCISCAN ACRES SUBDIVISION
INDIAN SCHOOL AND BROADWAY
ALBUQUERQUE, NM 87102

REVISIONS:

No.	DATE	DESCRIPTION
1	7/17/2012	PLAT REVISIONS
2	7/24/2012	DRB COMMENTS
3	8/5/2012	DRB COMMENTS
4	9/6/2012	DRB COMMENTS
5	1/8/2013	ADMIN. AMENDMENT
4.5	3/19/13	ADMIN. AMENDMENT
6	1/15/14	PARTIAL CERT.
7	1/21/14	PARTIAL CERT.

Copyright: Design Group

Drawn by	EH
Checked by	MW
Date	8/10/2012
Project number	2439
Cad file name	

SHEET TITLE:

SITE DEVELOPMENT PLAN FOR BUILDING PERMIT

SHEET NUMBER:

1.1