## **CITY OF ALBUQUERQUE**



April 14, 2017

Jeff Wooten P.E. Wooten Engineering 1005 21<sup>St</sup> Street Se suite A5 Albuquerque, NM 87124

## Re: T-Mobile Retail Building – 2706 Carlisle Blvd Request for Certificate of Occupancy- Transportation Development Engineer's Stamp dated 10-26-17 (H16D148) Certification dated 04-13-17

Dear Mr. Wotten,

Based upon the information provided in your submittal received 04-13-17, Transportation Development has no objection to the issuance of a <u>Permanent</u> <u>Certificate of Occupancy</u>. This letter serves as a "green tag" from Transportation Development for a <u>Permanent Certificate of Occupancy</u> to be issued by the Building and Safety Division.

PO Box 1293

If you have any questions, please contact me at (505) 924-3981.

Albuquerque

Sincerely.

New Mexico 87103

Plan Checker, Transportation & Hydrology Development Review Services

www.cabq.gov

| mao | via: emai | 1      |
|-----|-----------|--------|
| C:  | CO Clerk  | , File |

Monica Ortiz

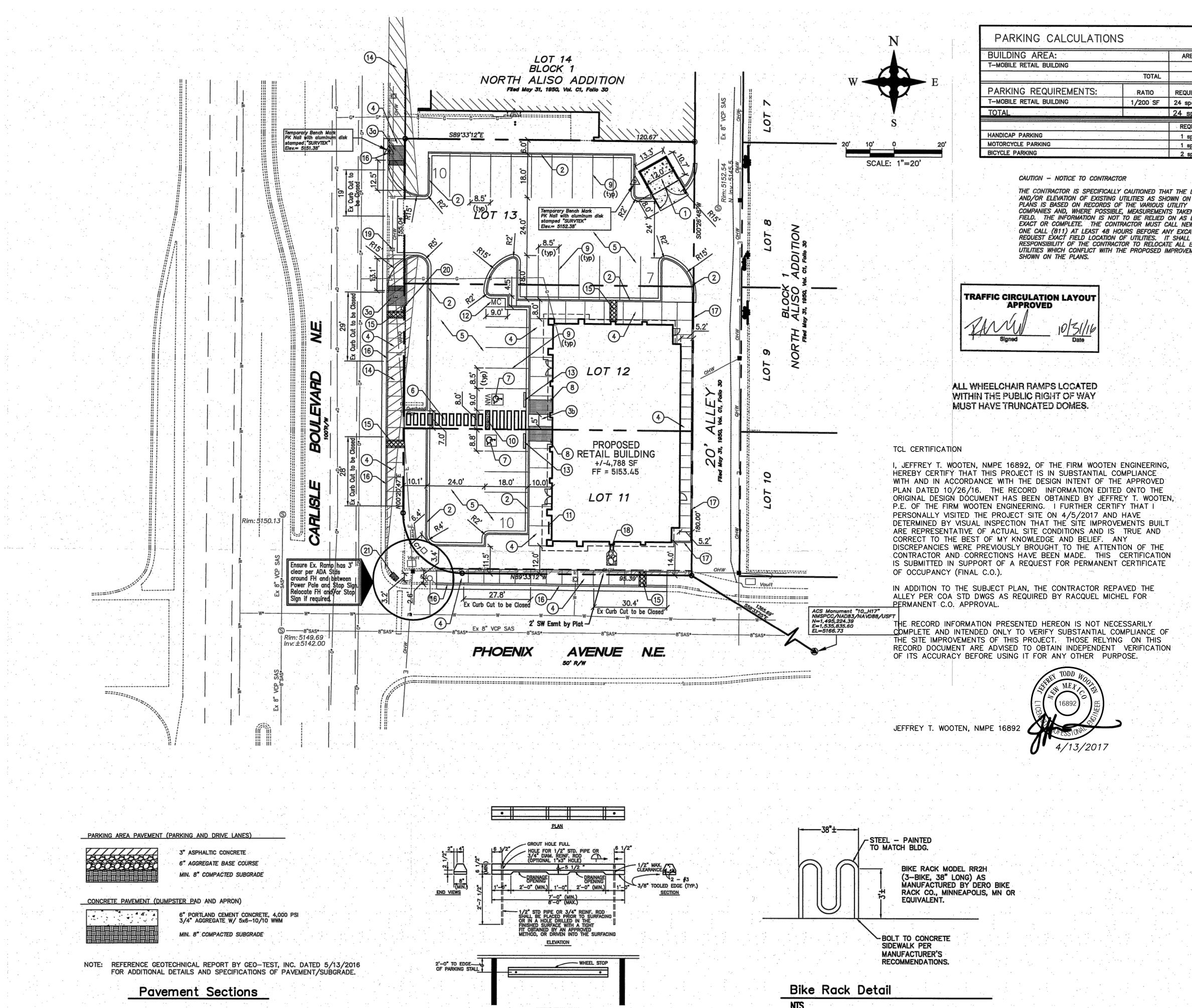


## City of Albuquerque

Planning Department Development & Building Services Division DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 09/2015)

| Project Title:  | Building Permit #: | City Drainage #:  |  |  |  |
|---|--------------------|---|--|--|--|
| DRB#: EPC#:   |                    | Work Order#:  |  |  |  |
| Legal Description:  |                    |   |  |  |  |
| City Address:   |                    |   |  |  |  |
| Engineering Firm:   |                    | Contact:  |  |  |  |
| Address:  |                    |   |  |  |  |
| Phone#: Fax#:   |                    | E-mail:   |  |  |  |
| Owner:  |                    | Contact:  |  |  |  |
| Address:  |                    |   |  |  |  |
| Phone#: Fax#:   |                    | _ E-mail:   |  |  |  |
| Architect:  |                    | Contact:  |  |  |  |
| Address:  |                    |   |  |  |  |
| Phone#: Fax#:   |                    | E-mail:   |  |  |  |
| Other Contact:  |                    | Contact:  |  |  |  |
| Address:  |                    |   |  |  |  |
| Phone#: Fax#:   |                    | E-mail:   |  |  |  |
| TRAFFIC/ TRANSPORTATION<br>MS4/ EROSION & SEDIMENT CONTROL  |                    | BUILDING PERMIT APPROVAL CERTIFICATE OF OCCUPANCY                   |  |  |  |
| TYPE OF SUBMITTAL:  |                    |   |  |  |  |
| ENGINEER/ ARCHITECT CERTIFICATION                           |                    | RY PLAT APPROVAL  |  |  |  |
|   |                    | SITE PLAN FOR SUB'D APPROVAL<br>SITE PLAN FOR BLDG. PERMIT APPROVAL |  |  |  |
| CONCEPTUAL G & D PLAN                                       | FINAL PLAT         |   |  |  |  |
| GRADING PLAN  |                    |   |  |  |  |
| DRAINAGE MASTER PLAN  | FOUNDATIO          | FOUNDATION PERMIT APPROVAL  |  |  |  |
| DRAINAGE REPORT   | GRADING P          | GRADING PERMIT APPROVAL   |  |  |  |
| CLOMR/LOMR  | SO-19 APPR         | SO-19 APPROVAL  |  |  |  |
| TRAFFIC CIRCUITATION LAVOUT (TOL)                           |                    | PAVING PERMIT APPROVAL  |  |  |  |
| TRAFFIC CIRCULATION LAYOUT (TCL) TRAFFIC IMPACT STUDY (TIS) |                    | GRADING/ PAD CERTIFICATION  |  |  |  |
| EROSION & SEDIMENT CONTROL PLAN (ESC)                       |                    | WORK ORDER APPROVAL   |  |  |  |
|   | CLOMR/LON          | /IK   |  |  |  |
| OTHER (SPECIFY)   | PRE-DESIGN         | MEETING   |  |  |  |
|   | OTHER (SPE         | ECIFY)  |  |  |  |
| IS THIS A RESUBMITTAL?: Yes No                              |                    |   |  |  |  |
|   |                    |   |  |  |  |
| DATE SUBMITTED:By: _  |                    |   |  |  |  |

COA STAFF: ELECTRONIC SUBMITTAL RECEIVED: \_\_\_\_



Precast Concrete Wheel Stop Detail

INSTALLATION PLAN

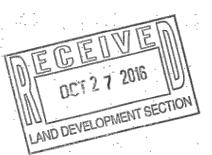
| AREA (SQUARE FEET)<br>4,788 SF<br>4,788 SF   | 0         1A         0         7         Cl3         10         8         10         10           0         1         0         7         1         10         A         10         A         10         A         10  |
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| REQUIREDPROVIDED24 spaces27 spaces24 spaces27 spaces   |  |
| REQUIREDPROVIDED1 spaces2 spaces1 spaces1 spaces2 spaces3 spaces   | UNIT 2)<br>UNIT 2)<br>UNI   |
| AT THE LOCATION<br>DWN ON THESE<br>UTILITY<br>TS TAKEN IN THE<br>ON AS BEING<br>ALL NEW MEXICO<br>NY EXCAVATION TO<br>T SHALL BE THE | MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL<br>MENAUL |
| TE ALL EXISTING<br>MPROVEMENTS   | VICINITY MAP - Zone Atlas Page H-16<br>NTS<br>Legal Desc: Lots 11-13, Blk 1, N. Aliso Add'n, 0.4949 Acres  |

## KEYED NOTES

- 1. DUMPSTER ENCLOSURE. REF ARCHTIECTURAL DETAILS. SEE PAVING DETAILS THIS SHEET FOR CONCRETE APRON.
- 2. INSTALL MEDIAN CURB/GUTTER (6" HIGH) PER COA STD DWG 2415B.
- 3a. INSTALL PARALLEL CURB RAMP PER NMDOT STD DWGS 608-001, SHEETS 1-12 WITH 'CAST-IN-PLACE REPLACEABLE' DETECTABLE WARNING SURFACE (DWS). MATCH ADJACENT SIDEWALK WIDTH.
- 3b. INSTALL PARALLEL CURB RAMP PER NMDOT STD DWGS 608-001, SHEETS 1-12, DETECTABLE WARNING SURFACE NOT REQUIRED ON THIS RAMP.
- 4. REMOVE EXISTING CURB, GUTTER, SIDEWALKS, AND DRIVEPADS AS REQUIRED AND THEN INSTALL CONCRETE SIDEWALK PER COA STD DWG 2430.
- 5. ASPHALT PAVEMENT, SEE PAVING DETAILS THIS SHEET.
- 6. INSTALL BOX STRIPING PER ARCHITECTURAL SITE PLAN & DETAILS. TWO COATS MINIMUM.
- 7. PAINTED WHEELCHAIR SYMBOL (BLUE) TO MEET ADA STANDARDS. TWO COATS MINIMUM.
- 8. INSTALL H/C SIGNAGE PER DETAIL ON DETAIL SHEET. ADD VAN ACCESSIBLE SIGNAGE WHERE NOTED. WHEN A POLE MOUNTED SIGN WILL NOT FIT WITHIN DESIGNATED AREA, SIGN SHALL BE BOLTED TO BUILDING. H/C SIGN MUST INCLUDE LANGUAGE PER 66-7-352.4C NMSA 1978 "VIOLATORS ARE SUBJECT TO A FINE AND/OR TOWING."
- 9. INSTALL 4" WIDE PAINT STRIPES (WHITE) FOR ALL PARKING MINIMUM TWO COATS.
- 10. PAINT WORDS 'NO PARKING' WITH LETTERS AT LEAST ONE FOOT HIGH AND 2
- 11. INSTALL BIKE RACK WITH THREE (3) SPACES PER DETAIL THIS SHEET.
- 12. MOTORCYCLE PARKING. ADD PAINT LABELS AND SIGNAGE PER COA STDS. TWO COATS MINIMUM.

INCHES WIDE. TWO COATS MINIMUM.

- 13. INSTALL PRE-CAST WHEEL STOP PER DETAIL THIS SHEET.
- 14. 13'x130' CLEAR SIGHT TRIANGLE. LANDSCAPING & SIGNAGE SHALL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES, AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THIS AREA.
- 15. INSTALL SIDEWALK CULVERT PER COA STD DWG 2236. EXTEND ALL PERIMETER SIDEWALK CULVERTS ALONG PUBLIC ROADWAYS A MINIMUM OF 1' INSIDE THE PROPERTY LINE. REF. GRADING PLAN. CUT OPENING AS REQUIRED FOR DOWNSPOUT AT NORTH END OF BUILDING.
- 16. REMOVE EXISTING CURB, GUTTER, SIDEWALKS, AND DRIVEPADS AS REQUIRED AND THEN INSTALL STANDARD CURB/GUTTER (8" HIGH) PER COA STD DWG 2415A.
- 17. INSTALL HEADER CURB PER COA STD DWG 2415B. REF. GRADING PLAN.
- 18. INSTALL PRECAST CONCRETE SPLASH BLOCK WITH 3"+ COBBLE TO BOTTOM OF POND.
- 19. COORDINATE LIGHT POLE REMOVAL/RELOCATION WITH PNM. ALL COSTS ASSOCIATED WITH THE LIGHT POLE RELOCATION ARE THE RESPONSIBILITY OF THE DEVELOPER.
- 20. PREFERRED NEW LIGHT POLE LOCATION PER KEYED NOTE 19, ABOVE.
- 21. INSTALL 'CAST-IN-PLACE REPLACEABLE DETECTABLE WARNING SURFACE (DWS) ONTO EXISTING RAMP PER NMDOT STD DWGS 608-001, SHEET 8.





Wooten Engineering

1005 21st St SE, Suite A5 Rio Rancho, N.M. 87124 Phone: (505) 980-3560

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SCALE:

9/2/2016

AS NOTED

Site

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