

# CITY OF ALBUQUERQUE

Planning Department  
Alan Varela, Director



Mayor Timothy M. Keller

October 6, 2022

Lauren Nuffer, P.E.  
Kimley-Horn  
13455 Noel Road Two Galleria Office Tower Suite 700  
Dallas, TX 75024

**RE: Raising Cain's C0705**  
**2004 Wyoming Blvd NE**  
**Grading and Drainage Plan**  
**Engineer's Stamp Date: 09/30/2022**  
**Hydrology File: H20D003A**

Dear Ms. Nuffer:

Based upon the information provided in your submittal received 8/9/2022 and minor comments addressed, the Grading and Drainage Plans are approved for Grading Permit. Note, the interim grading plan is effecting only to include the temporary sediment ponds shown on the ESC sheets. A new Grading and Drainage plan will need to be submitted for future building permit approval to show future paving and storm water quality improvements necessary per DPM chapter 6.

Please attach a copy of this approved plan in the construction sets for Building Permit processing along with a copy of this letter. Prior to approval in support of Permanent Release of Occupancy by Hydrology, Engineer Certification per the DPM checklist will be required.

As a reminder, if the project total area of disturbance (including the staging area and any work within the adjacent Right-of-Way) is 1 acre or more, then an Erosion and Sediment Control (ESC) Plan and Owner's certified Notice of Intent (NOI) is required to be submitted to the Stormwater Quality Engineer (Doug Hughes, PE, [jhughes@cabq.gov](mailto:jhughes@cabq.gov), 924-3420) 14 days prior to any earth disturbance.

Please provide the Drainage Covenant with Exhibit A for the stormwater quality ponds per Article 6-15(C) of the DPM prior to Permanent Release of Occupancy. Please submit the original copies along with the \$ 25.00 recording fee check made payable to Bernalillo County to Carrie Compton ([cacompton@cabq.gov](mailto:cacompton@cabq.gov)) on the 4th floor of Plaza de Sol. Please note that Hydrology will need a pdf copy of the recorded Drainage Covenant prior to Hydrology's approval of Permanent Release of Occupancy.

If you have any questions, please contact me at 924-3695 or [dggutierrez@cabq.gov](mailto:dggutierrez@cabq.gov).

Sincerely,

David G. Gutierrez, P.E.  
Senior Engineer, Hydrology  
Planning Department

# CIVIL CONSTRUCTION PLANS FOR

## INTERIM CONDITIONS

## DEMOLITION AND STABILIZATION

### 2004 WYOMING BLVD NE

### CITY OF ALBUQUERQUE

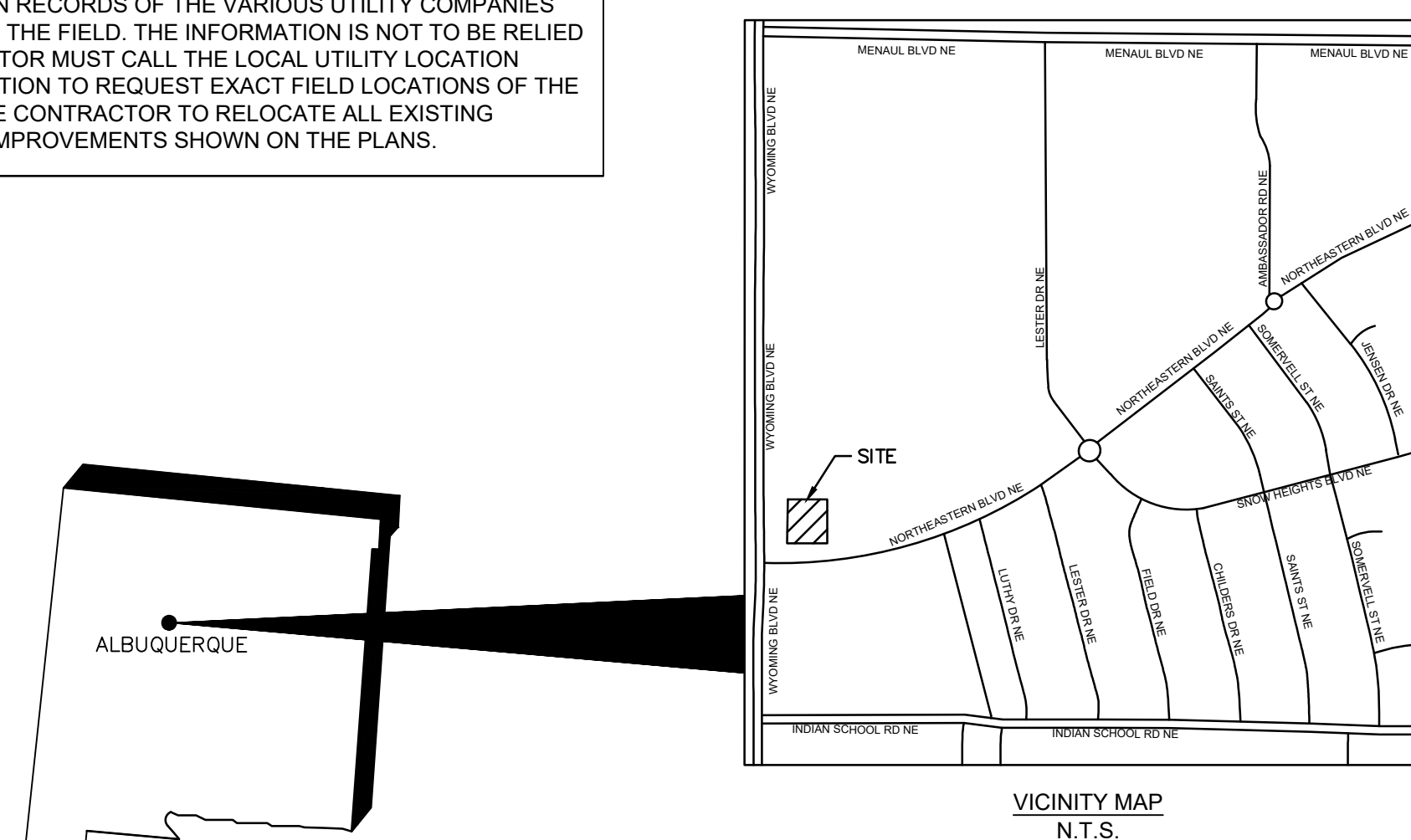
### BERNALILLO COUNTY, NEW MEXICO 87112

#### GOVERNMENT/UTILITY CONTACTS

FIRE DEPARTMENT	ALBUQUERQUE FIRE RESCUE STATION 13 4901 PROSPECT AVE. NE ALBUQUERQUE, NM 87110 (505) 888-8178
UTILITIES DEPARTMENT	ALBUQUERQUE BERNALILLO COUNTY WATER UTILITY AUTHORITY 1441 MISSION AVE. NE ALBUQUERQUE, NM 87113 (505) 842-9287
STORM DEPARTMENT	CITY OF ALBUQUERQUE 2400 BROADWAY BLVD. SE ALBUQUERQUE, NM 87102 (505) 768-3830
INSPECTIONS	CITY OF ALBUQUERQUE 2400 BROADWAY BLVD. SE ALBUQUERQUE, NM 87102 (505) 768-3830
ENVIRONMENTAL AGENCY	CITY OF ALBUQUERQUE 2400 BROADWAY BLVD. SE ALBUQUERQUE, NM 87102 (505) 768-3830
ELECTRIC COMPANY	PUBLIC SERVICE COMPANY OF NEW MEXICO 414 SILVER AVE. SW ALBUQUERQUE, NM 87158 (888) 245-3659
GAS COMPANY	NEW MEXICO GAS COMPANY 4625 EDITH BLVD. NE ALBUQUERQUE, NM 87107 (505) 697-3335
TELEPHONE COMPANY	CLOUD BASED PHONE SYSTEM ALBUQUERQUE 4300 SAN MATEO BLVD. NE #983 ALBUQUERQUE, NM 87110 (505) 412-4800
INTERNET COMPANY	LOBO INTERNET SERVICES, LTD. 2419 SAN PEDRO DR. NE ALBUQUERQUE, NM 87110 (505) 830-1012

#### CAUTION: NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.



#### SITE LOCATION MAP

(NOT TO SCALE)

NOTES:

- IF REPRODUCED, THE SCALES SHOWN ON THESE PLANS ARE BASED ON A 24" X 36" SHEET.

#### INDEX OF SHEETS

SHEET NO.	DESCRIPTION
C1.0	COVER SHEET
C1.1	ALTA SURVEY
C1.2	ALTA SURVEY
C2.0	GENERAL NOTES
C2.1	GENERAL AND CITY NOTES
C3.0	DEMOLITION PLAN
C4.0	EROSION CONTROL PLAN
C4.1	EROSION CONTROL DETAILS
C5.0	GRADING PLAN



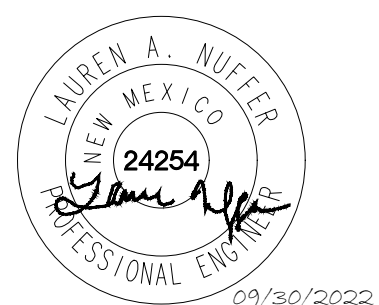
INTERIM CONDITIONS  
DEMOLITION AND STABILIZATION  
2004 WYOMING BLVD NE  
BERNALILLO COUNTY, NEW MEXICO 87112

Engineer's Information:

**Kimley»Horn**

© 2022 KIMLEY-HORN  
AND ASSOCIATES, INC.  
13455 Noel Road  
Two Galleria Office Tower  
Suite 700  
Dallas, TX 75240  
CONTACT: LAUREN NUFFER, P.E.  
(972) 770-1300  
LAUREN.NUFFER@KIMLEY-HORN.COM  
LIZ WILLMOT, P.E.  
LIZ.WILLMOT@KIMLEY-HORN.COM

Professional of Record:



#### HYDROLOGY SUBMITTAL

REVISIONS:

NO.	DESCRIPTION	DATE
1		
2		
3		
4		
5		
6		
7		
8		
9		

Sheet Title:

**COVER SHEET**

Date: 08.09.2022

Project Number: 069313444

Drawn By: LW/NW

Sheet Number:

**C1.0**

**ENGINEER**  
**Kimley»Horn**

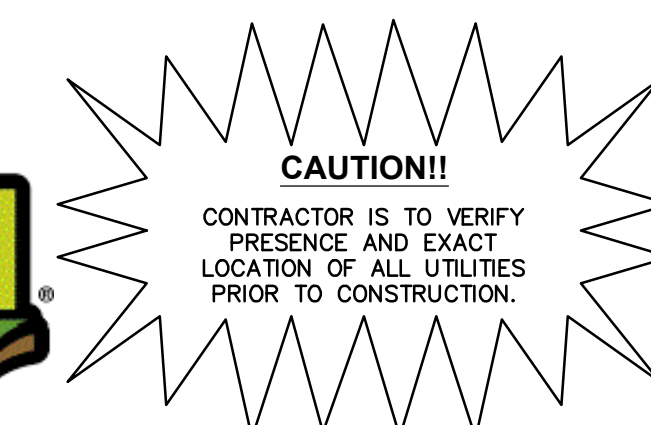
13455 Noel Road  
Two Galleria Office Tower  
Suite 700  
Dallas, Texas 75240  
PH. (972)-770-1300  
CONTACT: LAUREN NUFFER, P.E.  
LAUREN.NUFFER@KIMLEY-HORN.COM  
LIZ WILLMOT, P.E.  
LIZ.WILLMOT@KIMLEY-HORN.COM

**OWNER/DEVELOPER**



RAISING CANES RESTAURANT, LLC  
6800 BISHOP ROAD  
PLANO, TX 75024  
(P) 469 644-3198  
CONTACT: PATRICIA HILL  
PATRICIA.HILL@ME.COM

**AUGUST 2022**







## Zoning Data

NUMBER OF STRIPED PARKING SPACES:

REGULAR = 83  
HANDICAPPED= 2  
TOTAL= 85

## Notes Corresponding to Schedule B-II

TITLE COMMITMENT ISSUED BY OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY, COMMITMENT NUMBER 2010149, DATED NOVEMBER 16, 2020

- SCH. B-II  
ITEM NO.
- DESCRIPTION
- [9] RESERVATIONS, EXCEPTIONS AND PROVISIONS CONTAINED IN THE PATENT FROM THE UNITED STATES OF AMERICA TO JAMES DENNIS, FILED JANUARY 28, 1927, RECORDED IN BOOK 97, PAGE 253, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [10] COVENANTS, CONDITIONS AND RESTRICTIONS SET FORTH IN THE DOCUMENT ENTITLED DECLARATION OF EASEMENTS, COVENANTS AND RESTRICTIONS FILED OCTOBER 25, 1985, IN BOOK MISC. 284A, PAGE 494, AS DOCUMENT NO. 85-89923; PARTIAL RELEASE AND TERMINATION OF DECLARATION OF EASEMENTS, COVENANTS AND RESTRICTIONS, FILED JULY 9, 1996, IN BOOK 96-18, PAGE 9413, AS DOCUMENT NO. 96076225, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [11] DECLARATION OF COVENANTS, RESTRICTIONS AND EASEMENTS, WYOMING MALL, DATED MARCH 31, 2009, BY AND BETWEEN WEINGARTEN/INVESTMENTS, INC., A TEXAS CORPORATION, AND COLE WM ALBUQUERQUE NM, LLC, A DELAWARE LIMITED LIABILITY COMPANY, RECORDED APRIL 1, 2009, AS DOCUMENT NO. 2009034905, RECORDS OF BERNALILLO COUNTY, NEW MEXICO, AS AMENDED BY FIRST AMENDMENT TO DECLARATION OF COVENANTS, RESTRICTIONS AND EASEMENTS, WYOMING MALL, DATED MAY 20, 2009, BY AND BETWEEN WEINGARTEN/INVESTMENTS, INC., A TEXAS CORPORATION, AND COLE WM ALBUQUERQUE NM, LLC, A DELAWARE LIMITED LIABILITY COMPANY, RECORDED JULY 10, 2009, AS DOCUMENT NO. 2009077417, RATIFICATION, CONFIRMATION AND ACKNOWLEDGMENT DECLARATION OF COVENANTS, RESTRICTIONS AND EASEMENTS WYOMING MALL, FILED AUGUST 22, 2013 AS DOCUMENT NO. 2013094695, RECORDS OF BERNALILLO COUNTY, NEW MEXICO; AND THAT CERTAIN RATIFICATION, CONFIRMATION AND ACKNOWLEDGMENT - DECLARATION OF COVENANTS, RESTRICTIONS AND EASEMENTS - WYOMING MALL, EXECUTED BY WEINGARTEN REALTY INVESTORS, A TEXAS REAL ESTATE INVESTMENT TRUST, AND WEINGARTEN/INVESTMENTS, INC., A TEXAS CORPORATION, WHICH INSTRUMENT IS BEING RECORDED IN THE RECORDS OF BERNALILLO COUNTY, NEW MEXICO, CONCURRENTLY HERewith.
- [12] EASEMENTS WITH COVENANTS AND RESTRICTIONS AFFECTING LAND ("EOR") DATED FEBRUARY 18, 2007, BETWEEN WAL-MART STORES EAST, LIMITED PARTNERSHIP AND WEINGARTEN REALTY INVESTORS, A TEXAS REAL ESTATE INVESTMENT TRUST, FILED MARCH 2, 2007, IN BOOK A133, PAGE 4237, AS DOCUMENT NO. 2007034300, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [13] EASEMENTS, COVENANTS AND OTHER MATTERS SHOWN ON THE RECORDED MAP OF SAID SUBDIVISION FILED JULY 12, 1978, IN PLAT BOOK D8, PAGE 168, AS AMENDED OCTOBER 18, 1978, IN PLAT BOOK D9, PAGE 11, RECORDS OF BERNALILLO COUNTY, NEW MEXICO; AND ON THE RECORDED MAP OF SAID SUBDIVISION FILED APRIL 2, 2007, IN PLAT BOOK 2007C, PAGE 80, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [14] RESERVATION OF EASEMENTS FOR PUBLIC UTILITIES, WHETHER MUNICIPALLY OR PRIVATELY OWNED, WITHIN THE VACATED PORTIONS OF NORTHEASTERN BLVD. NE AND WYOMING BLVD. NE AS DISCLOSED IN VACATING ORDINANCES NO. 2558 AND NO. 1197 RECORDED AUGUST 24, 1964, IN BOOK MISC. 48, PAGE 260, AND RECORDED FEBRUARY 18, 1957, IN BOOK 0377, PAGE 289, AS DOCUMENT NO. 21104, AND AS SHOWN ON THE RECORDED PLAT, RECORDED IN BOOK D9, PAGE 11, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [15] PERMANENT EASEMENT FOR THE CONSTRUCTION, INSTALLATION, MAINTENANCE, REPAIR, MODIFICATION, REPLACEMENT AND OPERATION OF A PUBLIC SIDEWALK, PARCEL 5-A, WYOMING MALL, BETWEEN WEINGARTEN REALTY INVESTORS AND THE CITY OF ALBUQUERQUE, A NEW MEXICO MUNICIPAL CORPORATION, FILED JULY 6, 2007, AS DOCUMENT NO. 2007097669, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [16] PUBLIC SERVICE COMPANY OF NEW MEXICO EASEMENT (ELECTRIC) DATED JULY 16, 2007, BY AND BETWEEN WEINGARTEN REALTY INVESTORS, A TEXAS REAL ESTATE INVESTMENT TRUST, AND PUBLIC SERVICE COMPANY OF NEW MEXICO, A NEW MEXICO CORPORATION, FILED JULY 27, 2007, AS DOCUMENT NO. 2007109309, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [17] PERMANENT EASEMENT PUBLIC SERVICE COMPANY OF NEW MEXICO UNDERGROUND EASEMENT DATED NOVEMBER 9, 2007, BY AND BETWEEN WEINGARTEN REALTY INVESTORS, A TEXAS REAL ESTATE INVESTMENT TRUST AND PUBLIC SERVICE COMPANY OF NEW MEXICO FOR THE CONSTRUCTION, INSTALLATION, MAINTENANCE, REPAIR, MODIFICATION, REPLACEMENT AND OPERATION OF A PUBLIC ROADWAY, PARCEL 4-A, WYOMING MALL, BETWEEN WEINGARTEN REALTY INVESTORS AND THE CITY OF ALBUQUERQUE, A NEW MEXICO MUNICIPAL CORPORATION, FILED FEBRUARY 28, 2008, AS DOCUMENT NO. 2008022487, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [18] LEASE AS EVIDENCED BY MEMORANDUM OF LEASE BY AND BETWEEN BELDOMFIA PROPERTIES LLC, A CALIFORNIA LIMITED LIABILITY COMPANY (LANDLORD) AND ARG ENTERPRISES INC., A CALIFORNIA CORPORATION (TENANT), RECORDED SEPTEMBER 12, 2007 AS DOCUMENT NO. 2007131156, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [19] ASSIGNMENT OF RIGHTS UNDER PROPERTY AGREEMENT FROM WEINGARTEN REALTY INVESTORS, A TEXAS REAL ESTATE INVESTMENT TRUST, JOINED TO THE EXTENT OF ITS INTEREST BY WEINGARTEN/INVESTMENTS, INC., A TEXAS CORPORATION TO WFC WYOMING NM LLC, A DELAWARE LIMITED LIABILITY COMPANY, RECORDED AUGUST 22, 2013 AS DOCUMENT NO. 2013094697, RECORDS OF BERNALILLO COUNTY, NEW MEXICO.
- [20] RIGHTS OF PARTIES IN POSSESSION AS TENANTS ONLY UNDER UNRECORDED RENTAL OR LEASE AGREEMENTS.

## Key Note Legend

- AFFECTS SUBJECT PROPERTY: AS SHOWN
- AFFECTS SUBJECT PROPERTY: NON-SURVEY MATTER
- △ DOES NOT AFFECT SUBJECT PROPERTY

## Notes

1. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION OF VISIBLE ABOVE GROUND APPURTENANCES. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.
2. NO OBSERVABLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS WITHIN RECENT MONTHS.
3. EASEMENT BEARINGS AND DISTANCES SHOWN HEREON ARE RECORD AND EASEMENTS HAVE BEEN ROTATED TO MATCH BASIS OF BEARINGS AND BOUNDARY UNLESS OTHERWISE INDICATED
4. ELEVATION DATUM IS BASED ON NAVD 1988 FROM AGRS MONUMENT "14\_H20", PUBLISHED ELEVATION (FEET) = 5415.798
5. THIS MAP HAS BEEN PRODUCED ACCORDING TO PROCEDURES THAT HAVE BEEN DEMONSTRATED TO PRODUCE DATA THAT MEETS THE MINIMUM STANDARDS FOR A TOPOGRAPHIC MAP COMPILED AT A SCALE OF 1"=20' WITH A CONTOUR INTERVAL OF ONE FOOT.

## Statement of Encroachments

- (A) APPARENT ENCROACHMENT OF RETAINING WALL ONTO UNDERGROUND UTILITY EASEMENT
- (B) APPARENT ENCROACHMENT OF FIRE HYDRANT ONTO SUBJECT PROPERTY

# ALTA/NSPS Land Title Survey

## A Portion of Parcel 5-A

# Wyoming Mall

Albuquerque, Bernalillo County, New Mexico

March 2021

## Legal Description

A LEASE SITE LYING AND SITUATE WITHIN SECTION 8, TOWNSHIP 10 NORTH, RANGE 4 EAST, N.M.P.M. CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO, COMPRISING OF A PORTION OF PARCEL 5-A, WYOMING MALL, AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT THEREOF FILED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO ON APRIL 2, 2007, IN MAP BOOK 2007C, PAGE 80, SAID LEASE SITE BEING MORE PARTICULARLY DESCRIBED BY NEW MEXICO STATE PLANE COORDINATE SYSTEM GRID BEARINGS (NAD 83-CENTRAL ZONE) AND GROUND DISTANCES (US SURVEY FEET) AS FOLLOWS,

BEGINNING AT THE SOUTHEAST CORNER OF DESCRIBED LEASE SITE, MARKED BY A SET NO. 5 REBAR WITH PINK PLASTIC CAP "PS 11993", FROM WHENCE A TIE TO A.G.R.S. MONUMENT "14\_H20" BEARS N 301°3'01" E, A DISTANCE OF 2036.60 FEET;

THENCE FROM SAID POINT OF BEGINNING, N 89°51'54" W, A DISTANCE OF 247.22 FEET TO THE SOUTHWEST CORNER OF DESCRIBED LEASE SITE, LYING ON THE EAST RIGHT OF WAY LINE OF WYOMING BOULEVARD, N.E., MARKED BY A SET NO. 5 REBAR WITH PINK PLASTIC CAP "PS 11993";

THENCE ALONG SAID EAST RIGHT OF WAY LINE, N 13°51'57" E, A DISTANCE OF 117.58 FEET TO AN ANGLE POINT, MARKED BY A SET CHISELED "X";

THENCE CONTINUING ALONG SAID EAST RIGHT OF WAY LINE, N 00°08'06" E, A DISTANCE OF 84.18 FEET TO THE NORTHWEST CORNER OF DESCRIBED LEASE SITE, MARKED BY A FOUND PK NAIL WITH WASHER "PS 14269";

THENCE LEAVING SAID EAST RIGHT OF WAY LINE, S 89°51'54" E, A DISTANCE OF 136.27 FEET TO AN ANGLE POINT, MARKED BY A SET PK NAIL WITH WASHER "PS 11993";

THENCE S 68°28'15" E, A DISTANCE OF 89.19 FEET TO THE NORTHEAST CORNER OF DESCRIBED LEASE SITE, MARKED BY A SET PK NAIL WITH WASHER "PS 11993";

THENCE S 00°08'06" W, A DISTANCE OF 165.86 FEET TO THE POINT OF BEGINNING, CONTAINING 1.0045 ACRES (43,755 SQUARE FEET), MORE OR LESS.

## Access Note

INGRESS AND EGRESS TO THE PREMISES IS PROVIDED BY WYOMING BOULEVARD, N.E. AND NORTHEASTERN BOULEVARD, N.E. THE SAME BEING PAVED AND DEDICATED RIGHTS OF WAY MAINTAINED BY THE CITY OF ALBUQUERQUE.

## Surveyor's Certificate

TO WFC WYOMING NM LLC, A DELAWARE LIMITED LIABILITY COMPANY AND OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 8, 9, 11, 13(B), 16 AND 17 OF TABLE A THEREOF. THE FIELDWORK WAS COMPLETED ON FEBRUARY 22, 2021.

DATE OF PLAT OR MAP: MARCH 12, 2021

I FURTHER CERTIFY THAT THIS SURVEY MEETS THE MINIMUM STANDARD REQUIREMENTS FOR BOUNDARY SURVEYS AS ADOPTED BY THE NEW MEXICO BOARD OF LICENSURE FOR ENGINEERS AND SURVEYORS.

TOPOGRAPHY BY FIELD SURVEYS USING GPS RTK MEASUREMENTS BASED ON SITE HORIZONTAL/VERTICAL CALIBRATION UTILIZING AGRS MONUMENTS. ELEVATIONS BASED ON AGRS MONUMENT "14\_H20" (NAVD 1988). THIS SURVEY MEETS THE MINIMUM STANDARDS FOR TOPOGRAPHIC SURVEYING IN NEW MEXICO AS ADOPTED BY THE NEW MEXICO BOARD OF LICENSURE FOR PROFESSIONAL ENGINEERS AND SURVEYORS.

*Larry W. Medrano*  
Digitally signed by Larry W Medrano  
Reason: I agree to the terms defined by the placement of my signature on this document  
Date: 2021.03.12 13:49:52-0700

LARRY W. MEDRANO  
N.M.P.S. No. 11993  
DATE

DIGITAL SIGNATURE IS INVALID WITHOUT DIGITAL CERTIFICATION



## Flood Note

BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE "X" OF THE FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 35001C0356H, WHICH BEARS AN EFFECTIVE DATE OF AUGUST 16, 2012 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA. NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE AND AN ELEVATION CERTIFICATE MAY BE NEEDED TO VERIFY THIS DETERMINATION OR APPLY FOR A VARIANCE FROM THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

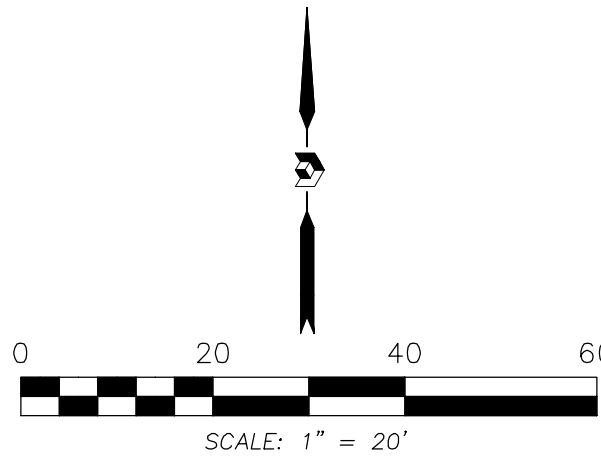
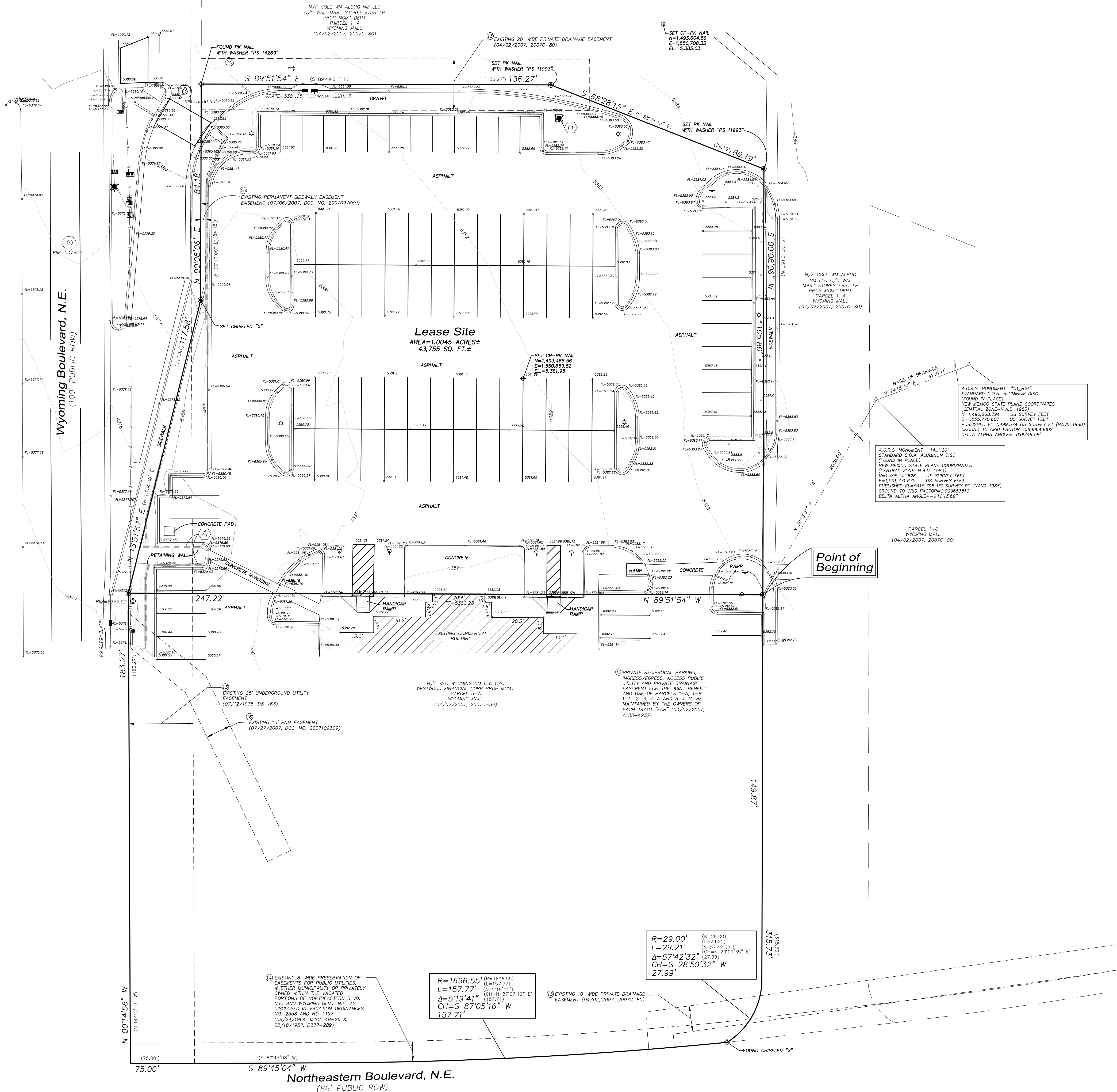
COORDINATE AND DIMENSION INFORMATION			PLSS INFORMATION			PROPERTY INFORMATION			<div><div><div></div><div></div><div></div></div><div><div>PRECISION</div><div>SURVEYS, INC.</div></div><div>© Copyright 2021 Precision Surveys, Inc.</div></div> <div><div>OFFICE LOCATION: 9200 San Mateo Boulevard, NE Albuquerque, NM 87113</div><div>505.856.5700    PHONE 505.856.7900    FAX</div></div> <div>CREW/TECH: MT/ML</div> <div>DATE OF SURVEY 02/19-22/2021</div> <div>DRAWN BY: JK</div> <div>CHECKED BY: LM</div> <div>PSI JOB NO. 216009ALT</div> <div>SHEET NUMBER 1 OF 2</div>											
STATE PLANE ZONE: NM-C	GRID (GROUND COORDINATES): GRID	TYPE: STANDARD	LAND GRANT N/A			PROPERTY OWNER WFC WYOMING NM LLC C/O WESTWOOD FINANCIAL CORP PROP MGMT														
HORIZONTAL DATUM: NAD83	VERTICAL DATUM: NAVD88	ROTATION ANGLE: 0° 00' 00.00"    MATCHES DRAWING UNITS YES	SECTION 8	TOWNSHIP 10 NORTH	RANGE 4 EAST	MERIDIAN NMPM	SUBDIVISION NAME WYOMING MALL													
CONTROL USED: ALBUQUERQUE GEODETIC REFERENCE SYSTEM			BASE POINT FOR SCALING AND/OR ROTATION: N = 0 E = 0			CITY ALBUQUERQUE						COUNTY BERNALILLO			STATE NM			UPC 102005901708131301		
COMBINED SCALE FACTOR: GRID TO GROUND: 1.000348716 GROUND TO GRID: 0.999651406			BEARING ANNOTATION: GROUND GRID			ELEVATION TRANSLATION: 00.00'    ELEVATIONS VALID: YES														



ALTA/NSPS Land Title Survey

A Portion of Parcel 5-A  
Wyoming Mall

Albuquerque, Bernalillo County, New Mexico  
March 2021



Legend

N 90°00'00" E	MEASURED BEARINGS AND DISTANCES
(N 90°00'00" E)	RECORD BEARINGS AND DISTANCES
○	FOUND AND USED MONUMENT AS DESIGNATED
⊙	DESIGNATES NO. 5 REBAR WITH PINK PLASTIC CAP "PS 11993" SET THIS SURVEY
⊕	CENTRAL POINT SET THIS SURVEY
△	FOUND ALUMINUM AGRS MONUMENT AS DESIGNATED
⊙	SERVICE/DROP POLE AS DESIGNATED
⊙	UTILITY POLE
⊙	ANCHOR POLE
⊙	LIGHT POLE
⊙	ELECTRIC BOX
⊙	ELECTRIC METER
⊙	TRAFFIC SIGNAL BOX
⊙	TRAFFIC LIGHT
⊙	UNKNOWN PEDESTAL
⊙	WATER VALVE
⊙	FIRE HYDRANT
⊙	SANITARY SEWER MANHOLE
⊙	STORM SEWER MANHOLE
⊙	SEW
⊙	CURB INLET
⊙	HANDICAP PARKING SPACE
⊙	CURB AND GUTTER
⊙	OVERHEAD UTILITY LINE

COORDINATE AND DIMENSION INFORMATION				PLSS INFORMATION				PROPERTY INFORMATION	
STATE PLANE ZONE: NM-C	GRID	TYPE: STANDARD		LAND GRANT: N/A				PROPERTY OWNER: WFC WYOMING NM LLC C/O WESTWOOD FINANCIAL CORP PROP MGMT	
HORIZONTAL DATUM: NAD83	VERTICAL DATUM: NAVD88	ROTATION ANGLE: 0° 00' 00.00" YES	MATCHES DRAWING UNITS	SECTION: 8	TOWNSHIP: 10 NORTH	RANGE: 4 EAST	MERIDIAN: NMMP	SUBDIVISION NAME: WYOMING MALL	
CONTROL USED: ALBUQUERQUE GEODETIC REFERENCE SYSTEM		BASE POINT FOR SCALING AND/OR ROTATION: N = 0 E = 0		CITY: ALBUQUERQUE	COUNTY: BERNALILLO	STATE: NM		ADDRESS: 2004 WYOMING BLVD, N.E.	
COMBINED SCALE FACTOR: GRID TO GROUND: 1.000348716 GROUND TO GRID: 0.999651406		DISTANCE ANNOTATION: GROUND BEARING ANNOTATION: GRID	ELEVATION TRANSLATION: ±0.00'						



OFFICE LOCATION:  
9200 San Mateo Boulevard, NE  
Albuquerque, NM 87113  
505.856.5700 PHONE  
505.856.7900 FAX

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PROJECT INFORMATION	
CREW/TECH: MT/ML	DATE OF SURVEY: 02/19-22/2021
DRAWN BY: JK	CHECKED BY: LM
PSI JOB NO. 216009ALT	SHEET NUMBER 2 OF 2



KIMLEY HORN GENERAL NOTES:

- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THESE PLANS, CITY (OR TOWN) STANDARD DETAILS AND SPECIFICATIONS, THE FINAL GEOTECHNICAL REPORT AND ALL ISSUED ADDENDA, AND COMMONLY ACCEPTED CONSTRUCTION STANDARDS. THE CITY SPECIFICATIONS SHALL GOVERN WHERE OTHER SPECIFICATIONS DO NOT EXIST. IN CASE OF CONFLICTING SPECIFICATIONS OR DETAILS, THE MORE RESTRICTIVE SPECIFICATION AND DETAIL SHALL BE FOLLOWED.
- THE CONTRACTOR SHALL COMPLY WITH CITY (OR TOWN) "GENERAL NOTES" FOR CONSTRUCTION, IF EXISTING AND REQUIRED BY THE CITY. FOR INSTANCES WHERE THEY CONFLICT WITH THESE KH GENERAL NOTES, THEN THE MORE RESTRICTIVE SHALL APPLY.
- THE CONTRACTOR SHALL FURNISH ALL MATERIAL AND LABOR TO CONSTRUCT THE FACILITY AS SHOWN AND DESCRIBED IN THE CONSTRUCTION DOCUMENTS IN ACCORDANCE WITH THE APPROPRIATE AUTHORITIES' SPECIFICATIONS AND REQUIREMENTS.
- THE CONTRACTOR SHALL VISIT THE SITE PRIOR TO BIDDING TO DETERMINE EXISTING CONDITIONS.
- THE EXISTING CONDITIONS SHOWN ON THESE PLANS WERE PROVIDED BY THE TOPOGRAPHIC SURVEY PREPARED BY THE PROJECT SURVEYOR, AND ARE BASED ON THE BENCHMARKS SHOWN. THE CONTRACTOR SHALL REFERENCE THE SAME BENCHMARKS.
- THE CONTRACTOR SHALL REVIEW AND VERIFY THE EXISTING TOPOGRAPHIC SURVEY SHOWN ON THE PLANS REPRESENTS EXISTING FIELD CONDITIONS PRIOR TO CONSTRUCTION, AND SHALL REPORT ANY DISCREPANCIES FOUND TO THE OWNER AND ENGINEER IMMEDIATELY.
- IF THE CONTRACTOR DOES NOT ACCEPT THE EXISTING TOPOGRAPHIC SURVEY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY AT THEIR OWN EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED PROFESSIONAL LAND SURVEYOR TO THE OWNER AND ENGINEER FOR REVIEW.
- CONTRACTOR SHALL PROVIDE ALL CONSTRUCTION SURVEYING AND STAKING.
- CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL CONTROL, INCLUDING BENCHMARKS PRIOR TO COMMENCING CONSTRUCTION OR STAKING OF IMPROVEMENTS. PROPERTY LINES AND CORNERS SHALL BE HELD AS THE HORIZONTAL CONTROL.
- THE CONTRACTOR SHALL REVIEW AND VERIFY ALL DIMENSIONS, ELEVATIONS, AND FIELD CONDITIONS THAT MAY AFFECT CONSTRUCTION. ANY DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ARCHITECT AND ENGINEER BEFORE COMMENCING WORK. NO FIELD CHANGES OR DEVIATIONS FROM DESIGN ARE TO BE MADE WITHOUT PRIOR APPROVAL OF THE ARCHITECT, ENGINEER, AND IF APPLICABLE THE CITY AND OWNER. NO CONSIDERATION WILL BE GIVEN TO CHANGE ORDERS FOR WHICH THE CITY, ENGINEER, AND OWNER WERE NOT CONTACTED PRIOR TO CONSTRUCTION OF THE AFFECTED ITEM.
- CONTRACTOR SHALL THOROUGHLY CHECK COORDINATION OF CIVIL, LANDSCAPE, MEP, ARCHITECTURAL, AND OTHER PLANS PRIOR TO COMMENCING CONSTRUCTION. OWNER/ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCY PRIOR TO COMMENCING WITH CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES WHICH MAY HAVE BURIED OR AERIAL UTILITIES WITHIN OR NEAR THE CONSTRUCTION AREA BEFORE COMMENCING WORK TO HAVE THEM LOCATE THEIR EXISTING UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE AN ADEQUATE MINIMUM NOTICE TO ALL UTILITY COMPANIES PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR SHALL CALL NEW MEXICO 811 AN ADEQUATE AMOUNT OF TIME PRIOR TO COMMENCING CONSTRUCTION OR ANY EXCAVATION.
- CONTRACTOR SHALL USE EXTREME CAUTION AS THE SITE CONTAINS VARIOUS KNOWN AND UNKNOWN PUBLIC AND PRIVATE UTILITIES.
- THE LOCATIONS, ELEVATIONS, DEPTH, AND DIMENSIONS OF EXISTING UTILITIES SHOWN ON THE PLANS WERE OBTAINED FROM AVAILABLE UTILITY COMPANY MAPS AND PLANS, AND ARE CONSIDERED APPROXIMATE AND INCOMPLETE. IT SHALL BE THE CONTRACTORS' RESPONSIBILITY TO VERIFY THE PRESENCE, LOCATION, ELEVATION, DEPTH, AND DIMENSION OF EXISTING UTILITIES SUFFICIENTLY IN ADVANCE OF CONSTRUCTION SO THAT ADJUSTMENTS CAN BE MADE TO PROVIDE ADEQUATE CLEARANCES. THE ENGINEER SHALL BE NOTIFIED WHEN A PROPOSED IMPROVEMENT CONFLICTS WITH AN EXISTING UTILITY.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ANY ADJUSTMENTS AND RELOCATIONS OF EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO, ADJUSTING EXISTING MANHOLES TO MATCH PROPOSED GRADE, RELOCATING EXISTING POLES AND GUY WIRES THAT ARE LOCATED IN PROPOSED DRIVEWAYS, ADJUSTING THE HORIZONTAL OR VERTICAL ALIGNMENT OF EXISTING UNDERGROUND UTILITIES TO ACCOMMODATE PROPOSED GRADE OR CROSSING WITH A PROPOSED UTILITY, AND ANY OTHERS THAT MAY BE ENCOUNTERED THAT ARE UNKNOWN AT THIS TIME AND NOT SHOWN ON THESE PLANS.
- CONTRACTOR SHALL ARRANGE FOR OR PROVIDE, AT ITS EXPENSE, ALL GAS, TELECOMMUNICATIONS, CABLE, OVERHEAD AND UNDERGROUND POWER LINE, AND UTILITY POLE ADJUSTMENTS NEEDED.
- CONTRACTOR IS RESPONSIBLE FOR COORDINATING INSTALLATION OF FRANCHISE UTILITIES THAT ARE NECESSARY FOR ON-SITE AND OFF-SITE CONSTRUCTION, AND SERVICE TO THE PROPOSED DEVELOPMENT.
- THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ALL DAMAGES DUE TO THE CONTRACTORS' FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UTILITIES. THE OWNER OR ENGINEER WILL ASSUME NO LIABILITY FOR ANY DAMAGES SUSTAINED OR COST INCURRED BECAUSE OF THE OPERATIONS IN THE VICINITY OF EXISTING UTILITIES OR STRUCTURES. IF IT IS NECESSARY TO SHORE, BRACE, SWING OR RELOCATE A UTILITY, THE UTILITY COMPANY OR DEPARTMENT AFFECTED SHALL BE CONTACTED BY THE CONTRACTOR AND THEIR PERMISSION OBTAINED REGARDING THE METHOD TO USE FOR SUCH WORK.
- BRACING OF UTILITY POLES MAY BE REQUIRED BY THE UTILITY COMPANIES WHEN TRENCHING OR EXCAVATING NEAR THE POLES. THE COST OF BRACING POLES WILL BE BORNE BY THE CONTRACTOR, WITH NO SEPARATE PAY ITEM FOR THIS WORK. THE COST IS INCIDENTAL TO THE PAY ITEM.
- CONTRACTOR SHALL USE ALL NECESSARY SAFETY PRECAUTIONS TO AVOID CONTACT WITH OVERHEAD AND UNDERGROUND POWER LINES. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE LOCAL, STATE, FEDERAL AND UTILITY OWNER REGULATIONS PERTAINING TO WORK SETBACKS FROM POWER LINES.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL REQUIRED CONSTRUCTION PERMITS, APPROVALS, AND BONDS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL HAVE AVAILABLE AT THE JOB SITE AT ALL TIMES A COPY OF THE CONTRACT DOCUMENTS INCLUDING PLANS, GEOTECHNICAL REPORT AND ADDENDA, PROJECT AND CITY SPECIFICATIONS, AND SPECIAL CONDITIONS. COPIES OF ANY REQUIRED CONSTRUCTION PERMITS, EROSION CONTROL PLANS, SWPPP AND INSPECTION REPORTS.
- ALL SHOP DRAWINGS AND OTHER DOCUMENTS THAT REQUIRE ENGINEER REVIEW SHALL BE SUBMITTED BY THE CONTRACTOR SUFFICIENTLY IN ADVANCE OF CONSTRUCTION OF THAT ITEM, SO THAT NO LESS THAN 10 BUSINESS DAYS FOR REVIEW AND RESPONSE IS AVAILABLE.
- ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES, JURISDICTIONAL AGENCIES, AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO USE OF THE FACILITY AND THE FINAL CONNECTION OF SERVICES.
- CONTRACTOR SHALL ARRANGE FOR REQUIRED CITY INSPECTIONS.
- CONTRACTORS BID PRICE SHALL INCLUDE ALL INSPECTION FEES.
- ALL SYMBOLS SHOWN ON THESE PLANS (E.G. FIRE HYDRANT, METERS, VALVES, INLETS, ETC...) ARE FOR PRESENTATION PURPOSES ONLY AND ARE NOT TO SCALE. CONTRACTOR SHALL COORDINATE FINAL SIZES AND LOCATIONS WITH APPROPRIATE CITY INSPECTOR.
- THE SCOPE OF WORK FOR THE CIVIL IMPROVEMENTS SHOWN ON THESE PLANS TERMINATES 5-FEET FROM THE BUILDING. REFERENCE THE BUILDING PLANS (E.G. ARCHITECTURAL, STRUCTURAL, MEP) FOR AREAS WITHIN 5-FEET OF THE BUILDING AND WITHIN THE BUILDING FOOTPRINT.
- REFER TO ARCHITECTURAL AND STRUCTURAL PLANS FOR ALL FINAL BUILDING DIMENSIONS.
- THE PROPOSED BUILDING (S) SHOWN ON THESE PLANS WERE PROVIDED TO KIMLEY HORN AND ASSOCIATES, INC. (KH) BY THE PROJECT ARCHITECT AT THE TIME THESE PLANS WERE PREPARED. IT MAY NOT BE THE FINAL CORRECT VERSION BECAUSE THE BUILDING DESIGN WAS ONGOING. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR CONFIRMING THE FINAL CORRECT VERSION OF THE BUILDING FOOTPRINT WITH THE ARCHITECT AND STRUCTURAL ENGINEER PRIOR TO LAYOUT. DIMENSIONS AND/OR COORDINATES SHOWN ON THESE PLANS WERE BASED ON THE ABOVE STATED ARCHITECTURAL FOOTPRINT, AND ARE THEREFORE A PRELIMINARY LOCATION OF THE BUILDING. THE CONTRACTOR IS SOLELY RESPONSIBLE TO VERIFY WHAT PART OF THE

KIMLEY HORN GENERAL NOTES (CONTINUED):

- BUILDING THE ARCHITECT'S FOOTPRINT REPRESENTS (E.G. SLAB, OUTSIDE WALL, MASONRY LEGGE, ETC...) AND TO CONFIRM ITS FINAL POSITION ON THE SITE BASED ON THE FINAL ARCHITECTURAL FOOTPRINT, CIVIL DIMENSION CONTROL PLAN, SURVEY BOUNDARY AND/OR PLAT. ANY DIFFERENCES FOUND SHALL BE REPORTED TO KH IMMEDIATELY.
- ALL CONSTRUCTION SHALL COMPLY WITH THE PROJECT'S FINAL GEOTECHNICAL REPORT (OR LATEST EDITION), INCLUDING SUBSEQUENT ADDENDA.
- CONTRACTOR IS RESPONSIBLE FOR ALL MATERIALS TESTING AND CERTIFICATION, UNLESS SPECIFIED OTHERWISE BY OWNER. ALL MATERIALS TESTING SHALL BE COORDINATED WITH THE APPROPRIATE CITY INSPECTOR AND COMPLY WITH CITY STANDARD SPECIFICATIONS AND GEOTECHNICAL REPORT. TESTING SHALL BE PERFORMED BY AN APPROVED INDEPENDENT AGENCY FOR TESTING MATERIALS. OWNER SHALL APPROVE THE AGENCY NOMINATED BY THE CONTRACTOR FOR MATERIALS TESTING.
- ALL COPIES OF MATERIALS TEST RESULTS SHALL BE SENT TO THE OWNER, ENGINEER AND ARCHITECT DIRECTLY FROM THE TESTING AGENCY.
- IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO SHOW, BY THE STANDARD TESTING PROCEDURES OF THE MATERIALS, THAT THE WORK CONSTRUCTED MEETS THE PROJECT REQUIREMENTS AND CITY SPECIFICATIONS.
- DUE TO THE POTENTIAL FOR DIFFERENTIAL SOIL MOVEMENT ADJACENT TO THE BUILDING, THE CONTRACTOR SHALL ADHERE TO GEOTECHNICAL REPORT'S RECOMMENDATION FOR SUBGRADE PREPARATION SPECIFIC TO FLATWORK ADJACENT TO THE PROPOSED BUILDING. THE OWNER AND CONTRACTOR ARE ADVISED TO OBTAIN A GEOTECHNICAL ENGINEER RECOMMENDATION SPECIFIC TO FLATWORK ADJACENT TO THE BUILDING, IF NONE IS CURRENTLY EXISTING.
- ALL CONTRACTORS MUST CONFINE THEIR ACTIVITIES TO THE WORK AREA. NO ENCROACHMENTS OUTSIDE OF THE WORK AREA WILL BE ALLOWED. ANY DAMAGE RESULTING THEREFROM SHALL BE CONTRACTOR'S SOLE RESPONSIBILITY TO REPAIR.
- CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES, UTILITIES, MANHOLES, POLES, GUY WIRES, VALVE COVERS, VAULT LIDS, FIRE HYDRANTS, COMMUNICATION BOXES/PEDESTALS, AND OTHER FACILITIES TO REMAIN AND SHALL REPAIR ANY DAMAGES AT NO COST TO THE OWNER.
- THE CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY PHYSICAL DAMAGE TO PRIVATE PROPERTY OR PUBLIC IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO: FENCES, WALLS, SIGNS, PAVEMENT, CURBS, UTILITIES, SIDEWALKS, GRASS, TREES, LANDSCAPING, AND IRRIGATION SYSTEMS, ETC... TO ORIGINAL CONDITION OR BETTER AT NO COST TO THE OWNER.
- ALL AREAS IN EXISTING RIGHT-OF-WAY DISTURBED BY SITE CONSTRUCTION SHALL BE REPAIRED TO ORIGINAL CONDITION OR BETTER, INCLUDING AS NECESSARY GRADING, LANDSCAPING, CULVERTS, AND PAVEMENT.
- THE CONTRACTOR SHALL SALVAGE ALL EXISTING POWER POLES, SIGNS, WATER VALVES, FIRE HYDRANTS, METERS, ETC... THAT ARE TO BE RELOCATED DURING CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN ADEQUATE SITE DRAINAGE DURING ALL PHASES OF CONSTRUCTION, INCLUDING MAINTAINING EXISTING DITCHES OR CULVERTS FREE OF OBSTRUCTIONS AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND SUBMITTING A TRENCH SAFETY PLAN, PREPARED BY A PROFESSIONAL ENGINEER IN THE STATE OF NEW MEXICO, TO THE CITY PRIOR TO CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TRENCH SAFETY REQUIREMENTS IN ACCORDANCE WITH CITY, STATE, AND FEDERAL REQUIREMENTS, INCLUDING OSHA FOR ALL TRENCHES. NO OPEN TRENCHES SHALL BE ALLOWED OVERNIGHT WITHOUT PRIOR WRITTEN APPROVAL OF THE CITY.
- THE CONTRACTOR SHALL KEEP TRENCHES FREE FROM WATER.
- THE SAFETY IS SOLELY THE RESPONSIBILITY OF THE CONTRACTOR.
- THESE PLANS DO NOT EXTEND TO OR INCLUDE DESIGNS OR SYSTEMS PERTAINING TO THE SAFETY OF THE CONTRACTOR OR ITS EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE ENGINEER'S SEAL HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF ALL REQUIRED SAFETY PROCEDURES AND PROGRAMS.
- SIGNS RELATED TO SITE OPERATION OR SAFETY ARE NOT INCLUDED IN THESE PLANS.
- CONTRACTOR OFFICE AND STAGING AREA SHALL BE AGREED ON BY THE OWNER AND CONTRACTOR PRIOR TO BEGINNING OF CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR ALL PERMITTING REQUIREMENTS FOR THE CONSTRUCTION OFFICE, TRAILER, STORAGE, AND STAGING OPERATIONS AND LOCATIONS.
- LIGHT POLES, SIGNS, AND OTHER OBSTRUCTIONS SHALL NOT BE PLACED IN ACCESSIBLE ROUTES.
- ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE "ASHTO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
- TOP RIM ELEVATIONS OF ALL EXISTING AND PROPOSED MANHOLES SHALL BE COORDINATED WITH TOP OF PAVEMENT OR FINISHED GRADE AND SHALL BE ADJUSTED TO BE FLUSH WITH THE ACTUAL FINISHED GRADE AT THE TIME OF PAVING.
- CONTRACTOR SHALL ADJUST ALL EXISTING AND PROPOSED VALVES, FIRE HYDRANTS, AND OTHER UTILITY APPURTENANCES TO MATCH ACTUAL FINISHED GRADES AT THE TIME OF PAVING.
- THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION SEQUENCING AND PHASING, AND SHALL CONTACT THE APPROPRIATE CITY OFFICIALS, INCLUDING BUILDING OFFICIAL, ENGINEERING INSPECTOR, AND FIRE MARSHAL TO LEARN OF ANY REQUIREMENTS.
- CONTRACTOR IS RESPONSIBLE FOR PREPARATION, SUBMITTAL, AND APPROVAL BY THE CITY OF A TRAFFIC CONTROL PLAN PRIOR TO THE START OF CONSTRUCTION, AND THEN THE IMPLEMENTATION OF THE PLAN.
- CONTRACTOR SHALL KEEP A NEAT AND ACCURATE RECORD OF CONSTRUCTION, INCLUDING ANY DEVIATIONS OR VARIANCES FROM THE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AS-BUILT PLANS TO THE ENGINEER AND CITY IDENTIFYING ALL DEVIATIONS AND VARIATIONS FROM THESE PLANS MADE DURING CONSTRUCTION.
- UNDERGROUND LUMING, GAS, SEWER, WATER, ELECTRICITY, TELEPHONE AND OTHER CABLES) MARKING EXCAVATIONS AS DEFINED BY CHAPTER 6, ARTICLE 5 SECTION 6-5-2.2 OF THE 2005 STREET, EXCAVATION AND BARRICADE ORDINANCE ARE REQUIRED TO COMPLY WITH ALL PROVISIONS THEREOF.

KIMLEY HORN GENERAL EROSION CONTROL NOTES:

- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL EROSION CONTROL AND WATER QUALITY REQUIREMENTS, LAWS, AND ORDINANCES THAT APPLY TO THE CONSTRUCTION SITE LAND DISTURBANCE.
- EROSION CONTROL DEVICES SHOWN ON THE EROSION CONTROL PLAN FOR THE PROJECT SHALL BE INSTALLED PRIOR TO THE START OF LAND DISTURBANCE.
- ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS FOR THE PROJECT.
- CONTRACTOR IS SOLELY RESPONSIBLE FOR INSTALLATION, IMPLEMENTATION, MAINTENANCE, AND EFFECTIVENESS OF ALL EROSION CONTROL DEVICES, BEST MANAGEMENT PRACTICES (BMPs), AND FOR UPDATING THE EROSION CONTROL PLAN DURING CONSTRUCTION AS FIELD CONDITIONS CHANGE.
- CONTRACTOR SHALL DOCUMENT THE DATES OF INSTALLATION, MAINTENANCE OR MODIFICATION, AND REMOVAL FOR EACH BMP EMPLOYED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IF APPLICABLE.
- AS STORM SEWER INLETS ARE INSTALLED ON-SITE, TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED AT EACH INLET PER APPROVED DETAILS.
- THE EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL THE AREA IT PROTECTS HAS BEEN PERMANENTLY STABILIZED.
- CONTRACTOR SHALL PROVIDE ADEQUATE EROSION CONTROL DEVICES NEEDED DUE TO PROJECT PHASING.
- CONTRACTOR SHALL OBSERVE THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES AND MAKE FIELD ADJUSTMENTS AND MODIFICATIONS AS NEEDED TO PREVENT SEDIMENT FROM LEAVING THE SITE. IF THE EROSION CONTROL DEVICES DO NOT EFFECTIVELY CONTROL EROSION AND PREVENT SEDIMENTATION FROM WASHING OFF THE SITE, THEN THE CONTRACTOR SHALL NOTIFY THE ENGINEER.
- OFF-SITE SOIL BORROW, SPOIL, AND STORAGE AREAS (IF APPLICABLE) ARE CONSIDERED AS PART OF THE PROJECT SITE AND MUST ALSO COMPLY WITH THE EROSION CONTROL REQUIREMENTS FOR THIS PROJECT. THIS INCLUDES THE INSTALLATION OF BMPs TO CONTROL DISTURBED AREAS PRIOR TO FINAL APPROVAL OF THE PROJECT. CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE SWPPP AND EROSION CONTROL PLAN TO INCLUDE BMPs FOR ANY OFF-SITE THAT ARE NOT ANTICIPATED OR SHOWN ON THE EROSION CONTROL PLAN.

KIMLEY HORN GENERAL EROSION CONTROL NOTES (CONTINUED):

- ALL STAGING, STOCKPILES, SPOIL, AND STORAGE SHALL BE LOCATED SUCH THAT THEY WILL NOT ADVERSELY AFFECT STORM WATER QUALITY. PROTECTIVE MEASURES SHALL BE PROVIDED IF NEEDED TO ACCOMPLISH THIS REQUIREMENT, SUCH AS COVERING OR ENCIRCLING THE AREA WITH AN APPROPRIATE BARRIER.
- CONTRACTORS SHALL INSPECT ALL EROSION CONTROL DEVICES, BMPs, DISTURBED AREAS, AND VEHICLE ENTRY AND EXIT AREAS WEEKLY AND WITHIN 24 HOURS OF ALL RAINFALL EVENTS OF 0.5 INCHES OR GREATER, AND KEEP A RECORD OF THIS INSPECTION IN THE SWPPP BOOKLET IF APPLICABLE, TO VERIFY THAT THE DEVICES AND EROSION CONTROL PLAN ARE FUNCTIONING PROPERLY.
- CONTRACTOR SHALL CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE AT ALL PRIMARY POINTS OF ACCESS IN ACCORDANCE WITH CITY SPECIFICATIONS. CONTRACTOR SHALL ENSURE THAT ALL CONSTRUCTION TRAFFIC USES THE STABILIZED ENTRANCE AT ALL TIMES FOR ALL INGRESS/EGRESS.
- SITE ENTRY AND EXITS SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT THE TRACKING AND FLOWING OF SEDIMENT AND DIRT ONTO OFF-SITE ROADWAYS. ALL SEDIMENT AND DIRT FROM THE SITE THAT IS DEPOSITED ONTO AN OFF-SITE ROADWAY SHALL BE REMOVED IMMEDIATELY.
- THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL SILT AND DEBRIS FROM THE AFFECTED OFF-SITE ROADWAYS THAT ARE A RESULT OF THE CONSTRUCTION, AS REQUESTED BY OWNER AND CITY. AT A MINIMUM, THIS SHOULD OCCUR ONCE PER DAY FOR THE OFF-SITE ROADWAYS.
- WHEN WASHING OF VEHICLES IS REQUIRED TO REMOVE SEDIMENT PRIOR TO EXITING THE SITE, IT SHALL BE DONE IN AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP BMP.
- CONTRACTOR SHALL INSTALL A TEMPORARY SEDIMENT BASIN, PER NMED AND CITY STANDARDS, IF NO ENGINEERING DESIGN HAS BEEN PROVIDED FOR A SEDIMENTATION BASIN ON THESE PLANS, THEN THE CONTRACTOR SHALL ARRANGE FOR AN APPROPRIATE DESIGN TO BE PROVIDED.
- ALL FINES IMPOSED FOR SEDIMENT OR DIRT DISCHARGED FROM THE SITE SHALL BE PAID BY THE RESPONSIBLE CONTRACTOR.
- WHEN SEDIMENT OR DIRT HAS CLOGGED THE CONSTRUCTION ENTRANCE VOID SPACES BETWEEN STONES OR DIRT IS BEING TRACKED ONTO A ROADWAY, THE AGGREGATE PAD MUST BE WASHED DOWN OR REPLACED. RUNOFF FROM THE WASH-DOWN OPERATION SHALL NOT BE ALLOWED TO DRAIN DIRECTLY OFF SITE WITHOUT FIRST FLOWING THROUGH ANOTHER BMP TO CONTROL SEDIMENTATION. PERIODIC RE-GRADING OR NEW STONE MAY BE REQUIRED TO MAINTAIN THE EFFECTIVENESS OF THE CONSTRUCTION ENTRANCE.
- TEMPORARY SEEDING OR OTHER APPROVED STABILIZATION SHALL BE INITIATED WITHIN 14 DAYS OF THE LAST DISTURBANCE OF ANY AREA, UNLESS ADDITIONAL CONSTRUCTION IN THE AREA IS EXPECTED WITHIN 21 DAYS OF THE LAST DISTURBANCE.
- CONTRACTOR SHALL FOLLOW GOOD HOUSEKEEPING PRACTICES DURING CONSTRUCTION, ALWAYS CLEANING UP DIRT, LOOSE MATERIAL, AND TRASH AS CONSTRUCTION PROGRESSES.
- UPON COMPLETION OF FINE GRADING, ALL SURFACES OF DISTURBED AREAS SHALL BE PERMANENTLY STABILIZED. STABILIZATION IS ACHIEVED WHEN THE AREA IS EITHER COVERED BY PERMANENT IMPERVIOUS STRUCTURES, SUCH AS BUILDINGS, SIDEWALK, PAVEMENT, OR A UNIFORM PERENNIAL VEGETATIVE COVER.
- AT THE CONCLUSION OF THE PROJECT, ALL INLETS, DRAIN PIPE, CHANNELS, DRAINAGWAYS AND BORROW DITCHES AFFECTED BY THE CONSTRUCTION SHALL BE DREGDED, AND THE SEDIMENT GENERATED BY THE PROJECT SHALL BE REMOVED AND DISPOSED IN ACCORDANCE WITH APPLICABLE REGULATIONS.

KIMLEY HORN GENERAL STORM WATER DISCHARGE AUTHORIZATION NOTES:

- CONTRACTOR SHALL COMPLY WITH ALL NMED AND EPA STORM WATER POLLUTION PREVENTION REQUIREMENTS.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF THE NMED GENERAL PERMIT TO DISCHARGE UNDER THE NEW MEXICO POLLUTANT DISCHARGE ELIMINATION SYSTEM.
- THE CONTRACTOR SHALL ENSURE THAT ALL PRIMARY OPERATORS SUBMIT A NOI TO NMED AT LEAST SEVEN DAYS PRIOR TO COMMENCING CONSTRUCTION (IF APPLICABLE), OR IF UTILIZING ELECTRONIC SUBMITTAL, PRIOR TO COMMENCING CONSTRUCTION. ALL PRIMARY OPERATORS SHALL PROVIDE A COPY OF THE SIGNED NOI TO THE OPERATOR OF ANY MSA (TYPICALLY THE CITY) RECEIVING DISCHARGE FROM THE SITE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMPLEMENTATION OF THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IF APPLICABLE, INCLUDING POSTING SITE NOTICE, INSPECTIONS, DOCUMENTATION, AND SUBMISSION OF ANY INFORMATION REQUIRED BY THE NMED AND EPA (E.G. NOI).
- ALL CONTRACTORS AND SUBCONTRACTORS PROVIDING SERVICES RELATED TO THE SWPPP SHALL SIGN THE REQUIRED CONTRACTOR CERTIFICATION STATEMENT ACKNOWLEDGING THEIR RESPONSIBILITIES AS SPECIFIED IN THE SWPPP.
- A COPY OF THE SWPPP, INCLUDING NOI, SITE NOTICE, CONTRACTOR CERTIFICATIONS, AND ANY REVISIONS, SHALL BE SUBMITTED TO THE CITY BY THE CONTRACTOR AND SHALL BE RETAINED ON-SITE DURING CONSTRUCTION.
- A NOTICE OF TERMINATION (NOT) SHALL BE SUBMITTED TO NMED BY ANY PRIMARY OPERATOR WITHIN 30 DAYS AFTER ALL SOIL ACTIVITIES AT THE SITE HAVE BEEN COMPLETED AND A UNIFORM VEGETATIVE COVER HAS BEEN ESTABLISHED ON ALL UNPAVED AREAS AND AREAS NOT COVERED BY STRUCTURES, A TRANSFER OF OPERATIONAL CONTROL HAS OCCURRED, OR THE OPERATOR HAS OBTAINED ALTERNATIVE AUTHORIZATION UNDER A DIFFERENT PERMIT. A COPY OF THE NOT SHALL BE PROVIDED TO THE OPERATOR OF ANY MSA RECEIVING DISCHARGE FROM THE SITE.

KIMLEY HORN GENERAL DEMOLITION NOTES:

- KH IS NOT RESPONSIBLE FOR THE MEANS AND METHODS EMPLOYED BY THE CONTRACTOR TO IMPLEMENT THIS DEMOLITION PLAN. THIS PRELIMINARY DEMOLITION PLAN SIMPLY INDICATES THE KNOWN OBJECTS ON THE SUBJECT TRACT THAT ARE TO BE DEMOLISHED AND REMOVED FROM THE SITE.
- KH DOES NOT WARRANT OR REPRESENT THAT THE PLAN, WHICH WAS PREPARED BASED ON SURVEY AND UTILITY INFORMATION PROVIDED BY OTHERS, SHOWS ALL IMPROVEMENTS AND UTILITIES, THAT THE IMPROVEMENTS AND UTILITIES ARE SHOWN ACCURATELY, OR THAT THE UTILITIES SHOWN CAN BE REMOVED. THE CONTRACTOR IS RESPONSIBLE FOR PERFORMING ITS OWN SITE RECONNAISSANCE TO SCOPE ITS WORK AND TO CONFIRM WITH THE OWNERS OF IMPROVEMENTS AND UTILITIES THE ABILITY AND PROCESS FOR THE REMOVAL OF THEIR FACILITIES.
- THIS PLAN IS INTENDED TO GIVE A GENERAL GUIDE TO THE CONTRACTOR, NOTHING MORE. THE GOAL OF THE DEMOLITION IS TO LEAVE THE SITE IN A STATE SUITABLE FOR THE CONSTRUCTION OF THE PROPOSED DEVELOPMENT. REMOVAL OR PRESERVATION OF IMPROVEMENTS, UTILITIES, ETC., TO ACCOMPLISH THIS GOAL ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR IS STRONGLY CAUTIONED TO REVIEW THE FOLLOWING REPORTS DESCRIBING SITE CONDITIONS PRIOR TO BIDDING AND IMPLEMENTING THE DEMOLITION PLAN:
  - ENVIRONMENTAL SITE ASSESSMENT PROVIDED BY THE OWNER.
  - ASBESTOS BUILDING INSPECTION REPORT(S) PROVIDED BY THE OWNER.
  - GEOTECHNICAL REPORT PROVIDED BY THE OWNER.
  - OTHER REPORTS THAT ARE APPLICABLE AND AVAILABLE.
- CONTRACTOR SHALL CONTACT THE OWNER TO VERIFY WHETHER ADDITIONAL REPORTS OR AMENDMENTS TO THE ABOVE CITED REPORTS HAVE BEEN PREPARED AND TO OBTAIN REVIEW AND COMPLIANCE WITH THE RECOMMENDATION OF SUCH STUDIES PRIOR TO STARTING ANY WORK ON THE SITE.
- CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS REGARDING THE DEMOLITION OF OBJECTS ON THE SITE AND THE DISPOSAL OF THE DEMOLISHED MATERIALS OFF-SITE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO REVIEW THE SITE, DETERMINE THE APPLICABLE REGULATIONS, RECEIVE THE REQUIRED PERMITS AND AUTHORIZATIONS, AND COMPLY.
- KH DOES NOT REPRESENT THAT THE REPORTS AND SURVEYS REFERENCED ABOVE ARE ACCURATE, COMPLETE, OR COMPREHENSIVE SHOWING ALL ITEMS THAT WILL NEED TO BE DEMOLISHED AND REMOVED.
- SURFACE PAVEMENT INDICATED MAY OVERLAY OTHER HIDDEN STRUCTURES, SUCH AS ADDITIONAL LAYERS OF PAVEMENT, FOUNDATIONS OR WALLS, THAT ARE ALSO TO BE REMOVED.

KIMLEY HORN GENERAL GRADING AND DRAINAGE NOTES:

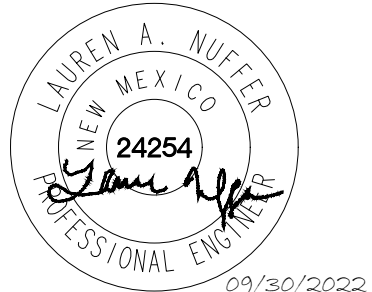
- THE CONTRACTOR AND GRADING SUBCONTRACTOR SHALL VERIFY THE SUITABILITY OF EXISTING AND PROPOSED SITE CONDITIONS INCLUDING GRADES AND DIMENSIONS BEFORE START OF CONSTRUCTION. THE CIVIL ENGINEER SHALL BE NOTIFIED IMMEDIATELY OF ANY DISCREPANCIES.
- CONTRACTOR SHALL OBTAIN ANY REQUIRED GRADING PERMITS FROM THE CITY.
- UNLESS OTHERWISE NOTED, PROPOSED CONTOURS AND SPOT ELEVATIONS SHOWN IN PAVED AREA REFLECT TOP OF PAVEMENT SURFACE. IN LOCATIONS ALONG A CURB LINE, ADD 6-INCHES (OR THE HEIGHT OF THE CURB) TO THE PAVING GRADE FOR TOP OF CURB ELEVATION.
- PROPOSED SPOT ELEVATIONS AND CONTOURS OUTSIDE THE PAVEMENT ARE TO TOP OF FINISHED GRADE.
- PROPOSED CONTOURS ARE APPROXIMATE. PROPOSED SPOT ELEVATIONS AND DESIGNATED GRADIENT ARE TO BE USED IN CASE OF DISCREPANCY.
- ALL FINISHED GRADES SHALL TRANSITION UNIFORMLY BETWEEN THE FINISHED ELEVATIONS SHOWN.
- CONTOURS AND SPOT GRADES SHOWN ARE ELEVATIONS OF TOP OF THE FINISHED SURFACE. WHEN PERFORMING THE GRADING OPERATIONS, THE CONTRACTOR SHALL PROVIDE AN APPROPRIATE ELEVATION HOLD-DOWN ALLOWANCE FOR THE THICKNESS OF PAVEMENT, SIDEWALK, TOPSOIL, MULCH, STONE, LANDSCAPING, RIP-RAP AND ALL OTHER SURFACE MATERIALS THAT WILL CONTRIBUTE TO THE TOP OF FINISHED GRADE. FOR EXAMPLE, THE CONTRACTOR SHALL DELIVER ELEVATION OF ALL EXPOSED EXCAVATION MATERIALS IN A MANNER THAT ADHERES TO LOCAL, STATE AND FEDERAL LAWS AND REGULATIONS. THE CONTRACTOR SHALL KEEP A RECORD OF WHERE EXCESS EXCAVATION WAS DISPOSED, ALONG WITH THE RECEIVING LANDOWNER'S APPROVAL TO DO SO.
- CONTRACTOR IS RESPONSIBLE FOR REMOVAL AND REPLACEMENT OF TOPSOIL AT THE COMPLETION OF FINE GRADING. CONTRACTOR SHALL REFER TO LANDSCAPE ARCHITECTURE PLANS FOR SPECIFICATIONS AND REQUIREMENTS FOR TOPSOIL.
- CONTRACTOR SHALL MAINTAIN ADEQUATE SITE DRAINAGE DURING ALL PHASES OF CONSTRUCTION, INCLUDING MAINTAINING EXISTING DITCHES OR CULVERTS FREE OF OBSTRUCTIONS AT ALL TIMES.
- NO EARTHWORK FILL SHALL BE PLACED IN ANY EXISTING DRAINAGE WAY, SWALE, CHANNEL, DITCH, CREEK, OR FLOODPLAIN FOR ANY REASON OR ANY LENGTH OF TIME, UNLESS THESE PLANS SPECIFICALLY INDICATE THIS IS REQUIRED.
- TEMPORARY CULVERTS MAY BE REQUIRED IN SOME LOCATIONS TO CONVEY RUN-OFF.
- REFER TO DIMENSION CONTROL PLAN, AND PLAT FOR HORIZONTAL DIMENSIONS.
- THE CONTRACTOR SHALL CLEAR AND GRUB THE SITE AND PLACE, COMPACT, AND CONDITION FILL PER THE PROJECT GEOTECHNICAL ENGINEER'S SPECIFICATIONS. THE FILL MATERIAL PER TO BE USED SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER PRIOR TO PLACEMENT.
- CONTRACTOR IS RESPONSIBLE FOR ALL SOILS TESTING AND CERTIFICATION, UNLESS SPECIFIED OTHERWISE BY OWNER. ALL SOILS TESTING SHALL BE COORDINATED WITH THE APPROPRIATE CITY INSPECTOR AND SHALL COMPLY WITH CITY STANDARD SPECIFICATIONS AND THE GEOTECHNICAL REPORT. SOILS TESTING SHALL BE PERFORMED BY AN APPROVED INDEPENDENT AGENCY FOR TESTING SOILS. THE OWNER SHALL APPROVE THE AGENCY NOMINATED BY THE CONTRACTOR FOR SOILS TESTING.
- ALL COPIES OF SOILS TEST RESULTS SHALL BE SENT TO THE OWNER, ENGINEER AND ARCHITECT DIRECTLY FROM THE TESTING AGENCY.
- IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO SHOW, BY THE STANDARD TESTING PROCEDURES OF THE SOILS, THAT THE WORK CONSTRUCTED MEETS THE PROJECT REQUIREMENTS AND CITY SPECIFICATIONS.
- THE SCOPE OF WORK FOR CIVIL IMPROVEMENT SHOWN ON THESE PLANS TERMINATES 5-FEET FROM THE BUILDING. CONTRACTOR SHALL REFER TO THE GEOTECHNICAL REPORT AND STRUCTURAL PLANS AND SPECIFICATIONS FILL, CONDITIONING, AND PREPARATION IN THE BUILDING PAD.
- DUE TO THE POTENTIAL FOR DIFFERENTIAL SOIL MOVEMENT ADJACENT TO THE BUILDING, THE CONTRACTOR SHALL ADHERE TO GEOTECHNICAL REPORT'S RECOMMENDATION FOR SUBGRADE PREPARATION SPECIFIC TO FLATWORK ADJACENT TO THE PROPOSED BUILDING. THE OWNER AND CONTRACTOR ARE ADVISED TO OBTAIN A GEOTECHNICAL ENGINEER RECOMMENDATION SPECIFIC TO FLATWORK ADJACENT TO THE BUILDING, IF NONE IS CURRENTLY EXISTING.
- CONTRACTOR SHALL ENSURE THAT SUFFICIENT POSITIVE SLOPE AWAY FROM THE BUILDING PAD IS ACHIEVED FOR ENTIRE PERIMETER OF THE PROPOSED BUILDING(S) DURING GRADING OPERATIONS AND IN THE FINAL CONDITION. IF THE CONTRACTOR OBSERVES THAT THIS WILL NOT BE ACHIEVED, THE CONTRACTOR SHALL CONTACT THE ENGINEER TO REVIEW THE LOCATION.
- THE CONTRACTOR SHALL TAKE ALL AVAILABLE PRECAUTIONS TO CONTROL DUST. CONTRACTOR SHALL CONTROL DUST BY SPRINKLING WATER, OR BY OTHER MEANS APPROVED BY THE CITY, AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANIES FOR ANY REQUIRED UTILITY ADJUSTMENTS AND/OR RELOCATIONS NEEDED FOR GRADING OPERATIONS AND TO ACCOMMODATE PROPOSED GRADE, INCLUDING THE UNKNOWN UTILITIES NOT SHOWN ON THESE PLANS. CONTRACTOR SHALL REFER TO THE GENERAL NOTES "OVERALL" SECTION THESE PLANS FOR ADDITIONAL INFORMATION.
- EXISTING TREE LOCATIONS SHOWN ON THESE PLANS ARE APPROXIMATE. CONTRACTOR SHALL REPORT ANY DISCREPANCIES FOUND IN THE FIELD THAT AFFECT THE GRADING PLAN TO THE CIVIL ENGINEER.
- CONTRACTOR SHALL FIELD VERIFY ALL PROTECTED TREE LOCATIONS, INDIVIDUAL PROTECTED TREE CRITICAL ROOT ZONES, AND PROPOSED SITE GRADING, AND NOTIFY THE CIVIL ENGINEER AND LANDSCAPE ARCHITECT OF ANY CONFLICTS WITH THE TREE PRESERVATION PLAN BY THE LANDSCAPE ARCHITECT PRIOR TO COMMENCING THE WORK.
- TREE PROTECTION MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY STANDARD TREE PROTECTION DETAILS AND THE APPROVED TREE PRESERVATION PLANS BY THE LANDSCAPE ARCHITECT.
- CONTRACTOR SHALL REFER TO THE LANDSCAPING AND TREE PRESERVATIONS PLANS FOR ALL INFORMATION AND DETAILS REGARDING EXISTING TREES TO BE REMOVED AND PRESERVED.
- NO TREE SHALL BE REMOVED UNLESS A TREE REMOVAL PERMIT HAS BEEN ISSUED BY THE CITY, OR CITY HAS OTHERWISE CONFIRMED IN WRITING THAT ONE IS NOT NEEDED FOR THE TREE(S).
- NO TREE SHALL BE REMOVED OR DAMAGED WITHOUT PRIOR AUTHORIZATION OF THE OWNER OR OWNER'S REPRESENTATIVE. EXISTING TREES SHALL BE PRESERVED WHENEVER POSSIBLE AND GRADING IMPACT TO THEM HELD TO A MINIMUM.
- AFTER PLACEMENT OF SUBGRADE AND PRIOR TO PLACEMENT OF PAVEMENT, CONTRACTOR SHALL TEST AND OBSERVE PAVEMENT AREAS FOR EVIDENCE OF PONDING AND INADEQUATE SLOPE FOR DRAINAGE. ALL AREAS SHALL ADEQUATELY DRAIN TOWARDS THE INTENDED STRUCTURE TO CONVEY STORMWATER RUNOFF. CONTRACTOR SHALL IMMEDIATELY NOTIFY OWNER AND ENGINEER IF ANY AREAS OF POOR DRAINAGE ARE DISCOVERED.
- CONTRACTOR FIELD ADJUSTMENT OF PROPOSED SPOT GRADES IS ALLOWED, IF THE APPROVAL OF THE CIVIL ENGINEER IS OBTAINED.

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HYDROLOGY SUBMITTAL

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Sheet Title:

GENERAL NOTES

Date: 08.09.2022

Project Number: 069313444

Drawn By: LW/NW

Sheet Number:

C2.0



KIMLEY HORN GENERAL PAVING NOTES:

- ALL PAVING MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH THESE PLANS, THE CITY STANDARD DETAILS AND SPECIFICATIONS, THE FINAL GEOTECHNICAL REPORT AND ALL ISSUED ADDENDA, AND COMMONLY ACCEPTE CONSTRUCTION STANDARDS. THE CITY SPECIFICATIONS SHALL GOVERN WHERE OTHER SPECIFICATIONS DO NOT EXIST. IN CASE OF CONFLICTING SPECIFICATIONS OR DETAILS, THE MORE RESTRICTIVE SPECIFICATION/DETAIL SHALL BE FOLLOWED.
- ALL PRIVATE ON-SITE PAVING AND PAVING SUBGRADE SHALL COMPLY WITH THE PROJECT'S FINAL GEOTECHNICAL REPORT (OR LATEST EDITION), INCLUDING ALL ADDENDA.
- ALL FIRELANE PAVING AND PAVING SUBGRADE SHALL COMPLY WITH CITY STANDARDS AND DETAILS. IF THESE ARE DIFFERENT THAN THOSE IN THE GEOTECHNICAL REPORT, THEN THE MORE RESTRICTIVE SHALL BE FOLLOWED.
- ALL PUBLIC PAVING AND PAVING SUBGRADE SHALL COMPLY WITH CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
- CONTRACTOR IS RESPONSIBLE FOR ALL PAVING AND PAVING SUBGRADE TESTING AND CERTIFICATION, UNLESS SPECIFIED OTHERWISE BY OWNER. ALL PAVING AND PAVING SUBGRADE TESTING SHALL BE COORDINATED WITH THE APPROPRIATE CITY INSPECTOR. TESTING SHALL BE PERFORMED BY AN APPROVED INDEPENDENT AGENCY FOR TESTING PAVING AND SUBGRADE. OWNER SHALL APPROVE THE AGENCY NOMINATED BY THE CONTRACTOR FOR PAVING AND PAVING SUBGRADE TESTING.
- IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO SHOW, BY THE STANDARD TESTING PROCEDURES OF THE PAVING AND PAVING SUBGRADE, THAT THE WORK CONSTRUCTED MEETS THE PROJECT REQUIREMENTS AND CITY SPECIFICATIONS.
- DUE TO THE POTENTIAL FOR DIFFERENTIAL SOIL MOVEMENT ADJACENT TO THE BUILDING, THE CONTRACTOR SHALL ADHERE TO GEOTECHNICAL REPORT'S RECOMMENDATION FOR SUBGRADE PREPARATION SPECIFIC TO FLATWORK ADJACENT TO THE PROPOSED BUILDING. THE OWNER AND CONTRACTOR ARE ADVISED TO OBTAIN A GEOTECHNICAL ENGINEER RECOMMENDATION SPECIFIC TO FLATWORK ADJACENT TO THE BUILDING, IF NONE IS CURRENTLY EXISTING.
- CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAIL AND SPECIFICATIONS.
- PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS.
- ALL ACCESSIBLE RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS, LATEST EDITION.
- ANY COMPONENTS OF THE PROJECT SUBJECT TO RESIDENTIAL USE SHALL ALSO CONFORM TO THE FAIR HOUSING ACT, AND COMPLY WITH THE FAIR HOUSING ACT DESIGN MANUAL BY THE US DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT.
- CONTRACTOR SHALL CONSTRUCT PROPOSED PAVEMENT TO MATCH EXISTING PAVEMENT WITH A SMOOTH, FLUSH, CONNECTION.
- CONTRACTOR SHALL FURNISH AND INSTALL ALL PAVEMENT MARKINGS FOR FIRE LANES, PARKING STALLS, HANDICAPPED PARKING SYMBOLS, AND MISCELLANEOUS STRIPING WITHIN PARKING LOT AND AROUND BUILDING AS SHOWN ON THE PLANS. ALL PAINT AND PAVEMENT MARKINGS SHALL ADHERE TO CITY AND OWNER STANDARDS.
- REFER TO GEOTECHNICAL REPORT FOR PAVING JOINT LAYOUT PLAN REQUIREMENTS FOR PRIVATE PAVEMENT.
- REFER TO CITY STANDARD DETAILS AND SPECIFICATIONS FOR JOINT LAYOUT PLAN REQUIREMENTS FOR PUBLIC PAVEMENT.
- ALL REINFORCING STEEL SHALL CONFORM TO THE GEOTECHNICAL REPORT, CITY STANDARDS, AND ASTM A-615, GRADE 60, AND SHALL BE SUPPORTED BY BAR CHAIRS. CONTRACTOR SHALL USE THE MORE STRINGENT OF THE CITY AND GEOTECHNICAL STANDARDS.
- ALL JOINTS SHALL EXTEND THROUGH THE CURB.
- THE MINIMUM LENGTH OF OFFSET JOINTS AT RADIUS POINTS SHALL BE 2 FEET.
- CONTRACTOR SHALL SUBMIT A JOINTING PLAN TO THE ENGINEER AND OWNER PRIOR TO BEGINNING ANY OF THE PAVING WORK.
- ALL SAWCUTS SHALL BE FULL DEPTH FOR PAVEMENT REMOVAL AND CONNECTION TO EXISTING PAVEMENT.
- FIRE LANES SHALL BE MARKED AND LABELED AS A FIRELANE PER CITY STANDARDS.
- UNLESS THE PLANS SPECIFICALLY DICTATE TO THE CONTRARY, ON-SITE AND OTHER DIRECTIONAL SIGNS SHALL BE ORIENTED SO THEY ARE READILY VISIBLE TO THE ONCOMING TRAFFIC FOR WHICH THEY ARE INTENDED.
- CONTRACTOR IS RESPONSIBLE FOR INSTALLING NECESSARY CONDUIT FOR LIGHTING, IRRIGATION, ETC. PRIOR TO PLACEMENT OF PAVEMENT. ALL CONSTRUCTION DOCUMENTS (CIVIL, MEP, LANDSCAPE, IRRIGATION, AND ARCHITECT) SHALL BE CONSULTED.
- BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA, AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.

CITY OF ALBUQUERQUE NOTES

- CITY OF ALBUQUERQUE PUBLIC WORKS IS NOT PART OF 811. CONTRACTOR SHOULD CONTACT PUBLIC WORKS AT 505-924-3400, 24 HOURS BEFORE ANY CONSTRUCTION, BORING, OR UTILITY CONNECTION. PUBLIC WORK WILL MARK EXISTING UTILITIES BUT THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND FIELD VERIFYING ALL UTILITIES AND BE RESPONSIBLE TO REPAIR OR REPLACE ANY DAMAGES TO COB ASSETS OR UTILITIES CONFLICT.
- ALL APPLICATIONS MUST BE SUBMITTED AT LEAST 14 CALENDAR DAYS PRIOR TO YOUR ANTICIPATED START DATE.
- CONSTRUCTION SERVICES DIVISION CAN BE REACHED USING THE FOLLOWING CONTACT: (505) 924-3400
- PUBLIC UTILITY OWNERS (WATER, GAS, ELECTRIC, SEWER, COMMUNICATIONS) ARE ALLOWED TO RESOLVE EMERGENCIES WITHOUT AN APPROVED PERMIT, ONLY IF CONSTRUCTION COORDINATION IS NOTIFIED WITHIN 24 HOURS.
- A MUTCD-COMPLIANT TRAFFIC CONTROL SETUP IS REQUIRED FOR ALL ACTIVITIES, REGARDLESS OF IF SAID WORK IS SHORT-DURATION, EMERGENCY, OR SCHEDULED.
- ALL TRAFFIC CONTROL PLANS (TPCS) MUST BE DRAWN BY A TRAFFIC CONTROL SUPERVISOR, DESIGN SPECIALIST, OR PROFESSIONAL ENGINEER.
- ALL HIGH-SPEED ROADWAYS MUST HAVE A TRAFFIC CONTROL PLAN SUBMITTED WITH THE PLAN APPLICATION.
- LOW-SPEED ROADS DO NOT REQUIRE A TRAFFIC CONTROL PLAN SUBMITTED.
- SIDEWALKS AND/OR MULTI-USE PATHS (BIKE TRAILS, ECT.) DO NOT REQUIRE A TRAFFIC CONTROL PLAN SUBMITTED IF SAID PATHS ARE THE SOLITARY RIGHT-OF-WAY BEING AFFECTED (NO ACCOMPANYING ROAD CLOSURES).
- IF ACTIVITY IS BEYOND THE PROPERTY LINE, THERE IS NO NEED TO OBTAIN A RIGHT-OF-WAY PERMIT. IT MAY BE REQUIRED TO APPLY WITH ANOTHER CITY DEPARTMENT (PLANNING)
- THE ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES UNLESS A TRAFFIC CONTROL PLAN (TCP) IS SUBMITTED AND APPROVED FOR LANE CLOSURES. ACCESS TO ALL DRIVES AND SIDE ROADS, BOTH PUBLIC AND PRIVATE, ARE TO BE MAINTAINED AS MUCH AS POSSIBLE.
- CONTRACTOR SHALL CONTACT ENGINEERING DEPARTMENT AT 505-924-3400 FOR ENGINEERING INSPECTION A MINIMUM OF 48 HOURS PRIOR TO ANY CONSTRUCTION IN PUBLIC RIGHT-OF-WAY, DRIVEWAY APPROACH, SIDEWALK, DETENTION, AND UTILITY CONNECTION (WATER, SANITARY SEWER, AND STORM DRAINAGE).
- DRAINAGE FACILITIES ARE TO BE OWNED, OPERATED, AND MAINTAINED BY THE OWNERS AND NOT THE CITY OF ALBUQUERQUE.
- THE CONTRACTOR WILL BE RESPONSIBLE TO REPAIR/REPLACE ANY DAMAGES TO THE CITY OF ALBUQUERQUE'S ASSETS OR UTILITIES CONFLICT.
- CONTRACTOR SHALL TAKE PROPER MEASURES TO SECURE ANY OPEN TRENCHES, BORE PITS, & EXCAVATION TO ENSURE PUBLIC SAFETY. DURING NON-WORKING HOURS ALL TRENCH SHALL BE BACKFILLED (NO TRENCH LEFT OPEN OVERNIGHT) OR COVER PROPERLY.
- ALL EXCESS EXCAVATION TO BE REMOVED FROM THE ROAD RIGHT-OF-WAY AT THE END OF EACH DAY.
- IF IT BECOMES NECESSARY FOR EQUIPMENT TO OPERATE ON A PORTION OF THE PAVEMENT, PRECAUTIONS MUST BE TAKEN TO PREVENT FROM ANY DAMAGE WHATSOEVER TO THE PAVEMENT.
- ANY PAVEMENT, SIDEWALK, CURB AND RAMP DISTURBED DURING CONSTRUCTION IS THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND/OR REPLACE TO ORIGINAL OR BETTER CONDITION AND BE MATCHED WITH EXISTING GRADE.
- THE DEVELOPMENT HAS BEEN DESIGNED TO NOT IMPEDE, IMPOUND, OR BLOCK THE NATURAL FLOW OF WATER ACROSS ADJACENT AND CONTIGUOUS PROPERTIES.
- NEEDS TO BE A 2' MINIMUM VERTICAL CLEARANCE FOR THE WATER LINE CROSSING AND SANITARY SEWER CROSSING IS REQUIRED PER ABCWUA REQUIREMENTS.
- ALL WATERLINE TEES, BENDS, REDUCERS, VALVES, AND ENDS SHALL BE MECHANICALLY RESTRAINED AND BLOCKED. MECHANICAL RESTRAINT SHALL BE BY EBAA MEGALUG FOR PVC PIPE OR APPROVED EQUAL.
- CONTRACTOR SHALL MAINTAIN ADEQUATE DRAINAGE AT ALL TIMES AND TO ENSURE ALL DRAINAGE DITCHES REMAIN OPEN AND CLEAR AT ALL TIMES.
- ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE AASHTO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION.
- UNLESS THE TRAFFIC CONTROL DEVICES ARE PROTECTING AN ACTIVE CONSTRUCTION SITE, ALL NONESSENTIAL TRAFFIC CONTROL DEVICES SHALL BE REMOVED FROM ROAD VISIBILITY IN ORDER TO PREVENT CONFUSION FOR THE ROAD USER.
- ALL EXISTING DRIVEWAYS WITHIN THE TRAFFIC CONTROL ZONE SHALL REMAIN OPEN AT ALL TIMES UNLESS AGREED UPON BY THE OWNER OF THE DRIVEWAY. ALL AGREEMENTS FOR OFF-SITE DRIVEWAY CLOSURES SHALL BE INCLUDED WITH THE TRAFFIC CONTROL PLAN SUBMITTAL.
- NO WORK WITHIN THE CITY OF ALBUQUERQUE ROW MAY OCCUR WITHOUT THE FULL IMPLEMENTATION OF THE APPROVED TRAFFIC CONTROL PLAN INTENDED TO PROTECT THAT WORK.
- NO WORK WITHIN THE CITY OF ALBUQUERQUE ROW THAT REQUIRES TRENCHING, EXCAVATION OR DIGGING MAY BE LEFT UNPROTECTED WHEN WORKERS ARE NOT PRESENT. CONTACT CITY OF ALBUQUERQUE TRAFFIC ENGINEER FOR ACCEPTABLE METHODS OF WORK PROTECTION.
- NO STAGING OF EQUIPMENT, WORK, MATERIALS OR WORKERS MAY TAKE PLACE ON A CITY OF ALBUQUERQUE ROADWAY WITHOUT APPROPRIATE TRAFFIC CONTROL IN PLACE.

LEGEND

	PROPERTY LINE
	RIGHT OF WAY LINE
	STREET CENTERLINE
	EXISTING EASEMENT
	EXISTING WATER MAIN
	EXISTING SEWER MAIN
	EXISTING ELECTRICAL LINE
	EXISTING OVERHEAD POWER
	EXISTING GAS LINE
	EXISTING STORM DRAIN
	EXISTING CURB
	PROPOSED EASEMENT
	PROPOSED WATER MAIN
	PROPOSED SEWER MAIN
	PROPOSED FIRE LINE
	PROPOSED ELECTRICAL LINE
	PROPOSED GAS LINE
	PROPOSED STORM DRAIN
	PROPOSED CONCRETE CURB
	SILT FENCE
	EXISTING SEWER MANHOLE
	EXISTING FIRE HYDRANT
	PROPOSED FDC
	PROPOSED STORM INLET
	PROPOSED PARKING COUNT
	PROPOSED STORM DRAIN MANHOLE
	ACCESSIBLE PARKING SYMBOL
	EXISTING STREET LIGHT
	PROPOSED HIGH POINT
	PROPOSED GRADE BREAK
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED FLOW ARROW WITH SLOPE
	PROPOSED SPOT ELEVATION

ABBREVIATIONS

ABC	AGGREGATE BASE COURSE	MH	MANHOLE
AC	ACRES/ASPHALT CONCRETE	MIN	MINIMUM
ADA	AMERICANS WITH DISABILITIES ACT	N	NORTH
		NTS	NOT TO SCALE
ARCH	ARCHITECT/ARCHITECTURAL	N.P.D.E.S.	NATIONAL POLLUTION
B.C.	BRASS CAP		DISCHARGE ELIMINATION
BC	BACK OF CURB		SYSTEM
BCR	BEGINNING OF CURB RETURN	P	PAVEMENT ELEVATION
BK	BOOK	PC	POINT OF CURVATURE
BLDG	BUILDING	PCC	POINT OF COMPOUND
CF	CUBIC FEET		CURVATURE
CFS	CUBIC FEET PER SECOND	P.E.	PROFESSIONAL ENGINEER
CL	CENTERLINE	PCCP	PORTLAND CEMENT
C, CONC	CONCRETE		CONCRETE
CMP	CORRUGATED METAL PIPE	PG	PAGE
COR	CORNER	PT	POINT OF TANGENCY
CY	CUBIC YARDS	PUE	PUBLIC UTILITY EASEMENT
DET	DETAIL	PVC	POLYVINYL CHLORIDE
E	EAST/ELECTRIC	R	RADIUS/RANGE
ECR	END OF CURB RETURN	R/W	RIGHT-OF-WAY
ELEV	ELEVATION	REF	REFERENCE
ESMT	EASEMENT	REV	REVISION
EXIST, EX.	EXISTING	S	SOUTH
F/C	FACE OF CURB	SCH	SCHEDULE
FF	FINISHED FLOOR ELEVATION	SEC	SECTION
FL	FLOWLINE	SES	SERVICE ENTRY SECTION
FT	FEET	STBK	SETBACK
GB	GRADE BREAK	SF	SQUARE FEET
HDPE	HIGH DENSITY POLYETHYLENE	SPEC	SPECIFICATION
		SS	SANITARY SEWER
HP	HIGH POINT	ST	STREET
HR	HOURL	STD	STANDARD
IBC	INTERNATIONAL BUILDING CODE	SW	TOP OF SIDEWALK
		SY	SQUARE YARDS
IFC	INTERNATIONAL FIRE CODE	T	TELEPHONE/
INV	INVERT		TOWNSHIP/
IRR	IRRIGATION		TANGENT
L	LENGTH	TC	TOP OF CURB
LF	LINEAR FEET	TYP	TYPICAL
MAX	MAXIMUM	W	WEST/WATER/WIDTH
MEP	MECHANICAL/ELECTRICAL/ PLUMBING	W/	WITH

GEOTECHNICAL REPORT REPORT NO. 66215082

TERRACON CONSULTANTS, INC.  
ALBUQUERQUE, NM  
MAY 12, 2021

INTERIM CONDITIONS  
DEMOLITION AND STABILIZATION  
2004 WYOMING BLVD NE  
BERNALILLO COUNTY, NEW MEXICO 87112

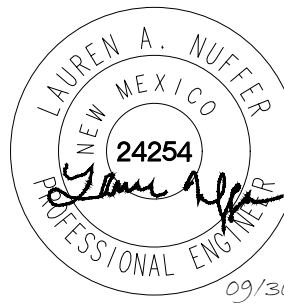
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GENERAL NOTES

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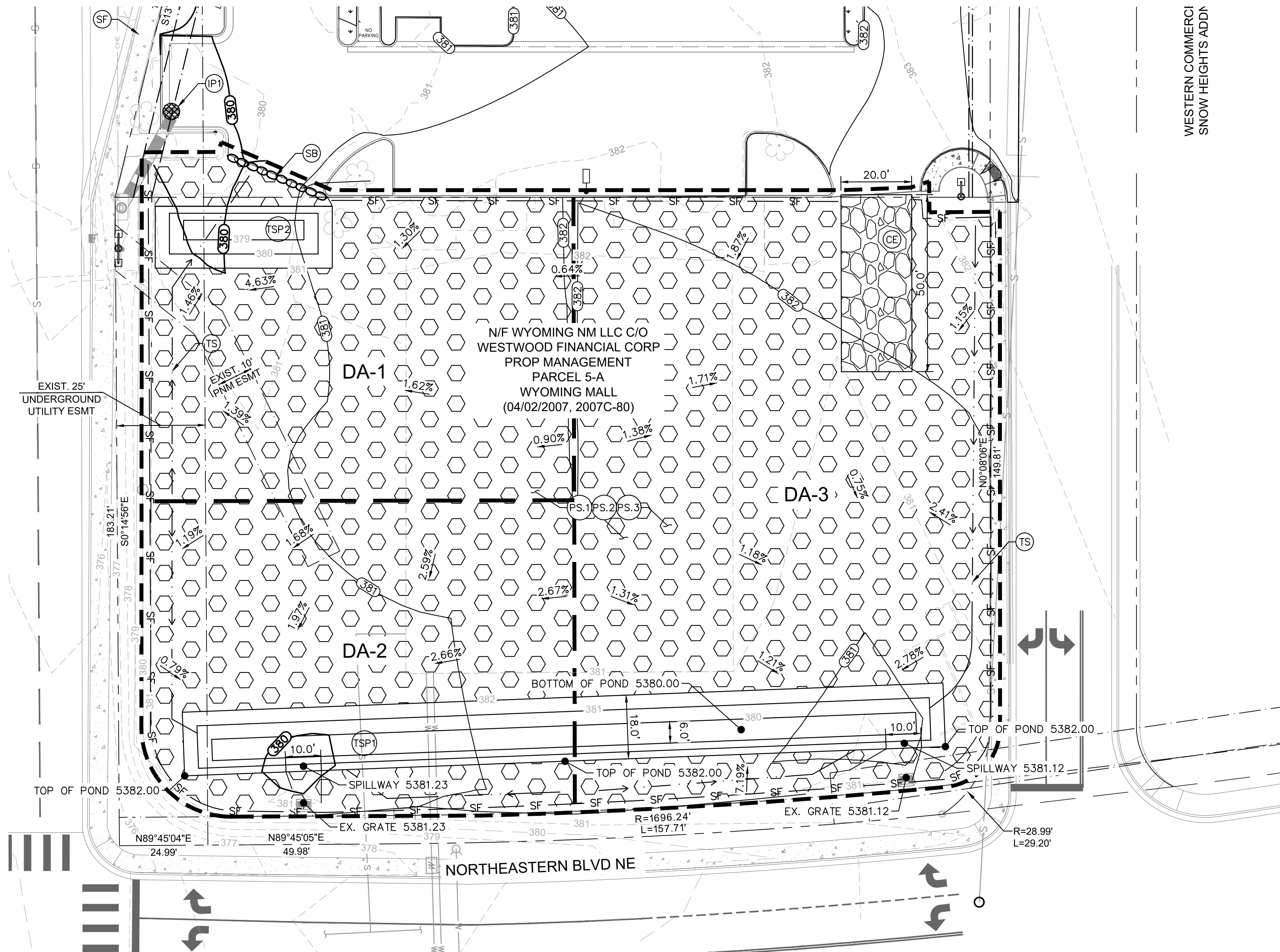
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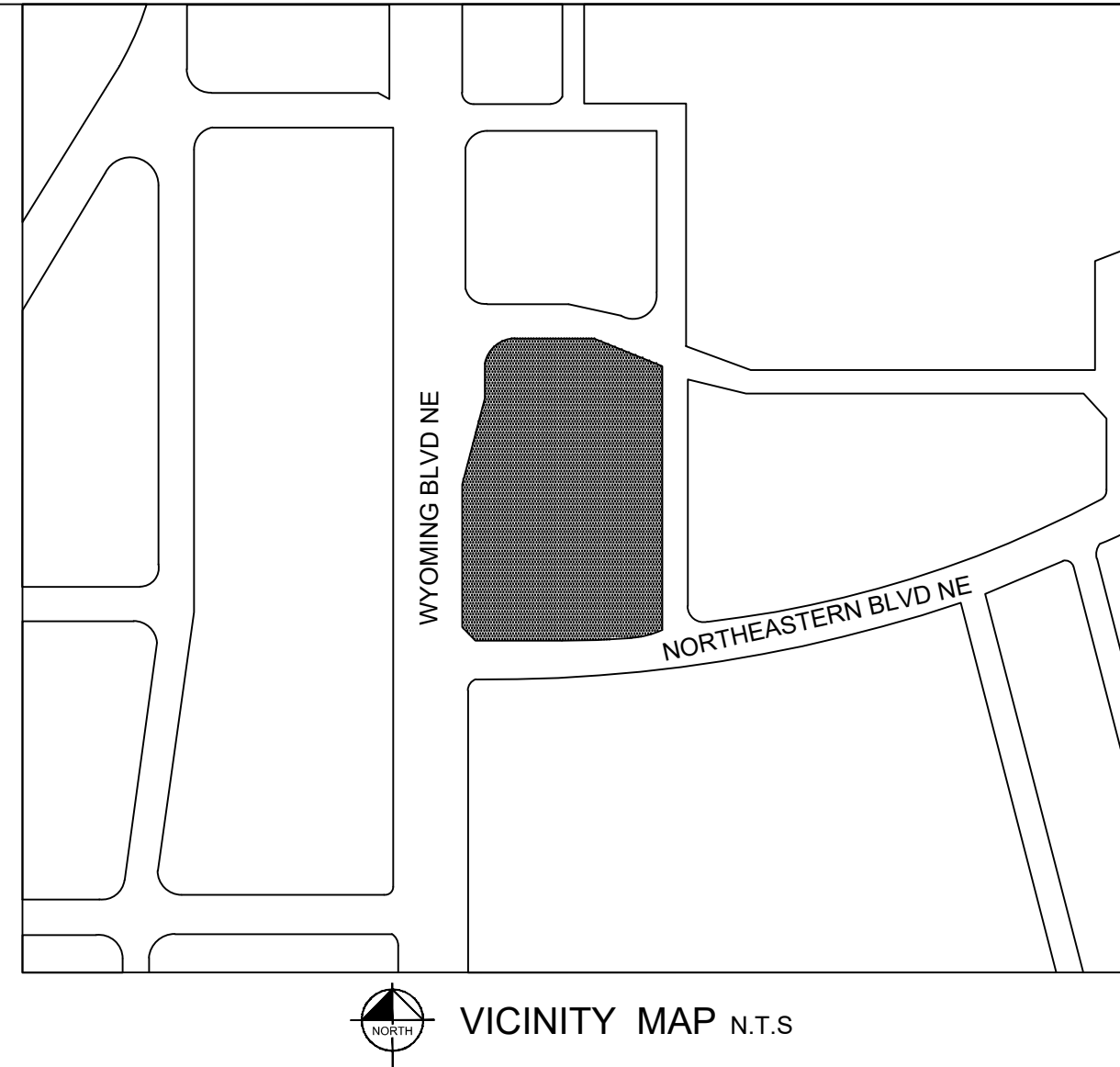
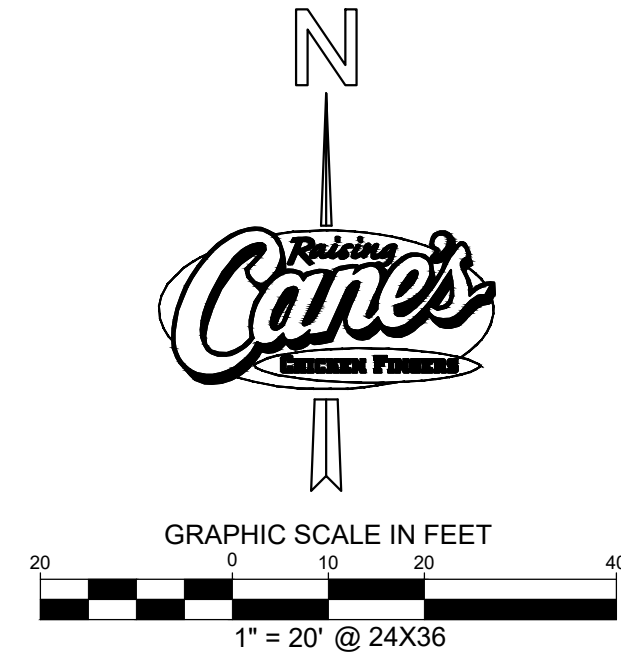
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WESTERN COMMERCIAL  
SNOW HEIGHTS ADDN



#### LEGEND

- PROPERTY LINE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED LIMITS OF DISTURBANCE
- PROPOSED RIDGE
- DIRECTION OF OVERLAND FLOW W/ GRADE
- SILT FENCE
- STRAW BALE
- TEMPORARY DRAINAGE SWALE
- OFFSITE TRACKING PREVENTION
- PROPOSED ROCK. CONTRACTOR TO INSTALL PS.1, PS.2, OR PS.3, TO BE APPROVED BY OWNER.
- PROPOSED SEEDING WITH TEMPORARY IRRIGATION PER ALBO SPEC 1012.2.3. CONTRACTOR TO INSTALL PS.1, PS.2, OR PS.3, TO BE APPROVED BY OWNER.
- PROPOSED HAY AND STRAW MULCH PER ALBO SPEC 1012.2.3.2. CONTRACTOR TO INSTALL PS.1, PS.2, OR PS.3, TO BE APPROVED BY OWNER.
- PROPOSED GRATE INLET PROTECTION. SEE DETAIL SHEET.
- TEMPORARY SILT POND TOTAL DEPTH  $\pm 2'$ , SILT COLLECTION DEPTH  $\pm 0.5'$  NORTH SIDE SLOPE = 4:1 SOUTH SIDE SLOPE = 2:1
- TEMPORARY SILT POND TOTAL DEPTH  $\pm 2'$ , SILT COLLECTION DEPTH  $\pm 0.5'$  NORTH SIDE SLOPE = 2:1 SOUTH SIDE SLOPE = 4:1

#### GENERAL NOTES

- ADD 5000' TO ALL ELEVATIONS SHOWN.
- CONTRACTOR TO VERIFY EXISTING IMPROVEMENTS SHOWN ON THE PLAN.
- CONTRACTOR TO PROTECT IN PLACE, DURING DEMOLITION AND CONSTRUCTION, ALL EXISTING IMPROVEMENTS THAT ARE TO REMAIN AS NOTED ON THE PLAN.
- ANY EXISTING STRUCTURE, IMPROVEMENT OR APPURTENANCE TO REMAIN THAT IS DAMAGED DURING DEMOLITION OR CONSTRUCTION SHALL BE IMMEDIATELY REPAIRED OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR SHALL VERIFY AND LOCATE ALL EXISTING UNDERGROUND UTILITIES BEFORE CONSTRUCTION AND PROTECT IN PLACE UNLESS NOTES ON THE PLANS.
- THE CONTRACTOR SHALL EXERCISE CAUTION TECHNIQUES TO PROTECT AND PRESERVE EXISTING PERMANENT SURVEY MONUMENTS. ALL SURVEY MONUMENTS DISTURBED (PROPOSED AND EXISTING) SHALL BE REPLACED AND/OR REHABILITATED IN ACCORDANCE WITH STANDARDS BY A PROFESSIONAL LAND SURVEYOR RETAINED BY THE CONTRACTOR.

#### EROSION CONTROL SCHEDULE AND PHASING

THE PROJECT SHALL GENERALLY CONFORM TO THE FOLLOWING PHASING SCHEDULE. REFERENCE THE SWPPP BOOK AND NMED GENERAL PERMIT FOR DETAILED REQUIREMENTS.

##### PHASE 1 - DEMOLITION

- INSTALL PERIMETER BMPs INCLUDING THE CONSTRUCTION ENTRANCE/EXIT, SWPPP SIGNAGE, SILT FENCE, AND ALL OTHER NECESSARY BMPs ACCORDING TO THE LOCATION SHOWN ON THE EROSION CONTROL PLAN. CLEAR ONLY THE MINIMUM AREA REQUIRED TO INSTALL BMPs.
- SET THE PROJECT OFFICE TRAILER AND PREPARE TEMPORARY PARKING AND STORAGE AREAS.
- DENOTE DATES OF BMP INSTALLATION AND MAINTENANCE ON SITE-MAPS.
- BEGIN DEMOLITION AND CLEARING OF THE SITE.
- STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WHENEVER CLEARING, GRADING, OR EARTH DISTURBING ACTIVITIES HAVE CEASED ON ANY PORTION OF THE SITE, OR TEMPORARILY CEASED AND WILL NOT RESUME WITHIN 14 DAYS PER GENERAL PERMIT REQUIREMENTS.

##### PHASE 2 - GRADING

- ENSURE APPROPRIATE BMPs ARE IN PLACE DOWNSTREAM OF SITE WORK OR WHERE RUNOFF MAY EXIT THE SITE.
  - BEGIN GRADING THE SITE.
  - SEED AND RE-VEGETATE SLOPES AS AREAS ARE BROUGHT TO GRADE OR STOCKPILES THAT WILL REMAIN INACTIVE FOR 14 DAYS PER GENERAL PERMIT REQUIREMENTS.
- PHASE 3 - LANDSCAPING AND DEVELOPMENT
- INSTALL LANDSCAPING PER THE LANDSCAPE PLANS AND DETAILS.
  - REMOVE EROSION CONTROL DEVICES WHEN FINAL STABILIZATION IS ACHIEVED PER THE CITY OF ALBUQUERQUE.
  - STABILIZE ANY AREAS DISTURBED BY REMOVAL OF BMPs.

#### SITE DATA

LOT AREA	44,381 SF $\pm$ 1.02 AC
TOTAL DISTURBED AREA	40,060 SF $\pm$ 0.92 AC
PRE-DEVELOPMENT RUNOFF COEFFICIENT	0.91



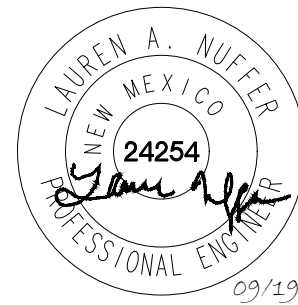
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#### HYDROLOGY SUBMITTAL

REVISIONS:

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Sheet Title:

**EROSION CONTROL  
PLAN**

Date: 08.09.2022

Project Number: 069313444

Drawn By: LW/NW

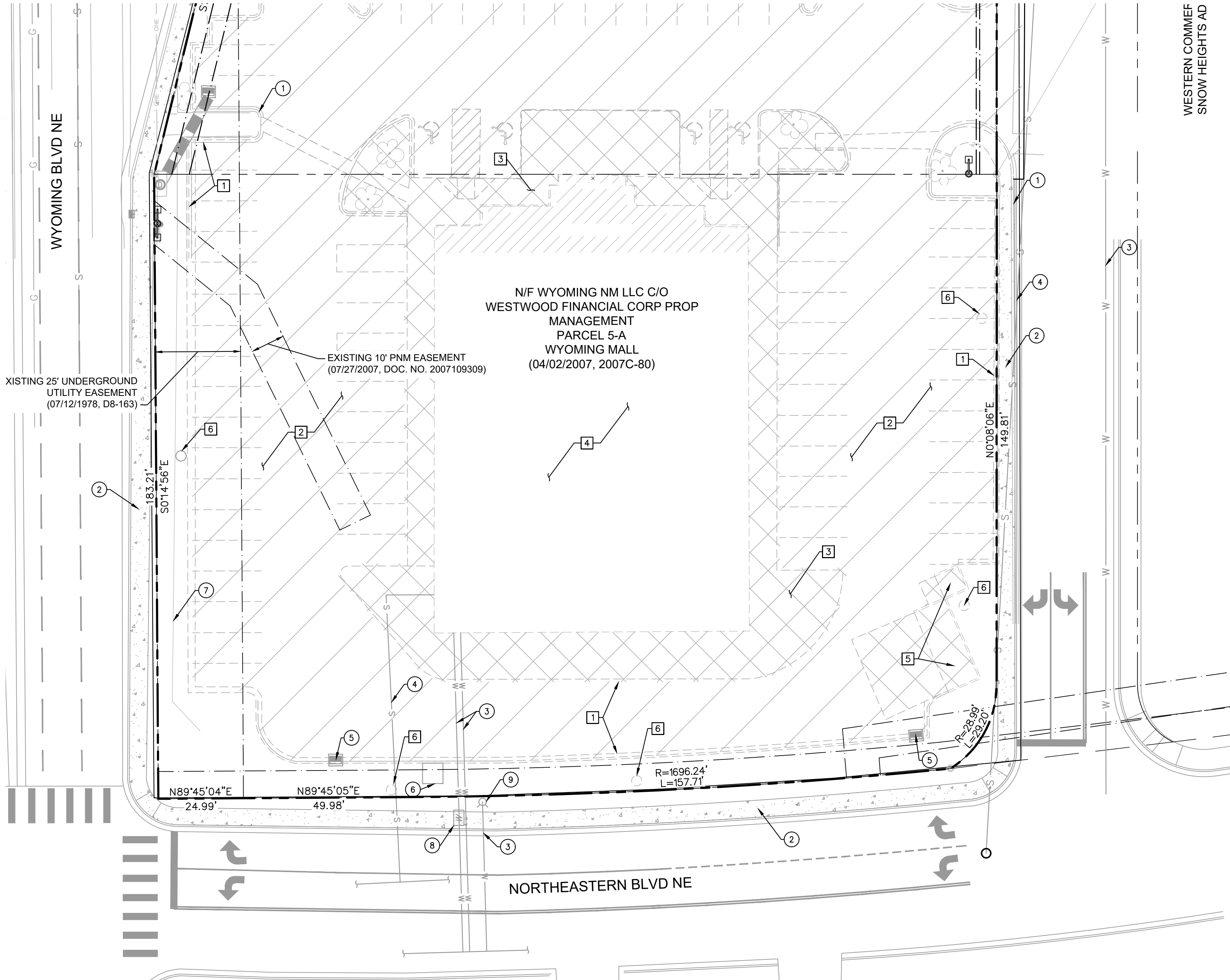
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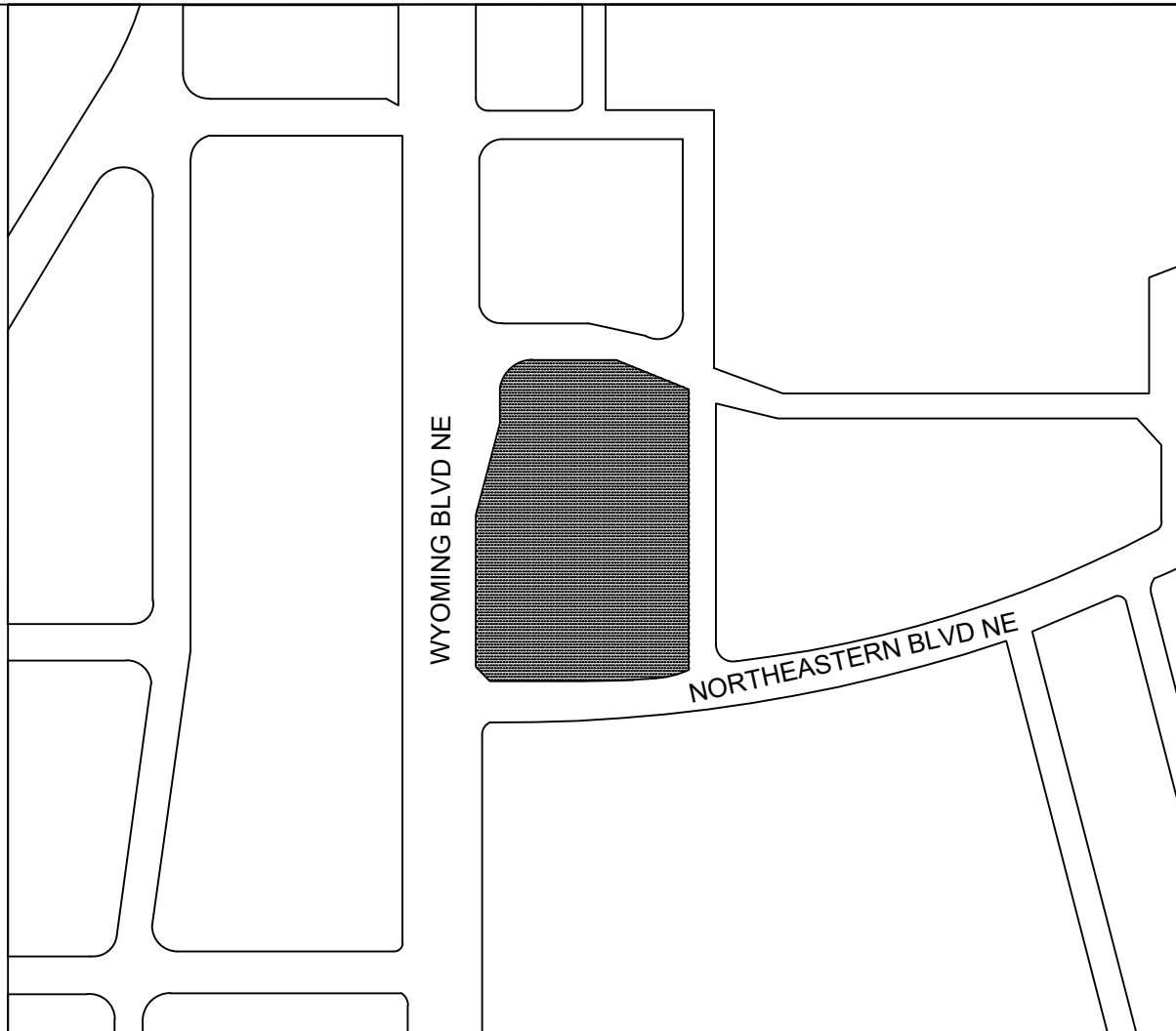
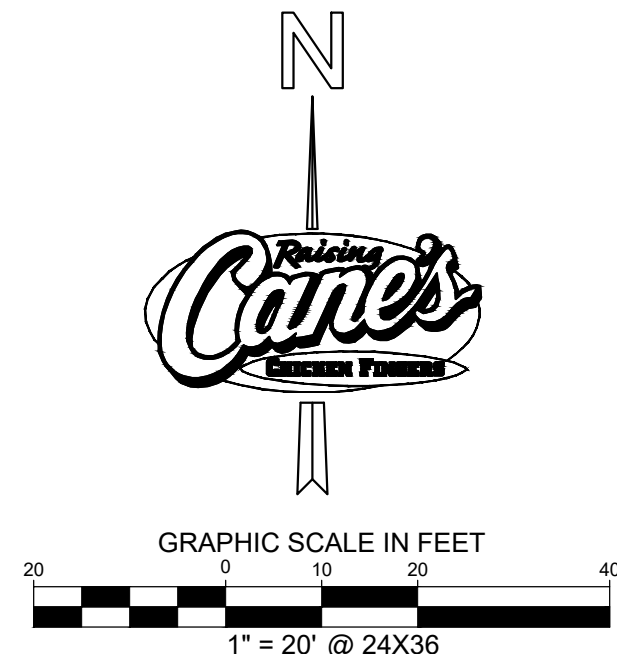








WESTERN COMMER  
SNOW HEIGHTS AD



VICINITY MAP N.T.S.

#### DEMOLITION LEGEND

- PROPERTY LINE
- REMOVE EXISTING CONCRETE
- REMOVE EXISTING ASPHALT PAVEMENT
- PROPOSED FULL DEPTH SAWCUT
- EXISTING CURB TO BE REMOVED
- EXISTING CURB TO REMAIN
- EXISTING SIDEWALK TO REMAIN
- EXISTING LIGHT POLE TO BE REMOVED

#### DEMOLITION LEGEND

- 1 REMOVE EXISTING CURB AND GUTTER.
- 2 REMOVE EXISTING ASPHALT PAVEMENT AND AGGREGATE BASE COURSE.
- 3 REMOVE EXISTING CONCRETE SIDEWALK AND AGGREGATE BASE COURSE.
- 4 REMOVE EXISTING BUILDING.
- 5 REMOVE EXISTING TRASH ENCLOSURE.
- 6 REMOVE EXISTING LIGHT POLE AND FOUNDATION.

#### PROTECTION NOTES

- 1 PROTECT IN PLACE EXISTING CURB.
- 2 PROTECT IN PLACE EXISTING SIDEWALK.
- 3 PROTECT IN PLACE EXISTING WATER LINE.
- 4 PROTECT IN PLACE EXISTING SEWER LINE.
- 5 PROTECT IN PLACE EXISTING STORM INLETS.
- 6 PROTECT IN PLACE EXISTING TRAFFIC CONTROL BOX.
- 7 PROTECT IN PLACE EXISTING SCREEN WALL.
- 8 PROTECT IN PLACE EXISTING WATER METER BOX.
- 9 PROTECT IN PLACE FH FROM RECORD DRAWINGS. CONTRACTOR TO FIELD LOCATE AND COORDINATE WITH ENGINEER OF RECORD IF ANY DISCREPANCIES.

#### DEMOLITION NOTES

- INSTALL ALL EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS REQUIRED TO CONTROL SEDIMENT FROM LEAVING THE SITE PRIOR TO COMMENCING DEMOLITION WORK ON SITE.
- THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSAL (IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES) ALL STRUCTURES, PADS, WALLS, FLUMES, FOUNDATIONS, PARKING, DRIVES, DRAINAGE STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE PROPOSED PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL PER THE SPECIFICATIONS. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
- THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
- THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY THEMSELVES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
- THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR MARKING ONSITE LOCATIONS OF EXISTING UTILITIES.
- ALL EXISTING SEWERS, PIPING AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE PROCEEDING WITH THE WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE GROUDED IF UNDER BUILDING.
- ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE. CONTRACTOR SHALL PAY CLOSE ATTENTION TO EXISTING UTILITIES WITHIN THE ANY ROAD RIGHT OF WAY DURING CONSTRUCTION.
- CONTRACTOR MUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC., (AND OTHER APPROPRIATE BEST MANAGEMENT PRACTICES) AS APPROVED BY CITY AND OWNER.
- SHOULD REMOVAL AND/OR RELOCATION ACTIVITIES DAMAGE EXISTING FACILITIES TO REMAIN, THE CONTRACTOR SHALL PROVIDE NEW MATERIALS/ STRUCTURES IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. EXCEPT FOR MATERIALS DESIGNED TO BE RELOCATED ON THIS PLAN, ALL OTHER CONSTRUCTION MATERIALS SHALL BE NEW.
- CONTRACTOR SHALL LIMIT SAW-CUT & PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS, BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR.
- THE CONTRACTOR SHALL COORDINATE WATER MAIN WORK WITH THE FIRE DEPT. AND THE CITY/COUNTY UTILITY DEPARTMENT TO PLAN PROPOSED IMPROVEMENTS AND TO ENSURE ADEQUATE FIRE PROTECTION IS CONSTANTLY AVAILABLE TO EXISTING BUILDINGS TO REMAIN AND SITE THROUGHOUT THIS SPECIFIC WORK AND THROUGH ALL PHASES OF CONSTRUCTION.

- CONTRACTOR WILL BE RESPONSIBLE FOR ARRANGING/PROVIDING ANY REQUIRED WATER MAIN SHUT OFFS WITH THE CITY/COUNTY DURING CONSTRUCTION. ANY COSTS ASSOCIATED WITH WATER MAIN SHUT OFFS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO EXTRA COMPENSATION WILL BE PROVIDED.
- DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT CONTRACTOR'S EXPENSE.
  - CONCRETE/ASPHALT/BRICK, ETC. CAN BE CRUSHED (ON SITE OR OFFSITE) AND REUSED AS BASE MATERIAL FOR FUTURE PAVING AND BUILDING AREAS AS LONG AS IT MEETS REQUIRED GRADATION PER PLANS, CITY REQUIREMENTS AND GEOTECH REPORT.
  - IT IS THE CONTRACTORS RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES WHICH MAY HAVE BURIED OR AERIAL UTILITIES WITHIN OR NEAR THE CONSTRUCTION AREA BEFORE COMMENCING WORK. THE CONTRACTOR SHALL PROVIDE 72 HOURS MINIMUM NOTICE TO ALL UTILITY COMPANIES PRIOR TO BEGINNING CONSTRUCTION.
  - CONTRACTOR SHALL REFERENCE THE LANDSCAPE PLAN FOR THE LOCATION OF THE TREES THAT ARE ALLOWED TO REMAIN/TO BE REMOVED ONCE A TREE REMOVAL PERMIT IS OBTAINED BY THE CONTRACTOR.
  - ALL EXISTING UTILITIES SHOWN ARE LOCATED ACCORDING TO THE INFORMATION AVAILABLE TO THE ENGINEER AT THE TIME THE DRAWINGS WERE PREPARED AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR THE ENGINEER. GUARANTEE IS NOT MADE THAT ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN OR THAT THE LOCATION OF THOSE SHOWN ARE ACCURATE. FINDING THE ACTUAL LOCATION OF ANY EXISTING UTILITIES IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE DONE BEFORE HE COMMENCES ANY WORK IN THE VICINITY. FURTHERMORE, THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ANY AND ALL DAMAGE DUE TO THE CONTRACTORS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. THE OWNER OR ENGINEER WILL ASSUME NO LIABILITY FOR ANY DAMAGES SUSTAINED OR COST INCURRED BECAUSE OF THE OPERATIONS IN THE VICINITY OF EXISTING UTILITIES OR STRUCTURES, NOR FOR TEMPORARY BRACING AND SHORING OF SAME. IF IT IS NECESSARY TO SHORE, BRACE, SWING OR RELOCATE A UTILITY, THE UTILITY COMPANY OR DEPARTMENT AFFECTED SHALL BE CONTACTED BY THE CONTRACTOR AND THEIR PERMISSION OBTAINED REGARDING THE METHOD TO USE FOR SUCH WORK.
  - THE CONTRACTOR SHALL HAVE AVAILABLE AT THE JOB SITE AT ALL TIMES ONE COPY OF THE CONTRACT DOCUMENTS INCLUDING PLANS, SPECIFICATIONS, COPIES OF ANY REQUIRED CONSTRUCTION PERMITS, AND EROSION CONTROL PLANS AND INSPECTION REPORTS (SWPPP).
  - ANY DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER/ENGINEER BEFORE COMMENCING WORK. NO FIELD CHANGES OR DEVIATIONS FROM DESIGN ARE TO BE MADE WITHOUT PRIOR APPROVAL OF THE OWNER AND NOTIFICATION TO THE ENGINEER. NO CONSIDERATION WILL BE GIVEN TO CHANGE ORDERS FOR WHICH THE OWNER AND ENGINEER WERE NOT CONTACTED PRIOR TO CONSTRUCTION OF THE AFFECTED ITEM.
  - CONTRACTOR IS RESPONSIBLE FOR PREPARING AND IMPLEMENTING A TRAFFIC CONTROL PLAN AND THE INSTALLATION OF TRAFFIC CONTROL DEVICES FOR ANY STREET WORK.
  - ALL DEMOLITION WORK OR CONSTRUCTION VEHICLE TRAFFIC WITHIN 10 FEET OF THE CANOPY OF ANY TREE TO BE SAVED SHALL CLOSELY COORDINATED WITH TREE PRESERVATION ACTIVITIES.
  - CONTRACTOR SHALL COMPLY TO THE FULLEST EXTENT WITH THE LATEST OSHA STANDARDS FOR EXCAVATION AND TRENCHING PROCEDURES. CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, ETC. AS NECESSARY FOR THESE OPERATIONS, AND SHALL COMPLY WITH ALL OSHA PERFORMANCE CRITERIA.
  - DO NOT INTERRUPT EXISTING UTILITIES SERVICING FACILITIES OCCUPIED AND USED BY THE OWNER OR OTHERS DURING OCCUPIED HOURS EXCEPT WHEN SUCH INTERRUPTIONS HAVE BEEN AUTHORIZED IN WRITING BY THE OWNER AND THE LOCAL MUNICIPALITIES. INTERRUPTIONS SHALL ONLY OCCUR AFTER ACCEPTABLE TEMPORARY SERVICE HAS BEEN PROVIDED.

- ANY RECYCLED MATERIAL TO BE STOCKPILED ON THE SITE SHALL BE STORED IN AS SMALL AN AREA AS PRACTICAL AND THE LOCATION OF ANY STOCKPILE SHALL BE WELL CLEAR OF THE BUILDING PAD AREA AND THE LOCATION MUST BE PRE-APPROVED BY THE ENGINEER AND OWNER PRIOR TO STOCKPILING.
- CONTRACTOR TO PULL ALL TREE REMOVAL PERMITS FOR ANY REQUIRED TREE REMOVAL.
- ALL IRRIGATION ON-SITE NOT TO BE RE-USED SHOULD BE REMOVED. REFER TO IRRIGATION PLAN.
- REMOVE AND/OR PLUG EXISTING UTILITIES SUCH AS STORM DRAINAGE, SANITARY SEWER, WATER, GAS, ELECTRIC, AND TELEPHONE AS SHOWN OR AS NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING EACH UTILITY COMPANY TO COORDINATE REMOVAL OF ALL UTILITIES AND FOR DETERMINING HORIZONTAL AND VERTICAL LOCATIONS OF UTILITIES PRIOR TO COMMENCING WORK.
- QUANTITIES SHOWN HERE ARE APPROXIMATE AND ARE PROVIDED FOR CONVENIENCE ONLY AND NOT FOR BID PURPOSES. CONTRACTOR SHALL VERIFY QUANTITIES NECESSARY TO DEMO FACILITIES SHOWN.
- REFERENCE ALTA SURVEY FOR TREE SIZE AND SPECIES INFO.
- REFERENCE PHASE I ESA BY TERRACON FOR ENVIRONMENTAL CONDITIONS THAT MAY BE PRESENT ON SITE.
- PRIOR TO DEMOLITION, CONTRACTOR IS TO FIELD VERIFY LOCATION OF ELECTRIC SERVICE LINE TO EXISTING TARGET PYLON SIGN. THE CONTRACTOR IS TO ENSURE POWER TO THE SIGN IS MAINTAINED AT ALL TIMES DURING CONSTRUCTION. ANY DAMAGE TO EXISTING FACILITIES TO REMAIN IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO REPLACE, AT THEIR OWN EXPENSE.
- WHERE EXISTING CURB AND GUTTER TO BE REMOVED IS IMMEDIATELY ADJACENT TO EXISTING SIDEWALK TO REMAIN, CONTRACTOR TO REMOVE EXISTING CURB AND GUTTER TO EXPANSION JOINT AT BACK OF CURB. EXISTING SIDEWALK TO BE PRESERVED AND PROTECTED.



#### CAUTION!!

CONTRACTOR IS TO VERIFY PRESENCE AND EXACT LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.

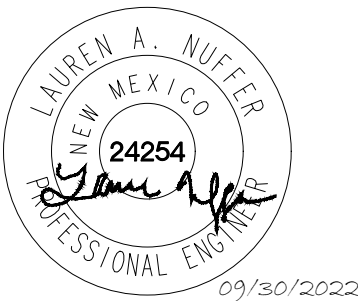
INTERIM CONDITIONS  
DEMOLITION AND STABILIZATION  
2004 WYOMING BLVD NE  
BERNALILLO COUNTY, NEW MEXICO 87112

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Professional of Record:



#### HYDROLOGY SUBMITTAL

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Sheet Title:

#### DEMOLITION PLAN

Date: 08.09.2022

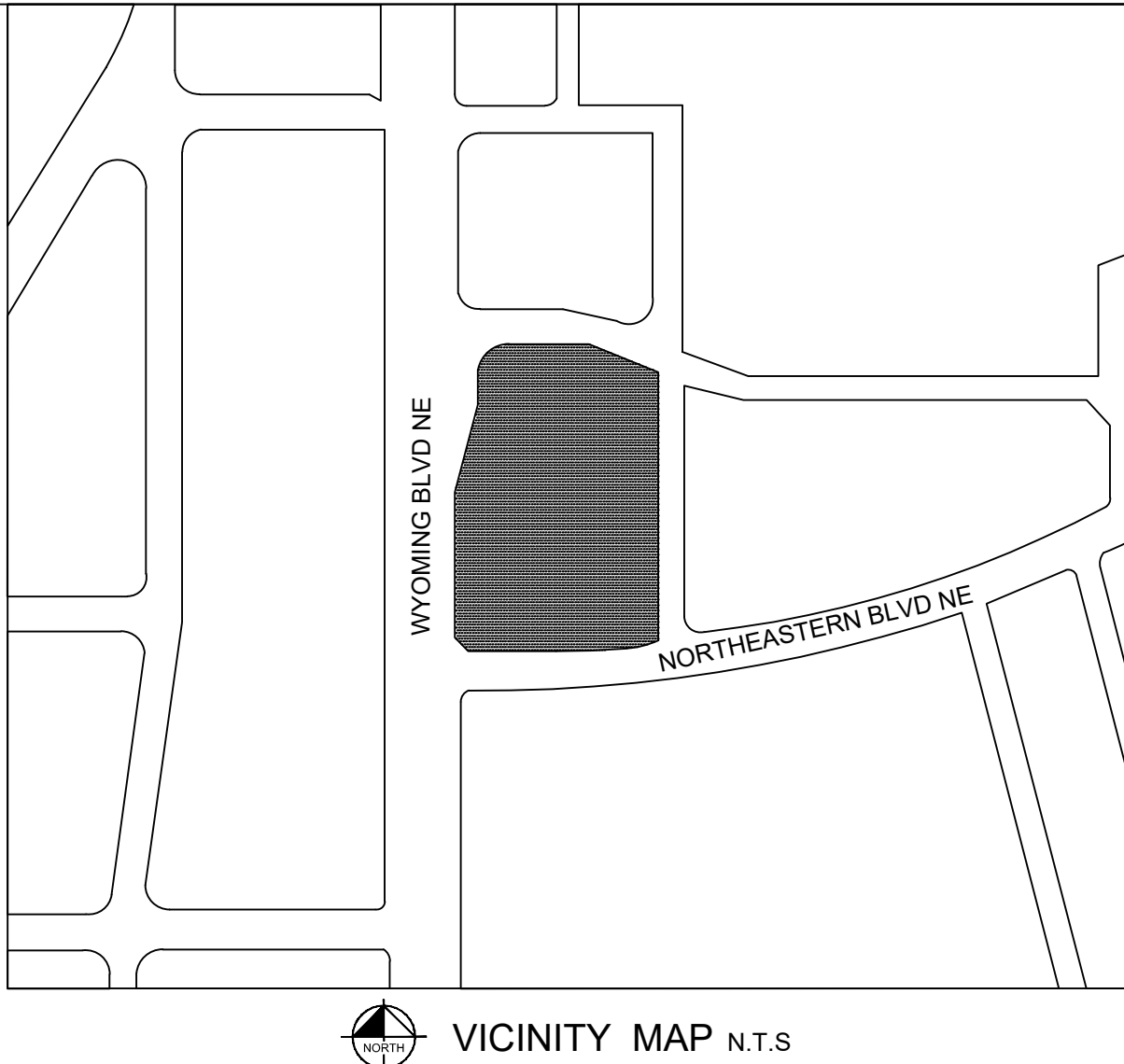
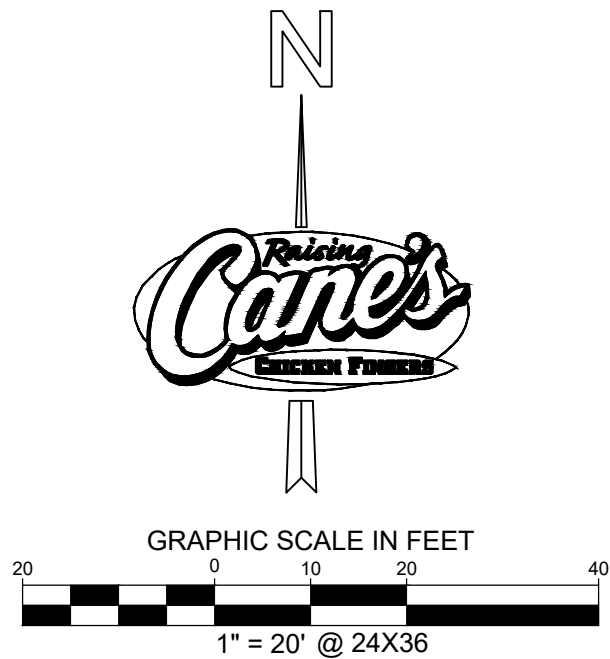
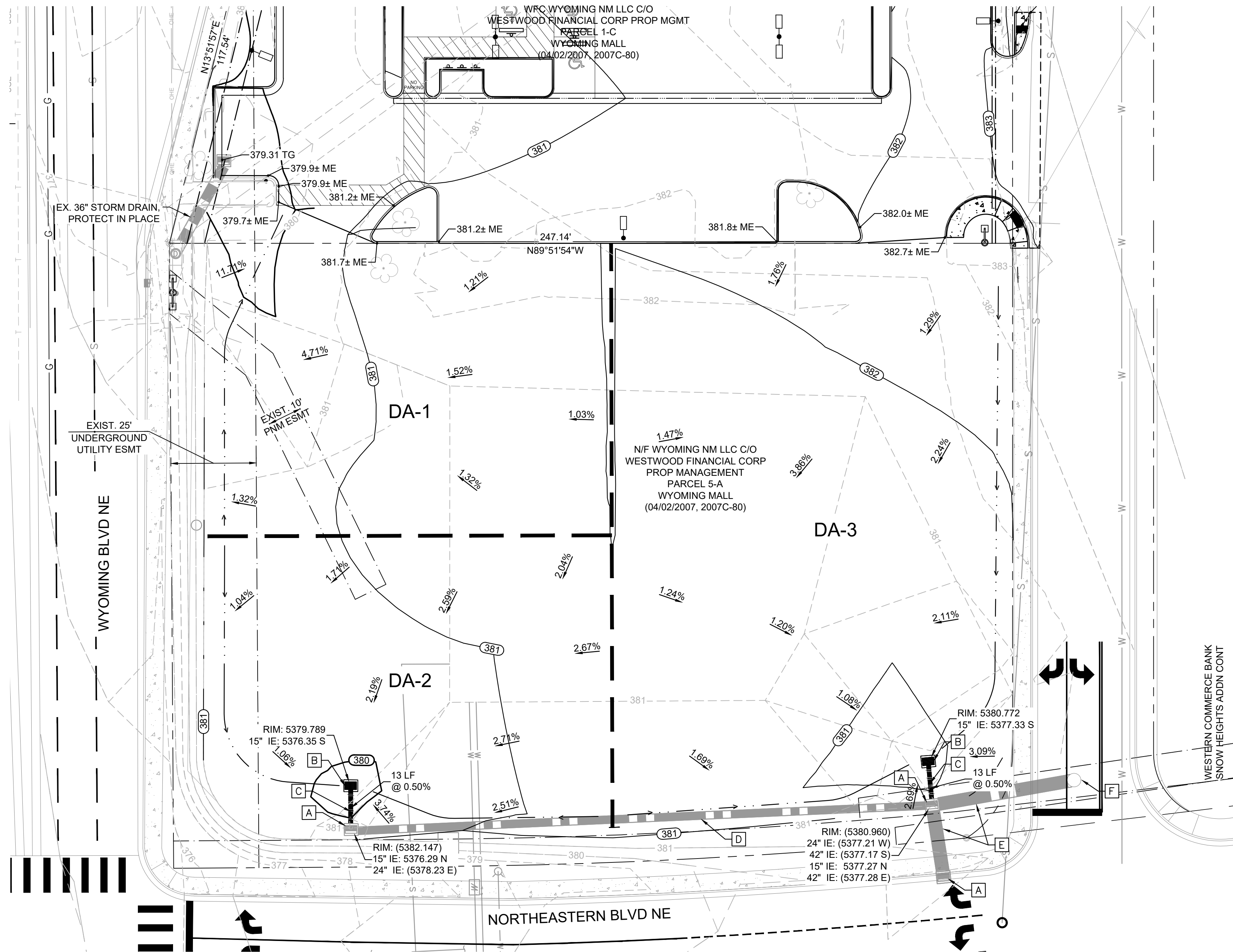
Project Number: 069313444

Drawn By: LW/NW

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LEGEND	
	PROPOSED PROPERTY LINE
	ADJACENT PROPERTY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED FLOW ARROW WITH SLOPE
	PROPOSED SPOT ELEVATION
	EXISTING STORM INLET
	GRADE BREAK
	HIGH POINT
	TOP OF PAVEMENT
	TOP OF SIDEWALK
	MATCH EXISTING ELEVATION
	TOP OF GRATE
	PROPOSED RIDGE
	PROPOSED SWALE

GRADING AND DRAINAGE CONSTRUCTION NOTES	
A	EX. CURB INLET TO REMAIN
B	STORM INLET TYPE "A" PER CITY OF ALBUQUERQUE STANDARD DETAILS, DWG NO. 2206
C	15" DIA. HDPE PIPE
D	EX. 24" HDPE PIPE TO REMAIN
E	EX. 42" HDPE PIPE TO REMAIN
F	EX. 42" HDPE PIPE TO REMAIN

#### FLOODPLAIN NOTE

BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE "X" OF THE FLOOD INSURANCE RATE MAP (FIRM), COMMUNITY PANEL NO. 35001C0356H, WHICH BEARS AN EFFECTIVE DATE OF AUGUST 16, 2012 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA.

#### GRADING NOTES

- ADD 5000' TO ALL CONTOUR ELEVATIONS SHOWN.
- ALL CONSTRUCTION TO BE IN ACCORDANCE WITH THESE PLANS AND THE CITY/AHJ STANDARDS AND SPECIFICATIONS.
- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL MAKE CERTAIN THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AUTHORITIES.
- THE GENERAL CONTRACTOR AND ALL SUB-CONTRACTORS SHALL VERIFY THE SUITABILITY OF ALL EXISTING AND PROPOSED SITE CONDITIONS INCLUDING GRADES AND DIMENSIONS BEFORE COMMENCEMENT OF ANY CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY OF ANY DISCREPANCIES. MINOR ADJUSTMENTS TO FINISH GRADE TO ACCOMPLISH SPOT DRAINAGE ARE ACCEPTABLE, IF NECESSARY, UPON PRIOR APPROVAL OF ENGINEER. PAVING INSTALLED SHALL "FLUSH OUT" AT ANY JUNCTURE WITH EXISTING PAVING.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- ALL CUT OR FILL SLOPES SHALL BE 4:1 OR FLATTER UNLESS OTHERWISE NOTED.
- TOPOGRAPHIC INFORMATION IS TAKEN FROM A PUBLICLY AVAILABLE INFORMATION. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.
- CONTRACTOR SHALL ADJUST EXISTING VALVES, MANHOLE RIMS, ETC. AS NECESSARY TO MATCH FINISHED GRADE.
- ANY PROPOSED CONTOURS SHOWN ARE APPROXIMATE. PROPOSED SPOT ELEVATIONS AND DESIGNATED GRADIENT ARE TO BE USED IN THE EVENT OF ANY DISCREPANCIES.
- REFER TO EROSION CONTROL PLAN FOR EROSION CONTROL DEVICES TO BE INSTALLED PRIOR TO COMMENCING GRADING OPERATIONS.
- ALL FILL TO BE PLACED SHALL BE IN ACCORDANCE WITH THE CURRENT APPLICABLE GEOTECHNICAL REPORT RECOMMENDATIONS.
- SIDE SLOPES NEED TO BE STABILIZED WITH NATIVE GRASS SEED (PER CITY OF ALBUQUERQUE SPECIFICATION 1012) WITH AGGREGATE MULCH OR EQUAL (MUST SATISFY THE "FINAL STABILIZATION CRITERIA" CONSTRUCTION GENERAL PERMIT X.X.XX.X).

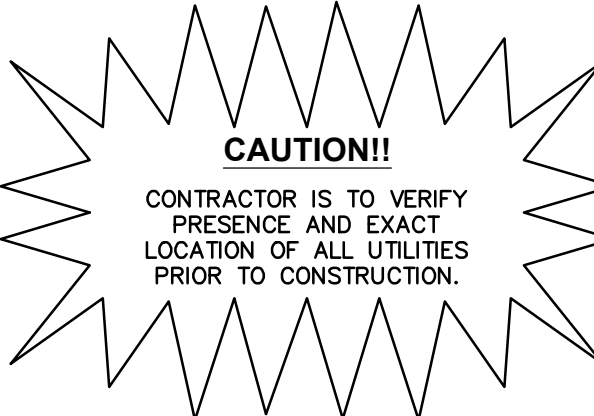
#### STORMWATER SUMMARY

IN EXISTING CONDITIONS, THE SITE IS FULLY DEVELOPED. THE PROPOSED SCOPE OF WORK IS TO DEMOLISH EXISTING IMPROVEMENTS, MASS GRADE THE SITE, AND STABILIZE THE DISTURBED AREA WITH EITHER A VEGETATIVE COVER OR AGGREGATE MULCH.

THERE WILL BE NO IMPERVIOUS COVER ONSITE FOLLOWING THE PROJECT SCOPE, WHICH WILL LOWER THE PEAK DISCHARGE FROM THE SITE. NO FLOW CONTROL IS PROPOSED. EXISTING DRAINAGE PATTERNS ARE MAINTAINED AND EXISTING STORMWATER INFRASTRUCTURE IS PRESERVED IN PLACE TO ALLOW FOR FUTURE USE.

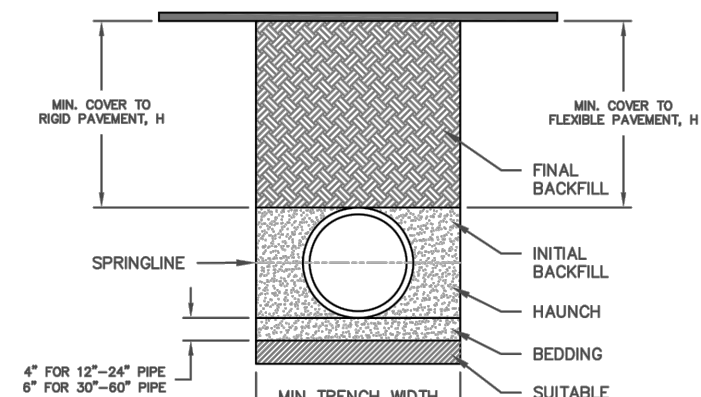
THIS IS AN INTERIM CONDITION GRADING AND DRAINAGE PLAN. THE ULTIMATE DEVELOPMENT ON THIS TRACT WILL BE HELD TO THE PRE-DEVELOPMENT PEAK FLOWS TOTALING 4.85 CFS AS SHOWN ON THE RATIONAL CALCULATIONS SUMMARY TABLE ON THIS SHEET.

FOR EXISTING CONDITIONS, REFER TO AS-BUILT "GRADING AND DRAINAGE PLAN - B", SHEET 6 OF TIERAA WEST, LLC PROJECT "ALBUQUERQUE WAL-MART NM:5491-00", DATED 09/08/05.



#### CITY OF ALBUQUERQUE STORM TRENCH INSTALLATION DETAIL DWG. 2240

##### HP STORM TRENCH INSTALLATION DETAIL



- NOTES:
- ALL PIPE SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH ASTM D2321, "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS". LATEST ADDITION WITH THE EXCEPTION THAT THE INITIAL BACKFILL MAY EXTEND TO THE CROWN OF THE PIPE. SOIL CLASSIFICATIONS ARE PER THE LATEST VERSION OF ASTM D2321, CLASS I/II MATERIALS (MH, CH) AS DEFINED IN PREVIOUS VERSIONS OF ASTM D2321 ARE NOT APPROPRIATE BACKFILL MATERIALS.
  - MEASURES SHOULD BE TAKEN TO PREVENT MIGRATION OF NATIVE PINES INTO BACKFILL MATERIAL, WHEN REQUIRED.
  - FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER, AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING A GEOTEXTILE MATERIAL.
  - BEDDING: SUITABLE MATERIAL SHALL BE CLASS I, II, OR III. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. COMPACTION SHALL BE 80% OF MAXIMUM DENSITY PER ASTM D1557 OR AS SHOWN ON THE PLANS, UNLESS OTHERWISE NOTED BY THE ENGINEER. MINIMUM BEDDING THICKNESS SHALL BE 4" (100mm) FOR 12"-24" (300mm-600mm) DIAMETER PIPE, 6" (150mm) FOR 30"-60" (750mm-1500mm) DIAMETER PIPE. THE MIDDLE 10 BENEATH THE PIPE INVERT SHALL BE LOOSELY PLACED.
  - INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE CLASS I, II, OR III IN THE PIPE ZONE EXTENDING TO THE CROWN OF THE PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION, FOR TRAFFIC APPLICATIONS, CLASS I, II, OR III MATERIAL. SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 80% OF MAXIMUM DENSITY PER ASTM D1557, CLASS IV MATERIALS AS DEFINED IN ASTM D2321 ARE NOT APPROPRIATE BACKFILL MATERIALS.
  - MINIMUM COVER: MINIMUM COVER, H, IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) IS 12" (300mm) FROM THE TOP OF PIPE TO GROUND SURFACE. ADDITIONAL COVER MAY BE REQUIRED TO PREVENT FLOTATION. FOR TRAFFIC APPLICATIONS, CLASS I OR II MATERIAL, COMPACTION SHALL BE 80% OF MAXIMUM DENSITY PER ASTM D1557, CLASS IV MATERIAL. SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 80% OF MAXIMUM DENSITY PER ASTM D1557, CLASS IV MATERIALS AS DEFINED IN ASTM D2321 ARE NOT APPROPRIATE BACKFILL MATERIALS. FOR TRAFFIC APPLICATIONS, MINIMUM COVER, H, IS 12" (300mm) UP TO 48" (1200mm) DIAMETER PIPE AND 24" (600mm) OF COVER FOR 60" (1500mm) DIAMETER PIPE. MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO BOTTOM OF RIGID PAVEMENT.
  - FOR ADDITIONAL INFORMATION SEE TECHNICAL NOTE 2.04.

TABLE 1. RECOMMENDED MINIMUM TRENCH WIDTHS

PIPE DIAM.	MIN. TRENCH WIDTH
12" (300mm)	41" (762mm)
18" (450mm)	51" (864mm)
24" (600mm)	61" (961mm)
30" (750mm)	71" (1219mm)
36" (900mm)	81" (1422mm)
42" (1050mm)	91" (1626mm)
48" (1200mm)	101" (1830mm)
54" (1350mm)	111" (2032mm)
60" (1500mm)	121" (2438mm)

TABLE 2. MINIMUM RECOMMENDED COVER BASED ON VEHICLE LOADING CONDITIONS

PIPE DIAM.	H-25	CLASS II
12" (300mm)	12" (300mm)	48" (1200mm)
18" (450mm)	12" (300mm)	48" (1200mm)
24" (600mm)	12" (300mm)	48" (1200mm)
30" (750mm)	12" (300mm)	48" (1200mm)
36" (900mm)	12" (300mm)	48" (1200mm)
42" (1050mm)	12" (300mm)	48" (1200mm)
48" (1200mm)	12" (300mm)	48" (1200mm)
54" (1350mm)	12" (300mm)	48" (1200mm)
60" (1500mm)	12" (300mm)	48" (1200mm)

TABLE 3. MAXIMUM COVER FOR HP STORM PIPE, R

PIPE DIA	CLASS I	CLASS II	CLASS III
12"	41"	51"	61"
18"	51"	61"	71"
24"	61"	71"	81"
30"	71"	81"	91"
36"	81"	91"	101"
42"	91"	101"	111"
48"	101"	111"	121"
54"	111"	121"	131"
60"	121"	131"	141"
66"	131"	141"	151"
72"	141"	151"	161"
78"	151"	161"	171"
84"	161"	171"	181"
90"	171"	181"	191"
96"	181"	191"	201"
102"	191"	201"	211"
108"	201"	211"	221"
114"	211"	221"	231"
120"	221"	231"	241"
126"	231"	241"	251"
132"	241"	251"	261"
138"	251"	261"	271"
144"	261"	271"	281"
150"	271"	281"	291"

FILL HEIGHT TABLE GENERATED USING AASHTO SECTION 12. LOW RESISTANCE FACTOR (R) OR DESIGN (R) PROCEDURE WITH THE FOLLOWING ASSUMPTIONS:  
NO HYDROSTATIC PRESSURE  
UNIT WEIGHT OF SOIL (W<sub>s</sub>) = 120 PCF

REVISIONS	CITY OF ALBUQUERQUE
	HP STORM TRENCH INSTALLATION DWG. 2240 MAY 2020

#### STORMWATER DISCHARGE SUMMARY TABLE

Kimley»Horn				
PROJECT NAME:		RC0705 Wyoming Blvd Albuquerque NM		
PROJECT NUMBER:		69313444		
CALCULATED BY:		NW		
CHECKED BY:		LW		
		DATE: 9/30/2022		
RATIONAL CALCULATIONS SUMMARY				
DESIGN POINT	LAND TREATMENT	WEIGHTED CN	TRIBUTARY AREA (AC)	PEAK FLOWS (CFS)
				Q100
Pre-Development Basins				
DA-1	C/D	0.86	0.21	1.03
DA-2	C/D	0.88	0.26	1.31
DA-3	C/D	0.89	0.86	4.37
		TOTAL	1.33	6.71
Interim Condition Basins				
DA-1	C	0.64	0.23	0.57
DA-2	C	0.67	0.24	0.61
DA-3	C	0.63	0.46	0.95
		TOTAL	0.93	2.13