



Lobo Plaza

(1300 Lomas Blvd NE, Albuquerque, New Mexico)

Traffic Impact Study

February 04, 2026

FINAL



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Lobo Plaza
1300 Lomas Blvd NE - Albuquerque, NM
Traffic Impact Study

Executive Summary

The purpose of this Traffic Impact Study (TIS) is to evaluate the transportation conditions before and after implementation of the proposed Lobo Plaza to determine the impact of the development on the adjacent transportation system and recommend mitigation measures where necessary. This study is prepared in accordance with the requirements of the City of Albuquerque (COA) and NMDOT. The City of Albuquerque scoping letter for this TIS is in Appendix pages A-180 through A-182.

Site Location and Study Area

The proposed Lobo Plaza is to be located at 1300 Lomas Blvd NE, Albuquerque, New Mexico, along the south side of Lomas Blvd approximately 0.4 miles east of I-25 interchange. See Vicinity Map below.



The study area includes the 5 intersections listed below and the 2 access intersections for the Development shown in Site Plan:

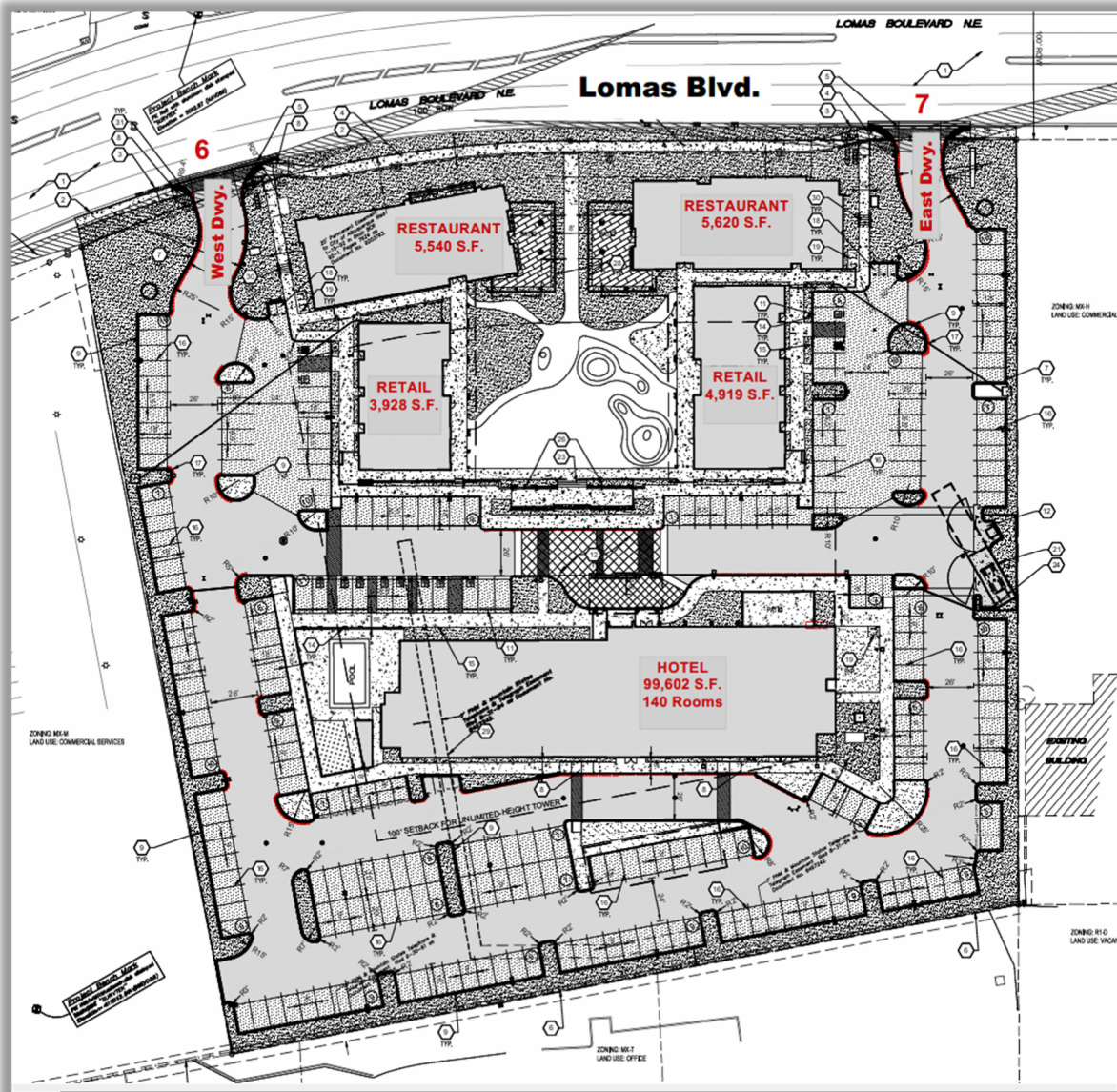
1. Lomas Blvd NE & I-25 Southbound On-Off Ramp (Signalized)
2. Lomas Blvd NE & I-25 Northbound On-Off Ramp (Signalized)
3. Lomas Blvd NE & University Blvd NE (Signalized)

4. Lomas Blvd NE & Medical Arts Ave NE (Unsignalized)
5. Lomas Blvd NE & Torc Driveway / Frontage Rd (Unsignalized)
6. Lomas Blvd NE & Legion Rd / Lobo Plaza West Driveway (Unsignalized)
7. Lomas Blvd NE & East Driveway (Unsignalized)

Development Description

The Lobo Plaza is proposed to be a total of 4.97 acres for the proposed 2028 Implementation Year and analyzed for the 2038 Horizon Year. The site will generate hotel and commercial traffic to the transportation system in the Study Area. The site is to be developed into the following:

- 2 – retail commercial buildings totaling up to 9,180 SQFT
- 2 – restaurant buildings totaling up to 11,600 SQFT
- 140 Rooms – hotel, totaling 23,951 GFA



Trip Generation rates for this project were calculated based on the Institute of Transportation Engineers' (ITE) Trip Generation Manual, 11th Edition. A summary of the resulting trip generation rate for the project are summarized in the following table:

| <i>Lobo Plaza (Lomas Blvd. / University Blvd.)</i> | | | | | | | |
|---|---|-----------|----------------|-------------|----------------|-------------|-------------|
| Trip Generation Data (ITE Trip Generation Manual - 11th Edition) | | | | | | | |
| | USE (ITE CODE) | 24 HR VOL | A. M. PEAK HR. | | P. M. PEAK HR. | | |
| | DESCRIPTION | GROSS | ENTER | EXIT | ENTER | EXIT | |
| Summary Sheet | | Units | | | | | |
| North-West Parcel | High Turnover (Sit-Down) Restaurant (932) | 5.98 | 641 | 31 | 26 | 33 | 21 |
| North-West Parcel | Strip Retail Plaza <40K - Equation (822) | 4.28 | 410 | 10 | 7 | 21 | 21 |
| | Hotel (310) | 140 | 1,094 | 35 | 28 | 39 | 37 |
| South-East Parcel | High Turnover (Sit-Down) Restaurant (932) | 5.62 | 602 | 30 | 24 | 31 | 20 |
| South East Parcel | Strip Retail Plaza <40K - Equation (822) | 4.90 | 436 | 11 | 7 | 23 | 23 |
| Subtotal | | | 3,183 | 117 | 92 | 147 | 122 |
| | | | | -2 | -2 | -22 | -22 |
| | | | | 115 | 90 | 125 | 100 |
| | | | | (25) | (19) | (32) | (26) |
| | | | | 90 | 71 | 93 | 74 |

The trip generation rate calculations adjusted the rates for both internal capture and for pass-by trips based on accepted ITE methodologies.

This Traffic Impact Study is designed to meet the standards and scope of study as defined by the City of Albuquerque Transportation Development Section of the Planning Department and the New Mexico Department of Transportation (District 3) as defined in the TIS Scoping Meeting held on July 11, 2025. Analysis in this Study will be conducted using current versions of Synchro 12 and / or Highway Capacity Software (HCS).

The results of existing condition of the Current Year (2025) AM and PM Peak Hours are summarized in the following table:

| Summary Results Table | | | | | |
|---|---------------|--------------|-----------|----------|----------|
| Intersection No. /Name | Result Source | Control Type | Condition | 2025 | |
| | | | | AM Peak | PM Peak |
| 1 - Lomas Blvd NE / I-25 Southbound On-Off Ramp | HCS 2025 | Signalized | EXISTING | D - 46.4 | C - 21.7 |
| 2 - Lomas Blvd NE / I-25 Northbound On-Off Ramp | | Signalized | EXISTING | C - 23.9 | C - 31.5 |
| 3 - Lomas Blvd NE / University Blvd NE | Synchro 12 | Signalized | EXISTING | D - 37.7 | D - 45.5 |
| 4 - Lomas Blvd NE / Medical Arts Ave NE | | Unsignalized | EXISTING | C - 20.6 | B - 10.8 |
| 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | | Unsignalized | EXISTING | B - 12.6 | B - 11.8 |
| 6 - Lomas Blvd NE / Legion Rd / West DWY | | Unsignalized | EXISTING | B - 10.5 | B - 13.1 |
| 7 - Lomas Blvd NE / East DWY | | Unsignalized | EXISTING | A - 9.5 | B - 10.5 |

Note : The LOS / Delay for unsignalized intersections is the highest calculated delay for any movement.

The results of the Implementation Year (2028) and Horizon Year (2038) AM and PM Peak Hour No Build and Build Conditions are summarized in the following table:

| Summary Results Table | | | | | | |
|---|--------------|-------------------|----------|----------|----------|----------|
| Intersection No. /Name | Control Type | Condition | 2028 | | 2038 | |
| | | | AM Peak | PM Peak | AM Peak | PM Peak |
| 1 - Lomas Blvd NE / I-25 Southbound On-Off Ramp (HCS Results) | Signalized | NO BUILD | D - 50.7 | C - 24.9 | E - 57.2 | C - 22.4 |
| | | BUILD | D - 50.7 | C - 24.9 | E - 57.1 | C - 22.5 |
| | | BUILD - MITIGATED | C - 27.5 | N/A | C - 33.1 | N/A |
| 2 - Lomas Blvd NE / I-25 Northbound On-Off Ramp (HCS Results) | Signalized | NO BUILD | C - 23.4 | C - 32.1 | C - 23.6 | D - 35.7 |
| | | BUILD | C - 21.5 | C - 32.3 | C - 23.8 | C - 34.7 |
| | | BUILD - MITIGATED | N/A | C - 21.8 | N/A | C - 23.6 |
| 3 - Lomas Blvd NE / University Blvd NE (Synchro Results) | Signalized | NO BUILD | D - 38.3 | D - 46.7 | D - 40.6 | D - 50.9 |
| | | BUILD | D - 39.5 | D - 47.9 | D - 42.2 | D - 52.2 |
| | | BUILD - MITIGATED | C - 26.9 | C - 40.5 | C - 28.2 | D - 42.7 |
| 4 - Lomas Blvd NE / Medical Arts Ave NE (Synchro Results) | Unsignalized | NO BUILD | C - 21.4 | B - 10.9 | C - 23.4 | B - 10.9 |
| | | BUILD | C - 22.9 | B - 11.0 | D - 25.1 | B - 11.3 |
| 5 - Lomas Blvd NE / Torc DWY / Frontage Rd (Synchro Results) | Unsignalized | NO BUILD | B - 13.0 | B - 11.8 | B - 13.5 | B - 12.0 |
| | | BUILD | B - 13.9 | B - 12.0 | B - 13.5 | B - 12.0 |
| 6 - Lomas Blvd NE / Legion Rd / West DWY (Synchro Results) | Unsignalized | NO BUILD | B - 10.5 | B - 12.8 | B - 10.6 | B - 13.2 |
| | | BUILD | B - 13.6 | B - 12.9 | B - 14.2 | B - 13.4 |
| | | BUILD - MITIGATED | C - 15.9 | B - 12.9 | C - 16.3 | B - 13.4 |
| 7 - Lomas Blvd NE / East DWY (Synchro Results) | Unsignalized | NO BUILD | A - 9.5 | B - 10.7 | A - 9.5 | B - 10.8 |
| | | BUILD | B - 13.5 | B - 11.5 | B - 14.0 | B - 11.9 |
| | | BUILD - MITIGATED | C - 15.2 | B - 11.9 | C - 15.6 | B - 11.9 |

Notes : 1) The LOS / Delay for unsignalized intersections is the highest calculated delay for any movement.

2) For intersections 6 and 7, the BUILD - MITIGATED conditions assumes a staged left turn out of the driveways.

A summary of the impacts and recommendations based on the results of the Traffic Impact Study can be found below.

Summary of Impacts

The proposed Lobo Plaza (1300 Lomas Blvd NE) will have no adverse impact on the adjacent transportation system. The overall levels-of-service were determined to be acceptable for all for all seven (7) intersections during both the AM Peak Hour and PM Peak Hour Implementation Year (2028) and Horizon Year (2038) conditions analyzed in this report. However, signal timing optimization is recommended to minimize delays at certain approaches at the signalized intersections.

In summary, the recommendations of this study are:

- Construct two (2) commercial driveways – east and west driveways on Lomas Blvd NE Driveway to provide access to Lobo Plaza. The access points should be full access unsignalized T-intersections and both the access points can be single approach (northbound) access with a shared left turn, through and right turn lane. While construction, the centerline-to-centerline distances that should be maintained between Legion Rd / East Driveway and West Driveway is approximately 410 feet.
- All design and construction associated with the Lobo Plaza shall maintain adequate traffic site distances at existing and proposed intersections / driveways. No landscaping elements or walls or other structures should be permitted that constrain site distances at an intersection / driveway that significantly compromises safety.

Recommendations

Intersection 1, Lomas Blvd NE and I-25 Southbound Ramp

Optimization of Signal timing during the AM Peak Hour as per the recommendations in this report is recommended.

Intersection 2 - Lomas Blvd NE and I-25 Northbound Ramp

Optimization of Signal timing during the PM Peak Hour as per the recommendations in this report is recommended.

Intersection 3 - Lomas Blvd NE and University Blvd NE

Optimization of Signal timing during the PM Peak Hour as per the recommendations in this report is recommended.

Intersection 4 - Lomas Blvd NE and Medical Arts Ave NE

There are no recommendations at this Intersection.

Intersection 5 – Lomas Blvd NE and Torc Driveway / Frontage Rd

There are no recommendations at this Intersection.

Intersection 6 - Lomas Blvd NE and Legion Rd / West Driveway

- There are no recommendations at the current / existing intersection of Lomas Blvd NE and Legion Rd.
- The recommendations for West Driveway include the construction of a 2-lane (one northbound approach lane and one southbound receiving lane) commercial full access unsignalized driveway that provides access to Lobo Plaza Development. All new access shall be constructed with adequate sight distance at all approaches at the intersection.

Intersection 7 - Lomas Blvd NE and East Driveway

The recommendations for East Driveway include the construction of a 2-lane (one northbound approach lane and one southbound receiving lane) full access unsignalized commercial driveway that provides access to Lobo Plaza Development. All new access shall be constructed with adequate sight distance at all approaches to the intersection.

In summary, the proposed Retail at Lobo Plaza will have no significant adverse impact on the adjacent transportation system provided that the recommendations of this report are implemented.

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Lobo Plaza
1300 Lomas Blvd NE - Albuquerque, NM
Traffic Impact Study

Introduction

The purpose of this Traffic Impact Study (TIS) is to evaluate the transportation conditions before and after implementation of the proposed Lobo Plaza to determine the impact of the development on the adjacent transportation system and recommend mitigation measures where necessary. This study is prepared in accordance with the requirements of the City of Albuquerque (COA) and NMDOT. The City of Albuquerque scoping letter for this TIS is in Appendix pages A-180 through A-182.

Description of Proposed Development

The proposed Lobo Plaza is to be located at 1300 Lomas Blvd NE, Albuquerque, New Mexico, along the south side of Lomas Blvd approximately 0.4 miles east of I-25 interchange. See Vicinity Map below.



The study area includes the 5 intersections listed below and the 2 access intersections for the Development shown in Figure 2:

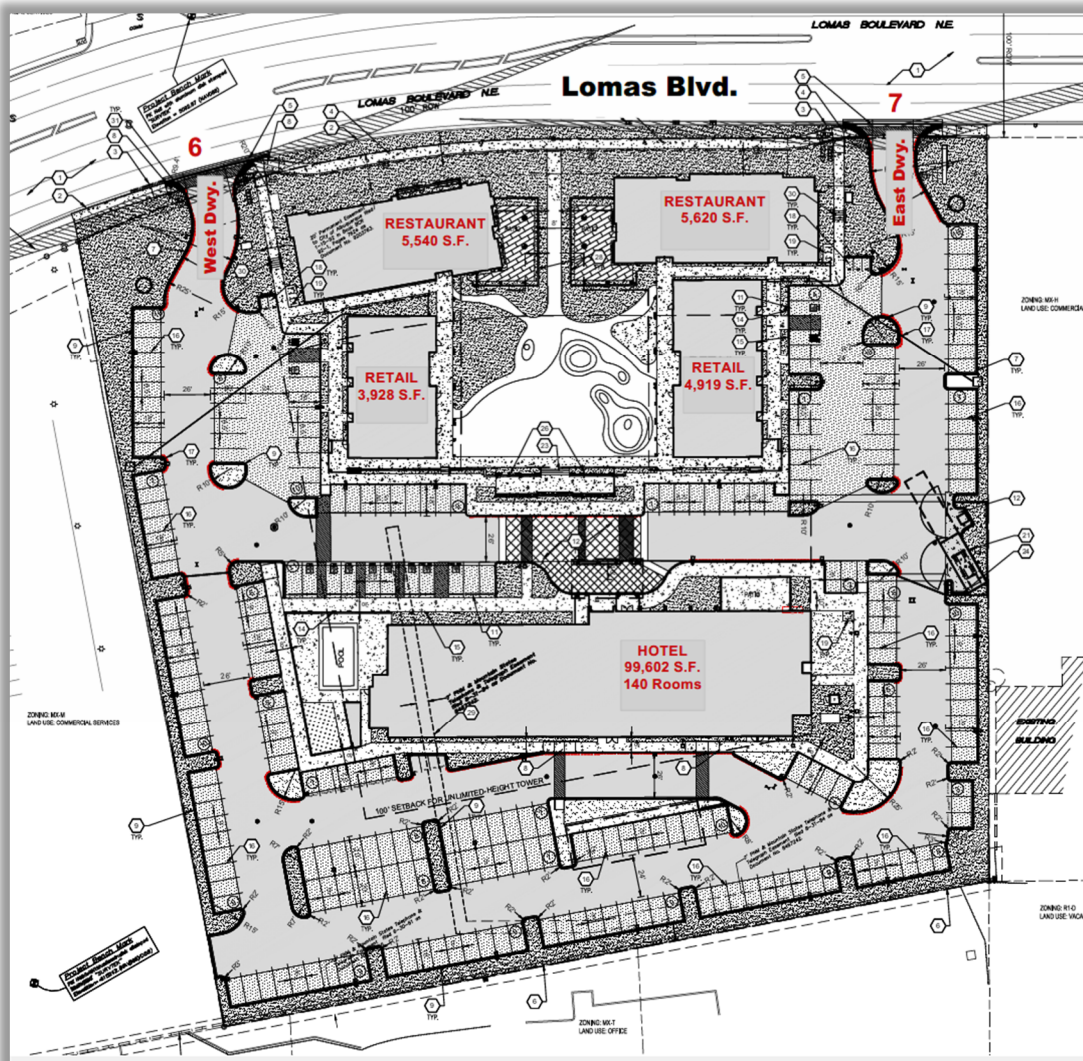
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2. Lomas Blvd NE & I-25 Northbound On-Off Ramp (Signalized)
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4. Lomas Blvd NE & Medical Arts Ave NE (Unsignalized)
5. Lomas Blvd NE & Torc Driveway / Frontage Rd (Unsignalized)
6. Lomas Blvd NE & Legion Rd / Lobo Plaza West Driveway (Unsignalized)
7. Lomas Blvd NE & East Driveway (Unsignalized)

Land Use and Intensity

The Lobo Plaza is proposed to be a total of 4.97 acres for the proposed 2028 Implementation Year and analyzed for the 2038 Horizon Year. The site will generate hotel and commercial traffic to the transportation system in the Study Area. The site is to be developed into the following:

- 2 – retail commercial buildings totaling up to 9,180 SQFT
- 2 – restaurant buildings totaling up to 11,600 SQFT
- 140 Rooms – hotel, totaling 23,951 GFA



- NOTE: The building sizes in the trip generation table are slightly higher than those on the site plan. The site plan is still under development, and building sizes may change somewhat in the future. Higher building sizes were used to allow for future increase in building sizes as the entitlement process continues.

Development Phasing and Timing

The Lobo Plaza Development expansion will be built in one phase with the Implementation Year in 2028 and the Horizon Year 2038.

Existing and Planned Zoning

Current and proposed zoning is MX-H (High Intensity Zone) under the City of Albuquerque's Integrated Development Ordinance (IDO). The purpose of the MX-H zone district is to provide for large-scale destination retail and high-intensity commercial, residential, light industrial, and institutional uses, as well as high-density residential uses, particularly along Transit Corridors and in Urban Centers. The MX-H zone district is intended to allow higher-density infill development in appropriate locations

Site Access

There are two proposed access driveways (West Driveway and East Driveway) on Lomas Blvd NE to the project. Both driveways are proposed as full-access commercial driveways, one on the west side of the project and the other on the east side. The proposed site plan demonstrating access is shown on the previous page and in Appendix page A-03.

Study Area Conditions

Study Area Definition

A Traffic Impact Study Scoping Meeting was held on July 11, 2025. The attendees included Ernest Armijo, P.E. (City of Albuquerque), Margaret Haynes, P.E. (NMDOT), Ronald R. Bohannon, P.E., Terry Brown P.E., (Tierra West LLC.). At the Scoping Meeting, it was determined that the study area for the TIS would include the 3 signalized intersections and 4 unsignalized intersections listed below and the access intersection for the Development:

1. Lomas Blvd NE & I-25 Southbound On-Off Ramp (Signalized)
2. Lomas Blvd NE & I-25 Northbound On-Off Ramp (Signalized)
3. Lomas Blvd NE & University Blvd NE (Signalized)
4. Lomas Blvd NE & Medical Arts Ave NE (Unsignalized)
5. Lomas Blvd NE & Torc Driveway / Frontage Rd (Unsignalized)
6. Lomas Blvd NE & Legion Rd / Lobo Plaza West Driveway (Unsignalized)
7. Lomas Blvd NE & East Driveway (Unsignalized)

Existing Land Use

The land for the project is undeveloped, and the study area is mostly developed with commercial and residential uses in the area.

Other Planned or Approved Development and Transportation Improvements

There are no other known land development projects in the study area that need to be incorporated into the background traffic volumes for this Study.

Existing Roadway System

Lomas Blvd NE is classified as a Regional Principle Arterial on the Future 2040 Long Range Roadway System Map. It is generally a six-lane divided roadway in the study area with raised curbs and gutters, sidewalks, and raised medians. The posted speed limit on Lomas Blvd in the study area is 35 MPH.

Interstate 25 (I-25) is classified as an Interstate Highway on the Futures 2040 Long Range Roadway System Map. It is generally a ten-lane divided freeway in the study area. The posted speed limit on I-25 in the study area is 65 MPH.

- The I-25 southbound on-off ramp is classified as Major Collector on the 2040 Long Range Roadway System Map. The off ramp is a four-lane road including two (2) southbound shared through left and right turn lanes, one (1) exclusive left turn lane and one exclusive right turn lane with a posted speed limit of 35 MPH; the on-ramp is a two-lane road.
- The I-25 northbound on-off ramp is classified as Major Collector on the 2040 Long Range Roadway System Map. The off ramp is a five-lane road including one (1) northbound through lane, two (2) northbound shared through left and right turn lanes, one (1) exclusive left turn lane and one exclusive right turn lane with no posted speed limit on the ramp; the on-ramp is a three-lane road in the vicinity of the study area.

University Blvd NE is classified as a Minor Arterial Roadway on the Futures 2040 Long Range Roadway System Map. It is generally a four-lane divided roadway with curb and gutter, and sidewalks north of Lomas Blvd, and a six-lane divided roadway with curb and gutter, and sidewalks south of Lomas Blvd. The posted speed limit on University Blvd in the study area is 30 MPH south of Lomas Blvd and 35 MPH north of Lomas Blvd.

Alternative Travel Modes

There are four Primary Routes in the study area along Lomas Blvd that stretch along Lomas Blvd in the Study Area. ABQ Ride does have the following Bus Stops in the close vicinity of the proposed development. They include Rapid Ride Routes

5 – Montgomery / Carlisle / Lomas

11 – Lomas Blvd

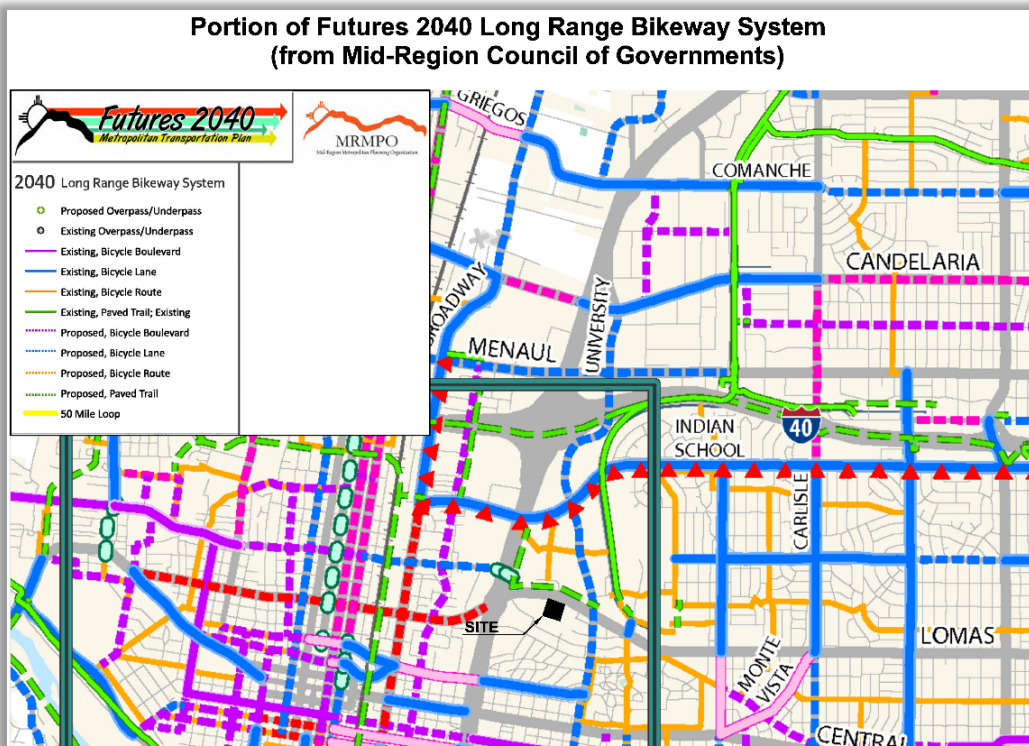
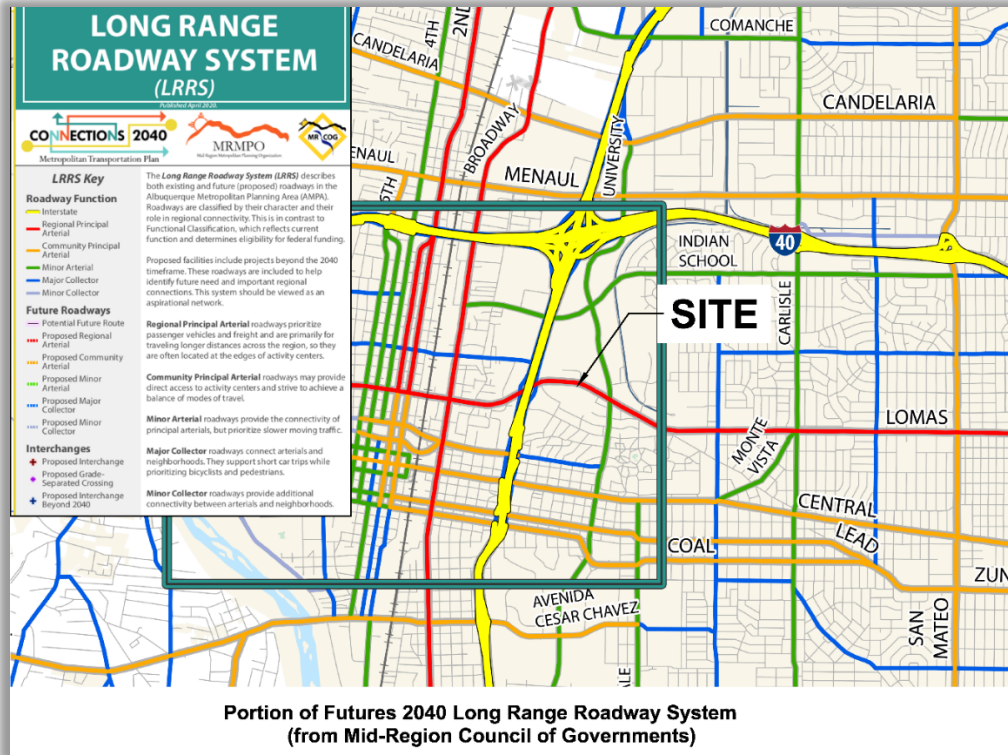
16 – Broadway / University / Gibson (within 1,000 feet of project)

790 – Coors / University

Lomas Blvd NE is proposed to be equipped with bicycle facilities and planned for a future proposed paved trail along the north side of the roadway according to the Futures 2040 Long Range Bikeway System Map.

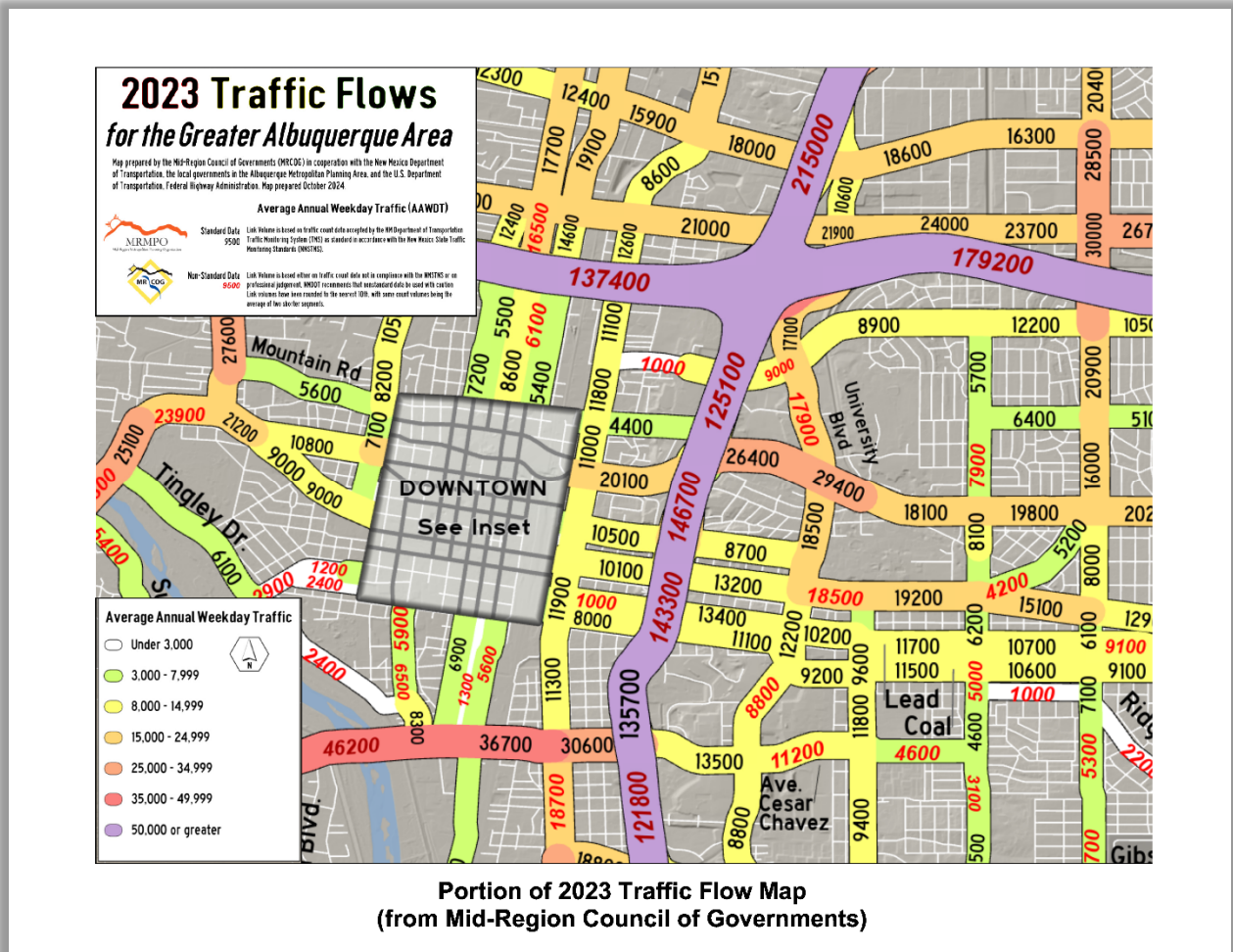
University Blvd NE does not have existing bicycle facilities but is planned for future bicycle facilities according to the Futures 2040 Long Range Bikeway System Map. It is designated for future proposed bicycle lanes.

See the following pages for Portions of the Long Range Roadway Map and the Long Range Bikeway Map:



Analysis of Existing Conditions

Base traffic volumes were projected from the historical annual background traffic growth rates on the 2023 Traffic Flow Map and demonstrated in the following diagram. Existing volumes were not analyzed since 2028 “No Build” analysis and will approximate existing conditions analysis.



Existing Traffic Volumes

Analysis of existing conditions (2025) was not performed for this Study since the Implementation Year (2028) No Build Conditions results should closely approximate the existing conditions analyses because the growth rate is 0.5% (originally -0.3% as per growth forecast based on Mid-Region Council of Government’s Historical AWDT data). Traffic count data for the study area as defined in the Scoping Meeting was collected in August of 2025 while school was in session. Summarized volumes can be found in Appendix Pages A-174 through A-179.

Existing Signal Phasing

The three signals analyzed for the Lobo Plaza include Lomas Blvd NE / I-25 Southbound Ramp, Lomas Blvd NE / I-25 Northbound Ramp, and Lomas Blvd NE / University Blvd NE. The three

signals are coordinated along Lomas Blvd with a cycle length of 110 seconds in the AM and 120 seconds in the PM. Signals are coordinated along the entire study segment of Lomas Blvd NE according to the COA Line spotter webpage ([LineSpotter](#)). Signal detection is as follows for the signalized intersections evaluated for the Lobo Plaza Development:

- 1.) Lomas Blvd NE / I-25 Southbound On-Off Ramp - Signal detection exists, currently operated by Video Camera Detection for EB, WB, and SB movements.
- 2.) Lomas Blvd NE / I-25 Northbound On-Off Ramp - Signal detection exists currently operated by Video Camera Detection for EB, WB, and NB movements.
- 3.) Lomas Blvd NE / University Blvd NE - Signal detection exists currently operated by Loop Detection for EB, WB, NB, and SB movements.

Level of Service (LOS)

According to the City of Albuquerque Design Process Manual (DPM), LOS standards are defined by Access Category. Table 7.5.89 identifies the minimum acceptable LOS standards according to Functional Classification & Roadway Type and City of Albuquerque’s ABC Comp Plan Type (see below).

| Functional Classification & Roadway Type | ABC Comp Plan Center Type | | | | | | |
|--|---------------------------|----------|--------------|-----------------|----------------|-------------------|----------------|
| | Transit Station Area | Downtown | Urban Center | Activity Center | Village Center | Employment Center | Outside Center |
| Premium Transit | E-F | E-F | E-F | E-F | E-F | E-F | E-F |
| Major Transit | E | E-F | E | E | D-E | D-E | D-E |
| Multi-modal | E | E | E | E | D-E | D-E | D-E |
| Commuter | E | E | D-E | D-E | D-E | D-E | D |
| Other Arterial | E | E | E | D-E | D-E | D-E | D |
| Minor Arterial | E | E | D-E | D-E | D-E | D | D |
| Collector | E | D-E | D | D | C-D | C-D | C-D |

Lomas Blvd is considered a Principle Arterial along the frontage of the proposed Lobo Plaza Development in the Study Area. Roadways or intersections that are classified within this study area should have a LOS D or better or mitigated to maintain the LOS (No Build) condition levels.

The NMDOT State Highway Access Management Requirements (SAMM) LOS standards are defined by Access Category on page 51. Table 15.C-1 identifies the minimum acceptable LOS standards by access category and facility type as shown below. Level of service (LOS) F shall not be accepted for individual movements.

| Facility Type ¹ | Access Categories (see Sub-Section 10.D) | | | | | | | |
|----------------------------|--|----------------|----------------|----------------|------|----------------|----------------|----------------|
| | UINT | UPA | UMA | UCOL | RINT | RPA | RMA | RCOL |
| Freeway Sections | D | - | - | - | C | - | - | - |
| Ramp Junctions | D | - ² | - ² | - ² | C | - ² | - ² | - ² |
| Weaving Areas | D | - ² | - ² | - ² | C | - ² | - ² | - ² |
| Multi-lane Highways | - | D | D | C | - | C | C | B |
| Two-Lane Highways | - | D | D | C | - | C | C | B |
| Signalized Intersections | - | D | D | D | - | C | C | C |
| Unsignalized Intersections | - | D | D | D | - | D | D | C |

Notes: 1. The Facility Types are per the Highway Capacity Manual.
2. Evaluate safety and operational concerns using the best available technique.

As shown in Table 15.C-1, all Urban Roadways or intersections that are classified within this study under the jurisdiction of the New Mexico Department of Transportation should have a LOS D or better or mitigated to maintain the LOS (No Build) condition levels.

Analysis of Existing Conditions

Traffic counts to collect turning movement volumes were collected for this project on Tuesday, August 26, 2025, and Wednesday August 27, 2025, while school was in session in Albuquerque. The counts were collected from 7:00 - 9:00 AM and 4:00 - 6:00 PM at the existing study intersections. The traffic count data is included in the Appendix on Pages A-199 through A-205. The analysis for the Current Year (2025) AM and PM Peak Hour Existing Conditions was conducted by applying the collected turning movement volume data to the existing intersections.

The signalized intersections of Lomas Blvd NE & Southbound On-Off Ramp and Lomas Blvd NE & Northbound On-Off Ramp were analyzed in HCS. The signalized intersection of Lomas Blvd NE & University Blvd NE, and the unsignalized intersections were analyzed using Synchro 11. See Appendix pages A-78 through A-91 for the Synchro and A-92 through A-95 for the HCS detailed results of the analysis. The Lane Volumes Analysis Maps (LVAM) maps and results depicting the Synchro analysis results of the signalized and unsignalized intersections are included in the Appendix pages A-36 and A-37 through A-43. Based on the operational results of the existing 2025 conditions summarized in the following table, all intersections are currently operating in an acceptable LOS in the AM and PM Peak Hours.

| Intersection No. /Name | Result Source | Control Type | Condition | 2025 | |
|---|---------------|--------------|-----------|----------|----------|
| | | | | AM Peak | PM Peak |
| 1 - Lomas Blvd NE / I-25 Southbound On-Off Ramp | HCS 2025 | Signalized | EXISTING | D - 46.4 | C - 21.7 |
| 2 - Lomas Blvd NE / I-25 Northbound On-Off Ramp | | Signalized | EXISTING | C - 23.9 | C - 31.5 |
| 3 - Lomas Blvd NE / University Blvd NE | Synchro 12 | Signalized | EXISTING | D - 37.7 | D - 45.5 |
| 4 - Lomas Blvd NE / Medical Arts Ave NE | | Unsignalized | EXISTING | C - 20.6 | B - 10.8 |
| 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | | Unsignalized | EXISTING | B - 12.6 | B - 11.8 |
| 6 - Lomas Blvd NE / Legion Rd / West DWY | | Unsignalized | EXISTING | B - 10.5 | B - 13.1 |
| 7 - Lomas Blvd NE / East DWY | | Unsignalized | EXISTING | A - 9.5 | B - 10.5 |

Note : The LOS / Delay for unsignalized intersections is the highest calculated delay for any movement.

Analysis of Implementation Year and Horizon Year Conditions

Traffic Projections

The anticipated implementation year for this project is 2028 and the Horizon Year is 2038. The Mid-Region Council of Government (MRCOG) Regional Transportation Model data from 2013 to 2022 was used to determine the historical growth rates. The calculated overall **growth rate** at the intersections is -3.0% for the Implementation Year and Horizon Year. Therefore, a 0.5% growth rate has been considered for further analysis. See Appendix A-10 through A-11 for the Historic Growth Rate Data Table.

Background Traffic

Background traffic volumes were calculated by applying historical annual background traffic growth rates to the existing traffic volumes for the implementation year and also the Horizon Year.

Trip Generation

The Implementation Year for this project is 2028 and the Horizon Year is 2038. According to the Institute of Traffic Engineers' (ITE) trip generation rates for the proposed Lobo Plaza, the hotel trips were calculated using Hotel (ITE Code 310) in the analysis. The commercial section for the proposed development includes one High Turnover (Sit-Down) Restaurant (ITE Codes 932) and a Strip Retail Plaza <40K (ITE Codes 822). A 30% pass-by trip rate reduction was applied to the trip generation rates for retail trips only. Vehicle Pass-By Rates by Land Use (ITE, 11th Edition) 822 and 932 were used to calculate Pass-By trip percentages for the Lobo Plaza Retail trips.

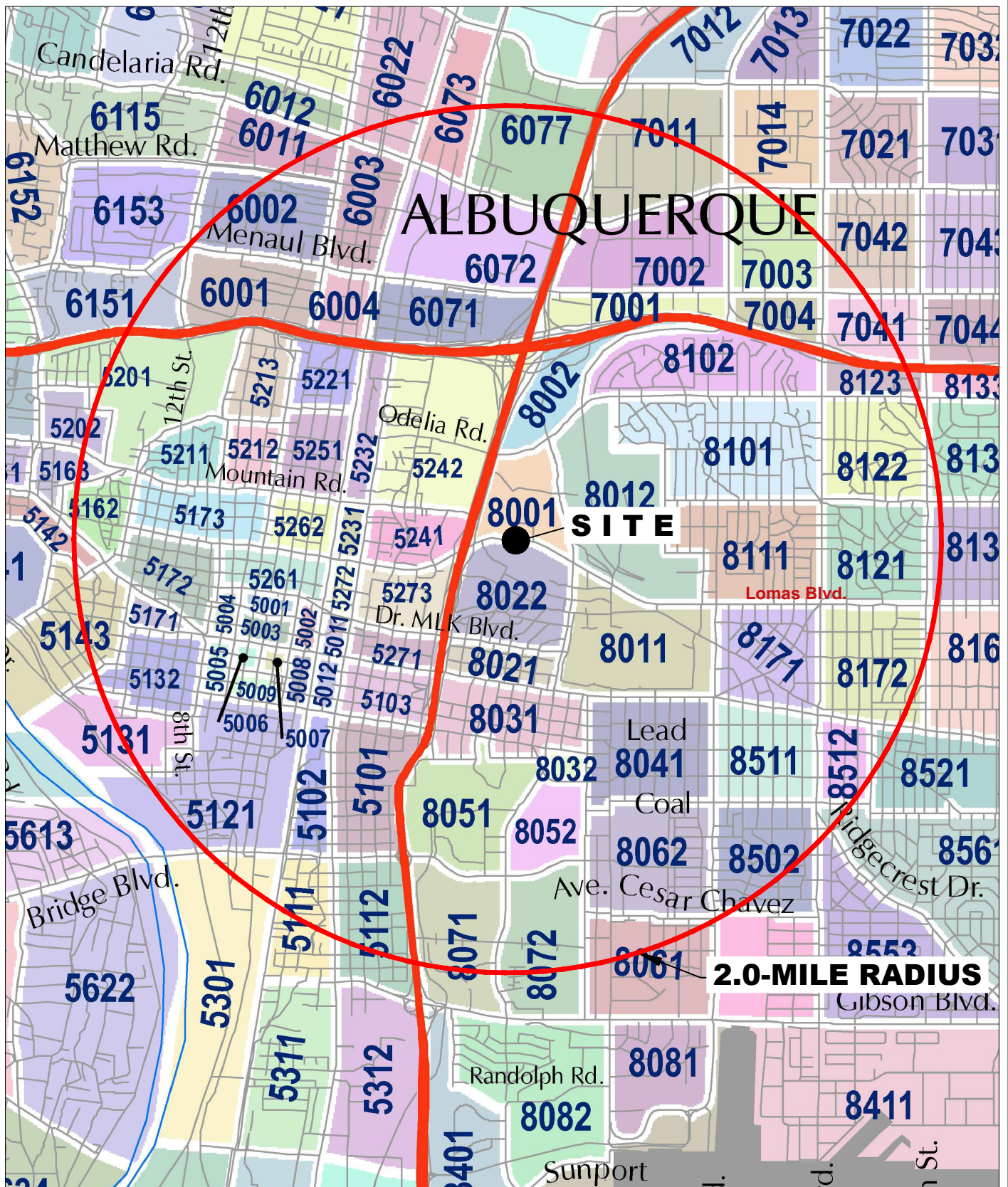
Trip Generation table for the commercial development combining the retail and the hotel trips can be found in the following table:

| <i>Lobo Plaza (Lomas Blvd. / University Blvd.)</i> | | | | | | | | |
|---|---|-------|--------------|-------------|----------------|-------------|----------------|--|
| Trip Generation Data (ITE Trip Generation Manual - 11th Edition) | | | | | | | | |
| Parcel | USE (ITE CODE) | Units | 24 HR VOL | | A. M. PEAK HR. | | P. M. PEAK HR. | |
| | DESCRIPTION | | GROSS | ENTER | EXIT | ENTER | EXIT | |
| Summary Sheet | | | | | | | | |
| North-West Parcel | High Turnover (Sit-Down) Restaurant (932) | 5.98 | 641 | 31 | 26 | 33 | 21 | |
| North-West Parcel | Strip Retail Plaza <40K - Equation (822) | 4.28 | 410 | 10 | 7 | 21 | 21 | |
| | Hotel (310) | 140 | 1,094 | 35 | 28 | 39 | 37 | |
| South-East Parcel | High Turnover (Sit-Down) Restaurant (932) | 5.62 | 602 | 30 | 24 | 31 | 20 | |
| South East Parcel | Strip Retail Plaza <40K - Equation (822) | 4.90 | 436 | 11 | 7 | 23 | 23 | |
| Subtotal | | | 3,183 | 117 | 92 | 147 | 122 | |
| <i>Internal Capture Trip Reduction (Based on OTISS PRO)</i> | | | | -2 | -2 | -22 | -22 | |
| <i>Trip Generation Rate Adjusted For Internal Capture</i> | | | | 115 | 90 | 125 | 100 | |
| <i>Pass-By Trip Reduction (Based on OTISS PRO) 30%</i> | | | | (25) | (19) | (32) | (26) | |
| Net New Trips to Adjacent Roadway System | | | | 90 | 71 | 93 | 74 | |

Trip Distribution and Trip Assignments

Trip assignments percentages for new trips entering and exiting are derived from data established in the trip distribution determination process and logical routing. Both residential and retail commercial trips were distributed based on Mid-Region Council of Governments' Socio-economic data (2016-2040 data set).

The retail commercial trips were assigned based on population distribution within a two-mile radius of the project (Gravity Model). The MRCOG DASZ Maps below from Bernalillo County provide a visual of the data analysis subzones for commercial trips that will be entering and exiting the site. The Data Table and Maps used to calculate the Trip Distribution percentages can be found in Appendix Pages A-12 through A-19 and the maps are shown below on Pages 11 through 17.



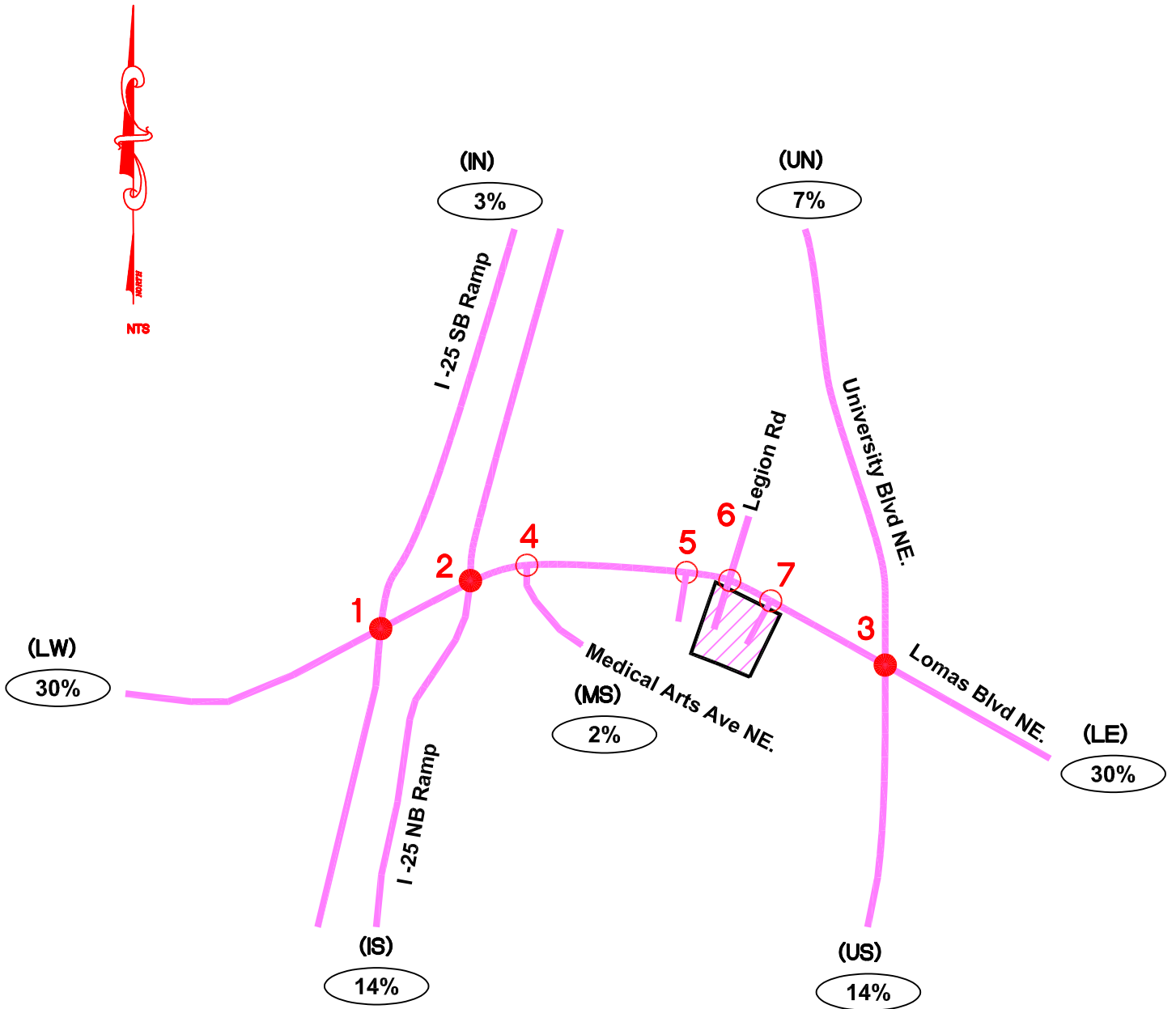
DATA ANALYSIS SUBZONE (DASZ) MAP

Lobo Plaza Development (Lomas Blvd. West of University Blvd.)

Lobo Plaza

1300 Lomas Blvd NE

Trip Distribution Map (%)



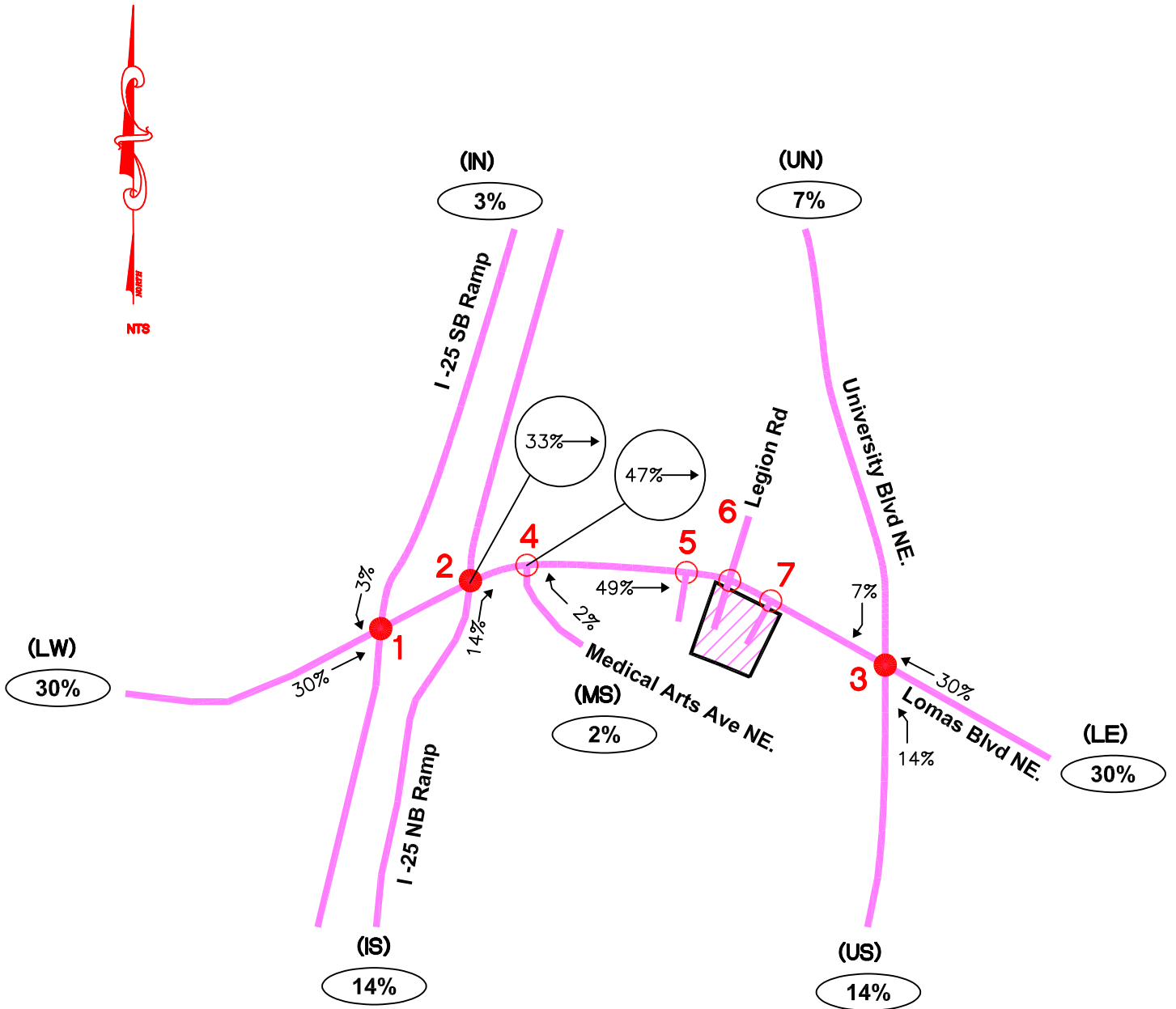
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Entering)



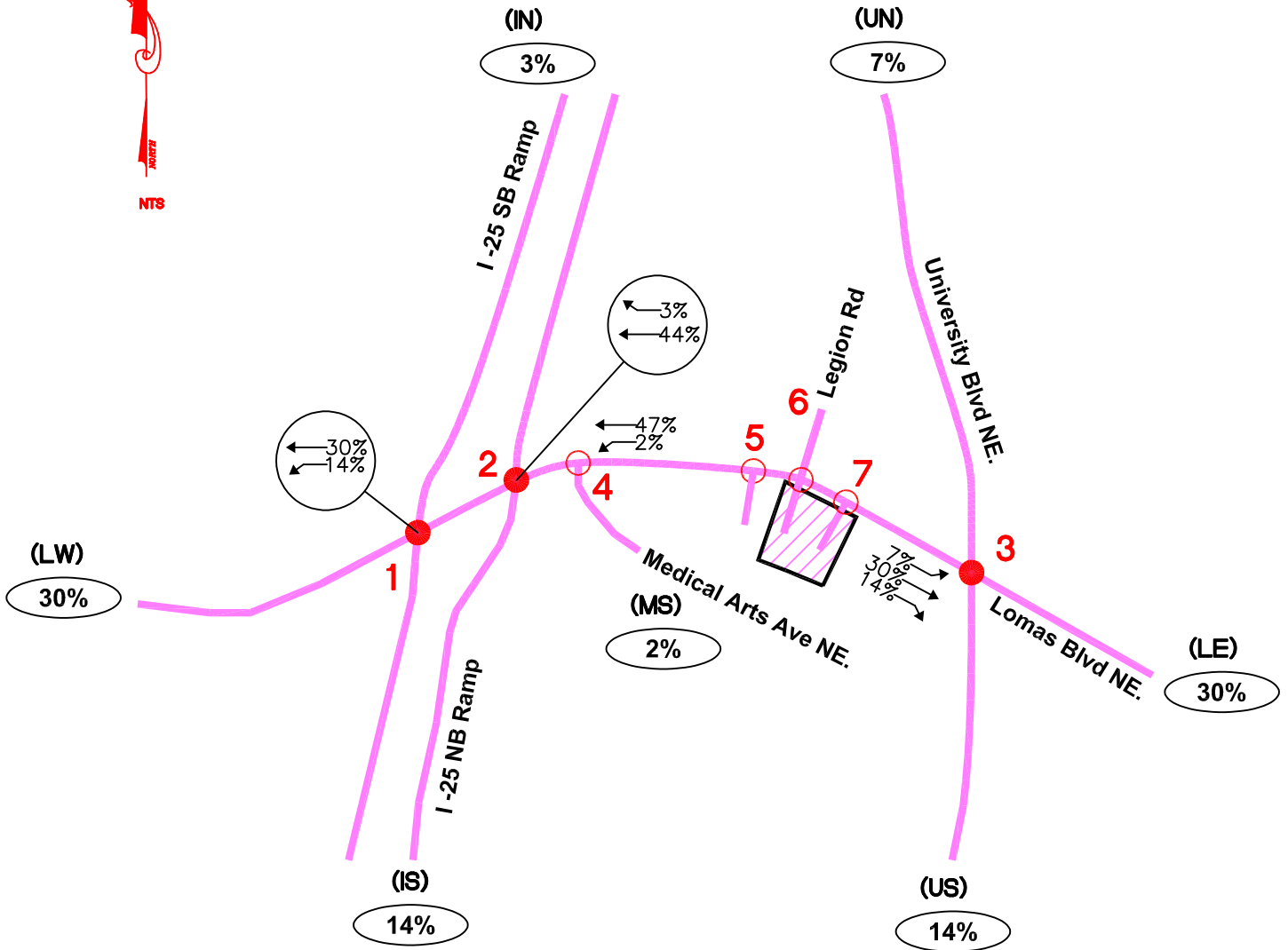
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Exiting)



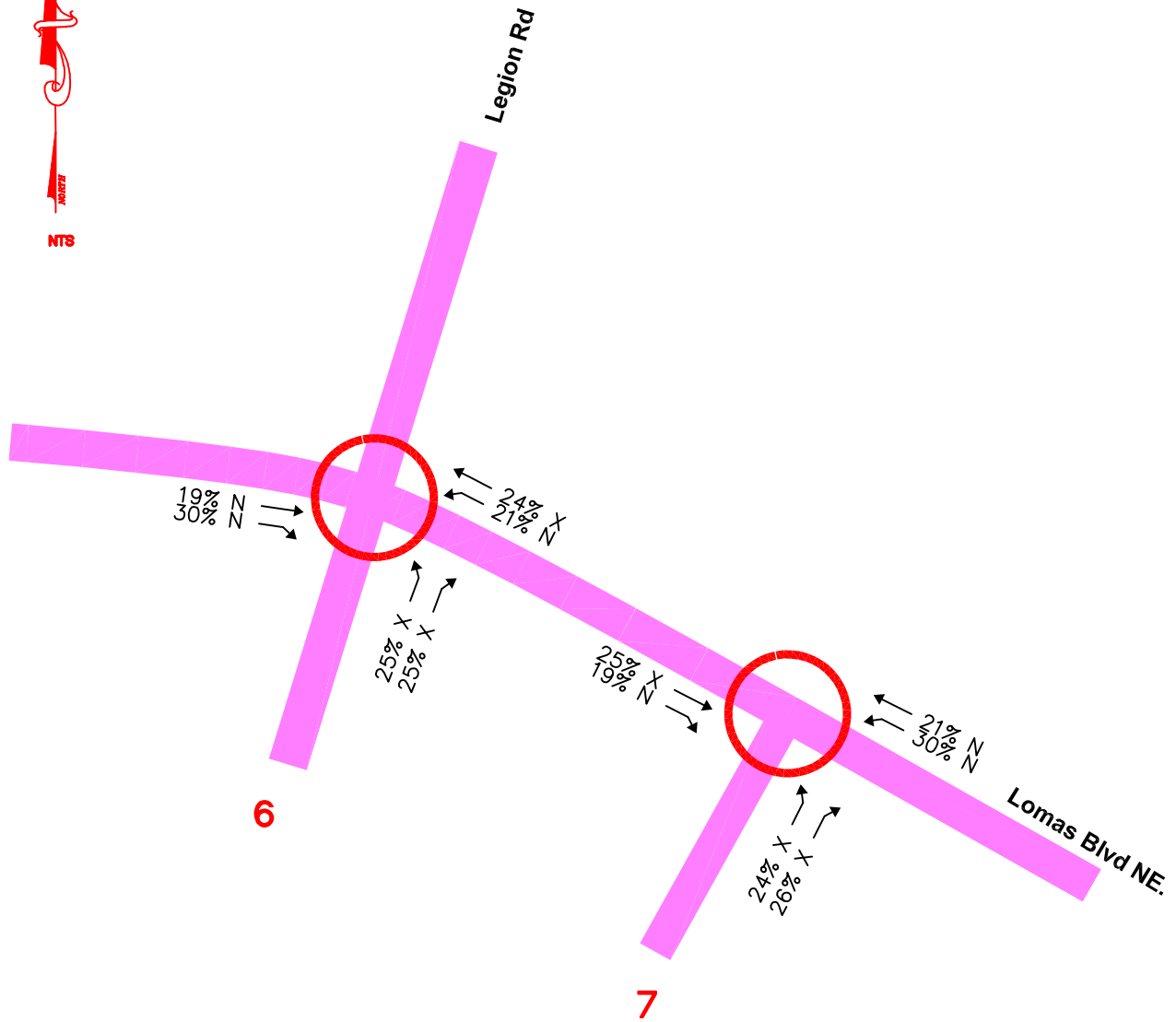
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Entering & % Exiting) DRIVEWAYS



XX% N (INBOUND)
XX% X (OUTBOUND)



SIGNALIZED INTERSECTION



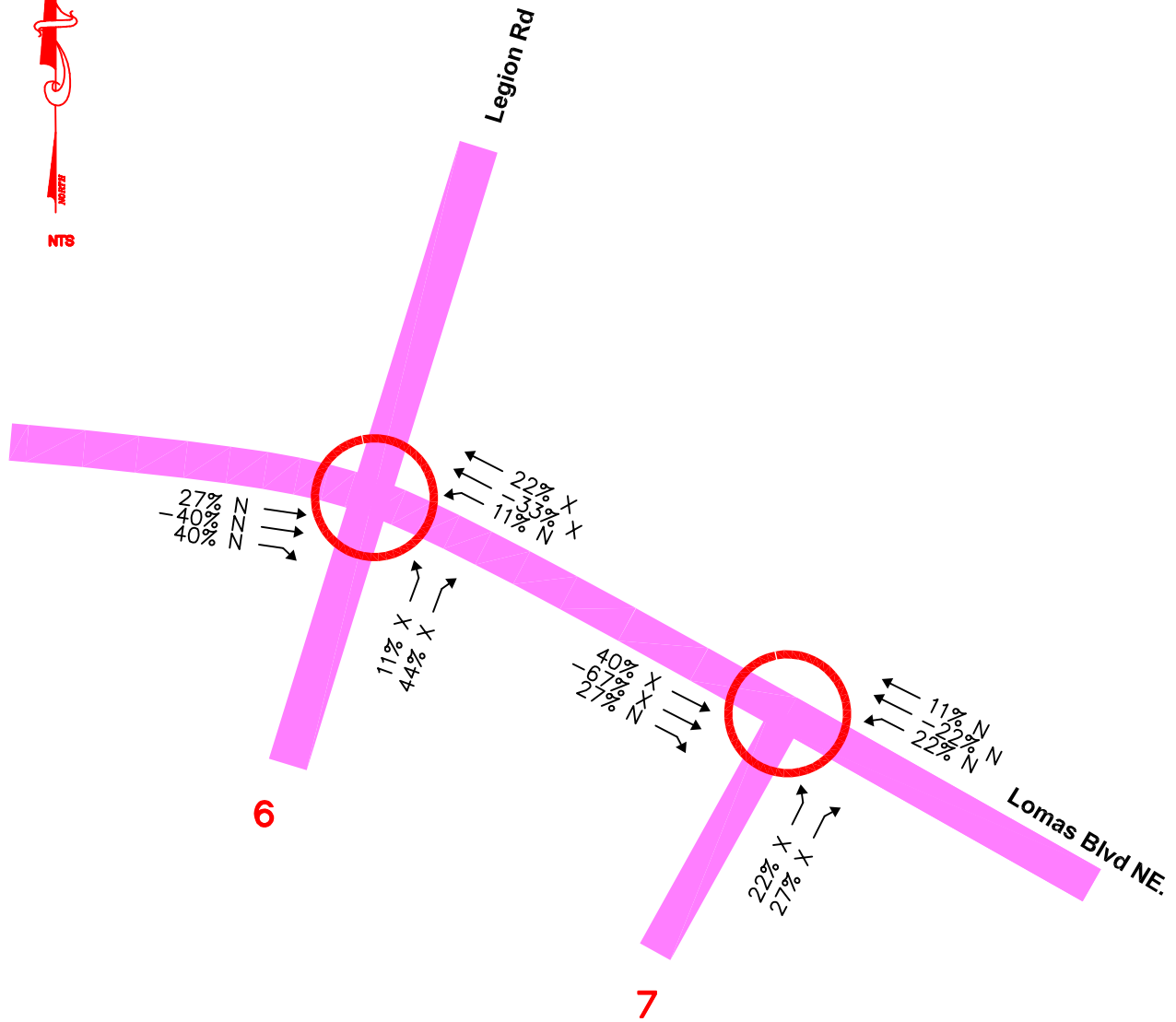
UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Pass-By AM)



XX% N (INBOUND)
XX% X (OUTBOUND)



SIGNALIZED INTERSECTION



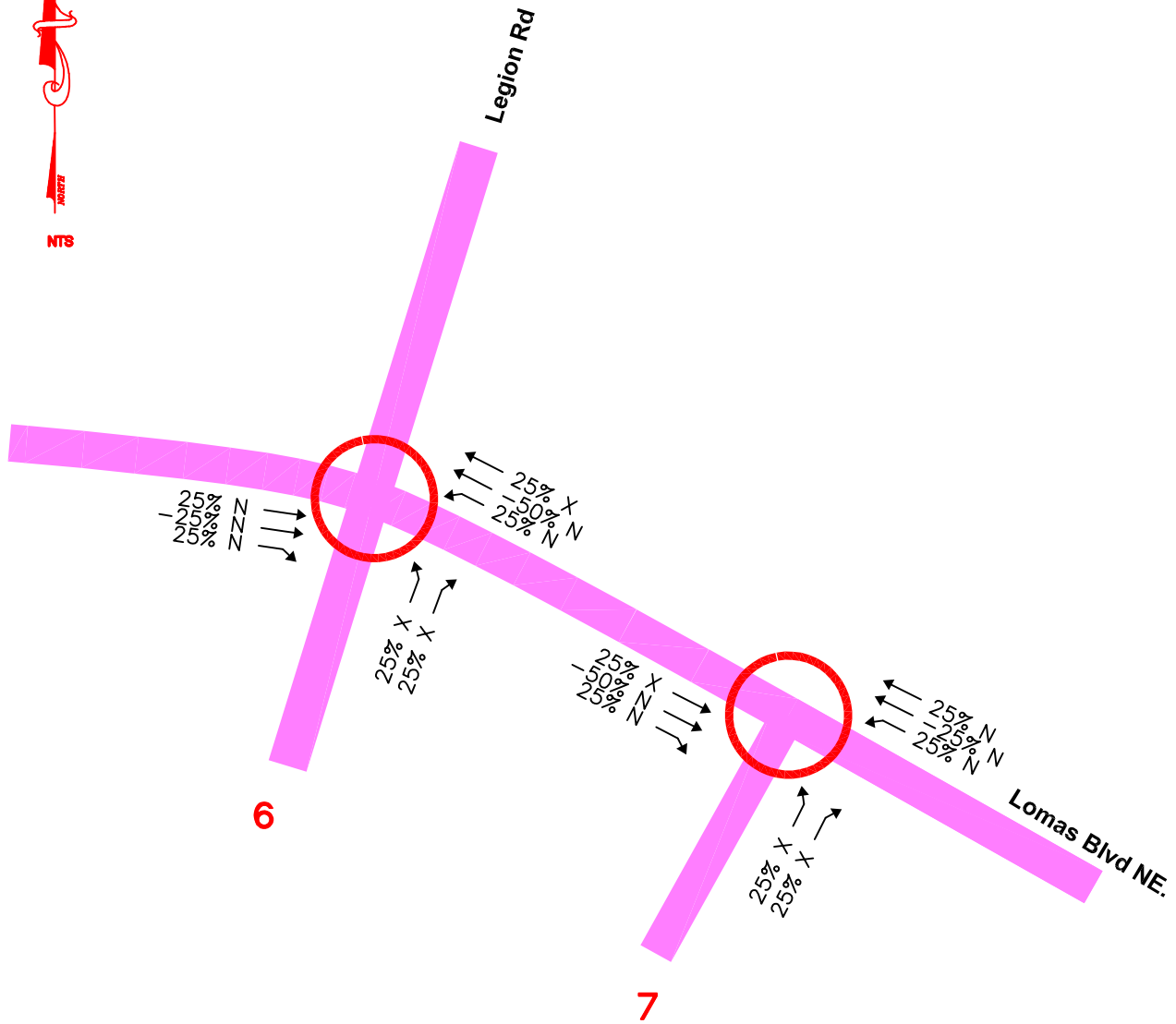
UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Pass-By PM)



XX% N (INBOUND)
XX% X (OUTBOUND)



SIGNALIZED INTERSECTION



UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

NO BUILD and BUILD Traffic Volumes

No Build volumes were generated by growing the existing volumes with the annual background traffic growth rate. Build volumes were calculated by increasing the No Build volumes by the trips generated by the project. The trip assignment percentages were used to distribute the trips generated to the individual traffic movements at each intersection. The turning movement counts for 2028 and 2038 AM, No Build, and Build conditions for each movement in each intersection the study area is provided in the Appendix on Pages A-20 through A-35 and A-53 through A-68.

Traffic Analysis

The Highway Capacity Manual establishes a criterion for the determinations of signalized and unsignalized levels-of-service. These levels determine if an intersection will accommodate the projected volumes from the new development. The average control delay is calculated for each intersection and for each lane group of each leg of the intersection. The analysis of the calculated control delay determines the level-of-service for each lane group. However, if the v/c ratio is 1.0 or greater, then the v/c ratio overrides the calculated delay and qualifies the lane group to be LOS "F". The control delay generally determines the level-of-service based on the following tables:

LEVEL-OF-SERVICE CRITERIA FOR SIGNALIZED INTERSECTIONS

| <u>Average Delay</u> <u>(secs)</u> | <u>Level-of-Service</u> |
|---|--------------------------------|
| ≤ 10 | A |
| > 10 and ≤ 20 | B |
| > 20 and ≤ 35 | C |
| > 35 and ≤ 55 | D |
| > 55 and ≤ 80 | E |
| > 80 | F |

LEVEL-OF-SERVICE CRITERIA FOR UNSIGNALIZED INTERSECTIONS

| <u>Average Delay</u> <u>(secs)</u> | <u>Level-of-Service</u> |
|---|--------------------------------|
| ≤ 10 | A |
| > 10 and ≤ 15 | B |
| > 15 and ≤ 25 | C |
| > 25 and ≤ 35 | D |
| > 35 and ≤ 50 | E |
| > 50 | F |

A level-of-service D or better is considered acceptable in urban areas. A capacity analysis was conducted in accordance with the HCM7 for the signalized and unsignalized intersections using

Synchro 11 and McTrans Highway Capacity Software (HCS) 2023 Street Version 8.2 modeling software.

The signalized intersections of Lomas Blvd NE & Southbound On-Off Ramp and Lomas Blvd NE & Northbound On-Off Ramp were analyzed in HCS. The signalized intersection of Lomas Blvd NE & University Blvd NE, and the unsignalized intersections were analyzed using Synchro 11. See Appendix pages A-96 through A-179 for the Synchro and A-180 through A-199 for the HCS detailed results of the analysis. The Lane Volumes Analysis Maps (LVAM) maps depicting the Synchro analysis results of the signalized and unsignalized intersections of 2028 and 2038, AM and PM No Build, Build and Mitigated Conditions are included in the Appendix Pages A-44 through A-52 and A-69 through A-77.

The following pages contain the Lanes / Volumes Analysis Tables for this study. The Lanes / Volumes Analysis Tables summarize numerically how this project impacts the roadway adjacent system and how those mitigation measures improve operations, and how the project driveways are expected to perform. Further detail is found in the individual Intersection analysis summary tables for each intersection in the next section of the report.

#1 – Lomas Blvd NE / I-25 Southbound On-Off Ramp – Signalized



Figure 1 - Lomas Blvd NE and I-25 Southbound On-Off Ramp

The intersection of Lomas Blvd NE and I-25 Southbound Ramp is a signalized intersection that was analyzed using HCS 2025 software by McTrans. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the signalized intersection of Lomas Blvd NE and I-25 Southbound On-Off Ramp are summarized in the following tables below and on Appendix Pages A-96 and A-107:

Signalized

| 1: Lomas Blvd NE & I-25 SB Ramp 2028 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
|--|--------------------|------|------|--------------------|------|------|-------------------|---|---|-------------------|------|------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD - SIGNALIZED Volumes | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 |
| V/C Ratio | | 0.45 | 0.00 | 0.13 | 0.33 | 0.00 | | | | 1.07 | 0.40 | 1.09 |
| Level-of-Service | | C | | C | C | | | | | F | C | F |
| Control Delay (Seconds) | | 28.0 | | 21.6 | 28.7 | | | | | 82.0 | 21.3 | 91.4 |
| Intersection LOS (HCS Result) | D - 50.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.0 | 0.0 | 1.3 | 10.0 | 0.0 | | | | 42.0 | 9.7 | 40.7 |
| 2028 BUILD - SIGNALIZED Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| V/C Ratio | | 0.46 | 0.00 | 0.17 | 0.34 | 0.00 | | | | 1.07 | 0.40 | 1.09 |
| Level-of-Service | | C | | C | C | | | | | F | C | F |
| Control Delay (Seconds) | | 28.2 | | 22.1 | 28.8 | | | | | 83.3 | 21.3 | 91.4 |
| Intersection LOS (HCS Result) | D - 50.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.3 | 0.0 | 1.6 | 10.2 | 0.0 | | | | 42.5 | 9.7 | 40.7 |
| Mitigate Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| 2028 BUILD MITIGATED - SIGNALIZED Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| V/C Ratio | | 0.70 | 0.00 | 0.28 | 0.48 | 0.00 | | | | 0.82 | 0.31 | 0.83 |
| Level-of-Service | | D | | C | C | | | | | C | B | C |
| Control Delay (Seconds) | | 41.4 | | 32.9 | 26.0 | | | | | 25.7 | 12.1 | 27.5 |
| Intersection LOS (HCS Result) | C - 27.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 12.3 | 0.0 | 2.0 | 8.0 | 0.0 | | | | 24.6 | 7.3 | 23.2 |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|-----|------|------|------|---|---|---|------|------|------|
| 2028 NO BUILD - SIGNALIZED Volumes | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 |
| V/C Ratio | 0.00 | 0.34 | | 0.56 | 0.27 | 0.00 | | | | 0.47 | 0.48 | 0.63 |
| Level-of-Service | | C | | B | B | | | | | D | D | D |
| Control Delay (Seconds) | | 21.3 | | 16.9 | 13.7 | | | | | 42.5 | 40.9 | 48.5 |
| Intersection LOS (HCS Result) | C - 24.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.0 | 0.0 | 8.1 | 8.7 | 0.0 | | | | 9.2 | 9.6 | 11.5 |
| 2028 BUILD - SIGNALIZED Volumes | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 |
| V/C Ratio | 0.00 | 0.36 | | 0.58 | 0.28 | | | | | 0.47 | 0.48 | 0.63 |
| Level-of-Service | | C | | B | B | | | | | D | D | D |
| Control Delay (Seconds) | | 21.4 | | 17.8 | 13.7 | | | | | 42.7 | 40.9 | 48.5 |
| Intersection LOS (HCS Result) | C - 24.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.3 | 0.0 | 8.4 | 8.9 | 0.0 | | | | 9.3 | 9.6 | 11.5 |

2028 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on the LOS and delays for 2028 AM, and PM Peak Hour Build conditions at the intersection of Lomas Blvd NE and I-25 Southbound On-Off Ramp. The delays and LOS at the intersection are expected to remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection in 2028 except for the southbound left turn and right turn movements during the AM Peak hour, therefore mitigations are recommended for AM Peak Hour only.

Signalized

| 1: Lomas Blvd NE & I-25 SB Ramp | | | | | | | | | | | | |
|--|--------------------|------|------|--------------------|------|------|-------------------|---|---|-------------------|------|-------|
| 2038 Conditions (HCS Results) | | | | | | | | | | | | |
| | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD - SIGNALIZED Volumes | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 |
| V/C Ratio | 0.00 | 0.47 | 0.00 | 0.12 | 0.28 | 0.00 | | | | 1.12 | 0.42 | 1.14 |
| Level-of-Service | | C | | C | B | | | | | F | C | F |
| Control Delay (Seconds) | | 28.3 | | 21.6 | 11.9 | | | | | 101.0 | 21.6 | 111.4 |
| Intersection LOS (HCS Result) | E - 57.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.5 | 0.0 | 1.1 | 3.8 | 0.0 | | | | 48.7 | 10.2 | 47.0 |
| 2038 BUILD - SIGNALIZED Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |
| V/C Ratio | 0.00 | 0.49 | 0.00 | 0.15 | 0.29 | 0.00 | | | | 1.12 | 0.42 | 1.14 |
| Level-of-Service | | C | | C | B | | | | | F | C | F |
| Control Delay (Seconds) | | 28.5 | | 22.0 | 12.1 | | | | | 102.5 | 21.6 | 111.4 |
| Intersection LOS (HCS Result) | E - 57.1 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.8 | 0.0 | 1.4 | 4.0 | 0.0 | | | | 49.2 | 10.2 | 47.0 |
| Mitigate Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| 2038 BUILD MITIGATED - SIGNALIZED Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |
| V/C Ratio | 0.00 | 0.76 | 0.00 | 0.29 | 0.50 | 0.00 | | | | 0.86 | 0.32 | 0.88 |
| Level-of-Service | | D | | D | D | | | | | C | B | C |
| Control Delay (Seconds) | | 44.0 | | 35.5 | 35.5 | | | | | 28.7 | 12.2 | 31.1 |
| Intersection LOS (HCS Result) | C - 27.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 13.2 | 0.0 | 2.2 | 12.6 | 0.0 | | | | 27.1 | 7.7 | 25.8 |
| PM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD - SIGNALIZED Volumes | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| V/C Ratio | 0.00 | 0.63 | | 0.54 | 0.26 | 0.00 | | | | 0.49 | 0.51 | 0.66 |
| Level-of-Service | | C | | B | A | | | | | D | D | D |
| Control Delay (Seconds) | | 21.5 | | 15.9 | 2.7 | | | | | 43.1 | 41.4 | 49.8 |
| Intersection LOS (HCS Result) | C - 22.4 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.5 | 0.0 | 6.8 | 1.7 | 0.0 | | | | 9.6 | 10.0 | 12.0 |
| 2038 BUILD - SIGNALIZED Volumes | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 |
| V/C Ratio | 0.00 | 0.37 | | 0.56 | 0.27 | | | | | 0.50 | 0.51 | 0.66 |
| Level-of-Service | | C | | B | A | | | | | D | D | D |
| Control Delay (Seconds) | | 21.6 | | 16.5 | 3.3 | | | | | 43.3 | 41.4 | 49.8 |
| Intersection LOS (HCS Result) | C - 22.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.7 | 0.0 | 6.9 | 2.1 | 0.0 | | | | 9.8 | 10.0 | 12.0 |

2038 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on LOS and delays for 2038 AM, and PM Build conditions at the intersection of Lomas Blvd NE and I-25 Southbound On-Off Ramp. The delays and LOS at the intersection remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate an acceptable LOS at the intersection in 2038 except for the southbound left turn and right turn movements during the AM Peak hour, therefore mitigations are recommended for the AM Peak Hour only.

2028 and 2038 Queueing Analysis demonstrates that. There are long queues projected for the southbound approach during the AM Peak Hour. However, queueing issues are mitigated for the AM Peak Hour by optimizing the signal timing splits while keeping the cycle length the same.

Mitigation: Optimization of Signal timing during the AM Peak Hour is recommended as a mitigation measure at the intersection of Lomas Blvd NE and I-25 Southbound On-Off Ramp for both the Implementation Year and Horizon Year. Results of the mitigated condition analysis during the AM Peak Hour for both the 2028 and 2038 Build conditions shown in the above tables demonstrate that all movements are expected to operate at acceptable level-of-service during recommended signal splits at the signalized intersection of Lomas Blvd NE and I-25 Southbound Ramp. Additionally, optimization of signal split is expected to reduce the V/C ratios to less than 1 as shown in the above tables, which requires no additional multi-period analysis.

#2 – Lomas Blvd NE / I-25 Northbound On-Off Ramp – Signalized



Figure 2 – Lomas Blvd NE and I-25 Northbound On-Off Ramp

The intersection of Lomas Blvd NE and I-25 Northbound On-Off Ramp is a signalized intersection that was analyzed using HCS 2025 software by McTrans. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the signalized intersection of Lomas Blvd NE and I-25 Northbound Ramp are summarized in the following tables below and on Appendix Pages A-108 and A-119:

Signalized

| 2: Lomas Blvd NE & I-25 NB Ramp | | | | | | | | | | | | |
|--|--------------------|-------|---|--------------------|------|------|-------------------|------|------|-------------------|---|---|
| 2028 Conditions (HCS Results) | | | | | | | | | | | | |
| | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD - SIGNALIZED Volumes | 219 | 1,510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| V/C Ratio | 0.35 | 0.47 | | | 0.31 | 0.32 | 0.29 | 0.36 | 0.38 | | | |
| Level-of-Service | B | B | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.6 | 19.8 | | | 22.4 | 23.3 | 32.4 | 32.2 | 34.1 | | | |
| Intersection LOS (HCS Result) | C - 23.4 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 13.7 | | | 6.8 | 6.7 | 5.8 | 7.3 | 6.8 | | | |
| 2028 BUILD - SIGNALIZED Volumes | 219 | 1,540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.26 | 0.40 | | | 0.33 | 0.32 | 0.52 | 0.64 | 0.72 | | | |
| Level-of-Service | A | A | | | C | C | D | D | D | | | |
| Control Delay (Seconds) | 7.7 | 8.7 | | | 22.6 | 23.4 | 44.0 | 44.2 | 47.8 | | | |
| Intersection LOS (HCS Result) | C - 21.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 3.1 | 9.0 | | | 7.2 | 6.8 | 6.7 | 8.4 | 8.4 | | | |

| PM Peak Hour | | | | | | | | | | | | |
|---|-----------------|------|---|---|-------|------|------|------|------|---|---|---|
| 2028 NO BUILD - SIGNALIZED Volumes | 333 | 723 | 0 | 0 | 1,117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| V/C Ratio | 0.70 | 0.24 | | | 0.55 | 0.94 | 0.24 | 0.32 | 0.36 | | | |
| Level-of-Service | C | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 30.6 | 15.1 | | | 28.8 | 57.8 | 33.5 | 33.7 | 35.9 | | | |
| Intersection LOS (HCS Result) | C - 32.1 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 10.7 | 7.4 | | | 13.0 | 27.3 | 5.3 | 7.2 | 7.4 | | | |
| 2028 BUILD - SIGNALIZED Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.71 | 0.25 | | | 0.57 | 0.94 | 0.24 | 0.32 | 0.39 | | | |
| Level-of-Service | C | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 31.9 | 14.9 | | | 29.1 | 58.4 | 33.5 | 33.7 | 36.5 | | | |
| Intersection LOS (HCS Result) | C - 32.3 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 10.7 | 7.6 | | | 13.4 | 27.5 | 5.3 | 7.2 | 7.9 | | | |
| Mitigage Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| 2028 BUILD MITIGATED - SIGNALIZED Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.86 | 0.21 | | | 0.37 | 0.61 | 0.36 | 0.47 | 0.57 | | | |
| Level-of-Service | D | A | | | B | B | D | D | D | | | |
| Control Delay (Seconds) | 39.0 | 7.9 | | | 11.8 | 17.0 | 44.1 | 43.8 | 50.6 | | | |
| Intersection LOS (HCS Result) | C - 21.8 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 12.5 | 4.8 | | | 8.5 | 15.1 | 6.2 | 8.2 | 9.2 | | | |

2028 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on the LOS and delays for 2028 AM, and PM Peak Hour Build conditions at the intersection of Lomas Blvd NE and I-25 Northbound On-Off Ramp. The delays and LOS at the intersection are expected to remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection in 2028 except for the westbound right turn movement during the PM Peak hour condition, therefore mitigations are recommended for PM Peak Hour only.

Signalized

| 2: Lomas Blvd NE & I-25 NB Ramp 2038 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
|--|--------------------|-------|---|--------------------|------|------|-------------------|------|------|-------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD - SIGNALIZED Volumes | 230 | 1,585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| V/C Ratio | 0.37 | 0.48 | | | 0.33 | 0.33 | 0.31 | 0.38 | 0.40 | | | |
| Level-of-Service | B | B | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.7 | 19.7 | | | 22.6 | 23.6 | 32.7 | 32.5 | 34.5 | | | |
| Intersection LOS (HCS Result) | C - 23.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 13.8 | | | 7.2 | 7.1 | 6.1 | 7.7 | 7.2 | | | |
| 2038 BUILD - SIGNALIZED Volumes | 230 | 1,615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.37 | 0.51 | | | 0.34 | 0.33 | 0.31 | 0.38 | 0.42 | | | |
| Level-of-Service | B | C | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.9 | 20.0 | | | 22.8 | 23.6 | 32.7 | 32.5 | 35.2 | | | |
| Intersection LOS (HCS Result) | C - 23.8 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 14.0 | | | 7.5 | 7.1 | 6.1 | 7.7 | 7.8 | | | |

| PM Peak Hour | | | | | | | | | | | | |
|---|-----------------|------|---|---|-------|------|------|------|------|---|---|---|
| 2038 NO BUILD - SIGNALIZED Volumes | 349 | 758 | 0 | 0 | 1,172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| V/C Ratio | 0.49 | 0.20 | | | 0.58 | 0.98 | 0.49 | 0.64 | 0.73 | | | |
| Level-of-Service | C | A | | | C | E | D | D | D | | | |
| Control Delay (Seconds) | 27.3 | 8.9 | | | 29.3 | 67.4 | 48.0 | 48.6 | 52.8 | | | |
| Intersection LOS (HCS Result) | D - 35.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 17.6 | 6.2 | | | 13.7 | 30.6 | 6.6 | 8.8 | 9.0 | | | |
| 2038 BUILD - SIGNALIZED Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.76 | 0.26 | | | 0.60 | 0.99 | 0.26 | 0.34 | 0.41 | | | |
| Level-of-Service | D | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 38.4 | 14.8 | | | 29.7 | 68.2 | 33.7 | 33.9 | 36.9 | | | |
| Intersection LOS (HCS Result) | C - 34.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 11.4 | 7.8 | | | 14.1 | 30.8 | 5.6 | 7.6 | 8.3 | | | |
| Mitigate Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| 2038 BUILD MITIGATED - SIGNALIZED Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.94 | 0.23 | | | 0.39 | 0.64 | 0.37 | 0.49 | 0.60 | | | |
| Level-of-Service | D | A | | | B | B | D | D | D | | | |
| Control Delay (Seconds) | 54.7 | 8.1 | | | 11.9 | 17.8 | 44.5 | 44.1 | 51.8 | | | |
| Intersection LOS (HCS Result) | C - 23.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 16.6 | 5.1 | | | 8.9 | 16.1 | 6.6 | 8.5 | 9.7 | | | |

2038 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on LOS and delays for 2038 AM, and PM Build conditions at the intersection of Lomas Blvd NE and I-25 Northbound On-Off Ramp. The delays and LOS at the intersection remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate an acceptable LOS at the intersection in 2038 except for the westbound right turn movement during the PM Peak hour, therefore mitigations are recommended for the PM Peak Hour only.

2028 and 2038 Queuing Analysis demonstrates that no additional queuing capacity is needed for the intersection of Lomas Blvd NE and I-25 Northbound On-Off Ramp for 2028 and 2038.

Mitigation: Optimization of Signal Split during the PM Peak Hour is recommended as a mitigation measure at the intersection of Lomas Blvd NE and I-25 Northbound On-Off Ramp for both the Implementation Year and Horizon Year. Results of the mitigated condition analysis during the PM

Peak Hour for both the 2028 and 2038 Build conditions shown in the above tables demonstrate that all movements are expected to operate at acceptable level-of-service during recommended signal splits at the signalized intersection of Lomas Blvd NE and I-25 Northbound Ramp.

#3 – Lomas Blvd NE/ University Blvd NE – Signalized



Figure 3 – Lomas Blvd NE and University Blvd NE

The intersection of Lomas Blvd NE and University Blvd NE is a signalized intersection that was analyzed using Synchro 12 software by Trafficware. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the signalized intersection of Lomas Blvd NE and University Blvd NE are summarized in the following tables below and on Appendix Pages A-120 through A-131.

Signalized

| 3: Lomas Blvd NE & University Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (University Blvd NE) | | | SB (University Blvd NE) | | |
|---|--------------------|------|------|--------------------|------|------|-------------------------|------|------|-------------------------|------|------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD - SIGNALIZED Volumes | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| V/C Ratio | 0.60 | 0.59 | 0.68 | 0.68 | 0.37 | 0.38 | 0.51 | 0.53 | 0.35 | 0.40 | 0.89 | 0.89 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 18.1 | 39.5 | 44.9 | 25.2 | 23.0 | 24.0 | 33.1 | 38.0 | 29.9 | 29.4 | 61.4 | 61.0 |
| Intersection LOS | D - 38.3 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.5 | 18.1 | 20.2 | 4.9 | 8.3 | 8.5 | 3.5 | 9.6 | 7.0 | 4.0 | 19.0 | 19.4 |
| 2028 BUILD - SIGNALIZED Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| V/C Ratio | 0.62 | 0.60 | 0.69 | 0.69 | 0.39 | 0.40 | 0.57 | 0.53 | 0.35 | 0.40 | 0.92 | 0.92 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 18.5 | 39.9 | 45.6 | 25.8 | 23.3 | 24.3 | 33.6 | 38.0 | 29.9 | 29.4 | 66.7 | 66.4 |
| Intersection LOS | D - 39.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.6 | 18.5 | 20.6 | 5.0 | 8.6 | 8.8 | 4.0 | 9.6 | 7.0 | 4.0 | 19.9 | 20.2 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2028 BUILD MITIGATED - SIGNALIZED Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| V/C Ratio | 0.68 | 0.69 | 0.79 | 0.76 | 0.52 | 0.53 | 0.43 | 0.40 | 0.30 | 0.33 | 0.68 | 0.68 |
| Level-of-Service | C | B | C | D | C | D | C | C | C | C | D | D |
| Control Delay (Seconds) | 21.4 | 12.8 | 20.8 | 44.0 | 32.8 | 35.0 | 25.3 | 28.9 | 24.3 | 22.7 | 37.5 | 37.4 |
| Intersection LOS | C - 26.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.3 | 6.4 | 8.7 | 4.8 | 10.1 | 10.5 | 3.4 | 8.4 | 6.1 | 3.5 | 15.3 | 15.6 |

PM Peak Hour

| | | | | | | | | | | | | |
|---|-----------------|------|------|------|-------|------|------|------|------|------|------|------|
| 2028 NO BUILD - SIGNALIZED Volumes | 175 | 499 | 179 | 215 | 1,031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| V/C Ratio | 0.60 | 0.39 | 0.40 | 0.55 | 0.61 | 0.61 | 0.90 | 0.53 | 0.40 | 0.49 | 1.01 | 1.01 |
| Level-of-Service | C | D | D | C | C | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 27.9 | 42.3 | 43.7 | 24.1 | 33.5 | 35.6 | 58.6 | 34.7 | 26.0 | 28.5 | 84.7 | 86.3 |
| Intersection LOS | D - 46.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.2 | 11.2 | 11.4 | 7.1 | 14.1 | 15.1 | 10.5 | 11.9 | 9.4 | 5.6 | 28.2 | 26.5 |
| 2028 BUILD - SIGNALIZED Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| V/C Ratio | 0.63 | 0.42 | 0.43 | 0.57 | 0.64 | 0.64 | 0.90 | 0.52 | 0.40 | 0.48 | 1.02 | 1.02 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 29.2 | 43.4 | 45.0 | 25.2 | 35.1 | 37.5 | 61.3 | 33.9 | 25.3 | 28.3 | 86.7 | 88.4 |
| Intersection LOS | D - 47.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.5 | 11.7 | 11.9 | 7.3 | 14.7 | 15.8 | 12.0 | 11.8 | 9.3 | 5.6 | 28.6 | 26.9 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2028 BUILD MITIGATED - SIGNALIZED Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| V/C Ratio | 0.69 | 0.42 | 0.43 | 0.69 | 0.72 | 0.72 | 0.87 | 0.44 | 0.38 | 0.48 | 0.85 | 0.85 |
| Level-of-Service | D | D | D | D | D | D | D | C | C | C | D | D |
| Control Delay (Seconds) | 36.5 | 43.4 | 45.0 | 38.8 | 40.4 | 44.0 | 47.0 | 27.5 | 23.1 | 25.9 | 50.8 | 51.9 |
| Intersection LOS | D - 40.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.3 | 11.7 | 11.9 | 5.1 | 15.7 | 17.0 | 9.9 | 10.7 | 8.9 | 5.3 | 22.6 | 21.3 |

2028 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on the LOS and delays for 2028 AM, and PM Peak Hour Build conditions at the intersection of Lomas Blvd NE and University Blvd NE. The delays and LOS at the intersection are expected to remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection in 2028 except for the southbound through and right turn movements during the AM Peak Hour Condition and northbound left turn movement during the PM Peak Hour condition; therefore, mitigations are recommended for both the AM and PM Peak Hours.

Signalized

| 3: Lomas Blvd NE & University Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (University Blvd NE) | | | SB (University Blvd NE) | | |
|--|---------------------------|------|------|---------------------------|-------|------|--------------------------------|------|------|--------------------------------|-------|-------|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD - SIGNALIZED Volumes | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| V/C Ratio | 0.64 | 0.63 | 0.72 | 0.73 | 0.40 | 0.41 | 0.56 | 0.56 | 0.37 | 0.43 | 0.93 | 0.93 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 19.1 | 40.8 | 47.3 | 27.4 | 23.9 | 25.0 | 33.8 | 38.5 | 29.9 | 29.6 | 68.6 | 68.1 |
| Intersection LOS | D - 40.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.0 | 19.0 | 21.3 | 5.4 | 8.8 | 9.0 | 3.7 | 10.0 | 7.3 | 4.2 | 20.8 | 21.2 |
| 2038 BUILD - SIGNALIZED Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| V/C Ratio | 0.66 | 0.64 | 0.74 | 0.74 | 0.41 | 0.42 | 0.61 | 0.56 | 0.37 | 0.43 | 0.97 | 0.97 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 19.8 | 41.3 | 48.1 | 28.2 | 24.2 | 25.3 | 34.3 | 38.5 | 29.9 | 29.6 | 75.5 | 75.1 |
| Intersection LOS | D - 42.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.1 | 19.4 | 21.8 | 5.4 | 9.1 | 9.3 | 4.2 | 10.0 | 7.3 | 4.2 | 21.9 | 22.3 |
| Mitigage Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2038 BUILD MITIGATED - SIGNALIZED Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| V/C Ratio | 0.70 | 0.72 | 0.83 | 0.74 | 0.51 | 0.52 | 0.50 | 0.45 | 0.32 | 0.37 | 0.77 | 0.77 |
| Level-of-Service | C | B | C | D | C | C | C | C | C | C | D | D |
| Control Delay (Seconds) | 20.4 | 13.3 | 22.9 | 35.1 | 31.6 | 33.6 | 27.9 | 31.4 | 25.0 | 24.5 | 44.0 | 43.8 |
| Intersection LOS | C - 28.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.4 | 6.7 | 9.3 | 7.2 | 10.3 | 10.7 | 3.7 | 9.1 | 6.6 | 3.8 | 17.2 | 17.5 |
| PM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD - SIGNALIZED Volumes | 183 | 524 | 187 | 226 | 1,082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| V/C Ratio | 0.65 | 0.42 | 0.43 | 0.59 | 0.66 | 0.66 | 0.90 | 0.55 | 0.42 | 0.52 | 1.06 | 1.06 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 29.8 | 43.6 | 45.2 | 25.6 | 35.6 | 38.1 | 60.9 | 34.9 | 25.7 | 28.6 | 98.9 | 100.7 |
| Intersection LOS | D - 50.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.7 | 11.7 | 12.0 | 7.7 | 15.1 | 16.3 | 11.0 | 12.5 | 9.7 | 5.9 | 31.5 | 29.6 |
| 2038 BUILD - SIGNALIZED Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| V/C Ratio | 0.68 | 0.45 | 0.46 | 0.61 | 0.69 | 0.69 | 0.91 | 0.54 | 0.41 | 0.51 | 1.07 | 1.07 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 31.4 | 44.7 | 46.5 | 27.0 | 37.3 | 40.3 | 63.5 | 34.1 | 24.9 | 28.4 | 101.4 | 103.2 |
| Intersection LOS | D - 52.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.0 | 12.3 | 12.5 | 7.8 | 15.8 | 17.1 | 11.6 | 12.3 | 9.6 | 5.9 | 32.0 | 30.1 |
| Mitigage Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2038 BUILD MITIGATED - SIGNALIZED Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| V/C Ratio | 0.82 | 0.45 | 0.46 | 0.80 | 0.76 | 0.77 | 0.90 | 0.46 | 0.40 | 0.47 | 0.86 | 0.86 |
| Level-of-Service | D | D | D | D | D | D | D | C | C | C | D | D |
| Control Delay (Seconds) | 52.3 | 44.6 | 46.3 | 53.0 | 42.5 | 47.0 | 51.8 | 27.9 | 24.1 | 23.4 | 49.7 | 50.8 |
| Intersection LOS | D - 42.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.9 | 12.2 | 12.4 | 8.4 | 16.8 | 18.3 | 10.6 | 11.2 | 9.5 | 5.3 | 23.4 | 22.0 |

2038 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on LOS and delays for 2038 AM, and PM Peak Hour Build conditions at the intersection of Lomas Blvd NE and University Blvd NE. The delays and LOS at the intersection are expected to remain virtually the same for the No Build and Build Conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection in 2038 except for the southbound through and right turn movements during the AM Peak Hour condition and northbound left turn movement during the PM Peak Hour condition; therefore, mitigations are recommended for both the AM and PM Peak Hours.

2028 and 2038 Queueing Analysis demonstrates that V/C ratios are less than 1 for all movements in the AM, and PM Peak Hour No Build and Build condition, although the V/C ratios for the southbound through and right turn movements during AM and northbound left turn movement during the PM Peak hour conditions were close to 1.

2028 and 2038 Queueing Analysis demonstrates that additional queueing capacity is needed for the EBL in 2028 and 2038. Extending the left-turn lane at 7 Bar Loop Development is not advisable, as it does not contribute to improving traffic flow. Additionally, V/C ratios remain below 1 for all movements AM, Noon, and PM for both No Build and Build scenarios.

Mitigation: Optimization of Signal Split during both the AM and PM Peak Hour conditions is recommended as a mitigation measure at the intersection of Lomas Blvd NE and University Blvd NE for both the Implementation Year and Horizon Year. Results of the mitigated condition analysis during the AM and PM Peak Hours for both the 2028 and 2038 Build conditions shown in the above tables demonstrate that all movements are expected to operate at acceptable level-of-service during recommended signal splits at the intersection. Additionally, optimization of signal split is expected to reduce the V/C ratios as shown in the above tables.

#4 – Lomas Blvd NE/ Medical Arts Ave NE – Unsignalized



Figure 4 – Lomas Blvd NE and Medical Arts Ave NE

The intersection of Lomas Blvd NE and Medical Arts Ave NE is an unsignalized right-in, right-out, left-in only driveway. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the unsignalized intersection of Lomas Blvd NE and Medical Arts Ave NE are summarized in the following tables below and on Appendix paged A-132 and A-143.

Unsignalized

| 4: Lomas Blvd NE & Medical Arts Ave NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Medical Arts Ave NE) | | | SB (Medical Arts Ave NE) | | |
|--|--------------------|-------|-----|--------------------|-----|---|--------------------------|---|------|--------------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 0 | | 1 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | | 1,472 | 256 | 73 | 761 | | 0 | | 69 | | | |
| V/C Ratio | | | | 0.25 | | | | | 0.09 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 21.4 | | | | | 10.4 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.0 | | | | | 0.3 | | | |
| 2028 BUILD Volumes | | 1,514 | 256 | 74 | 794 | | 0 | | 71 | | | |
| V/C Ratio | | | | 0.27 | | | | | 0.10 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 22.9 | | | | | 10.6 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.1 | | | | | 0.3 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|----|------|-------|--|---|--|------|--|--|--|
| 2028 NO BUILD Volumes | | 759 | 69 | 28 | 1,689 | | 0 | | 97 | | | |
| V/C Ratio | | | | 0.04 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 10.9 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |
| 2028 BUILD Volumes | | 803 | 69 | 29 | 1,724 | | 0 | | 99 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 11.0 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |

Unsignalized

| 4: Lomas Blvd NE & Medical Arts Ave NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Medical Arts Ave NE) | | | SB (Medical Arts Ave NE) | | |
|---|---------------------------|-------|-----|---------------------------|-----|---|---------------------------------|---|------|---------------------------------|---|---|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 0 | | 1 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | | 1,544 | 268 | 77 | 799 | | 0 | | 72 | | | |
| V/C Ratio | | | | 0.28 | | | | | 0.10 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 23.4 | | | | | 10.5 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.1 | | | | | 0.3 | | | |
| 2038 BUILD Volumes | | 1,586 | 268 | 78 | 832 | | 0 | | 74 | | | |
| V/C Ratio | | | | 0.31 | | | | | 0.10 | | | |
| Level-of-Service | | | | D | | | | | B | | | |
| Control Delay (Seconds) | | | | 25.1 | | | | | 10.5 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.2 | | | | | 0.3 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|----|------|-------|--|---|--|------|--|--|--|
| 2038 NO BUILD Volumes | | 797 | 72 | 30 | 1,772 | | 0 | | 102 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 10.9 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |
| 2038 BUILD Volumes | | 841 | 72 | 31 | 1,807 | | 0 | | 104 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.13 | | | |
| Level-of-Service | | | | B | | | | | B | | | |
| Control Delay (Seconds) | | | | 11.3 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.2 | | | | | 0.4 | | | |

2028 and 2038 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on the LOS and delays for the 2038 AM and PM Build conditions. The LOS remains virtually the same for the No Build and Build conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection. Therefore, mitigations are not recommended at the unsignalized intersection of Lomas Blvd NE and Medical Arts Ave NE.

2028 and 2038 Queueing Analysis demonstrates that no additional queueing capacity is needed for the intersection of Lomas Blvd NE and Medical Arts Ave NE for 2028 and 2038. V/C ratios are also less than 1 for all movements in the AM, and PM for No Build and Build conditions. No mitigations are recommended for the unsignalized intersection of Lomas Blvd NE and Medical Arts Ave NE.

Therefore, no recommendation is made for the intersection of Lomas Blvd / Medical Arts Ave NE.

#5 – Lomas Blvd NE / Torc Dwy / Frontage Rd – Unsignalized

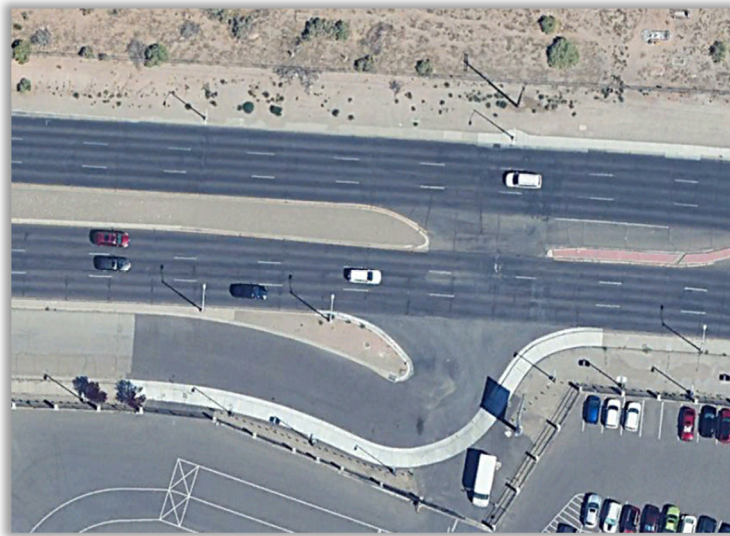


Figure 5 - 7 Bar Loop S and Driveway "A"

The intersection of Lomas Blvd NE and Torc Dwy / Frontage Rd is an unsignalized intersection. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the unsignalized intersection of Lomas Blvd NE and Torc Dwy / Frontage Rd are summarized in the following tables below and on Appendix Pages A-144 and A-155.

Unsignalized

| 5: Torc DWY / Frontage Rd & Lomas Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Torc DWY / Frontage Rd) | | | SB (Torc DWY / Frontage Rd) | | |
|---|--------------------|-------|---|--------------------|-----|---|-----------------------------|---|---|-----------------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 1> | | 0 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | | 1,583 | 0 | 4 | 784 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 12.5 | | | 13.0 | | | | | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | 0.1 | | | | | |
| 2028 BUILD Volumes | | 1,627 | 0 | 4 | 819 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 13.0 | | | 13.9 | | | | | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|--|-----|---|------|-------|--|------|--|---|--|--|--|
| 2028 NO BUILD Volumes | | 901 | 0 | 4 | 1,713 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 11.8 | | | | | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2028 BUILD Volumes | | 947 | 0 | 4 | 1,749 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 12.0 | | | | | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

Unsignalized

| 5: Torc DWY / Frontage Rd & Lomas Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Torc DWY / Frontage Rd) | | | SB (Torc DWY / Frontage Rd) | | |
|---|--------------------|-------|---|--------------------|-----|---|-----------------------------|---|---|-----------------------------|---|---|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 1> | | 0 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | | 1,661 | 0 | 4 | 822 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 12.7 | | | 13.5 | | | | | |
| Intersection LOS | | | | | | | TWSC | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2038 BUILD Volumes | | 1,705 | 0 | 4 | 857 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 13.3 | | | 13.5 | | | | | |
| Intersection LOS | | | | | | | TWSC | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.1 | | | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|--|-----|---|------|-------|--|-------------|--|---|--|--|--|
| 2038 NO BUILD Volumes | | 946 | 0 | 4 | 1,798 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 12.0 | | | | | |
| Intersection LOS | | | | | | | TWSC | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2038 BUILD Volumes | | 992 | 0 | 4 | 1,834 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 10.2 | | | 12.0 | | | | | |
| Intersection LOS | | | | | | | TWSC | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

Both Implementation Year and the Horizon Year analysis in the above tables show that the unsignalized intersection of Lomas Blvd NE and Torc Dwy / Frontage Rd is operating at an acceptable level-of-service for all conditions evaluated in this study. Therefore, no recommendation is made for the intersection of Lomas Blvd / Torc Driveway.

#6 – Lomas Blvd NE / Legion Rd / West Driveway – Unsignalized

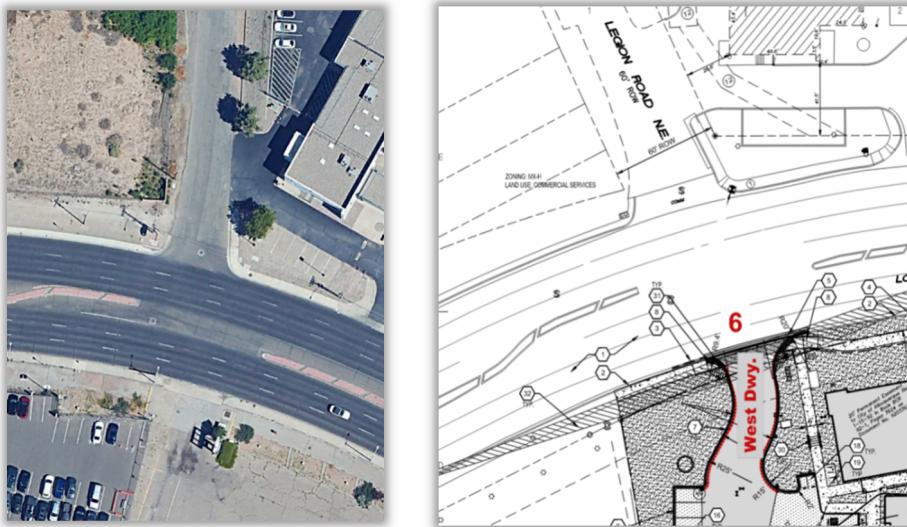


Figure 6 – Lomas Blvd NE and Legion Rd / West Driveway

The intersection of Lomas Blvd NE and Legion Rd / West Driveway is an unsignalized intersection. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the unsignalized intersection Lomas Blvd NE and Legion Rd / West Driveway are summarized in the following tables below and on Appendix Pages A-156 and A-167. The scenario, mitigated condition for this intersection considered as the operation of the driveway if staged left turns are executed.

Unsignalized

| 6: Lomas Blvd NE & Legion Rd / West DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Legion Rd / West DWY) | | | SB (Legion Rd / West DWY) | | |
|---|--------------------|-------|----|--------------------|-----|---|---------------------------|------|----|---------------------------|------|----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 20 | 1,563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | | | | | | | | 0.03 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 9.7 | | | 0.0 | | | | 0.0 | | | 10.5 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.1 | |
| 2028 BUILD Volumes | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.10 | | | 0.03 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 9.8 | | | 13.1 | | | | 13.6 | | | 11.1 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.12 | | | 0.03 | |
| Level-of-Service | A | | | B | | | | C | | | B | |
| Control Delay (Seconds) | 9.8 | | | 13.1 | | | | 15.9 | | | 11.1 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.4 | | | 0.1 | |

PM Peak Hour

| | | | | | | | | | | | | |
|-------------------------------------|------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2028 NO BUILD Volumes | 12 | 885 | 4 | 0 | 1,677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | | | | | 0.01 | | | 0.07 | |
| Level-of-Service | B | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 12.8 | | | 0.0 | | | | 9.7 | | | 11.3 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.2 | |
| 2028 BUILD Volumes | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.10 | | | 0.08 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 12.9 | | | 10.4 | | | | 11.7 | | | 11.5 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.2 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.10 | | | 0.08 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 12.9 | | | 10.4 | | | | 11.9 | | | 12.2 | |
| Intersection LOS | | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.3 | |

Unsignalized

| 6: Lomas Blvd NE & Legion Rd / West DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Legion Rd / West DWY) | | | SB (Legion Rd / West DWY) | | |
|--|---------------------------|-------|----|---------------------------|-----|---|----------------------------------|------|----|----------------------------------|------|----|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 21 | 1,640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | | | | | | | | 0.03 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.1 | | | 0.0 | | | | 9.9 | | | 10.6 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.1 | |
| 2038 BUILD Volumes | 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.11 | | | 0.04 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 10.0 | | | 13.4 | | | | 14.2 | | | 10.9 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.2 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.13 | | | 0.04 | |
| Level-of-Service | B | | | B | | | | C | | | B | |
| Control Delay (Seconds) | 10.0 | | | 13.4 | | | | 16.3 | | | 11.4 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.2 | | | | 0.4 | | | 0.1 | |

| PM Peak Hour | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2038 NO BUILD Volumes | 13 | 929 | 4 | 0 | 1,759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | | | | | 0.01 | | | 0.08 | |
| Level-of-Service | B | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 13.2 | | | 0.0 | | | | 9.7 | | | 11.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.3 | |
| 2038 BUILD Volumes | 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.11 | | | 0.08 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 13.4 | | | 10.3 | | | | 12.0 | | | 11.6 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.4 | | | 0.3 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.11 | | | 0.09 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 13.4 | | | 10.3 | | | | 12.0 | | | 12.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.4 | | | 0.3 | |

2028 and 2038 LOS Analysis demonstrates that the Lobo Plaza will have very minimal impact on the LOS and delays for the 2038 AM and PM Build conditions. The west driveway will operate at acceptable levels-of-service for all conditions analyzed in this Study. A westbound left turn lane on Lomas Blvd exists and an eastbound right turn deceleration lane is not warranted based on City of Albuquerque deceleration lane warrant criteria. This study does advise that the existing westbound left turn lane be maintained. The LOS remains virtually the same for the No Build and Build conditions. The No Build and Build conditions demonstrate acceptable LOS for all movements at the intersection. Therefore, mitigations are not recommended at the unsignalized intersection of Lomas Blvd NE and Legion Rd / West Driveway.

2028 and 2038 Queuing Analysis demonstrates that no additional queuing capacity is needed for the intersection of Lomas Blvd NE and Legion Rd / West Driveway for 2028 and 2038. V/C ratios are also less than 1 for all movements in the AM, and PM for No Build and Build conditions. No mitigations are recommended for the unsignalized intersection of Lomas Blvd NE and Legion Rd / West Driveway.

#7 – Lomas Blvd NE / East Driveway – Unsignalized

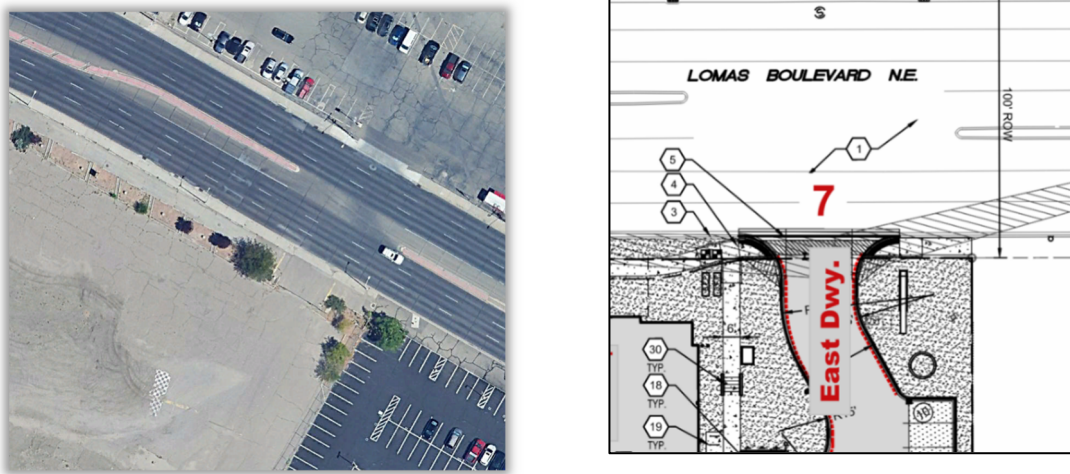


Figure 7 - Lomas Blvd NE and East Driveway

The intersection of Lomas Blvd NE and East Driveway is an unsignalized intersection. The results of the 2028 (Implementation Year) and 2038 (Horizon Year) analysis of the unsignalized intersection of Lomas Blvd NE and East Driveway are summarized in the following tables below and on Appendix pages A-168 and A-179. The scenario, mitigated condition for this intersection considered as the operation of the driveway if staged left turns are executed.

Unsignalized

| 7: Lomas Blvd NE & East DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (East DWY) | | | SB (East DWY) | | |
|--|---------------------------|-------|----|---------------------------|-----|---|----------------------|------|----|----------------------|------|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 0 | 1,573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| V/C Ratio | | | | | | | | | | | 0.01 | |
| Level-of-Service | A | | | A | | | | A | | | A | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.0 | |
| 2028 BUILD Volumes | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.90 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | B | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.2 | | | | 13.5 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.0 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.11 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | C | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.2 | | | | 15.2 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.4 | | | 0.0 | |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2028 NO BUILD Volumes | 0 | 914 | 0 | 0 | 1,673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| V/C Ratio | | | | | | | | | | | 0.02 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.1 | |
| 2028 BUILD Volumes | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.5 | | | | 11.5 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.5 | | | | 11.9 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |

Unsignalized

| 7: Lomas Blvd NE & East DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (East DWY) | | | SB (East DWY) | | |
|-------------------------------------|--------------------|-------|----|--------------------|-----|---|---------------|------|----|---------------|------|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2038 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 0 | 1,651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| V/C Ratio | | | | | | | | | | | 0.01 | |
| Level-of-Service | A | | | A | | | | A | | | A | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.0 | |
| 2038 BUILD Volumes | 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.10 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | B | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.6 | | | | 14.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.0 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.11 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | C | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.6 | | | | 15.6 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.4 | | | 0.0 | |

PM Peak Hour

| | | | | | | | | | | | | |
|-------------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2038 NO BUILD Volumes | 0 | 959 | 0 | 0 | 1,755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| V/C Ratio | | | | | | | | | | | 0.02 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.1 | |
| 2038 BUILD Volumes | 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.4 | | | | 11.9 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.4 | | | | 11.9 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |

2028 and 2038 LOS Analysis demonstrate that the Lobo Plaza driveway will operate at acceptable levels-of-service for all conditions analyzed in this Study. A westbound left turn lane on Lomas Blvd exists and an eastbound right turn deceleration lane is not warranted based on City of Albuquerque deceleration lane warrant criteria. This study does advise that the existing westbound left turn lane be maintained.

2028 and 2038 Queueing Analysis demonstrates that no additional queueing capacity is needed for the intersection of Lomas Blvd NE and East Driveway for 2028 and 2038. V/C ratios are also less than 1 for all movements in the AM, and PM for No Build and Build conditions. No mitigations are recommended for the unsignalized intersection of Lomas Blvd NE and East Driveway.

Crash Analysis

Crash data was obtained from January 1, 2019, through December 31, 2023, from the American Association of State Highway and Transportation Officials' (AASHTO) AASHTOWare Safety website which maintains a database of crash records statewide for the State of New Mexico. Crash records were collected for all seven (7) study intersections along Lomas Blvd NE. Summary report of each crash intersection can be found in the Appendices from *A-153 to A-173*.

There were 279 recorded crashes in the study area for the five-year study period including 80 crashes in 2019, 39 crashes in 2020, 50 crashes in 2021, 57 crashes in 2022 and 53 crashes in 2023. A summary table depicting intersection wise crash numbers, types, contributing factors, lighting condition, injury severity and driver's action is presented in page 36.

Based on number of crashes during the five (5) year period (2019-2023), the intersection of Lomas Blvd NE and University Drive NE experienced the highest number of crashes (109) including 1 fatality in 2019. The fatality involved a pedestrian crash that involved a pedestrian violating the right of way of a vehicle and causing the collision. The pedestrian was under influence of alcohol and the crash occurred during Dark-Lighted condition.

In five (5) year period combing all seven intersections, majority crashes involved vehicles approaching from opposite directions, either going straight or making left turn and also crashing with fixed objects including utility pole, median, curb or signal. Majority of the crashes occurred during daylight conditions and involved property damage only.

Two (2) pedestrian crashes and two (2) pedalcycle crashes were reported in five-year period including one (1) pedestrian fatality in 2019.

Among the contributing factors, a significant number of vehicles driven by inattentive / distracted drivers have been reported causing crashes.

| INTERSECTION # | | 1 | 2 | 3 | 4 | 5 | 6 | 7 |
|--|---|--------------------------------|--------------------------------|------------------------------------|-------------------------------------|--|--------------------------------------|-------------------------------------|
| INTERSECTION NAMES | | Lomas Blvd NE & I - 25 SB Ramp | Lomas Blvd NE & I - 25 NB Ramp | Lomas Blvd NE & University Blvd NE | Lomas Blvd NE & Medical Arts Ave NE | Lomas Blvd NE & Frontage Rd / Torc Dwy | Lomas Blvd NE & West Dwy / Legion Rd | Lomas Blvd NE & Lobo Plaza East Dwy |
| Total Crashes | | 78 | 75 | 109 | 11 | 0 | 5 | 1 |
| YEAR | 2019 | 20 | 23 | 32 | 2 | | 3 | 0 |
| | 2020 | 9 | 14 | 11 | 5 | | 0 | 0 |
| | 2021 | 13 | 10 | 24 | 1 | | 2 | 0 |
| | 2022 | 22 | 13 | 19 | 2 | | 0 | 1 |
| | 2023 | 14 | 15 | 23 | 1 | | 0 | 0 |
| CRASH TYPE | Left Blank / Invalid Code | 57 | 43 | 67 | 5 | | 3 | 0 |
| | Fixed Object (Light Pole/Raised Median/Curb/Traffic Signal) | 2 | 3 | 4 | 0 | | 1 | 1 |
| | Other Vehicle - Both Going Straight/Entering At Angle | 2 | 9 | 4 | 0 | | 0 | 0 |
| | Other Vehicle - From Opposite Direction | 7 | 5 | 15 | 1 | | 0 | 0 |
| | Other Vehicle - From Opposite Direction/Both Going Straight | 2 | 1 | 0 | 0 | | 0 | 0 |
| | Other Vehicle - From Opposite Direction/One Left Turn | 4 | 2 | 1 | 0 | | 0 | 0 |
| | Other Vehicle - From Same Direction/Both Going Straight | 1 | 2 | 3 | 2 | | 0 | 0 |
| | Other Vehicle - From Same Direction/One Left Turn | 1 | 1 | 0 | 0 | | 0 | 0 |
| | Other Vehicle - From Same Direction/One Right Turn | 0 | 1 | 0 | 1 | | 0 | 0 |
| | Other Vehicle - From Same Direction/Rear End Collision | 0 | 2 | 6 | 0 | | 1 | 0 |
| | Other Vehicle - From Same Direction/Sideswipe Collision | 0 | 1 | 1 | 0 | | 0 | 0 |
| | Other Vehicle - One Left Turn/Entering At Angle | 1 | 2 | 4 | 1 | | 0 | 0 |
| | Other Vehicle - One Right Turn/Entering At Angle | 0 | 1 | 1 | 0 | | 0 | 0 |
| | Pedestrian Collision - Vehicle Going Straight | 0 | 0 | 1 | 0 | | 0 | 0 |
| Others (Entering At Angle/Stopped In Traffic) Parked Vehicle | 1 | 2 | 1 | 1 | | 0 | 0 | |
| Non-Collision | 0 | 0 | 1 | 0 | | 0 | 0 | |
| | % Fixed object | 3% | 4% | 4% | 0% | | 20% | 100% |
| | % Other Vehicle - Both Going Straight/Entering At Angle | 3% | 12% | 4% | 0% | | 0% | 0% |
| LIGHTING CONDITIOS | Daylight | 56 | 50 | 82 | 8 | | 4 | 1 |
| | Dark - Lighted | 13 | 21 | 16 | 2 | | 1 | 0 |
| | Dawn | 2 | 0 | 2 | 0 | | 0 | 0 |
| | Dusk | 2 | 0 | 0 | 0 | | 0 | 0 |
| | Dark - Not Lighted | 1 | 3 | 0 | 0 | | 0 | 0 |
| Other / Left Blank | 4 | 1 | 9 | 1 | | 0 | 0 | |
| | % Daylight | 72% | 67% | 75% | 73% | | 80% | 100% |
| | % Dark - Lighted | 17% | 28% | 15% | 18% | | 20% | 0% |
| SEVERITY (KABCO) | Fatal Crash (K) | 0 | 0 | 1 | 0 | | 0 | 0 |
| | Suspected Serious Injury (A) | 4 | 0 | 0 | 1 | | 0 | 0 |
| | Suspected Minor Injury (B) | 9 | 5 | 7 | 0 | | 0 | 1 |
| | Possible Injury (C) | 16 | 14 | 29 | 2 | | 0 | 0 |
| | Property Damage Only Crash (O) | 49 | 56 | 72 | 8 | | 5 | 0 |
| | % Fatal Crash | 0% | 0% | 1% | 0% | | 0% | 0% |
| | % Injury Crash | 37% | 25% | 33% | 27% | | 0% | 100% |
| | % Property Damage Only Crash | 63% | 75% | 66% | 73% | | 100% | 0% |
| BIKE / PED CRASHES | Pedestrian Involvement | 0 | 1 | 1 | 0 | | 0 | 0 |
| | Pedalcycle Involvement | 0 | 1 | 1 | 0 | | 0 | 0 |
| | % Pedestrian Involved | 0% | 1% | 1% | 0% | | 0% | 0% |
| | % Pedalcycle Involved | 0% | 1% | 1% | 0% | | 0% | 0% |
| CONTRIBUTING FACTORS (NUMBER OF VEHICLES) | Other, No Driver Error | 47 | 37 | 46 | 6 | | 3 | 0 |
| | Driver Inattention / Distracted Driver | 38 | 37 | 41 | 4 | | 2 | 1 |
| | Disregarded Traffic Signal | 14 | 17 | 14 | 1 | | 0 | 0 |
| | Failed to Yield Right of Way | 14 | 10 | 9 | 2 | | 0 | 0 |
| | Other Improper Driving | 10 | 5 | 6 | 0 | | 0 | 0 |
| | Excessive Speed / Speed Too Fast for Conditions | 5 | 4 | 10 | 0 | | 0 | 0 |
| | Under the Influence of Alcohol | 3 | 5 | 5 | 1 | | 0 | 0 |
| | Other Factors | 15 | 20 | 22 | 1 | | 0 | 0 |
| | % Vehicles Involved Distracted / Inattentive Drivers | 8% | 8% | 9% | 1% | | 0.44% | 0.22% |
| | % Vehicles Disregarded Traffic Signal | 3% | 4% | 3% | 0% | | 0% | 0% |
| DRIVER ACTIONS (NUMBER OF VEHICLES) | Going Straight | 81 | 81 | 95 | 8 | | 5 | 1 |
| | Left Turn | 33 | 17 | 20 | 4 | | 1 | 0 |
| | Stopped in Sign or Signal | 9 | 9 | 15 | 2 | | 0 | 0 |
| | Right Turn | 8 | 6 | 5 | 2 | | 0 | 0 |
| | Changing Lanes / Leaving Traffic Lane / Entering Traffic Lane | 4 | 0 | 4 | 0 | | 0 | 0 |
| | Stopped for Traffic | 3 | 3 | 8 | 1 | | 0 | 0 |
| | Ran Red Light | 0 | 0 | 2 | 0 | | 0 | 0 |
| | Pedestrian Error, Under the Influence of Alcohol | 0 | 0 | 1 | 0 | | 0 | 0 |
| | Other Actions (Aggressive Driver, Negotiating Curve, Parked) | 6 | 10 | 10 | 2 | | 0 | 0 |
| | | % Vehicles Involved Left Turn | 7% | 4% | 4% | 1% | | 0.22% |
| | % Vehicles Crashed While Stopped in Traffic Signal | 2% | 2% | 3% | 0% | | 0% | 0% |

NO CRASH IN 5 YEARS

XX Denotes Fatality

Queuing for NMDOT

The Queuing Summary of two signalized intersections of Lomas Blvd NE / I-25 Southbound On-Off Ramp and Lomas Blvd NE / I-25 Northbound On-Off Ramp are shown below:

| Queuing Summary | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
|--|--------------------|------|-----|--------------------|-----|-----|-------------------|-----|-----|-------------------|------|-------|
| 1: Lomas Blvd NE & I-25 SB Ramp | L | T | R | L | T | R | L | T | R | L | T | R |
| 2038 NO BUILD Conditions (Max Queue) | 0.0 | 10.5 | 0.0 | 6.8 | 3.8 | 0.0 | 0.0 | 0.0 | 0.0 | 48.7 | 10.2 | 47.0 |
| 2038 BUILD Conditions (Max Queue) | 0.0 | 10.8 | 0.0 | 6.9 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | 49.2 | 10.2 | 47.0 |
| Percent Heavy Commercial Traffic | 3% | | | | | | | | | | | |
| 2038 NO BUILD Conditions (Max Queue) - Ft. | 0 | 270 | 0 | 175 | 98 | 0 | 0 | 0 | 0 | 1,254 | 263 | 1,210 |
| 2038 BUILD Conditions (Max Queue) - Ft. | 0 | 278 | 0 | 178 | 103 | 0 | 0 | 0 | 0 | 1,267 | 263 | 1,210 |
| Length of Existing Lane | 0 | 500 | 275 | 400 | 500 | | | | | 1000 | 1000 | 1000 |

| Queuing Summary | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
|--|--------------------|------|-----|--------------------|------|------|-------------------|-----|-----|-------------------|-----|-----|
| 2: Lomas Blvd NE & I-25 NB Ramp | L | T | R | L | T | R | L | T | R | L | T | R |
| 2038 NO BUILD Conditions (Max Queue) | 17.6 | 13.8 | 0.0 | 0.0 | 13.7 | 30.6 | 6.6 | 8.8 | 9.0 | 0.0 | 0.0 | 0.0 |
| 2038 BUILD Conditions (Max Queue) | 11.4 | 14.0 | 0.0 | 0.0 | 14.1 | 30.8 | 6.1 | 7.7 | 8.3 | 0.0 | 0.0 | 0.0 |
| Percent Heavy Commercial Traffic | 3% | | | | | | | | | | | |
| 2038 NO BUILD Conditions (Max Queue) - Ft. | 453 | 355 | 0 | 0 | 353 | 788 | 170 | 227 | 232 | 0 | 0 | 0 |
| 2038 BUILD Conditions (Max Queue) - Ft. | 294 | 361 | 0 | 0 | 363 | 793 | 157 | 198 | 214 | 0 | 0 | 0 |
| Length of Existing Lane | 400 | 500 | | | 500 | 100 | 700 | 700 | 700 | | | |

Results shows that the proposed development has minimal adverse impacts on the turn bays –

- Intersection # 1: The Southbound left turn lane has a deficit of 254 feet in length to accommodate the 1254 feet queue generated in the No Build condition; however, the build condition is expected to add a 13 feet length to the queue, which is minimal.
- Intersection # 2: The westbound right turn lane has a deficit of 688 feet in length to accommodate the 788 feet queue generated in the No Build condition; however, the build condition is expected to add a 5 feet length to the queue, which is minimal.

Summary of Impacts

The proposed Lobo Plaza (1300 Lomas Blvd NE) will have no significant adverse impact on the adjacent transportation system. The overall levels-of-service were determined to be acceptable for all for all seven (7) intersections during both the AM Peak Hour and PM Peak Hour Implementation Year (2028) and Horizon Year (2038) conditions analyzed in this report. However, signal timing optimization is recommended to minimize delays at certain approaches at the signalized intersections. In summary, the recommendations of this study are:

- Construct two (2) commercial driveways – east and west driveways on Lomas Blvd NE Driveway to provide access to Lobo Plaza as shown on the site plan on Page A-3 in the Appendix. The access points should be full access unsignalized intersections and both the access points can be single approach (northbound) access with a shared left turn, through and right turn lane.
- All design and construction associated with the Lobo Plaza shall maintain adequate traffic site distances at existing and proposed intersections / driveways. No landscaping elements or walls or other structures should be permitted that constrain site distances at an intersection / driveway that compromises safety.

Recommendations

Intersection 1, Lomas Blvd NE and I-25 Southbound Ramp

Optimization of Signal timing during the AM Peak Hour as per the recommendations in this report is recommended.

Intersection 2 - Lomas Blvd NE and I-25 Northbound Ramp

Optimization of Signal timing during the PM Peak Hour as per the recommendations in this report is recommended.

Intersection 3 - Lomas Blvd NE and University Blvd NE

Optimization of Signal timing during the PM Peak Hour as per the recommendations in this report is recommended.

Intersection 4 - Lomas Blvd NE and Medical Arts Ave NE

There are no recommendations at this Intersection.

Intersection 5 – Lomas Blvd NE and Torc Driveway / Frontage Rd

There are no recommendations at this Intersection.

Intersection 6 - Lomas Blvd NE and Legion Rd / West Driveway

- There are no recommendations at the current / existing intersection of Lomas Blvd NE and Legion Rd.
- The recommendations for West Driveway include the construction of a 2-lane (one northbound approach lane and one southbound receiving lane) commercial full access unsignalized driveway that provides access to Lobo Plaza Development. All new access shall be constructed with adequate sight distance at all approaches at the intersection.

Intersection 7 - Lomas Blvd NE and East Driveway

The recommendations for East Driveway include the construction of a 2-lane (one northbound approach lane and one southbound receiving lane) full access unsignalized commercial driveway that provides access to Lobo Plaza Development. All new access shall be constructed with adequate sight distance at all approaches to the intersection.

In summary, the proposed Retail at Lobo Plaza will have no significant adverse impact on the adjacent transportation system provided that the recommendations of this report are implemented.

| | |
|---|--------------|
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| | |
|---|---------------|
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| Intersection 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | |
| Intersection 6 - Lomas Blvd NE / Legion Rd / West DWY | |
| Intersection 7 - Lomas Blvd NE / East DWY | |
| | |
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| Intersection 4 - Lomas Blvd NE / Medical Arts Ave NE | |
| Intersection 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | |
| Intersection 6 - Lomas Blvd NE / Legion Rd / West DWY | |
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| | |
| <u>2025 HCM 6 Analysis -</u> | |
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| Intersection 4 - Lomas Blvd NE / Medical Arts Ave NE | |
| Intersection 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | |
| Intersection 6 - Lomas Blvd NE / Legion Rd / West DWY | |
| Intersection 7 - Lomas Blvd NE / East DWY | |

| | |
|---|-----------------------|
| <u>2028 HCM 6 Analysis -</u> | A-180 to A-189 |
| 2028 AM Peak NO BUILD Conditions | |
| 2028 AM Peak BUILD Conditions | |
| 2028 AM Peak BUILD MITIGATED Conditions | |
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| 2028 PM Peak BUILD Conditions | |
| 2028 PM Peak BUILD MITIGATED Conditions | |
| | |
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| 2038 AM Peak BUILD Conditions | |
| 2038 AM Peak BUILD MITIGATED Conditions | |
| 2038 PM Peak NO BUILD Conditions | |
| 2038 PM Peak BUILD Conditions | |
| 2038 PM Peak BUILD MITIGATED Conditions | |
| | |
| <u>Crash Analysis</u> | A-200 to A-219 |
| Crash Summary Reports | |
| Intersection 1 - Lomas Blvd NE / I-25 Southbound Ramp | |
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| Intersection 3 - Lomas Blvd NE / University Blvd NE | |
| Intersection 4 - Lomas Blvd NE / Medical Arts Ave NE | |
| Intersection 5 - Lomas Blvd NE / Torc DWY / Frontage Rd | |
| Intersection 6 - Lomas Blvd NE / Legion Rd / West DWY | |
| Intersection 7 - Lomas Blvd NE / East DWY | |
| | |
| <u>Traffic Data</u> | |
| Traffic Count Data Sheets | A-220 to A-226 |
| Traffic Impact Study Scoping Letter | A-227 to A-229 |
| | |

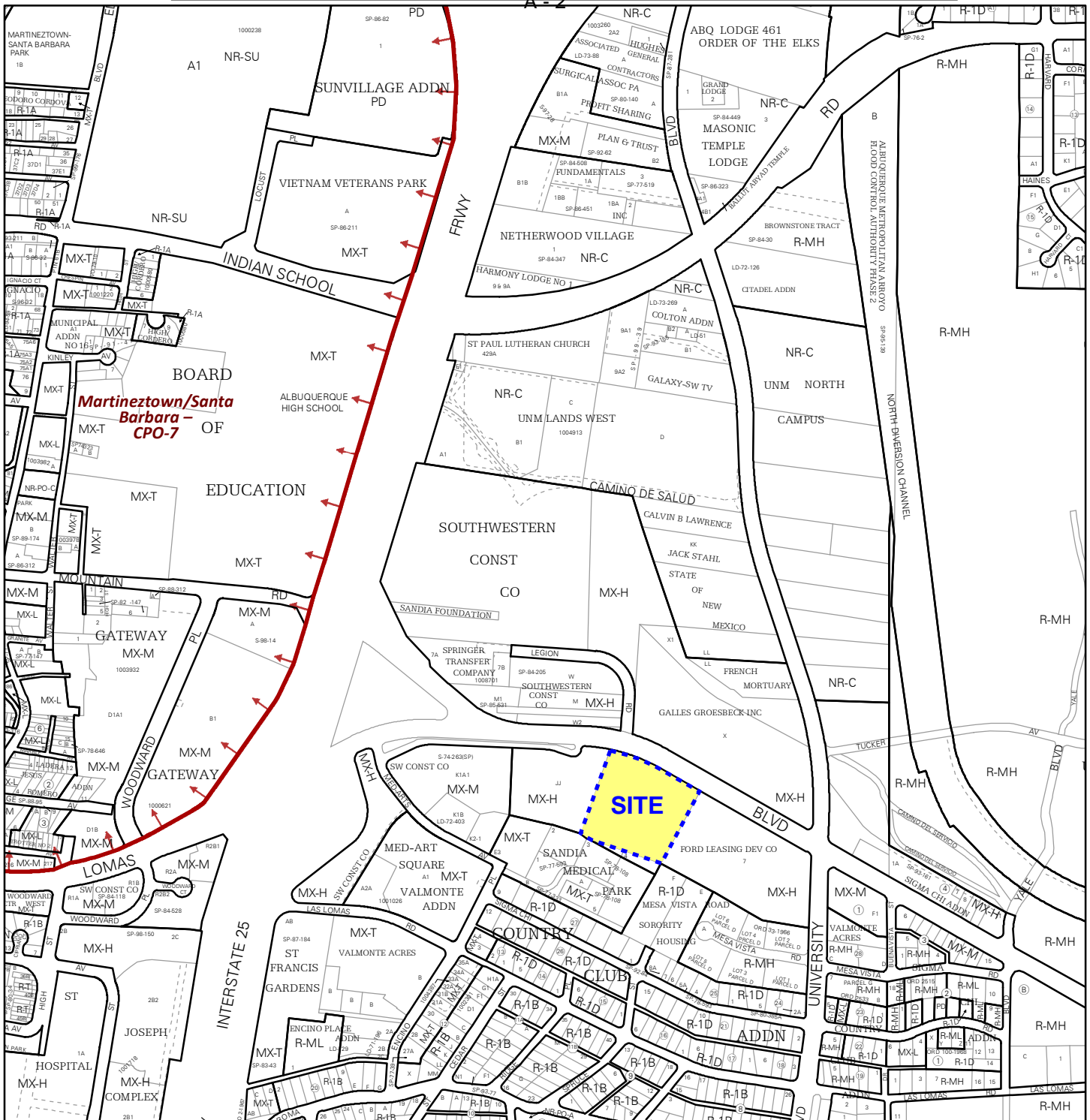
APPENDIX



Project Study Area Map
Lobo Plaza – 1300 Lomas Blvd NE
Albuquerque, New Mexico


Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Vicinity Map - Lobo Plaza (1300 Lomas Blvd NE, Albuquerque, NM)











For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

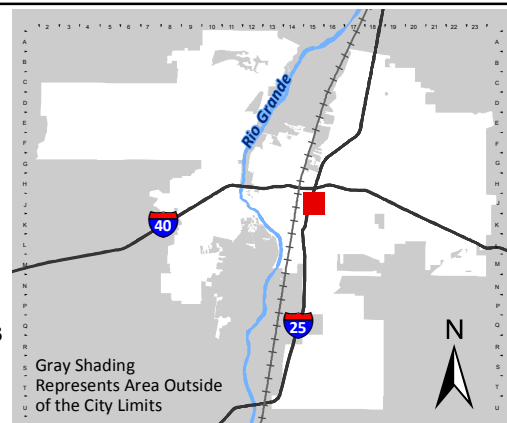
IDO Zone Atlas May 2018



IDO Zoning information as of May 17, 2018
The Zone Districts and Overlay Zones
are established by the
Integrated Development Ordinance (IDO).

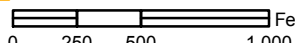
Zone Atlas Page:
J-15-Z

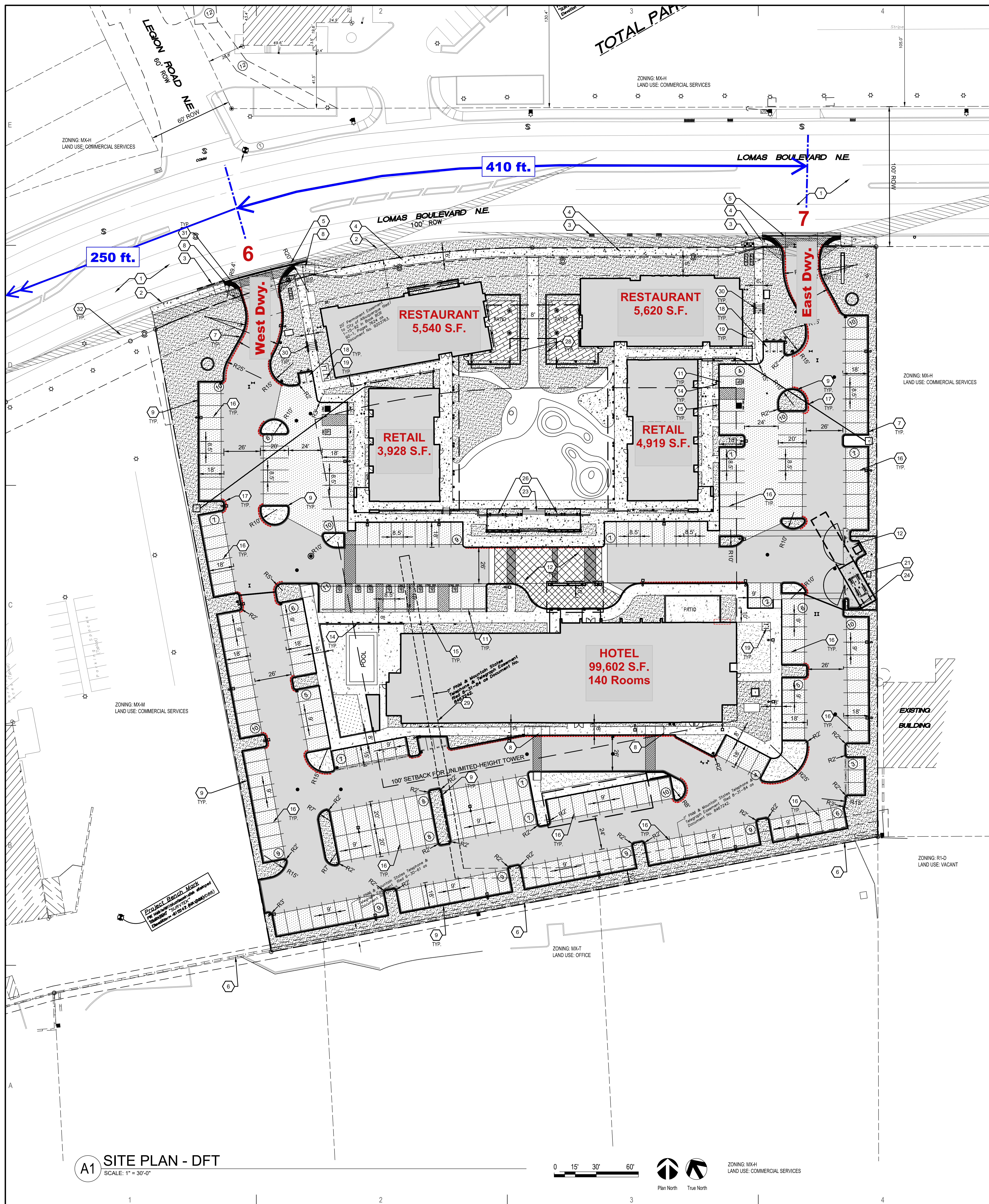
-  Easement
-  Escarpment
-  Petroglyph National Monument
-  Areas Outside of City Limits
-  Airport Protection Overlay (APO) Zone
-  Character Protection Overlay (CPO) Zone
-  Historic Protection Overlay (HPO) Zone
-  View Protection Overlay (VPO) Zone



Gray Shading
Represents Area Outside
of the City Limits

N





CLEAR SITE TRIANGLES:
 LANDSCAPE AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. SIGNS, WALLS, AND PLANTING BETWEEN 3 FEET AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE PERMITTED IN THE CLEAR SIGHT TRIANGLE.

SITE DATA:
 SITE ADDRESS: 1300 LOMAS BOULEVARD NE
 LEGAL DESCRIPTION: T10N SEC16 R3E CON 4.974 ACRE PARCEL I
 SITE AREA: 4.97 ACRES
 EXISTING ZONING: MX-M

GENERAL NOTES:
 1. THE PROJECT SITE IS WITHIN AN AREA OF CHANGE. AN AREA OF CONSISTENCY IS ADJACENT TO THE PROJECT SITE AT ITS SOUTHEAST CORNER.
 2. THE PROJECT SITE IS WITHIN THE LOMAS BOULEVARD MAJOR TRANSIT CORRIDOR. LOMAS 11 BUS ROUTE RUNS ALONG LOMAS BOULEVARD AND BUS STOPS ARE IN CLOSE PROXIMITY TO THE PROJECT SITE.
 3. SITE LIGHTING SHALL BE COMPLIANT WITH IDO SECTION 5-8 OUTDOOR AND SITE LIGHTING, TABLE 5-8-1 MAXIMUM HEIGHT FOR LIGHT POLES, AND TABLE 5-8-2 LIGHTING DESIGNATIONS BY ZONE DISTRICT.

BUILDING AREA TOTALS BY PHASE

PHASE 1A: 20,007 SF (RESTAURANTS AND RETAIL)
 PHASE 2: 99,602 SF (HOTEL)
 FULL BUILD OUT: 119,609 SF

PARKING CALCULATIONS

PROPOSED BUILDINGS (PHASE 1 - RESTAURANTS AND RETAIL):
 RESTAURANTS (2) 5,540SF AND 5,620SF = 11,160 SF
 PARKING REQUIRED IS 5.6 SPACES PER 1,000 SF GFA = 63 SPACES REQUIRED.

RETAIL (2) 4,919SF AND 3,928SF = 8,847 SF
 PARKING REQUIRED IS 3.5 SPACES PER 1,000 SF GFA = 31 SPACES REQUIRED.

PHASE 2
 HOTEL 140 ROOMS, 234 BEDS, 99,602SF TOTAL
 PARKING REQUIRED 1 SPACE / GUEST ROOM
 OR 1 SPACE PER 2 BEDS, WHICHEVER IS GREATER = 140 STALLS.

COMBINED PHASE 1 AND PHASE 2
 TOTAL 234 PARKING SPACES REQUIRED.

ACCESSIBLE PARKING REQUIRED 8 PER MMAC SECTION 1102.

PARKING PROVIDED: 247 TOTAL SPACES PROVIDED
 226 STANDARD
 9 COMPACT
 12 ADA ACCESSIBLE SPACES PROVIDED

MOTORCYCLE PARKING REQUIRED (TABLE 5-5-4) = 5 SPACES
 5 MOTORCYCLE SPACES REQUIRED, 9 MOTORCYCLE SPACES PROVIDED

BICYCLE PARKING PER TABLE (TABLE 5-5-5)
 NON-RESIDENTIAL (RESTAURANT/RETAIL) = 3 SPACES OF 10% OR REQUIRED OFF STREET PARKING SPACES.
 10% OF 91 SPACES = 10 SPACES ROUNDED UP
 HOTEL = 2 SPACES + 1 SPACE / 6,000 SF GFA OF CONFERENCE, RESTAURANT, BAR AND BANQUET SPACE
 MEETING SPACE 710SF, ROOF TOP BAR=8,700SF, REQUIRED 4 BIKES (ROUND UP).
 THEREFORE TOTAL BIKE PARKING= 14 REQUIRED.
 PROVIDED TO BE DISTRIBUTED AT EACH RESTAURANT/RETAIL AND 4 AT HOTEL (TOTAL OF 20).
 14 BICYCLE SPACES REQUIRED, 20 BICYCLE SPACES PROVIDED

PROJECT NUMBER: _____
Application Number: _____

Is an Infrastructure List required? () Yes () No If yes, then a set of approved DFC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DFI SITE DEVELOPMENT PLAN APPROVAL:

| | |
|---|------|
| Traffic Engineering, Transportation Division | Date |
| ABCWUA | Date |
| Parks and Recreation Department | Date |
| Hydrology | Date |
| Code Enforcement | Date |
| * Environmental Health Department (conditional) | Date |
| Solid Waste Management | Date |
| Planning Department | Date |

GENERAL SHEET NOTES

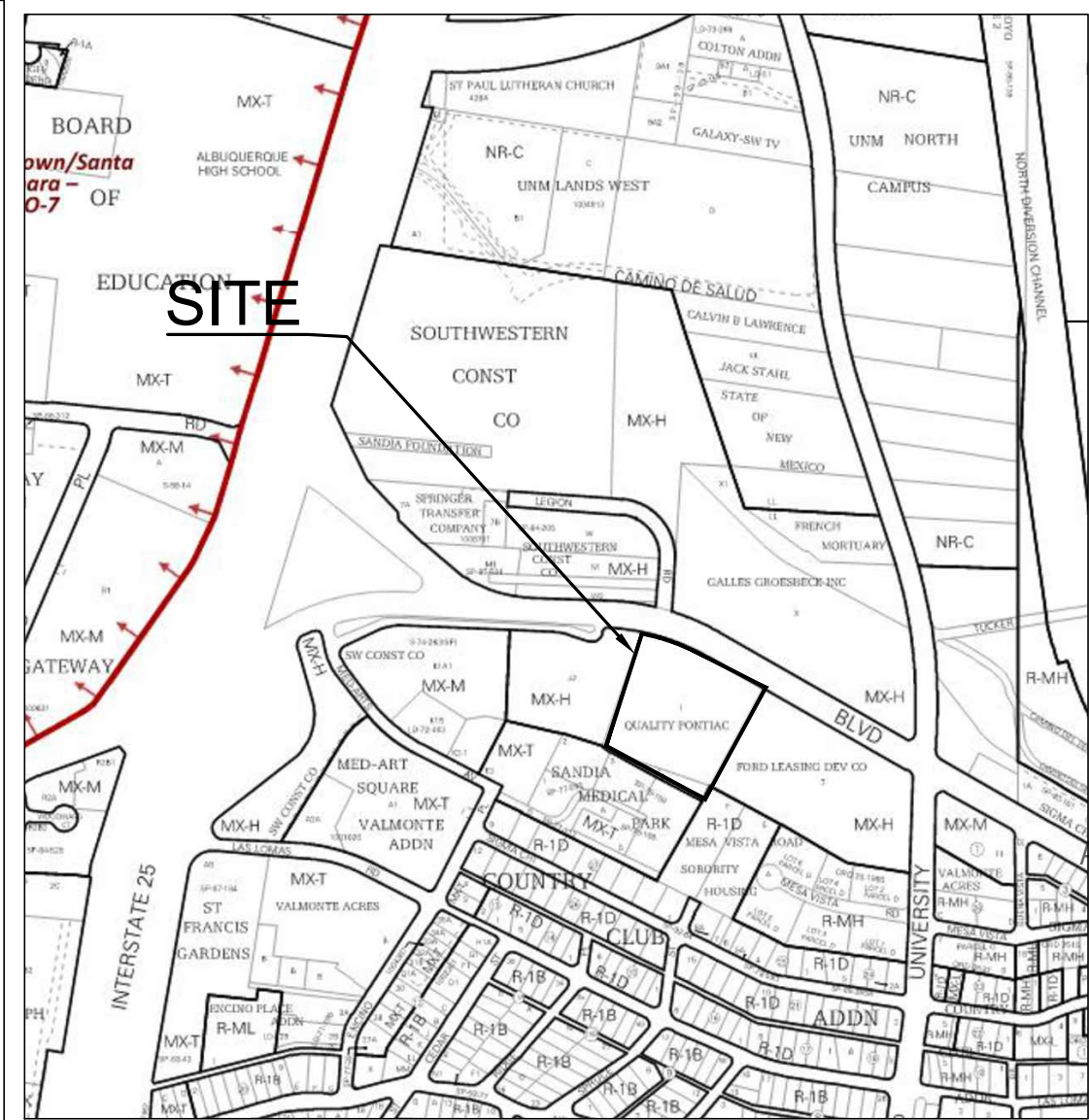
- CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS TO REMAIN INCLUDING, BUT NOT LIMITED TO, BUILDINGS, PAVING, CURBS, SITE WALLS, FOUNDATIONS AND UTILITIES. EXISTING FOUNDATIONS SHALL MAINTAIN THEIR ORIGINAL STRUCTURAL INTEGRITY AS THEY WERE DESIGNED.
- GENERAL CONTRACTOR SHALL PATCH, REPAIR AND/OR REPLACE ITEMS TO REMAIN THAT HAVE BEEN DAMAGED DURING CONSTRUCTION, TO THE OWNER'S SATISFACTION AND AT NO COST TO THE OWNER.
- CONTRACTOR SHALL ENSURE THAT UTILITY SERVICE TO EXISTING BUILDINGS TO REMAIN SHALL BE OPERATIONAL DURING CONSTRUCTION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFETY MEASURES TO PROTECT PEDESTRIANS AND VEHICLES DURING CONSTRUCTION.
- CONTRACTOR IS TO VERIFY ALL EXISTING UTILITY LOCATIONS BEFORE EXCAVATION.
- ALL SITE PLAN DIMENSIONS ARE IN DECIMAL UNITS.
- SIDEWALK JOINTS SHALL FOLLOW PATTERN AS SHOWN ON SITE PLAN.

KEYED NOTES

- EXISTING ASPHALT TO REMAIN.
- EXISTING CURB AND GUTTER.
- EXISTING STORMWATER CATCH BASIN.
- EXISTING CONCRETE SIDEWALK.
- CONCRETE VALLEY GUTTER PER COA STD DWG 2420.
- EXISTING CONCRETE RETAINING WALL.
- STANDARD CURB AND GUTTER.
- CONCRETE CURB ACCESS RAMP.
- DEPRESSED CURB AND GUTTER.
- CONCRETE SIDEWALK.
- CONCRETE PARKING BLOCK.
- CONCRETE PAVEMENT.
- ADA RESERVED PARKING STALL.
- ADA RESERVED PARKING SIGN.
- ADA VAN ACCESSIBLE RESERVED SIGN.
- 4" WIDE WHITE TRAFFIC PARKING.
- PAINT CURB RED WITH 6" TALL WHITE LETTERS READING "NO PARKING FIRE LANE".
- MOTORCYCLE PARKING SPACE WITH SIGN AND 12" TALL "MC" LETTERS.
- BIKE RACK.
- 12' LONG AND 6" TALL MONUMENT SIGN PER ARCHITECTURAL PLANS.
- REFUSE ENCLOSURE WITH ENCLOSURE GATES.
- CONCRETE RAMP WITH HANDRAILS.
- CONCRETE STAIRS WITH HANDRAILS.
- 6" TALL CMU SCREEN WALL.
- LIGHT POLE. SEE ELECTRICAL PLANS.
- 30" TALL CONCRETE RETAINING WALL.
- DECORATIVE CONCRETE PAVEMENT.
- 42" TALL PATIO WALLS.
- EXISTING UTILITY EASEMENT TO BE VACATED.
- CONCRETE STEPS WITH METAL HANDRAILS.
- MINI-SIGHT TRIANGLES AT SIDEWALKS (11' ON EACH SIDE).
- STOPPING SIGHT TRIANGLES (BASED IN 35 MPH SPEED LIMIT).

LEGEND

- PROPERTY LINE
- EASEMENT
- MAIN DRIVE ASPHALT PAVEMENT
- PARKING LOT ASPHALT PAVEMENT
- CONCRETE SIDEWALK
- DECORATIVE CONCRETE PATIO SIDEWALK
- HEAVY DUTY CONCRETE PAVEMENT
- LANDSCAPING AREA
- DECORATIVE CONCRETE PAVEMENT



CONSULTANTS

| | |
|-----------|----------|
| Architect | Engineer |
|-----------|----------|

LOBO PLAZA

1300 LOMAS BOULEVARD, NE
 Albuquerque, NM 87102

| No | Date | Description |
|-------------------|-----------------|-------------|
| Revision Schedule | | |
| ISSUE: | SITE PLAN - DFT | |
| PROJECT NUMBER: | 2427 | |
| FILE: | Site Plan - DFT | |
| DRAWN BY: | DAA | |
| CHECKED BY: | DAA | |
| DATE: | AUG 20, 2025 | |

SHEET TITLE

SITE PLAN - DFT

SP-DFT

Lobo Plaza (Lomas Blvd. / University Blvd.)
Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| | USE (ITE CODE) | DESCRIPTION | 24 HR VOL | A. M. PEAK HR. | | P. M. PEAK HR. | | |
|---|----------------|---|-----------|----------------|-------------|----------------|-------------|-------------|
| | | | GROSS | ENTER | EXIT | ENTER | EXIT | |
| Summary Sheet | | | Units | | | | | |
| North-West Parcel | | High Turnover (Sit-Down) Restaurant (932) | 5.98 | 641 | 31 | 26 | 33 | 21 |
| North-West Parcel | | Strip Retail Plaza <40K - Equation (822) | 4.28 | 410 | 10 | 7 | 21 | 21 |
| | | Hotel (310) | 140 | 1,094 | 35 | 28 | 39 | 37 |
| South-East Parcel | | High Turnover (Sit-Down) Restaurant (932) | 5.62 | 602 | 30 | 24 | 31 | 20 |
| South East Parcel | | Strip Retail Plaza <40K - Equation (822) | 4.90 | 436 | 11 | 7 | 23 | 23 |
| Subtotal | | | | 3,183 | 117 | 92 | 147 | 122 |
| <i>Internal Capture Trip Reduction (Based on OTISS PRO)</i> | | | | | -2 | -2 | -22 | -22 |
| <i>Trip Generation Rate Adjusted For Internal Capture</i> | | | | | 115 | 90 | 125 | 100 |
| <i>Pass-By Trip Reduction (Based on OTISS PRO)</i> | | | 30% | | (25) | (19) | (32) | (26) |
| Net New Trips to Adjacent Roadway System | | | | | 90 | 71 | 93 | 74 |

Lobo Plaza (Lomas Blvd. / University Blvd.)

Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| USE (ITE CODE) | 24 HOUR TWO-WAY VOLUME | A. M. PEAK HOUR | | P. M. PEAK HOUR | | |
|--|------------------------------------|-----------------------|-------|-----------------------|-------|------|
| | | GROSS | ENTER | EXIT | ENTER | EXIT |
| High Turnover (Sit-Down) Restaurant (932) | Units 5.98 1,000 S.F. | 641 | 31 | 26 | 33 | 21 |

ITE Trip Generation Equations:

Average Vehicle Trip Ends on a Weekday (24 HOUR TWO-WAY VOLUME)

$$T = 107.2 (X) + 0$$

50% Enter, 50% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7am and 9am (A.M. PEAK HOUR)

$$T = 9.57 (X) + 0$$

55% Enter, 45% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4pm and 6pm (P.M. PEAK HOUR)

$$T = 9.05 (X) + 0$$

61% Enter, 39% Exit

Comments:

North-West Parcel

Based on ITE Trip Generation Manual - 11th Edition

Lobo Plaza (Lomas Blvd. / University Blvd.)

Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| USE (ITE CODE) | 24 HOUR TWO-WAY VOLUME | A. M. PEAK HOUR | | P. M. PEAK HOUR | |
|--|------------------------------|-----------------------|------|-----------------------|------|
| | GROSS | ENTER | EXIT | ENTER | EXIT |
| Strip Retail Plaza <40K - Equation (822) | 410 | 10 | 7 | 21 | 21 |

Units
4.28
1,000 S.F.

ITE Trip Generation Equations:

Average Vehicle Trip Ends on a Weekday (24 HOUR TWO-WAY VOLUME)

$$T = 42.2 (X) + 229.68$$

50% Enter, 50% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7am and 9am (A.M. PEAK HOUR)

$$\ln(T) = 0.66 \ln(X) + 1.84$$

60% Enter, 40% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4pm and 6pm (P.M. PEAK HOUR)

$$\ln(T) = 0.71 \ln(X) + 2.72$$

50% Enter, 50% Exit

Comments:

North-West Parcel

Based on ITE Trip Generation Manual - 11th Edition

Lobo Plaza (Lomas Blvd. / University Blvd.)

Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| USE (ITE CODE) | 24 HOUR TWO-WAY VOLUME | A. M. PEAK HOUR | | P. M. PEAK HOUR | | |
|--------------------|------------------------|-----------------|------|-----------------|------|----|
| | GROSS | ENTER | EXIT | ENTER | EXIT | |
| Hotel (310) | Units 140 | 1,094 | 35 | 28 | 39 | 37 |
| | Rooms | | | | | |

ITE Trip Generation Equations:

Average Vehicle Trip Ends on a Weekday (24 HOUR TWO-WAY VOLUME)

$$T = 10.84 (X) + -423.51$$

50% Enter, 50% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7am and 9am (A.M. PEAK HOUR)

$$T = 0.5 (X) + -7.45$$

56% Enter, 44% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4pm and 6pm (P.M. PEAK HOUR)

$$T = 0.74 (X) + -27.89$$

51% Enter, 49% Exit

Comments:

Based on ITE Trip Generation Manual - 11th Edition

Lobo Plaza (Lomas Blvd. / University Blvd.)

Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| USE (ITE CODE) | 24 HOUR TWO-WAY VOLUME | A. M. PEAK HOUR | | P. M. PEAK HOUR | | |
|--|------------------------------------|-----------------------|-------|-----------------------|-------|------|
| | | GROSS | ENTER | EXIT | ENTER | EXIT |
| High Turnover (Sit-Down) Restaurant (932) | Units 5.62 1,000 S.F. | 602 | 30 | 24 | 31 | 20 |

ITE Trip Generation Equations:

Average Vehicle Trip Ends on a Weekday (24 HOUR TWO-WAY VOLUME)

$$T = 107.2 (X) + 0$$

50% Enter, 50% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7am and 9am (A.M. PEAK HOUR)

$$T = 9.57 (X) + 0$$

55% Enter, 45% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4pm and 6pm (P.M. PEAK HOUR)

$$T = 9.05 (X) + 0$$

61% Enter, 39% Exit

Comments:

South-East Parcel

Based on ITE Trip Generation Manual - 11th Edition

Lobo Plaza (Lomas Blvd. / University Blvd.) Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

| USE (ITE CODE) | 24 HOUR TWO-WAY VOLUME | A. M. PEAK HOUR | | P. M. PEAK HOUR | | |
|--|------------------------------------|-----------------------|-------|-----------------------|-------|------|
| | | GROSS | ENTER | EXIT | ENTER | EXIT |
| Strip Retail Plaza <40K - Equation (822) | Units 4.90 1,000 S.F. | 436 | 11 | 7 | 23 | 23 |

ITE Trip Generation Equations:

Average Vehicle Trip Ends on a Weekday (24 HOUR TWO-WAY VOLUME)

$$T = 42.2 (X) + 229.68$$

50% Enter, 50% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 7am and 9am (A.M. PEAK HOUR)

$$\ln(T) = 0.66 \ln(X) + 1.84$$

60% Enter, 40% Exit

Average Vehicle Trip Ends on a Weekday, Peak Hour of Adjacent Street Traffic, One Hour Between 4pm and 6pm (P.M. PEAK HOUR)

$$\ln(T) = 0.71 \ln(X) + 2.72$$

50% Enter, 50% Exit

Comments:

South East Parcel

Based on ITE Trip Generation Manual - 11th Edition

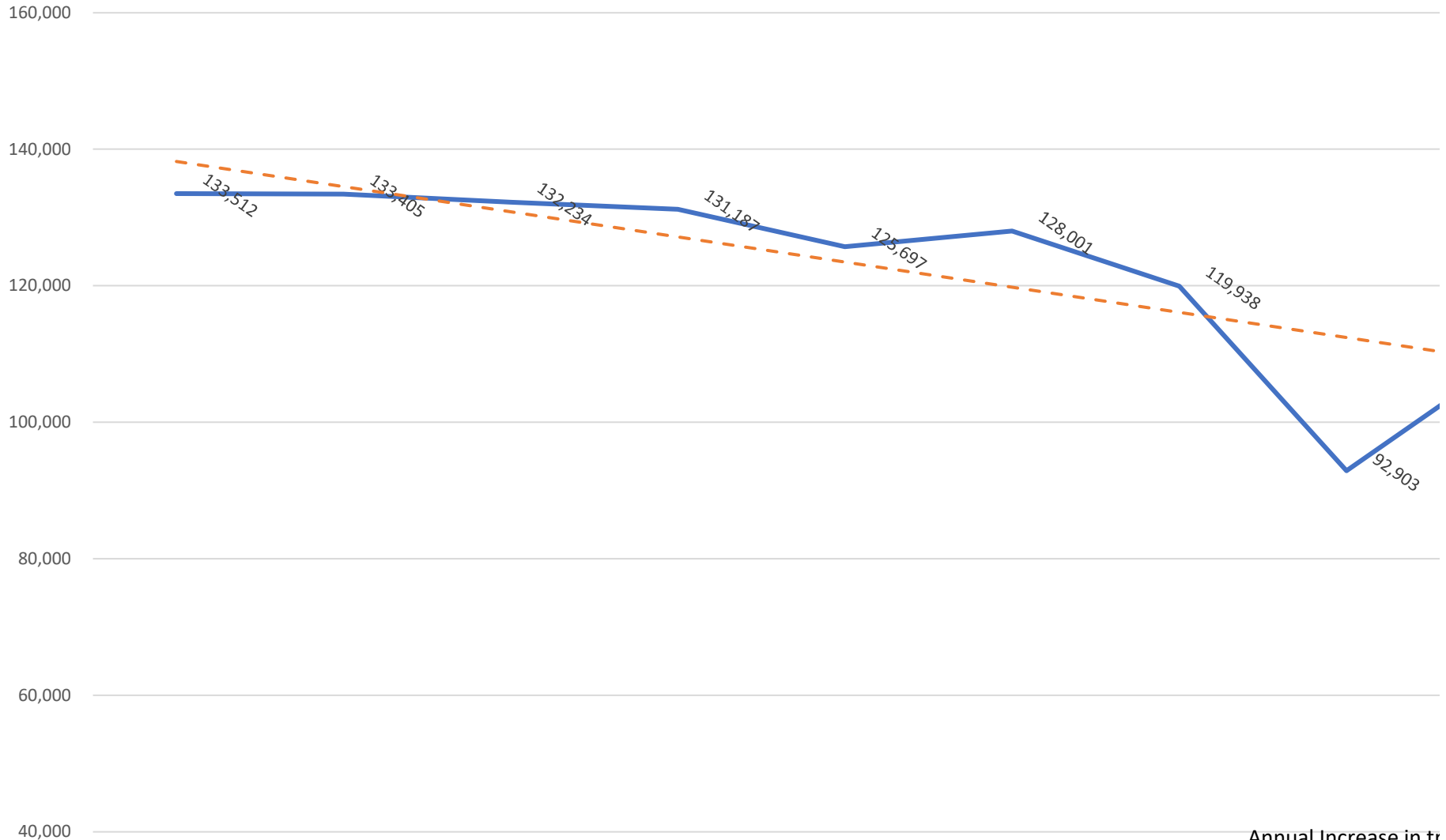
Historic Growth Data Table
LOBO PLAZA
1300 LOMAS BLVD NE, ALBUQUERQUE, NM

Traffic Flows (AWDT) from Mid-Region Council of Governments

| COG ID | Location | | 2013 | 2014 | 2015 | 2016 | 2017 | 2017 | 2019 | 2020 | 2021 | 2022 |
|---|---|--|----------------|----------------|----------------|----------------|----------------|----------------|----------------|---------------|----------------|----------------|
| Intersection #1: LOMAS / LOMAS | | | | | | | | | | | | |
| Street: | Location: | | | | | | | | | | | |
| 24164 LOMAS | EAST OF PAN AM EAST - WEST OF UNIVERSITY | | 32,435 | 32,305 | 32,434 | 29,365 | 29,571 | 29,885 | 28,776 | 21,709 | 24,761 | 25,142 |
| 24356 LOMAS | EAST OF UNIVERSITY - WEST OF YALE | | 32,322 | 31,768 | 31,895 | 31,991 | 33,687 | 33,933 | 33,849 | 25,536 | 30,249 | 27,982 |
| 24320 LOMAS | EAST OF BROADWAY - WEST OF PAN AMERICAN WEST | | 23,311 | 25,510 | 26,326 | 27,116 | 23,951 | 24,081 | 23,325 | 18,749 | 21,526 | 21,852 |
| 24284 UNIVERSITY | NORTH OF LOMAS - SOUTH OF INDIAN SCHOOL | | 23,208 | 21,790 | 21,877 | 22,954 | 21,339 | 21,495 | 15,427 | 12,906 | 16,790 | 17,010 |
| 24872 UNIVERSITY | NORTH OF MARTIN LUTHER KING JR - SOUTH OF LOMAS | | 22,236 | 22,032 | 19,702 | 19,761 | 17,149 | 18,607 | 18,561 | 14,003 | 16,587 | 17,617 |
| Total Intersection Traffic Flows | | | 133,512 | 133,405 | 132,234 | 131,187 | 125,697 | 128,001 | 119,938 | 92,903 | 109,913 | 109,603 |

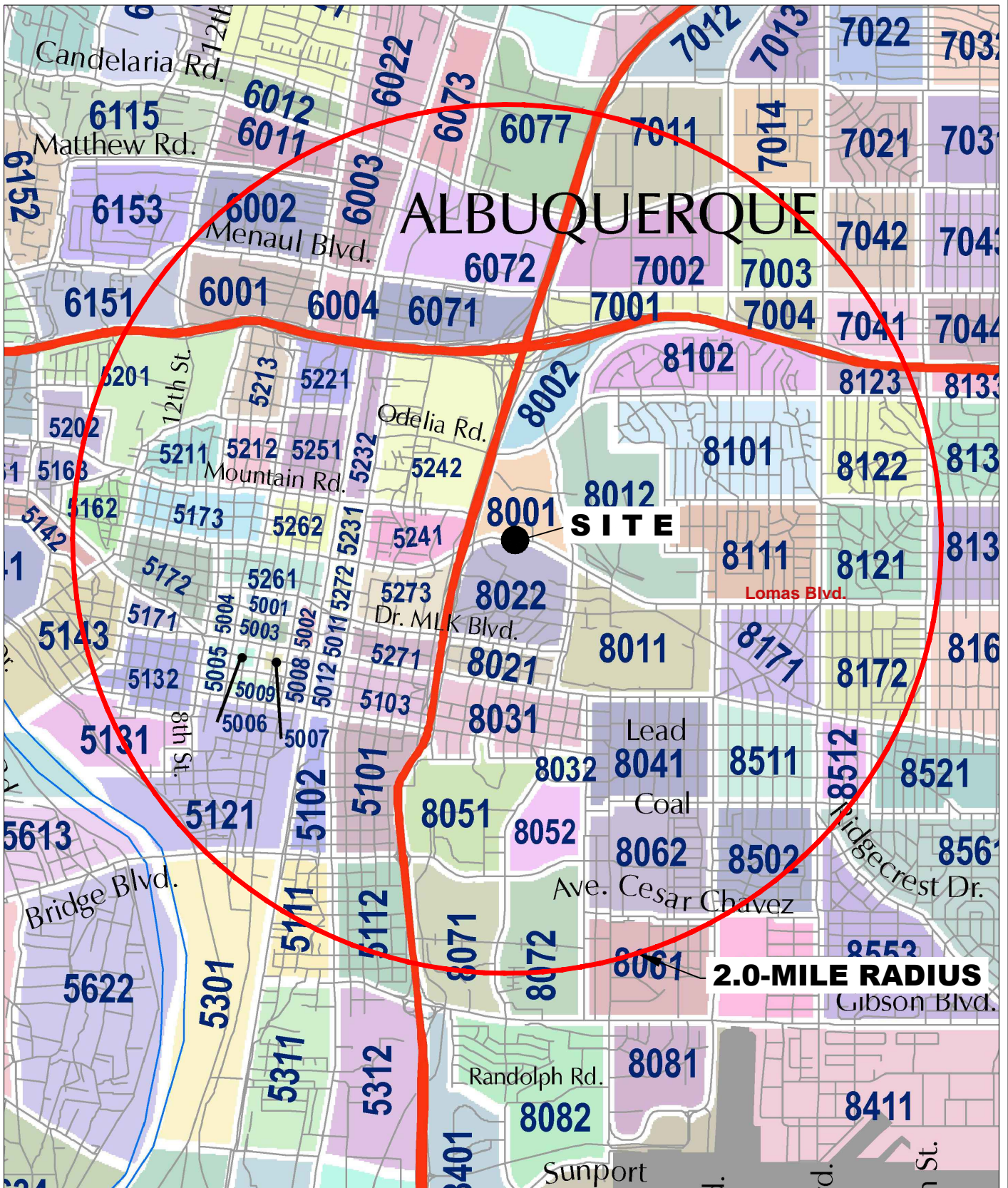
NOTE: Enter Data in cells with dark red text.
 (Intersection Names are entered automatically, but can be overwritten.)

Annual AWDT History (2013 through 2022)



$y = -3683.2x + 141897$
 $R^2 = 0.678$

Annual Increase in tr
Rate of increase = -3
Use - 0.5% Annual G
trend is negative



DATA ANALYSIS SUBZONE (DASZ) MAP

Lobo Plaza Development (Lomas Blvd. West of University Blvd.)

Trip Distribution Table
Lobo Plaza Development (Lomas Blvd.)

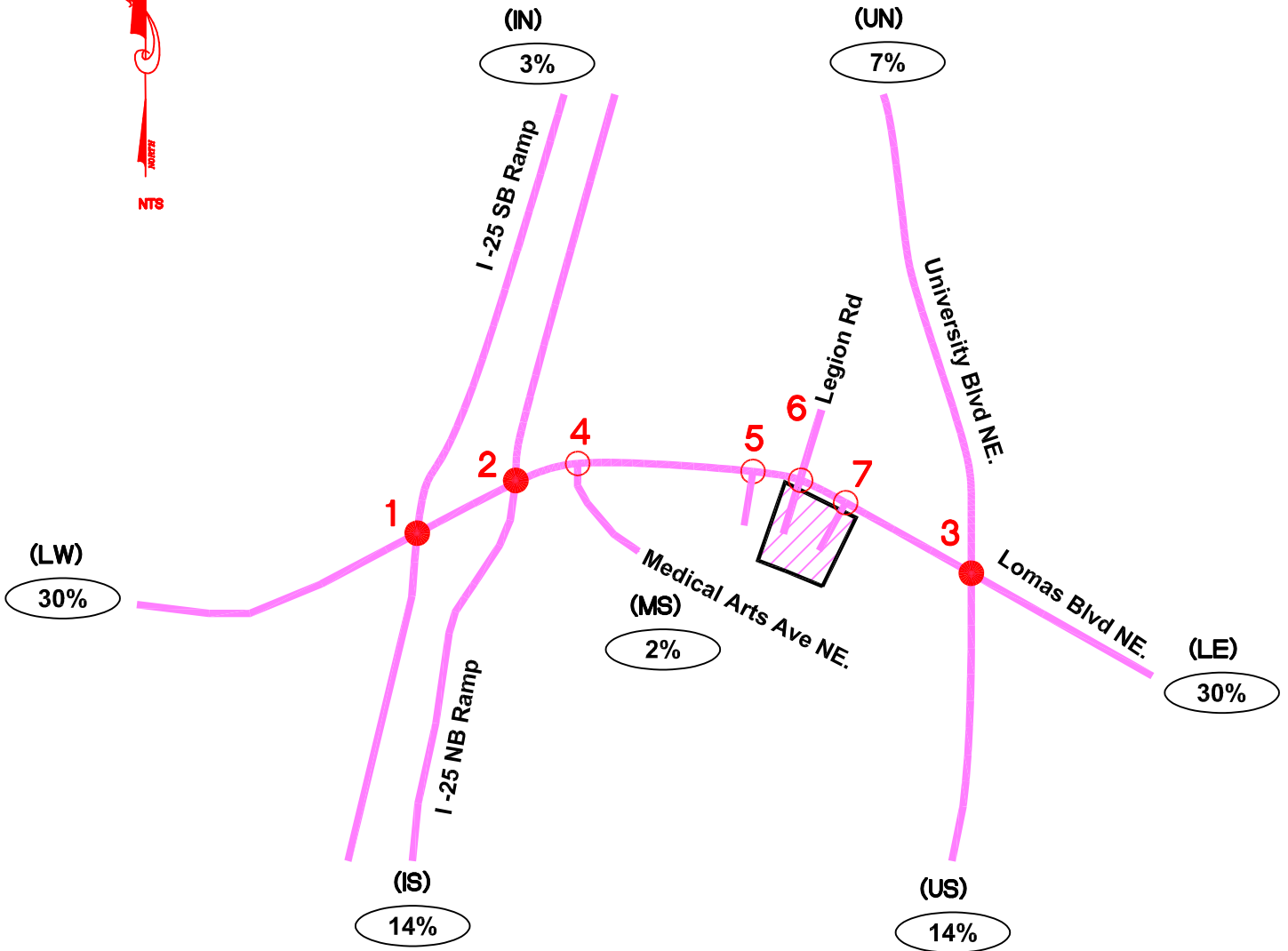
Data Analysis Subzone Population Data for determination of Local Trip Distribution for Proposed **Retail Commercial Trips**

2016 and 2040 Data Taken from Mid-Region Council of Governments
2040 Socioeconomic Forecasts by Data Analysis Subzones for the Mid-Region of New Mexico

| DASZ # | % Sub Area in Study | 2016 | | | 2040 | | | (LW) Lomas Blvd. West | | (IN) I-25 Southbound Ramp North | | (IS) I-25 Northbound Ramp South | | (MS) Medical Arts Ave. South | | (US) University Blvd. South | | (UN) University Blvd. North | | (LE) Lomas Blvd. West | | | | | | |
|--------------------------------|---------------------|------------|------------|--------------------------------------|---------------------|--------------------|-------------|------------------------|------------|---------------------------------|------------------------|---------------------------------|-------------|------------------------------|------------|-----------------------------|------------------------|-----------------------------|-------------|------------------------|------------|-------------|------------------------|------------|----|-------|
| | | Population | Population | Interpolated Population for the Year | Population in Study | Percent Population | % Utilizing | % Population Utilizing | Population | % Utilizing | % Population Utilizing | Population | % Utilizing | % Population Utilizing | Population | % Utilizing | % Population Utilizing | Population | % Utilizing | % Population Utilizing | Population | % Utilizing | % Population Utilizing | Population | | |
| | | 2016 | 2040 | 2028 | | | | | | | | | | | | | | | | | | | | | | |
| Boundary Specified on DASZ Map | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5001 | 100% | 0 | 277 | 139 | 139 | 0.22% | 100% | 0.22% | 139 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5002 | 100% | 0 | 483 | 242 | 242 | 0.39% | 100% | 0.39% | 242 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5003 | 100% | 0 | 181 | 91 | 91 | 0.15% | 100% | 0.15% | 91 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5004 | 100% | 95 | 577 | 336 | 336 | 0.54% | 100% | 0.54% | 336 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5005 | 100% | 447 | 1143 | 795 | 795 | 1.27% | 100% | 1.27% | 795 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5006 | 100% | 10 | 186 | 98 | 98 | 0.16% | 100% | 0.16% | 98 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5007 | 100% | 133 | 497 | 315 | 315 | 0.50% | 100% | 0.50% | 315 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5008 | 100% | 152 | 533 | 343 | 343 | 0.55% | 100% | 0.55% | 343 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5009 | 100% | 413 | 822 | 618 | 618 | 0.99% | 100% | 0.99% | 618 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5011 | 100% | 0 | 754 | 377 | 377 | 0.60% | 100% | 0.60% | 377 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5012 | 100% | 45 | 172 | 109 | 109 | 0.17% | 100% | 0.17% | 109 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5101 | 100% | 1604 | 1729 | 1,667 | 1,667 | 2.66% | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 100% | 2.66% | 1,667 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5102 | 100% | 454 | 497 | 476 | 476 | 0.76% | 20% | 0.15% | 95 | 0% | 0.00% | 0 | 80% | 0.81% | 381 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5103 | 100% | 666 | 964 | 815 | 815 | 1.30% | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 100% | 1.30% | 815 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5111 | 40% | 1133 | 1152 | 1,143 | 457 | 0.73% | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 100% | 0.73% | 457 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5112 | 80% | 1630 | 1582 | 1,606 | 1,285 | 2.05% | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 100% | 2.05% | 1,285 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5121 | 80% | 2677 | 3730 | 3,204 | 2,563 | 4.09% | 80% | 3.28% | 2,050 | 0% | 0.00% | 0 | 20% | 0.82% | 513 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5131 | 40% | 121 | 136 | 129 | 52 | 0.08% | 70% | 0.06% | 36 | 0% | 0.00% | 0 | 30% | 0.02% | 16 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5132 | 98% | 1721 | 2669 | 2,195 | 2,151 | 3.44% | 70% | 2.41% | 1,506 | 0% | 0.00% | 0 | 30% | 1.03% | 645 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5142 | 20% | 348 | 837 | 593 | 119 | 0.19% | 100% | 0.19% | 119 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5143 | 30% | 922 | 1238 | 1,080 | 324 | 0.52% | 90% | 0.47% | 292 | 0% | 0.00% | 0 | 10% | 0.05% | 32 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5162 | 90% | 496 | 848 | 672 | 605 | 0.97% | 100% | 0.97% | 605 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5163 | 10% | 44 | 78 | 61 | 6 | 0.01% | 100% | 0.01% | 6 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5171 | 100% | 501 | 833 | 667 | 667 | 1.07% | 80% | 0.85% | 534 | 0% | 0.00% | 0 | 20% | 0.21% | 133 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5172 | 100% | 1003 | 1654 | 1,329 | 1,329 | 2.12% | 100% | 2.12% | 1,329 | 0% | 0.00% | 0 | 20% | 0.42% | 266 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5173 | 100% | 966 | 1748 | 1,357 | 1,357 | 2.17% | 100% | 2.17% | 1,357 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5201 | 60% | 1133 | 1369 | 1,251 | 751 | 1.20% | 100% | 1.20% | 751 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5202 | 20% | 4 | 0 | 0 | 0 | 0.00% | 100% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5211 | 100% | 781 | 1013 | 897 | 897 | 1.43% | 100% | 1.43% | 897 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5212 | 100% | 452 | 743 | 598 | 598 | 0.96% | 100% | 0.96% | 598 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5213 | 100% | 236 | 266 | 251 | 251 | 0.40% | 100% | 0.40% | 251 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5221 | 100% | 26 | 108 | 67 | 67 | 0.11% | 100% | 0.11% | 67 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5231 | 100% | 0 | 0 | 0 | 0 | 0.00% | 90% | 0.00% | 0 | 10% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5232 | 100% | 0 | 14 | 7 | 7 | 0.01% | 90% | 0.01% | 6 | 10% | 0.00% | 1 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5241 | 100% | 482 | 912 | 697 | 697 | 1.11% | 20% | 0.22% | 139 | 80% | 0.89% | 558 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5242 | 100% | 1642 | 1902 | 1,772 | 1,772 | 2.83% | 30% | 0.85% | 532 | 70% | 1.98% | 1,240 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5251 | 100% | 218 | 489 | 354 | 354 | 0.57% | 100% | 0.57% | 354 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5261 | 100% | 125 | 693 | 409 | 409 | 0.65% | 100% | 0.65% | 409 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5262 | 100% | 68 | 728 | 398 | 398 | 0.64% | 100% | 0.64% | 398 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5271 | 100% | 465 | 1119 | 792 | 792 | 1.27% | 10% | 0.13% | 79 | 90% | 1.14% | 713 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5272 | 100% | 0 | 98 | 49 | 49 | 0.08% | 100% | 0.08% | 49 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5273 | 100% | 656 | 1077 | 867 | 867 | 1.38% | 100% | 1.38% | 867 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 5301 | 5% | 21 | 64 | 43 | 2 | 0.00% | 90% | 0.00% | 2 | 10% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% |
| 6001 | 100% | 498 | 658 | 578 | 578 | 0.92% | 50% | 0.46% | 289 | 50% | 0.09% | 58 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 40% | 0.37% | 231 | 0% | 0.00% |
| 6002 | 80% | 1219 | 2030 | 1,625 | 1,300 | 2.08% | 50% | 1.04% | 650 | 50% | 0.21% | 130 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 40% | 0.83% | 520 | 0% | 0.00% |
| 6003 | 100% | 585 | 1015 | 800 | 800 | 1.28% | 40% | 0.51% | 320 | 60% | 0.13% | 80 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 50% | 0.64% | 400 | 0% | 0.00% |
| 6004 | 100% | 72 | 163 | 128 | 128 | 0.20% | 40% | 0.08% | 51 | 60% | 0.02% | 13 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 50% | 0.10% | 64 | 0% | 0.00% |
| 6011 | 20% | 835 | 835 | 681 | 136 | 0.22% | 100% | 0.02% | 14 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 0% | 0.00% | 0 | 90% | 0.20% | 122 | 0% | 0.00% |
| 6022 | 10% | 1019 | 1439 | 1,229 | 123 | 0.20% | 50% | 0.10% | 62 | 0% | 0 | | | | | | | | | | | | | | | |

Lobo Plaza

1300 Lomas Blvd NE
Trip Distribution Map (%)



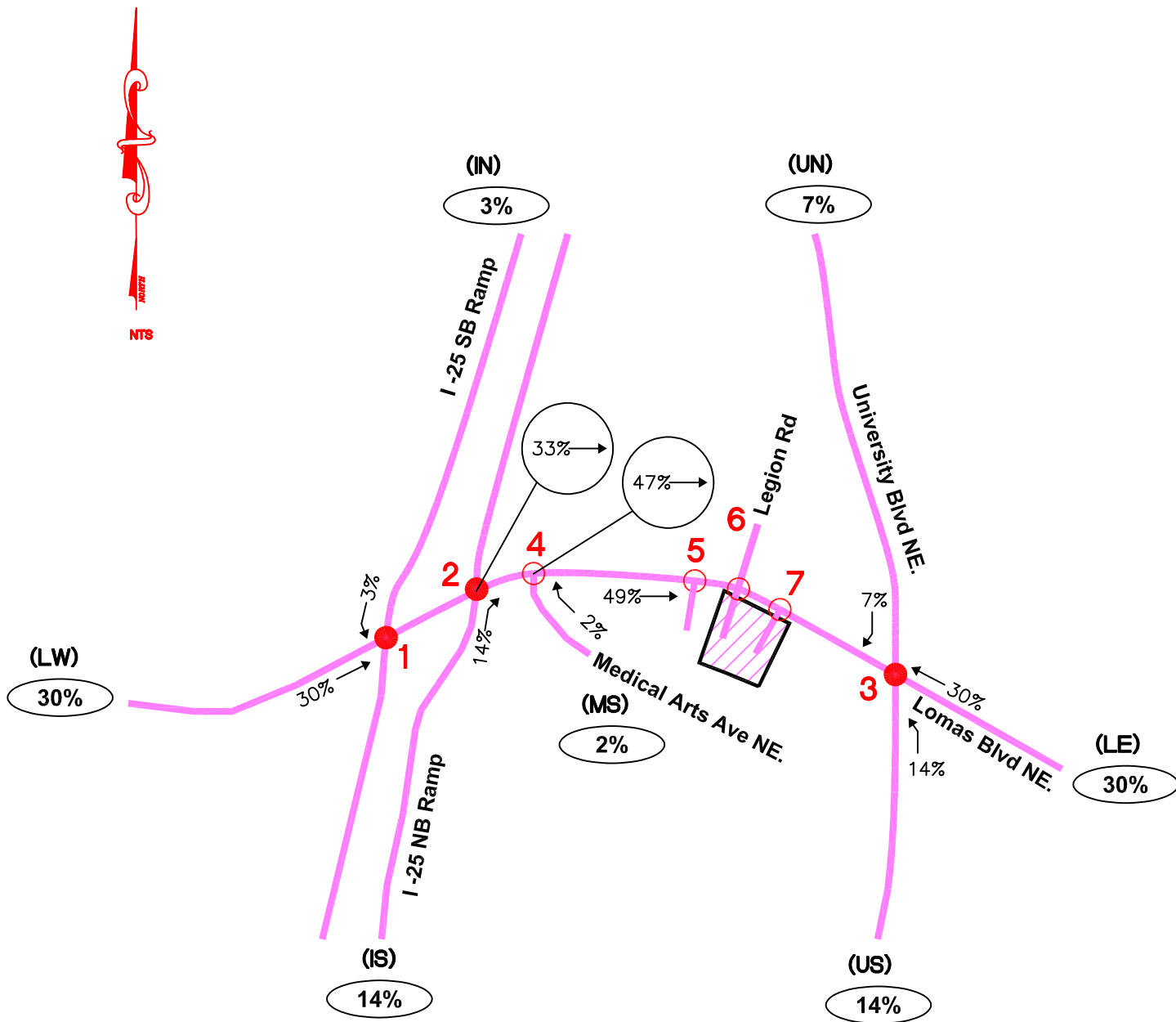
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Entering)



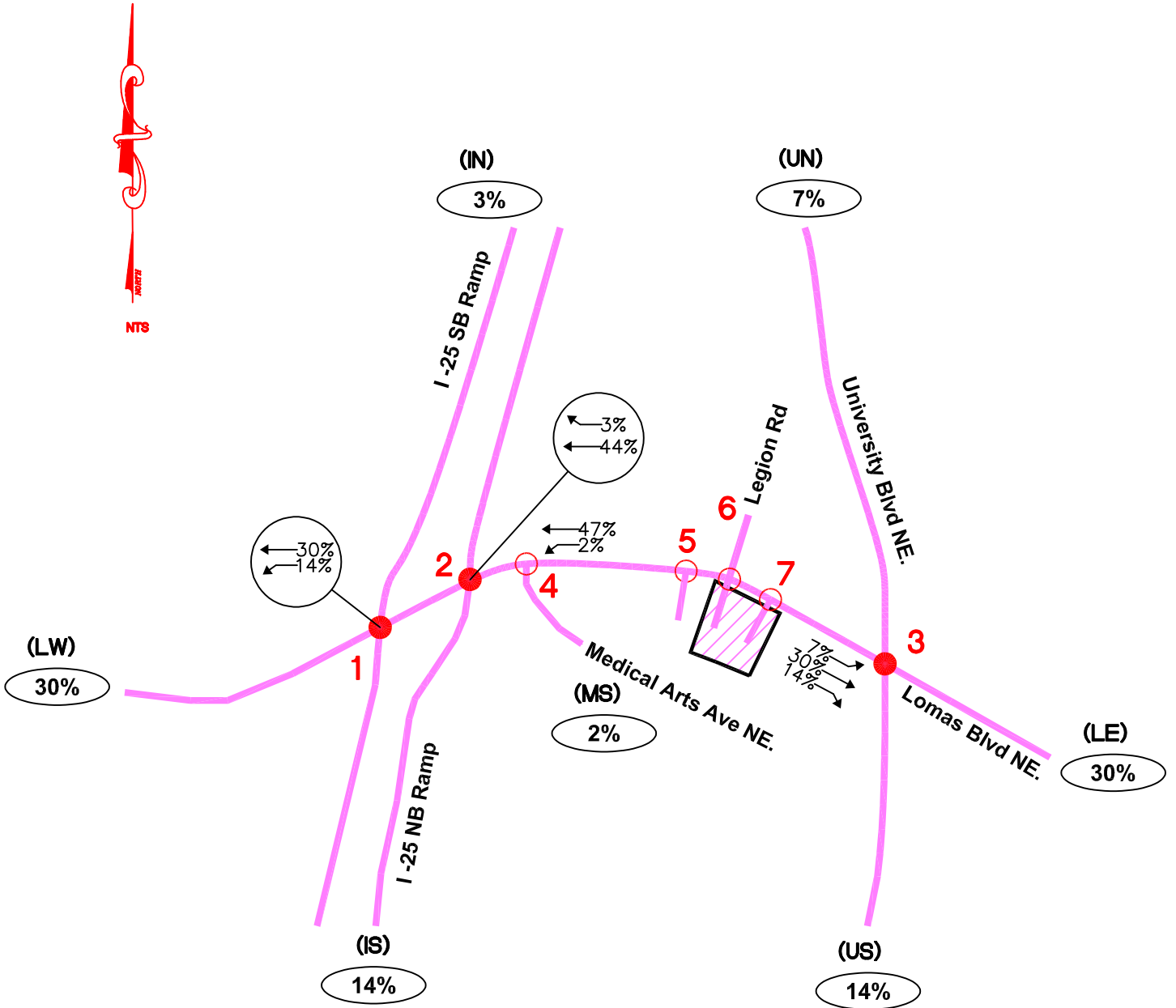
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Exiting)



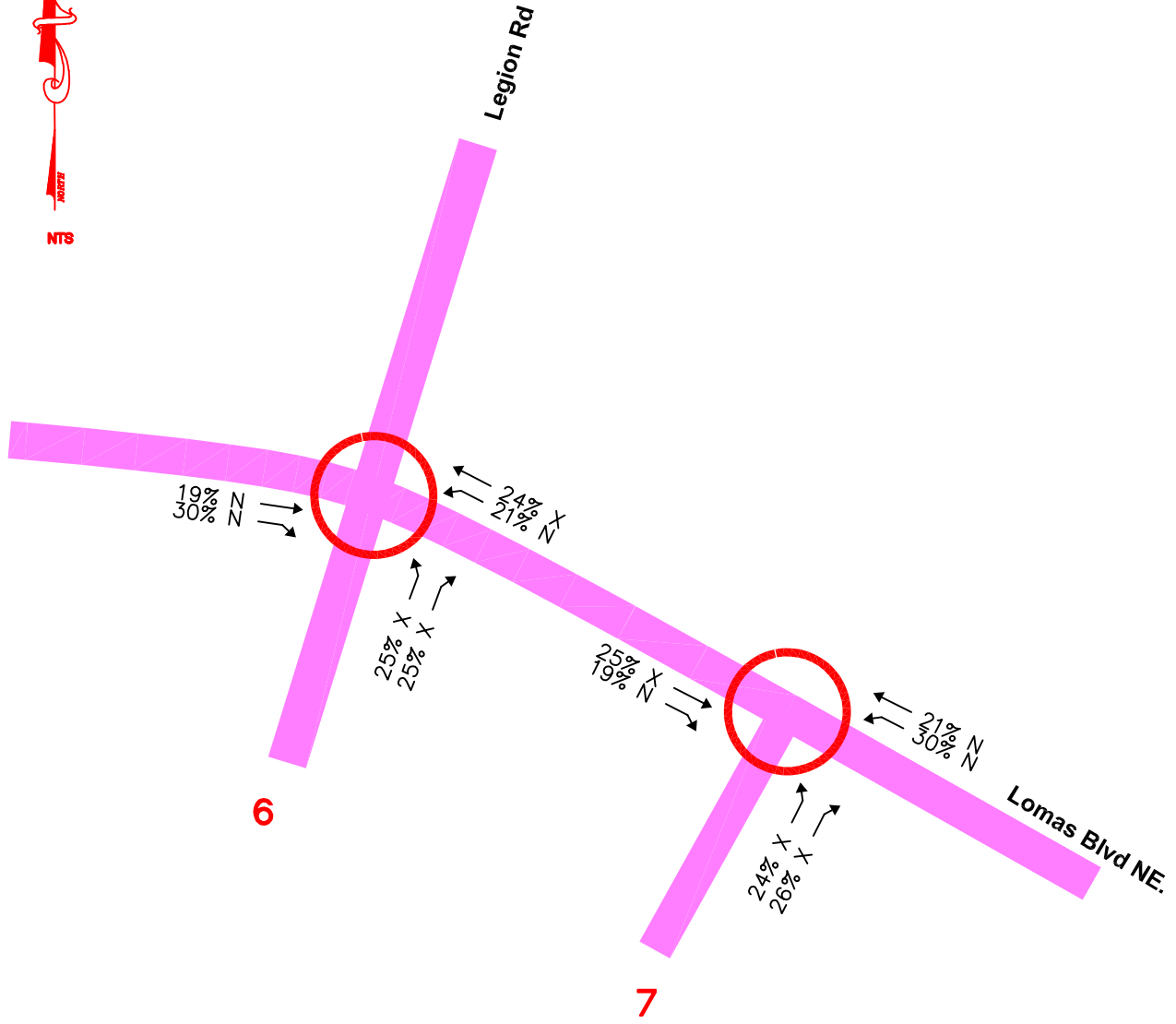
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Entering & % Exiting) DRIVEWAYS



XX% N (INBOUND)
XX% X (OUTBOUND)



SIGNALIZED INTERSECTION



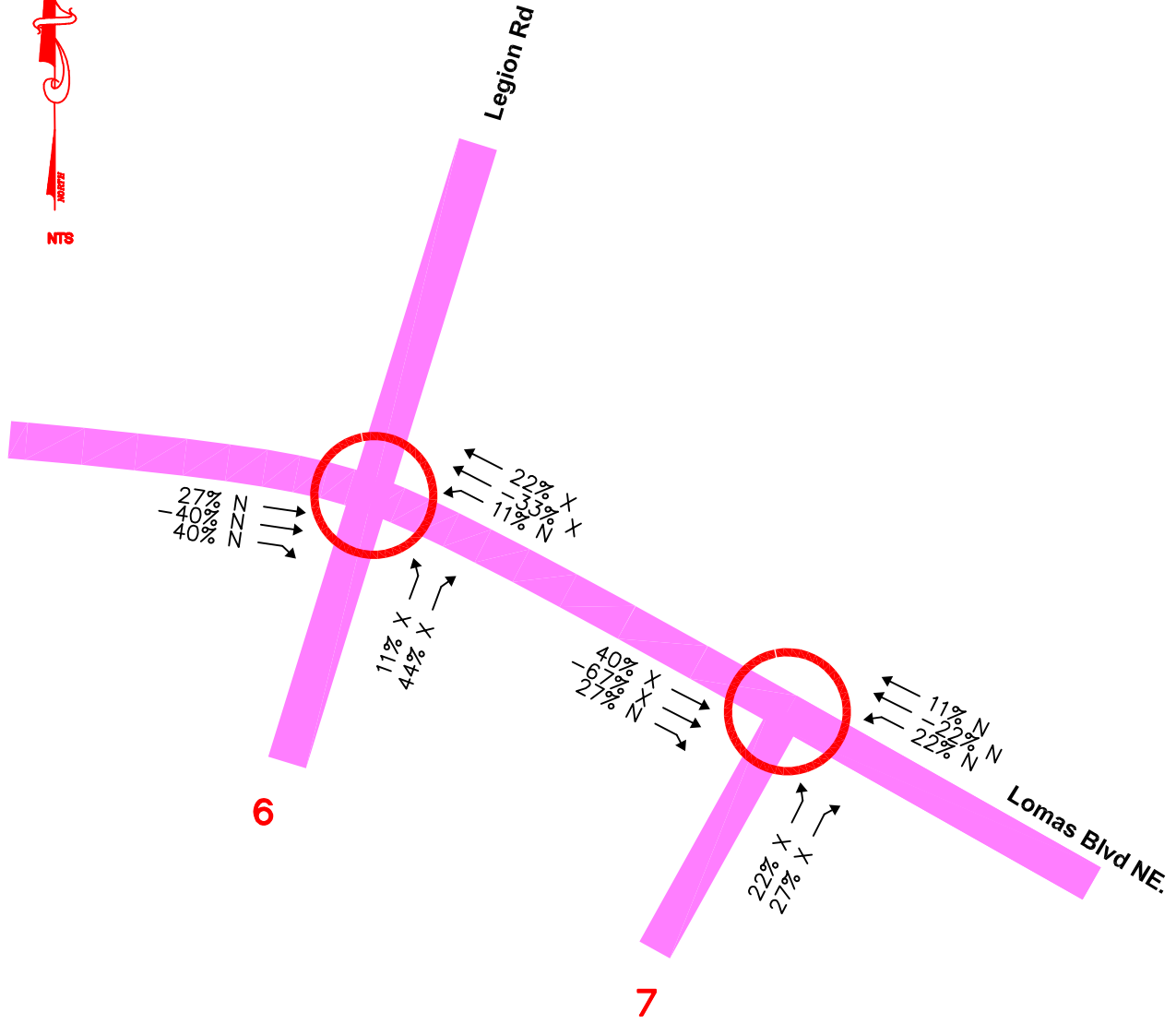
UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Pass-By AM)



XX% N (INBOUND)
XX% X (OUTBOUND)



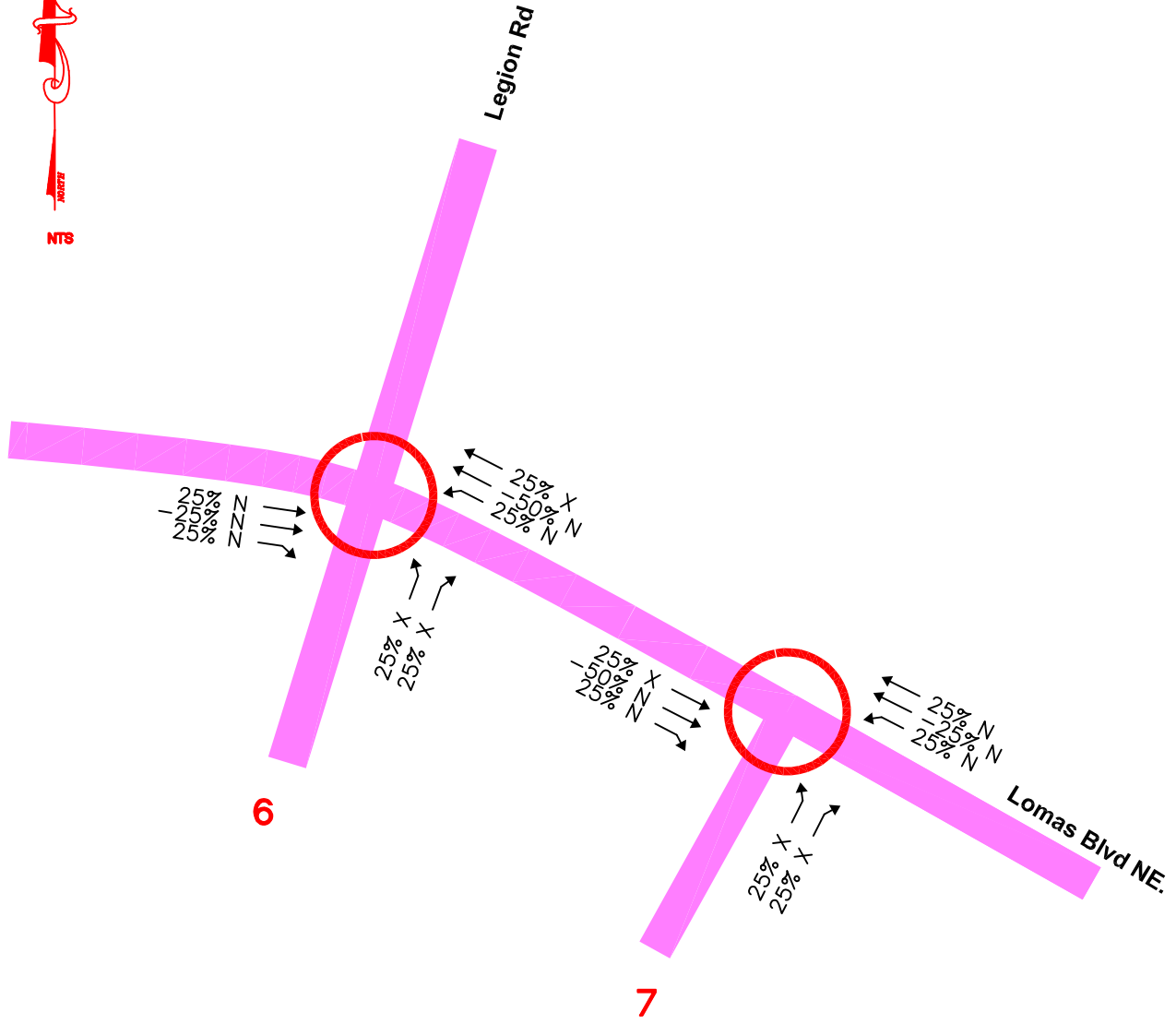
SIGNALIZED INTERSECTION
UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

Lobo Plaza

1300 Lomas Blvd NE

Trip Assignments Map (% Pass-By PM)



XX% N (INBOUND)
XX% X (OUTBOUND)



SIGNALIZED INTERSECTION



UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068

Projected Turning Movements SUMMARY
PROPOSED DEVELOPMENT (2028) - 100% Development

INTERSECTION: **Summary**

Lomas Blvd NE / I - 25 SB Ramp

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (1) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| 2028 (NO BUILD - A.M.) | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 |
| 2028 (BUILD - A.M.) | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| 2028 (NO BUILD - P.M.) | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 |
| 2028 (BUILD - P.M.) | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 |

Lomas Blvd NE / I - 25 NB Ramp

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (2) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 216 | 1,488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| 2028 (NO BUILD - A.M.) | 219 | 1,510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| 2028 (BUILD - A.M.) | 219 | 1,540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| 2028 (NO BUILD - P.M.) | 333 | 723 | 0 | 0 | 1,117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| 2028 (BUILD - P.M.) | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |

Lomas Blvd NE / University Blvd NE

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (3) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| 2028 (NO BUILD - A.M.) | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| 2028 (BUILD - A.M.) | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| 2028 (NO BUILD - P.M.) | 175 | 499 | 179 | 215 | 1,031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| 2028 (BUILD - P.M.) | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |

Lomas Blvd NE / Medical Arts Ave NE

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|----------------------------------|------|-------|----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (4) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 0 | 1,450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| 2028 (NO BUILD - A.M.) | 0 | 1,472 | 256 | 73 | 761 | 0 | 0 | 0 | 69 | 0 | 0 | 0 |
| 2028 (BUILD - A.M.) | 0 | 1,514 | 256 | 74 | 794 | 0 | 0 | 0 | 71 | 0 | 0 | 0 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|----------------------------------|------|-------|----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |
| 2028 (NO BUILD - P.M.) | 0 | 759 | 69 | 28 | 1,689 | 0 | 0 | 0 | 97 | 0 | 0 | 0 |
| 2028 (BUILD - P.M.) | 0 | 803 | 69 | 29 | 1,724 | 0 | 0 | 0 | 99 | 0 | 0 | 0 |

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068

Projected Turning Movements SUMMARY
PROPOSED DEVELOPMENT (2028) - 100% Development

INTERSECTION: **Summary**

Lomas Blvd NE / Torc Dwy / Frtg Rd

1.00

PHF

| (5) | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 1,560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 2028 (NO BUILD - A.M.) | 0 | 1,583 | 0 | 4 | 784 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 2028 (BUILD - A.M.) | 0 | 1,627 | 0 | 4 | 819 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

1.00

PHF

0.0% H.C.
 Existing (2025)
 2028 (NO BUILD - P.M.)
 2028 (BUILD - P.M.)

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 2028 (NO BUILD - P.M.) | 0 | 901 | 0 | 4 | 1,713 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 2028 (BUILD - P.M.) | 0 | 947 | 0 | 4 | 1,749 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

Lomas Blvd NE / West Dwy / Legion Rd

1.00

PHF

(6)
 0.0% H.C.
 Existing (2025)
 2028 (NO BUILD - A.M.)
 2028 (BUILD - A.M.)

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|-----------------------------------|------|-------|-----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 20 | 1,540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| 2028 (NO BUILD - A.M.) | 20 | 1,563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| 2028 (BUILD - A.M.) | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |

1.00

PHF

Existing (2025)
 2028 (NO BUILD - P.M.)
 2028 (BUILD - P.M.)

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|-----------------------------------|------|-------|-----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| 2028 (NO BUILD - P.M.) | 12 | 885 | 4 | 0 | 1,677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| 2028 (BUILD - P.M.) | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |

Lomas Blvd NE / East Dwy

1.00

PHF

(7)
 0.0% H.C.
 Existing (2025)
 2028 (NO BUILD - A.M.)
 2028 (BUILD - A.M.)

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|-----------------------|------|-------|-----------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 1,550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 2028 (NO BUILD - A.M.) | 0 | 1,573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 2028 (BUILD - A.M.) | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

1.00

PHF

Existing (2025)
 2028 (NO BUILD - P.M.)
 2028 (BUILD - P.M.)

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|-----------------------|------|-------|-----------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| 2028 (NO BUILD - P.M.) | 0 | 914 | 0 | 0 | 1,673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| 2028 (BUILD - P.M.) | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / I - 25 SB Ramp

INTERSECTION: E-W Street: **Lomas Blvd NE** (1)
 N-S Street: **I - 25 SB Ramp**

Year of Existing Counts: **2025**
 Implementation Year: **2028**

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|----------|---------------------------|------------|----------|-----------------------------|----------|----------|-----------------------------|------------|------------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| Background Traffic Growth | 0 | 13 | 0 | 1 | 11 | 0 | 0 | 0 | 0 | 12 | 10 | 11 |
| Subtotal (NO BUILD - A.M.) | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 3.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 14.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 27 | 0 | 10 | 21 | 0 | 0 | 0 | 0 | 3 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|----------|---------------------------|------------|----------|-----------------------------|----------|----------|-----------------------------|------------|------------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| Background Traffic Growth | 0 | 13 | 0 | 5 | 14 | 0 | 0 | 0 | 0 | 3 | 6 | 4 |
| Subtotal | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 |
| Subtotal (NO BUILD - P.M.) | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 3.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 14.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 28 | 0 | 10 | 22 | 0 | 0 | 0 | 0 | 3 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 |

Number of Commercial Trips Generated
 Entering: **90** Exiting: **71** A.M. 100% Commercial Development
93 **74** P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|---------------------------------|---------------------------|-----|---|---------------------------|-----|---|-----------------------------|---|---|-----------------------------|-----|-----|
| 2025 AM Peak Hr. Volumes | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| 2025 PM Peak Hr. Volumes | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |

MRCOG Forecast Volumes Worksheet

Based on 2025 Traffic Count

| | | | | |
|----------------------------|------------|--------------|----------|--------------|
| 2025 AM Link Volume | 875 | 784 | 0 | 2,236 |
| 2025 PM Link Volume | 864 | 1,236 | 0 | 860 |

Based on MRCOG Model (2040 Data Set)

| | | | | |
|----------------------------|-------------|-------------|-------------|-------------|
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |

Growth Rate to Apply to Existing Counts to Match 2040 Forecasts

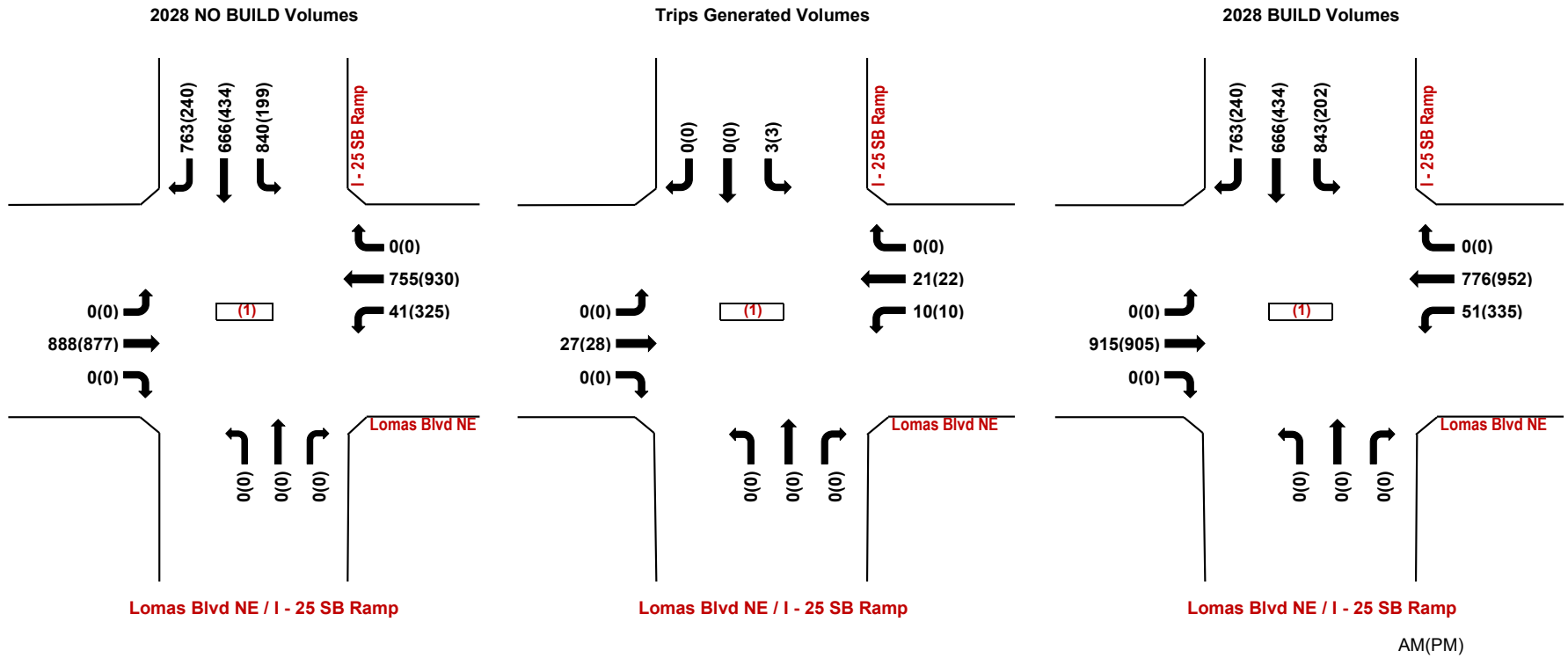
| | | | | |
|---------------------------|--------------|--------------|----------------|---------------|
| 2025-2040 AM Growth Rates | 4.52% | 0.54% | #DIV/0! | -4.35% |
| 2025-2040 PM Growth Rates | 0.46% | 2.79% | #DIV/0! | 5.22% |

Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts

| | | | | |
|---------------------------|--------|-------|-------|--------|
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|----------------------------|---------------------------|----------|----------|---------------------------|----------|----------|-----------------------------|----------|----------|-----------------------------|----------|--|
| AM Pass-by Trips | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net AM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| PM Pass-by Trips | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net PM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Entering | 25 | 19 | AM | | | | | | | | | |
| Exiting | 32 | 26 | PM | | | | | | | | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / I - 25 NB Ramp

INTERSECTION: E-W Street: **Lomas Blvd NE** (2)
 N-S Street: **I - 25 NB Ramp**

Year of Existing Counts: 2025
 Implementation Year: 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|----------|---------------------------|------------|------------|-----------------------------|------------|------------|-----------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 216 | 1,488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| Background Traffic Growth | 3 | 22 | 0 | 0 | 10 | 3 | 2 | 8 | 2 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 219 | 1,510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 44.00% | 3.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 30 | 0 | 0 | 31 | 2 | 0 | 0 | 13 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 219 | 1,540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 219 | 1,540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|----------|---------------------------|--------------|------------|-----------------------------|------------|------------|-----------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| Background Traffic Growth | 5 | 11 | 0 | 0 | 17 | 9 | 2 | 8 | 2 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 333 | 723 | 0 | 0 | 1,117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 44.00% | 3.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 31 | 0 | 0 | 33 | 2 | 0 | 0 | 13 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |

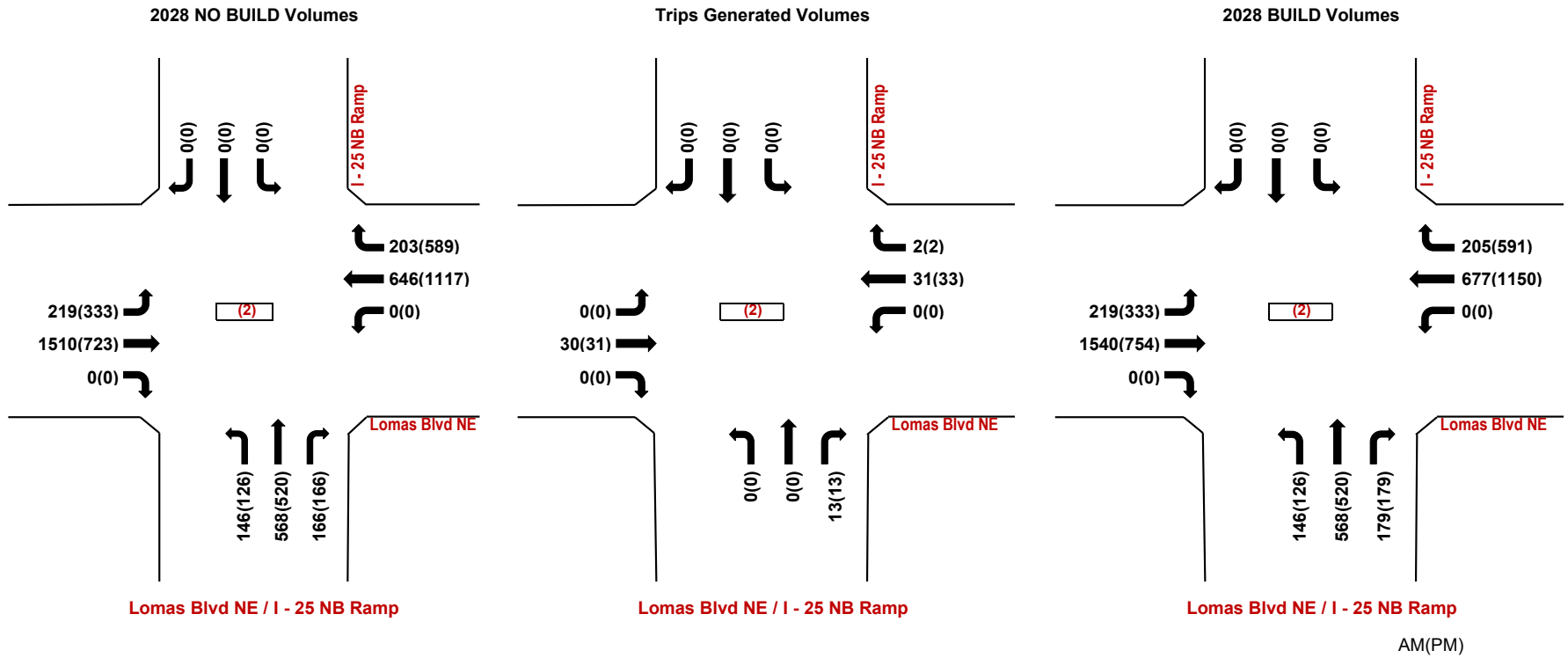
Number of Commercial Trips Generated: Entering 90, Exiting 71, A.M. 100% Commercial Development, 93, 74, P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|-----|-----------------------------|-----|-----|-----------------------------|---|---|
| 2025 AM Peak Hr. Volumes | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| 2025 PM Peak Hr. Volumes | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|-------|---------|
| 2025 AM Link Volume | 1,704 | 836 | 868 | 0 |
| 2025 PM Link Volume | 1,040 | 1,680 | 800 | 0 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.92% | 0.10% | 5.69% | #DIV/0! |
| 2025-2040 PM Growth Rates | -0.75% | 0.29% | 4.91% | #DIV/0! |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | |
|----------------------------|-------------------------|-------------------------------|----------|----------|
| AM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25, Exiting 32 | Entering 19 AM, Exiting 26 PM | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / University Blvd NE

INTERSECTION : E-W Street: **Lomas Blvd NE** (3)
 N-S Street: **University Blvd NE**

Year of Existing Counts 2025
 Implementation Year 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|------------|---------------------------|------------|------------|---------------------------------|------------|------------|---------------------------------|------------|-----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| Background Traffic Growth | 4 | 13 | 7 | 3 | 9 | 3 | 1 | 7 | 3 | 2 | 11 | 1 |
| Subtotal (NO BUILD - A.M.) | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 0.00% | 0.00% | 30.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 7.00% |
| Percent Commercial Trips Generated(Exiting) | 7.00% | 30.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 5 | 21 | 10 | 0 | 27 | 0 | 13 | 0 | 0 | 0 | 0 | 6 |
| Subtotal AM Pk Hr. BUILD Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|------------|---------------------------|--------------|------------|---------------------------------|------------|------------|---------------------------------|------------|------------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| Background Traffic Growth | 3 | 7 | 3 | 3 | 15 | 2 | 4 | 9 | 4 | 2 | 9 | 5 |
| Subtotal (NO BUILD - P.M.) | 175 | 499 | 179 | 215 | 1,031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 0.00% | 0.00% | 30.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 7.00% |
| Percent Commercial Trips Generated(Exiting) | 7.00% | 30.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 5 | 22 | 10 | 0 | 28 | 0 | 13 | 0 | 0 | 0 | 0 | 7 |
| Subtotal PM Pk Hr. BUILD Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |

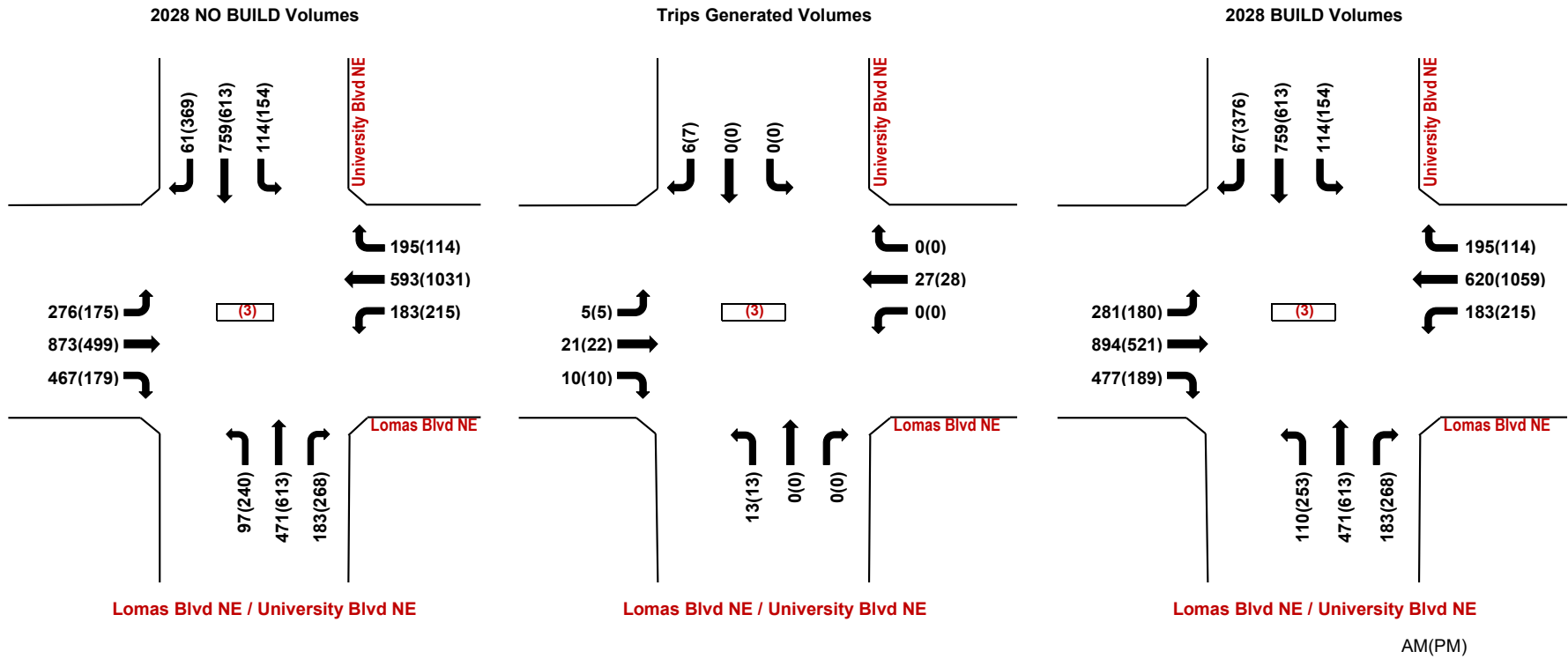
Number of Commercial Trips Generated
 Entering 90 Exiting 71 A.M. 100% Commercial Development
 93 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|--------------------------|---------------------------|-----|-----|---------------------------|-------|-----|---------------------------------|-----|-----|---------------------------------|-----|-----|
| 2025 AM Peak Hr. Volumes | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| 2025 PM Peak Hr. Volumes | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|--------|-------|--------|
| 2025 AM Link Volume | 1,592 | 956 | 740 | 920 |
| 2025 PM Link Volume | 840 | 1,340 | 1,104 | 1,120 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.52% | -0.75% | 7.83% | -1.04% |
| 2025-2040 PM Growth Rates | 0.66% | 2.05% | 1.72% | 2.46% |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | |
|----------------------------|-------------|---------------|----------|----------|
| AM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | |
| | 32 | 26 PM | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / Medical Arts Ave NE

INTERSECTION : E-W Street: **Lomas Blvd NE** (4)
 N-S Street: **Medical Arts Ave NE**

Year of Existing Counts 2025
 Implementation Year 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|------------|---------------------------|------------|----------|----------------------------------|----------|-----------|----------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 22 | 4 | 1 | 11 | 0 | 0 | 0 | 1 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,472 | 256 | 73 | 761 | 0 | 0 | 0 | 69 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 2.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 2.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 42 | 0 | 1 | 33 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,514 | 256 | 74 | 794 | 0 | 0 | 0 | 71 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,514 | 256 | 74 | 794 | 0 | 0 | 0 | 71 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|-----------|---------------------------|--------------|----------|----------------------------------|----------|-----------|----------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 11 | 1 | 0 | 25 | 0 | 0 | 0 | 1 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 0 | 759 | 69 | 28 | 1,689 | 0 | 0 | 0 | 97 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 2.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 2.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 44 | 0 | 1 | 35 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 803 | 69 | 29 | 1,724 | 0 | 0 | 0 | 99 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 803 | 69 | 29 | 1,724 | 0 | 0 | 0 | 99 | 0 | 0 | 0 |

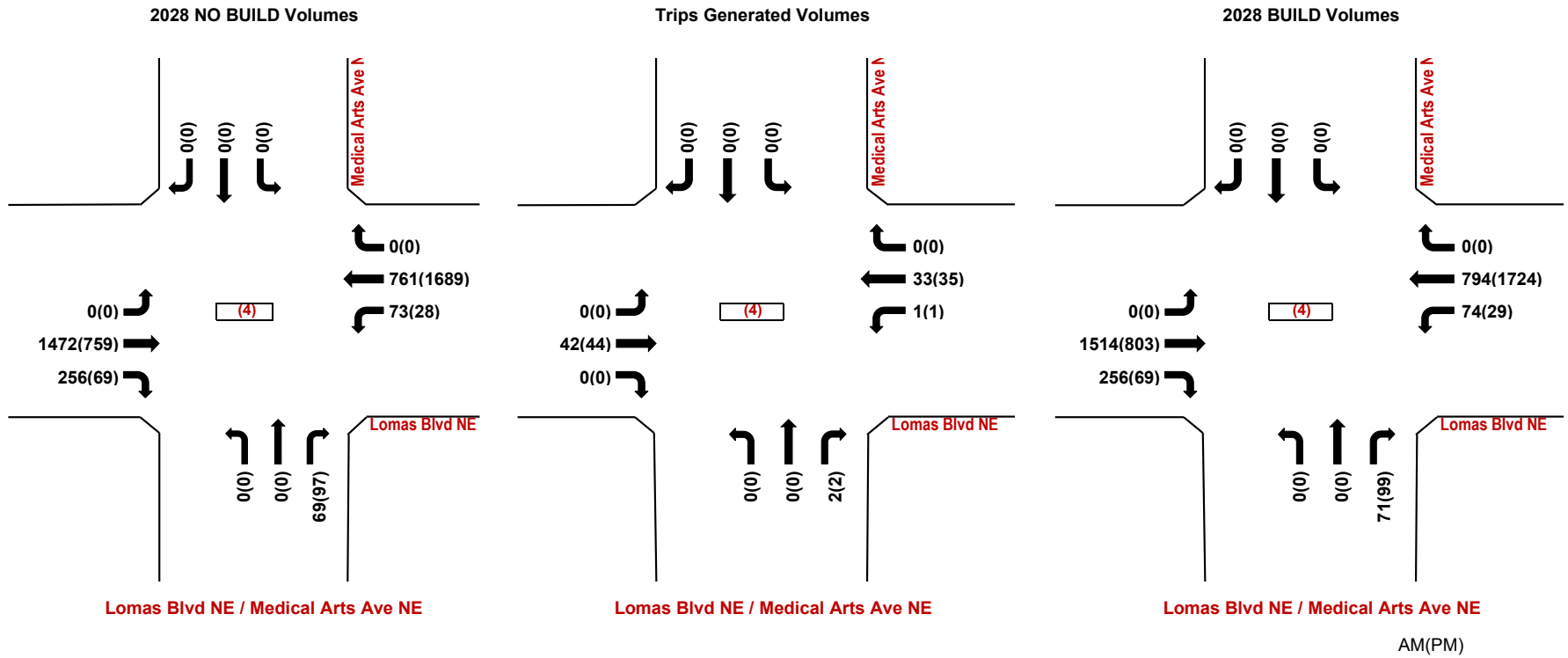
Number of Commercial Trips Generated
 Entering 90 71 A.M. 100% Commercial Development
 Exiting 93 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|--------------------------|---------------------------|------|-----|---------------------------|-------|---|----------------------------------|---|----|----------------------------------|---|---|
| 2025 AM Peak Hr. Volumes | 0 | 1450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| 2025 PM Peak Hr. Volumes | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|---------|---------|
| 2025 AM Link Volume | 1,702 | 822 | 68 | 0 |
| 2025 PM Link Volume | 816 | 1,692 | 96 | 0 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.92% | 0.21% | 151.08% | #DIV/0! |
| 2025-2040 PM Growth Rates | 0.87% | 0.24% | 89.79% | #DIV/0! |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | |
|----------------------------|-------------|---------------|----------|----------|
| AM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | |
| | 32 | 26 PM | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / Torc Dwy / Frtg Rd

INTERSECTION: E-W Street: **Lomas Blvd NE** (5)
 N-S Street: **Torc Dwy / Frtg Rd**

Year of Existing Counts: 2025
 Implementation Year: 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------|-------|---------------------------|--------|-------|---------------------------------|-------|-------|---------------------------------|-------|-------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 23 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,583 | 0 | 4 | 784 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 44 | 0 | 0 | 35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,627 | 0 | 4 | 819 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,627 | 0 | 4 | 819 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------|-------|---------------------------|--------|-------|---------------------------------|-------|-------|---------------------------------|-------|-------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 13 | 0 | 0 | 25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 0 | 901 | 0 | 4 | 1,713 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 46 | 0 | 0 | 36 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 947 | 0 | 4 | 1,749 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 947 | 0 | 4 | 1,749 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

Number of Commercial Trips Generated: Entering 90, Exiting 71 A.M. 100% Commercial Development
 93, 74 P.M.

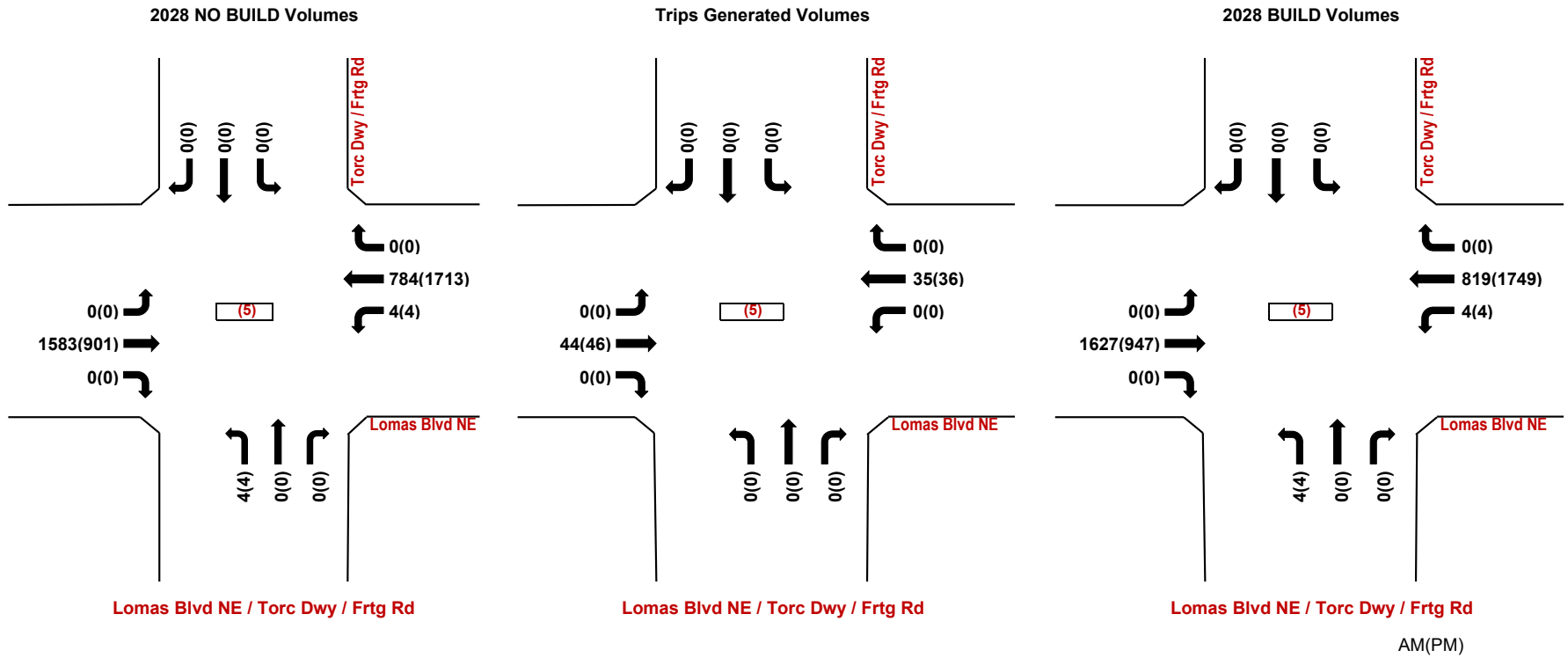
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|---------------------------------|---|---|---------------------------------|---|--|
| 2025 AM Peak Hr. Volumes | 0 | 1560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | |
| 2025 PM Peak Hr. Volumes | 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|-------|---------|
| 2025 AM Link Volume | 1,560 | 776 | 4 | 0 |
| 2025 PM Link Volume | 888 | 1,692 | 4 | 0 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.39% | 0.62% | ##### | #DIV/0! |
| 2025-2040 PM Growth Rates | 0.26% | 0.24% | ##### | #DIV/0! |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|----------------------------|---------------------------|---------------|-------|---------------------------|-------|-------|---------------------------------|-------|-------|---------------------------------|-------|--|
| AM Pass-by Trips | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net AM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| PM Pass-by Trips | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Percent Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net PM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | | | | | | | | | |
| | 32 | 26 PM | | | | | | | | | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / West Dwy / Legion Rd

INTERSECTION : E-W Street: **Lomas Blvd NE** (6)
 N-S Street: **West Dwy / Legion Rd**

Year of Existing Counts 2025
 Implementation Year 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|-----------|---------------------------|------------|----------|-----------------------------------|----------|-----------|-----------------------------------|----------|-----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 20 | 1,540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Background Traffic Growth | 0 | 23 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 20 | 1,563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 17 | 27 | 19 | 17 | 0 | 18 | 0 | 18 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 20 | 1,580 | 27 | 19 | 788 | 4 | 18 | 0 | 18 | 8 | 0 | 12 |
| Pass-by Trip Adjustments | 0 | -3 | 10 | 3 | -4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|-----------|---------------------------|--------------|----------|-----------------------------------|----------|-----------|-----------------------------------|----------|-----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| Background Traffic Growth | 0 | 13 | 0 | 0 | 25 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| Subtotal (NO BUILD - P.M.) | 12 | 885 | 4 | 0 | 1,677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 18 | 28 | 20 | 18 | 0 | 19 | 0 | 19 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 12 | 903 | 32 | 20 | 1,695 | 4 | 19 | 0 | 27 | 4 | 0 | 41 |
| Pass-by Trip Adjustments | 0 | 0 | 8 | 8 | -9 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |

Number of Commercial Trips Generated
 Entering 90 71 A.M. 100% Commercial Development
 Exiting 93 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|-----------------------------------|---|---|-----------------------------------|---|----|
| 2025 AM Peak Hr. Volumes | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| 2025 PM Peak Hr. Volumes | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |

MRCOG Forecast Volumes Worksheet

Based on 2025 Traffic Count

| | | | | |
|---------------------|-------|-------|---|----|
| 2025 AM Link Volume | 1,560 | 764 | 0 | 20 |
| 2025 PM Link Volume | 888 | 1,656 | 8 | 44 |

Based on MRCOG Model (2040 Data Set)

| | | | | |
|---------------------|------|------|------|------|
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |

Growth Rate to Apply to Existing Counts to Match 2040 Forecasts

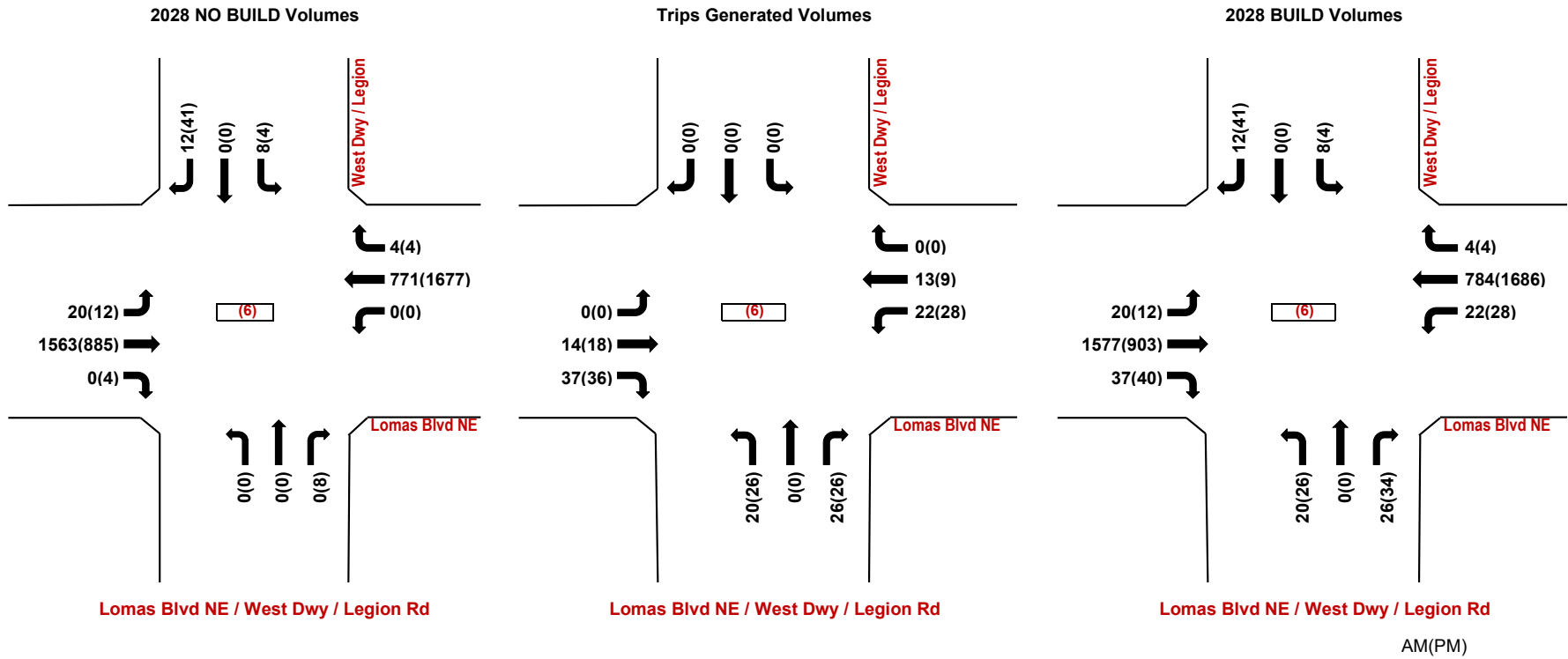
| | | | | |
|---------------------------|--------|-------|---------|---------|
| 2025-2040 AM Growth Rates | -0.39% | 0.73% | #DIV/0! | 252.33% |
| 2025-2040 PM Growth Rates | 0.26% | 0.39% | ##### | 225.76% |

Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts

| | | | | |
|---------------------------|--------|-------|-------|--------|
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|----------------------------|---------------------------|-----------|-----------|---------------------------|-----------|----------|-----------------------------------|----------|----------|-----------------------------------|----------|----------|
| AM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | -13.00% | 40.00% | 11.00% | -33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | -3 | 10 | 3 | -8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 22.00% | 0.00% | 11.00% | 0.00% | 40.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | -3 | 10 | 3 | -4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | 0.00% | 25.00% | 25.00% | -50.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 8 | 8 | -16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 8 | 8 | -9 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Entering | 25 | 19 | AM | | | | | | | | | |
| Exiting | 32 | 26 | PM | | | | | | | | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / East Dwy

INTERSECTION: E-W Street: **Lomas Blvd NE** (7)
 N-S Street: **East Dwy**

Year of Existing Counts: 2025
 Implementation Year: 2028

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------|--------|---------------------------|--------|-------|-----------------------|-------|--------|-----------------------|-------|-------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Background Traffic Growth | 0 | 23 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 26.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 18 | 17 | 27 | 19 | 0 | 17 | 0 | 18 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,591 | 17 | 27 | 782 | 0 | 17 | 0 | 18 | 0 | 0 | 4 |
| Pass-by Trip Adjustments | 0 | -9 | 7 | 6 | -3 | 0 | 4 | 0 | 5 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------|--------|---------------------------|--------|-------|-----------------------|-------|--------|-----------------------|-------|-------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Background Traffic Growth | 0 | 14 | 0 | 0 | 25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 0 | 914 | 0 | 0 | 1,673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 26.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 19 | 18 | 28 | 20 | 0 | 18 | 0 | 19 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 933 | 18 | 28 | 1,693 | 0 | 18 | 0 | 19 | 0 | 0 | 12 |
| Pass-by Trip Adjustments | 0 | -9 | 8 | 8 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |

Number of Commercial Trips Generated

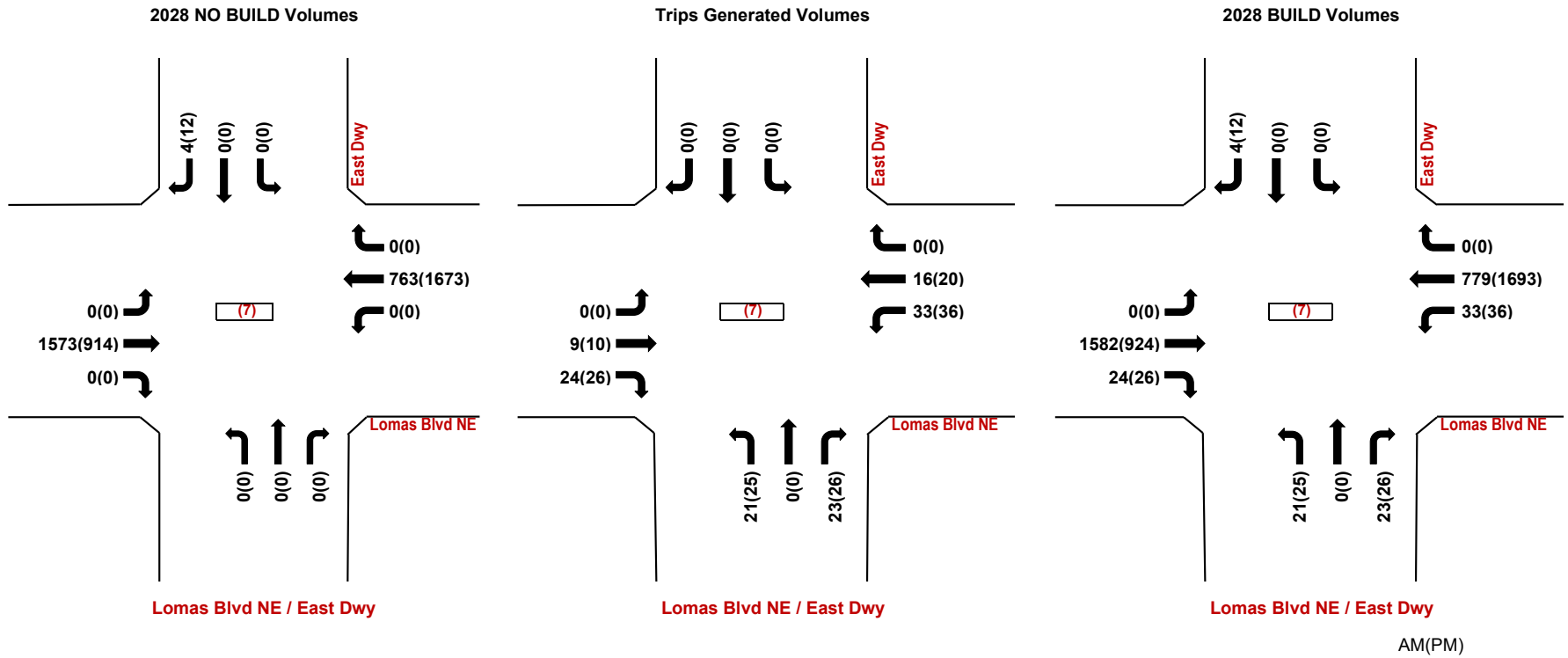
| | | | | |
|----------|----|----|------|-----------------------------|
| Entering | 90 | 71 | A.M. | 100% Commercial Development |
| Exiting | 93 | 74 | P.M. | |

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|-----------------------|---|---|-----------------------|----|--|
| 2025 AM Peak Hr. Volumes | 0 | 1550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 4 | |
| 2025 PM Peak Hr. Volumes | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 12 | |

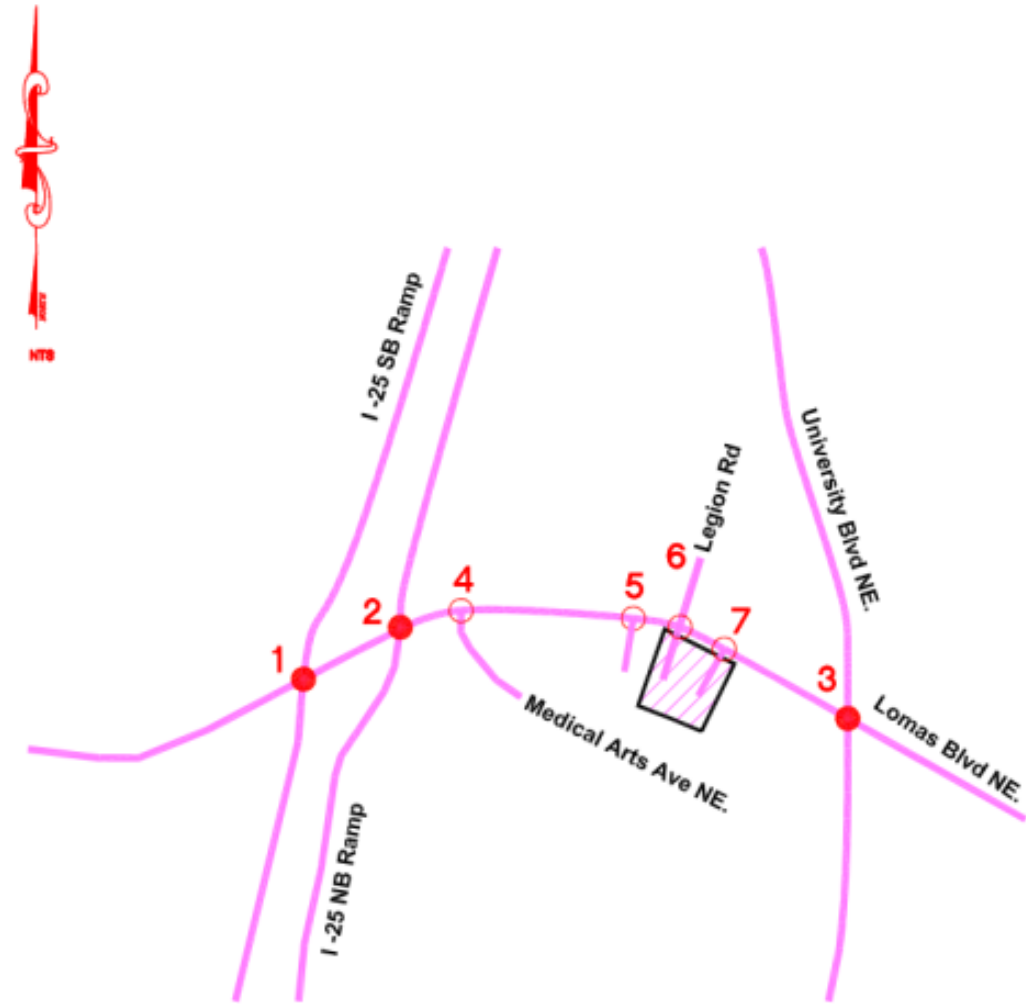
MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|---------|---------|
| 2025 AM Link Volume | 1,550 | 752 | 0 | 4 |
| 2025 PM Link Volume | 900 | 1,648 | 0 | 12 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.35% | 0.85% | #DIV/0! | ##### |
| 2025-2040 PM Growth Rates | 0.17% | 0.42% | #DIV/0! | 845.56% |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | | | | | | | | | |
|----------------------------|---------------------------|---------|--------|---------------------------|---------|-------|-----------------------|-------|--------|-----------------------|-------|--|
| AM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | | | | | | | | | | | | |
| Volume Entering | | | | | | | | | | | | |
| Percent Exiting | | | | | | | | | | | | |
| Volume Exiting | | | | | | | | | | | | |
| Net AM Passby Trips | | | | | | | | | | | | |
| PM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | | | | | | | | | | | | |
| Volume Entering | | | | | | | | | | | | |
| Percent Exiting | | | | | | | | | | | | |
| Volume Exiting | | | | | | | | | | | | |
| Net PM Passby Trips | | | | | | | | | | | | |
| Pass-by Trips | | | | | | | | | | | | |
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| Percent Entering | 0.00% | -67.00% | 27.00% | 22.00% | -11.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | -17 | 7 | 6 | -3 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 40.00% | 0.00% | 0.00% | 0.00% | 0.00% | 22.00% | 0.00% | 27.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 8 | 0 | 0 | 0 | 0 | 4 | 0 | 5 | 0 | 0 | |
| Net AM Passby Trips | 0 | -9 | 7 | 6 | -3 | 0 | 4 | 0 | 5 | 0 | 0 | |
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| Percent Entering | 0.00% | -50.00% | 25.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | -16 | 8 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 7 | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | |
| Net PM Passby Trips | 0 | -9 | 8 | 8 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | |
| Entering | 25 | 19 | AM | | | | | | | | | |
| Exiting | 32 | 26 | PM | | | | | | | | | |



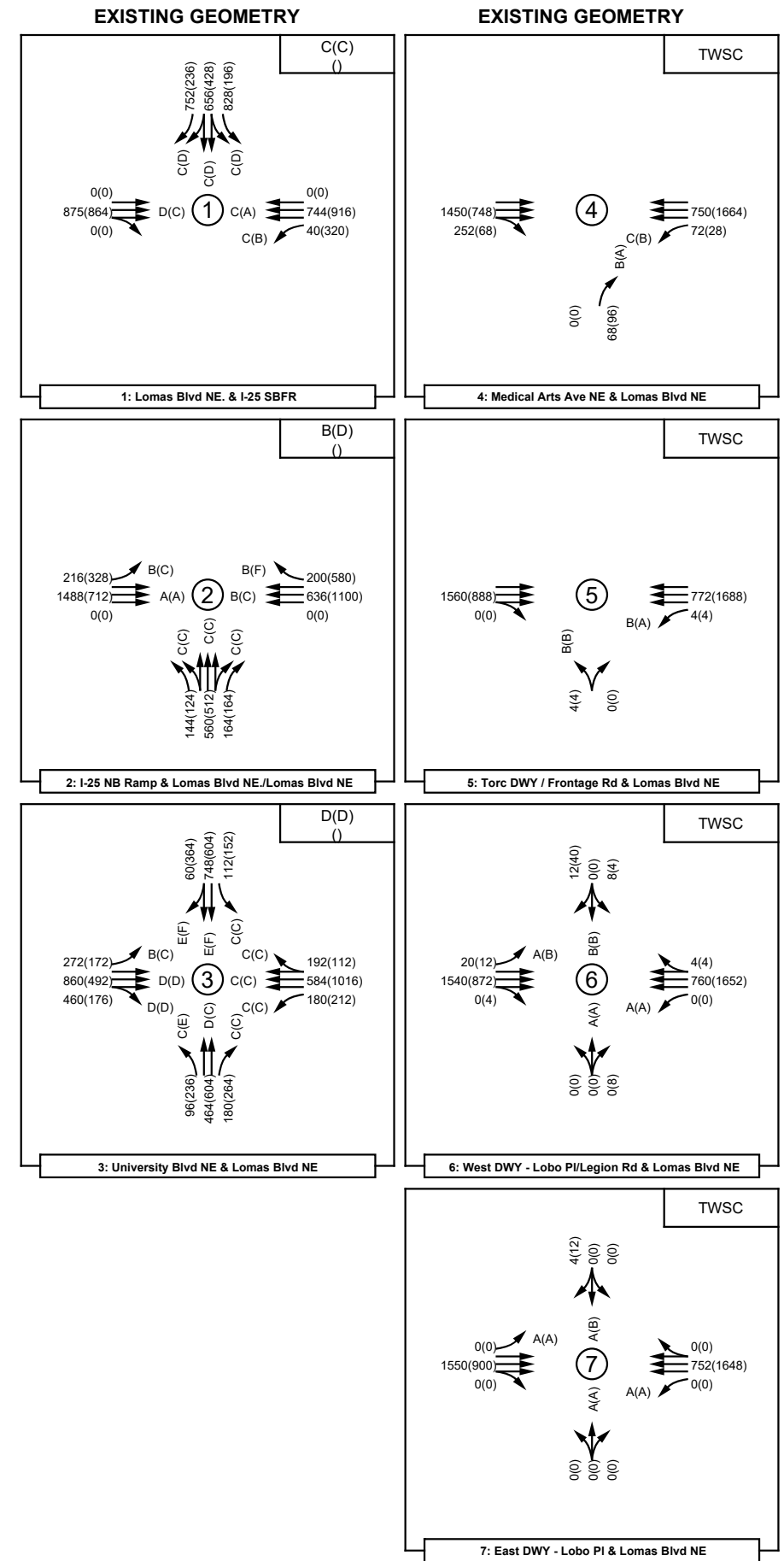
Lobo Plaza 1300 Lomas Blvd NE Intersection Map



- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl, NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

2025 EXISTING VOLUME - SIGNALIZED 2025 EXISTING VOLUME - UNSIGNALIZED



2025068 - Lobo Plaza
1300 Lomas Blvd NE, Albuquerque, NM
LOS / Volume Analysis Map

HCS Results Summary Sheet

1: Lomas Blvd NE & I-25 SB Ramp

2025 Conditions (HCS Results)

Lomas Blvd NE

I-25 SB Ramp

Signalized

| 1: Lomas Blvd NE & I-25 SB Ramp 2025 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
|--|--------------------|------|------|--------------------|------|------|-------------------|---|---|-------------------|------|------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| V/C Ratio | 0.00 | 0.44 | 0.00 | 0.11 | 0.26 | 0.00 | | | | 1.05 | 0.40 | 1.07 |
| Level-of-Service | | C | | C | B | | | | | F | C | F |
| Control Delay (Seconds) | | 27.9 | | 21.3 | 10.2 | | | | | 77.1 | 21.3 | 86.0 |
| Intersection LOS (HCS Result) | D - 46.4 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.9 | 0.0 | 1.0 | 3.1 | 0.0 | | | | 40.3 | 9.6 | 39.0 |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|-----|------|------|------|---|---|---|------|------|------|
| 2025 EXISTING VOLUMES | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| V/C Ratio | 0.00 | 0.34 | | 0.49 | 0.24 | 0.00 | | | | 0.46 | 0.48 | 0.62 |
| Level-of-Service | | C | | B | A | | | | | D | D | D |
| Control Delay (Seconds) | | 21.2 | | 14.9 | 1.9 | | | | | 42.3 | 40.8 | 48.0 |
| Intersection LOS (HCS Result) | C - 21.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 8.9 | 0.0 | 6.5 | 1.1 | 0.0 | | | | 9.1 | 9.5 | 11.3 |

HCS Results Summary Sheet

2: Lomas Blvd NE & I-25 NB Ramp
2025 Conditions (HCS Results)
Lomas Blvd NE
I-25 NB Ramp
Signalized

| 2: Lomas Blvd NE & I-25 NB Ramp 2025 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
|--|--------------------|-------|------|--------------------|------|------|-------------------|------|------|-------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | 216 | 1,488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| V/C Ratio | 0.35 | 0.47 | 0.00 | 0.00 | 0.31 | 0.31 | 0.29 | 0.54 | 0.37 | | | |
| Level-of-Service | B | B | | | C | C | C | D | C | | | |
| Control Delay (Seconds) | 13.5 | 19.8 | | | 22.4 | 23.3 | 32.3 | 35.4 | 34.0 | | | |
| Intersection LOS (HCS Result) | C - 23.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 13.7 | 0.0 | 0.0 | 6.7 | 6.6 | 5.7 | 10.7 | 6.7 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|------|------|-------|------|------|------|------|---|---|---|
| 2025 EXISTING VOLUMES | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| V/C Ratio | 0.69 | 0.23 | 0.00 | 0.00 | 0.54 | 0.92 | 0.24 | 0.32 | 0.36 | | | |
| Level-of-Service | C | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 29.8 | 14.7 | | | 28.7 | 55.4 | 33.5 | 33.7 | 35.8 | | | |
| Intersection LOS (HCS Result) | C - 31.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 10.5 | 7.1 | 0.0 | 0.0 | 12.8 | 26.4 | 5.2 | 7.1 | 7.2 | | | |

Synchro Results Summary Sheet

3: Lomas Blvd NE & University Blvd NE

2025 Conditions

Lomas Blvd NE

University Blvd NE

Signalized

| 3: Lomas Blvd NE & University Blvd NE 2025 Conditions | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (University Blvd NE) | | | SB (University Blvd NE) | | |
|--|--------------------|------|------|--------------------|------|------|-------------------------|------|------|-------------------------|------|------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| V/C Ratio | 0.59 | 0.58 | 0.66 | 0.67 | 0.37 | 0.38 | 0.50 | 0.53 | 0.35 | 0.39 | 0.88 | 0.88 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 17.9 | 39.1 | 44.2 | 24.6 | 22.7 | 23.7 | 32.9 | 37.9 | 30.0 | 29.4 | 59.9 | 59.5 |
| Intersection LOS | D - 37.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.3 | 17.8 | 19.8 | 4.8 | 8.2 | 8.4 | 3.5 | 9.4 | 6.9 | 4.0 | 18.5 | 18.9 |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-----------------|------|------|------|-------|------|------|------|------|------|------|------|
| 2025 EXISTING VOLUMES | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| V/C Ratio | 0.58 | 0.38 | 0.39 | 0.54 | 0.60 | 0.60 | 0.89 | 0.53 | 0.40 | 0.48 | 0.99 | 0.99 |
| Level-of-Service | C | D | D | C | C | C | E | C | C | C | F | F |
| Control Delay (Seconds) | 27.2 | 41.8 | 43.2 | 23.5 | 32.8 | 34.7 | 57.1 | 34.8 | 26.3 | 28.5 | 81.1 | 82.7 |
| Intersection LOS | D - 45.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.1 | 11.1 | 11.3 | 6.9 | 13.8 | 14.7 | 10.8 | 11.8 | 9.3 | 5.6 | 27.3 | 25.7 |

Synchro Results Summary Sheet

4: Lomas Blvd NE & Medical Arts Ave NE

2025 Conditions

Lomas Blvd NE

Medical Arts Ave NE

Unsignalized

| 4: Lomas Blvd NE & Medical Arts Ave NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Medical Arts Ave NE) | | | SB (Medical Arts Ave NE) | | |
|--|--------------------|-------|-----|--------------------|-----|---|--------------------------|---|------|--------------------------|---|---|
| 2025 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 0 | | 1 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | | 1,450 | 252 | 72 | 750 | | 0 | | 68 | | | |
| V/C Ratio | | | | 0.24 | | | | | 0.09 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 20.6 | | | | | 10.4 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.9 | | | | | 0.3 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|----|------|-------|--|---|--|------|--|--|--|
| 2025 EXISTING VOLUMES | | 748 | 68 | 28 | 1,664 | | 0 | | 96 | | | |
| V/C Ratio | | | | 0.04 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 10.8 | | | | | 9.9 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |

Synchro Results Summary Sheet

5: Torc DWY / Frontage Rd & Lomas Blvd NE

2025 Conditions

Lomas Blvd NE

Torc DWY / Frontage Rd

Unsignalized

| 5: Torc DWY / Frontage Rd & Lomas Blvd NE 2025 Conditions | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Torc DWY / Frontage Rd) | | | SB (Torc DWY / Frontage Rd) | | |
|--|--------------------|-------|---|--------------------|-----|---|-----------------------------|---|---|-----------------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 1> | | 0 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | | 1,560 | 0 | 4 | 772 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.10 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 12.2 | | | 12.6 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|---|------|-------|--|------|--|---|--|--|--|
| 2025 EXISTING VOLUMES | | 888 | 0 | 4 | 1,688 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.8 | | | 11.8 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

Synchro Results Summary Sheet

6: Lomas Blvd NE & Legion Rd / West DWY

2025 Conditions

Lomas Blvd NE

Legion Rd / West DWY

Unsignalized

| 6: Lomas Blvd NE & Legion Rd / West DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Legion Rd / West DWY) | | | SB (Legion Rd / West DWY) | | |
|---|--------------------|-------|---|--------------------|-----|---|----------------------------|-----|---|----------------------------|------|----|
| 2025 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | 20 | 1,540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | | | | | | | | 0.03 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 9.6 | | | 0.0 | | | | 0.0 | | | 10.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.1 | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|---|-----|-------|---|---|------|---|---|------|----|
| 2025 EXISTING VOLUMES | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| V/C Ratio | 0.03 | | | | | | | 0.01 | | | 0.07 | |
| Level-of-Service | B | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 13.1 | | | 0.0 | | | | 9.7 | | | 11.2 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.2 | |

Synchro Results Summary Sheet

7: Lomas Blvd NE & East DWY

2025 Conditions

Lomas Blvd NE

East DWY

Unsignalized

| 7: Lomas Blvd NE & East DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (East DWY) | | | SB (East DWY) | | |
|--|---------------------------|-------|---|---------------------------|-----|---|----------------------|-----|---|----------------------|------|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2025 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2025 EXISTING VOLUMES | 0 | 1,550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| V/C Ratio | | | | | | | | | | | 0.01 | |
| Level-of-Service | A | | | A | | | | A | | | A | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.0 | |

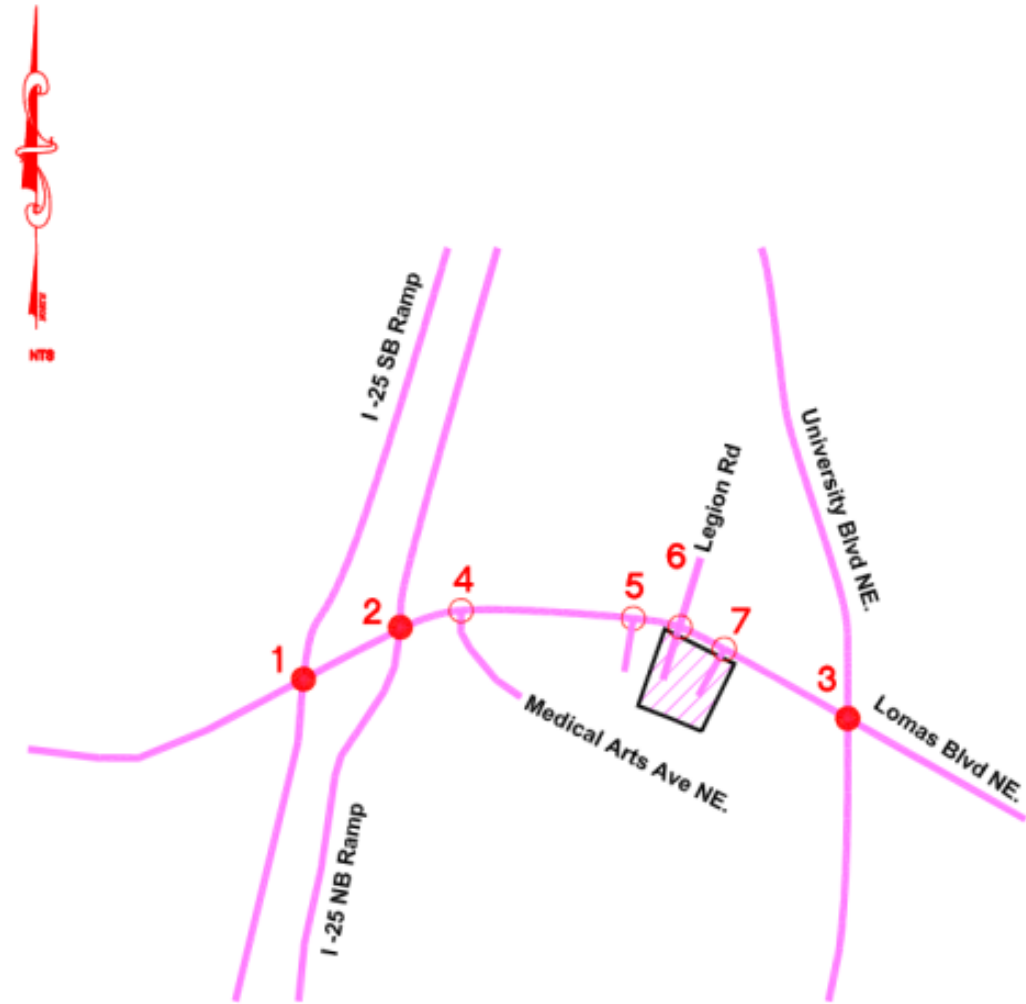
PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|---|-----|-------|---|---|-----|---|---|------|----|
| 2025 EXISTING VOLUMES | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| V/C Ratio | | | | | | | | | | | 0.02 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 10.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.1 | |

Lobo Plaza

1300 Lomas Blvd NE

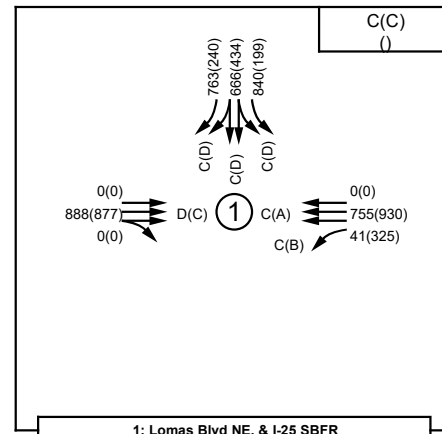
Intersection Map



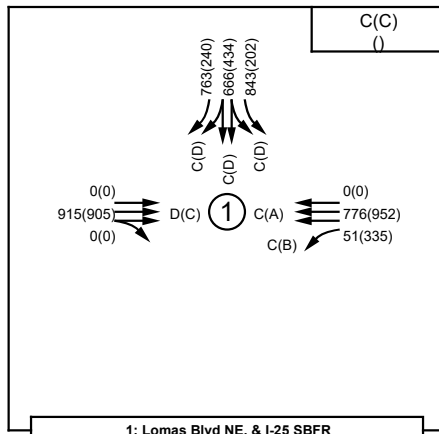
- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
 5571 Midway Park Pl, NE
 Albuquerque, NM 87109
 (505) 858-3100 (Voice)

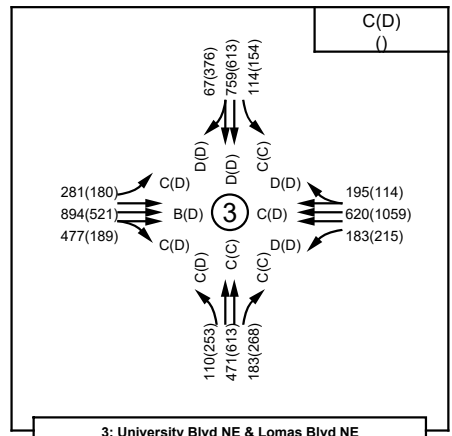
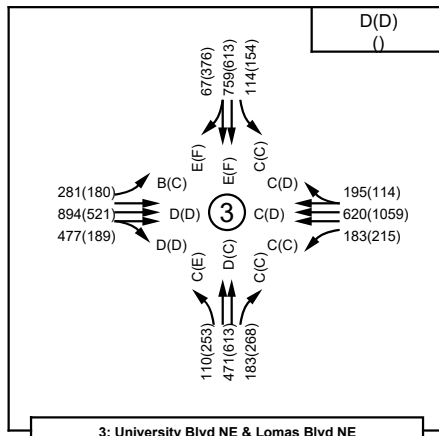
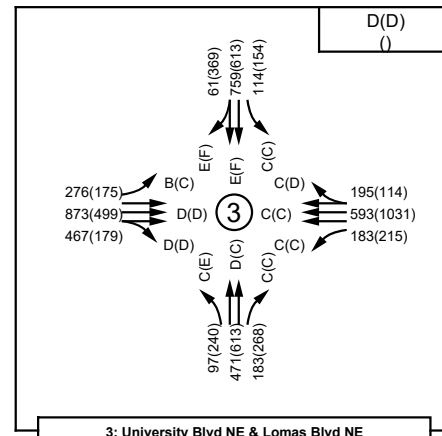
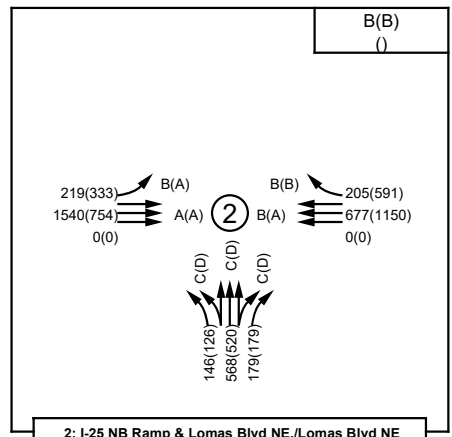
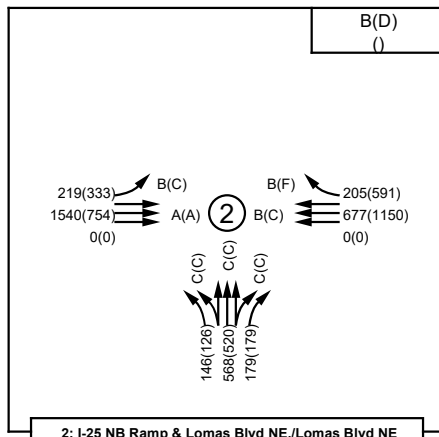
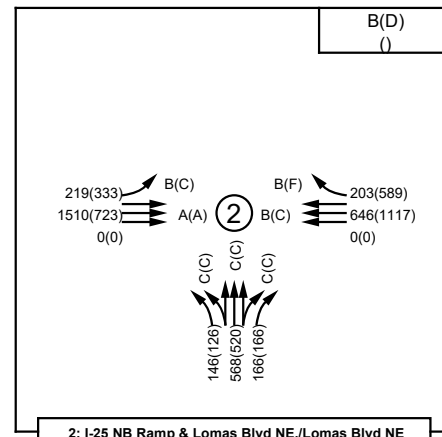
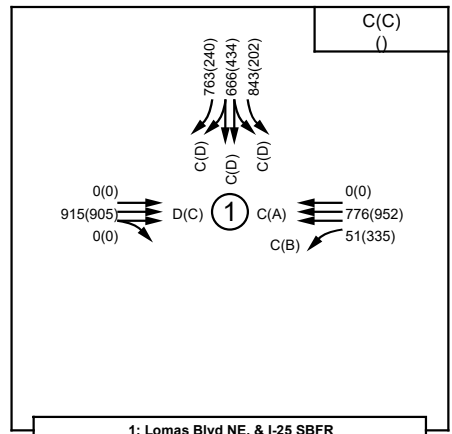
2028 NO BUILD - SIGNALIZED EXISTING GEOMETRY



2028 BUILD - SIGNALIZED EXISTING GEOMETRY

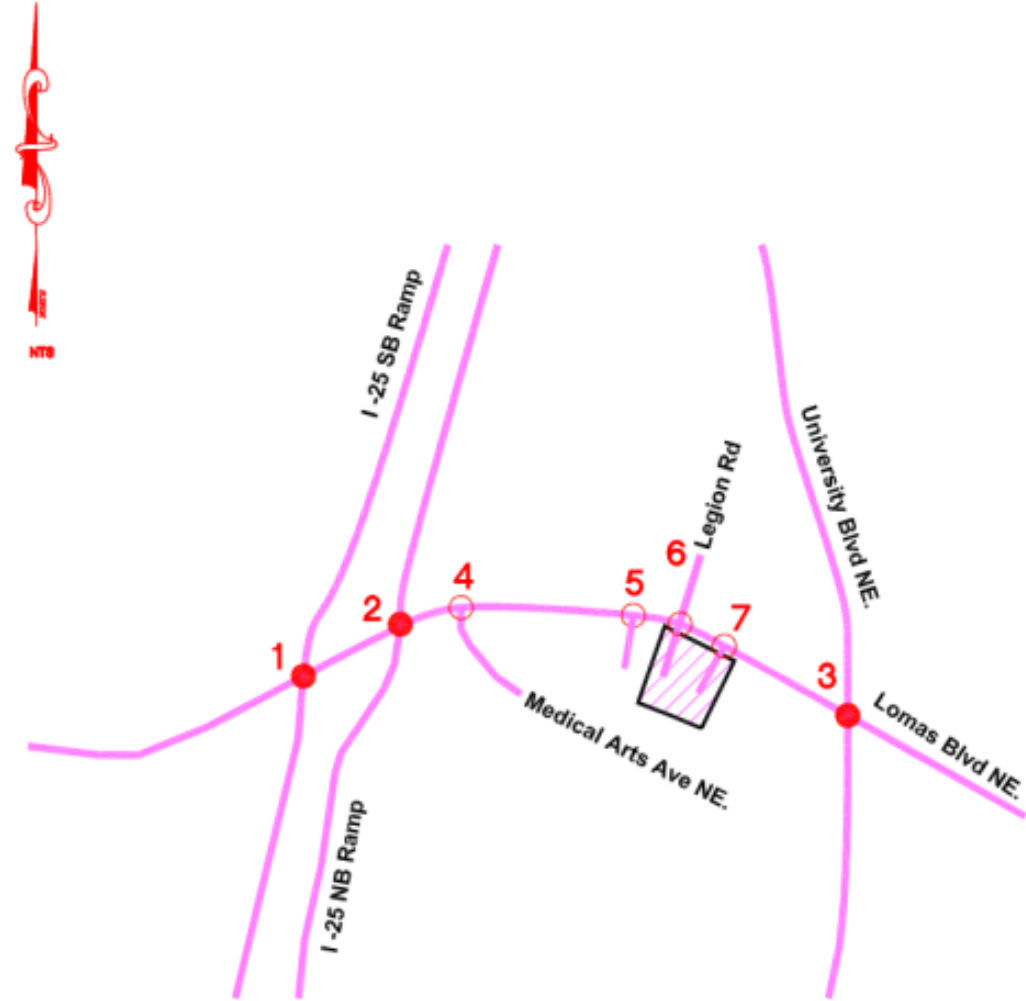


2028 BUILD MITIGATED - SIGNALIZED EXISTING GEOMETRY



2025068 - Lobo Plaza
 1300 Lomas Blvd NE, Albuquerque, NM
 LOS / Volume Analysis Map

Lobo Plaza 1300 Lomas Blvd NE Intersection Map



- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl, NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

2028 NO BUILD - UNSIGNALIZED EXISTING GEOMETRY

2028 BUILD - UNSIGNALIZED EXISTING GEOMETRY

2028 BUILD MITIGATED - UNSIGNALIZED EXISTING GEOMETRY

| | | |
|--|--|---|
| <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> | <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> | <p>NO MITIGATIONS RECOMMENDED</p> <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> |
| <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> | <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> | <p>NO MITIGATIONS RECOMMENDED</p> <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> |
| <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> | <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> | <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> |
| <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> | <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> | <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> |

2025068 - Lobo Plaza
 1300 Lomas Blvd NE, Albuquerque, NM
 LOS / Volume Analysis Map

HCS Results Summary Sheet

1: Lomas Blvd NE & I-25 SB Ramp

2028 Conditions (HCS Results)

Lomas Blvd NE

I-25 SB Ramp

Signalized

| 1: Lomas Blvd NE & I-25 SB Ramp | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
|--|---------------------------|------|------|---------------------------|------|------|--------------------------|---|---|--------------------------|------|------|
| 2028 Conditions (HCS Results) | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 |
| V/C Ratio | | 0.45 | 0.00 | 0.13 | 0.33 | 0.00 | | | | 1.07 | 0.40 | 1.09 |
| Level-of-Service | | C | | C | C | | | | | F | C | F |
| Control Delay (Seconds) | | 28.0 | | 21.6 | 28.7 | | | | | 82.0 | 21.3 | 91.4 |
| Intersection LOS (HCS Result) | D - 50.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.0 | 0.0 | 1.3 | 10.0 | 0.0 | | | | 42.0 | 9.7 | 40.7 |
| 2028 BUILD Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| V/C Ratio | | 0.46 | 0.00 | 0.17 | 0.34 | 0.00 | | | | 1.07 | 0.40 | 1.09 |
| Level-of-Service | | C | | C | C | | | | | F | C | F |
| Control Delay (Seconds) | | 28.2 | | 22.1 | 28.8 | | | | | 83.3 | 21.3 | 91.4 |
| Intersection LOS (HCS Result) | D - 50.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.3 | 0.0 | 1.6 | 10.2 | 0.0 | | | | 42.5 | 9.7 | 40.7 |
| Mitigate Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| 2028 BUILD MITIGATED Volumes | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| V/C Ratio | | 0.70 | 0.00 | 0.28 | 0.48 | 0.00 | | | | 0.82 | 0.31 | 0.83 |
| Level-of-Service | | D | | C | C | | | | | C | B | C |
| Control Delay (Seconds) | | 41.4 | | 32.9 | 26.0 | | | | | 25.7 | 12.1 | 27.5 |
| Intersection LOS (HCS Result) | C - 27.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 12.3 | 0.0 | 2.0 | 8.0 | 0.0 | | | | 24.6 | 7.3 | 23.2 |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|-----|------|------|------|---|---|---|------|------|------|
| 2028 NO BUILD Volumes | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 |
| V/C Ratio | 0.00 | 0.34 | | 0.56 | 0.27 | 0.00 | | | | 0.47 | 0.48 | 0.63 |
| Level-of-Service | | C | | B | B | | | | | D | D | D |
| Control Delay (Seconds) | | 21.3 | | 16.9 | 13.7 | | | | | 42.5 | 40.9 | 48.5 |
| Intersection LOS (HCS Result) | C - 24.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.0 | 0.0 | 8.1 | 8.7 | 0.0 | | | | 9.2 | 9.6 | 11.5 |
| 2028 BUILD Volumes | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 |
| V/C Ratio | 0.00 | 0.36 | | 0.58 | 0.28 | | | | | 0.47 | 0.48 | 0.63 |
| Level-of-Service | | C | | B | B | | | | | D | D | D |
| Control Delay (Seconds) | | 21.4 | | 17.8 | 13.7 | | | | | 42.7 | 40.9 | 48.5 |
| Intersection LOS (HCS Result) | C - 24.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.3 | 0.0 | 8.4 | 8.9 | 0.0 | | | | 9.3 | 9.6 | 11.5 |

HCS Results Summary Sheet

2: Lomas Blvd NE & I-25 NB Ramp

2028 Conditions (HCS Results)

Lomas Blvd NE

I-25 NB Ramp

Signalized

| 2: Lomas Blvd NE & I-25 NB Ramp 2028 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
|--|--------------------|-------|---|--------------------|------|------|-------------------|------|------|-------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 219 | 1,510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| V/C Ratio | 0.35 | 0.47 | | | 0.31 | 0.32 | 0.29 | 0.36 | 0.38 | | | |
| Level-of-Service | B | B | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.6 | 19.8 | | | 22.4 | 23.3 | 32.4 | 32.2 | 34.1 | | | |
| Intersection LOS (HCS Result) | C - 23.4 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 13.7 | | | 6.8 | 6.7 | 5.8 | 7.3 | 6.8 | | | |
| 2028 BUILD Volumes | 219 | 1,540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.26 | 0.40 | | | 0.33 | 0.32 | 0.52 | 0.64 | 0.72 | | | |
| Level-of-Service | A | A | | | C | C | D | D | D | | | |
| Control Delay (Seconds) | 7.7 | 8.7 | | | 22.6 | 23.4 | 44.0 | 44.2 | 47.8 | | | |
| Intersection LOS (HCS Result) | B - 21.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 3.1 | 9.0 | | | 7.2 | 6.8 | 6.7 | 8.4 | 8.4 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|---|---|-------|------|------|------|------|---|---|---|
| 2028 NO BUILD Volumes | 333 | 723 | 0 | 0 | 1,117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| V/C Ratio | 0.70 | 0.24 | | | 0.55 | 0.94 | 0.24 | 0.32 | 0.36 | | | |
| Level-of-Service | C | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 30.6 | 15.1 | | | 28.8 | 57.8 | 33.5 | 33.7 | 35.9 | | | |
| Intersection LOS (HCS Result) | D - 32.1 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 10.7 | 7.4 | | | 13.0 | 27.3 | 5.3 | 7.2 | 7.4 | | | |
| 2028 BUILD Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.71 | 0.25 | | | 0.57 | 0.94 | 0.24 | 0.32 | 0.39 | | | |
| Level-of-Service | C | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 31.9 | 14.9 | | | 29.1 | 58.4 | 33.5 | 33.7 | 36.5 | | | |
| Intersection LOS (HCS Result) | C - 32.3 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 10.7 | 7.6 | | | 13.4 | 27.5 | 5.3 | 7.2 | 7.9 | | | |
| Mitigage Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| 2028 BUILD MITIGATED Volumes | 333 | 754 | 0 | 0 | 1,150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| V/C Ratio | 0.86 | 0.21 | | | 0.37 | 0.61 | 0.36 | 0.47 | 0.57 | | | |
| Level-of-Service | D | A | | | B | B | D | D | D | | | |
| Control Delay (Seconds) | 39.0 | 7.9 | | | 11.8 | 17.0 | 44.1 | 43.8 | 50.6 | | | |
| Intersection LOS (HCS Result) | C - 21.8 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 12.5 | 4.8 | | | 8.5 | 15.1 | 6.2 | 8.2 | 9.2 | | | |

Synchro Results Summary Sheet

3: Lomas Blvd NE & University Blvd NE

2028 Conditions

Lomas Blvd NE

University Blvd NE

Signalized

| 3: Lomas Blvd NE & University Blvd NE 2028 Conditions | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (University Blvd NE) | | | SB (University Blvd NE) | | |
|--|--------------------|------|------|--------------------|------|------|-------------------------|------|------|-------------------------|------|------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| V/C Ratio | 0.60 | 0.59 | 0.68 | 0.68 | 0.37 | 0.38 | 0.51 | 0.53 | 0.35 | 0.40 | 0.89 | 0.89 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 18.1 | 39.5 | 44.9 | 25.2 | 23.0 | 24.0 | 33.1 | 38.0 | 29.9 | 29.4 | 61.4 | 61.0 |
| Intersection LOS | D - 38.3 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.5 | 18.1 | 20.2 | 4.9 | 8.3 | 8.5 | 3.5 | 9.6 | 7.0 | 4.0 | 19.0 | 19.4 |
| 2028 BUILD Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| V/C Ratio | 0.62 | 0.60 | 0.69 | 0.69 | 0.39 | 0.40 | 0.57 | 0.53 | 0.35 | 0.40 | 0.92 | 0.92 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 18.5 | 39.9 | 45.6 | 25.8 | 23.3 | 24.3 | 33.6 | 38.0 | 29.9 | 29.4 | 66.7 | 66.4 |
| Intersection LOS | D - 39.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.6 | 18.5 | 20.6 | 5.0 | 8.6 | 8.8 | 4.0 | 9.6 | 7.0 | 4.0 | 19.9 | 20.2 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2028 BUILD MITIGATED Volumes | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| V/C Ratio | 0.68 | 0.69 | 0.79 | 0.76 | 0.52 | 0.53 | 0.43 | 0.40 | 0.30 | 0.33 | 0.68 | 0.68 |
| Level-of-Service | C | B | C | D | C | D | C | C | C | C | D | D |
| Control Delay (Seconds) | 21.4 | 12.8 | 20.8 | 44.0 | 32.8 | 35.0 | 25.3 | 28.9 | 24.3 | 22.7 | 37.5 | 37.4 |
| Intersection LOS | C - 26.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.3 | 6.4 | 8.7 | 4.8 | 10.1 | 10.5 | 3.4 | 8.4 | 6.1 | 3.5 | 15.3 | 15.6 |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-----------------|------|------|------|-------|------|------|------|------|------|------|------|
| 2028 NO BUILD Volumes | 175 | 499 | 179 | 215 | 1,031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| V/C Ratio | 0.60 | 0.39 | 0.40 | 0.55 | 0.61 | 0.61 | 0.90 | 0.53 | 0.40 | 0.49 | 1.01 | 1.01 |
| Level-of-Service | C | D | D | C | C | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 27.9 | 42.3 | 43.7 | 24.1 | 33.5 | 35.6 | 58.6 | 34.7 | 26.0 | 28.5 | 84.7 | 86.3 |
| Intersection LOS | D - 46.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.2 | 11.2 | 11.4 | 7.1 | 14.1 | 15.1 | 10.5 | 11.9 | 9.4 | 5.6 | 28.2 | 26.5 |
| 2028 BUILD Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| V/C Ratio | 0.63 | 0.42 | 0.43 | 0.57 | 0.64 | 0.64 | 0.90 | 0.52 | 0.40 | 0.48 | 1.02 | 1.02 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 29.2 | 43.4 | 45.0 | 25.2 | 35.1 | 37.5 | 61.3 | 33.9 | 25.3 | 28.3 | 86.7 | 88.4 |
| Intersection LOS | D - 47.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.5 | 11.7 | 11.9 | 7.3 | 14.7 | 15.8 | 12.0 | 11.8 | 9.3 | 5.6 | 28.6 | 26.9 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2028 BUILD MITIGATED Volumes | 180 | 521 | 189 | 215 | 1,059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| V/C Ratio | 0.69 | 0.42 | 0.43 | 0.69 | 0.72 | 0.72 | 0.87 | 0.44 | 0.38 | 0.48 | 0.85 | 0.85 |
| Level-of-Service | D | D | D | D | D | D | D | C | C | C | D | D |
| Control Delay (Seconds) | 36.5 | 43.4 | 45.0 | 38.8 | 40.4 | 44.0 | 47.0 | 27.5 | 23.1 | 25.9 | 50.8 | 51.9 |
| Intersection LOS | D - 40.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.3 | 11.7 | 11.9 | 5.1 | 15.7 | 17.0 | 9.9 | 10.7 | 8.9 | 5.3 | 22.6 | 21.3 |

Synchro Results Summary Sheet

4: Lomas Blvd NE & Medical Arts Ave NE

2028 Conditions

Lomas Blvd NE

Medical Arts Ave NE

Unsignalized

| 4: Lomas Blvd NE & Medical Arts Ave NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Medical Arts Ave NE) | | | SB (Medical Arts Ave NE) | | |
|---|---------------------------|-------|-----|---------------------------|-----|---|---------------------------------|---|------|---------------------------------|---|---|
| 2028 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 0 | | 1 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | | 1,472 | 256 | 73 | 761 | | 0 | | 69 | | | |
| V/C Ratio | | | | 0.25 | | | | | 0.09 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 21.4 | | | | | 10.4 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.0 | | | | | 0.3 | | | |
| 2028 BUILD Volumes | | 1,514 | 256 | 74 | 794 | | 0 | | 71 | | | |
| V/C Ratio | | | | 0.27 | | | | | 0.10 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 22.9 | | | | | 10.6 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.1 | | | | | 0.3 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|----|------|-------|--|---|--|------|--|--|--|
| 2028 NO BUILD Volumes | | 759 | 69 | 28 | 1,689 | | 0 | | 97 | | | |
| V/C Ratio | | | | 0.04 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 10.9 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |
| 2028 BUILD Volumes | | 803 | 69 | 29 | 1,724 | | 0 | | 99 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 11.0 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |

Synchro Results Summary Sheet

5: Torc DWY / Frontage Rd & Lomas Blvd NE

2028 Conditions

Lomas Blvd NE

Torc DWY / Frontage Rd

Unsignalized

| 5: Torc DWY / Frontage Rd & Lomas Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Torc DWY / Frontage Rd) | | | SB (Torc DWY / Frontage Rd) | | |
|---|--------------------|-------|---|--------------------|-----|---|-----------------------------|---|---|-----------------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| 2028 Conditions | | | | | | | | | | | | |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 1> | | 0 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | | 1,583 | 0 | 4 | 784 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 12.5 | | | 13.0 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | 0.1 | | | | | |
| 2028 BUILD Volumes | | 1,627 | 0 | 4 | 819 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 13.0 | | | 13.9 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|---|------|-------|--|------|--|---|--|--|--|
| 2028 NO BUILD Volumes | | 901 | 0 | 4 | 1,713 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 11.8 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2028 BUILD Volumes | | 947 | 0 | 4 | 1,749 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 12.0 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

Synchro Results Summary Sheet

6: Lomas Blvd NE & Legion Rd / West DWY

2028 Conditions

Lomas Blvd NE

Legion Rd / West DWY

Unsignalized

| 6: Lomas Blvd NE & Legion Rd / West DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Legion Rd / West DWY) | | | SB (Legion Rd / West DWY) | | |
|---|--------------------|-------|----|--------------------|-----|---|----------------------------|------|-----|----------------------------|------|----|
| 2028 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 20 | 1,563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | | | | | | | | 0.03 | |
| Level-of-Service | A | | | A | | | | | A | | B | |
| Control Delay (Seconds) | 9.7 | | | 0.0 | | | | | 0.0 | | 10.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | | 0.0 | | 0.1 | |
| 2028 BUILD Volumes | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.10 | | | 0.03 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 9.8 | | | 13.1 | | | | 13.6 | | | 11.1 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 20 | 1,577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| V/C Ratio | 0.03 | | | 0.05 | | | | 0.12 | | | 0.03 | |
| Level-of-Service | A | | | B | | | | C | | | B | |
| Control Delay (Seconds) | 9.8 | | | 13.1 | | | | 15.9 | | | 11.1 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.4 | | | 0.1 | |

| PM Peak Hour | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2028 NO BUILD Volumes | 12 | 885 | 4 | 0 | 1,677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | | | | | 0.01 | | | 0.07 | |
| Level-of-Service | B | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 12.8 | | | 0.0 | | | | 9.7 | | | 11.3 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | 0.0 | | | 0.2 | |
| 2028 BUILD Volumes | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.10 | | | 0.08 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 12.9 | | | 10.4 | | | | 11.7 | | | 11.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.2 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 12 | 903 | 40 | 28 | 1,686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| V/C Ratio | 0.03 | | | 0.04 | | | | 0.10 | | | 0.08 | |
| Level-of-Service | B | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 12.9 | | | 10.4 | | | | 11.9 | | | 12.2 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | 0.3 | | | 0.3 | |

Synchro Results Summary Sheet

7: Lomas Blvd NE & East DWY

2028 Conditions

Lomas Blvd NE

East DWY

Unsignalized

| 7: Lomas Blvd NE & East DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (East DWY) | | | SB (East DWY) | | |
|--|---------------------------|-------|----|---------------------------|-----|---|----------------------|------|----|----------------------|------|---|
| 2028 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2028 NO BUILD Volumes | 0 | 1,573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| V/C Ratio | | | | | | | | | | | 0.01 | |
| Level-of-Service | A | | | A | | | | A | | | A | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.0 | |
| 2028 BUILD Volumes | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.90 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | B | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.2 | | | | 13.5 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.0 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 0 | 1,582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.11 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | C | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.2 | | | | 15.2 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.4 | | | 0.0 | |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2028 NO BUILD Volumes | 0 | 914 | 0 | 0 | 1,673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| V/C Ratio | | | | | | | | | | | 0.02 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.1 | |
| 2028 BUILD Volumes | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.5 | | | | 11.5 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2028 BUILD MITIGATED Volumes | 0 | 924 | 26 | 36 | 1,693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.5 | | | | 11.9 | | | 10.7 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068

Projected Turning Movements SUMMARY
PROPOSED DEVELOPMENT (2038) - 100% Development

INTERSECTION: **Summary**

Lomas Blvd NE / I - 25 SB Ramp

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (1) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| 2038 (NO BUILD - A.M.) | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 |
| 2038 (BUILD - A.M.) | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| 2038 (NO BUILD - P.M.) | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| 2038 (BUILD - P.M.) | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 |

Lomas Blvd NE / I - 25 NB Ramp

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (2) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 216 | 1,488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| 2038 (NO BUILD - A.M.) | 230 | 1,585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| 2038 (BUILD - A.M.) | 230 | 1,615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|-----------------------------|------|-------|-----------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| 2038 (NO BUILD - P.M.) | 349 | 758 | 0 | 0 | 1,172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| 2038 (BUILD - P.M.) | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |

Lomas Blvd NE / University Blvd NE

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (3) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| 2038 (NO BUILD - A.M.) | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| 2038 (BUILD - A.M.) | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| 2038 (NO BUILD - P.M.) | 183 | 524 | 187 | 226 | 1,082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| 2038 (BUILD - P.M.) | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |

Lomas Blvd NE / Medical Arts Ave NE

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|------------------------|---------------------------|-------|-------|---------------------------|------|-------|----------------------------------|------|-------|----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| (4) 0.0% H.C. | | | | | | | | | | | | |
| Existing (2025) | 0 | 1,450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| 2038 (NO BUILD - A.M.) | 0 | 1,544 | 268 | 77 | 799 | 0 | 0 | 0 | 72 | 0 | 0 | 0 |
| 2038 (BUILD - A.M.) | 0 | 1,586 | 268 | 78 | 832 | 0 | 0 | 0 | 74 | 0 | 0 | 0 |

1.00

PHF

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|------------------------|---------------------------|------|-------|---------------------------|-------|-------|----------------------------------|------|-------|----------------------------------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing (2025) | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |
| 2038 (NO BUILD - P.M.) | 0 | 797 | 72 | 30 | 1,772 | 0 | 0 | 0 | 102 | 0 | 0 | 0 |
| 2038 (BUILD - P.M.) | 0 | 841 | 72 | 31 | 1,807 | 0 | 0 | 0 | 104 | 0 | 0 | 0 |

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068

Projected Turning Movements SUMMARY
PROPOSED DEVELOPMENT (2038) - 100% Development

INTERSECTION: **Summary**

Lomas Blvd NE / Torc Dwy / Frtg Rd 1.00 PHF

(5) 0.0% H.C.

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|---------------------------|-------|-------|---------------------------|------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 0 | 1,560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 0 | 1,661 | 0 | 4 | 822 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 0 | 1,705 | 0 | 4 | 857 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

1.00 PHF

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|---------------------------|------|-------|---------------------------|-------|-------|---------------------------------|------|-------|---------------------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 0 | 946 | 0 | 4 | 1,798 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| 0 | 992 | 0 | 4 | 1,834 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

Lomas Blvd NE / West Dwy / Legion Rd 1.00 PHF

(6) 0.0% H.C.

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|---------------------------|-------|-------|---------------------------|------|-------|-----------------------------------|------|-------|-----------------------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 20 | 1,540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| 21 | 1,640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |

1.00 PHF

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|---------------------------|------|-------|---------------------------|-------|-------|-----------------------------------|------|-------|-----------------------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| 13 | 929 | 4 | 0 | 1,759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |

Lomas Blvd NE / East Dwy 1.00 PHF

(7) 0.0% H.C.

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|---------------------------|-------|-------|---------------------------|------|-------|-----------------------|------|-------|-----------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 0 | 1,550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 0 | 1,651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

1.00 PHF

| Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|---------------------------|------|-------|---------------------------|-------|-------|-----------------------|------|-------|-----------------------|------|-------|
| Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| 0 | 959 | 0 | 0 | 1,755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |

LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / I - 25 SB Ramp

INTERSECTION: E-W Street: **Lomas Blvd NE** (1)
 N-S Street: **I - 25 SB Ramp**

Year of Existing Counts: **2025**
 Horizon Year: **2038**

Growth Rates: **0.50%** **0.50%** **0.50%** **0.50%**

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|--|---------------------------|------------|----------|---------------------------|------------|----------|-----------------------------|----------|----------|-----------------------------|------------|------------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| Background Traffic Growth | 0 | 57 | 0 | 3 | 48 | 0 | 0 | 0 | 0 | 54 | 43 | 49 |
| Subtotal (NO BUILD - A.M.) | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 3.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 14.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 27 | 0 | 10 | 21 | 0 | 0 | 0 | 0 | 3 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|--|---------------------------|------------|----------|---------------------------|------------|----------|-----------------------------|----------|----------|-----------------------------|------------|------------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| Background Traffic Growth | 0 | 56 | 0 | 21 | 60 | 0 | 0 | 0 | 0 | 13 | 28 | 15 |
| Subtotal (NO BUILD - P.M.) | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 3.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 14.00% | 30.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 28 | 0 | 10 | 22 | 0 | 0 | 0 | 0 | 3 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 |

Number of Commercial Trips Generated: Entering **90** Exiting **71** A.M. 100% Commercial Development
 Entering **93** Exiting **74** P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|---------------------------------|---------------------------|-----|---|---------------------------|-----|---|-----------------------------|---|---|-----------------------------|-----|-----|
| 2025 AM Peak Hr. Volumes | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| 2025 PM Peak Hr. Volumes | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |

MRCOG Forecast Volumes Worksheet

Based on 2025 Traffic Count

| | | | | |
|----------------------------|------------|--------------|----------|--------------|
| 2025 AM Link Volume | 875 | 784 | 0 | 2,236 |
| 2025 PM Link Volume | 864 | 1,236 | 0 | 860 |

Based on MRCOG Model (2040 Data Set)

| | | | | |
|----------------------------|-------------|-------------|-------------|-------------|
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |

Growth Rate to Apply to Existing Counts to Match 2040 Forecasts

| | | | | |
|---------------------------|--------------|--------------|----------------|---------------|
| 2025-2040 AM Growth Rates | 4.52% | 0.54% | #DIV/0! | -4.35% |
| 2025-2040 PM Growth Rates | 0.46% | 2.79% | #DIV/0! | 5.22% |

Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts

| | | | | |
|---------------------------|--------|-------|-------|--------|
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

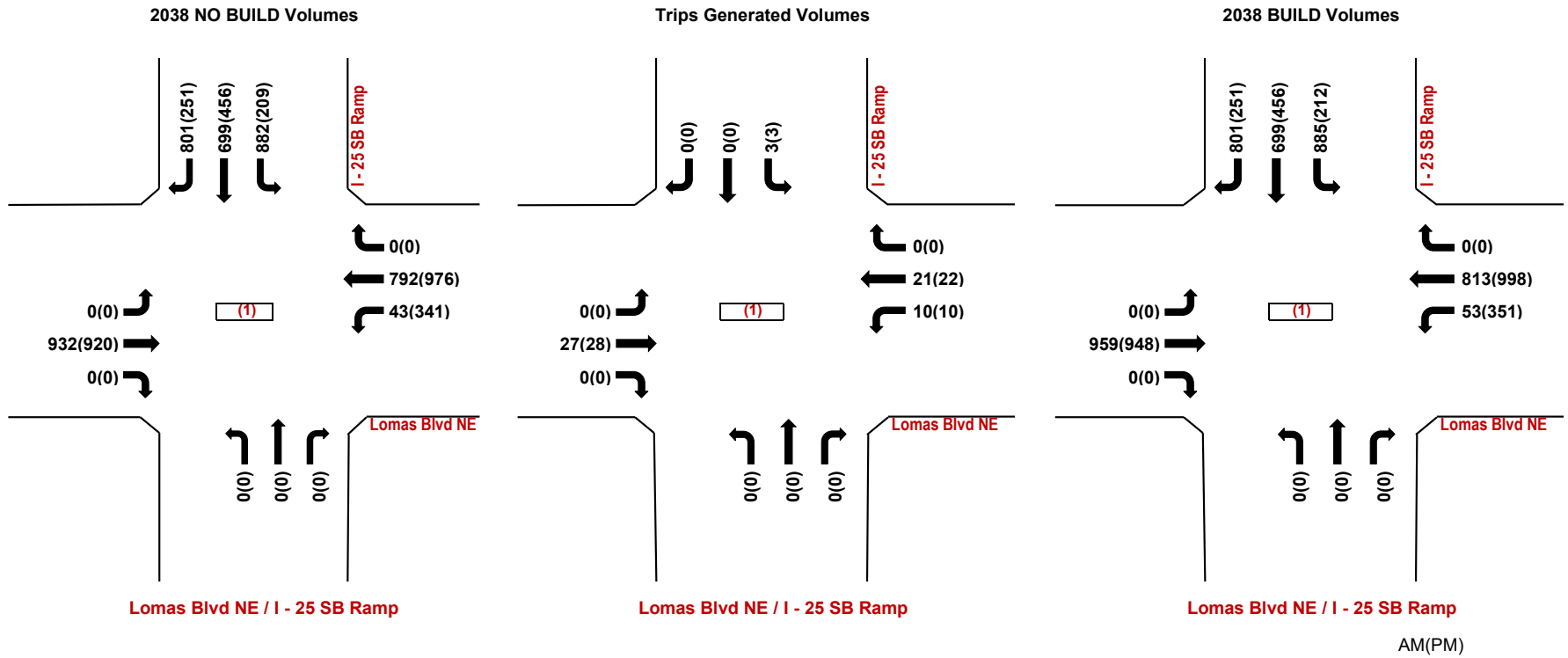
AM Pass-by Trips

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|----------------------------|---------------------------|----------|----------|---------------------------|----------|----------|-----------------------------|----------|----------|-----------------------------|----------|--|
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net AM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |

PM Pass-by Trips

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 SB Ramp) | | | Southbound (I - 25 SB Ramp) | | |
|----------------------------|---------------------------|----------|----------|---------------------------|----------|----------|-----------------------------|----------|----------|-----------------------------|----------|--|
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net PM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |

Pass-by Trips: Entering **25** Exiting **19 AM**
 Entering **32** Exiting **26 PM**



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / I - 25 NB Ramp

INTERSECTION: E-W Street: **Lomas Blvd NE** (2)
 N-S Street: **I - 25 NB Ramp**

Year of Existing Counts: 2025
 Horizon Year: 2038

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|----------|---------------------------|------------|------------|-----------------------------|------------|------------|-----------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 216 | 1,488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| Background Traffic Growth | 14 | 97 | 0 | 0 | 41 | 13 | 9 | 36 | 11 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 230 | 1,585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 44.00% | 3.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 30 | 0 | 0 | 31 | 2 | 0 | 0 | 13 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 230 | 1,615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 230 | 1,615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|----------|---------------------------|--------------|------------|-----------------------------|------------|------------|-----------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| Background Traffic Growth | 21 | 46 | 0 | 0 | 72 | 38 | 8 | 33 | 11 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 349 | 758 | 0 | 0 | 1,172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 44.00% | 3.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 31 | 0 | 0 | 33 | 2 | 0 | 0 | 13 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |

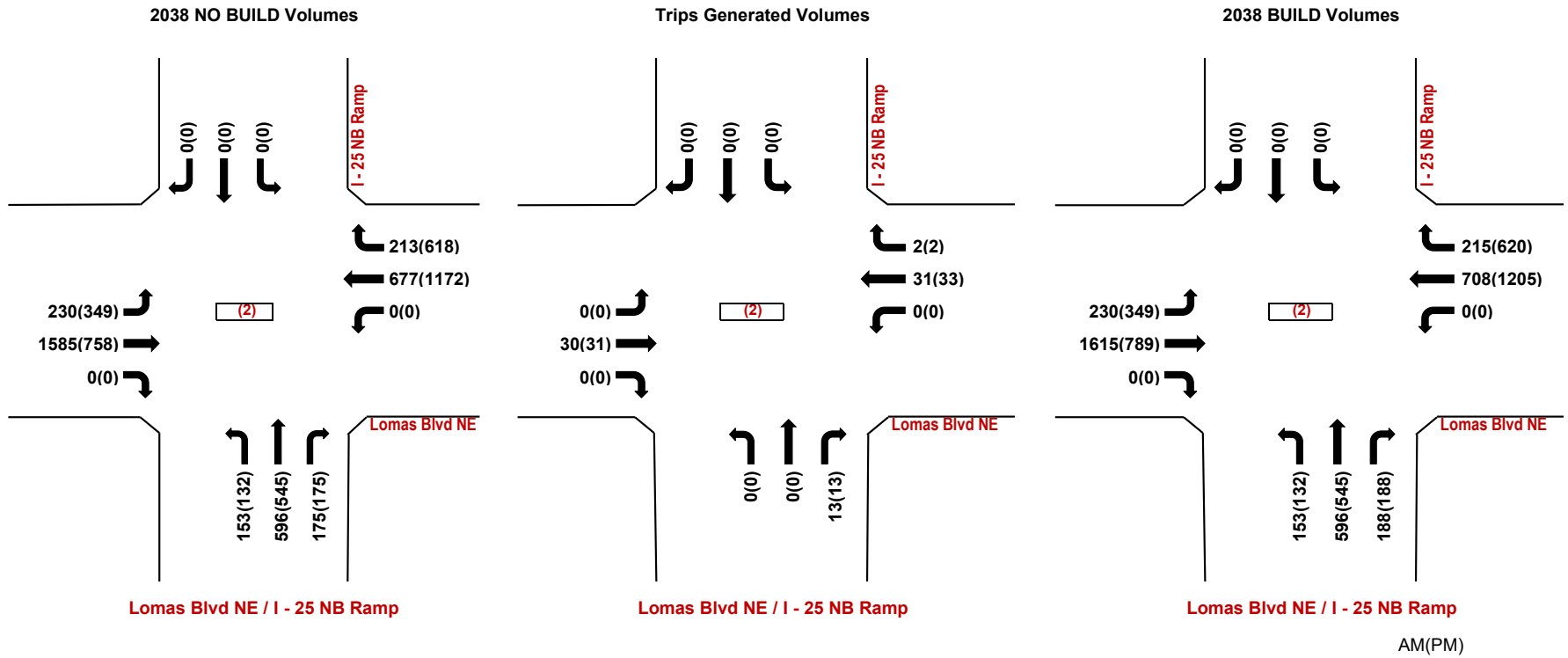
Number of Commercial Trips Generated: Entering 90, Exiting 71 A.M. 100% Commercial Development
 Entering 93, Exiting 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I - 25 NB Ramp) | | | Southbound (I - 25 NB Ramp) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|-----|-----------------------------|-----|-----|-----------------------------|---|---|
| 2025 AM Peak Hr. Volumes | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| 2025 PM Peak Hr. Volumes | 328 | 712 | 0 | 0 | 1,100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|-------|---------|
| 2025 AM Link Volume | 1,704 | 836 | 868 | 0 |
| 2025 PM Link Volume | 1,040 | 1,680 | 800 | 0 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.92% | 0.10% | 5.69% | #DIV/0! |
| 2025-2040 PM Growth Rates | -0.75% | 0.29% | 4.91% | #DIV/0! |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | |
|----------------------------|-------------|---------------|----------|----------|
| AM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | |
| | 32 | 26 PM | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / University Blvd NE

INTERSECTION : E-W Street: **Lomas Blvd NE** (3)
 N-S Street: **University Blvd NE**

Year of Existing Counts 2025
 Horizon Year 2038

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|------------|---------------------------|------------|------------|---------------------------------|------------|------------|---------------------------------|------------|-----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| Background Traffic Growth | 18 | 56 | 30 | 12 | 38 | 12 | 6 | 30 | 12 | 7 | 49 | 4 |
| Subtotal (NO BUILD - A.M.) | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 0.00% | 0.00% | 30.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 7.00% |
| Percent Commercial Trips Generated(Exiting) | 7.00% | 30.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 5 | 21 | 10 | 0 | 27 | 0 | 13 | 0 | 0 | 0 | 0 | 6 |
| Subtotal AM Pk Hr. BUILD Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|------------|---------------------------|--------------|------------|---------------------------------|------------|------------|---------------------------------|------------|------------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| Background Traffic Growth | 11 | 32 | 11 | 14 | 66 | 7 | 15 | 39 | 17 | 10 | 39 | 24 |
| Subtotal (NO BUILD - P.M.) | 183 | 524 | 187 | 226 | 1,082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 0.00% | 0.00% | 30.00% | 0.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 7.00% |
| Percent Commercial Trips Generated(Exiting) | 7.00% | 30.00% | 14.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 5 | 22 | 10 | 0 | 28 | 0 | 13 | 0 | 0 | 0 | 0 | 7 |
| Subtotal PM Pk Hr. BUILD Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |

Number of Commercial Trips Generated
 Entering 90 71 A.M. 100% Commercial Development
 Exiting 93 74 P.M.

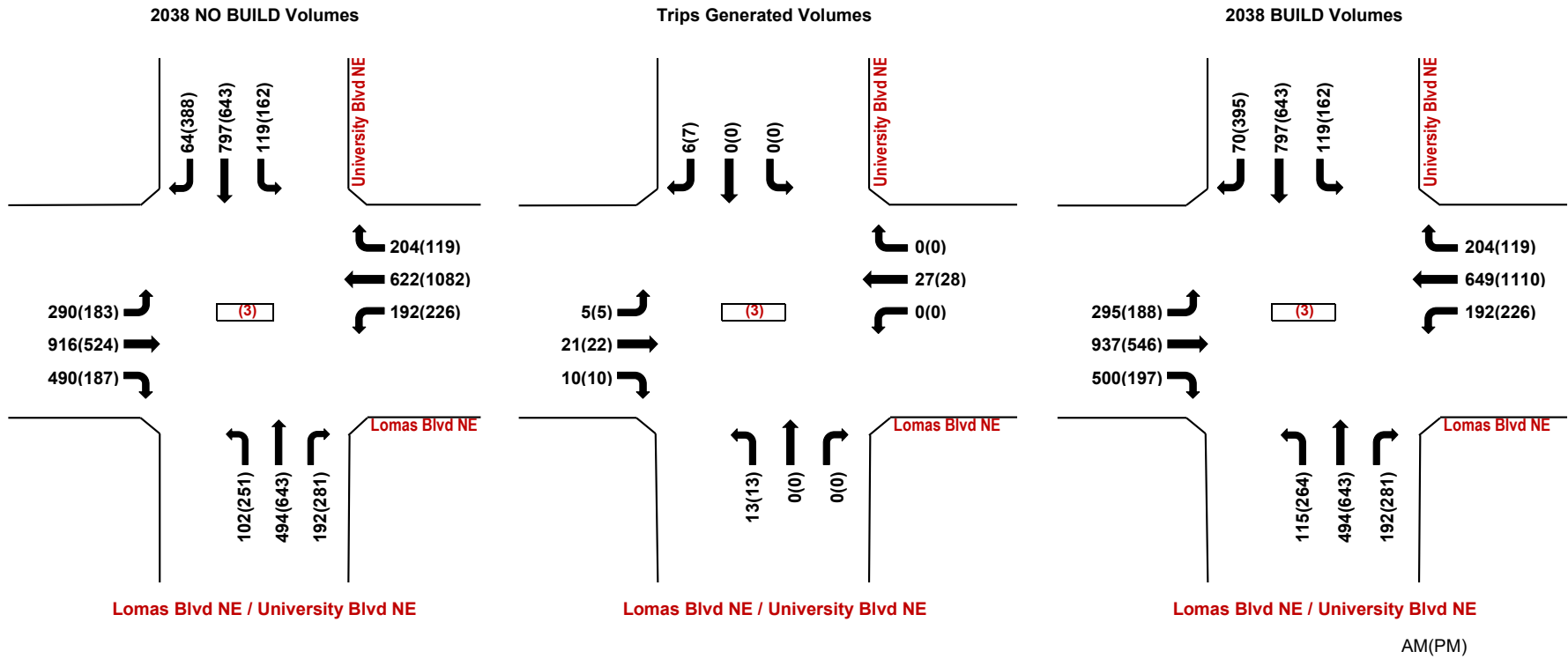
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|--------------------------|---------------------------|-----|-----|---------------------------|-------|-----|---------------------------------|-----|-----|---------------------------------|-----|-----|
| 2025 AM Peak Hr. Volumes | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| 2025 PM Peak Hr. Volumes | 172 | 492 | 176 | 212 | 1,016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|--------|-------|--------|
| 2025 AM Link Volume | 1,592 | 956 | 740 | 920 |
| 2025 PM Link Volume | 840 | 1,340 | 1,104 | 1,120 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.52% | -0.75% | 7.83% | -1.04% |
| 2025-2040 PM Growth Rates | 0.66% | 2.05% | 1.72% | 2.46% |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|----------------------------|---------------------------|---------------|----------|---------------------------|----------|----------|---------------------------------|----------|----------|---------------------------------|----------|----------|
| AM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | | | | | | | | | |
| | 32 | 26 PM | | | | | | | | | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / Medical Arts Ave NE

INTERSECTION: E-W Street: **Lomas Blvd NE** (4)
 N-S Street: **Medical Arts Ave NE**

Year of Existing Counts: 2025
 Horizon Year: 2038

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|------------|---------------------------|------------|----------|----------------------------------|----------|-----------|----------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 94 | 16 | 5 | 49 | 0 | 0 | 0 | 4 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,544 | 268 | 77 | 799 | 0 | 0 | 0 | 72 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 2.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 2.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 42 | 0 | 1 | 33 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,586 | 268 | 78 | 832 | 0 | 0 | 0 | 74 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,586 | 268 | 78 | 832 | 0 | 0 | 0 | 74 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|-----------|---------------------------|--------------|----------|----------------------------------|----------|------------|----------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 49 | 4 | 2 | 108 | 0 | 0 | 0 | 6 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 0 | 797 | 72 | 30 | 1,772 | 0 | 0 | 0 | 102 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 2.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 2.00% | 47.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 44 | 0 | 1 | 35 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 841 | 72 | 31 | 1,807 | 0 | 0 | 0 | 104 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 841 | 72 | 31 | 1,807 | 0 | 0 | 0 | 104 | 0 | 0 | 0 |

Number of Commercial Trips Generated
 Entering: 90 (A.M.), 93 (P.M.)
 Exiting: 71 (A.M.), 74 (P.M.)
 100% Commercial Development

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|--------------------------|---------------------------|------|-----|---------------------------|-------|---|----------------------------------|---|----|----------------------------------|---|---|
| 2025 AM Peak Hr. Volumes | 0 | 1450 | 252 | 72 | 750 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| 2025 PM Peak Hr. Volumes | 0 | 748 | 68 | 28 | 1,664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |

MRCOG Forecast Volumes Worksheet

Based on 2025 Traffic Count

| | | | | |
|---------------------|-------|-------|----|---|
| 2025 AM Link Volume | 1,702 | 822 | 68 | 0 |
| 2025 PM Link Volume | 816 | 1,692 | 96 | 0 |

Based on MRCOG Model (2040 Data Set)

| | | | | |
|---------------------|------|------|------|------|
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |

Growth Rate to Apply to Existing Counts to Match 2040 Forecasts

| | | | | |
|---------------------------|--------|-------|---------|---------|
| 2025-2040 AM Growth Rates | -0.92% | 0.21% | 151.08% | #DIV/0! |
| 2025-2040 PM Growth Rates | 0.87% | 0.24% | 89.79% | #DIV/0! |

Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts

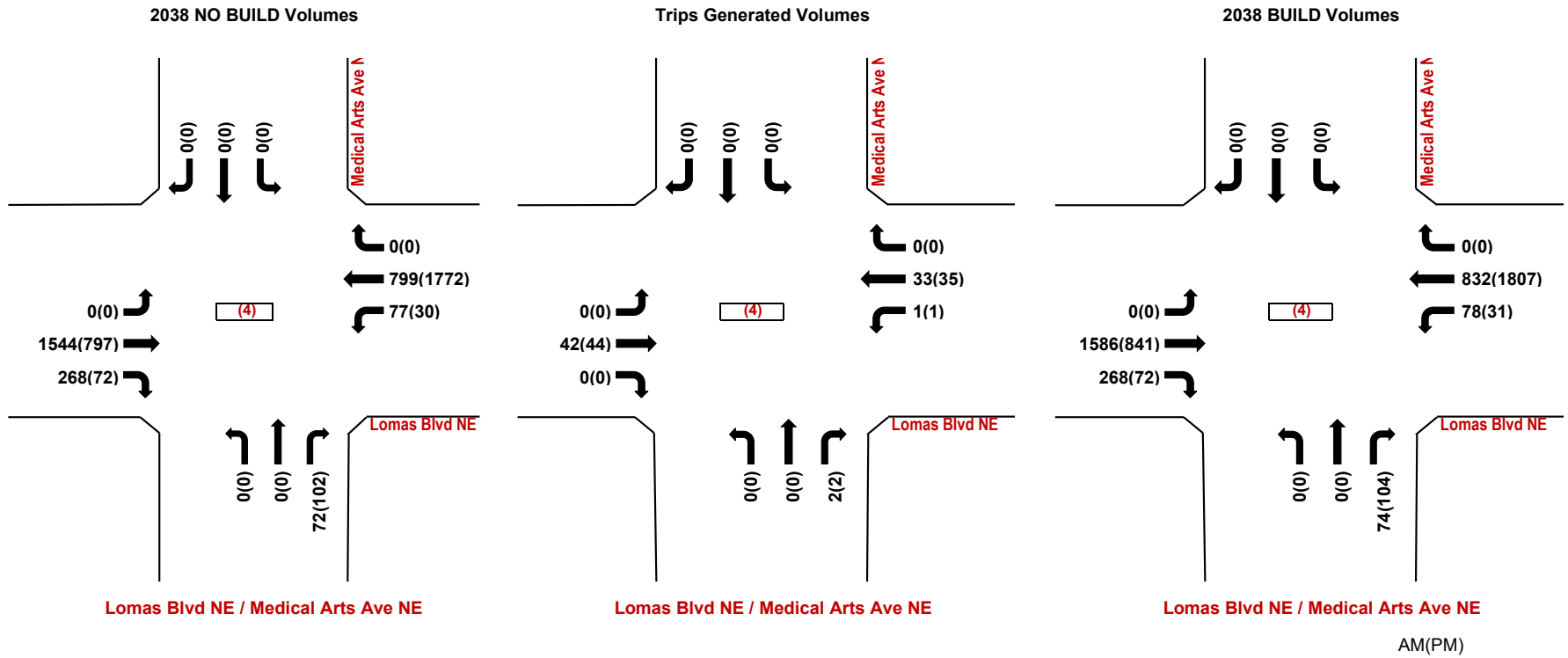
| | | | | |
|---------------------------|--------|-------|-------|--------|
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|----------------------------|---------------------------|----------|----------|---------------------------|----------|----------|----------------------------------|----------|----------|----------------------------------|----------|--|
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net AM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|----------------------------|---------------------------|----------|----------|---------------------------|----------|----------|----------------------------------|----------|----------|----------------------------------|----------|--|
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Net PM Passby Trips | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |

Pass-by Trips
 Entering: 25 (AM), 32 (PM)
 Exiting: 19 (AM), 26 (PM)



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / Torc Dwy / Frtg Rd

INTERSECTION : E-W Street: **Lomas Blvd NE** (5)
 N-S Street: **Torc Dwy / Frtg Rd**

Year of Existing Counts 2025
 Horizon Year 2038

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|----------|---------------------------|------------|----------|---------------------------------|----------|----------|---------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 101 | 0 | 0 | 50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,661 | 0 | 4 | 822 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 44 | 0 | 0 | 35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,705 | 0 | 4 | 857 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,705 | 0 | 4 | 857 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|----------|---------------------------|--------------|----------|---------------------------------|----------|----------|---------------------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 0 | 58 | 0 | 0 | 110 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - P.M.) | 0 | 946 | 0 | 4 | 1,798 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 49.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 46 | 0 | 0 | 36 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 992 | 0 | 4 | 1,834 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| Pass-by Trip Adjustments | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 992 | 0 | 4 | 1,834 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |

Number of Commercial Trips Generated
 Entering 90 71 A.M. 100% Commercial Development
 Exiting 93 74 P.M.

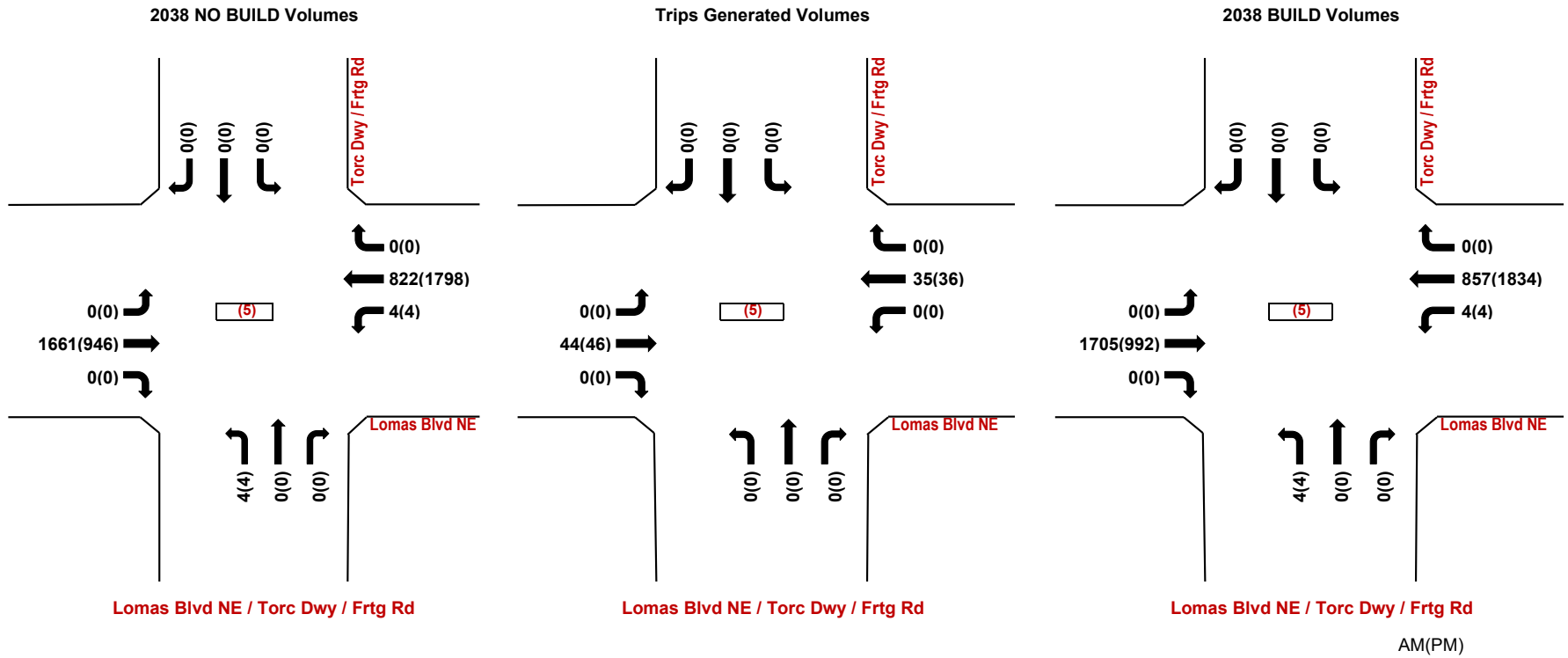
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Torc Dwy / Frtg Rd) | | | Southbound (Torc Dwy / Frtg Rd) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|---------------------------------|---|---|---------------------------------|---|--|
| 2025 AM Peak Hr. Volumes | 0 | 1560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | |
| 2025 PM Peak Hr. Volumes | 0 | 888 | 0 | 4 | 1,688 | 0 | 4 | 0 | 0 | 0 | 0 | |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|-------|---------|
| 2025 AM Link Volume | 1,560 | 776 | 4 | 0 |
| 2025 PM Link Volume | 888 | 1,692 | 4 | 0 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.39% | 0.62% | ##### | #DIV/0! |
| 2025-2040 PM Growth Rates | 0.26% | 0.24% | ##### | #DIV/0! |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| AM Pass-by Trips | | | | |
|----------------------------|-------------|---------------|----------|----------|
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | |
| Percent Entering | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 0 | 0 |
| Pass-by Trips | Entering 25 | Exiting 19 AM | | |
| | 32 | 26 PM | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / West Dwy / Legion Rd

INTERSECTION : E-W Street: **Lomas Blvd NE** (6)
 N-S Street: **West Dwy / Legion Rd**

Year of Existing Counts 2025
 Horizon Year 2038

Growth Rates 0.50% 0.50% 0.50% 0.50%

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|--|---------------------------|--------------|-----------|---------------------------|------------|----------|-----------------------------------|----------|-----------|-----------------------------------|----------|-----------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 20 | 1,540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Background Traffic Growth | 1 | 100 | 0 | 0 | 49 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| Subtotal (NO BUILD - A.M.) | 21 | 1,640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 17 | 27 | 19 | 17 | 0 | 18 | 0 | 18 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 21 | 1,657 | 27 | 19 | 826 | 4 | 18 | 0 | 18 | 9 | 0 | 13 |
| Pass-by Trip Adjustments | 0 | -3 | 10 | 3 | -4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|--|---------------------------|------------|-----------|---------------------------|--------------|----------|-----------------------------------|----------|-----------|-----------------------------------|----------|-----------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| Background Traffic Growth | 1 | 57 | 0 | 0 | 107 | 0 | 0 | 0 | 1 | 0 | 0 | 3 |
| Subtotal (NO BUILD - P.M.) | 13 | 929 | 4 | 0 | 1,759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 18 | 28 | 20 | 18 | 0 | 19 | 0 | 19 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 13 | 947 | 32 | 20 | 1,777 | 4 | 19 | 0 | 28 | 4 | 0 | 43 |
| Pass-by Trip Adjustments | 0 | 0 | 8 | 8 | -9 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |

Number of Commercial Trips Generated
 Entering 90 71 A.M. 100% Commercial Development
 Exiting 93 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|-----------------------------------|---|---|-----------------------------------|---|----|
| 2025 AM Peak Hr. Volumes | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| 2025 PM Peak Hr. Volumes | 12 | 872 | 4 | 0 | 1,652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |

MRCOG Forecast Volumes Worksheet

Based on 2025 Traffic Count

| | | | | |
|---------------------|-------|-------|---|----|
| 2025 AM Link Volume | 1,560 | 764 | 0 | 20 |
| 2025 PM Link Volume | 888 | 1,656 | 8 | 44 |

Based on MRCOG Model (2040 Data Set)

| | | | | |
|---------------------|------|------|------|------|
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |

Growth Rate to Apply to Existing Counts to Match 2040 Forecasts

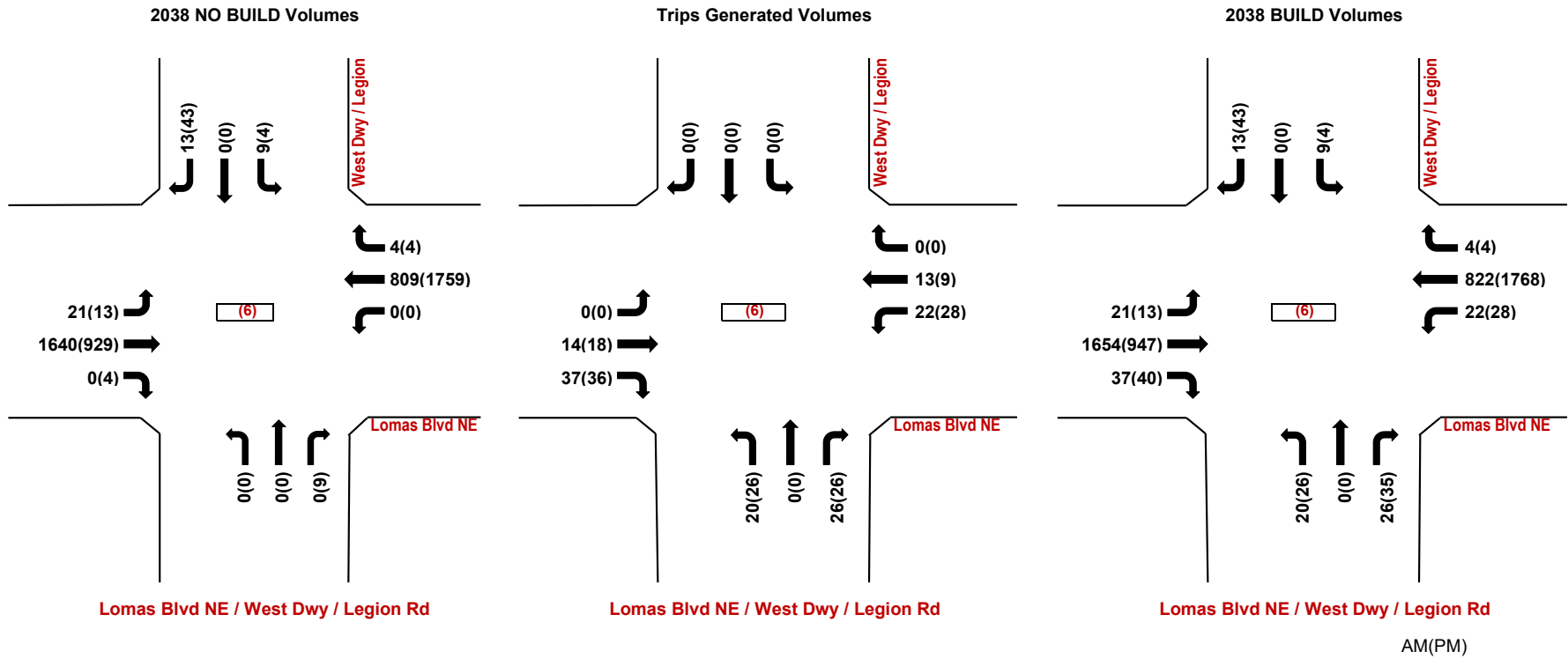
| | | | | |
|---------------------------|--------|-------|---------|---------|
| 2025-2040 AM Growth Rates | -0.39% | 0.73% | #DIV/0! | 252.33% |
| 2025-2040 PM Growth Rates | 0.26% | 0.39% | ##### | 225.76% |

Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts

| | | | | |
|---------------------------|--------|-------|-------|--------|
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

Pass-by Trip Calculations:

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|----------------------------|---------------------------|-----------|-----------|---------------------------|-----------|----------|-----------------------------------|----------|----------|-----------------------------------|----------|----------|
| AM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | -13.00% | 40.00% | 11.00% | -33.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | -3 | 10 | 3 | -8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 22.00% | 0.00% | 11.00% | 0.00% | 40.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| Net AM Passby Trips | 0 | -3 | 10 | 3 | -4 | 0 | 2 | 0 | 8 | 0 | 0 | 0 |
| PM Pass-by Trips | | | | | | | | | | | | |
| Percent Entering | 0.00% | 0.00% | 25.00% | 25.00% | -50.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Volume Entering | 0 | 0 | 8 | 8 | -16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Percent Exiting | 0.00% | 0.00% | 0.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% |
| Volume Exiting | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Net PM Passby Trips | 0 | 0 | 8 | 8 | -9 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Entering | 25 | 19 | AM | | | | | | | | | |
| Exiting | 32 | 26 | PM | | | | | | | | | |



LOBO PLAZA (1300 Lomas Blvd NE) - 2025068
 Projected Turning Movements Worksheet
Lomas Blvd NE / East Dwy

INTERSECTION: E-W Street: **Lomas Blvd NE** (7)
 N-S Street: **East Dwy**

Year of Existing Counts: 2025
 Horizon Year: 2038

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|--------------|-----------|---------------------------|------------|----------|-----------------------|----------|-----------|-----------------------|----------|----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 1,550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Background Traffic Growth | 0 | 101 | 0 | 0 | 49 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Subtotal (NO BUILD - A.M.) | 0 | 1,651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 26.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 18 | 17 | 27 | 19 | 0 | 17 | 0 | 18 | 0 | 0 | 0 |
| Subtotal AM Pk Hr. BUILD Volumes | 0 | 1,669 | 17 | 27 | 820 | 0 | 17 | 0 | 18 | 0 | 0 | 4 |
| Pass-by Trip Adjustments | 0 | -9 | 7 | 6 | -3 | 0 | 4 | 0 | 5 | 0 | 0 | 0 |
| Total AM Peak Hour BUILD Volumes | 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| | 0.50% | | | 0.50% | | | 0.50% | | | 0.50% | | |
|--|---------------------------|------------|-----------|---------------------------|--------------|----------|-----------------------|----------|-----------|-----------------------|----------|-----------|
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volumes | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Background Traffic Growth | 0 | 59 | 0 | 0 | 107 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| Subtotal (NO BUILD - P.M.) | 0 | 959 | 0 | 0 | 1,755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| Percent Commercial Trips Generated(Entering) | 0.00% | 0.00% | 19.00% | 30.00% | 21.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% |
| Percent Commercial Trips Generated(Exiting) | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 24.00% | 0.00% | 26.00% | 0.00% | 0.00% | 0.00% |
| Total Trips Generated | 0 | 19 | 18 | 28 | 20 | 0 | 18 | 0 | 19 | 0 | 0 | 0 |
| Subtotal PM Pk Hr. BUILD Volumes | 0 | 978 | 18 | 28 | 1,775 | 0 | 18 | 0 | 19 | 0 | 0 | 13 |
| Pass-by Trip Adjustments | 0 | -9 | 8 | 8 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 0 |
| Total PM Peak Hour BUILD Volumes | 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |

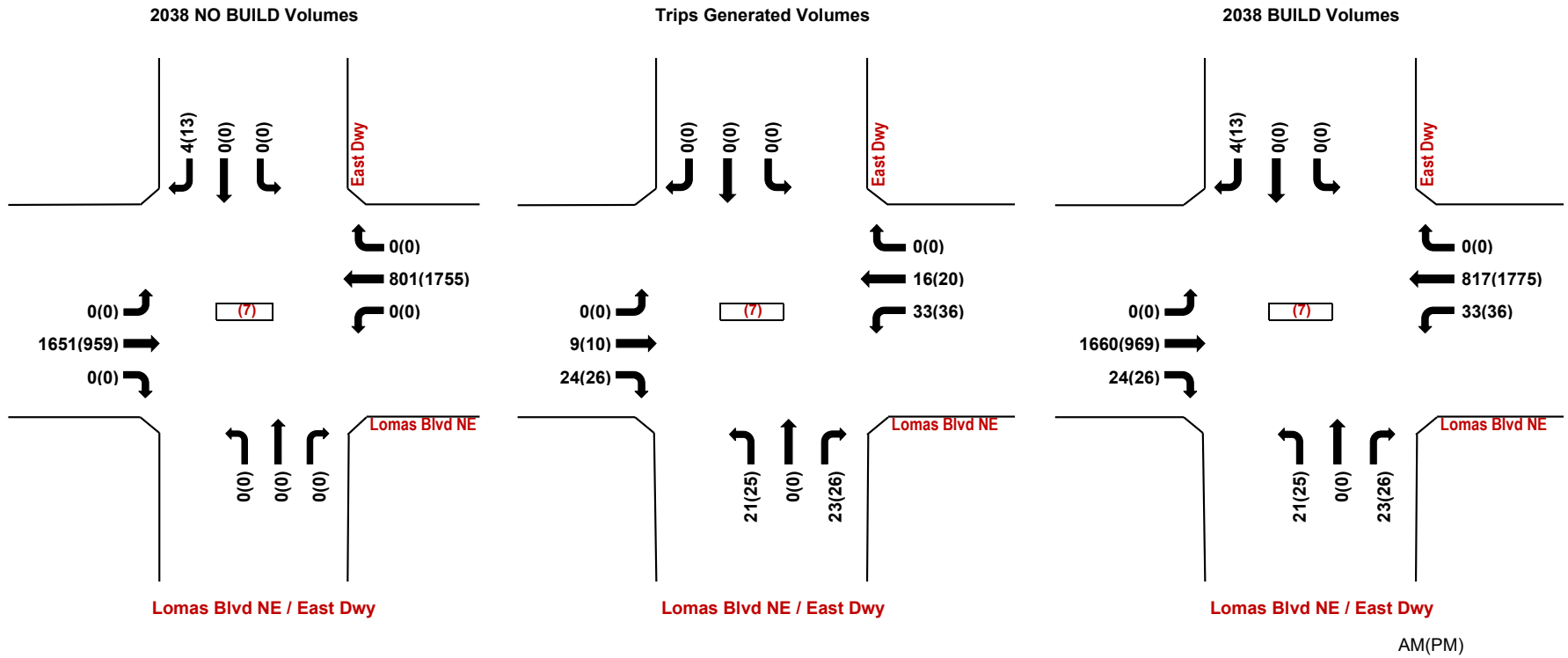
Number of Commercial Trips Generated: Entering 90, Exiting 71 A.M. 100% Commercial Development
 Entering 93, Exiting 74 P.M.

| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|--------------------------|---------------------------|------|---|---------------------------|-------|---|-----------------------|---|---|-----------------------|----|--|
| 2025 AM Peak Hr. Volumes | 0 | 1550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 4 | |
| 2025 PM Peak Hr. Volumes | 0 | 900 | 0 | 0 | 1,648 | 0 | 0 | 0 | 0 | 0 | 12 | |

MRCOG Forecast Volumes Worksheet

| Based on 2025 Traffic Count | | | | |
|--|--------|-------|---------|---------|
| 2025 AM Link Volume | 1,550 | 752 | 0 | 4 |
| 2025 PM Link Volume | 900 | 1,648 | 0 | 12 |
| Based on MRCOG Model (2040 Data Set) | | | | |
| 2016 AM Link Volume | 370 | 327 | 1248 | 1049 |
| 2016 PM Link Volume | 313 | 1024 | 1058 | 1246 |
| 2040 AM Link Volume | 1468 | 848 | 1609 | 777 |
| 2040 PM Link Volume | 923 | 1753 | 1389 | 1534 |
| Growth Rate to Apply to Existing Counts to Match 2040 Forecasts | | | | |
| 2025-2040 AM Growth Rates | -0.35% | 0.85% | #DIV/0! | ##### |
| 2025-2040 PM Growth Rates | 0.17% | 0.42% | #DIV/0! | 845.56% |
| Growth Rate to Apply to 2016 Model Volumes to Match 2025 Forecasts | | | | |
| 2016-2040 AM Growth Rates | 12.36% | 6.64% | 1.21% | -1.08% |
| 2016-2040 PM Growth Rates | 8.12% | 2.97% | 1.30% | 0.96% |

| Pass-by Trip Calculations: | | | | | | | | | | | | | |
|----------------------------|---------------------------|------------|----------|---------------------------|-----------|----------|-----------------------|----------|----------|-----------------------|----------|----------|--|
| AM Pass-by Trips | | | | | | | | | | | | | |
| | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | | |
| Percent Entering | 0.00% | -67.00% | 27.00% | 22.00% | -11.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | -17 | 7 | 6 | -3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 40.00% | 0.00% | 0.00% | 0.00% | 0.00% | 22.00% | 0.00% | 27.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 8 | 0 | 0 | 0 | 0 | 4 | 0 | 5 | 0 | 0 | 0 | |
| Net AM Passby Trips | 0 | -9 | 7 | 6 | -3 | 0 | 4 | 0 | 5 | 0 | 0 | 0 | |
| PM Pass-by Trips | | | | | | | | | | | | | |
| Percent Entering | 0.00% | -50.00% | 25.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | 0.00% | |
| Volume Entering | 0 | -16 | 8 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Percent Exiting | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | 0.00% | 25.00% | 0.00% | 25.00% | 0.00% | 0.00% | 0.00% | |
| Volume Exiting | 0 | 7 | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 0 | |
| Net PM Passby Trips | 0 | -9 | 8 | 8 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 0 | |
| Pass-by Trips | Entering 25 | Exiting 19 | AM | | | | | | | | | | |
| | 32 | 26 | PM | | | | | | | | | | |

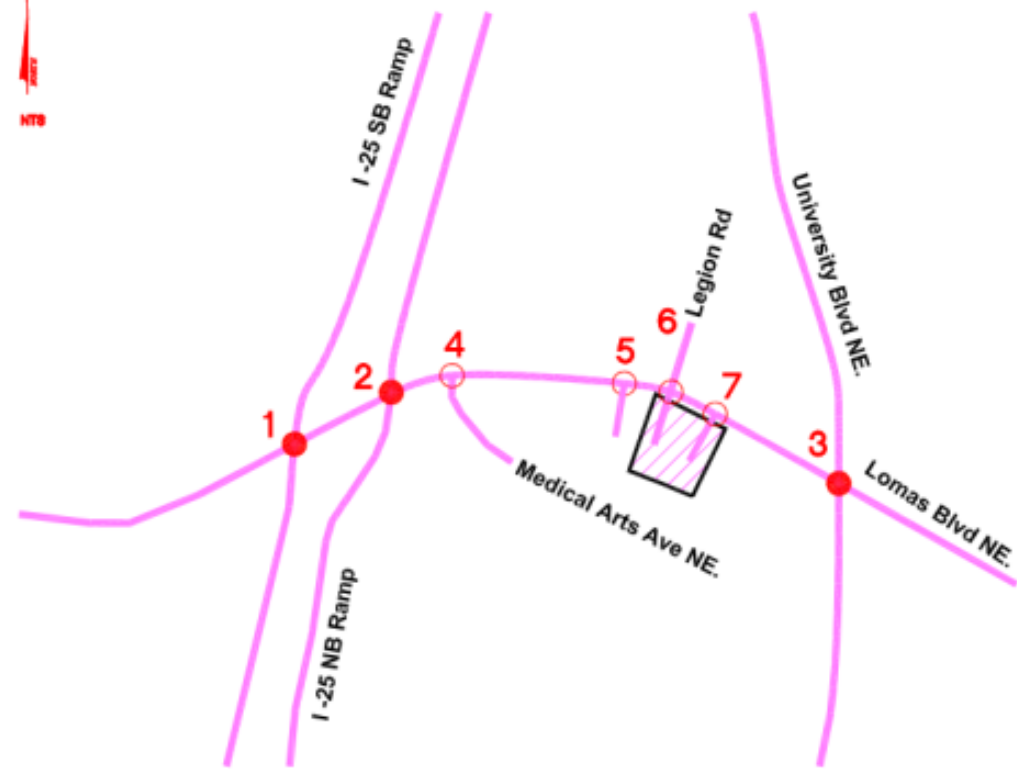


AM(PM)

Lobo Plaza

1300 Lomas Blvd NE

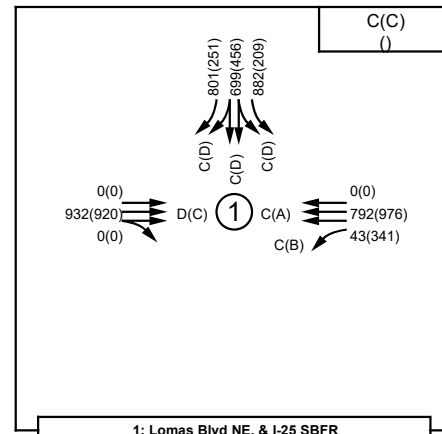
Intersection Map



- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

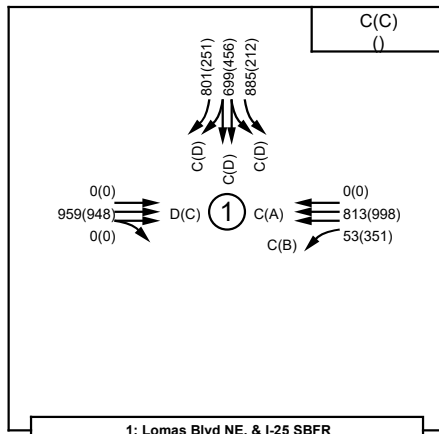
Tierra West, LLC
 5571 Midway Park Pl, NE
 Albuquerque, NM 87109
 (505) 858-3100 (Voice)

2038 NO BUILD - SIGNALIZED EXISTING GEOMETRY



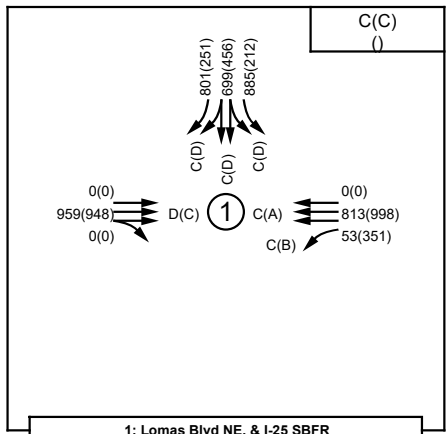
1: Lomas Blvd NE. & I-25 SBFR

2038 BUILD - SIGNALIZED EXISTING GEOMETRY

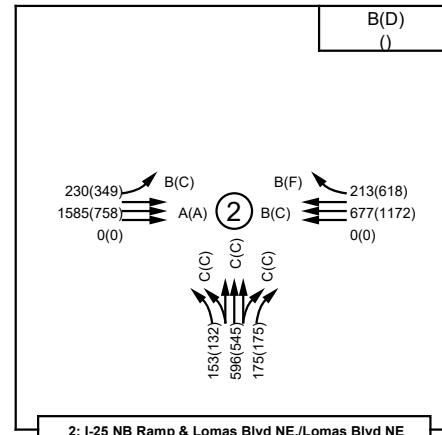


1: Lomas Blvd NE. & I-25 SBFR

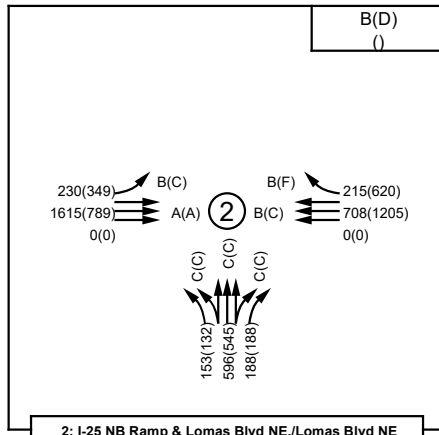
2038 BUILD MITIGATED - SIGNALIZED EXISTING GEOMETRY



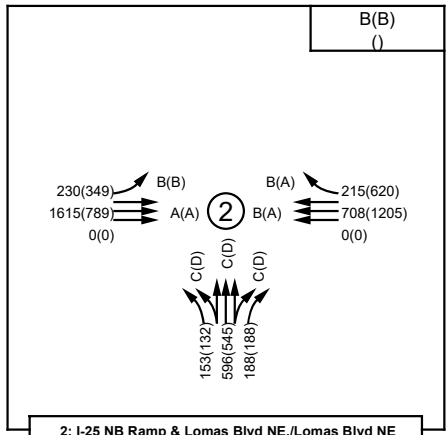
1: Lomas Blvd NE. & I-25 SBFR



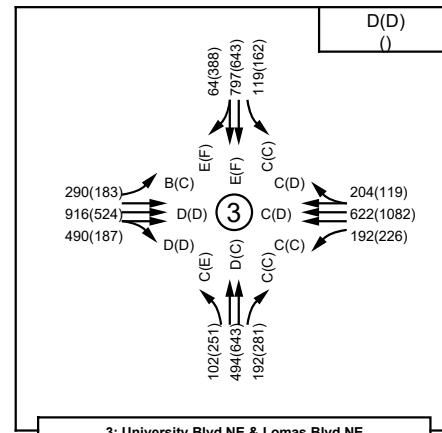
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



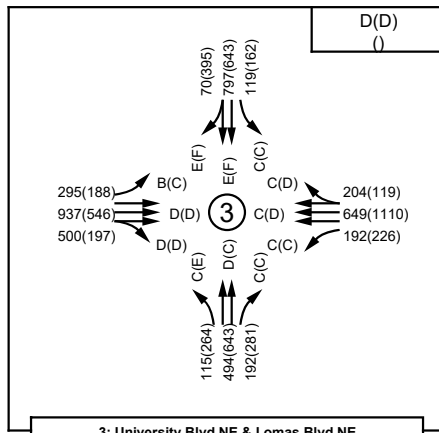
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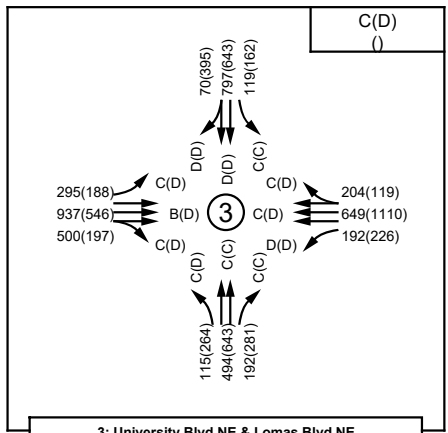
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



3: University Blvd NE & Lomas Blvd NE



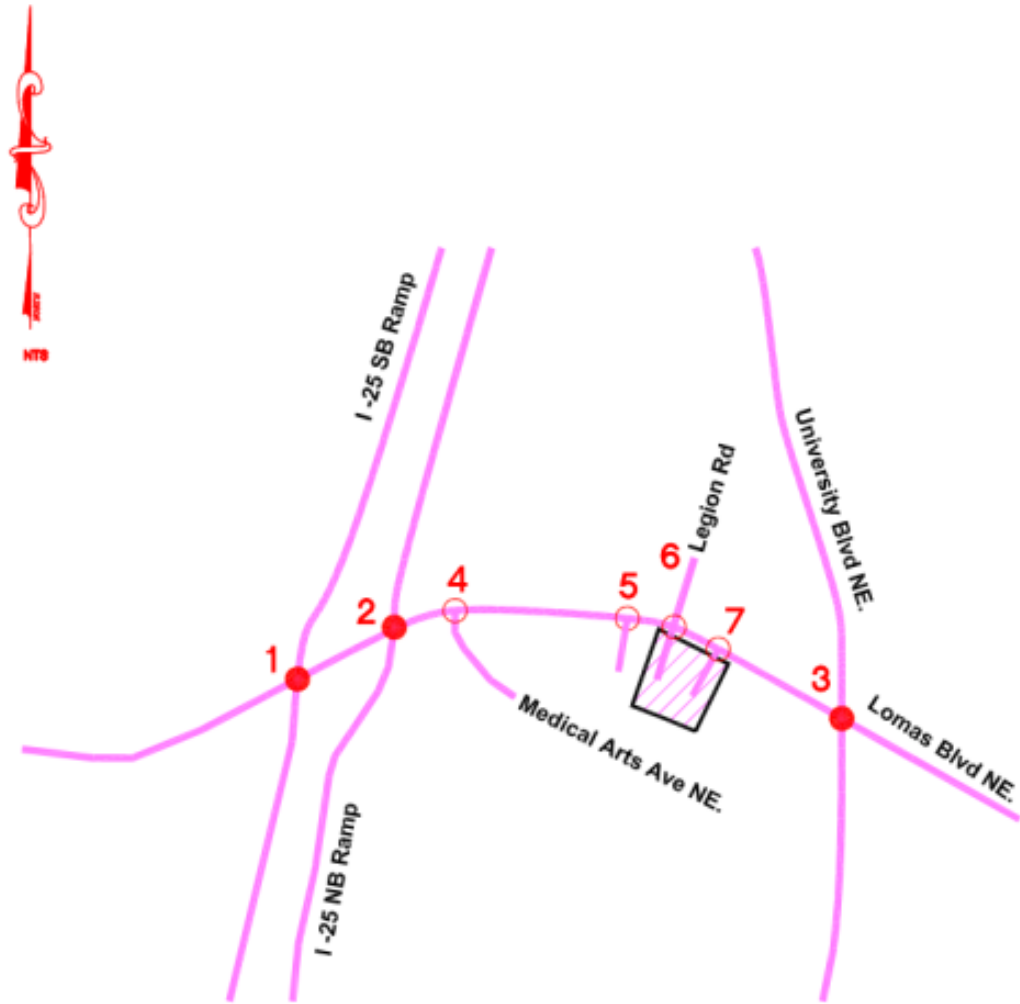
3: University Blvd NE & Lomas Blvd NE



3: University Blvd NE & Lomas Blvd NE

2025068 - Lobo Plaza
 1300 Lomas Blvd NE, Albuquerque, NM
 LOS / Volume Analysis Map

Lobo Plaza 1300 Lomas Blvd NE Intersection Map



- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION

Tierra West, LLC
5571 Midway Park Pl, NE
Albuquerque, NM 87109
(505) 858-3100 (Voice)

**2038 NO BUILD - UNSIGNALIZED
EXISTING GEOMETRY**

**2038 BUILD - UNSIGNALIZED
EXISTING GEOMETRY**

**2038 BUILD MITIGATED - UNSIGNALIZED
EXISTING GEOMETRY**

| | | |
|--|--|--|
| <p>TWSC</p> <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> | <p>TWSC</p> <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> | <p>TWSC</p> <p style="color: blue; text-align: center;">NO MITIGATIONS RECOMMENDED</p> <p>4: Medical Arts Ave NE & Lomas Blvd NE</p> |
| <p>TWSC</p> <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> | <p>TWSC</p> <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> | <p>TWSC</p> <p style="color: blue; text-align: center;">NO MITIGATIONS RECOMMENDED</p> <p>5: Torc DWY / Frontage Rd & Lomas Blvd NE</p> |
| <p>TWSC</p> <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> | <p>TWSC</p> <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> | <p>TWSC</p> <p>6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE</p> |
| <p>TWSC</p> <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> | <p>TWSC</p> <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> | <p>TWSC</p> <p>7: East DWY - Lobo Pl & Lomas Blvd NE</p> |

2025068 - Lobo Plaza
 1300 Lomas Blvd NE, Albuquerque, NM
 LOS / Volume Analysis Map

HCS Results Summary Sheet

1: Lomas Blvd NE & I-25 SB Ramp

2038 Conditions (HCS Results)

Lomas Blvd NE

I-25 SB Ramp

Signalized

| 1: Lomas Blvd NE & I-25 SB Ramp | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 SB Ramp) | | | SB (I-25 SB Ramp) | | |
|--------------------------------------|--------------------|------|------|--------------------|------|------|-------------------|---|---|-------------------|------|-------|
| 2038 Conditions (HCS Results) | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 |
| V/C Ratio | 0.00 | 0.47 | 0.00 | 0.12 | 0.28 | 0.00 | | | | 1.12 | 0.42 | 1.14 |
| Level-of-Service | | C | | C | B | | | | | F | C | F |
| Control Delay (Seconds) | | 28.3 | | 21.6 | 11.9 | | | | | 101.0 | 21.6 | 111.4 |
| Intersection LOS (HCS Result) | E - 57.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.5 | 0.0 | 1.1 | 3.8 | 0.0 | | | | 48.7 | 10.2 | 47.0 |
| 2038 BUILD Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |
| V/C Ratio | 0.00 | 0.49 | 0.00 | 0.15 | 0.29 | 0.00 | | | | 1.12 | 0.42 | 1.14 |
| Level-of-Service | | C | | C | B | | | | | F | C | F |
| Control Delay (Seconds) | | 28.5 | | 22.0 | 12.1 | | | | | 102.5 | 21.6 | 111.4 |
| Intersection LOS (HCS Result) | E - 57.1 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 10.8 | 0.0 | 1.4 | 4.0 | 0.0 | | | | 49.2 | 10.2 | 47.0 |
| Mitigage Lane Geometry | 0 | 3> | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 1 | <2> | 1 |
| 2038 BUILD MITIGATED Volumes | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 |
| V/C Ratio | 0.00 | 0.76 | 0.00 | 0.29 | 0.50 | 0.00 | | | | 0.86 | 0.32 | 0.88 |
| Level-of-Service | | D | | D | D | | | | | C | B | C |
| Control Delay (Seconds) | | 44.0 | | 35.5 | 35.5 | | | | | 28.7 | 12.2 | 31.1 |
| Intersection LOS (HCS Result) | C - 27.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 13.2 | 0.0 | 2.2 | 12.6 | 0.0 | | | | 27.1 | 7.7 | 25.8 |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|-----|------|------|------|---|---|---|------|------|------|
| 2038 NO BUILD Volumes | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| V/C Ratio | 0.00 | 0.63 | | 0.54 | 0.26 | 0.00 | | | | 0.49 | 0.51 | 0.66 |
| Level-of-Service | | C | | B | A | | | | | D | D | D |
| Control Delay (Seconds) | | 21.5 | | 15.9 | 2.7 | | | | | 43.1 | 41.4 | 49.8 |
| Intersection LOS (HCS Result) | C - 22.4 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.5 | 0.0 | 6.8 | 1.7 | 0.0 | | | | 9.6 | 10.0 | 12.0 |
| 2038 BUILD Volumes | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 |
| V/C Ratio | 0.00 | 0.37 | | 0.56 | 0.27 | | | | | 0.50 | 0.51 | 0.66 |
| Level-of-Service | | C | | B | A | | | | | D | D | D |
| Control Delay (Seconds) | | 21.6 | | 16.5 | 3.3 | | | | | 43.3 | 41.4 | 49.8 |
| Intersection LOS (HCS Result) | C - 22.5 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | 9.7 | 0.0 | 6.9 | 2.1 | 0.0 | | | | 9.8 | 10.0 | 12.0 |

HCS Results Summary Sheet

2: Lomas Blvd NE & I-25 NB Ramp

2038 Conditions (HCS Results)

Lomas Blvd NE

I-25 NB Ramp

Signalized

| 2: Lomas Blvd NE & I-25 NB Ramp 2038 Conditions (HCS Results) | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (I-25 NB Ramp) | | | SB (I-25 NB Ramp) | | |
|--|--------------------|-------|---|--------------------|------|------|-------------------|------|------|-------------------|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 230 | 1,585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| V/C Ratio | 0.37 | 0.48 | | | 0.33 | 0.33 | 0.31 | 0.38 | 0.40 | | | |
| Level-of-Service | B | B | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.7 | 19.7 | | | 22.6 | 23.6 | 32.7 | 32.5 | 34.5 | | | |
| Intersection LOS (HCS Result) | C - 23.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 13.8 | | | 7.2 | 7.1 | 6.1 | 7.7 | 7.2 | | | |
| 2038 BUILD Volumes | 230 | 1,615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.37 | 0.51 | | | 0.34 | 0.33 | 0.31 | 0.38 | 0.42 | | | |
| Level-of-Service | B | C | | | C | C | C | C | C | | | |
| Control Delay (Seconds) | 13.9 | 20.0 | | | 22.8 | 23.6 | 32.7 | 32.5 | 35.2 | | | |
| Intersection LOS (HCS Result) | C - 23.8 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 4.7 | 14.0 | | | 7.5 | 7.1 | 6.1 | 7.7 | 7.8 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|------|---|---|-------|------|------|------|------|---|---|---|
| 2038 NO BUILD Volumes | 349 | 758 | 0 | 0 | 1,172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| V/C Ratio | 0.49 | 0.20 | | | 0.58 | 0.98 | 0.49 | 0.64 | 0.73 | | | |
| Level-of-Service | C | A | | | C | E | D | D | D | | | |
| Control Delay (Seconds) | 27.3 | 8.9 | | | 29.3 | 67.4 | 48.0 | 48.6 | 52.8 | | | |
| Intersection LOS (HCS Result) | D - 35.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 17.6 | 6.2 | | | 13.7 | 30.6 | 6.6 | 8.8 | 9.0 | | | |
| 2038 BUILD Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.76 | 0.26 | | | 0.60 | 0.99 | 0.26 | 0.34 | 0.41 | | | |
| Level-of-Service | D | B | | | C | E | C | C | D | | | |
| Control Delay (Seconds) | 38.4 | 14.8 | | | 29.7 | 68.2 | 33.7 | 33.9 | 36.9 | | | |
| Intersection LOS (HCS Result) | C - 34.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 11.4 | 7.8 | | | 14.1 | 30.8 | 5.6 | 7.6 | 8.3 | | | |
| Mitigate Lane Geometry | 1 | 3 | 0 | 0 | 3 | 1 | 1 | <3> | 1 | 0 | 0 | 0 |
| 2038 BUILD MITIGATED Volumes | 349 | 789 | 0 | 0 | 1,205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| V/C Ratio | 0.94 | 0.23 | | | 0.39 | 0.64 | 0.37 | 0.49 | 0.60 | | | |
| Level-of-Service | D | A | | | B | B | D | D | D | | | |
| Control Delay (Seconds) | 54.7 | 8.1 | | | 11.9 | 17.8 | 44.5 | 44.1 | 51.8 | | | |
| Intersection LOS (HCS Result) | C - 23.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 16.6 | 5.1 | | | 8.9 | 16.1 | 6.6 | 8.5 | 9.7 | | | |

Synchro Results Summary Sheet

3: Lomas Blvd NE & University Blvd NE

2038 Conditions

Lomas Blvd NE

University Blvd NE

Signalized

| 3: Lomas Blvd NE & University Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (University Blvd NE) | | | SB (University Blvd NE) | | |
|--|---------------------------|------|------|---------------------------|------|------|--------------------------------|------|------|--------------------------------|------|------|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| V/C Ratio | 0.64 | 0.63 | 0.72 | 0.73 | 0.40 | 0.41 | 0.56 | 0.56 | 0.37 | 0.43 | 0.93 | 0.93 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 19.1 | 40.8 | 47.3 | 27.4 | 23.9 | 25.0 | 33.8 | 38.5 | 29.9 | 29.6 | 68.6 | 68.1 |
| Intersection LOS | D - 40.6 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.0 | 19.0 | 21.3 | 5.4 | 8.8 | 9.0 | 3.7 | 10.0 | 7.3 | 4.2 | 20.8 | 21.2 |
| 2038 BUILD Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| V/C Ratio | 0.66 | 0.64 | 0.74 | 0.74 | 0.41 | 0.42 | 0.61 | 0.56 | 0.37 | 0.43 | 0.97 | 0.97 |
| Level-of-Service | B | D | D | C | C | C | C | D | C | C | E | E |
| Control Delay (Seconds) | 19.8 | 41.3 | 48.1 | 28.2 | 24.2 | 25.3 | 34.3 | 38.5 | 29.9 | 29.6 | 75.5 | 75.1 |
| Intersection LOS | D - 42.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.1 | 19.4 | 21.8 | 5.4 | 9.1 | 9.3 | 4.2 | 10.0 | 7.3 | 4.2 | 21.9 | 22.3 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2038 BUILD MITIGATED Volumes | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| V/C Ratio | 0.70 | 0.72 | 0.83 | 0.74 | 0.51 | 0.52 | 0.50 | 0.45 | 0.32 | 0.37 | 0.77 | 0.77 |
| Level-of-Service | C | B | C | D | C | C | C | C | C | C | D | D |
| Control Delay (Seconds) | 20.4 | 13.3 | 22.9 | 35.1 | 31.6 | 33.6 | 27.9 | 31.4 | 25.0 | 24.5 | 44.0 | 43.8 |
| Intersection LOS | C - 28.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.4 | 6.7 | 9.3 | 7.2 | 10.3 | 10.7 | 3.7 | 9.1 | 6.6 | 3.8 | 17.2 | 17.5 |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-----------------|------|------|------|-------|------|------|------|------|------|-------|-------|
| 2038 NO BUILD Volumes | 183 | 524 | 187 | 226 | 1,082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| V/C Ratio | 0.65 | 0.42 | 0.43 | 0.59 | 0.66 | 0.66 | 0.90 | 0.55 | 0.42 | 0.52 | 1.06 | 1.06 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 29.8 | 43.6 | 45.2 | 25.6 | 35.6 | 38.1 | 60.9 | 34.9 | 25.7 | 28.6 | 98.9 | 100.7 |
| Intersection LOS | D - 50.9 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 6.7 | 11.7 | 12.0 | 7.7 | 15.1 | 16.3 | 11.0 | 12.5 | 9.7 | 5.9 | 31.5 | 29.6 |
| 2038 BUILD Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| V/C Ratio | 0.68 | 0.45 | 0.46 | 0.61 | 0.69 | 0.69 | 0.91 | 0.54 | 0.41 | 0.51 | 1.07 | 1.07 |
| Level-of-Service | C | D | D | C | D | D | E | C | C | C | F | F |
| Control Delay (Seconds) | 31.4 | 44.7 | 46.5 | 27.0 | 37.3 | 40.3 | 63.5 | 34.1 | 24.9 | 28.4 | 101.4 | 103.2 |
| Intersection LOS | D - 52.2 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 7.0 | 12.3 | 12.5 | 7.8 | 15.8 | 17.1 | 11.6 | 12.3 | 9.6 | 5.9 | 32.0 | 30.1 |
| Mitigate Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 1 | 2 | 1 | 1 | 2> | 0 |
| 2038 BUILD MITIGATED Volumes | 188 | 546 | 197 | 226 | 1,110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| V/C Ratio | 0.82 | 0.45 | 0.46 | 0.80 | 0.76 | 0.77 | 0.90 | 0.46 | 0.40 | 0.47 | 0.86 | 0.86 |
| Level-of-Service | D | D | D | D | D | D | D | C | C | C | D | D |
| Control Delay (Seconds) | 52.3 | 44.6 | 46.3 | 53.0 | 42.5 | 47.0 | 51.8 | 27.9 | 24.1 | 23.4 | 49.7 | 50.8 |
| Intersection LOS | D - 42.7 | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 8.9 | 12.2 | 12.4 | 8.4 | 16.8 | 18.3 | 10.6 | 11.2 | 9.5 | 5.3 | 23.4 | 22.0 |

Synchro Results Summary Sheet

4: Lomas Blvd NE & Medical Arts Ave NE

2038 Conditions

Lomas Blvd NE

Medical Arts Ave NE

Unsignalized

| 4: Lomas Blvd NE & Medical Arts Ave NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Medical Arts Ave NE) | | | SB (Medical Arts Ave NE) | | |
|--|--------------------|-------|-----|--------------------|-----|---|--------------------------|---|------|--------------------------|---|---|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 0 | | 1 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | | 1,544 | 268 | 77 | 799 | | 0 | | 72 | | | |
| V/C Ratio | | | | 0.28 | | | | | 0.10 | | | |
| Level-of-Service | | | | C | | | | | B | | | |
| Control Delay (Seconds) | | | | 23.4 | | | | | 10.5 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.1 | | | | | 0.3 | | | |
| 2038 BUILD Volumes | | 1,586 | 268 | 78 | 832 | | 0 | | 74 | | | |
| V/C Ratio | | | | 0.31 | | | | | 0.10 | | | |
| Level-of-Service | | | | D | | | | | B | | | |
| Control Delay (Seconds) | | | | 25.1 | | | | | 10.5 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 1.2 | | | | | 0.3 | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|----|------|-------|--|---|--|------|--|--|--|
| 2038 NO BUILD Volumes | | 797 | 72 | 30 | 1,772 | | 0 | | 102 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.12 | | | |
| Level-of-Service | | | | B | | | | | A | | | |
| Control Delay (Seconds) | | | | 10.9 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.1 | | | | | 0.4 | | | |
| 2038 BUILD Volumes | | 841 | 72 | 31 | 1,807 | | 0 | | 104 | | | |
| V/C Ratio | | | | 0.05 | | | | | 0.13 | | | |
| Level-of-Service | | | | B | | | | | B | | | |
| Control Delay (Seconds) | | | | 11.3 | | | | | 10.0 | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.2 | | | | | 0.4 | | | |

Synchro Results Summary Sheet

5: Torc DWY / Frontage Rd & Lomas Blvd NE

2038 Conditions

Lomas Blvd NE

Torc DWY / Frontage Rd

Unsignalized

| 5: Torc DWY / Frontage Rd & Lomas Blvd NE | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Torc DWY / Frontage Rd) | | | SB (Torc DWY / Frontage Rd) | | |
|---|--------------------|-------|---|--------------------|-----|---|-----------------------------|---|---|-----------------------------|---|---|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | | 3> | 0 | 1 | 3 | | 1> | | 0 | | | |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | | 1,661 | 0 | 4 | 822 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 12.7 | | | 13.5 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2038 BUILD Volumes | | 1,705 | 0 | 4 | 857 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 13.3 | | | 13.5 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.1 | | | | | |

PM Peak Hour

| | | | | | | | | | | | | |
|-----------------------------|-------------|-----|---|------|-------|--|------|--|---|--|--|--|
| 2038 NO BUILD Volumes | | 946 | 0 | 4 | 1,798 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | A | | | B | | | | | |
| Control Delay (Seconds) | | | | 9.9 | | | 12.0 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |
| 2038 BUILD Volumes | | 992 | 0 | 4 | 1,834 | | 4 | | 0 | | | |
| V/C Ratio | | | | 0.01 | | | 0.01 | | | | | |
| Level-of-Service | | | | B | | | B | | | | | |
| Control Delay (Seconds) | | | | 10.2 | | | 12.0 | | | | | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | | | | 0.0 | | | 0.0 | | | | | |

Synchro Results Summary Sheet

6: Lomas Blvd NE & Legion Rd / West DWY

2038 Conditions

Lomas Blvd NE

Legion Rd / West DWY

Unsignalized

| 6: Lomas Blvd NE & Legion Rd / West DWY | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (Legion Rd / West DWY) | | | SB (Legion Rd / West DWY) | | |
|---|--------------------|-------|----|--------------------|-----|---|----------------------------|-----|------|----------------------------|------|----|
| 2038 Conditions | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 21 | 1,640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | | | | | | | | 0.03 | |
| Level-of-Service | A | | | A | | | | | A | | B | |
| Control Delay (Seconds) | 0.1 | | | 0.0 | | | | | 9.9 | | 10.6 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | | 0.0 | | 0.1 | |
| 2038 BUILD Volumes | 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | 0.05 | | | | | 0.11 | | 0.04 | |
| Level-of-Service | B | | | B | | | | | B | | B | |
| Control Delay (Seconds) | 10.0 | | | 13.4 | | | | | 14.2 | | 10.9 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.2 | | | | | 0.3 | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 21 | 1,654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| V/C Ratio | 0.03 | | | 0.05 | | | | | 0.13 | | 0.04 | |
| Level-of-Service | B | | | B | | | | | C | | B | |
| Control Delay (Seconds) | 10.0 | | | 13.4 | | | | | 16.3 | | 11.4 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.2 | | | | | 0.4 | | 0.1 | |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|-----|------|---|------|----|
| 2038 NO BUILD Volumes | 13 | 929 | 4 | 0 | 1,759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | | | | | | 0.01 | | 0.08 | |
| Level-of-Service | B | | | A | | | | | A | | B | |
| Control Delay (Seconds) | 13.2 | | | 0.0 | | | | | 9.7 | | 11.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.0 | | | | | 0.0 | | 0.3 | |
| 2038 BUILD Volumes | 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | 0.04 | | | | | 0.11 | | 0.08 | |
| Level-of-Service | B | | | B | | | | | B | | B | |
| Control Delay (Seconds) | 13.4 | | | 10.3 | | | | | 12.0 | | 11.6 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | | 0.4 | | 0.3 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 13 | 947 | 40 | 28 | 1,768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| V/C Ratio | 0.03 | | | 0.04 | | | | | 0.11 | | 0.09 | |
| Level-of-Service | B | | | B | | | | | B | | B | |
| Control Delay (Seconds) | 13.4 | | | 10.3 | | | | | 12.0 | | 12.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.1 | | | 0.1 | | | | | 0.4 | | 0.3 | |

Synchro Results Summary Sheet

7: Lomas Blvd NE & East DWY

2038 Conditions

Lomas Blvd NE

East DWY

Unsignalized

| 7: Lomas Blvd NE & East DWY 2038 Conditions | EB (Lomas Blvd NE) | | | WB (Lomas Blvd NE) | | | NB (East DWY) | | | SB (East DWY) | | |
|--|--------------------|-------|----|--------------------|-----|---|---------------|------|----|---------------|------|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Existing Lane Geometry | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| AM Peak Hour | | | | | | | | | | | | |
| 2038 NO BUILD Volumes | 0 | 1,651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| V/C Ratio | | | | | | | | | | | 0.01 | |
| Level-of-Service | A | | | A | | | | A | | | A | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.0 | |
| 2038 BUILD Volumes | 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.10 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | B | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.6 | | | | 14.0 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.0 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 0 | 1,660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| V/C Ratio | | | | 0.07 | | | | 0.11 | | | 0.01 | |
| Level-of-Service | A | | | B | | | | C | | | A | |
| Control Delay (Seconds) | 0.0 | | | 13.6 | | | | 15.6 | | | 9.5 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.4 | | | 0.0 | |

PM Peak Hour

| | | | | | | | | | | | | |
|------------------------------|-------------|-----|----|------|-------|---|----|------|----|---|------|----|
| 2038 NO BUILD Volumes | 0 | 959 | 0 | 0 | 1,755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| V/C Ratio | | | | | | | | | | | 0.02 | |
| Level-of-Service | A | | | A | | | | A | | | B | |
| Control Delay (Seconds) | 0.0 | | | 0.0 | | | | 0.0 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.0 | | | | | | | 0.1 | |
| 2038 BUILD Volumes | 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.4 | | | | 11.9 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |
| Mitigated (Staged Left Turn) | 1 | 3> | 0 | 1 | 3> | 0 | 0 | <1> | 0 | 0 | <1> | 0 |
| 2038 BUILD MITIGATED Volumes | 0 | 969 | 26 | 36 | 1,775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| V/C Ratio | | | | 0.05 | | | | 0.09 | | | 0.02 | |
| Level-of-Service | A | | | B | | | | B | | | B | |
| Control Delay (Seconds) | 0.0 | | | 10.4 | | | | 11.9 | | | 10.8 | |
| Intersection LOS | TWSC | | | | | | | | | | | |
| 95th Percentile Queue (veh) | 0.0 | | | 0.2 | | | | 0.3 | | | 0.1 | |

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/17/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 875 | 40 | 744 | 828 | 656 | 752 |
| Future Volume (vph) | 875 | 40 | 744 | 828 | 656 | 752 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 28.0 | 44.0 | 44.0 | 55.0 | 55.0 | 55.0 |
| Actuated g/C Ratio | 0.25 | 0.40 | 0.40 | 0.50 | 0.50 | 0.50 |
| v/c Ratio | 0.68 | 0.16 | 0.37 | 0.72 | 0.75 | 0.67 |
| Control Delay (s/veh) | 40.2 | 14.3 | 12.6 | 27.7 | 25.0 | 22.0 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 40.2 | 14.3 | 12.6 | 27.7 | 25.0 | 22.0 |
| LOS | D | B | B | C | C | C |
| Approach Delay (s/veh) | 40.2 | | 12.7 | | 25.0 | |
| Approach LOS | D | | B | | C | |

| Intersection Summary | |
|--|------------------------|
| Cycle Length: 110 | |
| Actuated Cycle Length: 110 | |
| Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green | |
| Natural Cycle: 85 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.75 | |
| Intersection Signal Delay (s/veh): 26.0 | Intersection LOS: C |
| Intersection Capacity Utilization 67.0% | ICU Level of Service C |
| Analysis Period (min) 15 | |

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/17/2026

| | ↖ | → | ↘ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 | | |
| Future Volume (veh/h) | 0 | 875 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 875 | 0 | 40 | 744 | 0 | | | | 552 | 1419 | 501 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1289 | | 304 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 875 | 0 | 40 | 744 | 0 | | | | 552 | 1419 | 501 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 17.1 | 0.0 | 1.6 | 11.4 | 0.0 | | | | 25.0 | 34.0 | 25.7 | | |
| Cycle Q Clear(g_c), s | 0.0 | 17.1 | 0.0 | 1.6 | 11.4 | 0.0 | | | | 25.0 | 34.0 | 25.7 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 304 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| V/C Ratio(X) | 0.00 | 0.68 | | 0.13 | 0.37 | 0.00 | | | | 0.62 | 0.76 | 0.64 | | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 304 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.93 | 0.93 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 36.9 | 0.0 | 23.9 | 23.2 | 0.0 | | | | 20.0 | 22.3 | 20.2 | | |
| Incr Delay (d2), s/veh | 0.0 | 2.9 | 0.0 | 0.8 | 0.5 | 0.0 | | | | 3.3 | 3.1 | 3.9 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.7 | 0.0 | 1.3 | 7.9 | 0.0 | | | | 15.9 | 21.1 | 14.9 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 39.8 | 0.0 | 24.7 | 23.7 | 0.0 | | | | 23.3 | 25.3 | 24.1 | | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | | |
| Approach Vol, veh/h | | 875 | | | 784 | | | | | | | 2472 | | |
| Approach Delay, s/veh | | 39.8 | | | 23.7 | | | | | | | 24.6 | | |
| Approach LOS | | D | | | C | | | | | | | C | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 16.0 | 33.0 | 61.0 | 49.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 11.0 | 28.0 | 55.0 | 44.0 |
| Max Q Clear Time (g_c+1), s | 3.6 | 19.1 | 36.0 | 13.4 |
| Green Ext Time (p_c), s | 0.0 | 3.8 | 13.8 | 5.6 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 27.7 |
| HCM 7th LOS | C |

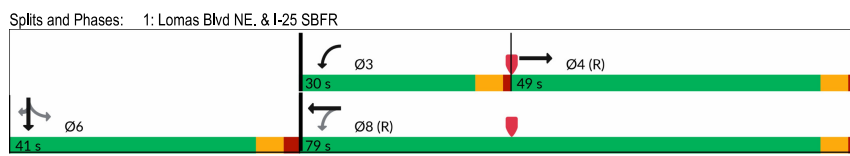
Notes
User approved volume balancing among the lanes for turning movement.
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/17/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↑↑↑ | ↖ |
| Traffic Volume (vph) | 864 | 320 | 916 | 196 | 428 | 236 |
| Future Volume (vph) | 864 | 320 | 916 | 196 | 428 | 236 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 44.0 | 74.0 | 74.0 | 35.0 | 35.0 | 35.0 |
| Actuated g/C Ratio | 0.37 | 0.62 | 0.62 | 0.29 | 0.29 | 0.29 |
| v/c Ratio | 0.47 | 0.61 | 0.30 | 0.38 | 0.52 | 0.39 |
| Control Delay (s/veh) | 30.1 | 49.0 | 1.7 | 36.8 | 37.6 | 11.9 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 30.1 | 49.0 | 1.7 | 36.8 | 37.6 | 11.9 |
| LOS | C | D | A | D | D | B |
| Approach Delay (s/veh) | 30.1 | | 13.9 | | 31.3 | |
| Approach LOS | C | | B | | C | |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green
 Natural Cycle: 55
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.61
 Intersection Signal Delay (s/veh): 23.7 Intersection LOS: C
 Intersection Capacity Utilization 77.6% ICU Level of Service D
 Analysis Period (min) 15



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/17/2026

| | ↖ | → | ↘ | ↖ | ← | ↙ | ↖ | ↘ | ↓ | ↙ | ↘ | |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ |
| Traffic Volume (veh/h) | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| Future Volume (veh/h) | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | No | | | | | | No | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 0 | 864 | 0 | 320 | 916 | 0 | | | | 196 | 546 | 157 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 |
| Cap, veh/h | 0 | 1857 | | 578 | 3124 | 0 | | | | 515 | 1082 | 459 |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 |
| Grp Volume(v), veh/h | 0 | 864 | 0 | 320 | 916 | 0 | | | | 196 | 546 | 157 |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 |
| Q Serve(g_s), s | 0.0 | 15.6 | 0.0 | 10.3 | 0.0 | 0.0 | | | | 10.6 | 14.7 | 9.4 |
| Cycle Q Clear(g_c), s | 0.0 | 15.6 | 0.0 | 10.3 | 0.0 | 0.0 | | | | 10.6 | 14.7 | 9.4 |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 578 | 3124 | 0 | | | | 515 | 1082 | 459 |
| V/C Ratio(X) | 0.00 | 0.47 | | 0.55 | 0.29 | 0.00 | | | | 0.38 | 0.50 | 0.34 |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 578 | 3124 | 0 | | | | 515 | 1082 | 459 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.70 | 0.70 | 0.00 | | | | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 0.0 | 29.0 | 0.0 | 10.4 | 0.0 | 0.0 | | | | 33.9 | 35.3 | 33.4 |
| Incr Delay (d2), s/veh | 0.0 | 0.8 | 0.0 | 2.7 | 0.2 | 0.0 | | | | 2.1 | 1.7 | 2.0 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 0.0 | 10.5 | 0.0 | 5.7 | 0.1 | 0.0 | | | | 8.4 | 11.1 | 6.9 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 29.9 | 0.0 | 13.1 | 0.2 | 0.0 | | | | 36.0 | 37.0 | 35.5 |
| LnGrp LOS | | C | | B | A | | | | | D | D | D |
| Approach Vol, veh/h | | 864 | | | 1236 | | | | | | 899 | |
| Approach Delay, s/veh | | 29.9 | | | 3.5 | | | | | | 36.5 | |
| Approach LOS | | C | | | A | | | | | | D | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 30.0 | 49.0 | 41.0 | 79.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 25.0 | 44.0 | 35.0 | 74.0 |
| Max Q Clear Time (g_c+1), s | 12.3 | 17.6 | 16.7 | 2.0 |
| Green Ext Time (p_c), s | 0.8 | 6.4 | 4.5 | 7.8 |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.0
 HCM 7th LOS: C

Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

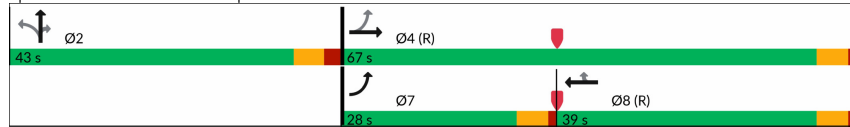
Tierra West, LLC
01/17/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 216 | 1488 | 636 | 200 | 144 | 560 | 164 |
| Future Volume (vph) | 216 | 1488 | 636 | 200 | 144 | 560 | 164 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 62.0 | 62.0 | 34.0 | 34.0 | 37.0 | 37.0 | 37.0 |
| Actuated g/C Ratio | 0.56 | 0.56 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.39 | 0.52 | 0.41 | 0.33 | 0.26 | 0.39 | 0.30 |
| Control Delay (s/veh) | 21.5 | 20.9 | 22.1 | 3.4 | 28.2 | 28.7 | 16.2 |
| Queue Delay | 0.0 | 0.3 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 21.5 | 21.2 | 22.1 | 3.4 | 28.2 | 28.7 | 16.2 |
| LOS | C | C | C | A | C | C | B |
| Approach Delay (s/veh) | | 21.2 | 17.6 | | | 26.5 | |
| Approach LOS | | C | B | | | C | |

Intersection Summary

Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.52
 Intersection Signal Delay (s/veh): 21.7
 Intersection LOS: C
 Intersection Capacity Utilization 67.0%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/17/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| Future Volume (veh/h) | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 561 | 164 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 595 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 561 | 164 | | | |
| Grp Sat Flow(s),veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.1 | 0.0 | 0.0 | 0.0 | 7.0 | 7.2 | 6.5 | 8.2 | 8.5 | | | |
| Cycle Q Clear(g_c), s | 6.1 | 0.0 | 0.0 | 0.0 | 7.0 | 7.2 | 6.5 | 8.2 | 8.5 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 595 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.36 | 0.52 | 0.00 | 0.00 | 0.41 | 0.41 | 0.24 | 0.30 | 0.31 | | | |
| Avail Cap(c_a), veh/h | 595 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.66 | 0.66 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.1 | 0.0 | 0.0 | 0.0 | 15.9 | 15.9 | 26.4 | 26.9 | 27.0 | | | |
| Incr Delay (d2), s/veh | 1.1 | 0.5 | 0.0 | 0.0 | 0.8 | 2.6 | 1.0 | 0.4 | 1.5 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 3.8 | 0.2 | 0.0 | 0.0 | 4.3 | 4.5 | 5.1 | 6.6 | 6.1 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.2 | 0.5 | 0.0 | 0.0 | 16.6 | 18.4 | 27.3 | 27.3 | 28.6 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1704 | | | 836 | | | 869 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.1 | | | 27.6 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | |
| Max Q Clear Time (g_c+1), s | | 10.5 | | | 2.0 | | | 8.1 | 9.2 | | | |
| Green Ext Time (p_c), s | | 5.0 | | | 16.4 | | | 0.5 | 5.2 | | | |

Intersection Summary

HCM 7th Control Delay, s/veh 12.1
 HCM 7th LOS B

Notes

User approved volume balancing among the lanes for turning movement.

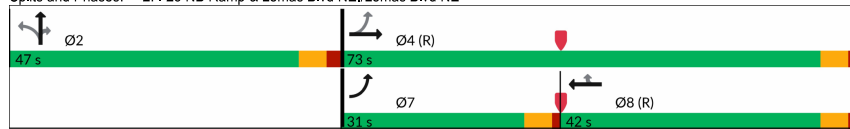
Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/17/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 328 | 712 | 1100 | 580 | 124 | 512 | 164 |
| Future Volume (vph) | 328 | 712 | 1100 | 580 | 124 | 512 | 164 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 8 | 2 | 2 | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 31.0 | 73.0 | 42.0 | 42.0 | 47.0 | 47.0 | 47.0 |
| Total Split (%) | 25.8% | 60.8% | 35.0% | 35.0% | 39.2% | 39.2% | 39.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 68.0 | 68.0 | 37.0 | 37.0 | 41.0 | 41.0 | 41.0 |
| Actuated g/C Ratio | 0.57 | 0.57 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.73 | 0.25 | 0.71 | 0.96 | 0.22 | 0.35 | 0.27 |
| Control Delay (s/veh) | 59.0 | 18.6 | 27.3 | 40.5 | 29.6 | 30.2 | 5.6 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 59.0 | 18.6 | 27.3 | 40.5 | 29.6 | 30.2 | 5.6 |
| LOS | E | B | C | D | C | C | A |
| Approach Delay (s/veh) | | 31.4 | 31.9 | | | 25.5 | |
| Approach LOS | | C | C | | | C | |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 75
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.96
 Intersection Signal Delay (s/veh): 30.3
 Intersection LOS: C
 Intersection Capacity Utilization 77.6%
 ICU Level of Service D
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/17/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|-------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 328 | 712 | 0 | 0 | 1100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| Future Volume (veh/h) | 328 | 712 | 0 | 0 | 1100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | No | No | No | No | No | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 328 | 712 | 0 | 0 | 1100 | 580 | 124 | 513 | 164 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 490 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| Arrive On Green | 0.43 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 328 | 712 | 0 | 0 | 1100 | 580 | 124 | 513 | 164 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 11.7 | 0.0 | 0.0 | 0.0 | 17.7 | 37.0 | 6.0 | 8.0 | 9.2 | | | |
| Cycle Q Clear(g_c), s | 11.7 | 0.0 | 0.0 | 0.0 | 17.7 | 37.0 | 6.0 | 8.0 | 9.2 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 490 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| V/C Ratio(X) | 0.67 | 0.25 | 0.00 | 0.00 | 0.70 | 1.20 | 0.21 | 0.27 | 0.31 | | | |
| Avail Cap(c_a), veh/h | 490 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.89 | 0.89 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 14.3 | 0.0 | 0.0 | 0.0 | 19.3 | 23.0 | 28.0 | 28.6 | 29.0 | | | |
| Incr Delay (d2), s/veh | 6.4 | 0.2 | 0.0 | 0.0 | 2.7 | 107.0 | 0.8 | 0.3 | 1.5 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 7.3 | 0.1 | 0.0 | 0.0 | 8.7 | 33.9 | 4.8 | 6.5 | 6.6 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 20.6 | 0.2 | 0.0 | 0.0 | 22.0 | 130.0 | 28.7 | 29.0 | 30.5 | | | |
| LnGrp LOS | C | A | | | C | F | C | C | C | | | |
| Approach Vol, veh/h | | 1040 | | | 1680 | | | 801 | | | | |
| Approach Delay, s/veh | | 6.6 | | | 59.3 | | | 29.3 | | | | |
| Approach LOS | | A | | | E | | | C | | | | |
| Timer - Assigned Phs | 2 | | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 47.0 | | | 73.0 | | | 31.0 | 42.0 | | | | |
| Change Period (Y+Rc), s | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | | |
| Max Green Setting (Gmax), s | 41.0 | | | 68.0 | | | 26.0 | 37.0 | | | | |
| Max Q Clear Time (g_c+1), s | 11.2 | | | 2.0 | | | 13.7 | 39.0 | | | | |
| Green Ext Time (p_c), s | 4.6 | | | 5.7 | | | 0.8 | 0.0 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 36.9
 HCM 7th LOS D

Notes
 User approved volume balancing among the lanes for turning movement.

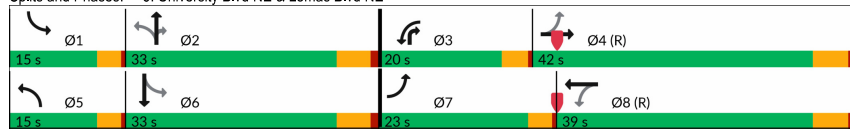
Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/17/2026

| | ↖ | → | ↗ | ← | ↖ | ↑ | ↗ | ↘ | ↓ |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
| Lane Configurations | ↖↖↖ | ↖↖↖ | ↖↖↖ | ↖↖↖ | ↖ | ↖↖ | ↖↖ | ↖ | ↖↖ |
| Traffic Volume (vph) | 272 | 860 | 180 | 584 | 96 | 464 | 180 | 112 | 748 |
| Future Volume (vph) | 272 | 860 | 180 | 584 | 96 | 464 | 180 | 112 | 748 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 6 | 2 | 6 | 2 | 6 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 23.0 | 42.0 | 20.0 | 39.0 | 15.0 | 33.0 | 20.0 | 15.0 | 33.0 |
| Total Split (%) | 20.9% | 38.2% | 18.2% | 35.5% | 13.6% | 30.0% | 18.2% | 13.6% | 30.0% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |
| Act Effct Green (s) | 57.3 | 40.4 | 51.7 | 38.1 | 40.5 | 29.4 | 47.0 | 41.5 | 29.9 |
| Actuated g/C Ratio | 0.52 | 0.37 | 0.47 | 0.35 | 0.37 | 0.27 | 0.43 | 0.38 | 0.27 |
| v/c Ratio | 0.65 | 0.72 | 0.69 | 0.45 | 0.44 | 0.50 | 0.24 | 0.32 | 0.85 |
| Control Delay (s/veh) | 23.6 | 32.7 | 35.0 | 26.4 | 27.5 | 36.6 | 4.6 | 23.7 | 48.3 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 23.6 | 32.7 | 35.0 | 26.4 | 27.5 | 36.6 | 4.6 | 23.7 | 48.3 |
| LOS | C | C | C | C | C | D | A | C | D |
| Approach Delay (s/veh) | | 31.1 | | 28.0 | | 27.6 | | | 45.3 |
| Approach LOS | | C | | C | | C | | | D |

| Intersection Summary | |
|---|----------------------------|
| Cycle Length: 110 | Actuated Cycle Length: 110 |
| Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green | |
| Natural Cycle: 95 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.85 | |
| Intersection Signal Delay (s/veh): 32.9 | Intersection LOS: C |
| Intersection Capacity Utilization 80.6% | ICU Level of Service D |
| Analysis Period (min) 15 | |

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/17/2026

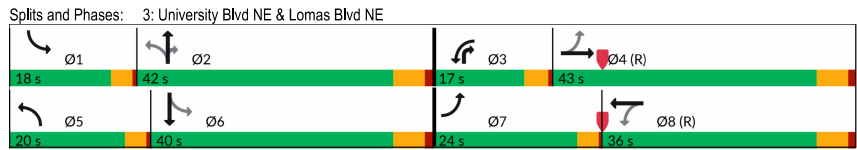
| | ↖ | → | ↗ | ← | ↖ | ↑ | ↗ | ↘ | ↓ | ↖ | | |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↖↖↖ | ↖↖↖ | ↖ | ↖↖↖ | ↖↖↖ | ↖ | ↖↖ | ↖↖ | ↖ | ↖↖ | ↖↖ | ↖ |
| Traffic Volume (veh/h) | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| Future Volume (veh/h) | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 464 | 1489 | 693 | 270 | 1588 | 510 | 191 | 881 | 515 | 284 | 852 | 68 |
| Arrive On Green | 0.03 | 0.15 | 0.15 | 0.08 | 0.42 | 0.42 | 0.06 | 0.25 | 0.25 | 0.06 | 0.26 | 0.26 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3795 | 1219 | 1767 | 3526 | 1572 | 1767 | 3306 | 265 |
| Grp Volume(v), veh/h | 272 | 860 | 460 | 180 | 518 | 258 | 96 | 464 | 180 | 112 | 399 | 409 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1636 | 1767 | 1763 | 1572 | 1767 | 1763 | 1808 |
| Q Serve(g_s), s | 9.0 | 26.1 | 30.4 | 6.3 | 11.6 | 12.0 | 4.4 | 12.5 | 9.6 | 5.1 | 23.9 | 23.9 |
| Cycle Q Clear(g_c), s | 9.0 | 26.1 | 30.4 | 6.3 | 11.6 | 12.0 | 4.4 | 12.5 | 9.6 | 5.1 | 23.9 | 23.9 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.74 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.15 |
| Lane Grp Cap(c), veh/h | 464 | 1489 | 693 | 270 | 1413 | 685 | 191 | 881 | 515 | 284 | 454 | 466 |
| V/C Ratio(X) | 0.59 | 0.58 | 0.66 | 0.67 | 0.37 | 0.38 | 0.50 | 0.53 | 0.35 | 0.39 | 0.88 | 0.88 |
| Avail Cap(c_a), veh/h | 592 | 1489 | 693 | 390 | 1413 | 685 | 277 | 881 | 515 | 357 | 454 | 466 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 16.7 | 37.4 | 39.3 | 21.8 | 22.0 | 22.1 | 30.9 | 35.6 | 28.1 | 28.5 | 39.2 | 39.2 |
| Incr Delay (d2), s/veh | 1.2 | 1.6 | 5.0 | 2.8 | 0.7 | 1.6 | 2.0 | 2.2 | 1.9 | 0.9 | 20.7 | 20.4 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 7.3 | 17.8 | 19.8 | 4.8 | 8.2 | 8.4 | 3.5 | 9.4 | 6.9 | 4.0 | 18.5 | 18.9 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 17.9 | 39.1 | 44.2 | 24.6 | 22.7 | 23.7 | 32.9 | 37.9 | 30.0 | 29.4 | 59.9 | 59.5 |
| LnGrp LOS | B | D | D | C | C | C | C | D | C | C | E | E |
| Approach Vol, veh/h | | 1592 | | | 956 | | | 740 | | | 920 | |
| Approach Delay, s/veh | | 36.9 | | | 23.3 | | | 35.3 | | | 56.0 | |
| Approach LOS | | D | | | C | | | D | | | E | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 10.5 | 33.0 | 12.5 | 54.0 | 9.6 | 33.8 | 15.0 | 51.5 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 11.5 | 27.5 | 16.0 | 36.5 | 11.5 | 27.5 | 19.5 | 33.5 | | | | |
| Max Q Clear Time (g_c+1), s | 7.1 | 14.5 | 8.3 | 32.4 | 6.4 | 25.9 | 11.0 | 14.0 | | | | |
| Green Ext Time (p_c), s | 0.1 | 3.0 | 0.3 | 2.8 | 0.1 | 0.8 | 0.5 | 4.9 | | | | |

| Intersection Summary | |
|--|------|
| HCM 7th Control Delay, s/veh | 37.7 |
| HCM 7th LOS | D |
| Notes | |
| User approved pedestrian interval to be less than phase max green. | |

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
01/17/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | | ↘ | |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|---|----|---|---|----|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT | | | | | |
| Lane Configurations | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖ | ↖↖ |
| Traffic Volume (vph) | 172 | 492 | 212 | 1016 | 236 | 604 | 264 | 152 | 604 | | | | | |
| Future Volume (vph) | 172 | 492 | 212 | 1016 | 236 | 604 | 264 | 152 | 604 | | | | | |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA | | | | | |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | | | |
| Permitted Phases | 4 | 8 | | 2 | | 2 | 6 | | | | | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | | | |
| Switch Phase | | | | | | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 | | | | | |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 | | | | | |
| Total Split (s) | 24.0 | 43.0 | 17.0 | 36.0 | 20.0 | 42.0 | 17.0 | 18.0 | 40.0 | | | | | |
| Total Split (%) | 20.0% | 35.8% | 14.2% | 30.0% | 16.7% | 35.0% | 14.2% | 15.0% | 33.3% | | | | | |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 | | | | | |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 | | | | | |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | | |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 | | | | | |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lead | Lag | | | | | |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | | | |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max | | | | | |
| Act Effect Green (s) | 52.9 | 38.3 | 51.8 | 38.1 | 56.2 | 39.6 | 57.3 | 49.4 | 36.1 | | | | | |
| Actuated g/C Ratio | 0.44 | 0.32 | 0.43 | 0.32 | 0.47 | 0.33 | 0.48 | 0.41 | 0.30 | | | | | |
| v/c Ratio | 0.66 | 0.42 | 0.57 | 0.71 | 0.84 | 0.52 | 0.30 | 0.43 | 0.91 | | | | | |
| Control Delay (s/veh) | 40.5 | 27.9 | 26.6 | 39.2 | 53.3 | 35.0 | 4.5 | 22.6 | 49.1 | | | | | |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | | |
| Total Delay (s/veh) | 40.5 | 27.9 | 26.6 | 39.2 | 53.3 | 35.0 | 4.5 | 22.6 | 49.1 | | | | | |
| LOS | D | C | C | D | D | C | A | C | D | | | | | |
| Approach Delay (s/veh) | 30.5 | | 37.2 | | 31.6 | | 45.5 | | | | | | | |
| Approach LOS | C | | D | | C | | D | | | | | | | |

| Intersection Summary | |
|---|------------------------|
| Cycle Length: 120 | |
| Actuated Cycle Length: 120 | |
| Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green | |
| Natural Cycle: 110 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.91 | |
| Intersection Signal Delay (s/veh): 36.6 | Intersection LOS: D |
| Intersection Capacity Utilization 88.9% | ICU Level of Service E |
| Analysis Period (min) 15 | |



HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
01/17/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | | ↘ | |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|----|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | ↖ | ↖↖ | ↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ | ↖↖ | ↖ |
| Traffic Volume (veh/h) | 172 | 492 | 176 | 212 | 1016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 | | |
| Future Volume (veh/h) | 172 | 492 | 176 | 212 | 1016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | No | | | No | | | No | | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 172 | 492 | 176 | 212 | 1016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | | |
| Cap, veh/h | 295 | 1303 | 451 | 395 | 1705 | 188 | 264 | 1145 | 657 | 314 | 608 | 366 | | |
| Arrive On Green | 0.03 | 0.12 | 0.12 | 0.09 | 0.37 | 0.37 | 0.11 | 0.32 | 0.32 | 0.08 | 0.29 | 0.29 | | |
| Sat Flow, veh/h | 1767 | 3716 | 1285 | 1767 | 4631 | 510 | 1767 | 3526 | 1572 | 1767 | 2115 | 1274 | | |
| Grp Volume(v), veh/h | 172 | 446 | 222 | 212 | 740 | 388 | 236 | 604 | 264 | 152 | 504 | 464 | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1624 | 1767 | 1689 | 1764 | 1767 | 1763 | 1572 | 1767 | 1763 | 1626 | | |
| Q Serve(g_s), s | 7.3 | 14.6 | 15.2 | 9.1 | 21.3 | 21.4 | 11.6 | 16.8 | 14.1 | 7.2 | 34.2 | 34.2 | | |
| Cycle Q Clear(g_c), s | 7.3 | 14.6 | 15.2 | 9.1 | 21.3 | 21.4 | 11.6 | 16.8 | 14.1 | 7.2 | 34.2 | 34.2 | | |
| Prop In Lane | 1.00 | | 0.79 | 1.00 | | 0.29 | 1.00 | | 1.00 | 1.00 | | 0.78 | | |
| Lane Grp Cap(c), veh/h | 295 | 1184 | 569 | 395 | 1244 | 649 | 264 | 1145 | 657 | 314 | 507 | 468 | | |
| V/C Ratio(X) | 0.58 | 0.38 | 0.39 | 0.54 | 0.60 | 0.60 | 0.89 | 0.53 | 0.40 | 0.48 | 0.99 | 0.99 | | |
| Avail Cap(c_a), veh/h | 456 | 1184 | 569 | 422 | 1244 | 649 | 304 | 1145 | 657 | 391 | 507 | 468 | | |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 25.4 | 40.9 | 41.2 | 22.4 | 30.7 | 30.7 | 32.5 | 33.0 | 24.4 | 27.3 | 42.6 | 42.6 | | |
| Incr Delay (d2), s/veh | 1.8 | 0.9 | 2.0 | 1.1 | 2.1 | 4.0 | 24.6 | 1.7 | 1.8 | 1.2 | 38.4 | 40.1 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 6.1 | 11.1 | 11.3 | 6.9 | 13.8 | 14.7 | 10.8 | 11.8 | 9.3 | 5.6 | 27.3 | 25.7 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 27.2 | 41.8 | 43.2 | 23.5 | 32.8 | 34.7 | 57.1 | 34.8 | 26.3 | 28.5 | 81.1 | 82.7 | | |
| LnGrp LOS | C | D | D | C | C | C | E | C | C | C | F | F | | |
| Approach Vol, veh/h | 840 | | 1340 | | 1104 | | 1120 | | | | | | | |
| Approach Delay, s/veh | 39.2 | | 31.9 | | 37.5 | | 74.6 | | | | | | | |
| Approach LOS | D | | C | | D | | E | | | | | | | |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|------------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 12.8 | 44.5 | 15.2 | 47.6 | 17.2 | 40.0 | 13.1 | 49.7 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 14.5 | 36.5 | 13.0 | 37.5 | 16.5 | 34.5 | 20.5 | 30.5 |
| Max Q Clear Time (g_c+11), s | 9.2 | 18.8 | 11.1 | 17.2 | 13.6 | 36.2 | 9.3 | 23.4 |
| Green Ext Time (p_c), s | 0.2 | 4.7 | 0.1 | 4.2 | 0.2 | 0.0 | 0.3 | 4.0 |

| Intersection Summary | |
|--|------|
| HCM 7th Control Delay, s/veh | 45.5 |
| HCM 7th LOS | D |
| Notes | |
| User approved pedestrian interval to be less than phase max green. | |

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.8 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1450 | 252 | 72 | 750 | 0 | 68 |
| Future Vol, veh/h | 1450 | 252 | 72 | 750 | 0 | 68 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1450 | 252 | 72 | 750 | 0 | 68 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1702 | 0 | 851 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 302 | - | *739 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 302 | - | *739 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 1.81 | 10.36 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 739 | - | - | 302 | - |
| HCM Lane V/C Ratio | 0.092 | - | - | 0.238 | - |
| HCM Ctrl Dly (s/v) | 10.4 | - | - | 20.6 | - |
| HCM Lane LOS | B | - | - | C | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 0.9 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 748 | 68 | 28 | 1664 | 0 | 96 |
| Future Vol, veh/h | 748 | 68 | 28 | 1664 | 0 | 96 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 748 | 68 | 28 | 1664 | 0 | 96 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 816 | 0 | 408 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 647 | 0 | 832 |
| Stage 1 | - | - | - | 0 | - |
| Stage 2 | - | - | - | 0 | - |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 647 | - | 832 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|------|
| HCM Ctrl Dly, s/v | 0 | 0.18 | 9.89 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 832 | - | - | 647 | - |
| HCM Lane V/C Ratio | 0.115 | - | - | 0.043 | - |
| HCM Ctrl Dly (s/v) | 9.9 | - | - | 10.8 | - |
| HCM Lane LOS | A | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.1 | - |

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1560 | 0 | 4 | 772 | 4 | 0 |
| Future Vol, veh/h | 1560 | 0 | 4 | 772 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1560 | 0 | 4 | 772 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1560 | 0 | 1877 |
| Stage 1 | - | - | - | - | 1560 |
| Stage 2 | - | - | - | - | 317 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 503 | - | *480 |
| Stage 1 | - | - | - | - | *361 |
| Stage 2 | - | - | - | - | *820 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 503 | - | *476 |
| Mov Cap-2 Maneuver | - | - | - | - | *476 |
| Stage 1 | - | - | - | - | *361 |
| Stage 2 | - | - | - | - | *813 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 12.62 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 476 | - | - | 503 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.008 | - |
| HCM Ctrl Dly (s/v) | 12.6 | - | - | 12.2 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 888 | 0 | 4 | 1688 | 4 | 0 |
| Future Vol, veh/h | 888 | 0 | 4 | 1688 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 888 | 0 | 4 | 1688 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 888 | 0 | 1571 |
| Stage 1 | - | - | - | - | 888 |
| Stage 2 | - | - | - | - | 683 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 747 | - | *534 |
| Stage 1 | - | - | - | - | *570 |
| Stage 2 | - | - | - | - | *663 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 747 | - | *531 |
| Mov Cap-2 Maneuver | - | - | - | - | *531 |
| Stage 1 | - | - | - | - | *570 |
| Stage 2 | - | - | - | - | *659 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 11.83 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 531 | - | - | 747 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.005 | - |
| HCM Ctrl Dly (s/v) | 11.8 | - | - | 9.8 | - |
| HCM Lane LOS | B | - | - | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.2 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Future Vol, veh/h | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 764 | 0 | 0 | 1540 | 0 | 0 | 1884 | 2344 | 770 | 1418 | 2342 | 382 |
| Stage 1 | - | - | - | - | - | - | 1580 | 1580 | - | 762 | 762 | - |
| Stage 2 | - | - | - | - | - | - | 304 | 764 | - | 656 | 1580 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 797 | - | - | 518 | - | - | *459 | 160 | *670 | *567 | 160 | *799 |
| Stage 1 | - | - | - | - | - | - | *315 | 391 | - | *591 | 629 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 628 | - | *688 | 391 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 797 | - | - | 518 | - | - | *441 | 156 | *670 | *553 | 156 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *441 | 156 | - | *553 | 156 | - |
| Stage 1 | - | - | - | - | - | - | *307 | 381 | - | *591 | 629 | - |
| Stage 2 | - | - | - | - | - | - | *808 | 628 | - | *670 | 381 | - |

| Approach | EB | WB | NB | SB |
|-------------------|------|----|----|-------|
| HCM Ctrl Dly, s/v | 0.12 | 0 | 0 | 10.47 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 797 | - | - | 518 | - | - | 678 |
| HCM Lane V/C Ratio | - | 0.025 | - | - | - | - | - | 0.029 |
| HCM Ctrl Dly (s/v) | 0 | 9.6 | - | - | 0 | - | - | 10.5 |
| HCM Lane LOS | | A | A | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0.1 | - | - | 0 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 12 | 872 | 4 | 0 | 1652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| Future Vol, veh/h | 12 | 872 | 4 | 0 | 1652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 12 | 872 | 4 | 0 | 1652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1656 | 0 | 0 | 876 | 0 | 0 | 1559 | 2554 | 438 | 2027 | 2554 | 828 |
| Stage 1 | - | - | - | - | - | - | 898 | 898 | - | 1654 | 1654 | - |
| Stage 2 | - | - | - | - | - | - | 661 | 1656 | - | 373 | 900 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 456 | - | - | 758 | - | - | *538 | 120 | *779 | *429 | 120 | *661 |
| Stage 1 | - | - | - | - | - | - | *533 | 583 | - | *288 | 367 | - |
| Stage 2 | - | - | - | - | - | - | *678 | 366 | - | *800 | 581 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 456 | - | - | 758 | - | - | *492 | 117 | *779 | *414 | 117 | *661 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *492 | 117 | - | *414 | 117 | - |
| Stage 1 | - | - | - | - | - | - | *519 | 567 | - | *288 | 367 | - |
| Stage 2 | - | - | - | - | - | - | *637 | 366 | - | *771 | 566 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|----|--|--|------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.18 | | | 0 | | | 9.67 | | | 11.18 | | |
| HCM LOS | | | | | | | A | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | 779 | 456 | - | - | 758 | - | - | 627 |
| HCM Lane V/C Ratio | 0.01 | 0.026 | - | - | - | - | - | 0.07 |
| HCM Ctrl Dly (s/v) | 9.7 | 13.1 | - | - | 0 | - | - | 11.2 |
| HCM Lane LOS | A | B | - | - | A | - | - | B |
| HCM 95th %tile Q(veh) | 0 | 0.1 | - | - | 0 | - | - | 0.2 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1550 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 752 | 0 | 0 | 1550 | 0 | 0 | 1851 | 2302 | 775 | 1372 | 2302 | 376 |
| Stage 1 | - | - | - | - | - | - | 1550 | 1550 | - | 752 | 752 | - |
| Stage 2 | - | - | - | - | - | - | 301 | 752 | - | 620 | 1550 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 809 | - | - | 511 | - | - | *492 | 175 | *670 | *567 | 175 | *799 |
| Stage 1 | - | - | - | - | - | - | *336 | 408 | - | *602 | 636 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 636 | - | *688 | 408 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 809 | - | - | 511 | - | - | *490 | 175 | *670 | *567 | 175 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *490 | 175 | - | *567 | 175 | - |
| Stage 1 | - | - | - | - | - | - | *336 | 408 | - | *602 | 636 | - |
| Stage 2 | - | - | - | - | - | - | *816 | 636 | - | *688 | 408 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 9.53 |
| HCM LOS | | | A | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 809 | - | - | 511 | - | - | 799 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 9.5 |
| HCM Lane LOS | A | A | - | - | A | - | - | A |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 900 | 0 | 0 | 1648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Future Vol, veh/h | 0 | 900 | 0 | 0 | 1648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 900 | 0 | 0 | 1648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1648 | 0 | 0 | 900 | 0 | 0 | 1559 | 2548 | 450 | 2008 | 2548 | 824 |
| Stage 1 | - | - | - | - | - | - | 900 | 900 | - | 1648 | 1648 | - |
| Stage 2 | - | - | - | - | - | - | 659 | 1648 | - | 360 | 900 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 462 | - | - | 736 | - | - | *538 | 122 | *779 | *448 | 122 | *661 |
| Stage 1 | - | - | - | - | - | - | *531 | 581 | - | *292 | 370 | - |
| Stage 2 | - | - | - | - | - | - | *678 | 370 | - | *800 | 581 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 462 | - | - | 736 | - | - | *528 | 122 | *779 | *448 | 122 | *661 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *528 | 122 | - | *448 | 122 | - |
| Stage 1 | - | - | - | - | - | - | *531 | 581 | - | *292 | 370 | - |
| Stage 2 | - | - | - | - | - | - | *666 | 370 | - | *800 | 581 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|-------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 10.55 |
| HCM LOS | | | A | B |

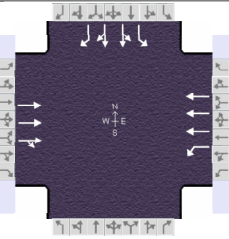
| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 462 | - | - | 736 | - | - | 661 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.018 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 10.5 |
| HCM Lane LOS | A | A | - | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0.1 |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

HCS Signalized Intersection Results Summary

A - 92

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM EXISTING | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2025 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2025_ExportToHCS_AE.xus | | | | |
| Project Description | 2025 AM Existing Volume - Existing Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 875 | 0 | 40 | 744 | | | | | 828 | 656 | 752 |

| Signal Information | | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|--------|-----|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | Green | 6.0 | 39.0 | 49.0 | 0.0 | 0.0 | 0.0 | | | | |
| Offset, s | 58 | Reference Point | Begin | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 44.0 | 11.0 | 55.0 | | | | 55.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g _s), s | | | 3.2 | | | | | 51.0 |
| Green Extension Time (g _e), s | | 0.0 | 0.0 | 0.0 | | | | 0.0 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 1.00 |

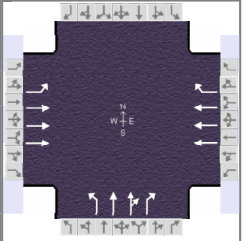
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 875 | 0 | 32 | 604 | | | | | 828 | 656 | 752 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 13.2 | 0.0 | 1.2 | 4.9 | | | | | 49.0 | 13.1 | 49.0 |
| Cycle Queue Clearance Time (g _c), s | | 13.2 | 0.0 | 1.2 | 4.9 | | | | | 49.0 | 13.1 | 49.0 |
| Green Ratio (g/C) | | 0.35 | | 0.43 | 0.45 | | | | | 0.45 | 0.45 | 0.45 |
| Capacity (c), veh/h | | 1974 | | 309 | 2297 | | | | | 787 | 1653 | 700 |
| Volume-to-Capacity Ratio (X) | | 0.443 | 0.000 | 0.105 | 0.263 | | | | | 1.052 | 0.397 | 1.074 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 252 | 0 | 26 | 78 | | | | | 1032 | 245 | 998 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 9.9 | 0.0 | 1.0 | 3.1 | | | | | 40.3 | 9.6 | 39.0 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.07 | 0.00 | | | | | 5.16 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 27.2 | | 20.6 | 9.9 | | | | | 30.5 | 20.5 | 30.5 |
| Incremental Delay (d ₂), s/veh | | 0.7 | 0.0 | 0.6 | 0.3 | | | | | 46.6 | 0.7 | 55.5 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 27.9 | | 21.3 | 10.2 | | | | | 77.1 | 21.3 | 86.0 |
| Level of Service (LOS) | | C | | C | B | | | | | F | C | F |
| Approach Delay, s/veh / LOS | 27.9 | C | | 10.8 | B | | 0.0 | | | 63.7 | E | |
| Intersection Delay, s/veh / LOS | | | 46.4 | | | | | | D | | | |

| Multimodal Results | EB | | WB | | NB | | SB | |
|----------------------------|------|---|------|---|------|---|------|---|
| Pedestrian LOS Score / LOS | 1.70 | B | 2.10 | B | 2.61 | C | 2.47 | B |
| Bicycle LOS Score / LOS | 0.97 | A | 0.92 | A | | | 2.33 | B |

HCS Signalized Intersection Results Summary

A - 93

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM EXISTING | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2025 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2025_ExportToHCS_AE.xus | | | | |
| Project Description | 2025 AM Existing Volume - Existing Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|------|---|----|-----|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 216 | 1488 | | | 636 | 200 | 144 | 560 | 164 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 45 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 18.0 | 45.0 | 31.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 23.0 | 73.0 | | 50.0 | | 37.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.3 | | | | | 16.0 | | |
| Green Extension Time (g _e), s | 0.5 | 0.0 | | 0.0 | | 3.2 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.02 | | | | | 0.07 | | |

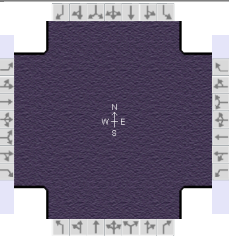
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 211 | 1451 | | | 636 | 200 | 144 | 560 | 164 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.3 | 23.2 | | | 9.4 | 9.5 | 7.0 | 14.0 | 9.2 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.3 | 23.2 | | | 9.4 | 9.5 | 7.0 | 14.0 | 9.2 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.41 | 0.41 | 0.28 | 0.28 | 0.28 | | | |
| Capacity (c), veh/h | 609 | 3125 | | | 2068 | 643 | 498 | 1046 | 443 | | | |
| Volume-to-Capacity Ratio (X) | 0.346 | 0.465 | | | 0.308 | 0.311 | 0.289 | 0.535 | 0.370 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 121 | 351 | | | 170 | 168 | 145 | 273 | 172 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 4.7 | 13.7 | | | 6.7 | 6.6 | 5.7 | 10.7 | 6.7 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.32 | 0.00 | | | 0.00 | 1.98 | 0.51 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 12.6 | 19.5 | | | 22.0 | 22.0 | 30.9 | 33.4 | 31.7 | | | |
| Incremental Delay (d ₂), s/veh | 0.9 | 0.3 | | | 0.4 | 1.3 | 1.5 | 2.0 | 2.4 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 13.5 | 19.8 | | | 22.4 | 23.3 | 32.3 | 35.4 | 34.0 | | | |
| Level of Service (LOS) | B | B | | | C | C | C | D | C | | | |
| Approach Delay, s/veh / LOS | 19.0 | B | | 22.6 | C | | 34.6 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 23.9 | | | | | | C | | | | | |

| Multimodal Results | EB | | WB | | NB | | SB | |
|----------------------------|------|---|------|---|------|---|------|---|
| Pedestrian LOS Score / LOS | 2.07 | B | 1.69 | B | 2.61 | C | 2.61 | C |
| Bicycle LOS Score / LOS | 1.42 | A | 0.95 | A | 1.20 | A | | |

HCS Signalized Intersection Results Summary

A - 94

| General Information | | | | Intersection Information | | | |
|---------------------|-------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM EXISTING | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2025 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2025_ExportToHCS_PE.xus | | | | |
| Project Description | 2028 EXISTING VOLUME - PM PEAK HOUR | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|-----|-----|---|----|---|---|-----|-----|-----|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Demand (v), veh/h | | 864 | 0 | 320 | 916 | | | | | 196 | 428 | 236 |

| Signal Information | | | | Signal Timing (s) | | | | | | | | Signal Phases | | | | | | |
|--------------------|-------|-----------------|-------|-------------------|------|------|------|-----|-----|-----|-----|---------------|-----|-----|-----|-----|-----|-----|
| Cycle, s | 120.0 | Reference Phase | 2 | Green | 20.0 | 55.0 | 29.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Offset, s | 64 | Reference Point | Begin | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Uncoordinated | No | Simult. Gap E/W | On | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 60.0 | 25.0 | 85.0 | | | | 35.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.1 |
| Queue Clearance Time (g _s), s | | | 10.3 | | | | | 18.1 |
| Green Extension Time (g _e), s | | 0.0 | 0.7 | 0.0 | | | | 2.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 0.04 | | | | | 0.19 |

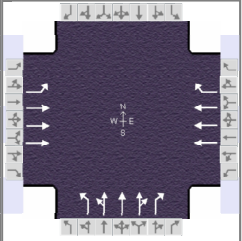
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 864 | 0 | 285 | 815 | | | | | 196 | 428 | 236 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 11.9 | 0.0 | 8.3 | 2.1 | | | | | 11.4 | 11.9 | 16.1 |
| Cycle Queue Clearance Time (g _c), s | | 11.9 | 0.0 | 8.3 | 2.1 | | | | | 11.4 | 11.9 | 16.1 |
| Green Ratio (g/C) | | 0.46 | | 0.64 | 0.67 | | | | | 0.24 | 0.24 | 0.24 |
| Capacity (c), veh/h | | 2551 | | 582 | 3370 | | | | | 427 | 897 | 380 |
| Volume-to-Capacity Ratio (X) | | 0.339 | 0.000 | 0.489 | 0.242 | | | | | 0.459 | 0.477 | 0.621 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 228 | 0 | 168 | 28 | | | | | 232 | 242 | 288 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 8.9 | 0.0 | 6.5 | 1.1 | | | | | 9.1 | 9.5 | 11.3 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.44 | 0.00 | | | | | 1.16 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 20.8 | | 13.1 | 1.8 | | | | | 38.8 | 39.0 | 40.6 |
| Incremental Delay (d ₂), s/veh | | 0.4 | 0.0 | 1.8 | 0.1 | | | | | 3.5 | 1.8 | 7.4 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 21.2 | | 14.9 | 1.9 | | | | | 42.3 | 40.8 | 48.0 |
| Level of Service (LOS) | | C | | B | A | | | | | D | D | D |
| Approach Delay, s/veh / LOS | 21.2 | C | | 5.2 | A | | 0.0 | | | 43.1 | | D |
| Intersection Delay, s/veh / LOS | | | 21.7 | | | | | | C | | | |

| Multimodal Results | EB | | WB | | NB | | SB | |
|----------------------------|------|---|------|---|------|---|------|---|
| Pedestrian LOS Score / LOS | 1.69 | B | 2.06 | B | 2.62 | C | 2.47 | B |
| Bicycle LOS Score / LOS | 0.96 | A | 1.17 | A | | | 1.20 | A |

HCS Signalized Intersection Results Summary

A - 95

| General Information | | | | Intersection Information | | | |
|---------------------|-------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM EXISTING | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2025 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2025_ExportToHCS_PE.xus | | | | |
| Project Description | 2028 EXISTING VOLUME - PM PEAK HOUR | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 328 | 712 | | | 1100 | 580 | 124 | 512 | 164 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|------|-----|-----|-----|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 49 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | 21.0 | 48.0 | 35.0 | 0.0 | 0.0 | 0.0 | | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 26.0 | 79.0 | | 53.0 | | 41.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 13.2 | | | | | 11.9 | | |
| Green Extension Time (g _e), s | 0.8 | 0.0 | | 0.0 | | 3.4 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.15 | | | | | 0.01 | | |

| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 334 | 726 | | | 1100 | 580 | 124 | 512 | 164 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 11.2 | 10.0 | | | 20.0 | 42.1 | 6.4 | 8.6 | 9.9 | | | |
| Cycle Queue Clearance Time (g _c), s | 11.2 | 10.0 | | | 20.0 | 42.1 | 6.4 | 8.6 | 9.9 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.40 | 0.40 | 0.29 | 0.29 | 0.29 | | | |
| Capacity (c), veh/h | 488 | 3117 | | | 2022 | 629 | 515 | 1624 | 459 | | | |
| Volume-to-Capacity Ratio (X) | 0.685 | 0.233 | | | 0.544 | 0.922 | 0.241 | 0.315 | 0.358 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 269 | 182 | | | 327 | 676 | 133 | 181 | 186 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 10.5 | 7.1 | | | 12.8 | 26.4 | 5.2 | 7.1 | 7.2 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.70 | 0.00 | | | 0.00 | 7.95 | 0.47 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 22.6 | 14.5 | | | 27.6 | 34.2 | 32.4 | 33.2 | 33.6 | | | |
| Incremental Delay (d ₂), s/veh | 7.2 | 0.2 | | | 1.1 | 21.1 | 1.1 | 0.5 | 2.2 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 29.8 | 14.7 | | | 28.7 | 55.4 | 33.5 | 33.7 | 35.8 | | | |
| Level of Service (LOS) | C | B | | | C | E | C | C | D | | | |
| Approach Delay, s/veh / LOS | 19.4 | B | | 37.9 | D | | 34.1 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 31.5 | | | | | | C | | | | | |

| Multimodal Results | EB | | WB | | NB | | SB | |
|----------------------------|------|---|------|---|------|---|------|---|
| Pedestrian LOS Score / LOS | 2.24 | B | 1.92 | B | 2.62 | C | 2.62 | C |
| Bicycle LOS Score / LOS | 1.06 | A | 1.41 | A | 0.93 | A | | |

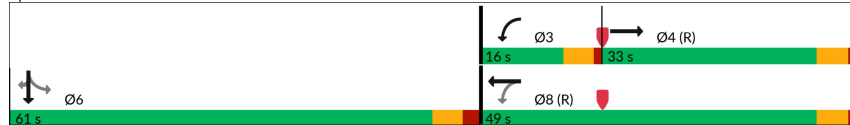
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖↖ | ↖ |
| Traffic Volume (vph) | 888 | 41 | 755 | 840 | 666 | 763 |
| Future Volume (vph) | 888 | 41 | 755 | 840 | 666 | 763 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

| Intersection Summary | |
|------------------------|--|
| Cycle Length: | 110 |
| Actuated Cycle Length: | 110 |
| Offset: | 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green |
| Natural Cycle: | 85 |
| Control Type: | Actuated-Coordinated |

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 | | |
| Future Volume (veh/h) | 0 | 888 | 0 | 41 | 755 | 0 | 0 | 0 | 0 | 840 | 666 | 763 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 888 | 0 | 41 | 755 | 0 | | | | 560 | 1439 | 509 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1289 | | 302 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 888 | 0 | 41 | 755 | 0 | | | | 560 | 1439 | 509 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 17.4 | 0.0 | 1.6 | 11.6 | 0.0 | | | | 25.5 | 34.8 | 26.3 | | |
| Cycle Q Clear(g_c), s | 0.0 | 17.4 | 0.0 | 1.6 | 11.6 | 0.0 | | | | 25.5 | 34.8 | 26.3 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 302 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| V/C Ratio(X) | 0.00 | 0.69 | | 0.14 | 0.37 | 0.00 | | | | 0.63 | 0.78 | 0.65 | | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 302 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.93 | 0.93 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 37.1 | 0.0 | 24.0 | 23.3 | 0.0 | | | | 20.1 | 22.5 | 20.3 | | |
| Incr Delay (d2), s/veh | 0.0 | 3.0 | 0.0 | 0.9 | 0.5 | 0.0 | | | | 3.5 | 3.2 | 4.1 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.9 | 0.0 | 1.3 | 8.0 | 0.0 | | | | 16.2 | 21.5 | 15.2 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 40.1 | 0.0 | 24.9 | 23.8 | 0.0 | | | | 23.6 | 25.7 | 24.4 | | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | | |
| Approach Vol, veh/h | | 888 | | | 796 | | | | | | | 2508 | | |
| Approach Delay, s/veh | | 40.1 | | | 23.8 | | | | | | | 25.0 | | |
| Approach LOS | | D | | | C | | | | | | | C | | |
| Timer - Assigned Phs | | 3 | 4 | | 6 | 8 | | | | | | | | |
| Phs Duration (G+Y+Rc), s | | 16.0 | 33.0 | | 61.0 | 49.0 | | | | | | | | |
| Change Period (Y+Rc), s | | 5.0 | 5.0 | | 6.0 | 5.0 | | | | | | | | |
| Max Green Setting (Gmax), s | | 11.0 | 28.0 | | 55.0 | 44.0 | | | | | | | | |
| Max Q Clear Time (g_c+1), s | | 3.6 | 19.4 | | 36.8 | 13.6 | | | | | | | | |
| Green Ext Time (p_c), s | | 0.0 | 3.8 | | 13.5 | 5.7 | | | | | | | | |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 28.0 |
| HCM 7th LOS | C |

Notes
User approved volume balancing among the lanes for turning movement.
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

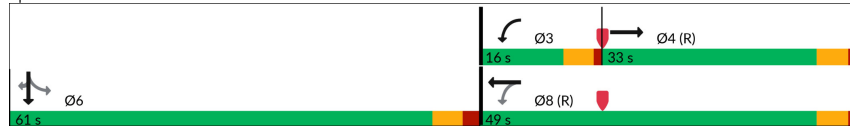
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↑ |
| Traffic Volume (vph) | 915 | 51 | 776 | 843 | 666 | 763 |
| Future Volume (vph) | 915 | 51 | 776 | 843 | 666 | 763 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green
 Natural Cycle: 85
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | ↖ | → | ↙ | ↘ | ← | ↖ | ↙ | ↘ | ↓ | ↗ | ↘ | ↙ | ↘ |
|------------------------------|------|------|------|------|------|------|-----|-----|------|------|------|------|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↑ | |
| Traffic Volume (veh/h) | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 | |
| Future Volume (veh/h) | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Work Zone On Approach | No | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | |
| Adj Flow Rate, veh/h | 0 | 915 | 0 | 51 | 776 | 0 | | | | 562 | 1441 | 509 | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | |
| Cap, veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | |
| Grp Volume(v), veh/h | 0 | 915 | 0 | 51 | 776 | 0 | | | | 562 | 1441 | 509 | |
| Grp Sat Flow(s),veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | |
| Q Serve(g_s), s | 0.0 | 18.1 | 0.0 | 2.1 | 11.9 | 0.0 | | | | 25.6 | 34.9 | 26.3 | |
| Cycle Q Clear(g_c), s | 0.0 | 18.1 | 0.0 | 2.1 | 11.9 | 0.0 | | | | 25.6 | 34.9 | 26.3 | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 | |
| V/C Ratio(X) | 0.00 | 0.71 | | 0.17 | 0.38 | 0.00 | | | | 0.64 | 0.78 | 0.65 | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.92 | 0.92 | 0.00 | | | | 1.00 | 1.00 | 1.00 | |
| Uniform Delay (d), s/veh | 0.0 | 37.3 | 0.0 | 24.3 | 23.4 | 0.0 | | | | 20.2 | 22.5 | 20.3 | |
| Incr Delay (d2), s/veh | 0.0 | 3.3 | 0.0 | 1.2 | 0.5 | 0.0 | | | | 3.5 | 3.3 | 4.1 | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 12.3 | 0.0 | 1.7 | 8.2 | 0.0 | | | | 16.3 | 21.6 | 15.2 | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 40.6 | 0.0 | 25.4 | 23.9 | 0.0 | | | | 23.6 | 25.7 | 24.4 | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | |
| Approach Vol, veh/h | | 915 | | | 827 | | | | | | | 2512 | |
| Approach Delay, s/veh | | 40.6 | | | 24.0 | | | | | | | 25.0 | |
| Approach LOS | | D | | | C | | | | | | | C | |
| Timer - Assigned Phs | | 3 | | 4 | | 6 | | | 8 | | | | |
| Phs Duration (G+Y+Rc), s | | 16.0 | | 33.0 | | 61.0 | | | 49.0 | | | | |
| Change Period (Y+Rc), s | | 5.0 | | 5.0 | | 6.0 | | | 5.0 | | | | |
| Max Green Setting (Gmax), s | | 11.0 | | 28.0 | | 55.0 | | | 44.0 | | | | |
| Max Q Clear Time (g_c+I1), s | | 4.1 | | 20.1 | | 36.9 | | | 13.9 | | | | |
| Green Ext Time (p_c), s | | 0.0 | | 3.7 | | 13.5 | | | 5.9 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 28.2
 HCM 7th LOS: C
Notes
 User approved volume balancing among the lanes for turning movement.
 Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

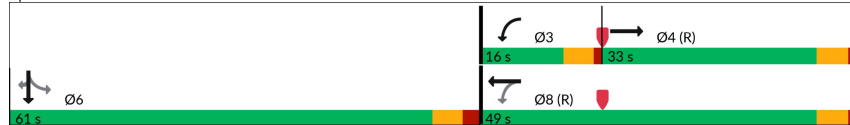
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/15/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↑ |
| Traffic Volume (vph) | 915 | 51 | 776 | 843 | 666 | 763 |
| Future Volume (vph) | 915 | 51 | 776 | 843 | 666 | 763 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green
 Natural Cycle: 85
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/15/2025

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↙ | ↘ | ↓ | ↘ | ↙ | ↘ |
|------------------------------|------|------|------|------|------|------|-----|-----|------|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↑ |
| Traffic Volume (veh/h) | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| Future Volume (veh/h) | 0 | 915 | 0 | 51 | 776 | 0 | 0 | 0 | 0 | 843 | 666 | 763 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | | | No | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 0 | 915 | 0 | 51 | 776 | 0 | | | | 562 | 1441 | 509 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 |
| Cap, veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 |
| Grp Volume(v), veh/h | 0 | 915 | 0 | 51 | 776 | 0 | | | | 562 | 1441 | 509 |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 |
| Q Serve(g_s), s | 0.0 | 18.1 | 0.0 | 2.1 | 11.9 | 0.0 | | | | 25.6 | 34.9 | 26.3 |
| Cycle Q Clear(g_c), s | 0.0 | 18.1 | 0.0 | 2.1 | 11.9 | 0.0 | | | | 25.6 | 34.9 | 26.3 |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 |
| V/C Ratio(X) | 0.00 | 0.71 | 0.00 | 0.17 | 0.38 | 0.00 | | | | 0.64 | 0.78 | 0.65 |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 297 | 2026 | 0 | | | | 884 | 1856 | 786 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.92 | 0.92 | 0.00 | | | | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 0.0 | 37.3 | 0.0 | 24.3 | 23.4 | 0.0 | | | | 20.2 | 22.5 | 20.3 |
| Incr Delay (d2), s/veh | 0.0 | 3.3 | 0.0 | 1.2 | 0.5 | 0.0 | | | | 3.5 | 3.3 | 4.1 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 0.0 | 12.3 | 0.0 | 1.7 | 8.2 | 0.0 | | | | 16.3 | 21.6 | 15.2 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 40.6 | 0.0 | 25.4 | 23.9 | 0.0 | | | | 23.6 | 25.7 | 24.4 |
| LnGrp LOS | | D | | C | C | | | | | C | C | C |
| Approach Vol, veh/h | | 915 | | | 827 | | | | | | 2512 | |
| Approach Delay, s/veh | | 40.6 | | | 24.0 | | | | | | 25.0 | |
| Approach LOS | | D | | | C | | | | | | C | |
| Timer - Assigned Phs | | 3 | | 4 | | 6 | | | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 16.0 | | 33.0 | | 61.0 | | | 49.0 | | | |
| Change Period (Y+Rc), s | | 5.0 | | 5.0 | | 6.0 | | | 5.0 | | | |
| Max Green Setting (Gmax), s | | 11.0 | | 28.0 | | 55.0 | | | 44.0 | | | |
| Max Q Clear Time (g_c+I1), s | | 4.1 | | 20.1 | | 36.9 | | | 13.9 | | | |
| Green Ext Time (p_c), s | | 0.0 | | 3.7 | | 13.5 | | | 5.9 | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 28.2
 HCM 7th LOS: C

Notes
 User approved volume balancing among the lanes for turning movement.
 Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

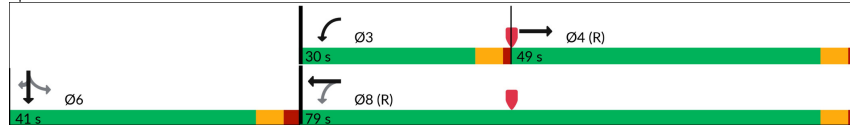
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 877 | 325 | 930 | 199 | 434 | 240 |
| Future Volume (vph) | 877 | 325 | 930 | 199 | 434 | 240 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBT, Start of Green
 Natural Cycle: 55
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | | |
| Traffic Volume (veh/h) | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 | | | |
| Future Volume (veh/h) | 0 | 877 | 0 | 325 | 930 | 0 | 0 | 0 | 0 | 199 | 434 | 240 | | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 0 | 877 | 0 | 325 | 930 | 0 | | | | 199 | 554 | 160 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | | |
| Cap, veh/h | 0 | 1857 | | 575 | 3124 | 0 | | | | 515 | 1082 | 459 | | | |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 | | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | | |
| Grp Volume(v), veh/h | 0 | 877 | 0 | 325 | 930 | 0 | | | | 199 | 554 | 160 | | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 0.0 | 15.9 | 0.0 | 10.5 | 0.0 | 0.0 | | | | 10.8 | 14.9 | 9.6 | | | |
| Cycle Q Clear(g_c), s | 0.0 | 15.9 | 0.0 | 10.5 | 0.0 | 0.0 | | | | 10.8 | 14.9 | 9.6 | | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 575 | 3124 | 0 | | | | 515 | 1082 | 459 | | | |
| V/C Ratio(X) | 0.00 | 0.47 | | 0.57 | 0.30 | 0.00 | | | | 0.39 | 0.51 | 0.35 | | | |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 575 | 3124 | 0 | | | | 515 | 1082 | 459 | | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.68 | 0.68 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 0.0 | 29.1 | 0.0 | 10.5 | 0.0 | 0.0 | | | | 33.9 | 35.4 | 33.5 | | | |
| Incr Delay (d2), s/veh | 0.0 | 0.9 | 0.0 | 2.7 | 0.2 | 0.0 | | | | 2.2 | 1.7 | 2.1 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 10.7 | 0.0 | 5.8 | 0.1 | 0.0 | | | | 8.5 | 11.2 | 7.0 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.0 | 0.0 | 13.3 | 0.2 | 0.0 | | | | 36.1 | 37.1 | 35.6 | | | |
| LnGrp LOS | | C | | B | A | | | | | D | D | D | | | |
| Approach Vol, veh/h | | 877 | | | 1255 | | | | | | | 913 | | | |
| Approach Delay, s/veh | | 30.0 | | | 3.6 | | | | | | | 36.6 | | | |
| Approach LOS | | C | | | A | | | | | | | D | | | |
| Timer - Assigned Phs | | 3 | 4 | 6 | 8 | | | | | | | | | | |
| Phs Duration (G+Y+Rc), s | | 30.0 | 49.0 | 41.0 | 79.0 | | | | | | | | | | |
| Change Period (Y+Rc), s | | 5.0 | 5.0 | 6.0 | 5.0 | | | | | | | | | | |
| Max Green Setting (Gmax), s | | 25.0 | 44.0 | 35.0 | 74.0 | | | | | | | | | | |
| Max Q Clear Time (g_c+1), s | | 12.5 | 17.9 | 16.9 | 2.0 | | | | | | | | | | |
| Green Ext Time (p_c), s | | 0.8 | 6.5 | 4.6 | 8.0 | | | | | | | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.1
 HCM 7th LOS: C

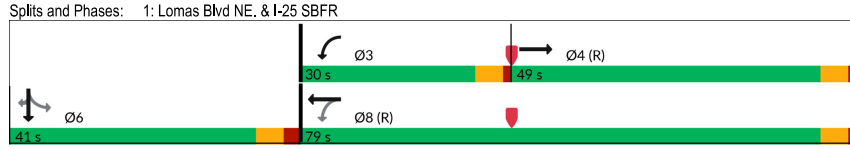
Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 905 | 335 | 952 | 202 | 434 | 240 |
| Future Volume (vph) | 905 | 335 | 952 | 202 | 434 | 240 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/13/2025

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 | | |
| Future Volume (veh/h) | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 905 | 0 | 335 | 952 | 0 | | | | 202 | 554 | 160 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 905 | 0 | 335 | 952 | 0 | | | | 202 | 554 | 160 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 16.5 | 0.0 | 11.0 | 0.0 | 0.0 | | | | 11.0 | 14.9 | 9.6 | | |
| Cycle Q Clear(g_c), s | 0.0 | 16.5 | 0.0 | 11.0 | 0.0 | 0.0 | | | | 11.0 | 14.9 | 9.6 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| V/C Ratio(X) | 0.00 | 0.49 | | 0.59 | 0.30 | 0.00 | | | | 0.39 | 0.51 | 0.35 | | |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.66 | 0.66 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 29.3 | 0.0 | 10.8 | 0.0 | 0.0 | | | | 34.0 | 35.4 | 33.5 | | |
| Incr Delay (d2), s/veh | 0.0 | 0.9 | 0.0 | 3.0 | 0.2 | 0.0 | | | | 2.2 | 1.7 | 2.1 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.0 | 0.0 | 5.9 | 0.1 | 0.0 | | | | 8.7 | 11.2 | 7.0 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.2 | 0.0 | 13.8 | 0.2 | 0.0 | | | | 36.2 | 37.1 | 35.6 | | |
| LnGrp LOS | | C | | B | A | | | | | D | D | D | | |
| Approach Vol, veh/h | | 905 | | | 1287 | | | | | | | 916 | | |
| Approach Delay, s/veh | | 30.2 | | | 3.7 | | | | | | | 36.7 | | |
| Approach LOS | | C | | | A | | | | | | | D | | |
| Timer - Assigned Phs | | 3 | | 4 | | 6 | | | | 8 | | | | |
| Phs Duration (G+Y+Rc), s | | 30.0 | | 49.0 | | 41.0 | | | | 79.0 | | | | |
| Change Period (Y+Rc), s | | 5.0 | | 5.0 | | 6.0 | | | | 5.0 | | | | |
| Max Green Setting (Gmax), s | | 25.0 | | 44.0 | | 35.0 | | | | 74.0 | | | | |
| Max Q Clear Time (g_c+1), s | | 13.0 | | 18.5 | | 16.9 | | | | 2.0 | | | | |
| Green Ext Time (p_c), s | | 0.8 | | 6.7 | | 4.6 | | | | 8.3 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.1
 HCM 7th LOS: C
Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

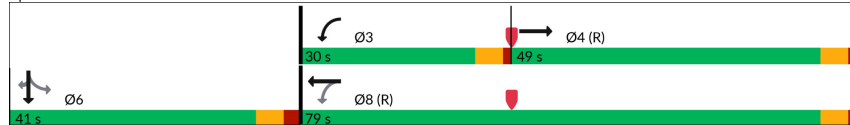
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
09/15/2025

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|----------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 905 | 335 | 952 | 202 | 434 | 240 |
| Future Volume (vph) | 905 | 335 | 952 | 202 | 434 | 240 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
 1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
 09/15/2025

| | ↖ | → | ↙ | ↖ | ← | ↙ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | |
| Traffic Volume (veh/h) | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 | |
| Future Volume (veh/h) | 0 | 905 | 0 | 335 | 952 | 0 | 0 | 0 | 0 | 202 | 434 | 240 | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Work Zone On Approach | No | | | | | No | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | |
| Adj Flow Rate, veh/h | 0 | 905 | 0 | 335 | 952 | 0 | | | | 202 | 554 | 160 | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | |
| Cap, veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | |
| Grp Volume(v), veh/h | 0 | 905 | 0 | 335 | 952 | 0 | | | | 202 | 554 | 160 | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | |
| Q Serve(g_s), s | 0.0 | 16.5 | 0.0 | 11.0 | 0.0 | 0.0 | | | | 11.0 | 14.9 | 9.6 | |
| Cycle Q Clear(g_c), s | 0.0 | 16.5 | 0.0 | 11.0 | 0.0 | 0.0 | | | | 11.0 | 14.9 | 9.6 | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | |
| V/C Ratio(X) | 0.00 | 0.49 | | 0.59 | 0.30 | 0.00 | | | | 0.39 | 0.51 | 0.35 | |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 568 | 3124 | 0 | | | | 515 | 1082 | 459 | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.87 | 0.87 | 0.00 | | | | 1.00 | 1.00 | 1.00 | |
| Uniform Delay (d), s/veh | 0.0 | 29.3 | 0.0 | 10.8 | 0.0 | 0.0 | | | | 34.0 | 35.4 | 33.5 | |
| Incr Delay (d2), s/veh | 0.0 | 0.9 | 0.0 | 3.9 | 0.2 | 0.0 | | | | 2.2 | 1.7 | 2.1 | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.0 | 0.0 | 6.5 | 0.1 | 0.0 | | | | 8.7 | 11.2 | 7.0 | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.2 | 0.0 | 14.7 | 0.2 | 0.0 | | | | 36.2 | 37.1 | 35.6 | |
| LnGrp LOS | | C | | B | A | | | | | D | D | D | |
| Approach Vol, veh/h | | 905 | | | 1287 | | | | | | 916 | | |
| Approach Delay, s/veh | | 30.2 | | | 4.0 | | | | | | 36.7 | | |
| Approach LOS | | C | | | A | | | | | | D | | |
| Timer - Assigned Phs | | 3 | | 4 | | 6 | | | | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 30.0 | | 49.0 | | 41.0 | | | | 79.0 | | | |
| Change Period (Y+Rc), s | | 5.0 | | 5.0 | | 6.0 | | | | 5.0 | | | |
| Max Green Setting (Gmax), s | | 25.0 | | 44.0 | | 35.0 | | | | 74.0 | | | |
| Max Q Clear Time (g_c+1), s | | 13.0 | | 18.5 | | 16.9 | | | | 2.0 | | | |
| Green Ext Time (p_c), s | | 0.8 | | 6.7 | | 4.6 | | | | 8.3 | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.3
 HCM 7th LOS: C

Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

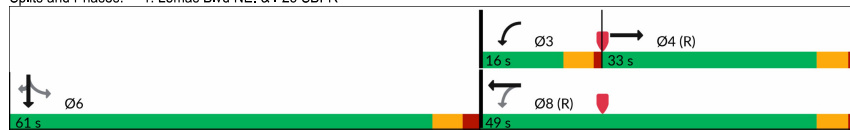
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 932 | 43 | 792 | 882 | 699 | 801 |
| Future Volume (vph) | 932 | 43 | 792 | 882 | 699 | 801 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 28.0 | 44.0 | 44.0 | 55.0 | 55.0 | 55.0 |
| Actuated g/C Ratio | 0.25 | 0.40 | 0.40 | 0.50 | 0.50 | 0.50 |
| v/c Ratio | 0.73 | 0.18 | 0.39 | 0.76 | 0.80 | 0.72 |
| Control Delay (s/veh) | 41.4 | 16.8 | 13.1 | 30.0 | 27.0 | 25.0 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 41.4 | 16.8 | 13.1 | 30.0 | 27.0 | 25.0 |
| LOS | D | B | B | C | C | C |
| Approach Delay (s/veh) | 41.4 | | 13.3 | | 27.3 | |
| Approach LOS | D | | B | | C | |

| Intersection Summary | |
|--|------------------------|
| Cycle Length: 110 | |
| Actuated Cycle Length: 110 | |
| Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green | |
| Natural Cycle: 85 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.80 | |
| Intersection Signal Delay (s/veh): 27.7 | Intersection LOS: C |
| Intersection Capacity Utilization 70.3% | ICU Level of Service C |
| Analysis Period (min) 15 | |

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↘ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 | | |
| Future Volume (veh/h) | 0 | 932 | 0 | 43 | 792 | 0 | 0 | 0 | 0 | 882 | 699 | 801 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 932 | 0 | 43 | 792 | 0 | | | | 588 | 1512 | 534 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1289 | | 294 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 932 | 0 | 43 | 792 | 0 | | | | 588 | 1512 | 534 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 18.5 | 0.0 | 1.7 | 12.2 | 0.0 | | | | 27.4 | 37.8 | 28.3 | | |
| Cycle Q Clear(g_c), s | 0.0 | 18.5 | 0.0 | 1.7 | 12.2 | 0.0 | | | | 27.4 | 37.8 | 28.3 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 294 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| V/C Ratio(X) | 0.00 | 0.72 | | 0.15 | 0.39 | 0.00 | | | | 0.67 | 0.81 | 0.68 | | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 294 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.92 | 0.92 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 37.5 | 0.0 | 24.2 | 23.5 | 0.0 | | | | 20.6 | 23.2 | 20.8 | | |
| Incr Delay (d2), s/veh | 0.0 | 3.5 | 0.0 | 1.0 | 0.5 | 0.0 | | | | 3.9 | 4.1 | 4.7 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 12.5 | 0.0 | 1.4 | 8.3 | 0.0 | | | | 17.3 | 23.2 | 16.3 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 41.0 | 0.0 | 25.2 | 24.0 | 0.0 | | | | 24.6 | 27.3 | 25.5 | | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | | |
| Approach Vol, veh/h | | 932 | | | 835 | | | | | | | 2634 | | |
| Approach Delay, s/veh | | 41.0 | | | 24.1 | | | | | | | 26.3 | | |
| Approach LOS | | D | | | C | | | | | | | C | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 16.0 | 33.0 | 61.0 | 49.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 11.0 | 28.0 | 55.0 | 44.0 |
| Max Q Clear Time (g_c+1), s | 3.7 | 20.5 | 39.8 | 14.2 |
| Green Ext Time (p_c), s | 0.0 | 3.6 | 12.1 | 6.0 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 29.0 |
| HCM 7th LOS | C |

Notes
User approved volume balancing among the lanes for turning movement.
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 959 | 53 | 813 | 885 | 699 | 801 |
| Future Volume (vph) | 959 | 53 | 813 | 885 | 699 | 801 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 28.0 | 44.0 | 44.0 | 55.0 | 55.0 | 55.0 |
| Actuated g/C Ratio | 0.25 | 0.40 | 0.40 | 0.50 | 0.50 | 0.50 |
| v/c Ratio | 0.75 | 0.22 | 0.40 | 0.77 | 0.80 | 0.73 |
| Control Delay (s/veh) | 42.1 | 21.5 | 13.0 | 30.2 | 27.0 | 25.3 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 42.1 | 21.5 | 13.0 | 30.2 | 27.0 | 25.3 |
| LOS | D | C | B | C | C | C |
| Approach Delay (s/veh) | 42.1 | | 13.5 | | 27.5 | |
| Approach LOS | D | | B | | C | |

| Intersection Summary | |
|--|------------------------|
| Cycle Length: 110 | |
| Actuated Cycle Length: 110 | |
| Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green | |
| Natural Cycle: 85 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.80 | |
| Intersection Signal Delay (s/veh): 27.9 | Intersection LOS: C |
| Intersection Capacity Utilization 70.9% | ICU Level of Service C |
| Analysis Period (min) 15 | |

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↘ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 | | |
| Future Volume (veh/h) | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 959 | 0 | 53 | 813 | 0 | | | | 590 | 1513 | 534 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 959 | 0 | 53 | 813 | 0 | | | | 590 | 1513 | 534 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 19.1 | 0.0 | 2.1 | 12.6 | 0.0 | | | | 27.6 | 37.9 | 28.3 | | |
| Cycle Q Clear(g_c), s | 0.0 | 19.1 | 0.0 | 2.1 | 12.6 | 0.0 | | | | 27.6 | 37.9 | 28.3 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| V/C Ratio(X) | 0.00 | 0.74 | | 0.18 | 0.40 | 0.00 | | | | 0.67 | 0.82 | 0.68 | | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.91 | 0.91 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 37.7 | 0.0 | 24.5 | 23.6 | 0.0 | | | | 20.6 | 23.2 | 20.8 | | |
| Incr Delay (d2), s/veh | 0.0 | 3.9 | 0.0 | 1.3 | 0.5 | 0.0 | | | | 4.0 | 4.1 | 4.7 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 12.9 | 0.0 | 1.8 | 8.5 | 0.0 | | | | 17.4 | 23.3 | 16.3 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 41.6 | 0.0 | 25.8 | 24.1 | 0.0 | | | | 24.6 | 27.3 | 25.5 | | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | | |
| Approach Vol, veh/h | | 959 | | | 866 | | | | | | | 2637 | | |
| Approach Delay, s/veh | | 41.6 | | | 24.2 | | | | | | | 26.3 | | |
| Approach LOS | | D | | | C | | | | | | | C | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|------------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 16.0 | 33.0 | 61.0 | 49.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 11.0 | 28.0 | 55.0 | 44.0 |
| Max Q Clear Time (g_c+I1), s | 4.1 | 21.1 | 39.9 | 14.6 |
| Green Ext Time (p_c), s | 0.0 | 3.4 | 12.1 | 6.2 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 29.2 |
| HCM 7th LOS | C |

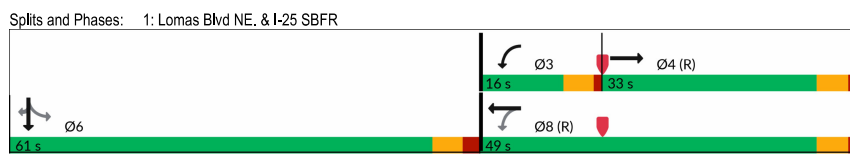
Notes
User approved volume balancing among the lanes for turning movement.
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 959 | 53 | 813 | 885 | 699 | 801 |
| Future Volume (vph) | 959 | 53 | 813 | 885 | 699 | 801 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 49.0 | 49.0 | 49.0 |
| Total Split (s) | 33.0 | 16.0 | 49.0 | 61.0 | 61.0 | 61.0 |
| Total Split (%) | 30.0% | 14.5% | 44.5% | 55.5% | 55.5% | 55.5% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 28.0 | 44.0 | 44.0 | 55.0 | 55.0 | 55.0 |
| Actuated g/C Ratio | 0.25 | 0.40 | 0.40 | 0.50 | 0.50 | 0.50 |
| v/c Ratio | 0.75 | 0.22 | 0.40 | 0.77 | 0.80 | 0.73 |
| Control Delay (s/veh) | 42.1 | 21.7 | 13.2 | 30.2 | 27.0 | 25.3 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 42.1 | 21.7 | 13.2 | 30.2 | 27.0 | 25.3 |
| LOS | D | C | B | C | C | C |
| Approach Delay (s/veh) | 42.1 | | 13.7 | | 27.5 | |
| Approach LOS | D | | B | | C | |

| Intersection Summary | |
|--|------------------------|
| Cycle Length: 110 | |
| Actuated Cycle Length: 110 | |
| Offset: 58 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green | |
| Natural Cycle: 85 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.80 | |
| Intersection Signal Delay (s/veh): 28.0 | Intersection LOS: C |
| Intersection Capacity Utilization 70.9% | ICU Level of Service C |
| Analysis Period (min) 15 | |



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↘ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 | | |
| Future Volume (veh/h) | 0 | 959 | 0 | 53 | 813 | 0 | 0 | 0 | 0 | 885 | 699 | 801 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 959 | 0 | 53 | 813 | 0 | | | | 590 | 1513 | 534 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| Arrive On Green | 0.00 | 0.25 | 0.00 | 0.10 | 0.40 | 0.00 | | | | 0.50 | 0.50 | 0.50 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 959 | 0 | 53 | 813 | 0 | | | | 590 | 1513 | 534 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 19.1 | 0.0 | 2.1 | 12.6 | 0.0 | | | | 27.6 | 37.9 | 28.3 | | |
| Cycle Q Clear(g_c), s | 0.0 | 19.1 | 0.0 | 2.1 | 12.6 | 0.0 | | | | 27.6 | 37.9 | 28.3 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| V/C Ratio(X) | 0.00 | 0.74 | | 0.18 | 0.40 | 0.00 | | | | 0.67 | 0.82 | 0.68 | | |
| Avail Cap(c_a), veh/h | 0 | 1289 | | 289 | 2026 | 0 | | | | 884 | 1856 | 786 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.91 | 0.91 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 37.7 | 0.0 | 24.5 | 23.6 | 0.0 | | | | 20.6 | 23.2 | 20.8 | | |
| Incr Delay (d2), s/veh | 0.0 | 3.9 | 0.0 | 1.3 | 0.5 | 0.0 | | | | 4.0 | 4.1 | 4.7 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 12.9 | 0.0 | 1.8 | 8.5 | 0.0 | | | | 17.4 | 23.3 | 16.3 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 41.6 | 0.0 | 25.8 | 24.1 | 0.0 | | | | 24.6 | 27.3 | 25.5 | | |
| LnGrp LOS | | D | | C | C | | | | | C | C | C | | |
| Approach Vol, veh/h | | 959 | | | 866 | | | | | | | 2637 | | |
| Approach Delay, s/veh | | 41.6 | | | 24.2 | | | | | | | 26.3 | | |
| Approach LOS | | D | | | C | | | | | | | C | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 16.0 | 33.0 | 61.0 | 49.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 11.0 | 28.0 | 55.0 | 44.0 |
| Max Q Clear Time (g_c+1), s | 4.1 | 21.1 | 39.9 | 14.6 |
| Green Ext Time (p_c), s | 0.0 | 3.4 | 12.1 | 6.2 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 29.2 |
| HCM 7th LOS | C |

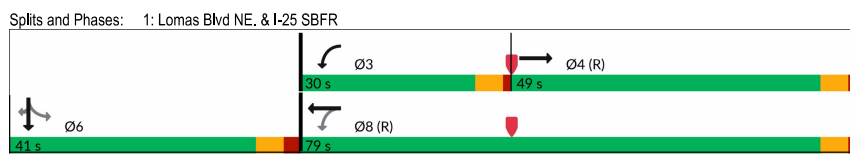
Notes
User approved volume balancing among the lanes for turning movement.
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↑↑↑ | ↖ |
| Traffic Volume (vph) | 920 | 341 | 976 | 209 | 456 | 251 |
| Future Volume (vph) | 920 | 341 | 976 | 209 | 456 | 251 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 44.0 | 74.0 | 74.0 | 35.0 | 35.0 | 35.0 |
| Actuated g/C Ratio | 0.37 | 0.62 | 0.62 | 0.29 | 0.29 | 0.29 |
| v/c Ratio | 0.50 | 0.67 | 0.31 | 0.40 | 0.56 | 0.43 |
| Control Delay (s/veh) | 30.6 | 55.3 | 1.7 | 37.4 | 38.3 | 16.1 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 30.6 | 55.3 | 1.7 | 37.4 | 38.3 | 16.1 |
| LOS | C | E | A | D | D | B |
| Approach Delay (s/veh) | 30.6 | | 15.6 | | 32.8 | |
| Approach LOS | C | | B | | C | |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.67
 Intersection Signal Delay (s/veh): 25.0 Intersection LOS: C
 Intersection Capacity Utilization 81.8% ICU Level of Service D
 Analysis Period (min) 15



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↙ | ↖ | ← | ↙ | ↖ | ↙ | ↖ | ↙ | ↖ | ↙ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↑↑↑ | ↖ |
| Traffic Volume (veh/h) | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| Future Volume (veh/h) | 0 | 920 | 0 | 341 | 976 | 0 | 0 | 0 | 0 | 209 | 456 | 251 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | | | No | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 0 | 920 | 0 | 341 | 976 | 0 | | | | 209 | 582 | 167 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 |
| Cap, veh/h | 0 | 1857 | | 564 | 3124 | 0 | | | | 515 | 1082 | 459 |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 |
| Grp Volume(v), veh/h | 0 | 920 | 0 | 341 | 976 | 0 | | | | 209 | 582 | 167 |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 |
| Q Serve(g_s), s | 0.0 | 16.9 | 0.0 | 11.3 | 0.0 | 0.0 | | | | 11.4 | 15.8 | 10.1 |
| Cycle Q Clear(g_c), s | 0.0 | 16.9 | 0.0 | 11.3 | 0.0 | 0.0 | | | | 11.4 | 15.8 | 10.1 |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 564 | 3124 | 0 | | | | 515 | 1082 | 459 |
| V/C Ratio(X) | 0.00 | 0.50 | | 0.60 | 0.31 | 0.00 | | | | 0.41 | 0.54 | 0.36 |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 564 | 3124 | 0 | | | | 515 | 1082 | 459 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.64 | 0.64 | 0.00 | | | | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 0.0 | 29.4 | 0.0 | 11.0 | 0.0 | 0.0 | | | | 34.1 | 35.7 | 33.7 |
| Incr Delay (d2), s/veh | 0.0 | 0.9 | 0.0 | 3.1 | 0.2 | 0.0 | | | | 2.4 | 1.9 | 2.2 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.2 | 0.0 | 6.0 | 0.1 | 0.0 | | | | 8.9 | 11.8 | 7.4 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.4 | 0.0 | 14.0 | 0.2 | 0.0 | | | | 36.5 | 37.6 | 35.9 |
| LnGrp LOS | | C | | B | A | | | | | D | D | D |
| Approach Vol, veh/h | | 920 | | | 1317 | | | | | | 958 | |
| Approach Delay, s/veh | | 30.4 | | | 3.8 | | | | | | 37.1 | |
| Approach LOS | | C | | | A | | | | | | D | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 30.0 | 49.0 | 41.0 | 79.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 25.0 | 44.0 | 35.0 | 74.0 |
| Max Q Clear Time (g_c+1), s | 13.3 | 18.9 | 17.8 | 2.0 |
| Green Ext Time (p_c), s | 0.8 | 6.8 | 4.7 | 8.6 |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.4
 HCM 7th LOS: C

Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

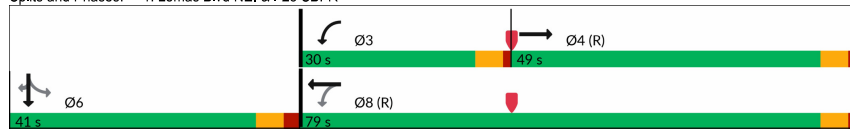
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 948 | 351 | 998 | 212 | 456 | 251 |
| Future Volume (vph) | 948 | 351 | 998 | 212 | 456 | 251 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 44.0 | 74.0 | 74.0 | 35.0 | 35.0 | 35.0 |
| Actuated g/C Ratio | 0.37 | 0.62 | 0.62 | 0.29 | 0.29 | 0.29 |
| v/c Ratio | 0.51 | 0.70 | 0.32 | 0.41 | 0.56 | 0.43 |
| Control Delay (s/veh) | 30.9 | 58.6 | 1.7 | 37.5 | 38.3 | 16.9 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 30.9 | 58.6 | 1.7 | 37.5 | 38.3 | 16.9 |
| LOS | C | E | A | D | D | B |
| Approach Delay (s/veh) | 30.9 | | 16.5 | | 33.1 | |
| Approach LOS | C | | B | | C | |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.70
 Intersection Signal Delay (s/veh): 25.5 Intersection LOS: C
 Intersection Capacity Utilization 82.0% ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | | |
| Traffic Volume (veh/h) | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 | | |
| Future Volume (veh/h) | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | | | | 0 | 0 | 0 | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Work Zone On Approach | No | | | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | | |
| Adj Flow Rate, veh/h | 0 | 948 | 0 | 351 | 998 | 0 | | | | 212 | 582 | 167 | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | | |
| Cap, veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 | | |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | | |
| Grp Volume(v), veh/h | 0 | 948 | 0 | 351 | 998 | 0 | | | | 212 | 582 | 167 | | |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | | |
| Q Serve(g_s), s | 0.0 | 17.5 | 0.0 | 11.9 | 0.0 | 0.0 | | | | 11.6 | 15.8 | 10.1 | | |
| Cycle Q Clear(g_c), s | 0.0 | 17.5 | 0.0 | 11.9 | 0.0 | 0.0 | | | | 11.6 | 15.8 | 10.1 | | |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | | |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| V/C Ratio(X) | 0.00 | 0.51 | | 0.63 | 0.32 | 0.00 | | | | 0.41 | 0.54 | 0.36 | | |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | | |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.61 | 0.61 | 0.00 | | | | 1.00 | 1.00 | 1.00 | | |
| Uniform Delay (d), s/veh | 0.0 | 29.6 | 0.0 | 11.2 | 0.0 | 0.0 | | | | 34.2 | 35.7 | 33.7 | | |
| Incr Delay (d2), s/veh | 0.0 | 1.0 | 0.0 | 3.3 | 0.2 | 0.0 | | | | 2.4 | 1.9 | 2.2 | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | | |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.6 | 0.0 | 6.1 | 0.1 | 0.0 | | | | 9.0 | 11.8 | 7.4 | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.6 | 0.0 | 14.5 | 0.2 | 0.0 | | | | 36.6 | 37.6 | 35.9 | | |
| LnGrp LOS | | C | | B | A | | | | | D | D | D | | |
| Approach Vol, veh/h | | 948 | | | 1349 | | | | | | 961 | | | |
| Approach Delay, s/veh | | 30.6 | | | 3.9 | | | | | | 37.1 | | | |
| Approach LOS | | C | | | A | | | | | | D | | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 30.0 | 49.0 | 41.0 | 79.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 25.0 | 44.0 | 35.0 | 74.0 |
| Max Q Clear Time (g_c+1), s | 13.9 | 19.5 | 17.8 | 2.0 |
| Green Ext Time (p_c), s | 0.8 | 7.0 | 4.7 | 8.8 |

Intersection Summary
 HCM 7th Control Delay, s/veh: 21.5
 HCM 7th LOS: C

Notes
 User approved pedestrian interval to be less than phase max green.
 User approved volume balancing among the lanes for turning movement.

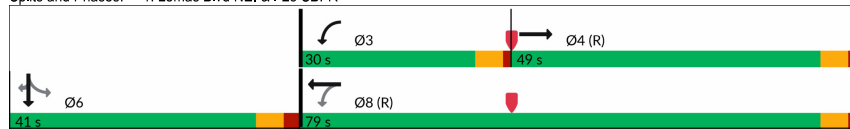
Timings
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | → | ↖ | ← | ↙ | ↓ | ↘ |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBT | WBL | WBT | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↖ | ↑↑↑ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 948 | 351 | 998 | 212 | 456 | 251 |
| Future Volume (vph) | 948 | 351 | 998 | 212 | 456 | 251 |
| Turn Type | NA | pm+pt | NA | Perm | NA | Perm |
| Protected Phases | 4 | 3 | 8 | | 6 | |
| Permitted Phases | | 8 | | 6 | | 6 |
| Detector Phase | 4 | 3 | 8 | 6 | 6 | 6 |
| Switch Phase | | | | | | |
| Minimum Initial (s) | 16.0 | 3.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 23.0 | 9.5 | 31.0 | 14.0 | 14.0 | 14.0 |
| Total Split (s) | 49.0 | 30.0 | 79.0 | 41.0 | 41.0 | 41.0 |
| Total Split (%) | 40.8% | 25.0% | 65.8% | 34.2% | 34.2% | 34.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lag | Lead | | | | |
| Lead-Lag Optimize? | Yes | Yes | | | | |
| Recall Mode | C-Max | Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 44.0 | 74.0 | 74.0 | 35.0 | 35.0 | 35.0 |
| Actuated g/C Ratio | 0.37 | 0.62 | 0.62 | 0.29 | 0.29 | 0.29 |
| v/c Ratio | 0.51 | 0.70 | 0.32 | 0.41 | 0.56 | 0.43 |
| Control Delay (s/veh) | 30.9 | 53.2 | 4.5 | 37.5 | 38.3 | 16.9 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 30.9 | 53.2 | 4.5 | 37.5 | 38.3 | 16.9 |
| LOS | C | D | A | D | D | B |
| Approach Delay (s/veh) | 30.9 | | 17.2 | | 33.1 | |
| Approach LOS | C | | B | | C | |

| Intersection Summary | |
|---|------------------------|
| Cycle Length: 120 | |
| Actuated Cycle Length: 120 | |
| Offset: 63.6 (53%), Referenced to phase 4:EBT and 8:WBL, Start of Green | |
| Natural Cycle: 60 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.70 | |
| Intersection Signal Delay (s/veh): 25.8 | Intersection LOS: C |
| Intersection Capacity Utilization 82.0% | ICU Level of Service E |
| Analysis Period (min) 15 | |

Splits and Phases: 1: Lomas Blvd NE, & I-25 SBFR



2038PB MIT - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 PM MIT Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
1: Lomas Blvd NE, & I-25 SBFR

Tierra West, LLC
01/28/2026

| | ↖ | → | ↙ | ↖ | ← | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
|------------------------------|------|------|------|------|------|------|-----|-----|-----|------|------|------|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | SBL | SBT | SBR |
| Lane Configurations | | ↑↑↑ | | ↖ | ↑↑↑ | | | | | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
| Traffic Volume (veh/h) | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 | 212 | 456 | 251 |
| Future Volume (veh/h) | 0 | 948 | 0 | 351 | 998 | 0 | 0 | 0 | 0 | 212 | 456 | 251 | 212 | 456 | 251 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | | | No | | | | | | | No | | |
| Adj Sat Flow, veh/h/ln | 0 | 1856 | 1856 | 1856 | 1856 | 0 | | | | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 0 | 948 | 0 | 351 | 998 | 0 | | | | 212 | 582 | 167 | 212 | 582 | 167 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 0 | 3 | 3 | 3 | 3 | 0 | | | | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | 515 | 1082 | 459 |
| Arrive On Green | 0.00 | 0.37 | 0.00 | 0.42 | 1.00 | 0.00 | | | | 0.29 | 0.29 | 0.29 | 0.29 | 0.29 | 0.29 |
| Sat Flow, veh/h | 0 | 5400 | 0 | 1767 | 5233 | 0 | | | | 1767 | 3711 | 1572 | 1767 | 3711 | 1572 |
| Grp Volume(v), veh/h | 0 | 948 | 0 | 351 | 998 | 0 | | | | 212 | 582 | 167 | 212 | 582 | 167 |
| Grp Sat Flow(s), veh/h/ln | 0 | 1689 | 0 | 1767 | 1689 | 0 | | | | 1767 | 1856 | 1572 | 1767 | 1856 | 1572 |
| Q Serve(g_s), s | 0.0 | 17.5 | 0.0 | 11.9 | 0.0 | 0.0 | | | | 11.6 | 15.8 | 10.1 | 11.6 | 15.8 | 10.1 |
| Cycle Q Clear(g_c), s | 0.0 | 17.5 | 0.0 | 11.9 | 0.0 | 0.0 | | | | 11.6 | 15.8 | 10.1 | 11.6 | 15.8 | 10.1 |
| Prop In Lane | 0.00 | | 0.00 | 1.00 | | 0.00 | | | | 1.00 | | 1.00 | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | 515 | 1082 | 459 |
| V/C Ratio(X) | 0.00 | 0.51 | | 0.63 | 0.32 | 0.00 | | | | 0.41 | 0.54 | 0.36 | 0.41 | 0.54 | 0.36 |
| Avail Cap(c_a), veh/h | 0 | 1857 | | 558 | 3124 | 0 | | | | 515 | 1082 | 459 | 515 | 1082 | 459 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 0.00 | 1.00 | 0.00 | 0.87 | 0.87 | 0.00 | | | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 0.0 | 29.6 | 0.0 | 11.2 | 0.0 | 0.0 | | | | 34.2 | 35.7 | 33.7 | 34.2 | 35.7 | 33.7 |
| Incr Delay (d2), s/veh | 0.0 | 1.0 | 0.0 | 4.6 | 0.2 | 0.0 | | | | 2.4 | 1.9 | 2.2 | 2.4 | 1.9 | 2.2 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 0.0 | 11.6 | 0.0 | 6.9 | 0.1 | 0.0 | | | | 9.0 | 11.8 | 7.4 | 9.0 | 11.8 | 7.4 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 0.0 | 30.6 | 0.0 | 15.9 | 0.2 | 0.0 | | | | 36.6 | 37.6 | 35.9 | 36.6 | 37.6 | 35.9 |
| LnGrp LOS | | C | | B | A | | | | | D | D | D | D | D | D |
| Approach Vol, veh/h | | 948 | | | 1349 | | | | | | | 961 | | | |
| Approach Delay, s/veh | | 30.6 | | | 4.3 | | | | | | | 37.1 | | | |
| Approach LOS | | C | | | A | | | | | | | D | | | |

| Timer - Assigned Phs | 3 | 4 | 6 | 8 |
|-----------------------------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 30.0 | 49.0 | 41.0 | 79.0 |
| Change Period (Y+Rc), s | 5.0 | 5.0 | 6.0 | 5.0 |
| Max Green Setting (Gmax), s | 25.0 | 44.0 | 35.0 | 74.0 |
| Max Q Clear Time (g_c+1), s | 13.9 | 19.5 | 17.8 | 2.0 |
| Green Ext Time (p_c), s | 0.8 | 7.0 | 4.7 | 8.8 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 21.6 |
| HCM 7th LOS | C |

Notes
User approved pedestrian interval to be less than phase max green.
User approved volume balancing among the lanes for turning movement.

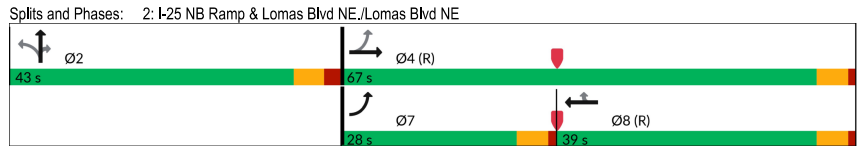
2038PB MIT - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 PM MIT Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
LoboPlaza_HY_2038.syn

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↘ | ↘↘↘ | ↘↘↘ | ↘ | ↘↘↘ | ↘↘↘ | ↘ |
| Traffic Volume (vph) | 219 | 1510 | 646 | 203 | 146 | 568 | 166 |
| Future Volume (vph) | 219 | 1510 | 646 | 203 | 146 | 568 | 166 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↘ | ↘↘↘ | | | ↘↘↘ | ↘ | ↘↘↘ | ↘↘↘ | ↘ | | | |
| Traffic Volume (veh/h) | 219 | 1510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| Future Volume (veh/h) | 219 | 1510 | 0 | 0 | 646 | 203 | 146 | 568 | 166 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 219 | 1510 | 0 | 0 | 646 | 203 | 146 | 567 | 166 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 592 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 219 | 1510 | 0 | 0 | 646 | 203 | 146 | 567 | 166 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.2 | 7.3 | 6.6 | 8.3 | 8.6 | | | |
| Cycle Q Clear(g_c), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.2 | 7.3 | 6.6 | 8.3 | 8.6 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 592 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.37 | 0.53 | 0.00 | 0.00 | 0.41 | 0.42 | 0.25 | 0.30 | 0.31 | | | |
| Avail Cap(c_a), veh/h | 592 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.64 | 0.64 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.1 | 0.0 | 0.0 | 0.0 | 15.9 | 15.9 | 26.4 | 27.0 | 27.1 | | | |
| Incr Delay (d2), s/veh | 1.1 | 0.5 | 0.0 | 0.0 | 0.8 | 2.6 | 1.0 | 0.4 | 1.5 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 3.8 | 0.2 | 0.0 | 0.0 | 4.4 | 4.5 | 5.2 | 6.6 | 6.1 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.3 | 0.5 | 0.0 | 0.0 | 16.7 | 18.5 | 27.4 | 27.4 | 28.6 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1729 | | | 849 | | | 879 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.1 | | | 27.6 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | 28.0 | 39.0 | | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | 5.0 | 5.0 | | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | 23.0 | 34.0 | | | | |
| Max Q Clear Time (g_c+1), s | | 10.6 | | | 2.0 | | 8.2 | 9.3 | | | | |
| Green Ext Time (p_c), s | | 5.0 | | | 16.8 | | 0.5 | 5.3 | | | | |

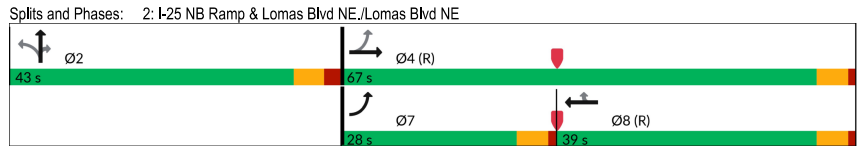
Intersection Summary
 HCM 7th Control Delay, s/veh 12.1
 HCM 7th LOS B

Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 219 | 1540 | 677 | 205 | 146 | 568 | 179 |
| Future Volume (vph) | 219 | 1540 | 677 | 205 | 146 | 568 | 179 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 2 | 2 | 2 | |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| Future Volume (veh/h) | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 567 | 179 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 567 | 179 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.7 | 7.4 | 6.6 | 8.3 | 9.4 | | | |
| Cycle Q Clear(g_c), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.7 | 7.4 | 6.6 | 8.3 | 9.4 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.37 | 0.54 | 0.00 | 0.00 | 0.43 | 0.42 | 0.25 | 0.30 | 0.34 | | | |
| Avail Cap(c_a), veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.62 | 0.62 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.2 | 0.0 | 0.0 | 0.0 | 16.0 | 15.9 | 26.4 | 27.0 | 27.3 | | | |
| Incr Delay (d2), s/veh | 1.1 | 0.5 | 0.0 | 0.0 | 0.9 | 2.7 | 1.0 | 0.4 | 1.7 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 3.8 | 0.2 | 0.0 | 0.0 | 4.6 | 4.6 | 5.2 | 6.6 | 6.7 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.3 | 0.5 | 0.0 | 0.0 | 16.8 | 18.6 | 27.4 | 27.4 | 29.1 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1759 | | | 882 | | | 892 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.3 | | | 27.7 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | |
| Max Q Clear Time (g_c+1), s | | 11.4 | | | 2.0 | | | 8.2 | 9.7 | | | |
| Green Ext Time (p_c), s | | 5.1 | | | 17.4 | | | 0.5 | 5.5 | | | |

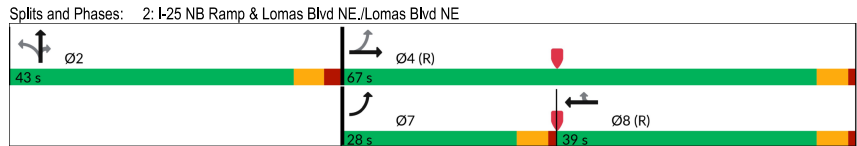
Intersection Summary
 HCM 7th Control Delay, s/veh: 12.2
 HCM 7th LOS: B

Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/15/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 219 | 1540 | 677 | 205 | 146 | 568 | 179 |
| Future Volume (vph) | 219 | 1540 | 677 | 205 | 146 | 568 | 179 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/15/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| Future Volume (veh/h) | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 568 | 179 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 567 | 179 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 219 | 1540 | 0 | 0 | 677 | 205 | 146 | 567 | 179 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.7 | 7.4 | 6.6 | 8.3 | 9.4 | | | |
| Cycle Q Clear(g_c), s | 6.2 | 0.0 | 0.0 | 0.0 | 7.7 | 7.4 | 6.6 | 8.3 | 9.4 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.37 | 0.54 | 0.00 | 0.00 | 0.43 | 0.42 | 0.25 | 0.30 | 0.34 | | | |
| Avail Cap(c_a), veh/h | 584 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.62 | 0.62 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.2 | 0.0 | 0.0 | 0.0 | 16.0 | 15.9 | 26.4 | 27.0 | 27.3 | | | |
| Incr Delay (d2), s/veh | 1.1 | 0.5 | 0.0 | 0.0 | 0.9 | 2.7 | 1.0 | 0.4 | 1.7 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 3.8 | 0.2 | 0.0 | 0.0 | 4.6 | 4.6 | 5.2 | 6.6 | 6.7 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.3 | 0.5 | 0.0 | 0.0 | 16.8 | 18.6 | 27.4 | 27.4 | 29.1 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1759 | | | 882 | | | 892 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.3 | | | 27.7 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | |
| Max Q Clear Time (g_c+1), s | | 11.4 | | | 2.0 | | | 8.2 | 9.7 | | | |
| Green Ext Time (p_c), s | | 5.1 | | | 17.4 | | | 0.5 | 5.5 | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 12.2
 HCM 7th LOS B

Notes
 User approved volume balancing among the lanes for turning movement.

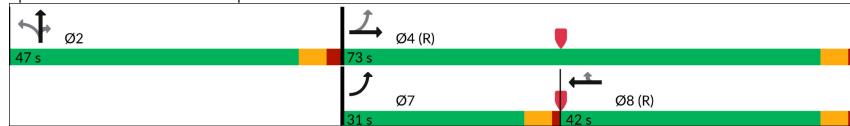
Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
09/13/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 333 | 723 | 1117 | 589 | 126 | 520 | 166 |
| Future Volume (vph) | 333 | 723 | 1117 | 589 | 126 | 520 | 166 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 31.0 | 73.0 | 42.0 | 42.0 | 47.0 | 47.0 | 47.0 |
| Total Split (%) | 25.8% | 60.8% | 35.0% | 35.0% | 39.2% | 39.2% | 39.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 75
 Control Type: Actuated-Coordinated

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|-------|------|------|------|-----|-----|-----|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 333 | 723 | 0 | 0 | 1117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| Future Volume (veh/h) | 333 | 723 | 0 | 0 | 1117 | 589 | 126 | 520 | 166 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 333 | 723 | 0 | 0 | 1117 | 589 | 126 | 519 | 166 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 488 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| Arrive On Green | 0.43 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 333 | 723 | 0 | 0 | 1117 | 589 | 126 | 519 | 166 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 12.0 | 0.0 | 0.0 | 0.0 | 18.1 | 37.0 | 6.1 | 8.1 | 9.3 | | | |
| Cycle Q Clear(g_c), s | 12.0 | 0.0 | 0.0 | 0.0 | 18.1 | 37.0 | 6.1 | 8.1 | 9.3 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 488 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| V/C Ratio(X) | 0.68 | 0.25 | 0.00 | 0.00 | 0.72 | 1.21 | 0.21 | 0.27 | 0.31 | | | |
| Avail Cap(c_a), veh/h | 488 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.89 | 0.89 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 14.4 | 0.0 | 0.0 | 0.0 | 19.4 | 23.0 | 28.0 | 28.7 | 29.1 | | | |
| Incr Delay (d2), s/veh | 6.8 | 0.2 | 0.0 | 0.0 | 2.8 | 114.4 | 0.8 | 0.4 | 1.5 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 7.5 | 0.1 | 0.0 | 0.0 | 8.8 | 35.4 | 4.8 | 6.6 | 6.7 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 21.2 | 0.2 | 0.0 | 0.0 | 22.2 | 137.4 | 28.8 | 29.0 | 30.6 | | | |
| LnGrp LOS | C | A | | | C | F | C | C | C | | | |
| Approach Vol, veh/h | 1056 | | | | 1706 | | | 811 | | | | |
| Approach Delay, s/veh | 6.8 | | | | 62.0 | | | 29.3 | | | | |
| Approach LOS | A | | | | E | | | C | | | | |
| Timer - Assigned Phs | 2 | | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 47.0 | | | 73.0 | | | 31.0 | 42.0 | | | | |
| Change Period (Y+Rc), s | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | | |
| Max Green Setting (Gmax), s | 41.0 | | | 68.0 | | | 26.0 | 37.0 | | | | |
| Max Q Clear Time (g_c+1), s | 11.3 | | | 2.0 | | | 14.0 | 39.0 | | | | |
| Green Ext Time (p_c), s | 4.7 | | | 5.8 | | | 0.8 | 0.0 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 38.3
 HCM 7th LOS: D
Notes
 User approved volume balancing among the lanes for turning movement.

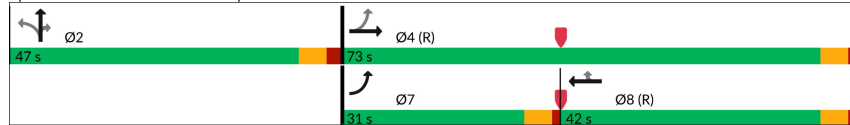
Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
09/13/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 333 | 754 | 1150 | 591 | 126 | 520 | 179 |
| Future Volume (vph) | 333 | 754 | 1150 | 591 | 126 | 520 | 179 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 31.0 | 73.0 | 42.0 | 42.0 | 47.0 | 47.0 | 47.0 |
| Total Split (%) | 25.8% | 60.8% | 35.0% | 35.0% | 39.2% | 39.2% | 39.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 75
 Control Type: Actuated-Coordinated

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|-------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| Future Volume (veh/h) | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 609 | 119 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 484 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| Arrive On Green | 0.43 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 609 | 119 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 12.0 | 0.0 | 0.0 | 0.0 | 19.1 | 37.0 | 6.1 | 9.7 | 6.5 | | | |
| Cycle Q Clear(g_c), s | 12.0 | 0.0 | 0.0 | 0.0 | 19.1 | 37.0 | 6.1 | 9.7 | 6.5 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 484 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| V/C Ratio(X) | 0.69 | 0.26 | 0.00 | 0.00 | 0.74 | 1.22 | 0.21 | 0.32 | 0.22 | | | |
| Avail Cap(c_a), veh/h | 484 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.88 | 0.88 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 14.6 | 0.0 | 0.0 | 0.0 | 19.6 | 23.0 | 28.0 | 29.2 | 28.1 | | | |
| Incr Delay (d2), s/veh | 6.9 | 0.2 | 0.0 | 0.0 | 3.1 | 116.1 | 0.8 | 0.4 | 1.0 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 7.5 | 0.1 | 0.0 | 0.0 | 9.1 | 35.8 | 4.8 | 7.8 | 4.6 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 21.5 | 0.2 | 0.0 | 0.0 | 22.7 | 139.1 | 28.8 | 29.6 | 29.1 | | | |
| LnGrp LOS | C | A | | | C | F | C | C | C | | | |
| Approach Vol, veh/h | 1087 | | | | 1741 | | | 854 | | | | |
| Approach Delay, s/veh | 6.7 | | | | 62.2 | | | 29.4 | | | | |
| Approach LOS | A | | | | E | | | C | | | | |
| Timer - Assigned Phs | 2 | | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 47.0 | | | 73.0 | | | 31.0 | 42.0 | | | | |
| Change Period (Y+Rc), s | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | | |
| Max Green Setting (Gmax), s | 41.0 | | | 68.0 | | | 26.0 | 37.0 | | | | |
| Max Q Clear Time (g_c+1), s | 11.7 | | | 2.0 | | | 14.0 | 39.0 | | | | |
| Green Ext Time (p_c), s | 5.2 | | | 6.1 | | | 0.8 | 0.0 | | | | |

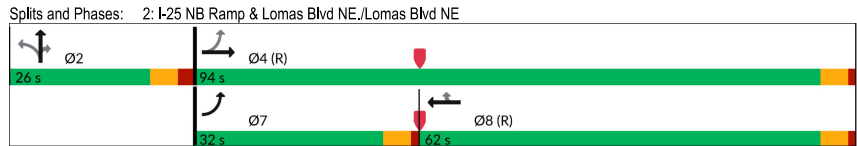
Intersection Summary
 HCM 7th Control Delay, s/veh 38.2
 HCM 7th LOS D

Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/15/2025

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|----------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↩ | ↩↩↩ | ↩↩↩ | ↩ | ↩↩↩ | ↩↩↩ | ↩ |
| Traffic Volume (vph) | 333 | 754 | 1150 | 591 | 126 | 520 | 179 |
| Future Volume (vph) | 333 | 754 | 1150 | 591 | 126 | 520 | 179 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 32.0 | 94.0 | 62.0 | 62.0 | 26.0 | 26.0 | 26.0 |
| Total Split (%) | 26.7% | 78.3% | 51.7% | 51.7% | 21.7% | 21.7% | 21.7% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 75
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
09/15/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↩ | ↩↩↩ | | | ↩↩↩ | ↩ | ↩↩↩ | ↩↩↩ | ↩ | | | |
| Traffic Volume (veh/h) | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| Future Volume (veh/h) | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 520 | 179 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | | No | | | | | | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 609 | 119 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 583 | 3757 | 0 | 0 | 2406 | 747 | 295 | 928 | 262 | | | |
| Arrive On Green | 0.45 | 1.00 | 0.00 | 0.00 | 0.95 | 0.95 | 0.17 | 0.17 | 0.17 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 333 | 754 | 0 | 0 | 1150 | 591 | 126 | 609 | 119 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 7.3 | 0.0 | 0.0 | 0.0 | 2.5 | 9.1 | 7.7 | 12.3 | 8.2 | | | |
| Cycle Q Clear(g_c), s | 7.3 | 0.0 | 0.0 | 0.0 | 2.5 | 9.1 | 7.7 | 12.3 | 8.2 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 583 | 3757 | 0 | 0 | 2406 | 747 | 295 | 928 | 262 | | | |
| V/C Ratio(X) | 0.57 | 0.20 | 0.00 | 0.00 | 0.48 | 0.79 | 0.43 | 0.66 | 0.45 | | | |
| Avail Cap(c_a), veh/h | 583 | 3757 | 0 | 0 | 2406 | 747 | 295 | 928 | 262 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.88 | 0.88 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 4.6 | 0.0 | 0.0 | 0.0 | 1.6 | 1.8 | 44.9 | 46.8 | 45.1 | | | |
| Incr Delay (d2), s/veh | 3.6 | 0.1 | 0.0 | 0.0 | 0.7 | 8.4 | 4.5 | 3.6 | 5.6 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 4.1 | 0.1 | 0.0 | 0.0 | 1.2 | 4.6 | 6.7 | 9.9 | 6.4 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 8.1 | 0.1 | 0.0 | 0.0 | 2.3 | 10.2 | 49.4 | 50.4 | 50.7 | | | |
| LnGrp LOS | A | A | | | A | B | D | D | D | | | |
| Approach Vol, veh/h | | 1087 | | | 1741 | | | 854 | | | | |
| Approach Delay, s/veh | | 2.6 | | | 5.0 | | | 50.3 | | | | |
| Approach LOS | | A | | | A | | | D | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 26.0 | | | 94.0 | | | 32.0 | 62.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 20.0 | | | 89.0 | | | 27.0 | 57.0 | | | |
| Max Q Clear Time (g_c+1), s | | 14.3 | | | 2.0 | | | 9.3 | 11.1 | | | |
| Green Ext Time (p_c), s | | 2.4 | | | 6.1 | | | 0.9 | 15.0 | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 14.8
 HCM 7th LOS B

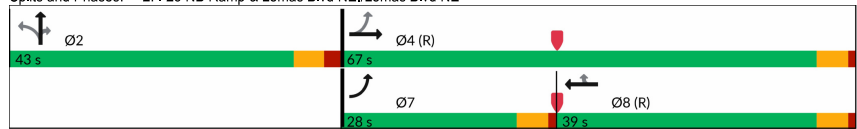
Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 230 | 1585 | 677 | 213 | 153 | 596 | 175 |
| Future Volume (vph) | 230 | 1585 | 677 | 213 | 153 | 596 | 175 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 2 | 2 | 2 | |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 62.0 | 62.0 | 34.0 | 34.0 | 37.0 | 37.0 | 37.0 |
| Actuated g/C Ratio | 0.56 | 0.56 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.43 | 0.56 | 0.44 | 0.35 | 0.27 | 0.42 | 0.31 |
| Control Delay (s/veh) | 22.7 | 22.1 | 22.7 | 4.4 | 28.5 | 29.1 | 16.9 |
| Queue Delay | 0.0 | 0.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 22.7 | 22.4 | 22.7 | 4.4 | 28.5 | 29.1 | 16.9 |
| LOS | C | C | C | A | C | C | B |
| Approach Delay (s/veh) | | 22.5 | 18.3 | | | 26.9 | |
| Approach LOS | | C | B | | | C | |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.56
 Intersection Signal Delay (s/veh): 22.6
 Intersection LOS: C
 Intersection Capacity Utilization 70.3%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 230 | 1585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| Future Volume (veh/h) | 230 | 1585 | 0 | 0 | 677 | 213 | 153 | 596 | 175 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 230 | 1585 | 0 | 0 | 677 | 213 | 153 | 597 | 175 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 583 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 230 | 1585 | 0 | 0 | 677 | 213 | 153 | 597 | 175 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.6 | 0.0 | 0.0 | 0.0 | 7.7 | 7.8 | 6.9 | 8.8 | 9.1 | | | |
| Cycle Q Clear(g_c), s | 6.6 | 0.0 | 0.0 | 0.0 | 7.7 | 7.8 | 6.9 | 8.8 | 9.1 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 583 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.39 | 0.56 | 0.00 | 0.00 | 0.43 | 0.44 | 0.26 | 0.32 | 0.33 | | | |
| Avail Cap(c_a), veh/h | 583 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.59 | 0.59 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.3 | 0.0 | 0.0 | 0.0 | 16.0 | 16.0 | 26.5 | 27.1 | 27.3 | | | |
| Incr Delay (d2), s/veh | 1.2 | 0.5 | 0.0 | 0.0 | 0.9 | 2.9 | 1.0 | 0.4 | 1.7 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 4.0 | 0.2 | 0.0 | 0.0 | 4.6 | 4.8 | 5.5 | 7.0 | 6.5 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.4 | 0.5 | 0.0 | 0.0 | 16.8 | 18.9 | 27.6 | 27.6 | 28.9 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | 1815 | | | | 890 | | | 925 | | | | |
| Approach Delay, s/veh | 1.9 | | | | 17.3 | | | 27.8 | | | | |
| Approach LOS | A | | | | B | | | C | | | | |
| Timer - Assigned Phs | 2 | | | 4 | | | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | | |
| Change Period (Y+Rc), s | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | | |
| Max Green Setting (Gmax), s | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | | |
| Max Q Clear Time (g_c+1), s | 11.1 | | | 2.0 | | | 8.6 | 9.8 | | | | |
| Green Ext Time (p_c), s | 5.3 | | | 18.3 | | | 0.5 | 5.6 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 12.3
 HCM 7th LOS B

Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 230 | 1615 | 708 | 215 | 153 | 596 | 188 |
| Future Volume (vph) | 230 | 1615 | 708 | 215 | 153 | 596 | 188 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 2 | 2 | 2 | |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 62.0 | 62.0 | 34.0 | 34.0 | 37.0 | 37.0 | 37.0 |
| Actuated g/C Ratio | 0.56 | 0.56 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.43 | 0.57 | 0.46 | 0.36 | 0.27 | 0.42 | 0.34 |
| Control Delay (s/veh) | 23.5 | 22.7 | 23.6 | 5.3 | 28.5 | 29.1 | 18.0 |
| Queue Delay | 0.0 | 0.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 23.5 | 23.1 | 23.6 | 5.3 | 28.5 | 29.1 | 18.0 |
| LOS | C | C | C | A | C | C | B |
| Approach Delay (s/veh) | | 23.1 | 19.3 | | | 27.0 | |
| Approach LOS | | C | B | | | C | |

Intersection Summary

Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.57
 Intersection Signal Delay (s/veh): 23.2
 Intersection LOS: C
 Intersection Capacity Utilization 70.9%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| Future Volume (veh/h) | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 597 | 188 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 597 | 188 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.6 | 0.0 | 0.0 | 0.0 | 8.1 | 7.9 | 6.9 | 8.8 | 9.9 | | | |
| Cycle Q Clear(g_c), s | 6.6 | 0.0 | 0.0 | 0.0 | 8.1 | 7.9 | 6.9 | 8.8 | 9.9 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.40 | 0.57 | 0.00 | 0.00 | 0.45 | 0.44 | 0.26 | 0.32 | 0.36 | | | |
| Avail Cap(c_a), veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.57 | 0.57 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.3 | 0.0 | 0.0 | 0.0 | 16.1 | 16.0 | 26.5 | 27.1 | 27.5 | | | |
| Incr Delay (d2), s/veh | 1.2 | 0.5 | 0.0 | 0.0 | 0.9 | 2.9 | 1.0 | 0.4 | 1.9 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 4.0 | 0.2 | 0.0 | 0.0 | 4.8 | 4.9 | 5.5 | 7.0 | 7.1 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.5 | 0.5 | 0.0 | 0.0 | 17.0 | 18.9 | 27.6 | 27.6 | 29.4 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1845 | | | 923 | | | 938 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.5 | | | 27.9 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | |
| Max Q Clear Time (g_c+1), s | | 11.9 | | | 2.0 | | | 8.6 | 10.1 | | | |
| Green Ext Time (p_c), s | | 5.4 | | | 19.0 | | | 0.5 | 5.8 | | | |

Intersection Summary

HCM 7th Control Delay, s/veh 12.3
 HCM 7th LOS B

Notes

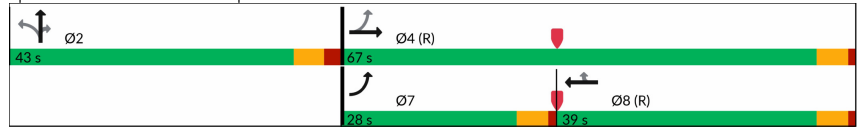
User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 230 | 1615 | 708 | 215 | 153 | 596 | 188 |
| Future Volume (vph) | 230 | 1615 | 708 | 215 | 153 | 596 | 188 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 2 | 2 | 2 | |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 28.0 | 67.0 | 39.0 | 39.0 | 43.0 | 43.0 | 43.0 |
| Total Split (%) | 25.5% | 60.9% | 35.5% | 35.5% | 39.1% | 39.1% | 39.1% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 62.0 | 62.0 | 34.0 | 34.0 | 37.0 | 37.0 | 37.0 |
| Actuated g/C Ratio | 0.56 | 0.56 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.43 | 0.57 | 0.46 | 0.36 | 0.27 | 0.42 | 0.34 |
| Control Delay (s/veh) | 23.5 | 22.7 | 23.3 | 5.4 | 28.5 | 29.1 | 18.0 |
| Queue Delay | 0.0 | 0.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 23.5 | 23.1 | 23.3 | 5.4 | 28.5 | 29.1 | 18.0 |
| LOS | C | C | C | A | C | C | B |
| Approach Delay (s/veh) | | 23.1 | 19.1 | | | 27.0 | |
| Approach LOS | | C | B | | | C | |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 45 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.57
 Intersection Signal Delay (s/veh): 23.1
 Intersection LOS: C
 Intersection Capacity Utilization 70.9%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE
 Tierra West, LLC
 01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| Future Volume (veh/h) | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 596 | 188 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 597 | 188 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 230 | 1615 | 0 | 0 | 708 | 215 | 153 | 597 | 188 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 6.6 | 0.0 | 0.0 | 0.0 | 8.1 | 7.9 | 6.9 | 8.8 | 9.9 | | | |
| Cycle Q Clear(g_c), s | 6.6 | 0.0 | 0.0 | 0.0 | 8.1 | 7.9 | 6.9 | 8.8 | 9.9 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| V/C Ratio(X) | 0.40 | 0.57 | 0.00 | 0.00 | 0.45 | 0.44 | 0.26 | 0.32 | 0.36 | | | |
| Avail Cap(c_a), veh/h | 576 | 2855 | 0 | 0 | 1566 | 486 | 594 | 1872 | 529 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.57 | 0.57 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 10.3 | 0.0 | 0.0 | 0.0 | 16.1 | 16.0 | 26.5 | 27.1 | 27.5 | | | |
| Incr Delay (d2), s/veh | 1.2 | 0.5 | 0.0 | 0.0 | 0.9 | 2.9 | 1.0 | 0.4 | 1.9 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 4.0 | 0.2 | 0.0 | 0.0 | 4.8 | 4.9 | 5.5 | 7.0 | 7.1 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 11.5 | 0.5 | 0.0 | 0.0 | 17.0 | 18.9 | 27.6 | 27.6 | 29.4 | | | |
| LnGrp LOS | B | A | | | B | B | C | C | C | | | |
| Approach Vol, veh/h | | 1845 | | | 923 | | | 938 | | | | |
| Approach Delay, s/veh | | 1.8 | | | 17.5 | | | 27.9 | | | | |
| Approach LOS | | A | | | B | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 43.0 | | | 67.0 | | | 28.0 | 39.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 37.0 | | | 62.0 | | | 23.0 | 34.0 | | | |
| Max Q Clear Time (g_c+1), s | | 11.9 | | | 2.0 | | | 8.6 | 10.1 | | | |
| Green Ext Time (p_c), s | | 5.4 | | | 19.0 | | | 0.5 | 5.8 | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 12.3
 HCM 7th LOS B

Notes
 User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 349 | 758 | 1172 | 618 | 132 | 545 | 175 |
| Future Volume (vph) | 349 | 758 | 1172 | 618 | 132 | 545 | 175 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | 2 | 2 | 2 | |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 31.0 | 73.0 | 42.0 | 42.0 | 47.0 | 47.0 | 47.0 |
| Total Split (%) | 25.8% | 60.8% | 35.0% | 35.0% | 39.2% | 39.2% | 39.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 68.0 | 68.0 | 37.0 | 37.0 | 41.0 | 41.0 | 41.0 |
| Actuated g/C Ratio | 0.57 | 0.57 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.79 | 0.27 | 0.76 | 1.05 | 0.23 | 0.38 | 0.28 |
| Control Delay (s/veh) | 62.8 | 20.2 | 28.5 | 64.9 | 29.8 | 30.5 | 5.6 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 62.8 | 20.2 | 28.5 | 64.9 | 29.8 | 30.5 | 5.6 |
| LOS | E | C | C | E | C | C | A |
| Approach Delay (s/veh) | | 33.6 | 41.1 | | | 25.8 | |
| Approach LOS | | C | D | | | C | |

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 80
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.05
 Intersection Signal Delay (s/veh): 35.4
 Intersection LOS: D
 Intersection Capacity Utilization 81.8%
 ICU Level of Service D
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



2038PN - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour NO BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
 01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|-------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 349 | 758 | 0 | 0 | 1172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| Future Volume (veh/h) | 349 | 758 | 0 | 0 | 1172 | 618 | 132 | 545 | 175 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | No | No | No | No | No | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 349 | 758 | 0 | 0 | 1172 | 618 | 132 | 546 | 175 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 480 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| Arrive On Green | 0.43 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 349 | 758 | 0 | 0 | 1172 | 618 | 132 | 546 | 175 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 13.0 | 0.0 | 0.0 | 0.0 | 19.8 | 37.0 | 6.4 | 8.6 | 9.9 | | | |
| Cycle Q Clear(g_c), s | 13.0 | 0.0 | 0.0 | 0.0 | 19.8 | 37.0 | 6.4 | 8.6 | 9.9 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 480 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| V/C Ratio(X) | 0.73 | 0.26 | 0.00 | 0.00 | 0.75 | 1.27 | 0.22 | 0.29 | 0.33 | | | |
| Avail Cap(c_a), veh/h | 480 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.87 | 0.87 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 14.9 | 0.0 | 0.0 | 0.0 | 19.7 | 23.0 | 28.1 | 28.8 | 29.3 | | | |
| Incr Delay (d2), s/veh | 8.1 | 0.2 | 0.0 | 0.0 | 3.4 | 138.9 | 0.8 | 0.4 | 1.6 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 7.9 | 0.1 | 0.0 | 0.0 | 9.3 | 40.7 | 5.1 | 7.0 | 7.1 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 23.0 | 0.2 | 0.0 | 0.0 | 23.1 | 161.9 | 28.9 | 29.2 | 30.9 | | | |
| LnGrp LOS | C | A | | | C | F | C | C | C | | | |
| Approach Vol, veh/h | | 1107 | | | 1790 | | | 853 | | | | |
| Approach Delay, s/veh | | 7.4 | | | 71.0 | | | 29.5 | | | | |
| Approach LOS | | A | | | E | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 47.0 | | | 73.0 | | | 31.0 | 42.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 41.0 | | | 68.0 | | | 26.0 | 37.0 | | | |
| Max Q Clear Time (g_c+1), s | | 11.9 | | | 2.0 | | | 15.0 | 39.0 | | | |
| Green Ext Time (p_c), s | | 4.9 | | | 6.1 | | | 0.8 | 0.0 | | | |

Intersection Summary

HCM 7th Control Delay, s/veh 42.8
 HCM 7th LOS D

Notes

User approved volume balancing among the lanes for turning movement.

2038PN - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour NO BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ |
| Traffic Volume (vph) | 349 | 789 | 1205 | 620 | 132 | 545 | 188 |
| Future Volume (vph) | 349 | 789 | 1205 | 620 | 132 | 545 | 188 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 31.0 | 73.0 | 42.0 | 42.0 | 47.0 | 47.0 | 47.0 |
| Total Split (%) | 25.8% | 60.8% | 35.0% | 35.0% | 39.2% | 39.2% | 39.2% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 68.0 | 68.0 | 37.0 | 37.0 | 41.0 | 41.0 | 41.0 |
| Actuated g/C Ratio | 0.57 | 0.57 | 0.31 | 0.31 | 0.34 | 0.34 | 0.34 |
| v/c Ratio | 0.79 | 0.28 | 0.78 | 1.06 | 0.23 | 0.38 | 0.29 |
| Control Delay (s/veh) | 62.6 | 21.0 | 29.6 | 66.7 | 29.8 | 30.5 | 5.6 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 62.6 | 21.0 | 29.6 | 66.7 | 29.8 | 30.5 | 5.6 |
| LOS | E | C | C | E | C | C | A |
| Approach Delay (s/veh) | | 33.8 | 42.2 | | | 25.7 | |
| Approach LOS | | C | D | | | C | |

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 80
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.06
 Intersection Signal Delay (s/veh): 36.0
 Intersection LOS: D
 Intersection Capacity Utilization 82.0%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|-------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔↔↔ | ↔ | | | |
| Traffic Volume (veh/h) | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| Future Volume (veh/h) | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | No | No | No | No | No | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 640 | 125 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 477 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| Arrive On Green | 0.43 | 1.00 | 0.00 | 0.00 | 0.62 | 0.62 | 0.34 | 0.34 | 0.34 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 640 | 125 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 13.0 | 0.0 | 0.0 | 0.0 | 20.9 | 37.0 | 6.4 | 10.3 | 6.8 | | | |
| Cycle Q Clear(g_c), s | 13.0 | 0.0 | 0.0 | 0.0 | 20.9 | 37.0 | 6.4 | 10.3 | 6.8 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 477 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| V/C Ratio(X) | 0.73 | 0.27 | 0.00 | 0.00 | 0.77 | 1.28 | 0.22 | 0.34 | 0.23 | | | |
| Avail Cap(c_a), veh/h | 477 | 2871 | 0 | 0 | 1562 | 485 | 604 | 1902 | 537 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.86 | 0.86 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 15.1 | 0.0 | 0.0 | 0.0 | 19.9 | 23.0 | 28.1 | 29.4 | 28.2 | | | |
| Incr Delay (d2), s/veh | 8.3 | 0.2 | 0.0 | 0.0 | 3.8 | 140.6 | 0.8 | 0.5 | 1.0 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 7.9 | 0.1 | 0.0 | 0.0 | 9.6 | 41.1 | 5.1 | 8.2 | 4.9 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 23.4 | 0.2 | 0.0 | 0.0 | 23.7 | 163.6 | 28.9 | 29.9 | 29.3 | | | |
| LnGrp LOS | C | A | | | C | F | C | C | C | | | |
| Approach Vol, veh/h | | 1138 | | | 1825 | | | 897 | | | | |
| Approach Delay, s/veh | | 7.3 | | | 71.2 | | | 29.6 | | | | |
| Approach LOS | | A | | | E | | | C | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 47.0 | | | 73.0 | | | 31.0 | 42.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 41.0 | | | 68.0 | | | 26.0 | 37.0 | | | |
| Max Q Clear Time (g_c+1), s | | 12.3 | | | 2.0 | | | 15.0 | 39.0 | | | |
| Green Ext Time (p_c), s | | 5.5 | | | 6.4 | | | 0.8 | 0.0 | | | |

Intersection Summary

HCM 7th Control Delay, s/veh 42.7
 HCM 7th LOS D

Notes

User approved volume balancing among the lanes for turning movement.

Timings
2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

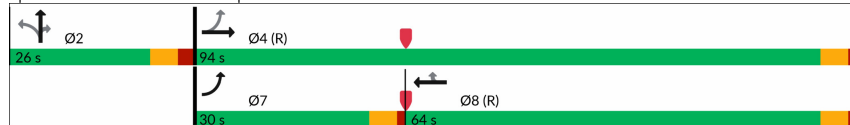
Tierra West, LLC
01/28/2026

| | EBL | EBT | WBT | WBR | NBL | NBT | NBR |
|------------------------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔↔↔ | ↔↔↔ | ↔ | ↔↔↔ | ↔ | ↔ |
| Traffic Volume (vph) | 349 | 789 | 1205 | 620 | 132 | 545 | 188 |
| Future Volume (vph) | 349 | 789 | 1205 | 620 | 132 | 545 | 188 |
| Turn Type | pm+pt | NA | NA | Perm | Perm | NA | Perm |
| Protected Phases | 7 | 4 | 8 | | | 2 | |
| Permitted Phases | 4 | | 8 | | 2 | | 2 |
| Detector Phase | 7 | 4 | 8 | 8 | 2 | 2 | 2 |
| Switch Phase | | | | | | | |
| Minimum Initial (s) | 3.0 | 16.0 | 16.0 | 16.0 | 8.0 | 8.0 | 8.0 |
| Minimum Split (s) | 9.5 | 32.0 | 23.0 | 23.0 | 24.0 | 24.0 | 24.0 |
| Total Split (s) | 30.0 | 94.0 | 64.0 | 64.0 | 26.0 | 26.0 | 26.0 |
| Total Split (%) | 25.0% | 78.3% | 53.3% | 53.3% | 21.7% | 21.7% | 21.7% |
| Yellow Time (s) | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| All-Red Time (s) | 1.0 | 1.0 | 1.0 | 1.0 | 2.0 | 2.0 | 2.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 5.0 | 5.0 | 5.0 | 5.0 | 6.0 | 6.0 | 6.0 |
| Lead/Lag | Lead | | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | | Yes | Yes | | | |
| Recall Mode | Max | C-Max | C-Max | C-Max | Max | Max | Max |
| Act Effct Green (s) | 89.0 | 89.0 | 59.0 | 59.0 | 20.0 | 20.0 | 20.0 |
| Actuated g/C Ratio | 0.74 | 0.74 | 0.49 | 0.49 | 0.17 | 0.17 | 0.17 |
| v/c Ratio | 0.68 | 0.21 | 0.49 | 0.74 | 0.47 | 0.78 | 0.46 |
| Control Delay (s/veh) | 46.0 | 3.8 | 14.7 | 15.2 | 52.3 | 55.8 | 10.9 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 46.0 | 3.8 | 14.7 | 15.2 | 52.3 | 55.8 | 10.9 |
| LOS | D | A | B | B | D | E | B |
| Approach Delay (s/veh) | | 16.7 | 14.9 | | | 46.8 | |
| Approach LOS | | B | B | | | D | |

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 49 (41%), Referenced to phase 4:EBTL and 8:WBT, Start of Green
 Natural Cycle: 80
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.78
 Intersection Signal Delay (s/veh): 22.6
 Intersection LOS: C
 Intersection Capacity Utilization 82.0%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE



HCM 7th Signalized Intersection Summary
 2: I-25 NB Ramp & Lomas Blvd NE./Lomas Blvd NE

Tierra West, LLC
 01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|-----|-----|-----|
| Lane Configurations | ↔ | ↔↔↔ | | | ↔↔↔ | ↔ | ↔↔↔ | ↔ | ↔ | | | |
| Traffic Volume (veh/h) | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| Future Volume (veh/h) | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 545 | 188 | 0 | 0 | 0 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 0 | 0 | 1856 | 1856 | 1856 | 1856 | 1856 | | | |
| Adj Flow Rate, veh/h | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 640 | 125 | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Percent Heavy Veh. % | 3 | 3 | 0 | 0 | 3 | 3 | 3 | 3 | 3 | | | |
| Cap, veh/h | 551 | 3757 | 0 | 0 | 2491 | 773 | 295 | 928 | 262 | | | |
| Arrive On Green | 0.42 | 1.00 | 0.00 | 0.00 | 0.98 | 0.98 | 0.17 | 0.17 | 0.17 | | | |
| Sat Flow, veh/h | 1767 | 5233 | 0 | 0 | 5233 | 1572 | 1767 | 5567 | 1572 | | | |
| Grp Volume(v), veh/h | 349 | 789 | 0 | 0 | 1205 | 620 | 132 | 640 | 125 | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 0 | 0 | 1689 | 1572 | 1767 | 1856 | 1572 | | | |
| Q Serve(g_s), s | 8.2 | 0.0 | 0.0 | 0.0 | 0.9 | 3.7 | 8.1 | 13.0 | 8.6 | | | |
| Cycle Q Clear(g_c), s | 8.2 | 0.0 | 0.0 | 0.0 | 0.9 | 3.7 | 8.1 | 13.0 | 8.6 | | | |
| Prop In Lane | 1.00 | | 0.00 | 0.00 | | 1.00 | 1.00 | | 1.00 | | | |
| Lane Grp Cap(c), veh/h | 551 | 3757 | 0 | 0 | 2491 | 773 | 295 | 928 | 262 | | | |
| V/C Ratio(X) | 0.63 | 0.21 | 0.00 | 0.00 | 0.48 | 0.80 | 0.45 | 0.69 | 0.48 | | | |
| Avail Cap(c_a), veh/h | 551 | 3757 | 0 | 0 | 2491 | 773 | 295 | 928 | 262 | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 1.00 | 1.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | | | |
| Upstream Filter(I) | 0.86 | 0.86 | 0.00 | 0.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | |
| Uniform Delay (d), s/veh | 5.6 | 0.0 | 0.0 | 0.0 | 0.5 | 0.5 | 45.0 | 47.1 | 45.3 | | | |
| Incr Delay (d2), s/veh | 4.7 | 0.1 | 0.0 | 0.0 | 0.7 | 8.6 | 4.9 | 4.2 | 6.1 | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| %ile BackOfQ(95%),veh/ln | 4.7 | 0.1 | 0.0 | 0.0 | 0.6 | 3.8 | 7.0 | 10.4 | 6.8 | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 10.3 | 0.1 | 0.0 | 0.0 | 1.2 | 9.1 | 49.9 | 51.3 | 51.4 | | | |
| LnGrp LOS | B | A | | | A | A | D | D | D | | | |
| Approach Vol, veh/h | | 1138 | | | 1825 | | | 897 | | | | |
| Approach Delay, s/veh | | 3.2 | | | 3.9 | | | 51.1 | | | | |
| Approach LOS | | A | | | A | | | D | | | | |
| Timer - Assigned Phs | | 2 | | | 4 | | | 7 | 8 | | | |
| Phs Duration (G+Y+Rc), s | | 26.0 | | | 94.0 | | | 30.0 | 64.0 | | | |
| Change Period (Y+Rc), s | | 6.0 | | | 5.0 | | | 5.0 | 5.0 | | | |
| Max Green Setting (Gmax), s | | 20.0 | | | 89.0 | | | 25.0 | 59.0 | | | |
| Max Q Clear Time (g_c+1), s | | 15.0 | | | 2.0 | | | 10.2 | 5.7 | | | |
| Green Ext Time (p_c), s | | 2.2 | | | 6.5 | | | 0.9 | 16.8 | | | |

Intersection Summary

HCM 7th Control Delay, s/veh 14.7
 HCM 7th LOS B

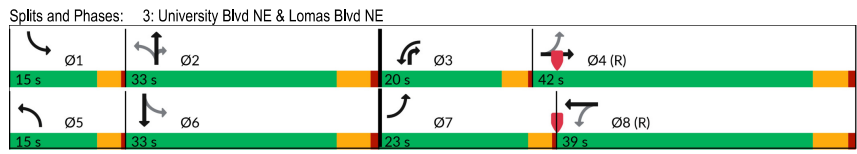
Notes

User approved volume balancing among the lanes for turning movement.

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (vph) | 276 | 873 | 183 | 593 | 97 | 471 | 183 | 114 | 759 |
| Future Volume (vph) | 276 | 873 | 183 | 593 | 97 | 471 | 183 | 114 | 759 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 2 | 6 | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 23.0 | 42.0 | 20.0 | 39.0 | 15.0 | 33.0 | 20.0 | 15.0 | 33.0 |
| Total Split (%) | 20.9% | 38.2% | 18.2% | 35.5% | 13.6% | 30.0% | 18.2% | 13.6% | 30.0% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 95
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE
 Tierra West, LLC
 09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (veh/h) | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| Future Volume (veh/h) | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 276 | 873 | 467 | 183 | 593 | 195 | 97 | 471 | 183 | 114 | 759 | 61 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 461 | 1482 | 690 | 268 | 1578 | 507 | 189 | 881 | 517 | 283 | 853 | 69 |
| Arrive On Green | 0.04 | 0.14 | 0.14 | 0.08 | 0.42 | 0.42 | 0.06 | 0.25 | 0.25 | 0.06 | 0.26 | 0.26 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3793 | 1220 | 1767 | 3526 | 1572 | 1767 | 3305 | 266 |
| Grp Volume(v), veh/h | 276 | 873 | 467 | 183 | 526 | 262 | 97 | 471 | 183 | 114 | 405 | 415 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1636 | 1767 | 1763 | 1572 | 1767 | 1763 | 1808 |
| Q Serve(g_s), s | 9.2 | 26.6 | 31.0 | 6.4 | 11.9 | 12.2 | 4.4 | 12.7 | 9.7 | 5.2 | 24.3 | 24.3 |
| Cycle Q Clear(g_c), s | 9.2 | 26.6 | 31.0 | 6.4 | 11.9 | 12.2 | 4.4 | 12.7 | 9.7 | 5.2 | 24.3 | 24.3 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.75 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.15 |
| Lane Grp Cap(c), veh/h | 461 | 1482 | 690 | 268 | 1405 | 680 | 189 | 881 | 517 | 283 | 455 | 467 |
| V/C Ratio(X) | 0.60 | 0.59 | 0.68 | 0.68 | 0.37 | 0.38 | 0.51 | 0.53 | 0.35 | 0.40 | 0.89 | 0.89 |
| Avail Cap(c_a), veh/h | 587 | 1482 | 690 | 386 | 1405 | 680 | 275 | 881 | 517 | 354 | 455 | 467 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 16.9 | 37.8 | 39.6 | 22.2 | 22.2 | 22.3 | 30.9 | 35.7 | 28.0 | 28.5 | 39.3 | 39.3 |
| Incr Delay (d2), s/veh | 1.3 | 1.7 | 5.3 | 3.0 | 0.8 | 1.6 | 2.1 | 2.3 | 1.9 | 0.9 | 22.1 | 21.7 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 7.5 | 18.1 | 20.2 | 4.9 | 8.3 | 8.5 | 3.5 | 9.6 | 7.0 | 4.0 | 19.0 | 19.4 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 18.1 | 39.5 | 44.9 | 25.2 | 23.0 | 24.0 | 33.1 | 38.0 | 29.9 | 29.4 | 61.4 | 61.0 |
| LnGrp LOS | B | D | D | C | C | C | C | D | C | C | E | E |
| Approach Vol, veh/h | | 1616 | | | 971 | | | 751 | | | 934 | |
| Approach Delay, s/veh | | 37.4 | | | 23.7 | | | 35.4 | | | 57.3 | |
| Approach LOS | | D | | | C | | | D | | | E | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 10.6 | 33.0 | 12.7 | 53.8 | 9.7 | 33.9 | 15.2 | 51.3 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 11.5 | 27.5 | 16.0 | 36.5 | 11.5 | 27.5 | 19.5 | 33.5 | | | | |
| Max Q Clear Time (g_c+1), s | 7.2 | 14.7 | 8.4 | 33.0 | 6.4 | 26.3 | 11.2 | 14.2 | | | | |
| Green Ext Time (p_c), s | 0.1 | 3.0 | 0.3 | 2.5 | 0.1 | 0.6 | 0.5 | 5.0 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 38.3
 HCM 7th LOS: D
Notes
 User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
09/13/2025

| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (vph) | 281 | 894 | 183 | 620 | 110 | 471 | 183 | 114 | 759 |
| Future Volume (vph) | 281 | 894 | 183 | 620 | 110 | 471 | 183 | 114 | 759 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 6 | 2 | 6 | 2 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 23.0 | 42.0 | 20.0 | 39.0 | 15.0 | 33.0 | 20.0 | 15.0 | 33.0 |
| Total Split (%) | 20.9% | 38.2% | 18.2% | 35.5% | 13.6% | 30.0% | 18.2% | 13.6% | 30.0% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary

Cycle Length: 110

Actuated Cycle Length: 110

Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green

Natural Cycle: 95

Control Type: Actuated-Coordinated

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
09/13/2025

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (veh/h) | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Future Volume (veh/h) | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 454 | 1481 | 690 | 264 | 1590 | 490 | 193 | 881 | 517 | 283 | 825 | 73 |
| Arrive On Green | 0.04 | 0.14 | 0.14 | 0.08 | 0.41 | 0.41 | 0.06 | 0.25 | 0.25 | 0.06 | 0.25 | 0.25 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3837 | 1183 | 1767 | 3526 | 1572 | 1767 | 3277 | 289 |
| Grp Volume(v), veh/h | 281 | 894 | 477 | 183 | 544 | 271 | 110 | 471 | 183 | 114 | 408 | 418 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1643 | 1767 | 1763 | 1572 | 1767 | 1763 | 1803 |
| Q Serve(g_s), s | 9.3 | 27.3 | 31.7 | 6.4 | 12.4 | 12.7 | 5.0 | 12.7 | 9.7 | 5.2 | 24.8 | 24.8 |
| Cycle Q Clear(g_c), s | 9.3 | 27.3 | 31.7 | 6.4 | 12.4 | 12.7 | 5.0 | 12.7 | 9.7 | 5.2 | 24.8 | 24.8 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.72 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.16 |
| Lane Grp Cap(c), veh/h | 454 | 1481 | 690 | 264 | 1400 | 681 | 193 | 881 | 517 | 283 | 444 | 454 |
| V/C Ratio(X) | 0.62 | 0.60 | 0.69 | 0.69 | 0.39 | 0.40 | 0.57 | 0.53 | 0.35 | 0.40 | 0.92 | 0.92 |
| Avail Cap(c_a), veh/h | 577 | 1481 | 690 | 382 | 1400 | 681 | 267 | 881 | 517 | 354 | 444 | 454 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 17.1 | 38.1 | 40.0 | 22.5 | 22.5 | 22.6 | 30.9 | 35.7 | 28.0 | 28.5 | 40.1 | 40.1 |
| Incr Delay (d2), s/veh | 1.4 | 1.8 | 5.6 | 3.3 | 0.8 | 1.7 | 2.6 | 2.3 | 1.9 | 0.9 | 26.6 | 26.3 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 7.6 | 18.5 | 20.6 | 5.0 | 8.6 | 8.8 | 4.0 | 9.6 | 7.0 | 4.0 | 19.9 | 20.2 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 18.5 | 39.9 | 45.6 | 25.8 | 23.3 | 24.3 | 33.6 | 38.0 | 29.9 | 29.4 | 66.7 | 66.4 |
| LnGrp LOS | B | D | D | C | C | C | C | D | C | C | E | E |
| Approach Vol, veh/h | | 1652 | | | 998 | | | 764 | | | 940 | |
| Approach Delay, s/veh | | 37.9 | | | 24.0 | | | 35.4 | | | 62.0 | |
| Approach LOS | | D | | | C | | | D | | | E | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 10.6 | 33.0 | 12.7 | 53.8 | 10.4 | 33.2 | 15.3 | 51.1 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 11.5 | 27.5 | 16.0 | 36.5 | 11.5 | 27.5 | 19.5 | 33.5 | | | | |
| Max Q Clear Time (g_c+I1), s | 7.2 | 14.7 | 8.4 | 33.7 | 7.0 | 26.8 | 11.3 | 14.7 | | | | |
| Green Ext Time (p_c), s | 0.1 | 3.0 | 0.3 | 2.1 | 0.1 | 0.4 | 0.5 | 5.1 | | | | |

Intersection Summary


HCM 7th Control Delay, s/veh: 39.5

HCM 7th LOS: D

Notes

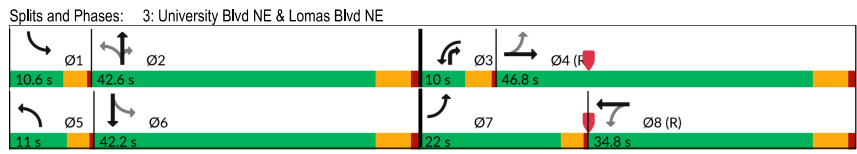
User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
09/15/2025




| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ |
| Traffic Volume (vph) | 281 | 894 | 183 | 620 | 110 | 471 | 183 | 114 | 759 |
| Future Volume (vph) | 281 | 894 | 183 | 620 | 110 | 471 | 183 | 114 | 759 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 6 | 2 | 2 | 6 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 22.0 | 46.8 | 10.0 | 34.8 | 11.0 | 42.6 | 10.0 | 10.6 | 42.2 |
| Total Split (%) | 20.0% | 42.5% | 9.1% | 31.6% | 10.0% | 38.7% | 9.1% | 9.6% | 38.4% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 95
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE
 Tierra West, LLC
 09/15/2025



| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ | ↖ ↗ |
| Traffic Volume (veh/h) | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Future Volume (veh/h) | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 281 | 894 | 477 | 183 | 620 | 195 | 110 | 471 | 183 | 114 | 759 | 67 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 410 | 1290 | 600 | 240 | 1197 | 369 | 257 | 1189 | 616 | 350 | 1110 | 98 |
| Arrive On Green | 0.26 | 0.76 | 0.76 | 0.05 | 0.31 | 0.31 | 0.06 | 0.34 | 0.34 | 0.06 | 0.34 | 0.34 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3837 | 1183 | 1767 | 3526 | 1572 | 1767 | 3277 | 289 |
| Grp Volume(v), veh/h | 281 | 894 | 477 | 183 | 544 | 271 | 110 | 471 | 183 | 114 | 408 | 418 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1643 | 1767 | 1763 | 1572 | 1767 | 1763 | 1803 |
| Q Serve(g_s), s | 11.8 | 14.6 | 20.0 | 6.0 | 14.5 | 14.9 | 4.4 | 11.2 | 8.8 | 4.6 | 21.9 | 21.9 |
| Cycle Q Clear(g_c), s | 11.8 | 14.6 | 20.0 | 6.0 | 14.5 | 14.9 | 4.4 | 11.2 | 8.8 | 4.6 | 21.9 | 21.9 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.72 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.16 |
| Lane Grp Cap(c), veh/h | 410 | 1290 | 600 | 240 | 1054 | 513 | 257 | 1189 | 616 | 350 | 597 | 611 |
| V/C Ratio(X) | 0.68 | 0.69 | 0.79 | 0.76 | 0.52 | 0.53 | 0.43 | 0.40 | 0.30 | 0.33 | 0.68 | 0.68 |
| Avail Cap(c_a), veh/h | 480 | 1290 | 600 | 240 | 1054 | 513 | 278 | 1189 | 616 | 361 | 597 | 611 |
| HCM Platoon Ratio | 2.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 18.2 | 9.8 | 10.4 | 30.6 | 31.0 | 31.2 | 24.1 | 27.9 | 23.0 | 22.2 | 31.3 | 31.3 |
| Incr Delay (d2), s/veh | 3.3 | 3.1 | 10.4 | 13.3 | 1.8 | 3.9 | 1.1 | 1.0 | 1.2 | 0.5 | 6.2 | 6.1 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 7.3 | 6.4 | 8.7 | 4.8 | 10.1 | 10.5 | 3.4 | 8.4 | 6.1 | 3.5 | 15.3 | 15.6 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 21.4 | 12.8 | 20.8 | 44.0 | 32.8 | 35.0 | 25.3 | 28.9 | 24.3 | 22.7 | 37.5 | 37.4 |
| LnGrp LOS | C | B | C | D | C | D | C | C | C | C | D | D |
| Approach Vol, veh/h | | 1652 | | | 998 | | | 764 | | | 940 | |
| Approach Delay, s/veh | | 16.6 | | | 35.5 | | | 27.2 | | | 35.7 | |
| Approach LOS | | B | | | D | | | C | | | D | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 9.9 | 42.6 | 10.0 | 47.5 | 9.7 | 42.8 | 17.7 | 39.8 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 7.1 | 37.1 | 6.0 | 41.3 | 7.5 | 36.7 | 18.5 | 29.3 | | | | |
| Max Q Clear Time (g_c+1), s | 6.6 | 13.2 | 8.0 | 22.0 | 6.4 | 23.9 | 13.8 | 16.9 | | | | |
| Green Ext Time (p_c), s | 0.0 | 3.7 | 0.0 | 9.5 | 0.0 | 4.1 | 0.4 | 4.2 | | | | |

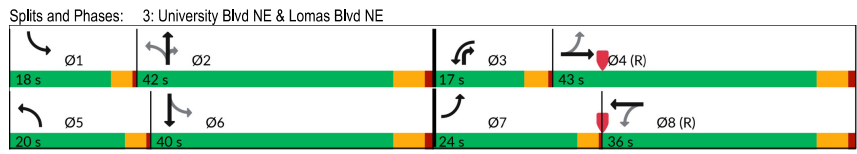
Intersection Summary
 HCM 7th Control Delay, s/veh: 26.9
 HCM 7th LOS: C

Notes
 User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
09/13/2025

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
| Lane Configurations | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ |
| Traffic Volume (vph) | 175 | 499 | 215 | 1031 | 240 | 613 | 268 | 154 | 613 |
| Future Volume (vph) | 175 | 499 | 215 | 1031 | 240 | 613 | 268 | 154 | 613 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 6 | 2 | 6 | 2 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 24.0 | 43.0 | 17.0 | 36.0 | 20.0 | 42.0 | 17.0 | 18.0 | 40.0 |
| Total Split (%) | 20.0% | 35.8% | 14.2% | 30.0% | 16.7% | 35.0% | 14.2% | 15.0% | 33.3% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE
 Tierra West, LLC
 09/13/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ | ↑↑↑ |
| Traffic Volume (veh/h) | 175 | 499 | 179 | 215 | 1031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| Future Volume (veh/h) | 175 | 499 | 179 | 215 | 1031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 175 | 499 | 179 | 215 | 1031 | 114 | 240 | 613 | 268 | 154 | 613 | 369 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 292 | 1285 | 446 | 391 | 1684 | 186 | 268 | 1152 | 663 | 314 | 608 | 366 |
| Arrive On Green | 0.03 | 0.11 | 0.11 | 0.09 | 0.36 | 0.36 | 0.12 | 0.33 | 0.33 | 0.08 | 0.29 | 0.29 |
| Sat Flow, veh/h | 1767 | 3712 | 1288 | 1767 | 4630 | 511 | 1767 | 3526 | 1572 | 1767 | 2116 | 1273 |
| Grp Volume(v), veh/h | 175 | 452 | 226 | 215 | 752 | 393 | 240 | 613 | 268 | 154 | 511 | 471 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1624 | 1767 | 1689 | 1764 | 1767 | 1763 | 1572 | 1767 | 1763 | 1626 |
| Q Serve(g_s), s | 7.5 | 14.9 | 15.5 | 9.3 | 21.9 | 21.9 | 11.9 | 17.0 | 14.3 | 7.3 | 34.5 | 34.5 |
| Cycle Q Clear(g_c), s | 7.5 | 14.9 | 15.5 | 9.3 | 21.9 | 21.9 | 11.9 | 17.0 | 14.3 | 7.3 | 34.5 | 34.5 |
| Prop In Lane | 1.00 | 0.79 | 1.00 | 1.00 | 0.29 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.78 |
| Lane Grp Cap(c), veh/h | 292 | 1169 | 562 | 391 | 1228 | 641 | 268 | 1152 | 663 | 314 | 507 | 468 |
| V/C Ratio(X) | 0.60 | 0.39 | 0.40 | 0.55 | 0.61 | 0.61 | 0.90 | 0.53 | 0.40 | 0.49 | 1.01 | 1.01 |
| Avail Cap(c_a), veh/h | 450 | 1169 | 562 | 415 | 1228 | 641 | 303 | 1152 | 663 | 389 | 507 | 468 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 25.9 | 41.4 | 41.6 | 22.7 | 31.2 | 31.3 | 33.2 | 32.9 | 24.2 | 27.3 | 42.7 | 42.8 |
| Incr Delay (d2), s/veh | 2.0 | 1.0 | 2.1 | 1.4 | 2.3 | 4.3 | 25.4 | 1.8 | 1.8 | 1.2 | 41.9 | 43.6 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 6.2 | 11.2 | 11.4 | 7.1 | 14.1 | 15.1 | 10.5 | 11.9 | 9.4 | 5.6 | 28.2 | 26.5 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 27.9 | 42.3 | 43.7 | 24.1 | 33.5 | 35.6 | 58.6 | 34.7 | 26.0 | 28.5 | 84.7 | 86.3 |
| LnGrp LOS | C | D | D | C | C | D | E | C | C | C | F | F |
| Approach Vol, veh/h | 853 | | | 1360 | | | 1121 | | | | 1136 | |
| Approach Delay, s/veh | 39.7 | | | 32.6 | | | 37.7 | | | | 77.8 | |
| Approach LOS | D | | | C | | | D | | | | E | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 12.9 | 44.7 | 15.4 | 47.0 | 17.6 | 40.0 | 13.3 | 49.1 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 14.5 | 36.5 | 13.0 | 37.5 | 16.5 | 34.5 | 20.5 | 30.5 | | | | |
| Max Q Clear Time (g_c+11), s | 9.3 | 19.0 | 11.3 | 17.5 | 13.9 | 36.5 | 9.5 | 23.9 | | | | |
| Green Ext Time (p_c), s | 0.2 | 4.7 | 0.1 | 4.3 | 0.2 | 0.0 | 0.3 | 3.8 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 46.7
 HCM 7th LOS: D
Notes
 User approved pedestrian interval to be less than phase max green.

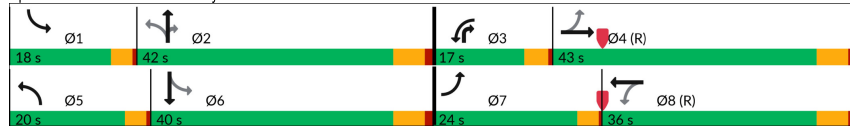
Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
09/13/2025

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (vph) | 180 | 521 | 215 | 1059 | 253 | 613 | 268 | 154 | 613 |
| Future Volume (vph) | 180 | 521 | 215 | 1059 | 253 | 613 | 268 | 154 | 613 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 2 | 6 | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 24.0 | 43.0 | 17.0 | 36.0 | 20.0 | 42.0 | 17.0 | 18.0 | 40.0 |
| Total Split (%) | 20.0% | 35.8% | 14.2% | 30.0% | 16.7% | 35.0% | 14.2% | 15.0% | 33.3% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
09/13/2025

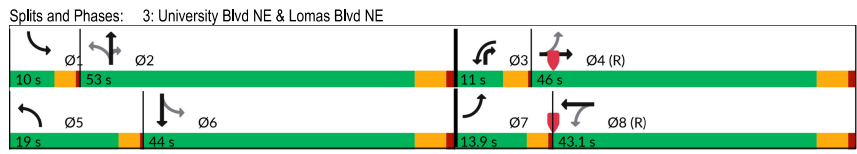
| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (veh/h) | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Future Volume (veh/h) | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 286 | 1250 | 440 | 378 | 1645 | 177 | 280 | 1177 | 676 | 319 | 603 | 370 |
| Arrive On Green | 0.03 | 0.11 | 0.11 | 0.10 | 0.35 | 0.35 | 0.12 | 0.33 | 0.33 | 0.08 | 0.29 | 0.29 |
| Sat Flow, veh/h | 1767 | 3698 | 1301 | 1767 | 4644 | 499 | 1767 | 3526 | 1572 | 1767 | 2099 | 1287 |
| Grp Volume(v), veh/h | 180 | 474 | 236 | 215 | 770 | 403 | 253 | 613 | 268 | 154 | 515 | 474 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1621 | 1767 | 1689 | 1766 | 1767 | 1763 | 1572 | 1767 | 1763 | 1624 |
| Q Serve(g_s), s | 7.8 | 15.7 | 16.3 | 9.4 | 22.9 | 22.9 | 12.8 | 16.8 | 14.1 | 7.3 | 34.5 | 34.5 |
| Cycle Q Clear(g_c), s | 7.8 | 15.7 | 16.3 | 9.4 | 22.9 | 22.9 | 12.8 | 16.8 | 14.1 | 7.3 | 34.5 | 34.5 |
| Prop In Lane | 1.00 | 0.80 | 1.00 | 1.00 | 0.28 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.79 |
| Lane Grp Cap(c), veh/h | 286 | 1142 | 548 | 378 | 1196 | 625 | 280 | 1177 | 676 | 319 | 507 | 467 |
| V/C Ratio(X) | 0.63 | 0.42 | 0.43 | 0.57 | 0.64 | 0.64 | 0.90 | 0.52 | 0.40 | 0.48 | 1.02 | 1.02 |
| Avail Cap(c_a), veh/h | 439 | 1142 | 548 | 400 | 1196 | 625 | 303 | 1177 | 676 | 395 | 507 | 467 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 26.9 | 42.3 | 42.5 | 23.5 | 32.4 | 32.4 | 33.8 | 32.2 | 23.5 | 27.2 | 42.8 | 42.8 |
| Incr Delay (d2), s/veh | 2.3 | 1.1 | 2.5 | 1.7 | 2.7 | 5.1 | 27.5 | 1.7 | 1.7 | 1.1 | 44.0 | 45.7 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 6.5 | 11.7 | 11.9 | 7.3 | 14.7 | 15.8 | 12.0 | 11.8 | 9.3 | 5.6 | 28.6 | 26.9 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 29.2 | 43.4 | 45.0 | 25.2 | 35.1 | 37.5 | 61.3 | 33.9 | 25.3 | 28.3 | 86.7 | 88.4 |
| LnGrp LOS | C | D | D | C | D | D | E | C | C | C | F | F |
| Approach Vol, veh/h | 890 | | | 1388 | | | 1134 | | | | 1143 | |
| Approach Delay, s/veh | 40.9 | | | 34.3 | | | 38.0 | | | | 79.6 | |
| Approach LOS | D | | | C | | | D | | | | E | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 12.9 | 45.6 | 15.5 | 46.1 | 18.4 | 40.0 | 13.6 | 48.0 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 14.5 | 36.5 | 13.0 | 37.5 | 16.5 | 34.5 | 20.5 | 30.5 | | | | |
| Max Q Clear Time (g_c+11), s | 9.3 | 18.8 | 11.4 | 18.3 | 14.8 | 36.5 | 9.8 | 24.9 | | | | |
| Green Ext Time (p_c), s | 0.2 | 4.7 | 0.1 | 4.5 | 0.1 | 0.0 | 0.3 | 3.4 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh: 47.9
 HCM 7th LOS: D
Notes
 User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
09/15/2025

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|----------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (vph) | 180 | 521 | 215 | 1059 | 253 | 613 | 268 | 154 | 613 |
| Future Volume (vph) | 180 | 521 | 215 | 1059 | 253 | 613 | 268 | 154 | 613 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 6 | | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 13.9 | 46.0 | 11.0 | 43.1 | 19.0 | 53.0 | 11.0 | 10.0 | 44.0 |
| Total Split (%) | 11.6% | 38.3% | 9.2% | 35.9% | 15.8% | 44.2% | 9.2% | 8.3% | 36.7% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated



HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE
 Tierra West, LLC
 09/15/2025

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ | ↔↔↔ |
| Traffic Volume (veh/h) | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Future Volume (veh/h) | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 180 | 521 | 189 | 215 | 1059 | 114 | 253 | 613 | 268 | 154 | 613 | 376 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 261 | 1248 | 439 | 311 | 1473 | 158 | 290 | 1396 | 714 | 324 | 719 | 441 |
| Arrive On Green | 0.03 | 0.11 | 0.11 | 0.06 | 0.32 | 0.32 | 0.11 | 0.40 | 0.40 | 0.05 | 0.34 | 0.34 |
| Sat Flow, veh/h | 1767 | 3698 | 1301 | 1767 | 4644 | 499 | 1767 | 3526 | 1572 | 1767 | 2099 | 1287 |
| Grp Volume(v), veh/h | 180 | 474 | 236 | 215 | 770 | 403 | 253 | 613 | 268 | 154 | 515 | 474 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1621 | 1767 | 1689 | 1766 | 1767 | 1763 | 1572 | 1767 | 1763 | 1624 |
| Q Serve(g_s), s | 7.9 | 15.7 | 16.3 | 7.0 | 24.2 | 24.3 | 10.7 | 15.3 | 13.5 | 6.5 | 32.5 | 32.5 |
| Cycle Q Clear(g_c), s | 7.9 | 15.7 | 16.3 | 7.0 | 24.2 | 24.3 | 10.7 | 15.3 | 13.5 | 6.5 | 32.5 | 32.5 |
| Prop In Lane | 1.00 | 0.80 | 1.00 | 1.00 | 0.28 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.79 |
| Lane Grp Cap(c), veh/h | 261 | 1140 | 547 | 311 | 1071 | 560 | 290 | 1396 | 714 | 324 | 604 | 556 |
| V/C Ratio(X) | 0.69 | 0.42 | 0.43 | 0.69 | 0.72 | 0.72 | 0.87 | 0.44 | 0.38 | 0.48 | 0.85 | 0.85 |
| Avail Cap(c_a), veh/h | 268 | 1140 | 547 | 311 | 1071 | 560 | 329 | 1396 | 714 | 324 | 604 | 556 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 29.4 | 42.3 | 42.6 | 32.4 | 36.2 | 36.3 | 27.0 | 26.5 | 21.5 | 24.8 | 36.6 | 36.6 |
| Incr Delay (d2), s/veh | 7.1 | 1.1 | 2.5 | 6.4 | 4.2 | 7.8 | 20.0 | 1.0 | 1.5 | 1.1 | 14.2 | 15.2 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 7.3 | 11.7 | 11.9 | 5.1 | 15.7 | 17.0 | 9.9 | 10.7 | 8.9 | 5.3 | 22.6 | 21.3 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 36.5 | 43.4 | 45.0 | 38.8 | 40.4 | 44.0 | 47.0 | 27.5 | 23.1 | 25.9 | 50.8 | 51.9 |
| LnGrp LOS | D | D | D | D | D | D | D | C | C | C | D | D |
| Approach Vol, veh/h | 890 | | | 1388 | | | 1134 | | | | 1143 | |
| Approach Delay, s/veh | 42.4 | | | 41.2 | | | 30.8 | | | | 47.9 | |
| Approach LOS | D | | | D | | | C | | | | D | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 10.0 | 53.0 | 11.0 | 46.0 | 16.4 | 46.6 | 13.4 | 43.6 | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | |
| Max Green Setting (Gmax), s | 6.5 | 47.5 | 7.0 | 40.5 | 15.5 | 38.5 | 10.4 | 37.6 | | | | |
| Max Q Clear Time (g_c+11), s | 8.5 | 17.3 | 9.0 | 18.3 | 12.7 | 34.5 | 9.9 | 26.3 | | | | |
| Green Ext Time (p_c), s | 0.0 | 5.5 | 0.0 | 4.7 | 0.2 | 2.2 | 0.0 | 5.7 | | | | |

Intersection Summary
 HCM 7th Control Delay, s/veh 40.5
 HCM 7th LOS D
Notes
 User approved pedestrian interval to be less than phase max green.

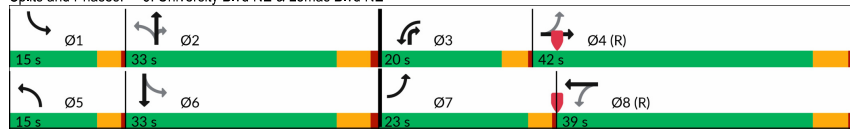
Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (vph) | 290 | 916 | 192 | 622 | 102 | 494 | 192 | 119 | 797 |
| Future Volume (vph) | 290 | 916 | 192 | 622 | 102 | 494 | 192 | 119 | 797 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 6 | 2 | 6 | 2 | 6 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 23.0 | 42.0 | 20.0 | 39.0 | 15.0 | 33.0 | 20.0 | 15.0 | 33.0 |
| Total Split (%) | 20.9% | 38.2% | 18.2% | 35.5% | 13.6% | 30.0% | 18.2% | 13.6% | 30.0% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |
| Act Effct Green (s) | 57.4 | 39.9 | 51.6 | 37.5 | 40.5 | 29.2 | 47.3 | 41.5 | 29.7 |
| Actuated g/C Ratio | 0.52 | 0.36 | 0.47 | 0.34 | 0.37 | 0.27 | 0.43 | 0.38 | 0.27 |
| v/c Ratio | 0.71 | 0.77 | 0.71 | 0.48 | 0.46 | 0.53 | 0.25 | 0.36 | 0.92 |
| Control Delay (s/veh) | 25.9 | 34.9 | 37.1 | 27.4 | 28.1 | 37.4 | 6.2 | 24.3 | 54.6 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 25.9 | 34.9 | 37.1 | 27.4 | 28.1 | 37.4 | 6.2 | 24.3 | 54.6 |
| LOS | C | C | D | C | C | D | A | C | D |
| Approach Delay (s/veh) | | 33.3 | | 29.2 | | 28.6 | | | 50.9 |
| Approach LOS | | C | | C | | C | | | D |

| Intersection Summary | |
|---|----------------------------|
| Cycle Length: 110 | Actuated Cycle Length: 110 |
| Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green | |
| Natural Cycle: 95 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.92 | |
| Intersection Signal Delay (s/veh): 35.4 | Intersection LOS: D |
| Intersection Capacity Utilization 84.9% | ICU Level of Service E |
| Analysis Period (min) 15 | |

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



2038AN - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 AM Peak Hour NO BUILD Conditions - Existing Geom.

Synchro 12 Report
LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (veh/h) | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| Future Volume (veh/h) | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 290 | 916 | 490 | 192 | 622 | 204 | 102 | 494 | 192 | 119 | 797 | 64 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 453 | 1461 | 680 | 263 | 1550 | 498 | 183 | 881 | 523 | 279 | 853 | 68 |
| Arrive On Green | 0.04 | 0.14 | 0.14 | 0.08 | 0.41 | 0.41 | 0.06 | 0.25 | 0.25 | 0.07 | 0.26 | 0.26 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3794 | 1220 | 1767 | 3526 | 1572 | 1767 | 3305 | 265 |
| Grp Volume(v), veh/h | 290 | 916 | 490 | 192 | 552 | 274 | 102 | 494 | 192 | 119 | 425 | 436 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1636 | 1767 | 1763 | 1572 | 1767 | 1763 | 1808 |
| Q Serve(g_s), s | 9.7 | 28.1 | 32.8 | 6.8 | 12.7 | 13.1 | 4.7 | 13.4 | 10.2 | 5.4 | 25.9 | 25.9 |
| Cycle Q Clear(g_c), s | 9.7 | 28.1 | 32.8 | 6.8 | 12.7 | 13.1 | 4.7 | 13.4 | 10.2 | 5.4 | 25.9 | 25.9 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.75 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.15 |
| Lane Grp Cap(c), veh/h | 453 | 1461 | 680 | 263 | 1380 | 668 | 183 | 881 | 523 | 279 | 455 | 466 |
| V/C Ratio(X) | 0.64 | 0.63 | 0.72 | 0.73 | 0.40 | 0.41 | 0.56 | 0.56 | 0.37 | 0.43 | 0.93 | 0.93 |
| Avail Cap(c_a), veh/h | 569 | 1461 | 680 | 374 | 1380 | 668 | 264 | 881 | 523 | 346 | 455 | 466 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 17.5 | 38.8 | 40.8 | 23.2 | 23.0 | 23.1 | 31.2 | 36.0 | 27.9 | 28.5 | 39.9 | 39.9 |
| Incr Delay (d2), s/veh | 1.6 | 2.0 | 6.5 | 4.3 | 0.9 | 1.9 | 2.6 | 2.6 | 2.0 | 1.0 | 28.7 | 28.2 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 8.0 | 19.0 | 21.3 | 5.4 | 8.8 | 9.0 | 3.7 | 10.0 | 7.3 | 4.2 | 20.8 | 21.2 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 19.1 | 40.8 | 47.3 | 27.4 | 23.9 | 25.0 | 33.8 | 38.5 | 29.9 | 29.6 | 68.6 | 68.1 |
| LnGrp LOS | B | D | D | C | C | C | C | D | C | C | E | E |
| Approach Vol, veh/h | | 1696 | | | 1018 | | | 788 | | | 980 | |
| Approach Delay, s/veh | | 39.0 | | | 24.8 | | | 35.8 | | | 63.6 | |
| Approach LOS | | D | | | C | | | D | | | E | |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|------------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 10.8 | 33.0 | 13.1 | 53.1 | 9.9 | 33.9 | 15.7 | 50.4 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 11.5 | 27.5 | 16.0 | 36.5 | 11.5 | 27.5 | 19.5 | 33.5 |
| Max Q Clear Time (g_c+11), s | 7.4 | 15.4 | 8.8 | 34.8 | 6.7 | 27.9 | 11.7 | 15.1 |
| Green Ext Time (p_c), s | 0.1 | 3.1 | 0.3 | 1.4 | 0.1 | 0.0 | 0.5 | 5.2 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 40.6 |
| HCM 7th LOS | D |

Notes
User approved pedestrian interval to be less than phase max green.

2038AN - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 AM Peak Hour NO BUILD Conditions - Existing Geom.

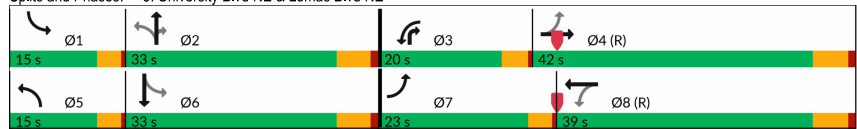
Synchro 12 Report
LoboPlaza_HY_2038.syn

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (vph) | 295 | 937 | 192 | 649 | 115 | 494 | 192 | 119 | 797 |
| Future Volume (vph) | 295 | 937 | 192 | 649 | 115 | 494 | 192 | 119 | 797 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 6 | 2 | 6 | 2 | 6 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 23.0 | 42.0 | 20.0 | 39.0 | 15.0 | 33.0 | 20.0 | 15.0 | 33.0 |
| Total Split (%) | 20.9% | 38.2% | 18.2% | 35.5% | 13.6% | 30.0% | 18.2% | 13.6% | 30.0% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |
| Act Effct Green (s) | 57.6 | 39.9 | 51.4 | 37.3 | 40.9 | 29.2 | 47.3 | 41.1 | 29.3 |
| Actuated g/C Ratio | 0.52 | 0.36 | 0.47 | 0.34 | 0.37 | 0.27 | 0.43 | 0.37 | 0.27 |
| v/c Ratio | 0.73 | 0.79 | 0.71 | 0.50 | 0.51 | 0.53 | 0.25 | 0.36 | 0.93 |
| Control Delay (s/veh) | 27.0 | 35.3 | 37.3 | 28.0 | 29.3 | 37.4 | 6.3 | 24.3 | 57.4 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 27.0 | 35.3 | 37.3 | 28.0 | 29.3 | 37.4 | 6.3 | 24.3 | 57.4 |
| LOS | C | D | D | C | C | D | A | C | E |
| Approach Delay (s/veh) | | 33.9 | | 29.7 | | 28.8 | | | 53.4 |
| Approach LOS | | C | | C | | C | | | D |

| Intersection Summary | |
|---|----------------------------|
| Cycle Length: 110 | Actuated Cycle Length: 110 |
| Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green | |
| Natural Cycle: 95 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 0.93 | |
| Intersection Signal Delay (s/veh): 36.2 | Intersection LOS: D |
| Intersection Capacity Utilization 86.4% | ICU Level of Service E |
| Analysis Period (min) 15 | |

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (veh/h) | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| Future Volume (veh/h) | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 447 | 1461 | 680 | 259 | 1561 | 482 | 187 | 881 | 523 | 279 | 826 | 73 |
| Arrive On Green | 0.04 | 0.14 | 0.14 | 0.08 | 0.41 | 0.41 | 0.06 | 0.25 | 0.25 | 0.07 | 0.25 | 0.25 |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3835 | 1184 | 1767 | 3526 | 1572 | 1767 | 3279 | 288 |
| Grp Volume(v), veh/h | 295 | 937 | 500 | 192 | 570 | 283 | 115 | 494 | 192 | 119 | 428 | 439 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1642 | 1767 | 1763 | 1572 | 1767 | 1763 | 1804 |
| Q Serve(g_s), s | 9.9 | 28.8 | 33.5 | 6.8 | 13.2 | 13.6 | 5.2 | 13.4 | 10.2 | 5.4 | 26.4 | 26.4 |
| Cycle Q Clear(g_c), s | 9.9 | 28.8 | 33.5 | 6.8 | 13.2 | 13.6 | 5.2 | 13.4 | 10.2 | 5.4 | 26.4 | 26.4 |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.72 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.16 |
| Lane Grp Cap(c), veh/h | 447 | 1461 | 680 | 259 | 1374 | 668 | 187 | 881 | 523 | 279 | 444 | 454 |
| V/C Ratio(X) | 0.66 | 0.64 | 0.74 | 0.74 | 0.41 | 0.42 | 0.61 | 0.56 | 0.37 | 0.43 | 0.97 | 0.97 |
| Avail Cap(c_a), veh/h | 561 | 1461 | 680 | 370 | 1374 | 668 | 257 | 881 | 523 | 346 | 444 | 454 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 17.8 | 39.1 | 41.1 | 23.4 | 23.3 | 23.4 | 31.0 | 36.0 | 27.9 | 28.5 | 40.7 | 40.7 |
| Incr Delay (d2), s/veh | 2.0 | 2.2 | 6.9 | 4.7 | 0.9 | 2.0 | 3.3 | 2.6 | 2.0 | 1.0 | 34.8 | 34.4 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 8.1 | 19.4 | 21.8 | 5.4 | 9.1 | 9.3 | 4.2 | 10.0 | 7.3 | 4.2 | 21.9 | 22.3 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 19.8 | 41.3 | 48.1 | 28.2 | 24.2 | 25.3 | 34.3 | 38.5 | 29.9 | 29.6 | 75.5 | 75.1 |
| LnGrp LOS | B | D | D | C | C | C | C | D | C | C | E | E |
| Approach Vol, veh/h | | 1732 | | | 1045 | | | 801 | | | 986 | |
| Approach Delay, s/veh | | 39.6 | | | 25.2 | | | 35.9 | | | 69.8 | |
| Approach LOS | | D | | | C | | | D | | | E | |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|-----------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 10.8 | 33.0 | 13.1 | 53.1 | 10.6 | 33.2 | 15.9 | 50.3 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 11.5 | 27.5 | 16.0 | 36.5 | 11.5 | 27.5 | 19.5 | 33.5 |
| Max Q Clear Time (g_c+1), s | 7.4 | 15.4 | 8.8 | 35.5 | 7.2 | 28.4 | 11.9 | 15.6 |
| Green Ext Time (p_c), s | 0.1 | 3.1 | 0.3 | 0.8 | 0.1 | 0.0 | 0.5 | 5.3 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 42.2 |
| HCM 7th LOS | D |

Notes
User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | | ↘ | | ↙ | |
|------------------------|-------|-------|-------|-------|-------|-------|-------|------|-------|--|-------|--|-----|--|-----|--|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT | | | | | | | |
| Lane Configurations | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | |
| Traffic Volume (vph) | 295 | 937 | 192 | 649 | 115 | 494 | 192 | 119 | 797 | | | | | | | |
| Future Volume (vph) | 295 | 937 | 192 | 649 | 115 | 494 | 192 | 119 | 797 | | | | | | | |
| Turn Type | pm+pt | | NA | | pm+pt | | NA | | pm+ov | | pm+pt | | NA | | | |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | | | | | |
| Permitted Phases | 4 | 8 | 3 | 8 | 2 | 2 | 2 | 6 | | | | | | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | | | | | |
| Switch Phase | | | | | | | | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 | | | | | | | |
| Minimum Split (s) | 10.5 | 25.0 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 | | | | | | | |
| Total Split (s) | 24.0 | 47.4 | 12.0 | 35.4 | 10.6 | 40.0 | 12.0 | 10.6 | 40.0 | | | | | | | |
| Total Split (%) | 21.8% | 43.1% | 10.9% | 32.2% | 9.6% | 36.4% | 10.9% | 9.6% | 36.4% | | | | | | | |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 | | | | | | | |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 | | | | | | | |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | | | | |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 | | | | | | | |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag | | | | | | | |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | | | | | |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max | | | | | | | |
| Act Effct Green (s) | 55.7 | 41.9 | 43.6 | 34.1 | 43.6 | 34.5 | 48.0 | 43.6 | 34.6 | | | | | | | |
| Actuated g/C Ratio | 0.51 | 0.38 | 0.40 | 0.31 | 0.40 | 0.31 | 0.44 | 0.40 | 0.31 | | | | | | | |
| v/c Ratio | 0.75 | 0.75 | 0.97 | 0.55 | 0.59 | 0.45 | 0.26 | 0.35 | 0.79 | | | | | | | |
| Control Delay (s/veh) | 28.5 | 29.4 | 86.0 | 31.0 | 32.4 | 31.7 | 9.4 | 22.6 | 40.4 | | | | | | | |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | | | | |
| Total Delay (s/veh) | 28.5 | 29.4 | 86.0 | 31.0 | 32.4 | 31.7 | 9.4 | 22.6 | 40.4 | | | | | | | |
| LOS | C | C | F | C | C | C | A | C | D | | | | | | | |
| Approach Delay (s/veh) | 29.3 | | 41.1 | | 26.5 | | 38.3 | | | | | | | | | |
| Approach LOS | C | | D | | C | | D | | | | | | | | | |

Intersection Summary
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 11 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 95
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.97
 Intersection Signal Delay (s/veh): 33.4 Intersection LOS: C
 Intersection Capacity Utilization 86.4% ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
 01/28/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | | ↘ | | ↙ | |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|-----|--|-----|--|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | | | |
| Lane Configurations | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | | ↖↖↖ | |
| Traffic Volume (veh/h) | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 | | | | |
| Future Volume (veh/h) | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 | | | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Work Zone On Approach | No | | No | | No | | No | | No | | No | | | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | | | | |
| Adj Flow Rate, veh/h | 295 | 937 | 500 | 192 | 649 | 204 | 115 | 494 | 192 | 119 | 797 | 70 | | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | | | | |
| Cap, veh/h | 421 | 1296 | 604 | 261 | 1258 | 389 | 232 | 1106 | 608 | 326 | 1034 | 91 | | | | |
| Arrive On Green | 0.27 | 0.77 | 0.77 | 0.07 | 0.33 | 0.33 | 0.06 | 0.31 | 0.06 | 0.32 | 0.32 | 0.32 | | | | |
| Sat Flow, veh/h | 1767 | 3377 | 1572 | 1767 | 3835 | 1184 | 1767 | 3526 | 1572 | 1767 | 3279 | 288 | | | | |
| Grp Volume(v), veh/h | 295 | 937 | 500 | 192 | 570 | 283 | 115 | 494 | 192 | 119 | 428 | 439 | | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1572 | 1767 | 1689 | 1642 | 1767 | 1763 | 1572 | 1767 | 1763 | 1804 | | | | |
| Q Serve(g_s), s | 12.2 | 15.9 | 22.3 | 8.0 | 15.0 | 15.4 | 4.8 | 12.3 | 9.4 | 5.0 | 24.2 | 24.2 | | | | |
| Cycle Q Clear(g_c), s | 12.2 | 15.9 | 22.3 | 8.0 | 15.0 | 15.4 | 4.8 | 12.3 | 9.4 | 5.0 | 24.2 | 24.2 | | | | |
| Prop In Lane | 1.00 | 1.00 | 1.00 | 1.00 | 0.72 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.16 | | | | |
| Lane Grp Cap(c), veh/h | 421 | 1296 | 604 | 261 | 1108 | 539 | 232 | 1106 | 608 | 326 | 556 | 569 | | | | |
| V/C Ratio(X) | 0.70 | 0.72 | 0.83 | 0.74 | 0.51 | 0.52 | 0.50 | 0.45 | 0.32 | 0.37 | 0.77 | 0.77 | | | | |
| Avail Cap(c_a), veh/h | 516 | 1296 | 604 | 261 | 1108 | 539 | 240 | 1106 | 608 | 331 | 556 | 569 | | | | |
| HCM Platoon Ratio | 2.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Uniform Delay (d), s/veh | 17.2 | 9.7 | 10.5 | 24.7 | 29.9 | 30.0 | 26.3 | 30.1 | 23.6 | 23.8 | 34.1 | 34.1 | | | | |
| Incr Delay (d2), s/veh | 3.2 | 3.5 | 12.4 | 10.4 | 1.7 | 3.6 | 1.6 | 1.3 | 1.4 | 0.7 | 9.9 | 9.7 | | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | |
| %ile BackOfQ(95%),veh/ln | 7.4 | 6.7 | 9.3 | 7.2 | 10.3 | 10.7 | 3.7 | 9.1 | 6.6 | 3.8 | 17.2 | 17.5 | | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 20.4 | 13.3 | 22.9 | 35.1 | 31.6 | 33.6 | 27.9 | 31.4 | 25.0 | 24.5 | 44.0 | 43.8 | | | | |
| LnGrp LOS | C | B | C | D | C | C | C | C | C | C | D | D | | | | |
| Approach Vol, veh/h | 1732 | | 1045 | | 801 | | 986 | | | | | | | | | |
| Approach Delay, s/veh | 17.3 | | 32.8 | | 29.4 | | 41.6 | | | | | | | | | |
| Approach LOS | B | | C | | C | | D | | | | | | | | | |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|-----------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 10.3 | 40.0 | 12.0 | 47.7 | 10.1 | 40.2 | 18.1 | 41.6 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 7.1 | 34.5 | 8.0 | 41.9 | 7.1 | 34.5 | 20.5 | 29.9 |
| Max Q Clear Time (g_c+1), s | 7.0 | 14.3 | 10.0 | 24.3 | 6.8 | 26.2 | 14.2 | 17.4 |
| Green Ext Time (p_c), s | 0.0 | 3.8 | 0.0 | 9.4 | 0.0 | 3.4 | 0.5 | 4.5 |

Intersection Summary
 HCM 7th Control Delay, s/veh: 28.2
 HCM 7th LOS: C

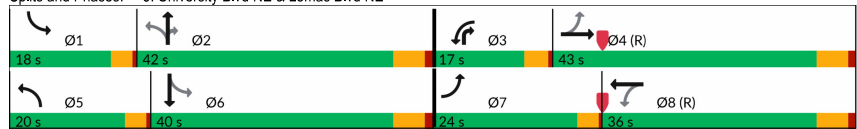
Notes
 User approved pedestrian interval to be less than phase max green.

Timings
3: University Blvd NE & Lomas Blvd NE
Tierra West, LLC
01/28/2026

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (vph) | 183 | 524 | 226 | 1082 | 251 | 643 | 281 | 162 | 643 |
| Future Volume (vph) | 183 | 524 | 226 | 1082 | 251 | 643 | 281 | 162 | 643 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 2 | 6 | | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 24.0 | 43.0 | 17.0 | 36.0 | 20.0 | 42.0 | 17.0 | 18.0 | 40.0 |
| Total Split (%) | 20.0% | 35.8% | 14.2% | 30.0% | 16.7% | 35.0% | 14.2% | 15.0% | 33.3% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |
| Act Effct Green (s) | 53.1 | 38.1 | 51.5 | 37.5 | 56.1 | 39.3 | 57.2 | 49.3 | 35.6 |
| Actuated g/C Ratio | 0.44 | 0.32 | 0.43 | 0.31 | 0.47 | 0.33 | 0.48 | 0.41 | 0.30 |
| v/c Ratio | 0.70 | 0.45 | 0.63 | 0.77 | 0.87 | 0.56 | 0.33 | 0.47 | 0.98 |
| Control Delay (s/veh) | 45.9 | 29.5 | 29.3 | 41.4 | 58.4 | 36.0 | 6.7 | 23.7 | 60.5 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 45.9 | 29.5 | 29.3 | 41.4 | 58.4 | 36.0 | 6.7 | 23.7 | 60.5 |
| LOS | D | C | C | D | E | D | A | C | E |
| Approach Delay (s/veh) | | 32.8 | | 39.5 | | 33.7 | | | 55.5 |
| Approach LOS | | C | | D | | C | | | E |

Intersection Summary
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.98
 Intersection Signal Delay (s/veh): 40.9
 Intersection LOS: D
 Intersection Capacity Utilization 93.6%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



2038PN - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour NO BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE
 Tierra West, LLC
 01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (veh/h) | 183 | 524 | 187 | 226 | 1082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| Future Volume (veh/h) | 183 | 524 | 187 | 226 | 1082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 183 | 524 | 187 | 226 | 1082 | 119 | 251 | 643 | 281 | 162 | 643 | 388 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 283 | 1245 | 431 | 382 | 1639 | 180 | 278 | 1162 | 675 | 312 | 608 | 367 |
| Arrive On Green | 0.03 | 0.11 | 0.11 | 0.10 | 0.35 | 0.35 | 0.12 | 0.33 | 0.33 | 0.08 | 0.29 | 0.29 |
| Sat Flow, veh/h | 1767 | 3715 | 1286 | 1767 | 4632 | 509 | 1767 | 3526 | 1572 | 1767 | 2114 | 1275 |
| Grp Volume(v), veh/h | 183 | 475 | 236 | 226 | 789 | 412 | 251 | 643 | 281 | 162 | 536 | 495 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1624 | 1767 | 1689 | 1764 | 1767 | 1763 | 1572 | 1767 | 1763 | 1626 |
| Q Serve(g_s), s | 7.9 | 15.7 | 16.3 | 9.9 | 23.6 | 23.7 | 12.7 | 17.9 | 14.9 | 7.6 | 34.5 | 34.5 |
| Cycle Q Clear(g_c), s | 7.9 | 15.7 | 16.3 | 9.9 | 23.6 | 23.7 | 12.7 | 17.9 | 14.9 | 7.6 | 34.5 | 34.5 |
| Prop In Lane | 1.00 | 0.79 | 1.00 | 1.00 | 0.29 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.78 |
| Lane Grp Cap(c), veh/h | 283 | 1132 | 544 | 382 | 1195 | 624 | 278 | 1162 | 675 | 312 | 507 | 467 |
| V/C Ratio(X) | 0.65 | 0.42 | 0.43 | 0.59 | 0.66 | 0.66 | 0.90 | 0.55 | 0.42 | 0.52 | 1.06 | 1.06 |
| Avail Cap(c_a), veh/h | 434 | 1132 | 544 | 398 | 1195 | 624 | 303 | 1162 | 675 | 382 | 507 | 467 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 27.4 | 42.5 | 42.7 | 23.5 | 32.7 | 32.7 | 33.7 | 33.0 | 23.8 | 27.3 | 42.7 | 42.8 |
| Incr Delay (d2), s/veh | 2.5 | 1.1 | 2.5 | 2.2 | 2.9 | 5.4 | 27.2 | 1.9 | 1.9 | 1.3 | 56.2 | 58.0 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 6.7 | 11.7 | 12.0 | 7.7 | 15.1 | 16.3 | 11.0 | 12.5 | 9.7 | 5.9 | 31.5 | 29.6 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 29.8 | 43.6 | 45.2 | 25.6 | 35.6 | 38.1 | 60.9 | 34.9 | 25.7 | 28.6 | 98.9 | 100.7 |
| LnGrp LOS | C | D | D | C | D | D | E | C | C | C | F | F |
| Approach Vol, veh/h | | 894 | | | 1427 | | | 1175 | | | | 1193 |
| Approach Delay, s/veh | | 41.2 | | | 34.7 | | | 38.2 | | | | 90.1 |
| Approach LOS | | D | | | C | | | D | | | | F |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|------------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 13.3 | 45.0 | 16.0 | 45.7 | 18.3 | 40.0 | 13.7 | 48.0 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 14.5 | 36.5 | 13.0 | 37.5 | 16.5 | 34.5 | 20.5 | 30.5 |
| Max Q Clear Time (g_c+11), s | 9.6 | 19.9 | 11.9 | 18.3 | 14.7 | 36.5 | 9.9 | 25.7 |
| Green Ext Time (p_c), s | 0.2 | 4.9 | 0.1 | 4.5 | 0.1 | 0.0 | 0.3 | 3.0 |

Intersection Summary
 HCM 7th Control Delay, s/veh 50.9
 HCM 7th LOS D

Notes
 User approved pedestrian interval to be less than phase max green.

2038PN - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour NO BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

Timings
3: University Blvd NE & Lomas Blvd NE

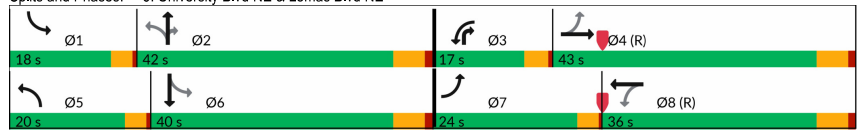
Tierra West, LLC
01/28/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|---|---|---|
| Lane Group | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT | | | |
| Lane Configurations | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
| Traffic Volume (vph) | 188 | 546 | 226 | 1110 | 264 | 643 | 281 | 162 | 643 | | | |
| Future Volume (vph) | 188 | 546 | 226 | 1110 | 264 | 643 | 281 | 162 | 643 | | | |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA | | | |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | |
| Permitted Phases | 4 | 8 | 2 | 6 | 2 | 6 | 2 | 6 | 2 | | | |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 | | | |
| Switch Phase | | | | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 | | | |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 | | | |
| Total Split (s) | 24.0 | 43.0 | 17.0 | 36.0 | 20.0 | 42.0 | 17.0 | 18.0 | 40.0 | | | |
| Total Split (%) | 20.0% | 35.8% | 14.2% | 30.0% | 16.7% | 35.0% | 14.2% | 15.0% | 33.3% | | | |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 | | | |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 | | | |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 | | | |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag | | | |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max | | | |
| Act Effct Green (s) | 53.3 | 38.1 | 51.2 | 37.2 | 56.1 | 39.3 | 57.2 | 48.8 | 35.1 | | | |
| Actuated g/C Ratio | 0.44 | 0.32 | 0.43 | 0.31 | 0.47 | 0.33 | 0.48 | 0.41 | 0.29 | | | |
| v/c Ratio | 0.71 | 0.47 | 0.65 | 0.79 | 0.89 | 0.56 | 0.33 | 0.47 | 0.99 | | | |
| Control Delay (s/veh) | 45.8 | 30.2 | 30.3 | 42.6 | 61.6 | 36.0 | 7.1 | 23.7 | 64.5 | | | |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Total Delay (s/veh) | 45.8 | 30.2 | 30.3 | 42.6 | 61.6 | 36.0 | 7.1 | 23.7 | 64.5 | | | |
| LOS | D | C | C | D | E | D | A | C | E | | | |
| Approach Delay (s/veh) | | 33.4 | | 40.7 | | 34.8 | | | 59.0 | | | |
| Approach LOS | | C | | D | | C | | | E | | | |

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.99
 Intersection Signal Delay (s/veh): 42.4
 Intersection LOS: D
 Intersection Capacity Utilization 95.4%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



2038PB - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
 3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
 01/28/2026

| | ↖ | | → | | ↗ | | ← | | ↖ | | ↗ | | ↖ | | ↗ | |
|------------------------------|------|------|------|------|------|------|------|------|------|------|-------|-------|---|---|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR | | | | |
| Lane Configurations | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ | ↖ |
| Traffic Volume (veh/h) | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 | | | | |
| Future Volume (veh/h) | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 | | | | |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | | | | |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No | | | | |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | | | | |
| Adj Flow Rate, veh/h | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 | | | | |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | | | | |
| Cap, veh/h | 278 | 1211 | 425 | 370 | 1599 | 171 | 290 | 1186 | 688 | 317 | 603 | 370 | | | | |
| Arrive On Green | 0.03 | 0.11 | 0.11 | 0.10 | 0.34 | 0.34 | 0.13 | 0.34 | 0.34 | 0.08 | 0.29 | 0.29 | | | | |
| Sat Flow, veh/h | 1767 | 3701 | 1298 | 1767 | 4645 | 498 | 1767 | 3526 | 1572 | 1767 | 2098 | 1288 | | | | |
| Grp Volume(v), veh/h | 188 | 497 | 246 | 226 | 807 | 422 | 264 | 643 | 281 | 162 | 540 | 498 | | | | |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1622 | 1767 | 1689 | 1766 | 1767 | 1763 | 1572 | 1767 | 1763 | 1624 | | | | |
| Q Serve(g_s), s | 8.2 | 16.6 | 17.1 | 10.1 | 24.7 | 24.7 | 13.5 | 17.8 | 14.7 | 7.6 | 34.5 | 34.5 | | | | |
| Cycle Q Clear(g_c), s | 8.2 | 16.6 | 17.1 | 10.1 | 24.7 | 24.7 | 13.5 | 17.8 | 14.7 | 7.6 | 34.5 | 34.5 | | | | |
| Prop In Lane | 1.00 | 0.80 | 1.00 | 1.00 | 0.28 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.79 | | | | |
| Lane Grp Cap(c), veh/h | 278 | 1105 | 531 | 370 | 1163 | 608 | 290 | 1186 | 688 | 317 | 507 | 467 | | | | |
| V/C Ratio(X) | 0.68 | 0.45 | 0.46 | 0.61 | 0.69 | 0.69 | 0.91 | 0.54 | 0.41 | 0.51 | 1.07 | 1.07 | | | | |
| Avail Cap(c_a), veh/h | 424 | 1105 | 531 | 383 | 1163 | 608 | 303 | 1186 | 688 | 387 | 507 | 467 | | | | |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | | | | |
| Uniform Delay (d), s/veh | 28.5 | 43.4 | 43.6 | 24.3 | 33.9 | 33.9 | 34.3 | 32.3 | 23.1 | 27.1 | 42.7 | 42.8 | | | | |
| Incr Delay (d2), s/veh | 2.9 | 1.3 | 2.9 | 2.7 | 3.4 | 6.4 | 29.2 | 1.8 | 1.8 | 1.3 | 58.7 | 60.5 | | | | |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | |
| %ile BackOfQ(95%),veh/ln | 7.0 | 12.3 | 12.5 | 7.8 | 15.8 | 17.1 | 11.6 | 12.3 | 9.6 | 5.9 | 32.0 | 30.1 | | | | |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 31.4 | 44.7 | 46.5 | 27.0 | 37.3 | 40.3 | 63.5 | 34.1 | 24.9 | 28.4 | 101.4 | 103.2 | | | | |
| LnGrp LOS | C | D | D | C | D | D | E | C | C | C | F | F | | | | |
| Approach Vol, veh/h | | 931 | | | 1455 | | | 1188 | | | 1200 | | | | | |
| Approach Delay, s/veh | | 42.5 | | | 36.6 | | | 38.5 | | | 92.3 | | | | | |
| Approach LOS | | D | | | D | | | D | | | F | | | | | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | | | | | |
| Phs Duration (G+Y+Rc), s | 13.3 | 45.9 | 16.1 | 44.8 | 19.1 | 40.0 | 14.1 | 46.8 | | | | | | | | |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 | | | | | | | | |
| Max Green Setting (Gmax), s | 14.5 | 36.5 | 13.0 | 37.5 | 16.5 | 34.5 | 20.5 | 30.5 | | | | | | | | |
| Max Q Clear Time (g_c+11), s | 9.6 | 19.8 | 12.1 | 19.1 | 15.5 | 36.5 | 10.2 | 26.7 | | | | | | | | |
| Green Ext Time (p_c), s | 0.2 | 4.9 | 0.1 | 4.6 | 0.1 | 0.0 | 0.3 | 2.5 | | | | | | | | |

Intersection Summary

HCM 7th Control Delay, s/veh: 52.2
 HCM 7th LOS: D

Notes
 User approved pedestrian interval to be less than phase max green.

2038PB - 2025068 - Lobo Plaza, Lomas Blvd NE
 2038 PM Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
 LoboPlaza_HY_2038.syn

Timings
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | WBL | WBT | NBL | NBT | NBR | SBL | SBT |
|------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (vph) | 188 | 546 | 226 | 1110 | 264 | 643 | 281 | 162 | 643 |
| Future Volume (vph) | 188 | 546 | 226 | 1110 | 264 | 643 | 281 | 162 | 643 |
| Turn Type | pm+pt | NA | pm+pt | NA | pm+pt | NA | pm+ov | pm+pt | NA |
| Protected Phases | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Permitted Phases | 4 | 8 | 2 | 6 | 2 | 6 | 2 | 6 | 6 |
| Detector Phase | 7 | 4 | 3 | 8 | 5 | 2 | 3 | 1 | 6 |
| Switch Phase | | | | | | | | | |
| Minimum Initial (s) | 6.0 | 19.5 | 6.0 | 20.0 | 6.0 | 19.5 | 6.0 | 6.0 | 19.5 |
| Minimum Split (s) | 10.5 | 42.5 | 10.5 | 31.5 | 10.5 | 42.5 | 10.5 | 10.5 | 37.5 |
| Total Split (s) | 12.0 | 45.0 | 10.0 | 43.0 | 19.0 | 53.0 | 10.0 | 12.0 | 46.0 |
| Total Split (%) | 10.0% | 37.5% | 8.3% | 35.8% | 15.8% | 44.2% | 8.3% | 10.0% | 38.3% |
| Yellow Time (s) | 3.0 | 4.5 | 3.5 | 4.5 | 3.0 | 4.5 | 3.5 | 3.0 | 4.5 |
| All-Red Time (s) | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 1.0 | 0.5 | 0.5 | 1.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 4.0 | 3.5 | 5.5 |
| Lead/Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lead | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Recall Mode | None | C-Max | None | C-Max | None | Max | None | None | Max |
| Act Effct Green (s) | 50.0 | 39.5 | 45.0 | 37.5 | 61.5 | 47.6 | 59.1 | 51.2 | 40.8 |
| Actuated g/C Ratio | 0.42 | 0.33 | 0.38 | 0.31 | 0.51 | 0.40 | 0.49 | 0.43 | 0.34 |
| v/c Ratio | 1.00 | 0.45 | 0.86 | 0.79 | 0.93 | 0.46 | 0.33 | 0.45 | 0.86 |
| Control Delay (s/veh) | 105.6 | 20.7 | 58.8 | 41.5 | 68.3 | 28.1 | 8.0 | 20.9 | 41.5 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 105.6 | 20.7 | 58.8 | 41.5 | 68.3 | 28.1 | 8.0 | 20.9 | 41.5 |
| LOS | F | C | E | D | E | C | A | C | D |
| Approach Delay (s/veh) | | 37.8 | | 44.2 | | 32.3 | | | 38.7 |
| Approach LOS | | D | | D | | C | | | D |

| Intersection Summary | |
|---|------------------------|
| Cycle Length: 120 | |
| Actuated Cycle Length: 120 | |
| Offset: 12 (10%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green | |
| Natural Cycle: 110 | |
| Control Type: Actuated-Coordinated | |
| Maximum v/c Ratio: 1.00 | |
| Intersection Signal Delay (s/veh): 38.6 | Intersection LOS: D |
| Intersection Capacity Utilization 95.4% | ICU Level of Service F |
| Analysis Period (min) 15 | |

Splits and Phases: 3: University Blvd NE & Lomas Blvd NE



2038PB MIT - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 PM MIT Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
LoboPlaza_HY_2038.syn

HCM 7th Signalized Intersection Summary
3: University Blvd NE & Lomas Blvd NE

Tierra West, LLC
01/28/2026

| | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ | ↔ |
| Traffic Volume (veh/h) | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| Future Volume (veh/h) | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | No | No | No | No | No | No | No | No | No | No | No |
| Adj Sat Flow, veh/h/ln | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 | 1856 |
| Adj Flow Rate, veh/h | 188 | 546 | 197 | 226 | 1110 | 119 | 264 | 643 | 281 | 162 | 643 | 395 |
| Peak Hour Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Percent Heavy Veh. % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Cap, veh/h | 229 | 1218 | 427 | 281 | 1452 | 155 | 292 | 1396 | 701 | 342 | 750 | 461 |
| Arrive On Green | 0.02 | 0.11 | 0.11 | 0.05 | 0.31 | 0.31 | 0.11 | 0.40 | 0.40 | 0.07 | 0.36 | 0.36 |
| Sat Flow, veh/h | 1767 | 3701 | 1298 | 1767 | 4645 | 498 | 1767 | 3526 | 1572 | 1767 | 2098 | 1288 |
| Grp Volume(v), veh/h | 188 | 497 | 246 | 226 | 807 | 422 | 264 | 643 | 281 | 162 | 540 | 498 |
| Grp Sat Flow(s), veh/h/ln | 1767 | 1689 | 1622 | 1767 | 1689 | 1766 | 1767 | 1763 | 1572 | 1767 | 1763 | 1624 |
| Q Serve(g_s), s | 8.5 | 16.5 | 17.1 | 6.0 | 25.9 | 25.9 | 10.9 | 16.2 | 14.5 | 6.9 | 34.1 | 34.1 |
| Cycle Q Clear(g_c), s | 8.5 | 16.5 | 17.1 | 6.0 | 25.9 | 25.9 | 10.9 | 16.2 | 14.5 | 6.9 | 34.1 | 34.1 |
| Prop In Lane | 1.00 | 0.80 | 1.00 | 1.00 | 0.28 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.79 |
| Lane Grp Cap(c), veh/h | 229 | 1112 | 534 | 281 | 1055 | 552 | 292 | 1396 | 701 | 342 | 630 | 581 |
| V/C Ratio(X) | 0.82 | 0.45 | 0.46 | 0.80 | 0.76 | 0.77 | 0.90 | 0.46 | 0.40 | 0.47 | 0.86 | 0.86 |
| Avail Cap(c_a), veh/h | 229 | 1112 | 534 | 281 | 1055 | 552 | 328 | 1396 | 701 | 342 | 630 | 581 |
| HCM Platoon Ratio | 0.33 | 0.33 | 0.33 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Uniform Delay (d), s/veh | 31.4 | 43.3 | 43.5 | 37.6 | 37.3 | 37.3 | 26.6 | 26.8 | 22.4 | 22.3 | 35.7 | 35.7 |
| Incr Delay (d2), s/veh | 20.9 | 1.3 | 2.9 | 15.4 | 5.3 | 9.7 | 25.2 | 1.1 | 1.7 | 1.0 | 14.0 | 15.1 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 8.9 | 12.2 | 12.4 | 8.4 | 16.8 | 18.3 | 10.6 | 11.2 | 9.5 | 5.3 | 23.4 | 22.0 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 52.3 | 44.6 | 46.3 | 53.0 | 42.5 | 47.0 | 51.8 | 27.9 | 24.1 | 23.4 | 49.7 | 50.8 |
| LnGrp LOS | D | D | D | D | D | D | D | C | C | C | D | D |
| Approach Vol, veh/h | | 931 | | | 1455 | | | 1188 | | | | 1200 |
| Approach Delay, s/veh | | 46.6 | | | 45.5 | | | 32.3 | | | | 46.6 |
| Approach LOS | | D | | | D | | | C | | | | D |

| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|-----------------------------|------|------|------|------|------|------|------|------|
| Phs Duration (G+Y+Rc), s | 12.0 | 53.0 | 10.0 | 45.0 | 16.6 | 48.4 | 12.0 | 43.0 |
| Change Period (Y+Rc), s | 3.5 | 5.5 | 4.0 | 5.5 | 3.5 | 5.5 | 3.5 | 5.5 |
| Max Green Setting (Gmax), s | 8.5 | 47.5 | 6.0 | 39.5 | 15.5 | 40.5 | 8.5 | 37.5 |
| Max Q Clear Time (g_c+1), s | 8.9 | 18.2 | 8.0 | 19.1 | 12.9 | 36.1 | 10.5 | 27.9 |
| Green Ext Time (p_c), s | 0.0 | 5.8 | 0.0 | 4.8 | 0.2 | 2.5 | 0.0 | 5.3 |

| Intersection Summary | |
|------------------------------|------|
| HCM 7th Control Delay, s/veh | 42.7 |
| HCM 7th LOS | D |

Notes
User approved pedestrian interval to be less than phase max green.

2038PB MIT - 2025068 - Lobo Plaza, Lomas Blvd NE
2038 PM MIT Peak Hour BUILD Conditions - Existing Geom.

Synchro 12 Report
LoboPlaza_HY_2038.syn

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.9 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1472 | 256 | 73 | 761 | 0 | 69 |
| Future Vol, veh/h | 1472 | 256 | 73 | 761 | 0 | 69 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1472 | 256 | 73 | 761 | 0 | 69 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1728 | 0 | 864 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 291 | - | *739 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 291 | - | *739 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 1.88 | 10.37 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 739 | - | - | 291 | - |
| HCM Lane V/C Ratio | 0.093 | - | - | 0.251 | - |
| HCM Ctrl Dly (s/v) | 10.4 | - | - | 21.4 | - |
| HCM Lane LOS | B | - | - | C | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.9 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1514 | 256 | 74 | 794 | 0 | 71 |
| Future Vol, veh/h | 1514 | 256 | 74 | 794 | 0 | 71 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1514 | 256 | 74 | 794 | 0 | 71 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1770 | 0 | 885 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 275 | - | 717 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 275 | - | 717 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 1.95 | 10.57 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 717 | - | - | 275 | - |
| HCM Lane V/C Ratio | 0.099 | - | - | 0.269 | - |
| HCM Ctrl Dly (s/v) | 10.6 | - | - | 22.9 | - |
| HCM Lane LOS | B | - | - | C | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1.1 | - |

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.9 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1514 | 256 | 74 | 794 | 0 | 71 |
| Future Vol, veh/h | 1514 | 256 | 74 | 794 | 0 | 71 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1514 | 256 | 74 | 794 | 0 | 71 |

| Major/Minor | Major1 | Major2 | Minor1 | | |
|----------------------|--------|--------|--------|---|------|
| Conflicting Flow All | 0 | 0 | 1770 | 0 | 885 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 275 | - | 717 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 275 | - | 717 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 1.95 | 10.57 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 717 | - | - | 275 | - |
| HCM Lane V/C Ratio | 0.099 | - | - | 0.269 | - |
| HCM Ctrl Dly (s/v) | 10.6 | - | - | 22.9 | - |
| HCM Lane LOS | B | - | - | C | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1.1 | - |

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 759 | 69 | 28 | 1689 | 0 | 97 |
| Future Vol, veh/h | 759 | 69 | 28 | 1689 | 0 | 97 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 759 | 69 | 28 | 1689 | 0 | 97 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 828 | 0 | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | - |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | - |
| Pot Cap-1 Maneuver | - | - | 638 | - | 0 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 638 | - | - |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|------|
| HCM Ctrl Dly, s/v | 0 | 0.18 | 9.95 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 824 | - | - | 638 | - |
| HCM Lane V/C Ratio | 0.118 | - | - | 0.044 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 10.9 | - |
| HCM Lane LOS | A | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.1 | - |

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 803 | 69 | 29 | 1724 | 0 | 99 |
| Future Vol, veh/h | 803 | 69 | 29 | 1724 | 0 | 99 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 803 | 69 | 29 | 1724 | 0 | 99 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 872 | 0 | 436 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 634 | - | *822 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 634 | - | *822 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|------|
| HCM Ctrl Dly, s/v | 0 | 0.18 | 9.98 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 822 | - | - | 634 | - |
| HCM Lane V/C Ratio | 0.12 | - | - | 0.046 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 11 | - |
| HCM Lane LOS | A | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.1 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 803 | 69 | 29 | 1724 | 0 | 99 |
| Future Vol, veh/h | 803 | 69 | 29 | 1724 | 0 | 99 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 803 | 69 | 29 | 1724 | 0 | 99 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 872 | 0 | 436 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 634 | - | *822 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 634 | - | *822 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|------|
| HCM Ctrl Dly, s/v | 0 | 0.18 | 9.98 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 822 | - | - | 634 | - |
| HCM Lane V/C Ratio | 0.12 | - | - | 0.046 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 11 | - |
| HCM Lane LOS | A | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.1 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.9 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1544 | 268 | 77 | 799 | 0 | 72 |
| Future Vol, veh/h | 1544 | 268 | 77 | 799 | 0 | 72 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1544 | 268 | 77 | 799 | 0 | 72 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1812 | 0 | - 906 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | - 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | - 3.93 |
| Pot Cap-1 Maneuver | - | - | 272 | - | 0 *727 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 272 | - | - *727 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|------|
| HCM Ctrl Dly, s/v | 0 | 2.06 | 10.5 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 727 | - | - | 272 | - |
| HCM Lane V/C Ratio | 0.099 | - | - | 0.284 | - |
| HCM Ctrl Dly (s/v) | 10.5 | - | - | 23.4 | - |
| HCM Lane LOS | B | - | - | C | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1.1 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 1 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1586 | 268 | 78 | 832 | 0 | 74 |
| Future Vol, veh/h | 1586 | 268 | 78 | 832 | 0 | 74 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1586 | 268 | 78 | 832 | 0 | 74 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1854 | 0 | 927 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 256 | - | *727 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 256 | - | *727 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 2.15 | 10.51 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 727 | - | - | 256 | - |
| HCM Lane V/C Ratio | 0.102 | - | - | 0.305 | - |
| HCM Ctrl Dly (s/v) | 10.5 | - | - | 25.1 | - |
| HCM Lane LOS | B | - | - | D | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1.2 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 1 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1586 | 268 | 78 | 832 | 0 | 74 |
| Future Vol, veh/h | 1586 | 268 | 78 | 832 | 0 | 74 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1586 | 268 | 78 | 832 | 0 | 74 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1854 | 0 | - 927 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | - 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | - 3.93 |
| Pot Cap-1 Maneuver | - | - | 256 | - | 0 *727 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 256 | - | - *727 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 2.15 | 10.51 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 727 | - | - | 256 | - |
| HCM Lane V/C Ratio | 0.102 | - | - | 0.305 | - |
| HCM Ctrl Dly (s/v) | 10.5 | - | - | 25.1 | - |
| HCM Lane LOS | B | - | - | D | - |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 1.2 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 797 | 72 | 30 | 1772 | 0 | 102 |
| Future Vol, veh/h | 797 | 72 | 30 | 1772 | 0 | 102 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 797 | 72 | 30 | 1772 | 0 | 102 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 869 | 0 | 435 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 636 | - | *822 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 636 | - | *822 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|----|
| HCM Ctrl Dly, s/v | 0 | 0.18 | 10 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 822 | - | - | 636 | - |
| HCM Lane V/C Ratio | 0.124 | - | - | 0.047 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 10.9 | - |
| HCM Lane LOS | A | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.1 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 841 | 72 | 31 | 1807 | 0 | 104 |
| Future Vol, veh/h | 841 | 72 | 31 | 1807 | 0 | 104 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 841 | 72 | 31 | 1807 | 0 | 104 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 913 | 0 | 457 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 603 | - | *822 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 603 | - | *822 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.19 | 10.01 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 822 | - | - | 603 | - |
| HCM Lane V/C Ratio | 0.127 | - | - | 0.051 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 11.3 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.2 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
4: Medical Arts Ave NE & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 841 | 72 | 31 | 1807 | 0 | 104 |
| Future Vol, veh/h | 841 | 72 | 31 | 1807 | 0 | 104 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 180 | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 841 | 72 | 31 | 1807 | 0 | 104 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 913 | 0 | 457 |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |
| Critical Hdwy | - | - | 5.36 | - | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.93 |
| Pot Cap-1 Maneuver | - | - | 603 | - | *822 |
| Stage 1 | - | - | - | - | 0 |
| Stage 2 | - | - | - | - | 0 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 603 | - | *822 |
| Mov Cap-2 Maneuver | - | - | - | - | - |
| Stage 1 | - | - | - | - | - |
| Stage 2 | - | - | - | - | - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.19 | 10.01 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 822 | - | - | 603 | - |
| HCM Lane V/C Ratio | 0.127 | - | - | 0.051 | - |
| HCM Ctrl Dly (s/v) | 10 | - | - | 11.3 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0.4 | - | - | 0.2 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

Intersection

Int Delay, s/veh 0

| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
|--------------------------|------|------|------|------|------|------|
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1583 | 0 | 4 | 784 | 4 | 0 |
| Future Vol, veh/h | 1583 | 0 | 4 | 784 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1583 | 0 | 4 | 784 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1583 | 0 | 1905 |
| Stage 1 | - | - | - | - | 1583 |
| Stage 2 | - | - | - | - | 322 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 486 | - | *457 |
| Stage 1 | - | - | - | - | *346 |
| Stage 2 | - | - | - | - | *820 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 486 | - | *453 |
| Mov Cap-2 Maneuver | - | - | - | - | *453 |
| Stage 1 | - | - | - | - | *346 |
| Stage 2 | - | - | - | - | *813 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.02 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 453 | - | - | 486 | - |
| HCM Lane V/C Ratio | 0.009 | - | - | 0.008 | - |
| HCM Ctrl Dly (s/v) | 13 | - | - | 12.5 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s
+: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1627 | 0 | 4 | 819 | 4 | 0 |
| Future Vol, veh/h | 1627 | 0 | 4 | 819 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1627 | 0 | 4 | 819 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1627 | 0 | 1963 |
| Stage 1 | - | - | - | - | 1627 |
| Stage 2 | - | - | - | - | 336 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 456 | - | *411 |
| Stage 1 | - | - | - | - | *319 |
| Stage 2 | - | - | - | - | *820 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 456 | - | *408 |
| Mov Cap-2 Maneuver | - | - | - | - | *408 |
| Stage 1 | - | - | - | - | *319 |
| Stage 2 | - | - | - | - | *813 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.92 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 408 | - | - | 456 | - |
| HCM Lane V/C Ratio | 0.01 | - | - | 0.009 | - |
| HCM Ctrl Dly (s/v) | 13.9 | - | - | 13 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1627 | 0 | 4 | 819 | 4 | 0 |
| Future Vol, veh/h | 1627 | 0 | 4 | 819 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1627 | 0 | 4 | 819 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1627 | 0 | 1963 |
| Stage 1 | - | - | - | - | 1627 |
| Stage 2 | - | - | - | - | 336 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 456 | - | *411 |
| Stage 1 | - | - | - | - | *319 |
| Stage 2 | - | - | - | - | *820 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 456 | - | *408 |
| Mov Cap-2 Maneuver | - | - | - | - | *408 |
| Stage 1 | - | - | - | - | *319 |
| Stage 2 | - | - | - | - | *813 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.92 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 408 | - | - | 456 | - |
| HCM Lane V/C Ratio | 0.01 | - | - | 0.009 | - |
| HCM Ctrl Dly (s/v) | 13.9 | - | - | 13 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

Intersection

Int Delay, s/veh 0

| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
|--------------------------|------|------|------|------|------|------|
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 901 | 0 | 4 | 1713 | 4 | 0 |
| Future Vol, veh/h | 901 | 0 | 4 | 1713 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 901 | 0 | 4 | 1713 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 901 | 0 | 1594 |
| Stage 1 | - | - | - | - | 901 |
| Stage 2 | - | - | - | - | 693 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 735 | - | *534 |
| Stage 1 | - | - | - | - | *559 |
| Stage 2 | - | - | - | - | *663 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 735 | - | *531 |
| Mov Cap-2 Maneuver | - | - | - | - | *531 |
| Stage 1 | - | - | - | - | *559 |
| Stage 2 | - | - | - | - | *659 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 11.83 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 531 | - | - | 735 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.005 | - |
| HCM Ctrl Dly (s/v) | 11.8 | - | - | 9.9 | - |
| HCM Lane LOS | B | - | - | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s
+: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 947 | 0 | 4 | 1749 | 4 | 0 |
| Future Vol, veh/h | 947 | 0 | 4 | 1749 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 947 | 0 | 4 | 1749 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | | |
|----------------------|--------|--------|--------|---|-----------|
| Conflicting Flow All | 0 | 0 | 947 | 0 | 1655 474 |
| Stage 1 | - | - | - | - | 947 - |
| Stage 2 | - | - | - | - | 708 - |
| Critical Hdwy | - | - | 5.36 | - | 5.76 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 - |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 - |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 3.93 |
| Pot Cap-1 Maneuver | - | - | 742 | - | *519 *764 |
| Stage 1 | - | - | - | - | *568 - |
| Stage 2 | - | - | - | - | *663 - |
| Platoon blocked, % | - | - | 0 | - | 0 0 |
| Mov Cap-1 Maneuver | - | - | 742 | - | *516 *764 |
| Mov Cap-2 Maneuver | - | - | - | - | *516 - |
| Stage 1 | - | - | - | - | *568 - |
| Stage 2 | - | - | - | - | *659 - |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 12.03 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 516 | - | - | 742 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.005 | - |
| HCM Ctrl Dly (s/v) | 12 | - | - | 9.9 | - |
| HCM Lane LOS | B | - | - | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/01/2026

Intersection

Int Delay, s/veh 0

| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
|--------------------------|------|------|------|------|------|------|
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 947 | 0 | 4 | 1749 | 4 | 0 |
| Future Vol, veh/h | 947 | 0 | 4 | 1749 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 947 | 0 | 4 | 1749 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 947 | 0 | 1655 |
| Stage 1 | - | - | - | - | 947 |
| Stage 2 | - | - | - | - | 708 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 742 | - | *519 |
| Stage 1 | - | - | - | - | *568 |
| Stage 2 | - | - | - | - | *663 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 742 | - | *516 |
| Mov Cap-2 Maneuver | - | - | - | - | *516 |
| Stage 1 | - | - | - | - | *568 |
| Stage 2 | - | - | - | - | *659 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 12.03 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 516 | - | - | 742 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.005 | - |
| HCM Ctrl Dly (s/v) | 12 | - | - | 9.9 | - |
| HCM Lane LOS | B | - | - | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s
+: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1661 | 0 | 4 | 822 | 4 | 0 |
| Future Vol, veh/h | 1661 | 0 | 4 | 822 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1661 | 0 | 4 | 822 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1661 | 0 | 1998 |
| Stage 1 | - | - | - | - | 1661 |
| Stage 2 | - | - | - | - | 337 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 469 | - | *429 |
| Stage 1 | - | - | - | - | *333 |
| Stage 2 | - | - | - | - | *820 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 469 | - | *426 |
| Mov Cap-2 Maneuver | - | - | - | - | *426 |
| Stage 1 | - | - | - | - | *333 |
| Stage 2 | - | - | - | - | *813 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.54 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 426 | - | - | 469 | - |
| HCM Lane V/C Ratio | 0.009 | - | - | 0.009 | - |
| HCM Ctrl Dly (s/v) | 13.5 | - | - | 12.7 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1705 | 0 | 4 | 857 | 4 | 0 |
| Future Vol, veh/h | 1705 | 0 | 4 | 857 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1705 | 0 | 4 | 857 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1705 | 0 | 2056 |
| Stage 1 | - | - | - | - | 1705 |
| Stage 2 | - | - | - | - | 351 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 439 | - | *430 |
| Stage 1 | - | - | - | - | *306 |
| Stage 2 | - | - | - | - | *803 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 439 | - | *426 |
| Mov Cap-2 Maneuver | - | - | - | - | *426 |
| Stage 1 | - | - | - | - | *306 |
| Stage 2 | - | - | - | - | *796 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.52 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 426 | - | - | 439 | - |
| HCM Lane V/C Ratio | 0.009 | - | - | 0.009 | - |
| HCM Ctrl Dly (s/v) | 13.5 | - | - | 13.3 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 1705 | 0 | 4 | 857 | 4 | 0 |
| Future Vol, veh/h | 1705 | 0 | 4 | 857 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 1705 | 0 | 4 | 857 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 1705 | 0 | 2056 |
| Stage 1 | - | - | - | - | 1705 |
| Stage 2 | - | - | - | - | 351 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 439 | - | *430 |
| Stage 1 | - | - | - | - | *306 |
| Stage 2 | - | - | - | - | *803 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 439 | - | *426 |
| Mov Cap-2 Maneuver | - | - | - | - | *426 |
| Stage 1 | - | - | - | - | *306 |
| Stage 2 | - | - | - | - | *796 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.06 | 13.52 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 426 | - | - | 439 | - |
| HCM Lane V/C Ratio | 0.009 | - | - | 0.009 | - |
| HCM Ctrl Dly (s/v) | 13.5 | - | - | 13.3 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

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| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 946 | 0 | 4 | 1798 | 4 | 0 |
| Future Vol, veh/h | 946 | 0 | 4 | 1798 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 946 | 0 | 4 | 1798 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 946 | 0 | 1673 |
| Stage 1 | - | - | - | - | 946 |
| Stage 2 | - | - | - | - | 727 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 743 | - | *519 |
| Stage 1 | - | - | - | - | *569 |
| Stage 2 | - | - | - | - | *648 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 743 | - | *516 |
| Mov Cap-2 Maneuver | - | - | - | - | *516 |
| Stage 1 | - | - | - | - | *569 |
| Stage 2 | - | - | - | - | *644 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 12.03 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 516 | - | - | 743 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.005 | - |
| HCM Ctrl Dly (s/v) | 12 | - | - | 9.9 | - |
| HCM Lane LOS | B | - | - | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 992 | 0 | 4 | 1834 | 4 | 0 |
| Future Vol, veh/h | 992 | 0 | 4 | 1834 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 992 | 0 | 4 | 1834 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 992 | 0 | 1734 |
| Stage 1 | - | - | - | - | 992 |
| Stage 2 | - | - | - | - | 742 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 700 | - | *519 |
| Stage 1 | - | - | - | - | *529 |
| Stage 2 | - | - | - | - | *648 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 700 | - | *516 |
| Mov Cap-2 Maneuver | - | - | - | - | *516 |
| Stage 1 | - | - | - | - | *529 |
| Stage 2 | - | - | - | - | *644 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 12.04 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 516 | - | - | 700 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.006 | - |
| HCM Ctrl Dly (s/v) | 12 | - | - | 10.2 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
5: Torc DWY / Frontage Rd & Lomas Blvd NE

Tierra West, LLC
02/02/2026

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑↑ | | ↑ | ↑↑↑ | ↑ | |
| Traffic Vol, veh/h | 992 | 0 | 4 | 1834 | 4 | 0 |
| Future Vol, veh/h | 992 | 0 | 4 | 1834 | 4 | 0 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | 65 | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 992 | 0 | 4 | 1834 | 4 | 0 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 992 | 0 | 1734 |
| Stage 1 | - | - | - | - | 992 |
| Stage 2 | - | - | - | - | 742 |
| Critical Hdwy | - | - | 5.36 | - | 5.76 |
| Critical Hdwy Stg 1 | - | - | - | - | 6.66 |
| Critical Hdwy Stg 2 | - | - | - | - | 6.06 |
| Follow-up Hdwy | - | - | 3.13 | - | 3.83 |
| Pot Cap-1 Maneuver | - | - | 700 | - | *519 |
| Stage 1 | - | - | - | - | *529 |
| Stage 2 | - | - | - | - | *648 |
| Platoon blocked, % | - | - | 0 | - | 0 |
| Mov Cap-1 Maneuver | - | - | 700 | - | *516 |
| Mov Cap-2 Maneuver | - | - | - | - | *516 |
| Stage 1 | - | - | - | - | *529 |
| Stage 2 | - | - | - | - | *644 |

| Approach | EB | WB | NB |
|-------------------|----|------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.02 | 12.04 |
| HCM LOS | | | B |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 516 | - | - | 700 | - |
| HCM Lane V/C Ratio | 0.008 | - | - | 0.006 | - |
| HCM Ctrl Dly (s/v) | 12 | - | - | 10.2 | - |
| HCM Lane LOS | B | - | - | B | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0 | - |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.2 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 20 | 1563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Future Vol, veh/h | 20 | 1563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 20 | 1563 | 0 | 0 | 771 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 775 | 0 | 0 | 1563 | 0 | 0 | 1911 | 2378 | 782 | 1438 | 2376 | 388 |
| Stage 1 | - | - | - | - | - | - | 1603 | 1603 | - | 773 | 773 | - |
| Stage 2 | - | - | - | - | - | - | 308 | 775 | - | 665 | 1603 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 786 | - | - | 501 | - | - | *433 | 148 | *670 | *567 | 149 | *799 |
| Stage 1 | - | - | - | - | - | - | *300 | 378 | - | *580 | 621 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 620 | - | *688 | 378 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 786 | - | - | 501 | - | - | *416 | 144 | *670 | *553 | 145 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *416 | 144 | - | *553 | 145 | - |
| Stage 1 | - | - | - | - | - | - | *292 | 369 | - | *580 | 621 | - |
| Stage 2 | - | - | - | - | - | - | *808 | 620 | - | *670 | 369 | - |

| Approach | EB | WB | NB | SB |
|-------------------|------|----|----|-------|
| HCM Ctrl Dly, s/v | 0.12 | 0 | 0 | 10.47 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 786 | - | - | 501 | - | - | 678 |
| HCM Lane V/C Ratio | - | 0.025 | - | - | - | - | - | 0.029 |
| HCM Ctrl Dly (s/v) | 0 | 9.7 | - | - | 0 | - | - | 10.5 |
| HCM Lane LOS | | A | A | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0.1 | - | - | 0 | - | - | 0.1 |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| Future Vol, veh/h | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 788 | 0 | 0 | 1614 | 0 | 0 | 1993 | 2468 | 807 | 1501 | 2484 | 394 |
| Stage 1 | - | - | - | - | - | - | 1636 | 1636 | - | 830 | 830 | - |
| Stage 2 | - | - | - | - | - | - | 358 | 832 | - | 671 | 1654 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 774 | - | - | 464 | - | - | *364 | 121 | *670 | *567 | 116 | *799 |
| Stage 1 | - | - | - | - | - | - | *280 | 361 | - | *524 | 581 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 580 | - | *688 | 352 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 774 | - | - | 464 | - | - | *333 | 112 | *670 | *506 | 108 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *333 | 112 | - | *506 | 108 | - |
| Stage 1 | - | - | - | - | - | - | *273 | 352 | - | *500 | 554 | - |
| Stage 2 | - | - | - | - | - | - | *769 | 553 | - | *644 | 343 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.12 | | | 0.36 | | | 13.59 | | | 10.72 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 465 | 774 | - | - | 464 | - | - | 649 |
| HCM Lane V/C Ratio | 0.099 | 0.026 | - | - | 0.047 | - | - | 0.031 |
| HCM Ctrl Dly (s/v) | 13.6 | 9.8 | - | - | 13.1 | - | - | 10.7 |
| HCM Lane LOS | B | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0.1 | - | - | 0.1 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| Future Vol, veh/h | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 20 | 1577 | 37 | 22 | 784 | 4 | 20 | 0 | 26 | 8 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 788 | 0 | 0 | 1614 | 0 | 0 | 1993 | 2468 | 807 | 1501 | 2484 | 394 |
| Stage 1 | - | - | - | - | - | - | 1636 | 1636 | - | 830 | 830 | - |
| Stage 2 | - | - | - | - | - | - | 358 | 832 | - | 671 | 1654 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 774 | - | - | 464 | - | - | *364 | 121 | *670 | *567 | 116 | *799 |
| Stage 1 | - | - | - | - | - | - | *280 | 361 | - | *524 | 581 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 580 | - | *688 | 352 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 774 | - | - | 464 | - | - | *333 | 112 | *670 | *506 | 108 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *240 | 242 | - | *456 | 230 | - |
| Stage 1 | - | - | - | - | - | - | *273 | 352 | - | *500 | 554 | - |
| Stage 2 | - | - | - | - | - | - | *769 | 553 | - | *644 | 343 | - |

| Approach | EB | WB | NB | SB |
|-------------------|------|------|------|-------|
| HCM Ctrl Dly, s/v | 0.12 | 0.36 | 15.9 | 11.06 |
| HCM LOS | | | C | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 376 | 774 | - | - | 464 | - | - | 614 |
| HCM Lane V/C Ratio | 0.122 | 0.026 | - | - | 0.047 | - | - | 0.033 |
| HCM Ctrl Dly (s/v) | 15.9 | 9.8 | - | - | 13.1 | - | - | 11.1 |
| HCM Lane LOS | C | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.4 | 0.1 | - | - | 0.1 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 12 | 885 | 4 | 0 | 1677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| Future Vol, veh/h | 12 | 885 | 4 | 0 | 1677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 12 | 885 | 4 | 0 | 1677 | 4 | 0 | 0 | 8 | 4 | 0 | 41 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1681 | 0 | 0 | 889 | 0 | 0 | 1582 | 2592 | 445 | 2057 | 2592 | 841 |
| Stage 1 | - | - | - | - | - | - | 911 | 911 | - | 1679 | 1679 | - |
| Stage 2 | - | - | - | - | - | - | 671 | 1681 | - | 378 | 913 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 473 | - | - | 746 | - | - | *523 | 121 | *779 | *457 | 121 | *646 |
| Stage 1 | - | - | - | - | - | - | *521 | 574 | - | *307 | 380 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 379 | - | *800 | 573 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 473 | - | - | 746 | - | - | *477 | 118 | *779 | *440 | 118 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *477 | 118 | - | *440 | 118 | - |
| Stage 1 | - | - | - | - | - | - | *508 | 559 | - | *307 | 380 | - |
| Stage 2 | - | - | - | - | - | - | *621 | 379 | - | *771 | 558 | - |

| Approach | EB | WB | NB | SB |
|-------------------|------|----|------|-------|
| HCM Ctrl Dly, s/v | 0.17 | 0 | 9.67 | 11.26 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | 779 | 473 | - | - | 746 | - | - | 620 |
| HCM Lane V/C Ratio | 0.01 | 0.025 | - | - | - | - | - | 0.073 |
| HCM Ctrl Dly (s/v) | 9.7 | 12.8 | - | - | 0 | - | - | 11.3 |
| HCM Lane LOS | A | B | - | - | A | - | - | B |
| HCM 95th %tile Q(veh) | 0 | 0.1 | - | - | 0 | - | - | 0.2 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| Future Vol, veh/h | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1690 | 0 | 0 | 943 | 0 | 0 | 1677 | 2693 | 472 | 2129 | 2711 | 845 |
| Stage 1 | - | - | - | - | - | - | 947 | 947 | - | 1744 | 1744 | - |
| Stage 2 | - | - | - | - | - | - | 730 | 1746 | - | 385 | 967 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 467 | - | - | 697 | - | - | *523 | 95 | *779 | *387 | 90 | *646 |
| Stage 1 | - | - | - | - | - | - | *488 | 550 | - | *266 | 345 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 344 | - | *800 | 537 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 467 | - | - | 697 | - | - | *458 | 88 | *779 | *346 | 85 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *458 | 88 | - | *346 | 85 | - |
| Stage 1 | - | - | - | - | - | - | *475 | 536 | - | *255 | 331 | - |
| Stage 2 | - | - | - | - | - | - | *596 | 330 | - | *745 | 523 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.16 | | | 0.17 | | | 11.7 | | | 11.49 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 597 | 467 | - | - | 697 | - | - | 600 |
| HCM Lane V/C Ratio | 0.1 | 0.026 | - | - | 0.04 | - | - | 0.075 |
| HCM Ctrl Dly (s/v) | 11.7 | 12.9 | - | - | 10.4 | - | - | 11.5 |
| HCM Lane LOS | B | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0.1 | - | - | 0.1 | - | - | 0.2 |

| Notes | |
|----------------------------|--------------------------------|
| ~: Volume exceeds capacity | \$: Delay exceeds 300s |
| +: Computation Not Defined | *: All major volume in platoon |

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| Future Vol, veh/h | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 12 | 903 | 40 | 28 | 1686 | 4 | 26 | 0 | 34 | 4 | 0 | 41 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1690 | 0 | 0 | 943 | 0 | 0 | 1677 | 2693 | 472 | 2129 | 2711 | 845 |
| Stage 1 | - | - | - | - | - | - | 947 | 947 | - | 1744 | 1744 | - |
| Stage 2 | - | - | - | - | - | - | 730 | 1746 | - | 385 | 967 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 467 | - | - | 697 | - | - | *523 | 95 | *779 | *387 | 90 | *646 |
| Stage 1 | - | - | - | - | - | - | *488 | 550 | - | *266 | 345 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 344 | - | *800 | 537 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 467 | - | - | 697 | - | - | *458 | 88 | *779 | *346 | 85 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *432 | 219 | - | *206 | 218 | - |
| Stage 1 | - | - | - | - | - | - | *475 | 536 | - | *255 | 331 | - |
| Stage 2 | - | - | - | - | - | - | *596 | 330 | - | *745 | 523 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.16 | | | 0.17 | | | 11.95 | | | 12.22 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 578 | 467 | - | - | 697 | - | - | 543 |
| HCM Lane V/C Ratio | 0.104 | 0.026 | - | - | 0.04 | - | - | 0.083 |
| HCM Ctrl Dly (s/v) | 11.9 | 12.9 | - | - | 10.4 | - | - | 12.2 |
| HCM Lane LOS | B | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0.1 | - | - | 0.1 | - | - | 0.3 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.2 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↖ ↑↑↑ | | | ↖ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 21 | 1640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| Future Vol, veh/h | 21 | 1640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 21 | 1640 | 0 | 0 | 809 | 4 | 0 | 0 | 0 | 9 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 813 | 0 | 0 | 1640 | 0 | 0 | 2006 | 2495 | 820 | 1509 | 2493 | 407 |
| Stage 1 | - | - | - | - | - | - | 1682 | 1682 | - | 811 | 811 | - |
| Stage 2 | - | - | - | - | - | - | 324 | 813 | - | 698 | 1682 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 750 | - | - | 484 | - | - | *404 | 126 | *654 | *551 | 127 | *799 |
| Stage 1 | - | - | - | - | - | - | *287 | 364 | - | *542 | 594 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 593 | - | *671 | 364 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 750 | - | - | 484 | - | - | *386 | 123 | *654 | *536 | 123 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *386 | 123 | - | *536 | 123 | - |
| Stage 1 | - | - | - | - | - | - | *279 | 354 | - | *542 | 594 | - |
| Stage 2 | - | - | - | - | - | - | *807 | 593 | - | *652 | 354 | - |

| Approach | EB | WB | NB | SB |
|-------------------|------|----|----|------|
| HCM Ctrl Dly, s/v | 0.13 | 0 | 0 | 10.6 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 750 | - | - | 484 | - | - | 665 |
| HCM Lane V/C Ratio | - | 0.028 | - | - | - | - | - | 0.033 |
| HCM Ctrl Dly (s/v) | 0 | 9.9 | - | - | 0 | - | - | 10.6 |
| HCM Lane LOS | | A | A | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0.1 | - | - | 0 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| Future Vol, veh/h | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 826 | 0 | 0 | 1691 | 0 | 0 | 2087 | 2585 | 846 | 1572 | 2601 | 413 |
| Stage 1 | - | - | - | - | - | - | 1715 | 1715 | - | 868 | 868 | - |
| Stage 2 | - | - | - | - | - | - | 373 | 870 | - | 704 | 1733 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 739 | - | - | 448 | - | - | *337 | 102 | *654 | *551 | 98 | *799 |
| Stage 1 | - | - | - | - | - | - | *267 | 347 | - | *490 | 556 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 555 | - | *671 | 338 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 739 | - | - | 448 | - | - | *306 | 94 | *654 | *489 | 91 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *306 | 94 | - | *489 | 91 | - |
| Stage 1 | - | - | - | - | - | - | *260 | 338 | - | *466 | 529 | - |
| Stage 2 | - | - | - | - | - | - | *767 | 528 | - | *626 | 329 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.12 | | | 0.35 | | | 14.18 | | | 10.88 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 438 | 739 | - | - | 448 | - | - | 634 |
| HCM Lane V/C Ratio | 0.105 | 0.028 | - | - | 0.049 | - | - | 0.035 |
| HCM Ctrl Dly (s/v) | 14.2 | 10 | - | - | 13.4 | - | - | 10.9 |
| HCM Lane LOS | B | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0.1 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| Future Vol, veh/h | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 21 | 1654 | 37 | 22 | 822 | 4 | 20 | 0 | 26 | 9 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 826 | 0 | 0 | 1691 | 0 | 0 | 2087 | 2585 | 846 | 1572 | 2601 | 413 |
| Stage 1 | - | - | - | - | - | - | 1715 | 1715 | - | 868 | 868 | - |
| Stage 2 | - | - | - | - | - | - | 373 | 870 | - | 704 | 1733 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 739 | - | - | 448 | - | - | *337 | 102 | *654 | *551 | 98 | *799 |
| Stage 1 | - | - | - | - | - | - | *267 | 347 | - | *490 | 556 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 555 | - | *671 | 338 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 739 | - | - | 448 | - | - | *306 | 94 | *654 | *489 | 91 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *232 | 226 | - | *421 | 215 | - |
| Stage 1 | - | - | - | - | - | - | *260 | 338 | - | *466 | 529 | - |
| Stage 2 | - | - | - | - | - | - | *767 | 528 | - | *626 | 329 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|------|--|--|
| HCM Ctrl Dly, s/v | 0.12 | | | 0.35 | | | 16.28 | | | 11.4 | | |
| HCM LOS | | | | | | | C | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 365 | 739 | - | - | 448 | - | - | 584 |
| HCM Lane V/C Ratio | 0.126 | 0.028 | - | - | 0.049 | - | - | 0.038 |
| HCM Ctrl Dly (s/v) | 16.3 | 10 | - | - | 13.4 | - | - | 11.4 |
| HCM Lane LOS | C | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.4 | 0.1 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCM 7th TWSC
6: West DWY - Lobo Pl/Legion Rd & Lomas Blvd NE

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 13 | 929 | 4 | 0 | 1759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| Future Vol, veh/h | 13 | 929 | 4 | 0 | 1759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 13 | 929 | 4 | 0 | 1759 | 4 | 0 | 0 | 9 | 4 | 0 | 43 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1763 | 0 | 0 | 933 | 0 | 0 | 1661 | 2720 | 467 | 2159 | 2720 | 882 |
| Stage 1 | - | - | - | - | - | - | 957 | 957 | - | 1761 | 1761 | - |
| Stage 2 | - | - | - | - | - | - | 704 | 1763 | - | 398 | 959 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 450 | - | - | 706 | - | - | *507 | 98 | *779 | *412 | 98 | *631 |
| Stage 1 | - | - | - | - | - | - | *479 | 543 | - | *289 | 362 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 360 | - | *800 | 542 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 450 | - | - | 706 | - | - | *459 | 95 | *779 | *396 | 95 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *459 | 95 | - | *396 | 95 | - |
| Stage 1 | - | - | - | - | - | - | *465 | 527 | - | *289 | 362 | - |
| Stage 2 | - | - | - | - | - | - | *604 | 360 | - | *768 | 526 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|----|--|--|------|--|--|------|--|--|
| HCM Ctrl Dly, s/v | 0.18 | | | 0 | | | 9.67 | | | 11.5 | | |
| HCM LOS | | | | | | | A | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | 779 | 450 | - | - | 706 | - | - | 601 |
| HCM Lane V/C Ratio | 0.012 | 0.029 | - | - | - | - | - | 0.078 |
| HCM Ctrl Dly (s/v) | 9.7 | 13.2 | - | - | 0 | - | - | 11.5 |
| HCM Lane LOS | A | B | - | - | A | - | - | B |
| HCM 95th %tile Q(veh) | 0 | 0.1 | - | - | 0 | - | - | 0.3 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| Future Vol, veh/h | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1772 | 0 | 0 | 987 | 0 | 0 | 1756 | 2821 | 494 | 2231 | 2839 | 886 |
| Stage 1 | - | - | - | - | - | - | 993 | 993 | - | 1826 | 1826 | - |
| Stage 2 | - | - | - | - | - | - | 763 | 1828 | - | 405 | 1013 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 444 | - | - | 705 | - | - | *492 | 84 | *764 | *398 | 80 | *631 |
| Stage 1 | - | - | - | - | - | - | *498 | 554 | - | *249 | 328 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 327 | - | *784 | 540 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 444 | - | - | 705 | - | - | *428 | 78 | *764 | *354 | 74 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *428 | 78 | - | *354 | 74 | - |
| Stage 1 | - | - | - | - | - | - | *483 | 537 | - | *239 | 315 | - |
| Stage 2 | - | - | - | - | - | - | *580 | 314 | - | *727 | 524 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.17 | | | 0.16 | | | 12.04 | | | 11.61 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 572 | 444 | - | - | 705 | - | - | 592 |
| HCM Lane V/C Ratio | 0.107 | 0.029 | - | - | 0.04 | - | - | 0.079 |
| HCM Ctrl Dly (s/v) | 12 | 13.4 | - | - | 10.3 | - | - | 11.6 |
| HCM Lane LOS | B | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.4 | 0.1 | - | - | 0.1 | - | - | 0.3 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| Future Vol, veh/h | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 65 | - | - | 135 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 13 | 947 | 40 | 28 | 1768 | 4 | 26 | 0 | 35 | 4 | 0 | 43 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1772 | 0 | 0 | 987 | 0 | 0 | 1756 | 2821 | 494 | 2231 | 2839 | 886 |
| Stage 1 | - | - | - | - | - | - | 993 | 993 | - | 1826 | 1826 | - |
| Stage 2 | - | - | - | - | - | - | 763 | 1828 | - | 405 | 1013 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 444 | - | - | 705 | - | - | *492 | 84 | *764 | *398 | 80 | *631 |
| Stage 1 | - | - | - | - | - | - | *498 | 554 | - | *249 | 328 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 327 | - | *784 | 540 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 444 | - | - | 705 | - | - | *428 | 78 | *764 | *354 | 74 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *427 | 208 | - | *166 | 208 | - |
| Stage 1 | - | - | - | - | - | - | *483 | 537 | - | *239 | 315 | - |
| Stage 2 | - | - | - | - | - | - | *580 | 314 | - | *727 | 524 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|------|--|--|------|--|--|-------|--|--|-------|--|--|
| HCM Ctrl Dly, s/v | 0.17 | | | 0.16 | | | 12.05 | | | 12.78 | | |
| HCM LOS | | | | | | | B | | | B | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 572 | 444 | - | - | 705 | - | - | 510 |
| HCM Lane V/C Ratio | 0.107 | 0.029 | - | - | 0.04 | - | - | 0.092 |
| HCM Ctrl Dly (s/v) | 12 | 13.4 | - | - | 10.3 | - | - | 12.8 |
| HCM Lane LOS | B | B | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.4 | 0.1 | - | - | 0.1 | - | - | 0.3 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1573 | 0 | 0 | 763 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 763 | 0 | 0 | 1573 | 0 | 0 | 1878 | 2336 | 787 | 1392 | 2336 | 382 |
| Stage 1 | - | - | - | - | - | - | 1573 | 1573 | - | 763 | 763 | - |
| Stage 2 | - | - | - | - | - | - | 305 | 763 | - | 629 | 1573 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 798 | - | - | 494 | - | - | *465 | 163 | *670 | *567 | 163 | *799 |
| Stage 1 | - | - | - | - | - | - | *320 | 395 | - | *590 | 628 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 628 | - | *688 | 395 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 798 | - | - | 494 | - | - | *462 | 163 | *670 | *567 | 163 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *462 | 163 | - | *567 | 163 | - |
| Stage 1 | - | - | - | - | - | - | *320 | 395 | - | *590 | 628 | - |
| Stage 2 | - | - | - | - | - | - | *816 | 628 | - | *688 | 395 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 9.53 |
| HCM LOS | | | A | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 798 | - | - | 494 | - | - | 799 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 9.5 |
| HCM Lane LOS | A | A | - | - | A | - | - | A |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 779 | 0 | 0 | 1606 | 0 | 0 | 1972 | 2439 | 803 | 1478 | 2451 | 390 |
| Stage 1 | - | - | - | - | - | - | 1594 | 1594 | - | 845 | 845 | - |
| Stage 2 | - | - | - | - | - | - | 378 | 845 | - | 633 | 1606 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 782 | - | - | 470 | - | - | *381 | 129 | *670 | *567 | 125 | *799 |
| Stage 1 | - | - | - | - | - | - | *306 | 383 | - | *511 | 571 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 571 | - | *688 | 377 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 782 | - | - | 470 | - | - | *352 | 120 | *670 | *510 | 116 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *352 | 120 | - | *510 | 116 | - |
| Stage 1 | - | - | - | - | - | - | *306 | 383 | - | *475 | 531 | - |
| Stage 2 | - | - | - | - | - | - | *759 | 531 | - | *664 | 377 | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|-------------------|----|--|--|------|--|--|-------|--|--|------|--|--|
| HCM Ctrl Dly, s/v | 0 | | | 0.54 | | | 13.48 | | | 9.53 | | |
| HCM LOS | | | | | | | B | | | A | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 468 | 782 | - | - | 470 | - | - | 799 |
| HCM Lane V/C Ratio | 0.094 | - | - | - | 0.07 | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 13.5 | 0 | - | - | 13.2 | - | - | 9.5 |
| HCM Lane LOS | B | A | - | - | B | - | - | A |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↱ ↑↑↑ | | | ↱ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1582 | 24 | 33 | 779 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 779 | 0 | 0 | 1606 | 0 | 0 | 1972 | 2439 | 803 | 1478 | 2451 | 390 |
| Stage 1 | - | - | - | - | - | - | 1594 | 1594 | - | 845 | 845 | - |
| Stage 2 | - | - | - | - | - | - | 378 | 845 | - | 633 | 1606 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 782 | - | - | 470 | - | - | *381 | 129 | *670 | *567 | 125 | *799 |
| Stage 1 | - | - | - | - | - | - | *306 | 383 | - | *511 | 571 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 571 | - | *688 | 377 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 782 | - | - | 470 | - | - | *352 | 120 | *670 | *510 | 116 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *274 | 256 | - | *421 | 240 | - |
| Stage 1 | - | - | - | - | - | - | *306 | 383 | - | *475 | 531 | - |
| Stage 2 | - | - | - | - | - | - | *759 | 531 | - | *664 | 377 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|------|
| HCM Ctrl Dly, s/v | 0 | 0.54 | 15.22 | 9.53 |
| HCM LOS | | | C | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|------|-----|-----|-------|
| Capacity (veh/h) | 396 | 782 | - | - | 470 | - | - | 799 |
| HCM Lane V/C Ratio | 0.111 | - | - | - | 0.07 | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 15.2 | 0 | - | - | 13.2 | - | - | 9.5 |
| HCM Lane LOS | C | A | - | - | B | - | - | A |
| HCM 95th %tile Q(veh) | 0.4 | 0 | - | - | 0.2 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 914 | 0 | 0 | 1673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Future Vol, veh/h | 0 | 914 | 0 | 0 | 1673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 914 | 0 | 0 | 1673 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1673 | 0 | 0 | 914 | 0 | 0 | 1583 | 2587 | 457 | 2039 | 2587 | 837 |
| Stage 1 | - | - | - | - | - | - | 914 | 914 | - | 1673 | 1673 | - |
| Stage 2 | - | - | - | - | - | - | 669 | 1673 | - | 366 | 914 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 479 | - | - | 723 | - | - | *523 | 123 | *779 | *476 | 123 | *646 |
| Stage 1 | - | - | - | - | - | - | *518 | 572 | - | *311 | 383 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 383 | - | *800 | 572 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 479 | - | - | 723 | - | - | *513 | 123 | *779 | *476 | 123 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *513 | 123 | - | *476 | 123 | - |
| Stage 1 | - | - | - | - | - | - | *518 | 572 | - | *311 | 383 | - |
| Stage 2 | - | - | - | - | - | - | *651 | 383 | - | *800 | 572 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|-------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 10.68 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 479 | - | - | 723 | - | - | 646 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.019 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 10.7 |
| HCM Lane LOS | | A | A | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| Future Vol, veh/h | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1693 | 0 | 0 | 950 | 0 | 0 | 1686 | 2702 | 475 | 2135 | 2715 | 847 |
| Stage 1 | - | - | - | - | - | - | 937 | 937 | - | 1765 | 1765 | - |
| Stage 2 | - | - | - | - | - | - | 749 | 1765 | - | 370 | 950 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 465 | - | - | 691 | - | - | *523 | 92 | *779 | *382 | 89 | *646 |
| Stage 1 | - | - | - | - | - | - | *497 | 556 | - | *254 | 334 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 334 | - | *800 | 548 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 465 | - | - | 691 | - | - | *486 | 88 | *779 | *350 | 85 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *486 | 88 | - | *350 | 85 | - |
| Stage 1 | - | - | - | - | - | - | *497 | 556 | - | *241 | 317 | - |
| Stage 2 | - | - | - | - | - | - | *617 | 317 | - | *773 | 548 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.22 | 11.54 | 10.68 |
| HCM LOS | | | B | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 602 | 465 | - | - | 691 | - | - | 646 |
| HCM Lane V/C Ratio | 0.085 | - | - | - | 0.052 | - | - | 0.019 |
| HCM Ctrl Dly (s/v) | 11.5 | 0 | - | - | 10.5 | - | - | 10.7 |
| HCM Lane LOS | B | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| Future Vol, veh/h | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 924 | 26 | 36 | 1693 | 0 | 25 | 0 | 26 | 0 | 0 | 12 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1693 | 0 | 0 | 950 | 0 | 0 | 1686 | 2702 | 475 | 2135 | 2715 | 847 |
| Stage 1 | - | - | - | - | - | - | 937 | 937 | - | 1765 | 1765 | - |
| Stage 2 | - | - | - | - | - | - | 749 | 1765 | - | 370 | 950 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 465 | - | - | 691 | - | - | *523 | 92 | *779 | *382 | 89 | *646 |
| Stage 1 | - | - | - | - | - | - | *497 | 556 | - | *254 | 334 | - |
| Stage 2 | - | - | - | - | - | - | *663 | 334 | - | *800 | 548 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 465 | - | - | 691 | - | - | *486 | 88 | *779 | *350 | 85 | *646 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *453 | 221 | - | *180 | 215 | - |
| Stage 1 | - | - | - | - | - | - | *497 | 556 | - | *241 | 317 | - |
| Stage 2 | - | - | - | - | - | - | *617 | 317 | - | *773 | 548 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.22 | 11.86 | 10.68 |
| HCM LOS | | | B | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 576 | 465 | - | - | 691 | - | - | 646 |
| HCM Lane V/C Ratio | 0.089 | - | - | - | 0.052 | - | - | 0.019 |
| HCM Ctrl Dly (s/v) | 11.9 | 0 | - | - | 10.5 | - | - | 10.7 |
| HCM Lane LOS | B | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 0 | 1651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1651 | 0 | 0 | 801 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 801 | 0 | 0 | 1651 | 0 | 0 | 1971 | 2452 | 826 | 1461 | 2452 | 401 |
| Stage 1 | - | - | - | - | - | - | 1651 | 1651 | - | 801 | 801 | - |
| Stage 2 | - | - | - | - | - | - | 320 | 801 | - | 660 | 1651 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 762 | - | - | 476 | - | - | *435 | 140 | *654 | *551 | 140 | *799 |
| Stage 1 | - | - | - | - | - | - | *307 | 381 | - | *552 | 601 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 601 | - | *671 | 381 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 762 | - | - | 476 | - | - | *433 | 140 | *654 | *551 | 140 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *433 | 140 | - | *551 | 140 | - |
| Stage 1 | - | - | - | - | - | - | *307 | 381 | - | *552 | 601 | - |
| Stage 2 | - | - | - | - | - | - | *816 | 601 | - | *671 | 381 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 9.53 |
| HCM LOS | | | A | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 762 | - | - | 476 | - | - | 799 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 9.5 |
| HCM Lane LOS | A | A | - | - | A | - | - | A |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 817 | 0 | 0 | 1684 | 0 | 0 | 2065 | 2555 | 842 | 1547 | 2567 | 409 |
| Stage 1 | - | - | - | - | - | - | 1672 | 1672 | - | 883 | 883 | - |
| Stage 2 | - | - | - | - | - | - | 393 | 883 | - | 664 | 1684 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 747 | - | - | 453 | - | - | *354 | 110 | *654 | *551 | 107 | *799 |
| Stage 1 | - | - | - | - | - | - | *293 | 370 | - | *478 | 547 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 547 | - | *671 | 363 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 747 | - | - | 453 | - | - | *327 | 102 | *654 | *493 | 99 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *327 | 102 | - | *493 | 99 | - |
| Stage 1 | - | - | - | - | - | - | *293 | 370 | - | *443 | 507 | - |
| Stage 2 | - | - | - | - | - | - | *756 | 507 | - | *647 | 363 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|------|
| HCM Ctrl Dly, s/v | 0 | 0.53 | 14.03 | 9.53 |
| HCM LOS | | | B | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 443 | 747 | - | - | 453 | - | - | 799 |
| HCM Lane V/C Ratio | 0.099 | - | - | - | 0.073 | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 14 | 0 | - | - | 13.6 | - | - | 9.5 |
| HCM Lane LOS | B | A | - | - | B | - | - | A |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.5 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Future Vol, veh/h | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 1660 | 24 | 33 | 817 | 0 | 21 | 0 | 23 | 0 | 0 | 4 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 817 | 0 | 0 | 1684 | 0 | 0 | 2065 | 2555 | 842 | 1547 | 2567 | 409 |
| Stage 1 | - | - | - | - | - | - | 1672 | 1672 | - | 883 | 883 | - |
| Stage 2 | - | - | - | - | - | - | 393 | 883 | - | 664 | 1684 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 747 | - | - | 453 | - | - | *354 | 110 | *654 | *551 | 107 | *799 |
| Stage 1 | - | - | - | - | - | - | *293 | 370 | - | *478 | 547 | - |
| Stage 2 | - | - | - | - | - | - | *820 | 547 | - | *671 | 363 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 747 | - | - | 453 | - | - | *327 | 102 | *654 | *493 | 99 | *799 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *265 | 240 | - | *372 | 225 | - |
| Stage 1 | - | - | - | - | - | - | *293 | 370 | - | *443 | 507 | - |
| Stage 2 | - | - | - | - | - | - | *756 | 507 | - | *647 | 363 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|------|
| HCM Ctrl Dly, s/v | 0 | 0.53 | 15.57 | 9.53 |
| HCM LOS | | | C | A |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 385 | 747 | - | - | 453 | - | - | 799 |
| HCM Lane V/C Ratio | 0.114 | - | - | - | 0.073 | - | - | 0.005 |
| HCM Ctrl Dly (s/v) | 15.6 | 0 | - | - | 13.6 | - | - | 9.5 |
| HCM Lane LOS | C | A | - | - | B | - | - | A |
| HCM 95th %tile Q(veh) | 0.4 | 0 | - | - | 0.2 | - | - | 0 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.1 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 959 | 0 | 0 | 1755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| Future Vol, veh/h | 0 | 959 | 0 | 0 | 1755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 959 | 0 | 0 | 1755 | 0 | 0 | 0 | 0 | 0 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1755 | 0 | 0 | 959 | 0 | 0 | 1661 | 2714 | 480 | 2139 | 2714 | 878 |
| Stage 1 | - | - | - | - | - | - | 959 | 959 | - | 1755 | 1755 | - |
| Stage 2 | - | - | - | - | - | - | 702 | 1755 | - | 384 | 959 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 456 | - | - | 731 | - | - | *492 | 111 | *764 | *492 | 111 | *631 |
| Stage 1 | - | - | - | - | - | - | *530 | 577 | - | *293 | 365 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 365 | - | *784 | 577 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 456 | - | - | 731 | - | - | *482 | 111 | *764 | *492 | 111 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *482 | 111 | - | *492 | 111 | - |
| Stage 1 | - | - | - | - | - | - | *530 | 577 | - | *293 | 365 | - |
| Stage 2 | - | - | - | - | - | - | *635 | 365 | - | *784 | 577 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|----|----|-------|
| HCM Ctrl Dly, s/v | 0 | 0 | 0 | 10.82 |
| HCM LOS | | | A | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-----|-----|-----|-------|
| Capacity (veh/h) | - | 456 | - | - | 731 | - | - | 631 |
| HCM Lane V/C Ratio | - | - | - | - | - | - | - | 0.021 |
| HCM Ctrl Dly (s/v) | 0 | 0 | - | - | 0 | - | - | 10.8 |
| HCM Lane LOS | | A | A | - | A | - | - | B |
| HCM 95th %tile Q(veh) | - | 0 | - | - | 0 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| Future Vol, veh/h | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1775 | 0 | 0 | 995 | 0 | 0 | 1764 | 2829 | 498 | 2235 | 2842 | 888 |
| Stage 1 | - | - | - | - | - | - | 982 | 982 | - | 1847 | 1847 | - |
| Stage 2 | - | - | - | - | - | - | 782 | 1847 | - | 388 | 995 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 441 | - | - | 698 | - | - | *492 | 82 | *764 | *394 | 79 | *631 |
| Stage 1 | - | - | - | - | - | - | *508 | 561 | - | *237 | 317 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 317 | - | *784 | 552 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 441 | - | - | 698 | - | - | *457 | 78 | *764 | *361 | 75 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *457 | 78 | - | *361 | 75 | - |
| Stage 1 | - | - | - | - | - | - | *508 | 561 | - | *225 | 301 | - |
| Stage 2 | - | - | - | - | - | - | *602 | 301 | - | *758 | 552 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.21 | 11.87 | 10.82 |
| HCM LOS | | | B | B |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 575 | 441 | - | - | 698 | - | - | 631 |
| HCM Lane V/C Ratio | 0.089 | - | - | - | 0.052 | - | - | 0.021 |
| HCM Ctrl Dly (s/v) | 11.9 | 0 | - | - | 10.4 | - | - | 10.8 |
| HCM Lane LOS | B | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|-------|------|------|-------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | ↵ ↑↑↑ | | | ↵ ↑↑↑ | | | ↕ | | | ↕ | | |
| Traffic Vol, veh/h | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| Future Vol, veh/h | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | 125 | - | - | 110 | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 1 | - | - | 1 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| Heavy Vehicles, % | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 | 3 |
| Mvmt Flow | 0 | 969 | 26 | 36 | 1775 | 0 | 25 | 0 | 26 | 0 | 0 | 13 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | |
|----------------------|--------|---|---|--------|---|---|--------|------|------|--------|------|------|
| Conflicting Flow All | 1775 | 0 | 0 | 995 | 0 | 0 | 1764 | 2829 | 498 | 2235 | 2842 | 888 |
| Stage 1 | - | - | - | - | - | - | 982 | 982 | - | 1847 | 1847 | - |
| Stage 2 | - | - | - | - | - | - | 782 | 1847 | - | 388 | 995 | - |
| Critical Hdwy | 5.36 | - | - | 5.36 | - | - | 6.46 | 6.56 | 7.16 | 6.46 | 6.56 | 7.16 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | 7.36 | 5.56 | - | 7.36 | 5.56 | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | 6.76 | 5.56 | - | 6.76 | 5.56 | - |
| Follow-up Hdwy | 3.13 | - | - | 3.13 | - | - | 3.83 | 4.03 | 3.93 | 3.83 | 4.03 | 3.93 |
| Pot Cap-1 Maneuver | 441 | - | - | 698 | - | - | *492 | 82 | *764 | *394 | 79 | *631 |
| Stage 1 | - | - | - | - | - | - | *508 | 561 | - | *237 | 317 | - |
| Stage 2 | - | - | - | - | - | - | *648 | 317 | - | *784 | 552 | - |
| Platoon blocked, % | 0 | - | - | 0 | - | - | 0 | 0 | 0 | 0 | 0 | 0 |
| Mov Cap-1 Maneuver | 441 | - | - | 698 | - | - | *457 | 78 | *764 | *361 | 75 | *631 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | *452 | 210 | - | *130 | 205 | - |
| Stage 1 | - | - | - | - | - | - | *508 | 561 | - | *225 | 301 | - |
| Stage 2 | - | - | - | - | - | - | *602 | 301 | - | *758 | 552 | - |

| Approach | EB | WB | NB | SB |
|-------------------|----|------|-------|-------|
| HCM Ctrl Dly, s/v | 0 | 0.21 | 11.92 | 10.82 |
| HCM LOS | | | B | B |

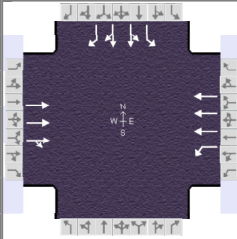
| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 571 | 441 | - | - | 698 | - | - | 631 |
| HCM Lane V/C Ratio | 0.089 | - | - | - | 0.052 | - | - | 0.021 |
| HCM Ctrl Dly (s/v) | 11.9 | 0 | - | - | 10.4 | - | - | 10.8 |
| HCM Lane LOS | B | A | - | - | B | - | - | B |
| HCM 95th %tile Q(veh) | 0.3 | 0 | - | - | 0.2 | - | - | 0.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s
 +: Computation Not Defined *: All major volume in platoon

HCS Signalized Intersection Results Summary

A - 180

| General Information | | | | Intersection Information | | | |
|---------------------|--|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2028_AN_ExportToHCS.xus | | | | |
| Project Description | 2028 AM Peak Hour No Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 888 | 0 | 41 | 755 | | | | | 840 | 666 | 763 |

| Signal Information | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|-----|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 58 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | | 6.0 | 39.0 | 49.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 44.0 | 11.0 | 55.0 | | | | 55.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g _s), s | | | 3.5 | | | | | 51.0 |
| Green Extension Time (g _e), s | | 0.0 | 0.0 | 0.0 | | | | 0.0 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 1.00 |

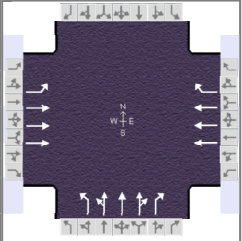
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 888 | 0 | 41 | 751 | | | | | 840 | 666 | 763 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 13.5 | 0.0 | 1.5 | 13.6 | | | | | 49.0 | 13.3 | 49.0 |
| Cycle Queue Clearance Time (g _c), s | | 13.5 | 0.0 | 1.5 | 13.6 | | | | | 49.0 | 13.3 | 49.0 |
| Green Ratio (g/C) | | 0.35 | | 0.43 | 0.45 | | | | | 0.45 | 0.45 | 0.45 |
| Capacity (c), veh/h | | 1974 | | 306 | 2297 | | | | | 787 | 1653 | 700 |
| Volume-to-Capacity Ratio (X) | | 0.450 | 0.000 | 0.133 | 0.327 | | | | | 1.067 | 0.403 | 1.089 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 256 | 0 | 32 | 255 | | | | | 1076 | 249 | 1041 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 10.0 | 0.0 | 1.3 | 10.0 | | | | | 42.0 | 9.7 | 40.7 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.08 | 0.00 | | | | | 5.38 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 27.3 | | 20.7 | 28.4 | | | | | 30.5 | 20.6 | 30.5 |
| Incremental Delay (d ₂), s/veh | | 0.7 | 0.0 | 0.9 | 0.4 | | | | | 51.5 | 0.7 | 60.9 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 28.0 | | 21.6 | 28.7 | | | | | 82.0 | 21.3 | 91.4 |
| Level of Service (LOS) | | C | | C | C | | | | | F | C | F |
| Approach Delay, s/veh / LOS | 28.0 | C | | 28.4 | C | | 0.0 | | | 67.3 | | E |
| Intersection Delay, s/veh / LOS | 50.7 | | | | | | D | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 181

| General Information | | | | Intersection Information | | | |
|---------------------|--|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2028_AN_ExportToHCS.xus | | | | |
| Project Description | 2028 AM Peak Hour No Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|------|---|----|-----|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 219 | 1510 | | | 646 | 203 | 146 | 568 | 166 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 45 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 18.0 | 45.0 | 31.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 23.0 | 73.0 | | 50.0 | | 37.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.3 | | | | | 11.3 | | |
| Green Extension Time (g _e), s | 0.5 | 0.0 | | 0.0 | | 3.6 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.02 | | | | | 0.02 | | |

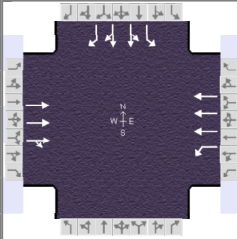
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 212 | 1463 | | | 646 | 203 | 146 | 568 | 166 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.3 | 23.4 | | | 9.5 | 9.6 | 7.1 | 9.0 | 9.3 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.3 | 23.4 | | | 9.5 | 9.6 | 7.1 | 9.0 | 9.3 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.41 | 0.41 | 0.28 | 0.28 | 0.28 | | | |
| Capacity (c), veh/h | 606 | 3125 | | | 2068 | 643 | 498 | 1569 | 443 | | | |
| Volume-to-Capacity Ratio (X) | 0.350 | 0.468 | | | 0.312 | 0.316 | 0.293 | 0.362 | 0.375 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 121 | 352 | | | 173 | 171 | 148 | 187 | 175 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 4.7 | 13.7 | | | 6.8 | 6.7 | 5.8 | 7.3 | 6.8 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.31 | 0.00 | | | 0.00 | 2.01 | 0.52 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 12.6 | 19.5 | | | 22.0 | 22.1 | 30.9 | 31.6 | 31.7 | | | |
| Incremental Delay (d ₂), s/veh | 0.9 | 0.3 | | | 0.4 | 1.3 | 1.5 | 0.6 | 2.4 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 13.6 | 19.8 | | | 22.4 | 23.3 | 32.4 | 32.2 | 34.1 | | | |
| Level of Service (LOS) | B | B | | | C | C | C | C | C | | | |
| Approach Delay, s/veh / LOS | 19.0 | B | | 22.6 | C | | 32.6 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 23.4 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 182

| General Information | | | | Intersection Information | | | |
|---------------------|-----------------------------|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2028_AB.xus | | | | |
| Project Description | 2028 AM Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|-----------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 915 | 0 | 51 | 776 | | | | | 843 | 666 | 763 |

| Signal Information | | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|--------|-----|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | Green | 6.0 | 39.0 | 49.0 | 0.0 | 0.0 | 0.0 | | | | |
| Offset, s | 58 | Reference Point | Begin | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|-----------------------------------|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 44.0 | 11.0 | 55.0 | | | | 55.0 |
| Change Period, ($Y+R_c$), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g_s), s | | | 3.9 | | | | | 51.0 |
| Green Extension Time (g_e), s | | 0.0 | 0.0 | 0.0 | | | | 0.0 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 1.00 |

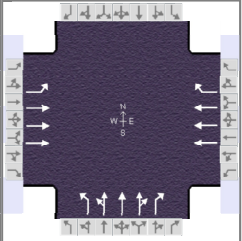
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|---|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 915 | 0 | 51 | 772 | | | | | 843 | 666 | 763 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g_s), s | | 14.0 | 0.0 | 1.9 | 14.0 | | | | | 49.0 | 13.3 | 49.0 |
| Cycle Queue Clearance Time (g_c), s | | 14.0 | 0.0 | 1.9 | 14.0 | | | | | 49.0 | 13.3 | 49.0 |
| Green Ratio (g/C) | | 0.35 | | 0.43 | 0.45 | | | | | 0.45 | 0.45 | 0.45 |
| Capacity (c), veh/h | | 1974 | | 300 | 2297 | | | | | 787 | 1653 | 700 |
| Volume-to-Capacity Ratio (X) | | 0.464 | 0.000 | 0.169 | 0.336 | | | | | 1.071 | 0.403 | 1.089 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 263 | 0 | 41 | 261 | | | | | 1087 | 249 | 1041 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 10.3 | 0.0 | 1.6 | 10.2 | | | | | 42.5 | 9.7 | 40.7 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.11 | 0.00 | | | | | 5.44 | 0.00 | 0.00 |
| Uniform Delay (d_1), s/veh | | 27.4 | | 20.9 | 28.4 | | | | | 30.5 | 20.6 | 30.5 |
| Incremental Delay (d_2), s/veh | | 0.8 | 0.0 | 1.2 | 0.4 | | | | | 52.8 | 0.7 | 60.9 |
| Initial Queue Delay (d_3), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 28.2 | | 22.1 | 28.8 | | | | | 83.3 | 21.3 | 91.4 |
| Level of Service (LOS) | | C | | C | C | | | | | F | C | F |
| Approach Delay, s/veh / LOS | 28.2 | C | | 28.4 | C | | 0.0 | | | 67.8 | | E |
| Intersection Delay, s/veh / LOS | | 50.7 | | | | | D | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 183

| General Information | | | | Intersection Information | | | |
|---------------------|-----------------------------|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2028_AB.xus | | | | |
| Project Description | 2028 AM Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|------|---|----|-----|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 219 | 1540 | | | 677 | 205 | 146 | 568 | 179 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|------|-----|-----|-----|--|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 45 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | 31.6 | 45.0 | 17.4 | 0.0 | 0.0 | 0.0 | | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 36.6 | 86.6 | | 50.0 | | 23.4 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 6.7 | | | | | 13.9 | | |
| Green Extension Time (g _e), s | 0.7 | 0.0 | | 0.0 | | 3.5 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.00 | | | | | 0.04 | | |

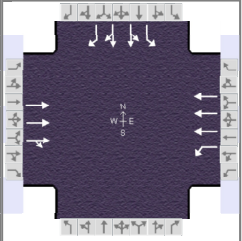
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 212 | 1490 | | | 677 | 205 | 146 | 568 | 179 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 4.7 | 16.3 | | | 10.1 | 9.7 | 8.3 | 10.5 | 11.9 | | | |
| Cycle Queue Clearance Time (g _c), s | 4.7 | 16.3 | | | 10.1 | 9.7 | 8.3 | 10.5 | 11.9 | | | |
| Green Ratio (g/C) | 0.71 | 0.74 | | | 0.41 | 0.41 | 0.16 | 0.16 | 0.16 | | | |
| Capacity (c), veh/h | 813 | 3748 | | | 2068 | 643 | 280 | 882 | 249 | | | |
| Volume-to-Capacity Ratio (X) | 0.261 | 0.398 | | | 0.327 | 0.319 | 0.521 | 0.644 | 0.718 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 79 | 231 | | | 183 | 173 | 171 | 216 | 216 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 3.1 | 9.0 | | | 7.2 | 6.8 | 6.7 | 8.4 | 8.4 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.20 | 0.00 | | | 0.00 | 2.03 | 0.60 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 7.2 | 8.5 | | | 22.2 | 22.1 | 42.5 | 43.4 | 43.9 | | | |
| Incremental Delay (d ₂), s/veh | 0.4 | 0.2 | | | 0.4 | 1.3 | 1.5 | 0.8 | 3.9 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 7.7 | 8.7 | | | 22.6 | 23.4 | 44.0 | 44.2 | 47.8 | | | |
| Level of Service (LOS) | A | A | | | C | C | D | D | D | | | |
| Approach Delay, s/veh / LOS | 8.5 | A | | 22.8 | C | | 44.9 | D | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 21.5 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 184

| General Information | | | | Intersection Information | | | |
|---------------------|---------------------------------------|---------------|-----------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD MIT | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2028_AB_Optimized.xus | | | | |
| Project Description | 2028 AM Build Mitigated - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Demand (v), veh/h | | 915 | 0 | 51 | 776 | | | | | 843 | 666 | 763 |

| Signal Information | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 58 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| | | Green | 4.0 | 26.0 | 64.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 31.0 | 9.0 | 40.0 | | | | 70.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g _s), s | | | 4.3 | | | | | 45.4 |
| Green Extension Time (g _e), s | | 0.0 | 0.0 | 0.0 | | | | 11.1 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 0.56 |

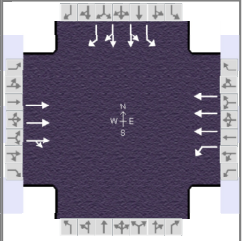
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 915 | 0 | 51 | 772 | | | | | 843 | 666 | 763 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 16.5 | 0.0 | 2.3 | 12.1 | | | | | 42.0 | 10.1 | 43.4 |
| Cycle Queue Clearance Time (g _c), s | | 16.5 | 0.0 | 2.3 | 12.1 | | | | | 42.0 | 10.1 | 43.4 |
| Green Ratio (g/C) | | 0.24 | | 0.29 | 0.32 | | | | | 0.58 | 0.58 | 0.58 |
| Capacity (c), veh/h | | 1316 | | 182 | 1608 | | | | | 1028 | 2159 | 915 |
| Volume-to-Capacity Ratio (X) | | 0.695 | 0.000 | 0.279 | 0.480 | | | | | 0.820 | 0.308 | 0.834 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 315 | 0 | 52 | 205 | | | | | 628 | 186 | 595 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 12.3 | 0.0 | 2.0 | 8.0 | | | | | 24.6 | 7.3 | 23.2 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.13 | 0.00 | | | | | 3.14 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 38.4 | | 29.3 | 25.0 | | | | | 18.4 | 11.7 | 18.7 |
| Incremental Delay (d ₂), s/veh | | 3.1 | 0.0 | 3.6 | 1.0 | | | | | 7.3 | 0.4 | 8.8 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 41.4 | | 32.9 | 26.0 | | | | | 25.7 | 12.1 | 27.5 |
| Level of Service (LOS) | | D | | C | C | | | | | C | B | C |
| Approach Delay, s/veh / LOS | 41.4 | D | | 26.5 | C | | 0.0 | | | 22.3 | | C |
| Intersection Delay, s/veh / LOS | | | 27.5 | | | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 185

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West, LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2028_PN.xus | | | | |
| Project Description | 2028 PM No Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|-----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 877 | 0 | 325 | 930 | | | | | 199 | 434 | 240 |

| Signal Information | | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|--|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | | | |
| Offset, s | 64 | Reference Point | Begin | | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | |
| | | Green | 20.0 | 55.0 | 29.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 60.0 | 25.0 | 85.0 | | | | 35.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.1 |
| Queue Clearance Time (g _s), s | | | 11.4 | | | | | 18.4 |
| Green Extension Time (g _e), s | | 0.0 | 0.8 | 0.0 | | | | 2.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 0.10 | | | | | 0.21 |

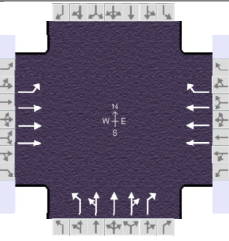
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 877 | 0 | 322 | 921 | | | | | 199 | 434 | 240 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 12.2 | 0.0 | 9.4 | 12.9 | | | | | 11.5 | 12.1 | 16.4 |
| Cycle Queue Clearance Time (g _c), s | | 12.2 | 0.0 | 9.4 | 12.9 | | | | | 11.5 | 12.1 | 16.4 |
| Green Ratio (g/C) | | 0.46 | | 0.64 | 0.67 | | | | | 0.24 | 0.24 | 0.24 |
| Capacity (c), veh/h | | 2551 | | 579 | 3370 | | | | | 427 | 897 | 380 |
| Volume-to-Capacity Ratio (X) | | 0.344 | 0.000 | 0.556 | 0.273 | | | | | 0.466 | 0.484 | 0.632 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 231 | 0 | 207 | 224 | | | | | 236 | 245 | 293 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 9.0 | 0.0 | 8.1 | 8.7 | | | | | 9.2 | 9.6 | 11.5 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.54 | 0.00 | | | | | 1.18 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 20.9 | | 13.7 | 13.5 | | | | | 38.9 | 39.1 | 40.7 |
| Incremental Delay (d ₂), s/veh | | 0.4 | 0.0 | 3.2 | 0.2 | | | | | 3.6 | 1.9 | 7.8 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 21.3 | | 16.9 | 13.7 | | | | | 42.5 | 40.9 | 48.5 |
| Level of Service (LOS) | | C | | B | B | | | | | D | D | D |
| Approach Delay, s/veh / LOS | 21.3 | C | | 14.5 | B | | 0.0 | | | 43.4 | | D |
| Intersection Delay, s/veh / LOS | | | 24.9 | | | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 186

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West, LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2028_PN.xus | | | | |
| Project Description | 2028 PM No Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 333 | 723 | | | 1117 | 589 | 126 | 520 | 166 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|------|-----|-----|-----|--|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | | | |
| Offset, s | 49 | Reference Point | Begin | | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | |
| | | Green | 21.0 | 48.0 | 35.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 26.0 | 79.0 | | 53.0 | | 41.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 13.4 | | | | | 12.0 | | |
| Green Extension Time (g _e), s | 0.8 | 0.0 | | 0.0 | | 3.4 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.17 | | | | | 0.01 | | |

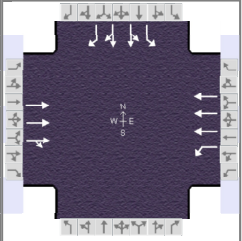
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 339 | 737 | | | 1117 | 589 | 126 | 520 | 166 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 11.4 | 10.4 | | | 20.4 | 43.1 | 6.5 | 8.8 | 10.0 | | | |
| Cycle Queue Clearance Time (g _c), s | 11.4 | 10.4 | | | 20.4 | 43.1 | 6.5 | 8.8 | 10.0 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.40 | 0.40 | 0.29 | 0.29 | 0.29 | | | |
| Capacity (c), veh/h | 484 | 3117 | | | 2022 | 629 | 515 | 1624 | 459 | | | |
| Volume-to-Capacity Ratio (X) | 0.701 | 0.236 | | | 0.552 | 0.936 | 0.244 | 0.320 | 0.362 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 274 | 189 | | | 332 | 699 | 136 | 184 | 188 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 10.7 | 7.4 | | | 13.0 | 27.3 | 5.3 | 7.2 | 7.4 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.71 | 0.00 | | | 0.00 | 8.22 | 0.48 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 22.9 | 14.9 | | | 27.7 | 34.5 | 32.4 | 33.2 | 33.7 | | | |
| Incremental Delay (d ₂), s/veh | 7.7 | 0.2 | | | 1.1 | 23.3 | 1.1 | 0.5 | 2.2 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 30.6 | 15.1 | | | 28.8 | 57.8 | 33.5 | 33.7 | 35.9 | | | |
| Level of Service (LOS) | C | B | | | C | E | C | C | D | | | |
| Approach Delay, s/veh / LOS | 20.0 | B | | 38.8 | D | | 34.1 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 32.1 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 187

| General Information | | | | Intersection Information | | | |
|---------------------|-----------------------------|---------------|---------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2028_PB_Revised.xus | | | | |
| Project Description | 2028 PM Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|-----|-----|---|----|---|---|-----|-----|-----|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Demand (v), veh/h | | 905 | 0 | 335 | 952 | | | | | 202 | 434 | 240 |

| Signal Information | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 64 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| | | Green | 20.0 | 55.0 | 29.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 60.0 | 25.0 | 85.0 | | | | 35.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.1 |
| Queue Clearance Time (g _s), s | | | 11.7 | | | | | 18.4 |
| Green Extension Time (g _e), s | | 0.0 | 0.8 | 0.0 | | | | 2.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 0.12 | | | | | 0.21 |

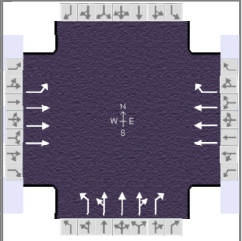
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 905 | 0 | 332 | 944 | | | | | 202 | 434 | 240 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 12.6 | 0.0 | 9.7 | 13.2 | | | | | 11.7 | 12.1 | 16.4 |
| Cycle Queue Clearance Time (g _c), s | | 12.6 | 0.0 | 9.7 | 13.2 | | | | | 11.7 | 12.1 | 16.4 |
| Green Ratio (g/C) | | 0.46 | | 0.64 | 0.67 | | | | | 0.24 | 0.24 | 0.24 |
| Capacity (c), veh/h | | 2551 | | 570 | 3370 | | | | | 427 | 897 | 380 |
| Volume-to-Capacity Ratio (X) | | 0.355 | 0.000 | 0.582 | 0.280 | | | | | 0.473 | 0.484 | 0.632 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 238 | 0 | 215 | 227 | | | | | 239 | 245 | 293 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 9.3 | 0.0 | 8.4 | 8.9 | | | | | 9.3 | 9.6 | 11.5 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.56 | 0.00 | | | | | 1.19 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 21.0 | | 14.2 | 13.6 | | | | | 39.0 | 39.1 | 40.7 |
| Incremental Delay (d ₂), s/veh | | 0.4 | 0.0 | 3.6 | 0.2 | | | | | 3.7 | 1.9 | 7.8 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 21.4 | | 17.8 | 13.7 | | | | | 42.7 | 40.9 | 48.5 |
| Level of Service (LOS) | | C | | B | B | | | | | D | D | D |
| Approach Delay, s/veh / LOS | 21.4 | C | | 14.8 | B | | 0.0 | | | 43.4 | | D |
| Intersection Delay, s/veh / LOS | | | 24.9 | | | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 188

| General Information | | | | Intersection Information | | | |
|---------------------|-----------------------------|---------------|---------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2028_PB_Revised.xus | | | | |
| Project Description | 2028 PM Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 333 | 754 | | | 1150 | 591 | 126 | 520 | 179 | | | |

| Signal Information | | | | Signal Phases | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|------|------|------|-----|-----|-----|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | |
| Offset, s | 49 | Reference Point | Begin | Green | 21.0 | 48.0 | 35.0 | 0.0 | 0.0 | 0.0 | |
| Uncoordinated | No | Simult. Gap E/W | On | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | |
| Force Mode | Fixed | Simult. Gap N/S | On | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 26.0 | 79.0 | | 53.0 | | 41.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 13.6 | | | | | 12.9 | | |
| Green Extension Time (g _e), s | 0.7 | 0.0 | | 0.0 | | 3.5 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.19 | | | | | 0.01 | | |

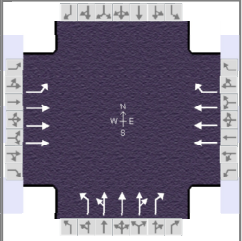
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|----|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 339 | 768 | | | 1150 | 591 | 126 | 520 | 179 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 11.6 | 10.7 | | | 21.2 | 43.4 | 6.5 | 8.8 | 10.9 | | | |
| Cycle Queue Clearance Time (g _c), s | 11.6 | 10.7 | | | 21.2 | 43.4 | 6.5 | 8.8 | 10.9 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.40 | 0.40 | 0.29 | 0.29 | 0.29 | | | |
| Capacity (c), veh/h | 478 | 3117 | | | 2022 | 629 | 515 | 1624 | 459 | | | |
| Volume-to-Capacity Ratio (X) | 0.710 | 0.246 | | | 0.569 | 0.940 | 0.244 | 0.320 | 0.390 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 275 | 193 | | | 343 | 704 | 136 | 184 | 203 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 10.7 | 7.6 | | | 13.4 | 27.5 | 5.3 | 7.2 | 7.9 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.71 | 0.00 | | | 0.00 | 8.28 | 0.48 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 23.8 | 14.8 | | | 28.0 | 34.6 | 32.4 | 33.2 | 34.0 | | | |
| Incremental Delay (d ₂), s/veh | 8.1 | 0.2 | | | 1.2 | 23.8 | 1.1 | 0.5 | 2.5 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 31.9 | 14.9 | | | 29.1 | 58.4 | 33.5 | 33.7 | 36.5 | | | |
| Level of Service (LOS) | C | B | | | C | E | C | C | D | | | |
| Approach Delay, s/veh / LOS | 20.1 | C | | | 39.1 | D | 34.3 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | | | | | 32.3 | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 189

| General Information | | | | Intersection Information | | | |
|---------------------|-----------------------------|---------------|---------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD MIT | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2028 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2028_PB_Optimized,MIT.xus | | | | |
| Project Description | 2028 PM Build - Exis. Geom. | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 333 | 754 | | | 1150 | 591 | 126 | 520 | 179 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 119 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 6.0 | 74.0 | 24.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 11.0 | 90.0 | | 79.0 | | 30.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.0 | | | | | 14.3 | | |
| Green Extension Time (g _e), s | 0.0 | 0.0 | | 0.0 | | 2.6 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 1.00 | | | | | 0.23 | | |

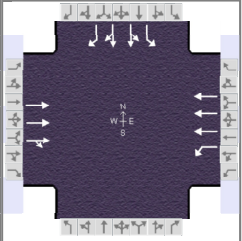
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|----|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 339 | 768 | | | 1150 | 591 | 126 | 520 | 179 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.0 | 7.7 | | | 13.5 | 27.7 | 7.4 | 9.9 | 12.3 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.0 | 7.7 | | | 13.5 | 27.7 | 7.4 | 9.9 | 12.3 | | | |
| Green Ratio (g/C) | 0.68 | 0.71 | | | 0.62 | 0.62 | 0.20 | 0.20 | 0.20 | | | |
| Capacity (c), veh/h | 393 | 3580 | | | 3117 | 970 | 353 | 1113 | 314 | | | |
| Volume-to-Capacity Ratio (X) | 0.864 | 0.214 | | | 0.369 | 0.609 | 0.356 | 0.467 | 0.569 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 320 | 122 | | | 218 | 386 | 160 | 210 | 236 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 12.5 | 4.8 | | | 8.5 | 15.1 | 6.2 | 8.2 | 9.2 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.83 | 0.00 | | | 0.00 | 4.54 | 0.56 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 18.5 | 7.8 | | | 11.4 | 14.1 | 41.3 | 42.4 | 43.3 | | | |
| Incremental Delay (d ₂), s/veh | 20.5 | 0.1 | | | 0.3 | 2.9 | 2.8 | 1.4 | 7.3 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 39.0 | 7.9 | | | 11.8 | 17.0 | 44.1 | 43.8 | 50.6 | | | |
| Level of Service (LOS) | D | A | | | B | B | D | D | D | | | |
| Approach Delay, s/veh / LOS | 17.4 | B | | | 13.5 | B | 45.3 | D | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 21.8 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 190

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2038_AN.xus | | | | |
| Project Description | 2038 AM Peak Hour No Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|-----------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 932 | 0 | 43 | 792 | | | | | 882 | 699 | 801 |

| Signal Information | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|-----|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 58 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | | 6.0 | 39.0 | 49.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|-----------------------------------|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 44.0 | 11.0 | 55.0 | | | | 55.0 |
| Change Period, ($Y+R_c$), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g_s), s | | | 3.3 | | | | | 51.0 |
| Green Extension Time (g_e), s | | 0.0 | 0.0 | 0.0 | | | | 0.0 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 1.00 |

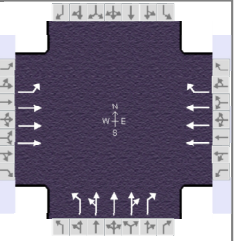
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|---|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 932 | 0 | 35 | 642 | | | | | 882 | 699 | 801 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g_s), s | | 14.3 | 0.0 | 1.3 | 6.0 | | | | | 49.0 | 14.2 | 49.0 |
| Cycle Queue Clearance Time (g_c), s | | 14.3 | 0.0 | 1.3 | 6.0 | | | | | 49.0 | 14.2 | 49.0 |
| Green Ratio (g/C) | | 0.35 | | 0.43 | 0.45 | | | | | 0.45 | 0.45 | 0.45 |
| Capacity (c), veh/h | | 1974 | | 296 | 2297 | | | | | 787 | 1653 | 700 |
| Volume-to-Capacity Ratio (X) | | 0.472 | 0.000 | 0.118 | 0.279 | | | | | 1.120 | 0.423 | 1.144 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 268 | 0 | 28 | 97 | | | | | 1246 | 261 | 1202 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 10.5 | 0.0 | 1.1 | 3.8 | | | | | 48.7 | 10.2 | 47.0 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.07 | 0.00 | | | | | 6.23 | 0.00 | 0.00 |
| Uniform Delay (d_1), s/veh | | 27.5 | | 20.8 | 11.6 | | | | | 30.5 | 20.8 | 30.5 |
| Incremental Delay (d_2), s/veh | | 0.8 | 0.0 | 0.7 | 0.3 | | | | | 70.5 | 0.8 | 80.9 |
| Initial Queue Delay (d_3), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 28.3 | | 21.6 | 11.9 | | | | | 101.0 | 21.6 | 111.4 |
| Level of Service (LOS) | | C | | C | B | | | | | F | C | F |
| Approach Delay, s/veh / LOS | 28.3 | C | | 12.4 | B | | 0.0 | | | 81.2 | F | |
| Intersection Delay, s/veh / LOS | 57.2 | | | | | | E | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 191

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2038_AN.xus | | | | |
| Project Description | 2038 AM Peak Hour No Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|------|---|----|-----|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 230 | 1585 | | | 677 | 213 | 153 | 596 | 175 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 45 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 18.0 | 45.0 | 31.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 23.0 | 73.0 | | 50.0 | | 37.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.5 | | | | | 11.9 | | |
| Green Extension Time (g _e), s | 0.5 | 0.0 | | 0.0 | | 3.8 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.03 | | | | | 0.03 | | |

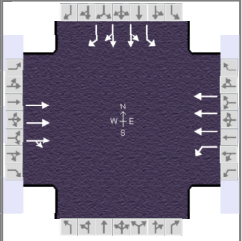
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 218 | 1501 | | | 677 | 213 | 153 | 596 | 175 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.5 | 24.0 | | | 10.1 | 10.2 | 7.5 | 9.5 | 9.9 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.5 | 24.0 | | | 10.1 | 10.2 | 7.5 | 9.5 | 9.9 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.41 | 0.41 | 0.28 | 0.28 | 0.28 | | | |
| Capacity (c), veh/h | 595 | 3125 | | | 2068 | 643 | 498 | 1569 | 443 | | | |
| Volume-to-Capacity Ratio (X) | 0.366 | 0.480 | | | 0.327 | 0.331 | 0.307 | 0.380 | 0.395 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 121 | 352 | | | 183 | 181 | 156 | 197 | 186 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 4.7 | 13.8 | | | 7.2 | 7.1 | 6.1 | 7.7 | 7.2 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.32 | 0.00 | | | 0.00 | 2.13 | 0.55 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 12.8 | 19.4 | | | 22.2 | 22.2 | 31.1 | 31.8 | 31.9 | | | |
| Incremental Delay (d ₂), s/veh | 0.9 | 0.3 | | | 0.4 | 1.4 | 1.6 | 0.7 | 2.6 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 13.7 | 19.7 | | | 22.6 | 23.6 | 32.7 | 32.5 | 34.5 | | | |
| Level of Service (LOS) | B | B | | | C | C | C | C | C | | | |
| Approach Delay, s/veh / LOS | 18.9 | B | | 22.8 | C | | 32.9 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 23.6 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 192

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2038_AB_ExportToHCS.xus | | | | |
| Project Description | 2038 AM Peak Hour Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|-----------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Demand (v), veh/h | | 959 | 0 | 53 | 813 | | | | | 885 | 699 | 801 |

| Signal Information | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 58 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| | | Green | 6.0 | 39.0 | 49.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|-----------------------------------|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 44.0 | 11.0 | 55.0 | | | | 55.0 |
| Change Period, ($Y+R_c$), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g_s), s | | | 3.6 | | | | | 51.0 |
| Green Extension Time (g_e), s | | 0.0 | 0.0 | 0.0 | | | | 0.0 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 1.00 |

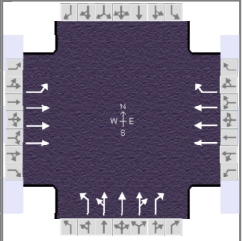
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|---|------|-------|-------|-------|-------|-----|----|---|---|-------|-------|-------|
| Approach Movement | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 959 | 0 | 43 | 665 | | | | | 885 | 699 | 801 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g_s), s | | 14.8 | 0.0 | 1.6 | 6.3 | | | | | 49.0 | 14.2 | 49.0 |
| Cycle Queue Clearance Time (g_c), s | | 14.8 | 0.0 | 1.6 | 6.3 | | | | | 49.0 | 14.2 | 49.0 |
| Green Ratio (g/C) | | 0.35 | | 0.43 | 0.45 | | | | | 0.45 | 0.45 | 0.45 |
| Capacity (c), veh/h | | 1974 | | 290 | 2297 | | | | | 787 | 1653 | 700 |
| Volume-to-Capacity Ratio (X) | | 0.486 | 0.000 | 0.150 | 0.289 | | | | | 1.124 | 0.423 | 1.144 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 276 | 0 | 35 | 102 | | | | | 1258 | 261 | 1202 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 10.8 | 0.0 | 1.4 | 4.0 | | | | | 49.2 | 10.2 | 47.0 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.09 | 0.00 | | | | | 6.29 | 0.00 | 0.00 |
| Uniform Delay (d_1), s/veh | | 27.7 | | 21.0 | 11.8 | | | | | 30.5 | 20.8 | 30.5 |
| Incremental Delay (d_2), s/veh | | 0.9 | 0.0 | 1.0 | 0.3 | | | | | 72.0 | 0.8 | 80.9 |
| Initial Queue Delay (d_3), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 28.5 | | 22.0 | 12.1 | | | | | 102.5 | 21.6 | 111.4 |
| Level of Service (LOS) | | C | | C | B | | | | | F | C | F |
| Approach Delay, s/veh / LOS | 28.5 | C | | 12.7 | B | 0.0 | | | | 81.8 | | F |
| Intersection Delay, s/veh / LOS | | | 57.1 | | | | | | | E | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 193

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2038_AB_ExportToHCS.xus | | | | |
| Project Description | 2038 AM Peak Hour Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|------|---|----|-----|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 230 | 1615 | | | 708 | 215 | 153 | 596 | 188 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 45 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 18.0 | 45.0 | 31.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 23.0 | 73.0 | | 50.0 | | 37.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.5 | | | | | 12.7 | | |
| Green Extension Time (g _e), s | 0.5 | 0.0 | | 0.0 | | 3.8 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.03 | | | | | 0.04 | | |

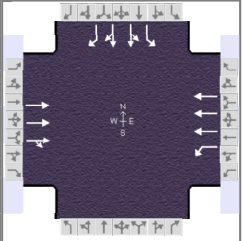
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 218 | 1529 | | | 708 | 215 | 153 | 596 | 188 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1629 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.5 | 25.5 | | | 10.6 | 10.3 | 7.5 | 9.5 | 10.7 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.5 | 25.5 | | | 10.6 | 10.3 | 7.5 | 9.5 | 10.7 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.41 | 0.41 | 0.28 | 0.28 | 0.28 | | | |
| Capacity (c), veh/h | 584 | 3020 | | | 2068 | 643 | 498 | 1569 | 443 | | | |
| Volume-to-Capacity Ratio (X) | 0.372 | 0.506 | | | 0.342 | 0.334 | 0.307 | 0.380 | 0.424 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 121 | 358 | | | 193 | 183 | 156 | 197 | 200 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 4.7 | 14.0 | | | 7.5 | 7.1 | 6.1 | 7.7 | 7.8 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.31 | 0.00 | | | 0.00 | 2.15 | 0.55 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 13.0 | 19.7 | | | 22.3 | 22.2 | 31.1 | 31.8 | 32.2 | | | |
| Incremental Delay (d ₂), s/veh | 0.9 | 0.3 | | | 0.5 | 1.4 | 1.6 | 0.7 | 3.0 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 13.9 | 20.0 | | | 22.8 | 23.6 | 32.7 | 32.5 | 35.2 | | | |
| Level of Service (LOS) | B | C | | | C | C | C | C | D | | | |
| Approach Delay, s/veh / LOS | 19.2 | B | | 23.0 | C | | 33.0 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 23.8 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 194

| General Information | | | | Intersection Information | | | |
|---------------------|--|---------------|-----------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | AM BUILD MIT | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2038_AB_Optimized.xus | | | | |
| Project Description | 2038 AM Peak Hour Build Mitigated - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 959 | 0 | 53 | 813 | | | | | 885 | 699 | 801 |

| Signal Information | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|-----|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 110.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 58 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | | 5.0 | 25.0 | 64.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 30.0 | 10.0 | 40.0 | | | | 70.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.2 |
| Queue Clearance Time (g _s), s | | | 4.4 | | | | | 49.8 |
| Green Extension Time (g _e), s | | 0.0 | 0.0 | 0.0 | | | | 9.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 1.00 | | | | | 0.70 |

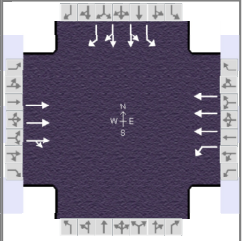
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|---|-----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 959 | 0 | 53 | 808 | | | | | 885 | 699 | 801 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 17.7 | 0.0 | 2.4 | 17.2 | | | | | 46.1 | 10.7 | 47.8 |
| Cycle Queue Clearance Time (g _c), s | | 17.7 | 0.0 | 2.4 | 17.2 | | | | | 46.1 | 10.7 | 47.8 |
| Green Ratio (g/C) | | 0.23 | | 0.29 | 0.32 | | | | | 0.58 | 0.58 | 0.58 |
| Capacity (c), veh/h | | 1265 | | 184 | 1608 | | | | | 1028 | 2159 | 915 |
| Volume-to-Capacity Ratio (X) | | 0.758 | 0.000 | 0.286 | 0.503 | | | | | 0.861 | 0.324 | 0.876 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 337 | 0 | 57 | 324 | | | | | 694 | 196 | 660 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 13.2 | 0.0 | 2.2 | 12.6 | | | | | 27.1 | 7.7 | 25.8 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.15 | 0.00 | | | | | 3.47 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 39.7 | | 31.8 | 43.9 | | | | | 19.3 | 11.9 | 19.6 |
| Incremental Delay (d ₂), s/veh | | 4.3 | 0.0 | 3.7 | 1.1 | | | | | 9.4 | 0.4 | 11.5 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 44.0 | | 35.5 | 45.0 | | | | | 28.7 | 12.2 | 31.1 |
| Level of Service (LOS) | | D | | D | D | | | | | C | B | C |
| Approach Delay, s/veh / LOS | 44.0 | | D | 44.4 | | D | 0.0 | | | 24.7 | | C |
| Intersection Delay, s/veh / LOS | | | | 33.1 | | | | | | C | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 195

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2038_PN_ExportToHCS.xus | | | | |
| Project Description | 2038 PM Peak Hour No Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|-----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 920 | 0 | 341 | 976 | | | | | 209 | 456 | 251 |

| Signal Information | | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|-----|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | | |
| Offset, s | 64 | Reference Point | Begin | | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | |
| | | Green | 20.0 | 55.0 | 29.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 60.0 | 25.0 | 85.0 | | | | 35.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.1 |
| Queue Clearance Time (g _s), s | | | 10.9 | | | | | 19.3 |
| Green Extension Time (g _e), s | | 0.0 | 0.7 | 0.0 | | | | 2.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 0.06 | | | | | 0.28 |

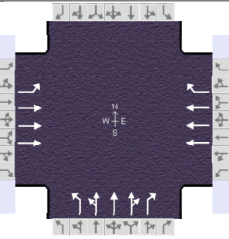
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|-----|----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 920 | 0 | 303 | 869 | | | | | 209 | 456 | 251 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 12.9 | 0.0 | 8.9 | 3.2 | | | | | 12.2 | 12.7 | 17.3 |
| Cycle Queue Clearance Time (g _c), s | | 12.9 | 0.0 | 8.9 | 3.2 | | | | | 12.2 | 12.7 | 17.3 |
| Green Ratio (g/C) | | 0.46 | | 0.64 | 0.67 | | | | | 0.24 | 0.24 | 0.24 |
| Capacity (c), veh/h | | 2551 | | 566 | 3370 | | | | | 427 | 897 | 380 |
| Volume-to-Capacity Ratio (X) | | 0.361 | 0.000 | 0.536 | 0.258 | | | | | 0.489 | 0.508 | 0.660 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 242 | 0 | 174 | 43 | | | | | 247 | 257 | 308 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 9.5 | 0.0 | 6.8 | 1.7 | | | | | 9.6 | 10.0 | 12.0 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.45 | 0.00 | | | | | 1.24 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 21.1 | | 13.9 | 2.6 | | | | | 39.1 | 39.3 | 41.1 |
| Incremental Delay (d ₂), s/veh | | 0.4 | 0.0 | 1.9 | 0.1 | | | | | 4.0 | 2.1 | 8.7 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 21.5 | | 15.9 | 2.7 | | | | | 43.1 | 41.4 | 49.8 |
| Level of Service (LOS) | | C | | B | A | | | | | D | D | D |
| Approach Delay, s/veh / LOS | 21.5 | C | | 6.1 | A | 0.0 | | | | 44.1 | | D |
| Intersection Delay, s/veh / LOS | | | 22.4 | | | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 196

| General Information | | | | Intersection Information | | | |
|---------------------|---|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM NO BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2038_PN_ExportToHCS.xus | | | | |
| Project Description | 2038 PM Peak Hour No Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 349 | 758 | | | 1172 | 618 | 132 | 545 | 175 | | | |

| Signal Information | | | | Signal Timing (s) | | | | | | | | | | |
|--------------------|-------|-----------------|-------|-------------------|------|------|------|-----|-----|-----|---|---|---|---|
| Cycle, s | 120.0 | Reference Phase | 2 | Green | 37.7 | 48.0 | 18.3 | 0.0 | 0.0 | 0.0 | 1 | 2 | 3 | 4 |
| Offset, s | 49 | Reference Point | Begin | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 5 | 6 | 7 | 8 |
| Uncoordinated | No | Simult. Gap E/W | On | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 42.7 | 95.7 | | 53.0 | | 24.3 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 14.7 | | | | | 14.7 | | |
| Green Extension Time (g _e), s | 1.3 | 0.0 | | 0.0 | | 3.5 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.00 | | | | | 0.02 | | |

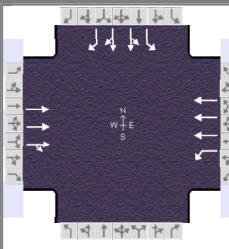
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 356 | 773 | | | 1172 | 618 | 132 | 545 | 175 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 12.7 | 9.4 | | | 21.7 | 46.6 | 8.2 | 11.0 | 12.7 | | | |
| Cycle Queue Clearance Time (g _c), s | 12.7 | 9.4 | | | 21.7 | 46.6 | 8.2 | 11.0 | 12.7 | | | |
| Green Ratio (g/C) | 0.73 | 0.76 | | | 0.40 | 0.40 | 0.15 | 0.15 | 0.15 | | | |
| Capacity (c), veh/h | 720 | 3822 | | | 2022 | 629 | 269 | 847 | 239 | | | |
| Volume-to-Capacity Ratio (X) | 0.494 | 0.202 | | | 0.580 | 0.982 | 0.491 | 0.643 | 0.731 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 451 | 158 | | | 350 | 782 | 170 | 226 | 230 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 17.6 | 6.2 | | | 13.7 | 30.6 | 6.6 | 8.8 | 9.0 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 1.17 | 0.00 | | | 0.00 | 9.20 | 0.60 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 25.0 | 8.8 | | | 28.1 | 35.6 | 46.6 | 47.8 | 48.5 | | | |
| Incremental Delay (d ₂), s/veh | 2.3 | 0.1 | | | 1.2 | 31.8 | 1.4 | 0.8 | 4.3 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 27.3 | 8.9 | | | 29.3 | 67.4 | 48.0 | 48.6 | 52.8 | | | |
| Level of Service (LOS) | C | A | | | C | E | D | D | D | | | |
| Approach Delay, s/veh / LOS | 14.7 | B | | 42.5 | D | | 49.4 | D | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 35.7 | | | | | | D | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 197

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 SBFR | File Name | 2038_PB_ExportToHCS.xus | | | | |
| Project Description | 2038 PM Peak Hour Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|----|-----|---|-----|-----|---|----|---|---|-----|-----|-----|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | | 948 | 0 | 351 | 998 | | | | | 212 | 456 | 251 |

| Signal Information | | | | | | | | | | | | |
|--------------------|-------|-----------------|-------|------|------|-----|-----|-----|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 64 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| | | Green | 20.0 | 55.0 | 29.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | |
| | | Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|-----|------|------|------|-----|-----|-----|------|
| Assigned Phase | | 2 | 1 | 6 | | | | 4 |
| Case Number | | 8.3 | 1.0 | 4.0 | | | | 9.0 |
| Phase Duration, s | | 60.0 | 25.0 | 85.0 | | | | 35.0 |
| Change Period, (Y+R _c), s | | 5.0 | 5.0 | 5.0 | | | | 6.0 |
| Max Allow Headway (MAH), s | | 0.0 | 4.1 | 0.0 | | | | 4.1 |
| Queue Clearance Time (g _s), s | | | 11.2 | | | | | 19.3 |
| Green Extension Time (g _e), s | | 0.0 | 0.8 | 0.0 | | | | 2.8 |
| Phase Call Probability | | | 1.00 | | | | | 1.00 |
| Max Out Probability | | | 0.08 | | | | | 0.28 |

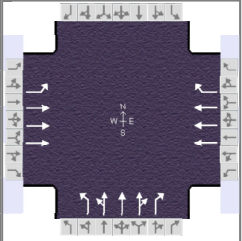
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|------|-------|-------|-------|-------|-----|----|---|---|-------|-------|-------|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | | 2 | 12 | 1 | 6 | | | | | 7 | 4 | 14 |
| Adjusted Flow Rate (v), veh/h | | 948 | 0 | 314 | 891 | | | | | 212 | 456 | 251 |
| Adjusted Saturation Flow Rate (s), veh/h/ln | | 1856 | 0 | 1767 | 1685 | | | | | 1767 | 1856 | 1572 |
| Queue Service Time (g _s), s | | 13.3 | 0.0 | 9.2 | 4.0 | | | | | 12.4 | 12.7 | 17.3 |
| Cycle Queue Clearance Time (g _c), s | | 13.3 | 0.0 | 9.2 | 4.0 | | | | | 12.4 | 12.7 | 17.3 |
| Green Ratio (g/C) | | 0.46 | | 0.64 | 0.67 | | | | | 0.24 | 0.24 | 0.24 |
| Capacity (c), veh/h | | 2551 | | 558 | 3370 | | | | | 427 | 897 | 380 |
| Volume-to-Capacity Ratio (X) | | 0.372 | 0.000 | 0.562 | 0.265 | | | | | 0.496 | 0.508 | 0.660 |
| Back of Queue (Q), ft/ln (95 th percentile) | | 249 | 0 | 178 | 53 | | | | | 250 | 257 | 308 |
| Back of Queue (Q), veh/ln (95 th percentile) | | 9.7 | 0.0 | 6.9 | 2.1 | | | | | 9.8 | 10.0 | 12.0 |
| Queue Storage Ratio (RQ) (95 th percentile) | | 0.00 | 0.00 | 0.46 | 0.00 | | | | | 1.25 | 0.00 | 0.00 |
| Uniform Delay (d ₁), s/veh | | 21.2 | | 14.5 | 3.2 | | | | | 39.2 | 39.3 | 41.1 |
| Incremental Delay (d ₂), s/veh | | 0.4 | 0.0 | 2.0 | 0.1 | | | | | 4.1 | 2.1 | 8.7 |
| Initial Queue Delay (d ₃), s/veh | | 0.0 | 0.0 | 0.0 | 0.0 | | | | | 0.0 | 0.0 | 0.0 |
| Control Delay (d), s/veh | | 21.6 | | 16.5 | 3.3 | | | | | 43.3 | 41.4 | 49.8 |
| Level of Service (LOS) | | C | | B | A | | | | | D | D | D |
| Approach Delay, s/veh / LOS | 21.6 | C | | 6.7 | A | 0.0 | | | | 44.1 | | D |
| Intersection Delay, s/veh / LOS | | | 22.5 | | | | | | C | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 198

| General Information | | | | Intersection Information | | | |
|---------------------|--------------------------------------|---------------|-------------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2038_PB_ExportToHCS.xus | | | | |
| Project Description | 2038 PM Peak Hour Build - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 349 | 789 | | | 1205 | 620 | 132 | 545 | 188 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 49 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 21.0 | 48.0 | 35.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 26.0 | 79.0 | | 53.0 | | 41.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 15.4 | | | | | 13.5 | | |
| Green Extension Time (g _e), s | 0.7 | 0.0 | | 0.0 | | 3.6 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 0.50 | | | | | 0.01 | | |

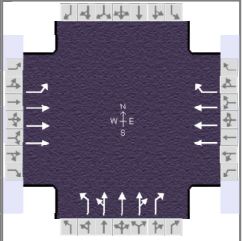
| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|------|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 356 | 804 | | | 1205 | 620 | 132 | 545 | 188 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 13.4 | 11.2 | | | 22.5 | 46.9 | 6.9 | 9.2 | 11.5 | | | |
| Cycle Queue Clearance Time (g _c), s | 13.4 | 11.2 | | | 22.5 | 46.9 | 6.9 | 9.2 | 11.5 | | | |
| Green Ratio (g/C) | 0.59 | 0.62 | | | 0.40 | 0.40 | 0.29 | 0.29 | 0.29 | | | |
| Capacity (c), veh/h | 467 | 3117 | | | 2022 | 629 | 515 | 1624 | 459 | | | |
| Volume-to-Capacity Ratio (X) | 0.762 | 0.258 | | | 0.596 | 0.986 | 0.256 | 0.336 | 0.410 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 292 | 199 | | | 361 | 789 | 143 | 194 | 212 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 11.4 | 7.8 | | | 14.1 | 30.8 | 5.6 | 7.6 | 8.3 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 0.76 | 0.00 | | | 0.00 | 9.28 | 0.50 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 28.0 | 14.6 | | | 28.4 | 35.7 | 32.5 | 33.4 | 34.2 | | | |
| Incremental Delay (d ₂), s/veh | 10.4 | 0.2 | | | 1.3 | 32.6 | 1.2 | 0.6 | 2.7 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 38.4 | 14.8 | | | 29.7 | 68.2 | 33.7 | 33.9 | 36.9 | | | |
| Level of Service (LOS) | D | B | | | C | E | C | C | D | | | |
| Approach Delay, s/veh / LOS | 22.1 | C | | 42.8 | D | | 34.5 | C | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 34.7 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

HCS Signalized Intersection Results Summary

A - 199

| General Information | | | | Intersection Information | | | |
|---------------------|--|---------------|-----------------------|--------------------------|----------|--|--|
| Agency | Tierra West LLC | | | Duration, h | 0.250 | | |
| Analyst | TOB | Analysis Date | | Area Type | Other | | |
| Jurisdiction | NMDOT | Time Period | PM BUILD MIT | PHF | 1.00 | | |
| Urban Street | Lomas Blvd NE. | Analysis Year | 2038 | Analysis Period | 1 > 7:00 | | |
| Intersection | I-25 NB Ramp | File Name | 2038_PB_Optimized.xus | | | | |
| Project Description | 2038 PM Peak Hour Build Mitigated - Exis. Geom | | | | | | |



| Demand Information | EB | | | WB | | | NB | | | SB | | |
|---------------------|-----|-----|---|----|------|-----|-----|-----|-----|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Approach Movement | | | | | | | | | | | | |
| Demand (v), veh/h | 349 | 789 | | | 1205 | 620 | 132 | 545 | 188 | | | |

| Signal Information | | | | Phase Diagram | | | | | | | | |
|--------------------|-------|-----------------|-------|---------------|-----|-----|--|--|--|--|--|--|
| Cycle, s | 120.0 | Reference Phase | 2 | | | | | | | | | |
| Offset, s | 119 | Reference Point | Begin | | | | | | | | | |
| Uncoordinated | No | Simult. Gap E/W | On | | | | | | | | | |
| Force Mode | Fixed | Simult. Gap N/S | On | | | | | | | | | |
| Green | 6.0 | 74.0 | 24.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Yellow | 4.0 | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | | | | | | |
| Red | 1.0 | 1.0 | 2.0 | 0.0 | 0.0 | 0.0 | | | | | | |

| Timer Results | EBL | EBT | WBL | WBT | NBL | NBT | SBL | SBT |
|--|------|------|-----|------|-----|------|-----|-----|
| Assigned Phase | 5 | 2 | | 6 | | 4 | | |
| Case Number | 1.0 | 4.0 | | 7.3 | | 9.0 | | |
| Phase Duration, s | 11.0 | 90.0 | | 79.0 | | 30.0 | | |
| Change Period, (Y+R _c), s | 5.0 | 5.0 | | 5.0 | | 6.0 | | |
| Max Allow Headway (MAH), s | 4.1 | 0.0 | | 0.0 | | 4.1 | | |
| Queue Clearance Time (g _s), s | 8.0 | | | | | 15.0 | | |
| Green Extension Time (g _e), s | 0.0 | 0.0 | | 0.0 | | 2.6 | | |
| Phase Call Probability | 1.00 | | | | | 1.00 | | |
| Max Out Probability | 1.00 | | | | | 0.30 | | |

| Movement Group Results | EB | | | WB | | | NB | | | SB | | |
|--|-------|-------|---|----|-------|-------|-------|-------|-------|----|---|---|
| | L | T | R | L | T | R | L | T | R | L | T | R |
| Assigned Movement | 5 | 2 | | | 6 | 16 | 7 | 4 | 14 | | | |
| Adjusted Flow Rate (v), veh/h | 356 | 804 | | | 1205 | 620 | 132 | 545 | 188 | | | |
| Adjusted Saturation Flow Rate (s), veh/h/ln | 1767 | 1685 | | | 1685 | 1572 | 1767 | 1856 | 1572 | | | |
| Queue Service Time (g _s), s | 6.0 | 8.2 | | | 14.4 | 29.9 | 7.7 | 10.4 | 13.0 | | | |
| Cycle Queue Clearance Time (g _c), s | 6.0 | 8.2 | | | 14.4 | 29.9 | 7.7 | 10.4 | 13.0 | | | |
| Green Ratio (g/C) | 0.68 | 0.71 | | | 0.62 | 0.62 | 0.20 | 0.20 | 0.20 | | | |
| Capacity (c), veh/h | 377 | 3580 | | | 3117 | 970 | 353 | 1113 | 314 | | | |
| Volume-to-Capacity Ratio (X) | 0.944 | 0.225 | | | 0.387 | 0.639 | 0.373 | 0.490 | 0.598 | | | |
| Back of Queue (Q), ft/ln (95 th percentile) | 425 | 132 | | | 229 | 413 | 168 | 218 | 248 | | | |
| Back of Queue (Q), veh/ln (95 th percentile) | 16.6 | 5.1 | | | 8.9 | 16.1 | 6.6 | 8.5 | 9.7 | | | |
| Queue Storage Ratio (RQ) (95 th percentile) | 1.10 | 0.00 | | | 0.00 | 4.86 | 0.59 | 0.00 | 0.00 | | | |
| Uniform Delay (d ₁), s/veh | 22.1 | 8.0 | | | 11.6 | 14.6 | 41.5 | 42.6 | 43.6 | | | |
| Incremental Delay (d ₂), s/veh | 32.6 | 0.1 | | | 0.4 | 3.2 | 3.0 | 1.5 | 8.1 | | | |
| Initial Queue Delay (d ₃), s/veh | 0.0 | 0.0 | | | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | | | |
| Control Delay (d), s/veh | 54.7 | 8.1 | | | 11.9 | 17.8 | 44.5 | 44.1 | 51.8 | | | |
| Level of Service (LOS) | D | A | | | B | B | D | D | D | | | |
| Approach Delay, s/veh / LOS | 22.4 | C | | | 13.9 | B | 45.8 | D | 0.0 | | | |
| Intersection Delay, s/veh / LOS | 23.6 | | | | | | C | | | | | |

| Multimodal Results | EB | WB | NB | SB |
|----------------------------|----|----|----|----|
| Pedestrian LOS Score / LOS | | | | |
| Bicycle LOS Score / LOS | | | | |

CRASH SUMMARY REPORT

Lomas Blvd NE & I-25 Southbound Ramp

Created on August 25, 2025

Created by Nishat Zaman

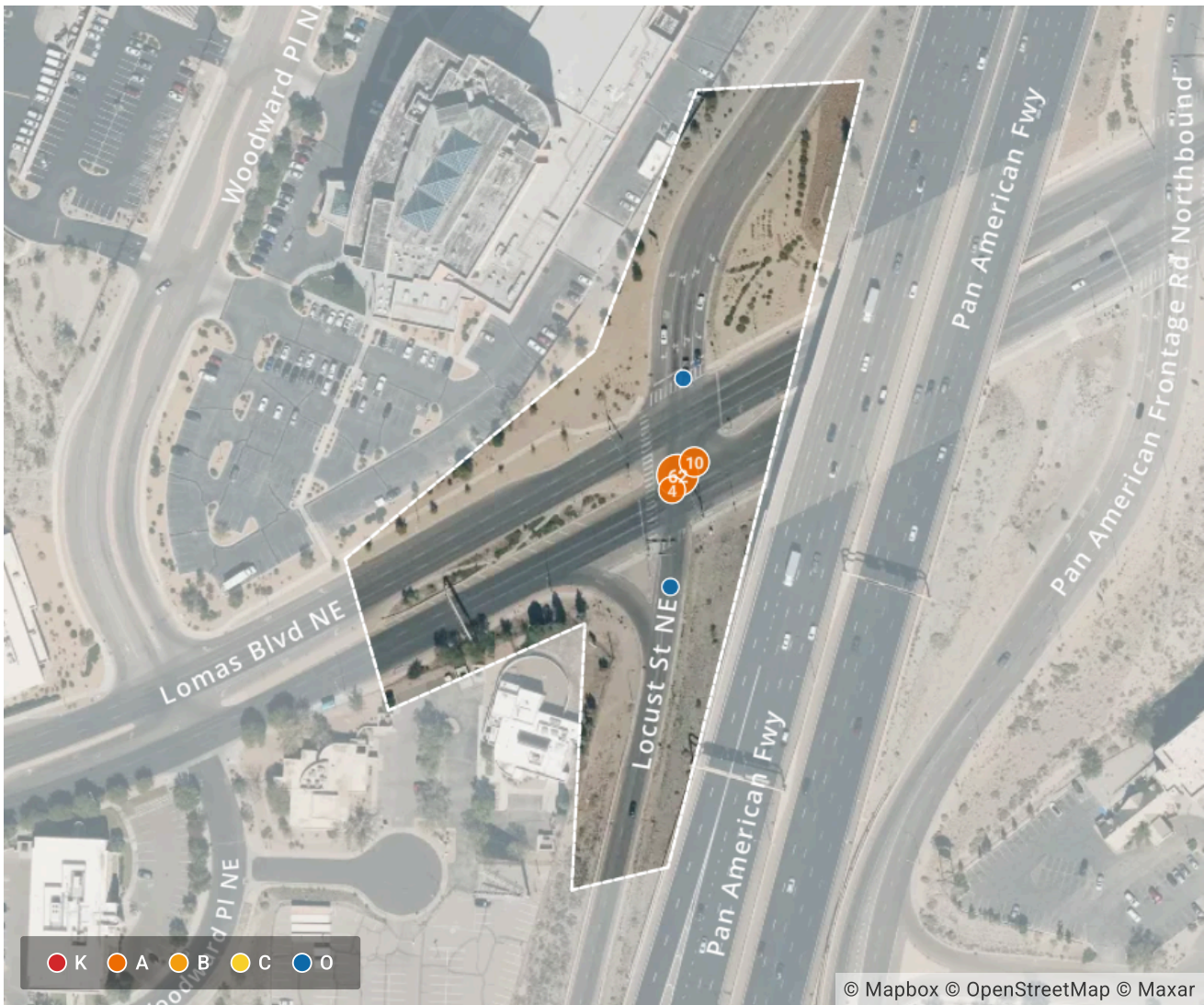
Requested by Tierra West LLC

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Polygon



© Mapbox © OpenStreetMap © Maxar

Total Crashes

78

Fatal Crashes

0

New Mexico Summary

Total Crashes

78

Crash

100.00%

Intersection Involved

40

51.28%

Alcohol Involved

3

3.85%

| | | |
|-----------------------------------|---|-------|
| Commercial Motor Vehicle Involved | 2 | 2.56% |
| + 3 more | 0 | 0% |

| KABCO Crash Severity | Crash | |
|------------------------------|-------|--------|
| (O) Property-Damage Only | 49 | 62.82% |
| (C) Possible Injury | 16 | 20.51% |
| (B) Suspected Minor Injury | 9 | 11.54% |
| (A) Suspected Serious Injury | 4 | 5.13% |
| (K) Fatal Injury | 0 | 0.00% |

| Crash Date (Year) | Crash | |
|-------------------|-------|--------|
| 2023 | 14 | 17.95% |
| 2022 | 22 | 28.21% |
| 2021 | 13 | 16.67% |
| 2020 | 9 | 11.54% |
| 2019 | 20 | 25.64% |
| + 6 more | 0 | 0% |

| Crash Classification | Crash | |
|----------------------|-------|--------|
| Other Vehicle | 28 | 35.90% |
| Fixed Object | 2 | 2.56% |
| Invalid Code | 1 | 1.28% |
| + 10 more | 0 | 0% |

| First Harmful Event - Analysis | Crash | |
|--------------------------------|-------|--------|
| MV in Transport | 62 | 79.49% |
| Not Available | 7 | 8.97% |
| Other Non-fixed Object | 2 | 2.56% |
| Median | 1 | 1.28% |
| Other Non-Collision | 1 | 1.28% |
| Traffic Signal Support | 1 | 1.28% |
| Unknown | 1 | 1.28% |
| Utility Pole/Light Support | 1 | 1.28% |
| + 55 more | 0 | 0% |

| First Harmful Event - Location | Crash | |
|--------------------------------|-------|--------|
| On Roadway | 25 | 32.05% |
| Not Available | 20 | 25.64% |
| On Roadside - Right | 2 | 2.56% |

+ 10 more

A - 202

0

0%

First Harmful Event - Manner of Impact

Crash

| | | |
|---------------|----|--------|
| Not Available | 20 | 25.64% |
| Front-to-Side | 16 | 20.51% |
| Front-to-Rear | 5 | 6.41% |
| Other | 3 | 3.85% |
| Sideswipe | 2 | 2.56% |
| + 5 more | 0 | 0% |

Injury Severity

Person

| | | |
|------------------------------|-----|--------|
| No Apparent Injury (O) | 148 | 72.20% |
| Possible Injury (C) | 35 | 17.07% |
| Suspected Minor Injury (B) | 18 | 8.78% |
| Suspected Serious Injury (A) | 4 | 1.95% |
| Fatal Injury (K) | 0 | 0.00% |

Contributing Factors

Vehicle

| | | |
|--------------------------------|----|--------|
| Other, No Driver Error | 47 | 36.43% |
| Driver Inattention | 38 | 29.46% |
| Disregarded Traffic Signal | 14 | 10.85% |
| Failed to Yield Right of Way | 14 | 10.85% |
| Other Improper Driving | 10 | 7.75% |
| Excessive Speed | 5 | 3.88% |
| Following too Closely | 3 | 2.33% |
| Under the Influence of Alcohol | 3 | 2.33% |
| + 47 more | 12 | 9.33% |

Driver Actions

Vehicle

| | | |
|--|----|--------|
| Going Straight | 81 | 62.79% |
| Left Turn | 33 | 25.58% |
| Stopped for Sign or Signal | 9 | 6.98% |
| Right Turn | 8 | 6.20% |
| Changing Lanes | 3 | 2.33% |
| Stopped for Traffic | 3 | 2.33% |
| Leaving Traffic Lane | 1 | 0.78% |
| Operated MV in Reckless or Aggressive Manner | 1 | 0.78% |
| + 15 more | 5 | 3.90% |

CRASH SUMMARY REPORT

Lomas Blvd NE - I-25 Northbound Ramp

Created on September 2, 2025

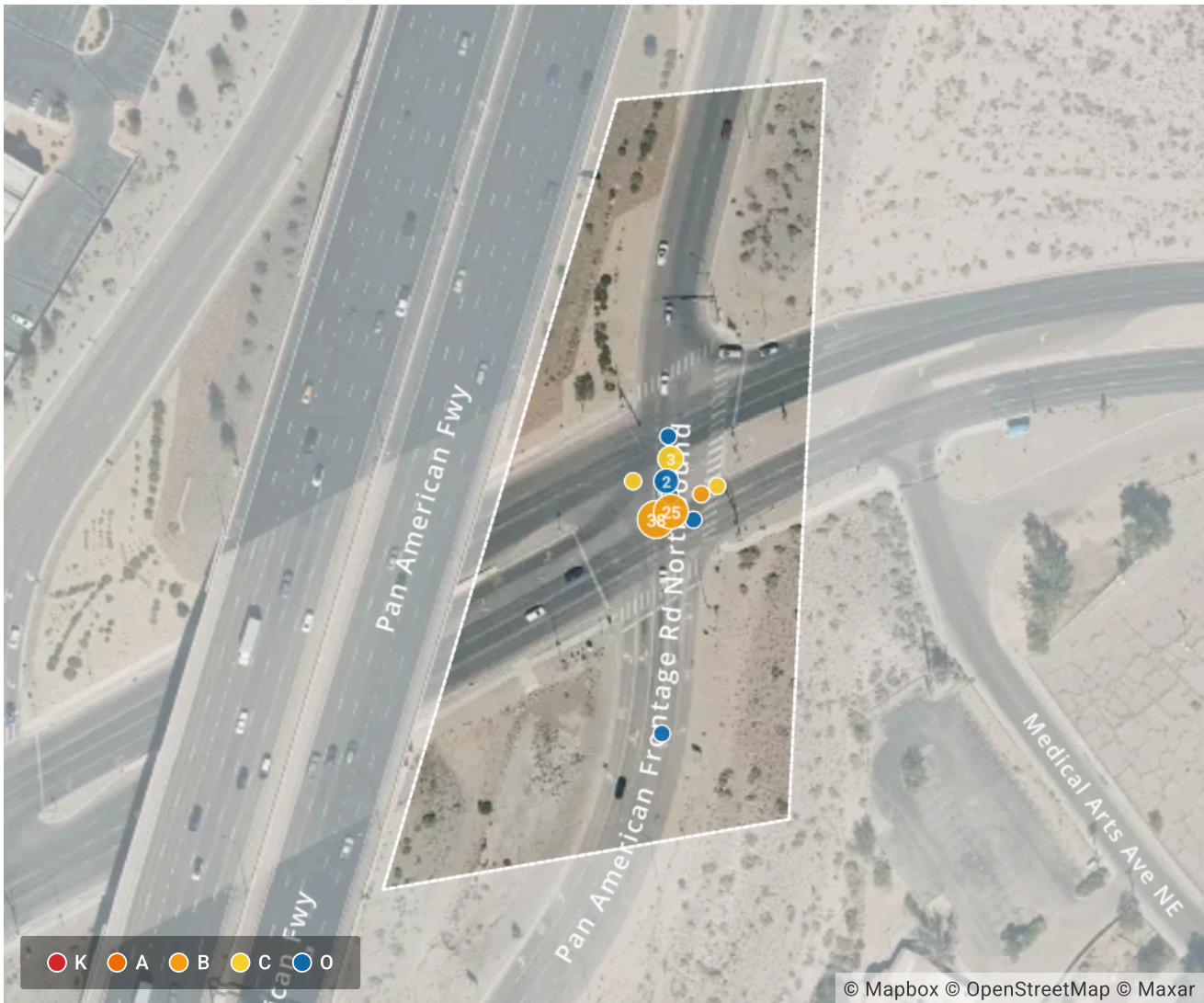
Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Polygon



| | | | |
|---------------|----|---------------|---|
| Total Crashes | 75 | Fatal Crashes | 0 |
|---------------|----|---------------|---|

| New Mexico Summary | | Crash |
|-----------------------|----|---------|
| Total Crashes | 75 | 100.00% |
| Intersection Involved | 25 | 33.33% |
| Alcohol Involved | 5 | 6.67% |

| | | |
|---------------------|---|-------|
| Pedalcycle Involved | 1 | 1.33% |
| Pedestrian Involved | 1 | 1.33% |
| + 2 more | 0 | 0% |

| KABCO Crash Severity | Crash | |
|----------------------------|-------|--------|
| (O) Property-Damage Only | 56 | 74.67% |
| (C) Possible Injury | 14 | 18.67% |
| (B) Suspected Minor Injury | 5 | 6.67% |
| + 2 more | 0 | 0% |

| Crash Date (Year) | Crash | |
|-------------------|-------|--------|
| 2023 | 15 | 20.00% |
| 2022 | 13 | 17.33% |
| 2021 | 10 | 13.33% |
| 2020 | 14 | 18.67% |
| 2019 | 23 | 30.67% |
| + 6 more | 0 | 0% |

| Crash Classification | Crash | |
|----------------------|-------|--------|
| Other Vehicle | 36 | 48.00% |
| Fixed Object | 3 | 4.00% |
| + 11 more | 0 | 0% |

| First Harmful Event - Analysis | Crash | |
|--------------------------------|-------|--------|
| MV in Transport | 55 | 73.33% |
| Not Available | 6 | 8.00% |
| Curb | 4 | 5.33% |
| Median | 2 | 2.67% |
| Utility Pole/Light Support | 2 | 2.67% |
| Other Post, Pole or Support | 1 | 1.33% |
| Pedalcycle | 1 | 1.33% |
| Pedestrian | 1 | 1.33% |
| + 55 more | 1 | 1.33% |

| First Harmful Event - Location | Crash | |
|--------------------------------|-------|--------|
| Not Available | 23 | 30.67% |
| On Roadway | 21 | 28.00% |
| On Roadside - Left | 1 | 1.33% |
| On Roadside - Right | 1 | 1.33% |

| | | |
|-------------|---|-------|
| On Shoulder | 1 | 1.33% |
| + 8 more | 0 | 0% |

| First Harmful Event - Manner of Impact | Crash | |
|--|-------|--------|
| Not Available | 23 | 30.67% |
| Front-to-Side | 12 | 16.00% |
| Front-to-Rear | 9 | 12.00% |
| Sideswipe | 2 | 2.67% |
| Front-to-Front | 1 | 1.33% |
| + 5 more | 0 | 0% |

| Injury Severity | Person | |
|----------------------------|--------|--------|
| No Apparent Injury (O) | 137 | 83.03% |
| Possible Injury (C) | 23 | 13.94% |
| Suspected Minor Injury (B) | 5 | 3.03% |
| + 2 more | 0 | 0% |

| Contributing Factors | Vehicle | |
|-------------------------------------|---------|--------|
| Driver Inattention | 37 | 30.58% |
| Other, No Driver Error | 37 | 30.58% |
| Disregarded Traffic Signal | 17 | 14.05% |
| Failed to Yield Right of Way | 10 | 8.26% |
| Other Improper Driving | 5 | 4.13% |
| Under the Influence of Alcohol | 5 | 4.13% |
| Driver Distracted by Other Activity | 4 | 3.31% |
| Excessive Speed | 4 | 3.31% |
| + 47 more | 20 | 16.54% |

| Driver Actions | Vehicle | |
|----------------------------|---------|--------|
| Going Straight | 81 | 66.94% |
| Left Turn | 17 | 14.05% |
| Stopped for Sign or Signal | 9 | 7.44% |
| Right Turn | 6 | 4.96% |
| Stopped for Traffic | 3 | 2.48% |
| Negotiating a Curve | 2 | 1.65% |
| Other | 2 | 1.65% |
| Unknown | 2 | 1.65% |
| + 15 more | 6 | 4.98% |

CRASH SUMMARY REPORT

Lomas Blvd NE & University Blvd NE

Created on August 25, 2025

Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Circle 250 ft



| | | | |
|---------------|-----|---------------|---|
| Total Crashes | 109 | Fatal Crashes | 1 |
|---------------|-----|---------------|---|

| New Mexico Summary | | Crash | |
|-----------------------|-----|-------|---------|
| Total Crashes | 109 | | 100.00% |
| Intersection Involved | 36 | | 33.03% |
| Alcohol Involved | 5 | | 4.59% |

| | | |
|---------------------|---|-------|
| Pedalcycle Involved | 1 | 0.92% |
| Pedestrian Involved | 1 | 0.92% |
| + 2 more | 0 | 0% |

| KABCO Crash Severity | Crash | |
|------------------------------|-------|--------|
| (O) Property-Damage Only | 72 | 66.06% |
| (C) Possible Injury | 29 | 26.61% |
| (B) Suspected Minor Injury | 7 | 6.42% |
| (K) Fatal Injury | 1 | 0.92% |
| (A) Suspected Serious Injury | 0 | 0.00% |

| Crash Date (Year) | Crash | |
|-------------------|-------|--------|
| 2023 | 23 | 21.10% |
| 2022 | 19 | 17.43% |
| 2021 | 24 | 22.02% |
| 2020 | 11 | 10.09% |
| 2019 | 32 | 29.36% |
| + 6 more | 0 | 0% |

| Crash Classification | Crash | |
|-----------------------|-------|--------|
| Other Vehicle | 52 | 47.71% |
| Fixed Object | 4 | 3.67% |
| Other (Non-Collision) | 1 | 0.92% |
| Pedestrian | 1 | 0.92% |
| + 9 more | 0 | 0% |

| First Harmful Event - Analysis | Crash | |
|--------------------------------|-------|--------|
| MV in Transport | 77 | 70.64% |
| Not Available | 14 | 12.84% |
| Curb | 2 | 1.83% |
| Median | 2 | 1.83% |
| Bridge Overhead Structure | 1 | 0.92% |
| Other Non-Collision | 1 | 0.92% |
| Pedalcycle | 1 | 0.92% |
| Pedestrian | 1 | 0.92% |
| + 55 more | 2 | 1.84% |

| First Harmful Event - Location | Crash | |
|--------------------------------|-------|--------|
| Not Available | 32 | 29.36% |

| | | |
|---------------------|----|--------|
| On Roadway | 28 | 25.69% |
| On Roadside - Right | 1 | 0.92% |
| Separator | 1 | 0.92% |
| + 9 more | 0 | 0% |

| First Harmful Event - Manner of Impact | Crash | |
|--|-------|--------|
| Not Available | 32 | 29.36% |
| Front-to-Rear | 14 | 12.84% |
| Front-to-Side | 11 | 10.09% |
| Front-to-Front | 2 | 1.83% |
| Sideswipe | 2 | 1.83% |
| + 5 more | 0 | 0% |

| Injury Severity | Person | |
|------------------------------|--------|--------|
| No Apparent Injury (O) | 219 | 81.72% |
| Possible Injury (C) | 41 | 15.30% |
| Suspected Minor Injury (B) | 7 | 2.61% |
| Fatal Injury (K) | 1 | 0.37% |
| Suspected Serious Injury (A) | 0 | 0.00% |

| Contributing Factors | Vehicle | |
|--------------------------------|---------|--------|
| Other, No Driver Error | 46 | 31.29% |
| Driver Inattention | 41 | 27.89% |
| Disregarded Traffic Signal | 14 | 9.52% |
| Failed to Yield Right of Way | 9 | 6.12% |
| Other Improper Driving | 6 | 4.08% |
| Excessive Speed | 5 | 3.40% |
| Speed too Fast for Conditions | 5 | 3.40% |
| Under the Influence of Alcohol | 5 | 3.40% |
| + 47 more | 22 | 14.96% |

| Driver Actions | Vehicle | |
|----------------------------|---------|--------|
| Going Straight | 95 | 64.63% |
| Left Turn | 20 | 13.61% |
| Stopped for Sign or Signal | 15 | 10.20% |
| Stopped for Traffic | 8 | 5.44% |
| Right Turn | 5 | 3.40% |
| Changing Lanes | 2 | 1.36% |
| Entering Traffic Lane | 2 | 1.36% |

| | | |
|---------------|----|-------|
| Ran Red Light | 2 | 1.36% |
| + 15 more | 11 | 7.48% |

CRASH SUMMARY REPORT

Lomas Blvd NE & Medical Arts Ave NE

Created on September 2, 2025

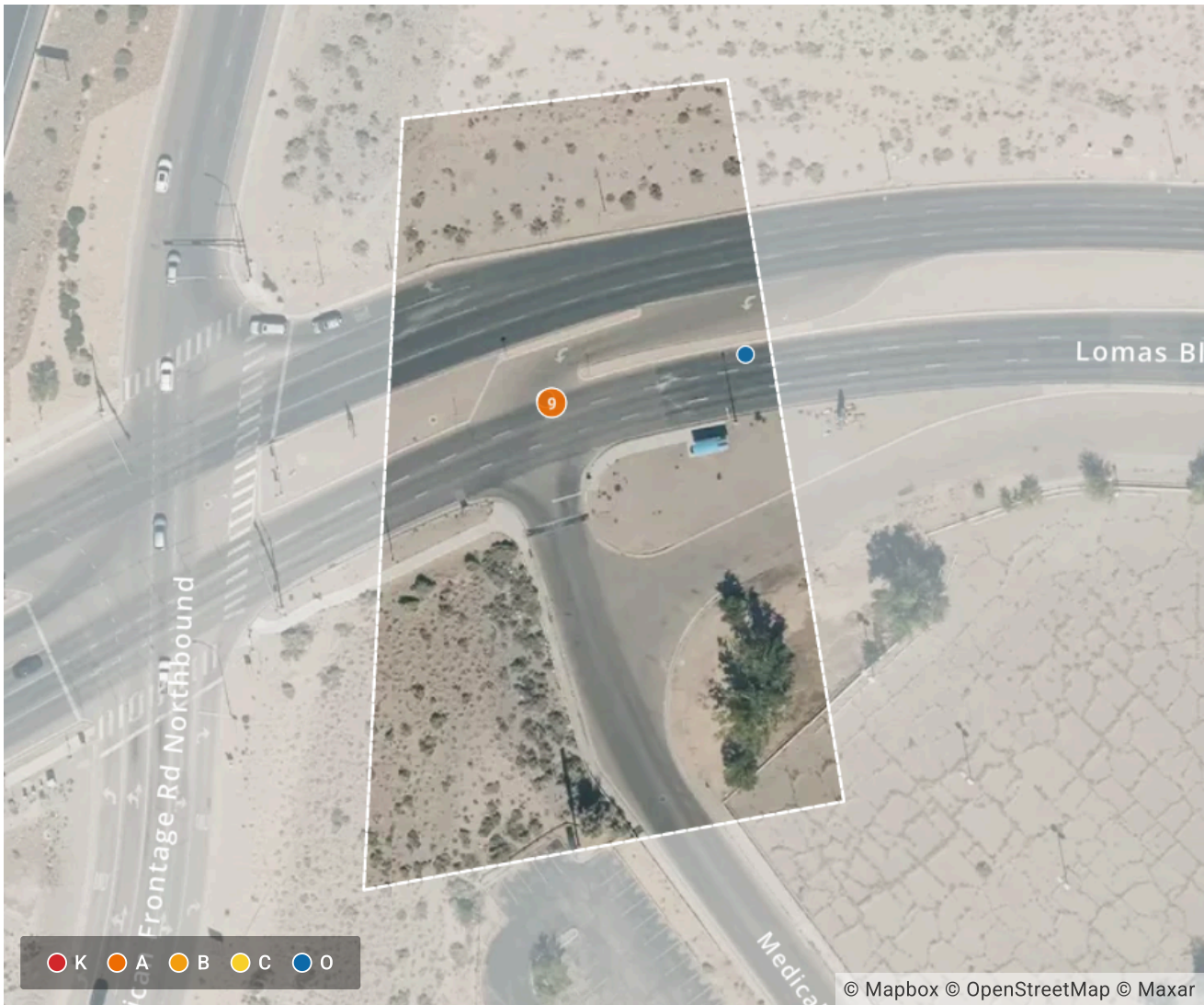
Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Polygon



| | | | |
|---------------|----|---------------|---|
| Total Crashes | 11 | Fatal Crashes | 0 |
|---------------|----|---------------|---|

| New Mexico Summary | | Crash | |
|-----------------------|----|---------|--|
| Total Crashes | 11 | 100.00% | |
| Intersection Involved | 2 | 18.18% | |
| Alcohol Involved | 1 | 9.09% | |

| | | |
|----------|---|----|
| + 4 more | 0 | 0% |
|----------|---|----|

| KABCO Crash Severity | Crash | |
|------------------------------|-------|--------|
| (O) Property-Damage Only | 8 | 72.73% |
| (C) Possible Injury | 2 | 18.18% |
| (A) Suspected Serious Injury | 1 | 9.09% |
| + 2 more | 0 | 0% |

| Crash Date (Year) | Crash | |
|-------------------|-------|--------|
| 2023 | 1 | 9.09% |
| 2022 | 2 | 18.18% |
| 2021 | 1 | 9.09% |
| 2020 | 5 | 45.45% |
| 2019 | 2 | 18.18% |
| + 6 more | 0 | 0% |

| Crash Classification | Crash | |
|----------------------|-------|--------|
| Other Vehicle | 6 | 54.55% |
| Parked Vehicle | 1 | 9.09% |
| + 11 more | 0 | 0% |

| First Harmful Event - Analysis | Crash | |
|--------------------------------|-------|--------|
| MV in Transport | 8 | 72.73% |
| Parked MV | 2 | 18.18% |
| Not Available | 1 | 9.09% |
| + 60 more | 0 | 0% |

| First Harmful Event - Location | Crash | |
|--------------------------------|-------|--------|
| Not Available | 2 | 18.18% |
| On Roadway | 2 | 18.18% |
| + 11 more | 0 | 0% |

| First Harmful Event - Manner of Impact | Crash | |
|--|-------|--------|
| Front-to-Rear | 4 | 36.36% |
| Not Available | 2 | 18.18% |
| + 8 more | 0 | 0% |

| Injury Severity | Person | |
|------------------------|--------|--------|
| No Apparent Injury (O) | 20 | 83.33% |

A - 212

| | | |
|------------------------------|---|--------|
| Possible Injury (C) | 3 | 12.50% |
| Suspected Serious Injury (A) | 1 | 4.17% |
| + 2 more | 0 | 0% |

| Contributing Factors | | Vehicle |
|--------------------------------|---|---------|
| Other, No Driver Error | 6 | 33.33% |
| Driver Inattention | 4 | 22.22% |
| Failed to Yield Right of Way | 2 | 11.11% |
| Avoid No Contact Vehicle | 1 | 5.56% |
| Disregarded Traffic Signal | 1 | 5.56% |
| Under the Influence of Alcohol | 1 | 5.56% |
| + 49 more | 0 | 0% |

| Driver Actions | | Vehicle |
|---------------------|---|---------|
| Going Straight | 8 | 44.44% |
| Left Turn | 4 | 22.22% |
| Parked | 2 | 11.11% |
| Right Turn | 2 | 11.11% |
| Stopped for Traffic | 1 | 5.56% |
| + 18 more | 0 | 0% |

CRASH SUMMARY REPORT

Lomas Blvd NE & Frontage Rd / Torc DWY

Created on September 2, 2025

Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Polygon



| | | | |
|---------------|---|---------------|---|
| Total Crashes | 0 | Fatal Crashes | 0 |
|---------------|---|---------------|---|

| | |
|--------------------|-------|
| New Mexico Summary | Crash |
| + 7 more | 0 |

| | |
|----------------------|-------|
| KABCO Crash Severity | Crash |
|----------------------|-------|

+ 5 more

0

Crash Date (Year)

Crash

+ 11 more

0

Crash Classification

Crash

+ 13 more

0

First Harmful Event - Analysis

Crash

+ 63 more

0

First Harmful Event - Location

Crash

+ 13 more

0

First Harmful Event - Manner of Impact

Crash

+ 10 more

0

Injury Severity

Person

+ 5 more

0

Contributing Factors

Vehicle

+ 55 more

0

Driver Actions

Vehicle

+ 23 more

0

CRASH SUMMARY REPORT

Lomas Blvd NE & Lobo Plaza West DWY / Legion Rd NE



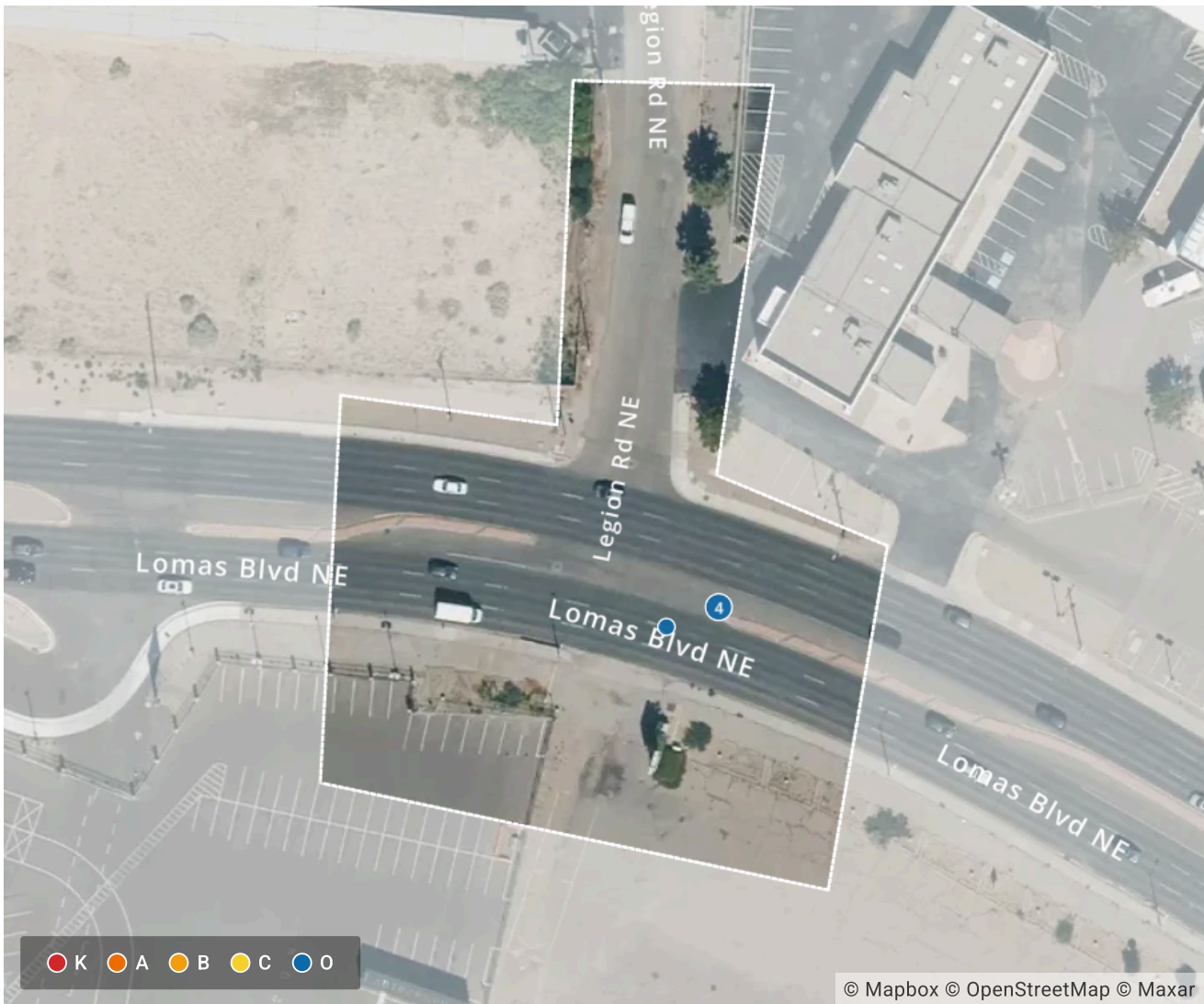
Created on September 2, 2025

Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023

Applied Filters

Shape: Polygon



| | | | |
|---------------|---|---------------|---|
| Total Crashes | 5 | Fatal Crashes | 0 |
|---------------|---|---------------|---|

| New Mexico Summary | | Crash | |
|-----------------------|---|---------|--|
| Total Crashes | 5 | 100.00% | |
| Intersection Involved | 1 | 20.00% | |

| | | | |
|---|--|----|---------|
| + 5 more | | 0 | 0% |
| KABCO Crash Severity | | | Crash |
| (O) Property-Damage Only | | 5 | 100.00% |
| + 4 more | | 0 | 0% |
| Crash Date (Year) | | | Crash |
| 2021 | | 2 | 40.00% |
| 2019 | | 3 | 60.00% |
| + 9 more | | 0 | 0% |
| Crash Classification | | | Crash |
| Other Vehicle | | 2 | 40.00% |
| Fixed Object | | 1 | 20.00% |
| + 11 more | | 0 | 0% |
| First Harmful Event - Analysis | | | Crash |
| MV in Transport | | 3 | 60.00% |
| Not Available | | 2 | 40.00% |
| + 61 more | | 0 | 0% |
| First Harmful Event - Location | | | Crash |
| Not Available | | 3 | 60.00% |
| On Roadway | | 2 | 40.00% |
| + 11 more | | 0 | 0% |
| First Harmful Event - Manner of Impact | | | Crash |
| Not Available | | 3 | 60.00% |
| Front-to-Side | | 2 | 40.00% |
| + 8 more | | 0 | 0% |
| Injury Severity | | | Person |
| No Apparent Injury (O) | | 12 | 100.00% |
| + 4 more | | 0 | 0% |
| Contributing Factors | | | Vehicle |
| Other, No Driver Error | | 3 | 50.00% |
| Driver Inattention | | 2 | 33.33% |
| + 53 more | | 0 | 0% |

A - 217

| Driver Actions | Vehicle |
|----------------|----------|
| Going Straight | 5 83.33% |
| Left Turn | 1 16.67% |
| + 21 more | 0 0% |

CRASH SUMMARY REPORT

Lomas Blvd NE & Lobo Plaza East DWY

Created on September 2, 2025

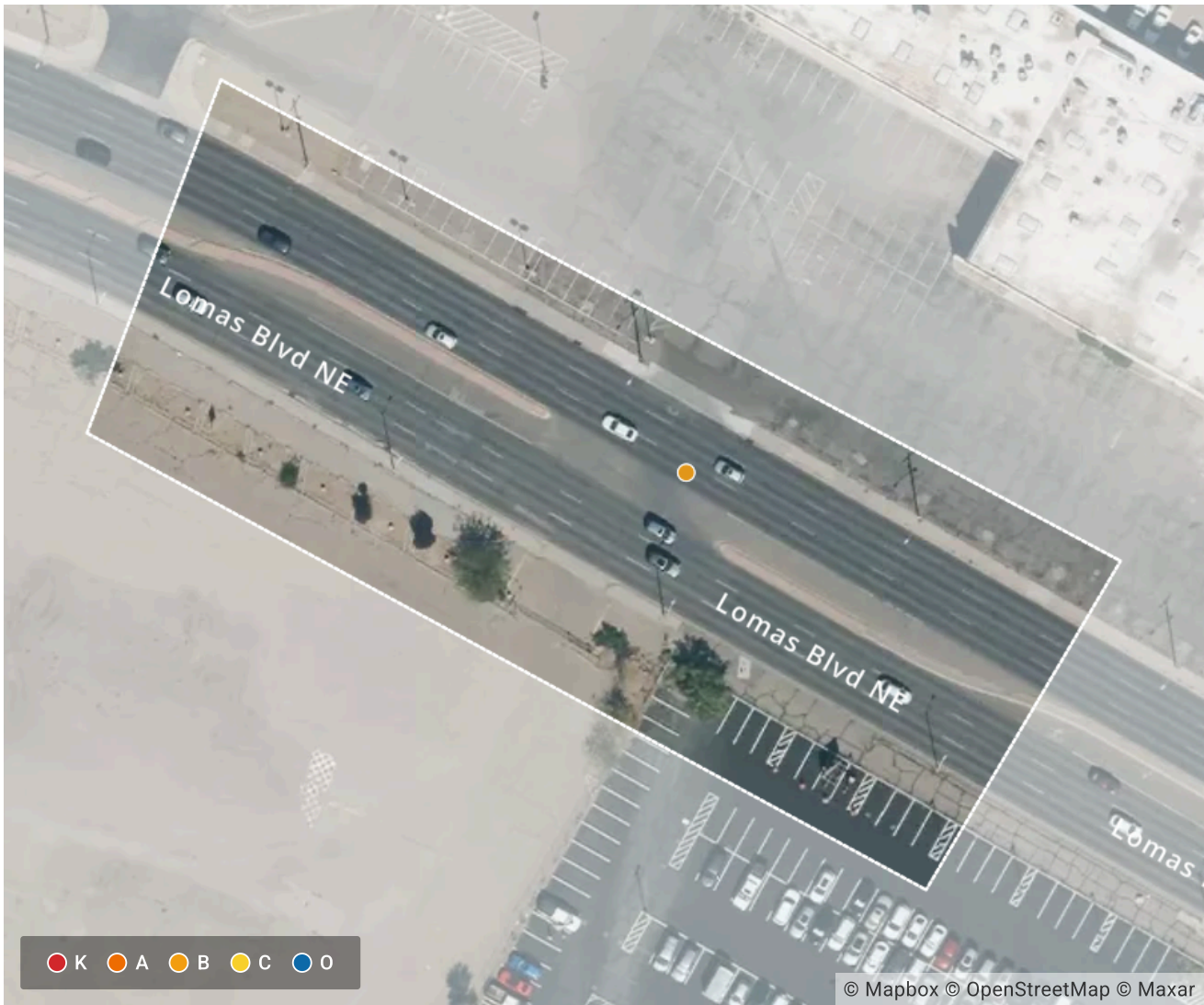
Created by Nishat Zaman

Data extents: January 1, 2019 to December 31, 2023



Applied Filters

Shape: Polygon



© Mapbox © OpenStreetMap © Maxar

| | | | |
|---------------|---|---------------|---|
| Total Crashes | 1 | Fatal Crashes | 0 |
|---------------|---|---------------|---|

| New Mexico Summary | | Crash | |
|--------------------|---|---------|--|
| Total Crashes | 1 | 100.00% | |
| + 6 more | 0 | 0% | |

| KABCO Crash Severity | | Crash |
|--|---|---------|
| (B) Suspected Minor Injury | 1 | 100.00% |
| + 4 more | 0 | 0% |
| Crash Date (Year) | | Crash |
| 2022 | 1 | 100.00% |
| + 10 more | 0 | 0% |
| Crash Classification | | Crash |
| + 13 more | 0 | 0% |
| First Harmful Event - Analysis | | Crash |
| Curb | 1 | 100.00% |
| + 62 more | 0 | 0% |
| First Harmful Event - Location | | Crash |
| + 13 more | 0 | 0% |
| First Harmful Event - Manner of Impact | | Crash |
| + 10 more | 0 | 0% |
| Injury Severity | | Person |
| No Apparent Injury (O) | 1 | 50.00% |
| Suspected Minor Injury (B) | 1 | 50.00% |
| + 3 more | 0 | 0% |
| Contributing Factors | | Vehicle |
| Driver Inattention | 1 | 100.00% |
| + 54 more | 0 | 0% |
| Driver Actions | | Vehicle |
| Going Straight | 1 | 100.00% |
| + 22 more | 0 | 0% |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
 N-S Street: **I-25 SB Ramp**

Speed Limit (Lomas Blvd NE)= **35**
 Speed Limit (I-25 SB Ramp)= **35**
8/27/25

Signalized

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I-25 SB Ramp) | | | Southbound (I-25 SB Ramp) | | |
|----------------------------------|----------|---------------------------|------------|----------|---------------------------|------------|--------------|---------------------------|----------|----------|---------------------------|------------|------------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 0 | 124 | 0 | 16 | 119 | 0 | 0 | 0 | 0 | 117 | 52 | 49 |
| 7:15 AM | 7:30 AM | 0 | 148 | 0 | 15 | 109 | 0 | 0 | 0 | 0 | 93 | 46 | 85 |
| 7:30 AM | 7:45 AM | 0 | 160 | 0 | 39 | 151 | 0 | 0 | 0 | 0 | 147 | 83 | 100 |
| 7:45 AM | 8:00 AM | 0 | 217 | 0 | 11 | 175 | 0 | 0 | 0 | 0 | 171 | 99 | 162 |
| 8:00 AM | 8:15 AM | 0 | 158 | 0 | 15 | 187 | 0 | 0 | 0 | 0 | 225 | 136 | 153 |
| 8:15 AM | 8:30 AM | 0 | 194 | 0 | 10 | 186 | 0 | 0 | 0 | 0 | 207 | 164 | 188 |
| 8:30 AM | 8:45 AM | 0 | 228 | 0 | 23 | 151 | 0 | 0 | 0 | 0 | 201 | 176 | 153 |
| 8:45 AM | 9:00 AM | 0 | 222 | 0 | 17 | 164 | 0 | 0 | 0 | 0 | 178 | 106 | 113 |
| 4X Peak 15-Min. Vol. (AM) | | 0 | 776 | 0 | 40 | 744 | 0 | 0 | 0 | 0 | 828 | 656 | 752 |
| % of Total Traffic | | 0.0% | 20.4% | 0.0% | 1.1% | 19.6% | 0.0% | 0.0% | 0.0% | 0.0% | 21.8% | 17.3% | 19.8% |
| % Directional | | | 20.4% | | | 20.7% | Intersection | | | 0.0% | | 58.9% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I-25 SB Ramp) | | | Southbound (I-25 SB Ramp) | | |
|----------------------------------|----------|---------------------------|------------|----------|---------------------------|------------|--------------|---------------------------|----------|----------|---------------------------|------------|------------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 0 | 168 | 0 | 52 | 189 | 0 | 0 | 0 | 0 | 55 | 108 | 43 |
| 4:15 PM | 4:30 PM | 0 | 209 | 0 | 61 | 208 | 0 | 0 | 0 | 0 | 28 | 83 | 36 |
| 4:30 PM | 4:45 PM | 0 | 191 | 0 | 68 | 201 | 0 | 0 | 0 | 0 | 53 | 95 | 43 |
| 4:45 PM | 5:00 PM | 0 | 197 | 0 | 48 | 247 | 0 | 0 | 0 | 0 | 48 | 89 | 42 |
| 5:00 PM | 5:15 PM | 0 | 216 | 0 | 80 | 229 | 0 | 0 | 0 | 0 | 49 | 107 | 59 |
| 5:15 PM | 5:30 PM | 0 | 216 | 0 | 70 | 224 | 0 | 0 | 0 | 0 | 30 | 73 | 44 |
| 5:30 PM | 5:45 PM | 0 | 146 | 0 | 57 | 194 | 0 | 0 | 0 | 0 | 17 | 77 | 34 |
| 5:45 PM | 6:00 PM | 0 | 120 | 0 | 56 | 201 | 0 | 0 | 0 | 0 | 36 | 42 | 54 |
| 4X Peak 15-Min. Vol. (PM) | | 0 | 864 | 0 | 320 | 916 | 0 | 0 | 0 | 0 | 196 | 428 | 236 |
| % of Total Traffic | | 0.0% | 29.2% | 0.0% | 10.8% | 30.9% | 0.0% | 0.0% | 0.0% | 0.0% | 6.6% | 14.5% | 8.0% |
| % Directional | | | 29.2% | | | 41.8% | Intersection | | | 0.0% | | 29.1% | |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
 N-S Street: **I-25 NB Ramp**

Speed Limit (Lomas Blvd NE)= **35**
 Speed Limit (I-25 NB Ramp)= **35**
8/27/25

Signalized

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I-25 NB Ramp) | | | Southbound (I-25 NB Ramp) | | |
|----------------------------------|----------|---------------------------|-------------|----------|---------------------------|------------|--------------|---------------------------|------------|------------|---------------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 54 | 193 | 0 | 0 | 90 | 16 | 44 | 85 | 72 | 0 | 0 | 0 |
| 7:15 AM | 7:30 AM | 48 | 196 | 0 | 0 | 91 | 29 | 31 | 91 | 45 | 0 | 0 | 0 |
| 7:30 AM | 7:45 AM | 47 | 256 | 0 | 0 | 166 | 68 | 31 | 75 | 31 | 0 | 0 | 0 |
| 7:45 AM | 8:00 AM | 62 | 302 | 0 | 0 | 157 | 55 | 35 | 117 | 25 | 0 | 0 | 0 |
| 8:00 AM | 8:15 AM | 39 | 340 | 0 | 0 | 173 | 44 | 41 | 110 | 40 | 0 | 0 | 0 |
| 8:15 AM | 8:30 AM | 54 | 372 | 0 | 0 | 159 | 50 | 36 | 140 | 41 | 0 | 0 | 0 |
| 8:30 AM | 8:45 AM | 62 | 378 | 0 | 0 | 139 | 38 | 38 | 123 | 60 | 0 | 0 | 0 |
| 8:45 AM | 9:00 AM | 57 | 363 | 0 | 0 | 131 | 45 | 42 | 97 | 74 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (AM) | | 216 | 1488 | 0 | 0 | 636 | 200 | 144 | 560 | 164 | 0 | 0 | 0 |
| % of Total Traffic | | 6.3% | 43.7% | 0.0% | 0.0% | 18.7% | 5.9% | 4.2% | 16.4% | 4.8% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 50.0% | | | 24.5% | Intersection | | 25.5% | | | 0.0% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (I-25 NB Ramp) | | | Southbound (I-25 NB Ramp) | | |
|----------------------------------|----------|---------------------------|------------|----------|---------------------------|-------------|--------------|---------------------------|------------|------------|---------------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 66 | 165 | 0 | 0 | 224 | 131 | 24 | 138 | 31 | 0 | 0 | 0 |
| 4:15 PM | 4:30 PM | 75 | 154 | 0 | 0 | 260 | 140 | 11 | 116 | 42 | 0 | 0 | 0 |
| 4:30 PM | 4:45 PM | 69 | 168 | 0 | 0 | 248 | 160 | 25 | 131 | 21 | 0 | 0 | 0 |
| 4:45 PM | 5:00 PM | 77 | 177 | 0 | 0 | 278 | 151 | 26 | 101 | 31 | 0 | 0 | 0 |
| 5:00 PM | 5:15 PM | 82 | 178 | 0 | 0 | 275 | 145 | 31 | 128 | 41 | 0 | 0 | 0 |
| 5:15 PM | 5:30 PM | 73 | 187 | 0 | 0 | 268 | 127 | 34 | 122 | 35 | 0 | 0 | 0 |
| 5:30 PM | 5:45 PM | 42 | 115 | 0 | 0 | 231 | 88 | 36 | 124 | 48 | 0 | 0 | 0 |
| 5:45 PM | 6:00 PM | 28 | 134 | 0 | 0 | 198 | 62 | 43 | 58 | 43 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (PM) | | 328 | 712 | 0 | 0 | 1100 | 580 | 124 | 512 | 164 | 0 | 0 | 0 |
| % of Total Traffic | | 9.3% | 20.2% | 0.0% | 0.0% | 31.3% | 16.5% | 3.5% | 14.5% | 4.7% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 29.5% | | | 47.7% | Intersection | | 22.7% | | | 0.0% | |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
 N-S Street: **University Blvd NE**

Signalized

Speed Limit (Lomas Blvd NE)= **35**
 Speed Limit (University Blvd NE)= **35**
8/27/25

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|----------------------------------|----------|---------------------------|------------|------------|---------------------------|------------|--------------|---------------------------------|------------|------------|---------------------------------|------------|-----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 61 | 166 | 15 | 23 | 102 | 36 | 8 | 66 | 16 | 26 | 53 | 8 |
| 7:15 AM | 7:30 AM | 43 | 151 | 14 | 46 | 111 | 40 | 10 | 81 | 29 | 23 | 71 | 17 |
| 7:30 AM | 7:45 AM | 44 | 148 | 40 | 56 | 212 | 42 | 24 | 101 | 45 | 47 | 121 | 25 |
| 7:45 AM | 8:00 AM | 34 | 155 | 64 | 54 | 171 | 48 | 35 | 145 | 45 | 51 | 150 | 26 |
| 8:00 AM | 8:15 AM | 48 | 152 | 82 | 56 | 188 | 49 | 24 | 135 | 45 | 39 | 151 | 18 |
| 8:15 AM | 8:30 AM | 39 | 168 | 125 | 52 | 143 | 44 | 27 | 105 | 27 | 42 | 157 | 27 |
| 8:30 AM | 8:45 AM | 68 | 215 | 115 | 45 | 146 | 48 | 24 | 116 | 45 | 28 | 187 | 15 |
| 8:45 AM | 9:00 AM | 59 | 222 | 100 | 49 | 134 | 37 | 26 | 108 | 38 | 39 | 177 | 27 |
| 4X Peak 15-Min. Vol. (AM) | | 272 | 860 | 460 | 180 | 584 | 192 | 96 | 464 | 180 | 112 | 748 | 60 |
| % of Total Traffic | | 6.5% | 20.4% | 10.9% | 4.3% | 13.9% | 4.6% | 2.3% | 11.0% | 4.3% | 2.7% | 17.8% | 1.4% |
| % Directional | | | 37.8% | | | 22.7% | Intersection | | | 17.6% | | 21.9% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (University Blvd NE) | | | Southbound (University Blvd NE) | | |
|----------------------------------|----------|---------------------------|------------|------------|---------------------------|-------------|--------------|---------------------------------|------------|------------|---------------------------------|------------|------------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 35 | 123 | 25 | 49 | 211 | 32 | 60 | 160 | 70 | 43 | 142 | 74 |
| 4:15 PM | 4:30 PM | 44 | 135 | 32 | 65 | 243 | 36 | 53 | 146 | 51 | 32 | 110 | 77 |
| 4:30 PM | 4:45 PM | 30 | 122 | 33 | 56 | 252 | 23 | 54 | 156 | 68 | 47 | 138 | 97 |
| 4:45 PM | 5:00 PM | 33 | 137 | 45 | 58 | 255 | 44 | 55 | 131 | 71 | 35 | 123 | 71 |
| 5:00 PM | 5:15 PM | 43 | 123 | 44 | 53 | 254 | 28 | 59 | 151 | 66 | 38 | 151 | 91 |
| 5:15 PM | 5:30 PM | 48 | 154 | 35 | 55 | 251 | 34 | 50 | 117 | 64 | 28 | 119 | 60 |
| 5:30 PM | 5:45 PM | 27 | 109 | 18 | 50 | 197 | 31 | 36 | 76 | 33 | 36 | 123 | 55 |
| 5:45 PM | 6:00 PM | 20 | 142 | 22 | 42 | 194 | 21 | 35 | 65 | 39 | 27 | 93 | 51 |
| 4X Peak 15-Min. Vol. (PM) | | 172 | 492 | 176 | 212 | 1016 | 112 | 236 | 604 | 264 | 152 | 604 | 364 |
| % of Total Traffic | | 3.9% | 11.2% | 4.0% | 4.8% | 23.1% | 2.5% | 5.4% | 13.7% | 6.0% | 3.5% | 13.7% | 8.3% |
| % Directional | | | 19.1% | | | 30.4% | Intersection | | | 25.1% | | 25.4% | |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
N-S Street: **Medical Arts Ave NE**

Signalized

Speed Limit (Lomas Blvd NE)= **35**
Speed Limit (Medical Arts Ave NE): **30**
8/27/25

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|----------------------------------|----------|---------------------------|-------------|------------|---------------------------|------------|--------------|----------------------------------|----------|-----------|----------------------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 0 | 240 | 27 | 3 | 105 | 0 | 0 | 0 | 4 | 0 | 0 | 0 |
| 7:15 AM | 7:30 AM | 0 | 202 | 40 | 10 | 121 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| 7:30 AM | 7:45 AM | 0 | 232 | 54 | 15 | 236 | 0 | 0 | 0 | 4 | 0 | 0 | 0 |
| 7:45 AM | 8:00 AM | 0 | 268 | 59 | 20 | 217 | 0 | 0 | 0 | 7 | 0 | 0 | 0 |
| 8:00 AM | 8:15 AM | 0 | 312 | 66 | 9 | 215 | 0 | 0 | 0 | 9 | 0 | 0 | 0 |
| 8:15 AM | 8:30 AM | 0 | 364 | 49 | 11 | 202 | 0 | 0 | 0 | 15 | 0 | 0 | 0 |
| 8:30 AM | 8:45 AM | 0 | 376 | 59 | 6 | 178 | 0 | 0 | 0 | 12 | 0 | 0 | 0 |
| 8:45 AM | 9:00 AM | 0 | 375 | 63 | 18 | 175 | 0 | 0 | 0 | 17 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (AM) | | 0 | 1500 | 252 | 72 | 700 | 0 | 0 | 0 | 68 | 0 | 0 | 0 |
| % of Total Traffic | | 0.0% | 57.9% | 9.7% | 2.8% | 27.0% | 0.0% | 0.0% | 0.0% | 2.6% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 67.6% | | | 29.8% | Intersection | | | 2.6% | | 0.0% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (Medical Arts Ave NE) | | | Southbound (Medical Arts Ave NE) | | |
|----------------------------------|----------|---------------------------|------------|-----------|---------------------------|-------------|--------------|----------------------------------|----------|-----------|----------------------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 0 | 180 | 17 | 10 | 371 | 0 | 0 | 0 | 21 | 0 | 0 | 0 |
| 4:15 PM | 4:30 PM | 0 | 189 | 6 | 9 | 389 | 0 | 0 | 0 | 23 | 0 | 0 | 0 |
| 4:30 PM | 4:45 PM | 0 | 175 | 14 | 7 | 421 | 0 | 0 | 0 | 22 | 0 | 0 | 0 |
| 4:45 PM | 5:00 PM | 0 | 187 | 17 | 7 | 416 | 0 | 0 | 0 | 24 | 0 | 0 | 0 |
| 5:00 PM | 5:15 PM | 0 | 207 | 13 | 2 | 250 | 0 | 0 | 0 | 11 | 0 | 0 | 0 |
| 5:15 PM | 5:30 PM | 0 | 213 | 8 | 8 | 383 | 0 | 0 | 0 | 12 | 0 | 0 | 0 |
| 5:30 PM | 5:45 PM | 0 | 158 | 5 | 5 | 319 | 0 | 0 | 0 | 15 | 0 | 0 | 0 |
| 5:45 PM | 6:00 PM | 0 | 170 | 9 | 4 | 264 | 0 | 0 | 0 | 7 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (PM) | | 0 | 748 | 68 | 28 | 1664 | 0 | 0 | 0 | 96 | 0 | 0 | 0 |
| % of Total Traffic | | 0.0% | 28.7% | 2.6% | 1.1% | 63.9% | 0.0% | 0.0% | 0.0% | 3.7% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 31.3% | | | 65.0% | Intersection | | | 3.7% | | 0.0% | |

Traffic Count Data Sheet

Year Counts Taken:

2025

E-W Street **Lomas blvd**

N-S Street: **Torc Dwy**

Speed Limit (Lomas blvd)=

45

Speed Limit (Torc Dwy)=

30

Signalized

8/26/25

| Begin Time | End Time | Eastbound (Lomas blvd) | | | Westbound (Lomas blvd) | | | Northbound (Torc Dwy) | | | Southbound (Torc Dwy) | | |
|----------------------------------|----------|------------------------|-------------|----------|------------------------|------------|---------------------|-----------------------|----------|----------|-----------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 7 | 242 | 0 | 0 | 108 | 2 | 0 | 0 | 1 | 0 | 0 | 0 |
| 7:15 AM | 7:30 AM | 1 | 203 | 0 | 0 | 131 | 1 | 1 | 0 | 0 | 0 | 0 | 0 |
| 7:30 AM | 7:45 AM | 3 | 238 | 0 | 0 | 262 | 0 | 0 | 0 | 1 | 0 | 0 | 0 |
| 7:45 AM | 8:00 AM | 4 | 267 | 1 | 0 | 222 | 0 | 0 | 0 | 1 | 0 | 0 | 0 |
| 8:00 AM | 8:15 AM | 3 | 312 | 1 | 0 | 228 | 2 | 2 | 0 | 1 | 0 | 0 | 0 |
| 8:15 AM | 8:30 AM | 2 | 347 | 0 | 0 | 199 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 8:30 AM | 8:45 AM | 1 | 382 | 0 | 0 | 177 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 8:45 AM | 9:00 AM | 0 | 390 | 0 | 1 | 193 | 0 | 1 | 0 | 0 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (AM) | | 0 | 1560 | 0 | 4 | 772 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| % of Total Traffic | | 0.0% | 66.7% | 0.0% | 0.2% | 33.0% | 0.0% | 0.2% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 66.7% | | | 33.2% | Intersection | | 0.2% | | | 0.0% | |

| Begin Time | End Time | Eastbound (Lomas blvd) | | | Westbound (Lomas blvd) | | | Northbound (Torc Dwy) | | | Southbound (Torc Dwy) | | |
|----------------------------------|----------|------------------------|------------|----------|------------------------|-------------|---------------------|-----------------------|----------|----------|-----------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 1 | 194 | 0 | 1 | 353 | 1 | 1 | 0 | 0 | 0 | 0 | 0 |
| 4:15 PM | 4:30 PM | 3 | 198 | 1 | 0 | 377 | 0 | 0 | 0 | 2 | 0 | 0 | 0 |
| 4:30 PM | 4:45 PM | 3 | 195 | 0 | 0 | 405 | 3 | 1 | 0 | 0 | 0 | 0 | 0 |
| 4:45 PM | 5:00 PM | 1 | 194 | 0 | 0 | 393 | 2 | 1 | 0 | 0 | 0 | 0 | 0 |
| 5:00 PM | 5:15 PM | 0 | 222 | 0 | 1 | 422 | 0 | 1 | 0 | 0 | 0 | 0 | 0 |
| 5:15 PM | 5:30 PM | 0 | 217 | 0 | 0 | 363 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 5:30 PM | 5:45 PM | 3 | 160 | 0 | 2 | 301 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 5:45 PM | 6:00 PM | 1 | 168 | 1 | 0 | 268 | 0 | 2 | 0 | 0 | 0 | 0 | 0 |
| 4X Peak 15-Min. Vol. (PM) | | 0 | 888 | 0 | 4 | 1688 | 0 | 4 | 0 | 0 | 0 | 0 | 0 |
| % of Total Traffic | | 0.0% | 34.4% | 0.0% | 0.2% | 65.3% | 0.0% | 0.2% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% |
| % Directional | | | 34.4% | | | 65.5% | Intersection | | 0.2% | | | 0.0% | |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
N-S Street: **West Dwy / Legion Rd**

Unsignalized

Speed Limit (Lomas Blvd NE)= **35**
Speed Limit (West Dwy / Legion Rd)= **35**
8/26/25

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|----------------------------------|----------|---------------------------|-------------|----------|---------------------------|------------|--------------|-----------------------------------|----------|----------|-----------------------------------|----------|-----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 7 | 242 | 0 | 0 | 108 | 2 | 0 | 0 | 1 | 2 | 0 | 1 |
| 7:15 AM | 7:30 AM | 1 | 203 | 0 | 0 | 131 | 1 | 1 | 0 | 0 | 0 | 0 | 0 |
| 7:30 AM | 7:45 AM | 3 | 238 | 0 | 0 | 262 | 0 | 0 | 0 | 1 | 1 | 0 | 1 |
| 7:45 AM | 8:00 AM | 4 | 267 | 1 | 0 | 222 | 0 | 0 | 0 | 1 | 1 | 0 | 1 |
| 8:00 AM | 8:15 AM | 3 | 312 | 1 | 0 | 228 | 2 | 2 | 0 | 1 | 0 | 0 | 1 |
| 8:15 AM | 8:30 AM | 2 | 347 | 0 | 0 | 199 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 8:30 AM | 8:45 AM | 1 | 382 | 0 | 0 | 177 | 1 | 0 | 0 | 0 | 1 | 0 | 0 |
| 8:45 AM | 9:00 AM | 5 | 385 | 0 | 0 | 190 | 1 | 0 | 0 | 0 | 2 | 0 | 3 |
| 4X Peak 15-Min. Vol. (AM) | | 20 | 1540 | 0 | 0 | 760 | 4 | 0 | 0 | 0 | 8 | 0 | 12 |
| % of Total Traffic | | 0.9% | 65.7% | 0.0% | 0.0% | 32.4% | 0.2% | 0.0% | 0.0% | 0.0% | 0.3% | 0.0% | 0.5% |
| % Directional | | | 66.6% | | | 32.6% | Intersection | | | | | 0.9% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (West Dwy / Legion Rd) | | | Southbound (West Dwy / Legion Rd) | | |
|----------------------------------|----------|---------------------------|------------|----------|---------------------------|-------------|--------------|-----------------------------------|----------|----------|-----------------------------------|----------|-----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 1 | 192 | 0 | 1 | 349 | 1 | 1 | 0 | 0 | 1 | 0 | 13 |
| 4:15 PM | 4:30 PM | 3 | 194 | 1 | 0 | 372 | 0 | 0 | 0 | 2 | 2 | 0 | 8 |
| 4:30 PM | 4:45 PM | 3 | 192 | 0 | 0 | 405 | 3 | 1 | 0 | 0 | 1 | 0 | 22 |
| 4:45 PM | 5:00 PM | 1 | 191 | 0 | 0 | 390 | 2 | 1 | 0 | 0 | 2 | 0 | 13 |
| 5:00 PM | 5:15 PM | 3 | 218 | 1 | 0 | 413 | 1 | 0 | 0 | 2 | 1 | 0 | 10 |
| 5:15 PM | 5:30 PM | 0 | 215 | 0 | 0 | 356 | 1 | 0 | 0 | 0 | 1 | 0 | 8 |
| 5:30 PM | 5:45 PM | 3 | 158 | 0 | 2 | 298 | 1 | 0 | 0 | 0 | 0 | 0 | 2 |
| 5:45 PM | 6:00 PM | 1 | 167 | 1 | 0 | 268 | 0 | 2 | 0 | 0 | 1 | 0 | 2 |
| 4X Peak 15-Min. Vol. (PM) | | 12 | 872 | 4 | 0 | 1652 | 4 | 0 | 0 | 8 | 4 | 0 | 40 |
| % of Total Traffic | | 0.5% | 33.6% | 0.2% | 0.0% | 63.6% | 0.2% | 0.0% | 0.0% | 0.3% | 0.2% | 0.0% | 1.5% |
| % Directional | | | 34.2% | | | 63.8% | Intersection | | | | | 1.7% | |

Traffic Count Data Sheet

Year Counts Taken: **2025**

E-W Street **Lomas Blvd NE**
 N-S Street: **East Dwy**

Speed Limit (Lomas Blvd NE)= **35**
 Speed Limit (East Dwy)= **25**
8/26/25

Signalized

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|----------------------------------|----------|---------------------------|-------------|----------|---------------------------|------------|--------------|-----------------------|----------|----------|-----------------------|----------|----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 7:00 AM | 7:15 AM | 0 | 243 | 0 | 0 | 108 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| 7:15 AM | 7:30 AM | 0 | 208 | 0 | 0 | 129 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| 7:30 AM | 7:45 AM | 0 | 239 | 0 | 0 | 254 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| 7:45 AM | 8:00 AM | 0 | 272 | 0 | 0 | 216 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 8:00 AM | 8:15 AM | 0 | 313 | 0 | 0 | 221 | 0 | 0 | 0 | 0 | 1 | 0 | 0 |
| 8:15 AM | 8:30 AM | 0 | 361 | 0 | 0 | 190 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| 8:30 AM | 8:45 AM | 0 | 380 | 0 | 0 | 179 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| 8:45 AM | 9:00 AM | 0 | 401 | 0 | 0 | 188 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 4X Peak 15-Min. Vol. (AM) | | 0 | 1604 | 0 | 0 | 752 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| % of Total Traffic | | 0.0% | 68.0% | 0.0% | 0.0% | 31.9% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.2% |
| % Directional | | | 68.0% | | | 31.9% | Intersection | | | 0.0% | | 0.2% | |

| Begin Time | End Time | Eastbound (Lomas Blvd NE) | | | Westbound (Lomas Blvd NE) | | | Northbound (East Dwy) | | | Southbound (East Dwy) | | |
|----------------------------------|----------|---------------------------|------------|----------|---------------------------|-------------|--------------|-----------------------|----------|----------|-----------------------|----------|-----------|
| | | L | T | R | L | T | R | L | T | R | L | T | R |
| 4:00 PM | 4:15 PM | 0 | 199 | 0 | 0 | 352 | 1 | 0 | 0 | 0 | 0 | 0 | 2 |
| 4:15 PM | 4:30 PM | 0 | 205 | 0 | 0 | 368 | 0 | 0 | 0 | 0 | 0 | 0 | 3 |
| 4:30 PM | 4:45 PM | 0 | 199 | 0 | 0 | 398 | 0 | 0 | 0 | 0 | 1 | 0 | 3 |
| 4:45 PM | 5:00 PM | 0 | 198 | 0 | 0 | 387 | 0 | 0 | 0 | 0 | 2 | 0 | 3 |
| 5:00 PM | 5:15 PM | 0 | 225 | 0 | 0 | 412 | 0 | 0 | 0 | 0 | 0 | 0 | 3 |
| 5:15 PM | 5:30 PM | 0 | 218 | 0 | 0 | 364 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 5:30 PM | 5:45 PM | 0 | 162 | 0 | 0 | 291 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 5:45 PM | 6:00 PM | 0 | 171 | 0 | 0 | 275 | 1 | 0 | 0 | 0 | 0 | 0 | 1 |
| 4X Peak 15-Min. Vol. (PM) | | 0 | 900 | 0 | 0 | 1648 | 0 | 0 | 0 | 0 | 0 | 0 | 12 |
| % of Total Traffic | | 0.0% | 35.2% | 0.0% | 0.0% | 64.4% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.5% |
| % Directional | | | 35.2% | | | 64.4% | Intersection | | | 0.0% | | 0.5% | |

SCOPE OF TRAFFIC IMPACT STUDY (TIS)

TO: Terry Brown, PE
Tierra West, LLC
5571 Midway Park Pl. NE
Albuquerque, NM 87109

MEETING DATE: July 11, 2025 (1:30 PM)

ATTENDEES: Ernest Armijo (City of Albuquerque); Margaret Haynes (NM DOT); Ron Bohannon and Terry Brown (Tierra West, LLC)

PROJECT: 2025068] Lobo Plaza (1300 Lomas Blvd. NE)

REQUESTED CITY ACTION: ___ Zone Change Site Development Plan

 ___ Subdivision Building Permit ___ Sector Plan ___ Sector Plan Amendment

 ___ Curb Cut Permit ___ Conditional Use ___ Annexation ___ Site Plan Amendment

ASSOCIATED APPLICATION: Description of development, where, what, etc. Include acreage, uses, etc. **Proposed High Turnover (Sit-Down) Restaurants with 11,600 Sq Ft Floor Area (ITE Land Use Code 932); Strip Retail Plazas (ITE Land Use Code 822) with 9,180 Sq Ft Floor Area and a Hotel (ITE Land Use Code 310) with 140 Rooms**

SCOPE OF REPORT:

The Traffic Impact Study should follow the standard report format, which is outlined in the DPM. The following supplemental information is provided for the preparation of this specific study.

1. Trip Generation - Use Trip Generation Manual, 10th Edition.
Local data may be used for certain land use types as determined by staff.
Consultant to provide.

2. Appropriate study area:
Signalized Intersections;

1. Lomas Blvd NE & I-25 Southbound On-Off Ramp
2. Lomas Blvd NE & I-25 Northbound On-Off Ramp
3. Lomas Blvd NE & University Blvd NE

Unsignalized Intersections;

1. Lomas Blvd NE & I-25 Southbound On-Off Ramp
2. Lomas Blvd NE & Torc Driveway / Frontage Rd
3. Lomas Blvd NE & Lobo Plaza West Driveway / Legion Rd
4. Lomas Blvd NE & Lobo Plaza East Driveway

Driveway Intersections: all site drives.

3. Intersection turning movement counts
Study Time – 7-9 a.m. peak hour, 4-6 p.m. peak hour
Consultant to provide for all intersections listed above. Base traffic volumes for the Traffic Impact Study will be **Collected from the field**

4. Type of intersection progression and factors to be used.
 Type III arrival type (see “Highway Capacity Manual, current edition” or equivalent as approved by staff). Unless otherwise justified, peak hour factors and % heavy commercial should be taken directly from the MRCOG turning movement data provided or as calculated from current count data by consultant.

5. Boundaries of area to be used for trip distribution.
 City Wide - residential, office or industrial;
 2 mile radius – commercial;
 Interstate or to be determined by consultant - motel/hotel
 APS district boundary mapping for each school and bus routes

6. Basis for trip distribution.

Residential – Use inverse relationship based upon distance and employment. Use employment data from 2040 Socioeconomic Forecasts, MRCOG – See MRCOG website for most current data.

Office/Industrial - Use inverse relationship based upon distance and population. Use population data from 2040 Socioeconomic Forecasts, MRCOG – See MRCOG website for most current data.

Commercial - Use relationship based upon population. Use population data from 2040 Socioeconomic Forecasts, MRCOG – See MRCOG website for most current data.

Residential - $T_s = (T_t) (S_e / D) / (S_e / D)$
 T_s = Development to Individual Subarea Trips
 T_t = Total Trips
 S_e = Subarea Employment
 D = Distance from Development to Subarea

Office/Industrial - $T_s = (T_t) (S_p / D) / (S_p / D)$
 T_s = Development to Individual Subarea Trips
 T_t = Total Trips
 S_p = Subarea Population
 D = Distance from Development to Subarea

Commercial -
 $T_s = (T_t) (S_p) / (S_p)$
 T_s = Development to Individual Subarea Trips
 T_t = Total Trips
 S_p = Subarea Population

7. Traffic Assignment. Logical routing on the major street system.

8. Proposed developments which have been approved but not constructed that are to be Included in the analyses. Projects in the area include:
 a. None

9. Method of intersection capacity analysis - planning or operational (see “2016 Highway Capacity Manual” or equivalent [i.e. HCS, Synchro, Teapac, etc.] as approved by staff). Must use latest version of design software and/or current edition of design manual.
 Implementation Year: 2028
 Horizon Year: 2038

10. Traffic conditions for analysis:
 - a. Existing analysis yes no - year (2025);
 - b. Phase implementation year(s) without proposed development – 2028
 - c. Phase implementation year(s) with proposed development – 2028
 - d. Project horizon year without proposed development – 2038
 - e. Project horizon year with proposed development – 2038
 - f. Other –

11. Background traffic growth.
Method: use 10-year historical growth based on standard data from the MRCOG Traffic Flow Maps. Minimum growth rate to be used is 1/2%.

12. Planned (programmed) traffic improvements.
List planned CIP improvements in study area and projected project implementation year:
 - a. Project – Location (Implementation Year)

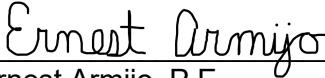
13. Items to be included in the study:
 - a. Intersection analysis.
 - b. Signal progression - An analysis is required if the driveway analysis indicates a traffic signal is possibly warranted. Analysis Method:
 - c. Arterial LOS analysis;
 - d. Recommended street, intersection and signal improvements.
 - e. Site design features such as turning lanes, median cuts, queuing requirements and site circulation, including driveway signalization and visibility.
 - f. Transportation system impacts.
 - g. Other mitigating measures.
 - h. Accident analyses yes no; Location(s): Eagle Ranch Rd. from Irving Blvd. to U.S. Eagle Credit Union driveway.
 - i. Weaving analyses yes no; Location(s):

14. Other:

SUBMITTAL REQUIREMENTS:

1. Number of copies of report required
 - a. 1 digital copy
2. Submittal Fee – \$1300 for up to 3 reviews

The Traffic Impact Study for this development proposal, project name, shall be performed in accordance with the above criteria. If there are any questions regarding the above items, please contact me at 924-3991.



Ernest Armijo, P.E.
Principal Engineer
City of Albuquerque, Planning
Transportation Development Section

9/10/2025

Date

via: email
C: TIS Task Force Attendees, file