

CITY OF ALBUQUERQUE



August 9, 2017

Van H. Gilbert
Van H. Gilbert Architect PC
2428 Baylor Dr. SE
Albuquerque, NM 87106

Re: Honda Gen-3; 8301 Lomas Blvd. NE
Traffic Circulation Layout
Traffic Circulation Layout
Architect's Stamp dated 08-10-17 (J19D072)

Dear Mr. Gilbert,

The TCL submittal received 08-09-17 is approved for Building Permit. A copy of the stamped and signed plan will be needed for each of the building permit plans. Please keep the original to be used for certification of the site for final C.O. for Transportation.

When the site construction is completed and a Certificate of Occupancy (C.O.) is requested, use the original City stamped approved TCL for certification. Redline any minor changes and adjustments that were made in the field. A NM registered architect or engineer must stamp, sign, and date the certification TCL along with indicating that the development was built in "substantial compliance" with the TCL. Submit this certification TCL with a completed Drainage and Transportation Information Sheet to front counter personnel for log in and evaluation by Transportation.

Once verification of certification is completed and approved, notification will be made to Building Safety to issue Final C.O. To confirm that a final C.O. has been issued, call Building Safety at 924-3690.

Sincerely,

Logan Patz
Senior Engineer, Planning Dept.
Development Review Services

mao via: email
C: CO Clerk, File

FASTRAX



City of Albuquerque

Planning Department

Development & Building Services Division

DRAINAGE AND TRANSPORTATION INFORMATION SHEET (REV 10/2015)

Project Title: Honda Gen-3 **Building Permit #:** _____ **Hydrology File #:** J19D 072
DRB#: _____ **EPC#:** _____ **Work Order#:** _____
Legal Description: LT 18-A-1-A Block 24 Plat of Lots 5-A, 7-A, 18-A-1-A Block 24 & Lot 13-A, A5-A & 18-1-A
City Address: and Pending Lots 19-24 Block 25 East Addition
8301 Lomas Boulevard NE, Albuquerque, NM 87110
Applicant: Garcia Automotive **Contact:** _____
Address: PO Box 26207, Albuquerque, NM 87125
Phone#: _____ **Fax#:** _____ **E-mail:** _____
Other Contact: Van H. Gilbert Architect PC **Contact:** Kevin DeGraauw
Address: 2428 Baylor Drive SE, Albuquerque, NM 87106
Phone#: 505-247-9955 **Fax#:** 505-247-1826 **E-mail:** kdegrauw@vhgarchitect.com

Check all that Apply:

DEPARTMENT:

- ☒ HYDROLOGY/ DRAINAGE
☒ TRAFFIC/ TRANSPORTATION
☐ MS4/ EROSION & SEDIMENT CONTROL

TYPE OF SUBMITTAL:

- ☐ ENGINEER/ARCHITECT CERTIFICATION
☐ CONCEPTUAL G & D PLAN
☐ GRADING PLAN
☐ DRAINAGE MASTER PLAN
☐ DRAINAGE REPORT
☐ CLOMR/LOMR
☒ TRAFFIC CIRCULATION LAYOUT (TCL)
☐ TRAFFIC IMPACT STUDY (TIS)
☐ EROSION & SEDIMENT CONTROL PLAN (ESC)
☐ OTHER (SPECIFY) _____

TYPE OF APPROVAL/ACCEPTANCE SOUGHT:

- ☐ BUILDING PERMIT APPROVAL
☐ CERTIFICATE OF OCCUPANCY
☐ PRELIMINARY PLAT APPROVAL
☐ SITE PLAN FOR SUB'D APPROVAL
☐ SITE PLAN FOR BLDG. PERMIT APPROVAL
☐ FINAL PLAT APPROVAL
☐ SIA/ RELEASE OF FINANCIAL GUARANTEE
☐ FOUNDATION PERMIT APPROVAL
☐ GRADING PERMIT APPROVAL
☐ SO-19 APPROVAL
☐ PAVING PERMIT APPROVAL
☐ GRADING/ PAD CERTIFICATION
☐ WORK ORDER APPROVAL
☐ CLOMR/LOMR

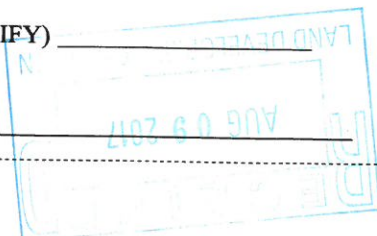
PRE-DESIGN MEETING?

☐ OTHER (SPECIFY) _____

IS THIS A RESUBMITTAL?: ☒ Yes ☐ No

DATE SUBMITTED: _____ By: _____

COA STAFF: _____ ELECTRONIC SUBMITTAL RECEIVED: _____



C:\Users\mlnee\Documents\REVIT LOCAL\15918 - Garcia Honda Gen III\15918 - Garcia Honda Gen III_NEW SERVICE BUILDING_2018_A_mlee.rvt

8/9/2017 12:52:30 PM

EXECUTIVE SUMMARY FOR THE HONDA GEN 3 TCL PLAN:

THE PROJECT IS LOCATED AT 8301 LOMAS BOULEVARD IN THE NORTHEAST QUADRANT OF ALBUQUERQUE. LOMAS BOULEVARD ABUTS THE SITE TO THE SOUTH, WITH NEIGHBORHOOD STREETS ABUTTING BOTH THE EAST AND WEST SIDES (UTAH STREET TO THE WEST AND VIRGINIA STREET TO THE EAST). A SMALL GROUP OF RESIDENTIAL PROPERTIES ARE IMMEDIATELY TO THE NORTH OF THE SITE AND INTERSTATE 40 IS A MAJOR BOUNDARY JUST NORTH OF RESIDENTIAL AREA. WYOMING BOULEVARD IS EAST OF THE SITE.

THE SITE IS SURROUNDED BY ACTIVE BUSINESSES, RETAIL SPACES, AND VIBRANT CAR DEALERSHIPS SO THE USAGE IS FITTING FOR THE AREA. THE SITE IS WELL BUFFERED BY EXISTING LANDSCAPING AND FENCE BUFFERS AS REQUIRED IN A C-2 ZONING CLASSIFICATION WHERE ADJACENT TO AN R-1 ZONING CLASSIFICATION. THERE CURRENTLY ARE NO SECTOR DEVELOPMENT PLANS THAT OVERLAY THIS SITE, ONLY A COMPREHENSIVE PLAN AREA ESTABLISHED URBAN. STANDARD C-2 REGULATIONS WILL APPLY AND THE WORK BEING APPLIED FOR WILL MEET THE CRITERIA FOR PARKING AND DEVELOPMENT AS OUTLINED IN THE CITY OF ALBUQUERQUE COMPREHENSIVE ZONING CODE.

AN EXISTING HONDA DEALERSHIP IS CURRENTLY LOCATED ON THE SITE. THE DEALER SPACE NEEDS TO BE REMODELED AND THE SERVICE DEPARTMENT EXPANDED AS SHOWN WITH THIS DRAWING PACKAGE. RECENTLY A STRIP OF RESIDENTIAL PROPERTIES TO THE NORTH WERE DEMOLISHED AND THE SITE WAS COMBINED INTO THE C-2 PROPERTY. THIS NOW EXTENDS THE SITE NORTH TO MARBLE AVENUE. WITH THIS LAND ADDITION, A SERVICE DEPARTMENT BUILDING CAN BE ADDED AS SHOWN IN THE DRAWING HERE.

THE CONCEPT FOR THIS SITE DEVELOPMENT IS TO FOLLOW A TYPICAL BRAND NAME CAR DEALERSHIP LAYOUT, WITH EXISTING RETAIL AND NEW CAR SALES FRONTING LOMAS AND UTAH, AND TO ALLOW THE SERVICE AND PARTS DEPARTMENT TO EXPAND TO THE NORTH FROM THE RETAIL ZONE. CUSTOMERS MAY ENTER ALONG LOMAS OR ALONG UTAH. THIS UTAH ENTRANCE WILL ALLOW ONE STOP SHOP EXPERIENCE FOR CAR OWNERS AS THEY CAN GET THEIR CAR SERVICED, PICK UP A PART OR ACCESSORY, AND ALSO SHOP FOR NEW CARS. THE NEW STREET DRIVE PADS AT UTAH ARE SET BACK FAR ENOUGH FROM THE LOMAS INTERSECTION TO ALLOW FOR PROPER APPROACHES, AND VISION TRIANGLES WERE USED IN THE LOCATION OF THE NEW BUILDING TO INCREASE SAFETY.

TRAFFIC WOULD CONTINUE TO USE THE EXISTING SUCCESSFUL PARKING AND CIRCULATION LAYOUT AS IS, WITH THE ONLY MODIFICATIONS BEING IMMEDIATELY ADJACENT TO THE NORTH AND ALONG THE NEW BUILDING. TWO-WAY TRAFFIC IS USED AT EVERY DRIVE LANE POSSIBLE TO EASE IN VEHICULAR SITE ACCESS, AND PEDESTRIAN FRIENDLY PATHS ONLY CROSS THESE TRAFFIC LANES AT PERPENDICULAR ANGLES.

THERE SHOULD BE LITTLE TO NO IMPACT TO ADJACENT SITES AS THIS CONTINUES TO BE A SUCCESSFUL CAR DEALERSHIP THAT HAS WORKED WELL FOR YEARS AT THIS LOCATION. LESS IMPACT IS ACTUALLY FELT AS THERE ARE LESS IMMEDIATE ADJACENT RESIDENTIAL PROPERTIES THAT SHARE PROPERTY LINES. MARBLE AVENUE ACTS AS A BUFFER TO THE NORTH EDGE OF THE SITE. NO VARIANCES SHOULD BE REQUESTED FOR THIS WORK TO ALLOW IT TO HAPPEN, AS THIS IS A PERMISSIVE USE OF C-2 AND THIS IS AN ESTABLISHED BUSINESS WITH EXISTING ACCESS POINTS AND AMPLE OFF-STREET PARKING TO BE REUSED.

CODE DATA, NEW AUTO DEALERSHIP SERVICE BUILDING (PHASE 1):

PROJECT NAME: HONDA GARCIA GEN 3, NEW AUTO DEALERSHIP SERVICE BUILDING (PHASE 1)

UPC: 101905844401540502

OWNER: SHEILA P. GARCIA

OWNER ADDRESS: PO BOX 26207 ALBUQUERQUE, NM 87125-6207

LEGAL DESCRIPTION: LOT 24-A, BLOCK 24

ZONE ATLAS MAP: J-19-Z

ZONE CLASSIFICATION: C-2

NOTE: TOTAL COMBINED ACREAGE OF ENTIRE SITE: 5.6 AC

OFF-STREET PARKING COUNT (CABQ Article 16: Zoning Code: Part 3, 14-16-3-1):

SERVICE USES (#27): 18,883 SF
FIRST 15,000 / 200 = 75
UP TO 45,000 SF 3,883 / 250 = 16
TOTAL: 91

OFFICES (#21): 668 SF / 200 = 04

TOTAL: 95
LESS 10% BUS DISCOUNT: 10
TOTAL REQUIRED: 85

PARKING PROVIDED ON LOT: 52
SERVICE BAYS/INTERIOR: 43
TOTAL PARKING PROVIDED: 95

HANDICAPPED PARKING SPACES REQUIRED: 04
HANDICAPPED PARKING SPACES PROVIDED: 04

MOTORCYCLE SPACES REQUIRED: 03
MOTORCYCLE SPACES PROVIDED: 03

BICYCLE SPACES REQUIRED: 14 / 20 = 04
BICYCLE SPACES PROVIDED: 04

CODE DATA, REMODELED AUTO DEALERSHIP SALES BUILDING, WITH ADDITIONS (PHASE 2):

PROJECT NAME: HONDA GARCIA GEN 3, REMODELED AUTO DEALERSHIP SALES BUILDING, WITH ADDITIONS (PHASE 2)

UPC: 101905844401540502

OWNER: SHEILA P. GARCIA

OWNER ADDRESS: PO BOX 26207 ALBUQUERQUE, NM 87125-6207

LEGAL DESCRIPTION: LT 18-A-1-A BLK 24 PLAT OF LOTS 5-A, 7-A, 18-A-1-A BLOCK 24 & LOT 13-A, AS-A & 18-1-A AND PENDING LOTS 19-24 BLOCK 25 EAST ADDITION

ZONE ATLAS MAP: J-19-Z

ZONE CLASSIFICATION: C-2

NOTE: TOTAL COMBINED ACREAGE OF ENTIRE SITE: 5.6 AC

OFF-STREET PARKING COUNT (CABQ Article 16: Zoning Code: Part 3, 14-16-3-1):

SERVICE USES (#27): 8,355 SF / 200 = 42
OFFICES (#21): 11,098 SF / 200 = 55

TOTAL: 97
LESS 10% BUS DISCOUNT: 10
TOTAL REQUIRED: 87

PARKING PROVIDED ON LOT: 81
SERVICE BAYS/INTERIOR: 37
TOTAL PARKING PROVIDED: 118

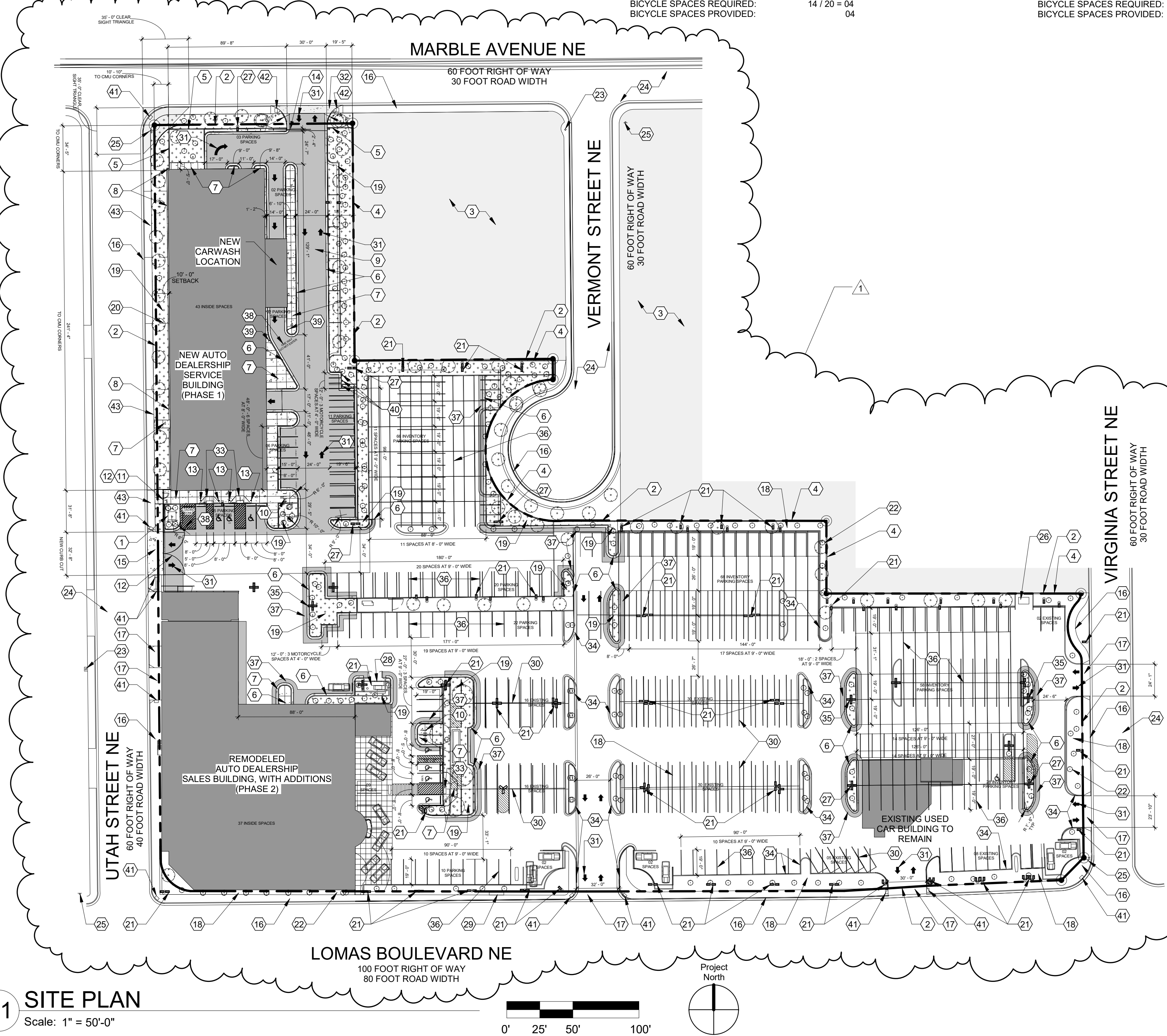
HANDICAPPED PARKING SPACES REQUIRED: 04
HANDICAPPED PARKING SPACES PROVIDED: 04

MOTORCYCLE SPACES REQUIRED: 03
MOTORCYCLE SPACES PROVIDED: 03

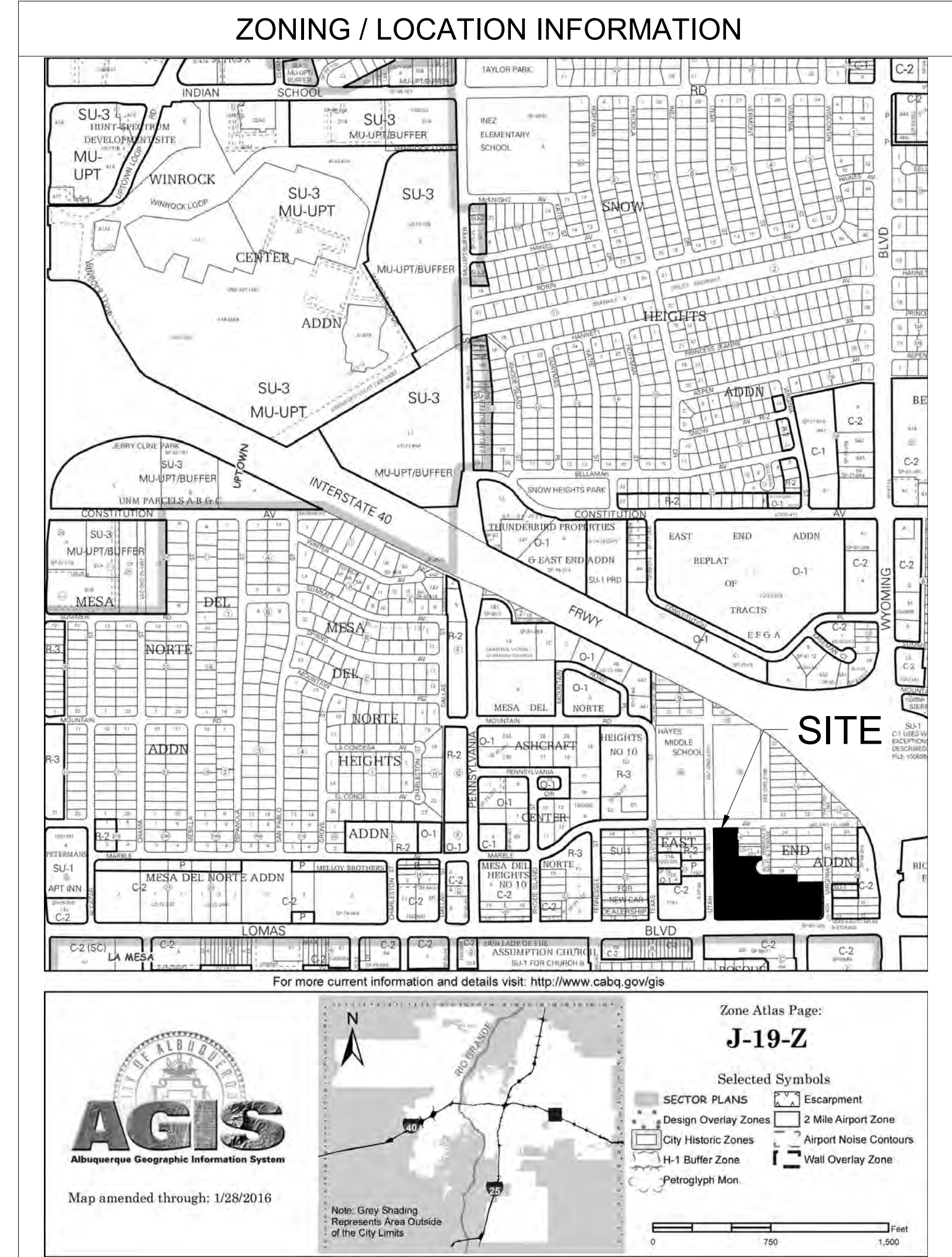
BICYCLE SPACES REQUIRED: 14 / 20 = 04
BICYCLE SPACES PROVIDED: 04

SHEET KEYNOTES - TCL-101

- 1 NEW TRANSFORMER
- 2 PROPERTY LINE
- 3 EXISTING RESIDENTIAL PROPERTIES
- 4 EXISTING SITE WALL, TYPICAL
- 5 NEW SITE WALL, MATCH EXISTING IN SIZE, TYPE, AND FINISH, REFER: C3/TCL-102
- 6 NEW 6" CONCRETE CURB AND GUTTER, TYPICAL; REFER: D1/TCL-102
- 7 NEW 6" THICK CONCRETE SIDEWALK
- 8 NEW CONCRETE MOW STRIP, REFER: B3/TCL-102
- 9 NEW ASPHALT PAVING, TYPICAL; REFER: D5/TCL-102
- 10 NEW BICYCLE RACK, REFER: B1/TCL-102
- 11 NEW METAL FENCE, REFER: A1/TCL-103
- 12 NEW METAL GATE, REFER: A1/TCL-103
- 13 NEW ADA PARKING SIGNAGE, REFER: A5 AND B5/TCL-102
- 14 NEW SLIDING METAL GATE, REFER: B1/TCL-103
- 15 NEW CURB CUT TO EXTENT AND DIMENSIONS SHOWN
- 16 EXISTING SIDEWALK, CURB, AND GUTTER, TYPICAL
- 17 EXISTING DRIVEWAY ENTRANCE
- 18 EXISTING LANDSCAPING, TYPICAL
- 19 NEW LANDSCAPING, TYPICAL
- 20 10' - 0" SETBACK
- 21 EXISTING LIGHT POLE
- 22 EXISTING TREE / PLANTINGS, TYPICAL
- 23 EXISTING FIRE HYDRANT
- 24 EXISTING PUBLIC STREET
- 25 EXISTING STOP SIGN / STREET SIGN
- 26 EXISTING DUMPSTER
- 27 NEW LIGHT POLE
- 28 EXISTING TRANSFORMER
- 29 EXISTING BUS STOP
- 30 EXISTING STRIPING TO REMAIN
- 31 NEW PAVEMENT MARKINGS, TYPICAL; REFER: C5/TCL-102
- 32 NEW CURB CUT ENTRANCE, REFER: A1/TCL-102
- 33 HC RAMP, REFER: C1/TCL-102
- 34 EXISTING CURB AND GUTTER, TYPICAL
- 35 RELOCATED LIGHT POLE
- 36 NEW PARKING SPACE STRIPING, TYPICAL
- 37 ASPHALT PATCH AND REPAIR AT CURB/GUTTER, TYPICAL
- 38 PAVEMENT SIGNAGE, TYPICAL; REFER: A3/TCL-102
- 39 NEW ONE WAY SIGNAGE, REFER: A5 AND B5/TCL-102
- 40 NEW MOTORCYCLE SIGNAGE, REFER: A5 AND B5/TCL-102
- 41 NEW 24" x 48" TRUNCATED DOME MAT, REPLACE RAMP TO CABQ STANDARDS IF EXISTING RAMP IS NOT UP TO CODE CODE
- 42 NEW 24" x 48" TRUNCATED DOME MAT AND ADA RAMP
- 43 EXISTING CURB CUTS TO BE DEMOLISHED, FILL IN WITH NEW GUTTER, CURB, AND SIDEWALK TO MATCH EXISTING



A1 SITE PLAN
Scale: 1" = 50'-0"



Van H. Gilbert Architect PC
ARCHITECTURE • INTERIORS • PLANNING



2428 Baylor Dr SE Albuquerque, NM 87106
Tel 505-247-9955 Fax 505-247-1826
E-mail info@vhgarchitect.com
Web Site www.vhgarchitect.com

CONSULTANTS

PROJECT

GARCIA AUTOMOTIVE GROUP

GARCIA HONDA - GENERATION 3 - NEW SERVICE BUILDING

ADDRESS

8301 LOMAS BLVD,

ALBUQUERQUE, NM

87110

PERMIT SET

DESIGNED TO EARN THE ENERGY STAR

The estimated energy performance for this design meets US EPA criteria. The building will be eligible for ENERGY STAR after maintaining superior performance for one year.

1 08/10/17 TCL COMMENTS

Mark Date Description

Date AUGUST, 2017

Project Number 15918

Drawn By MOL

Checked By KD, MB

Copyright © VAN H. GILBERT ARCHITECT PC

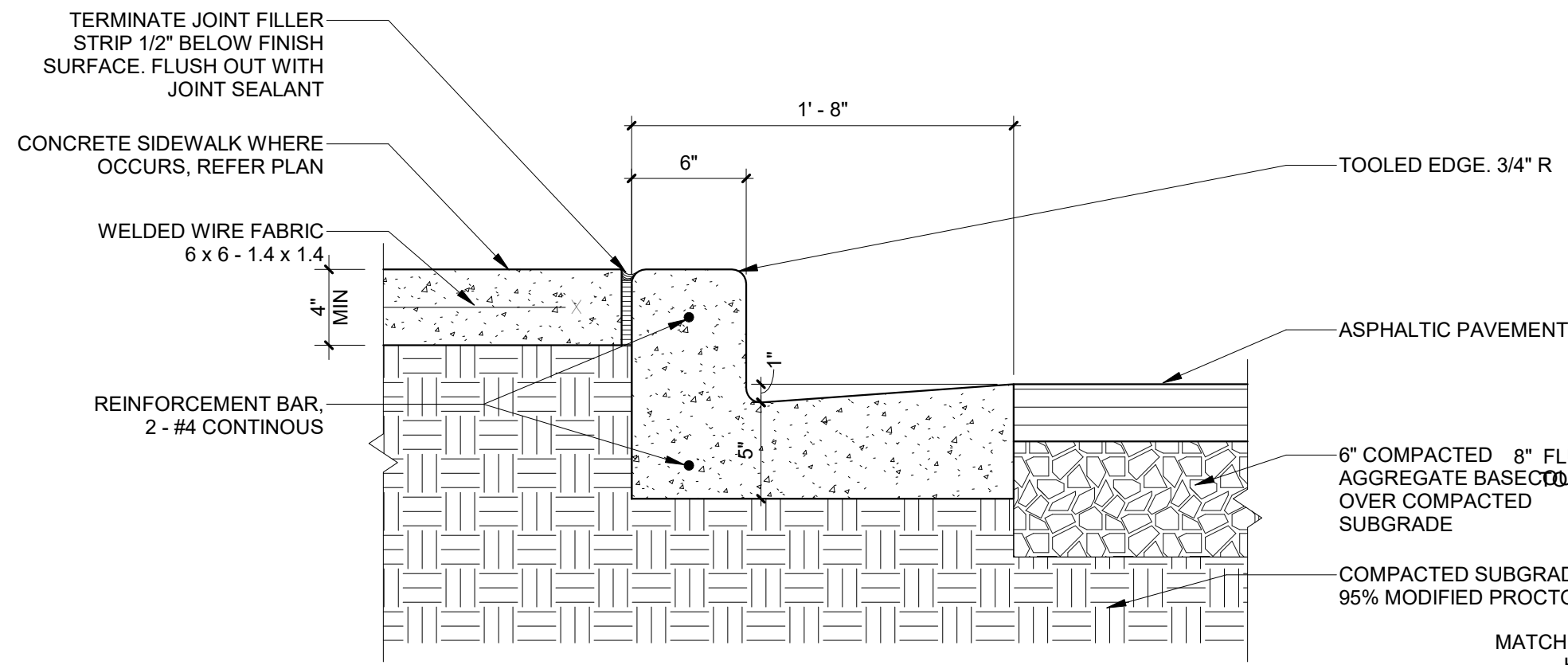
SHEET TITLE

TRAFFIC CIRCULATION LAYOUT PLAN

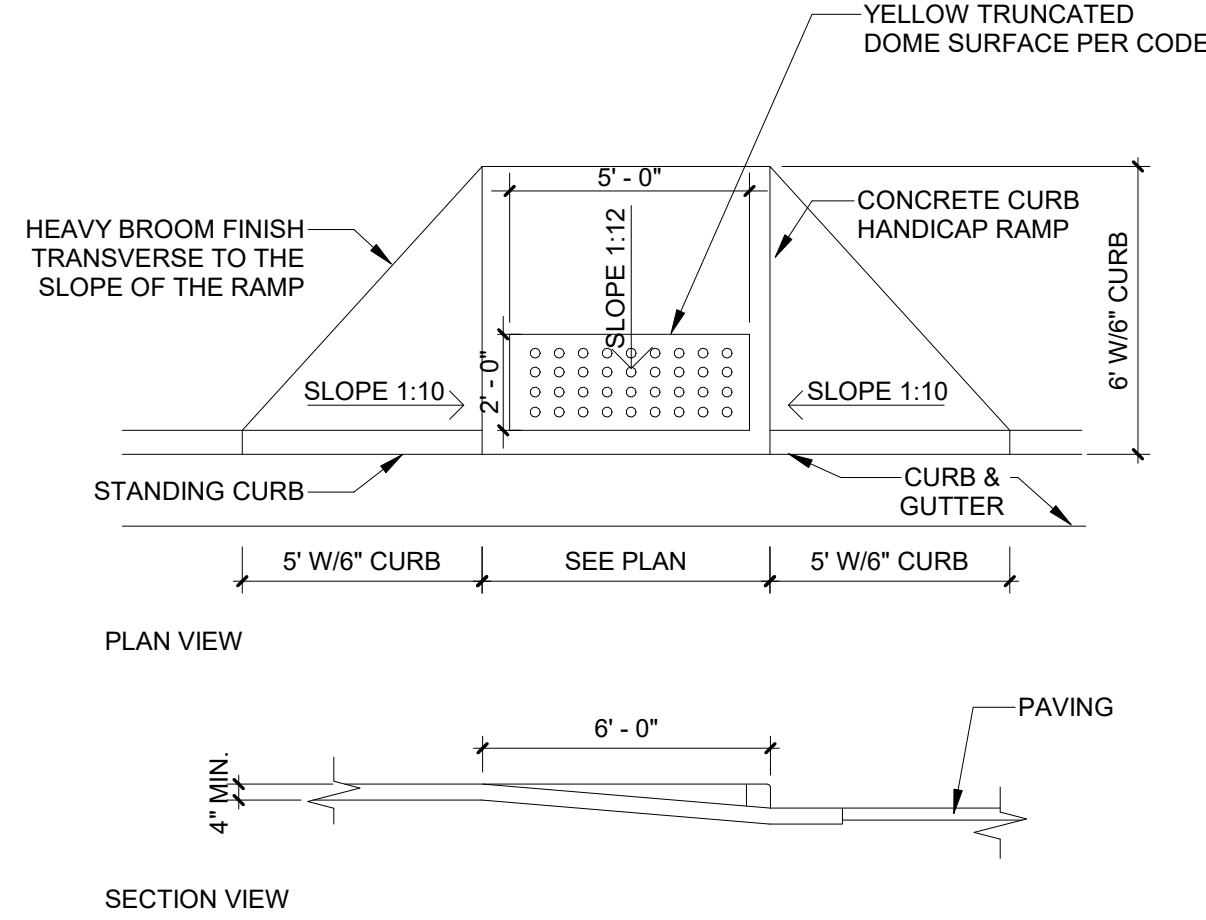
TCL-101

SHEET 1 OF 4

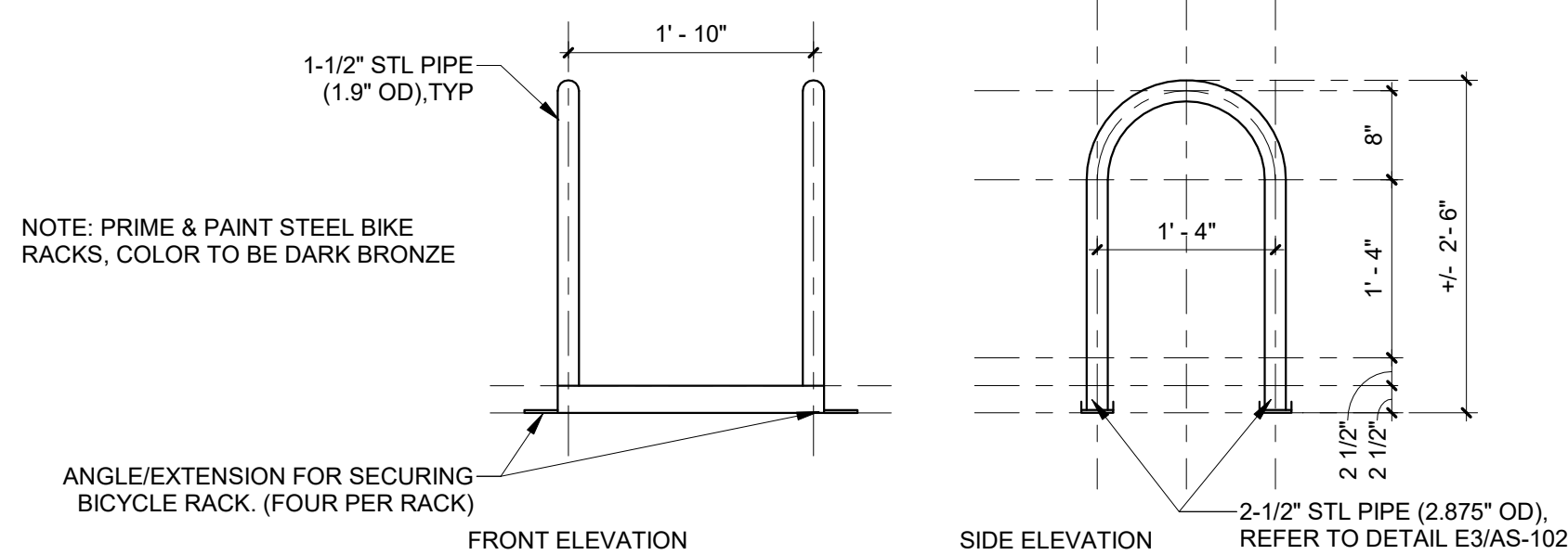
8/9/2017 12:52:34 PM C:\Users\mlee\Documents\REVIT LOCAL\15918 - Garcia Honda Gen III\15918 - Garcia Honda Gen III_NEW SERVICE BUILDING_2018_A_mlee.rvt



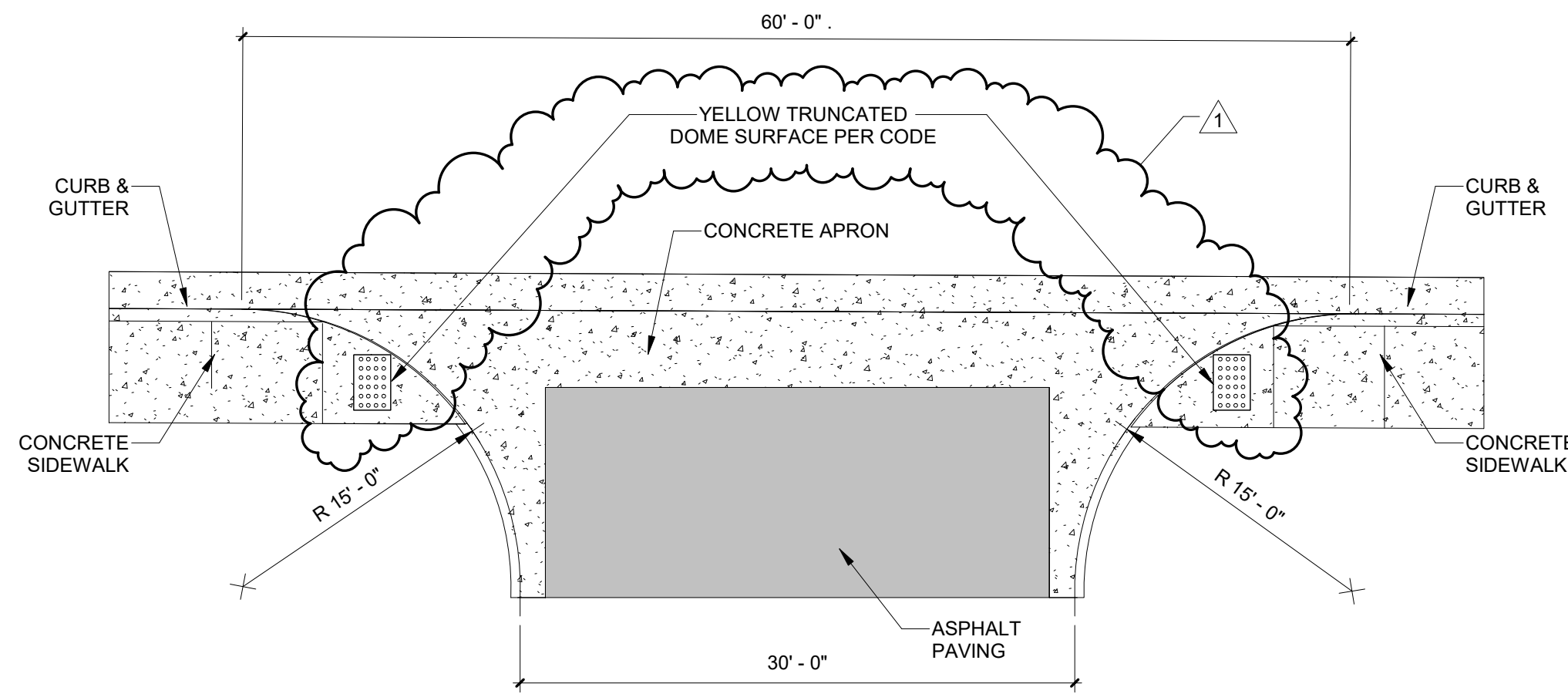
D1 CONCRETE CURB & GUTTER
Scale: 1 1/2" = 1'-0"



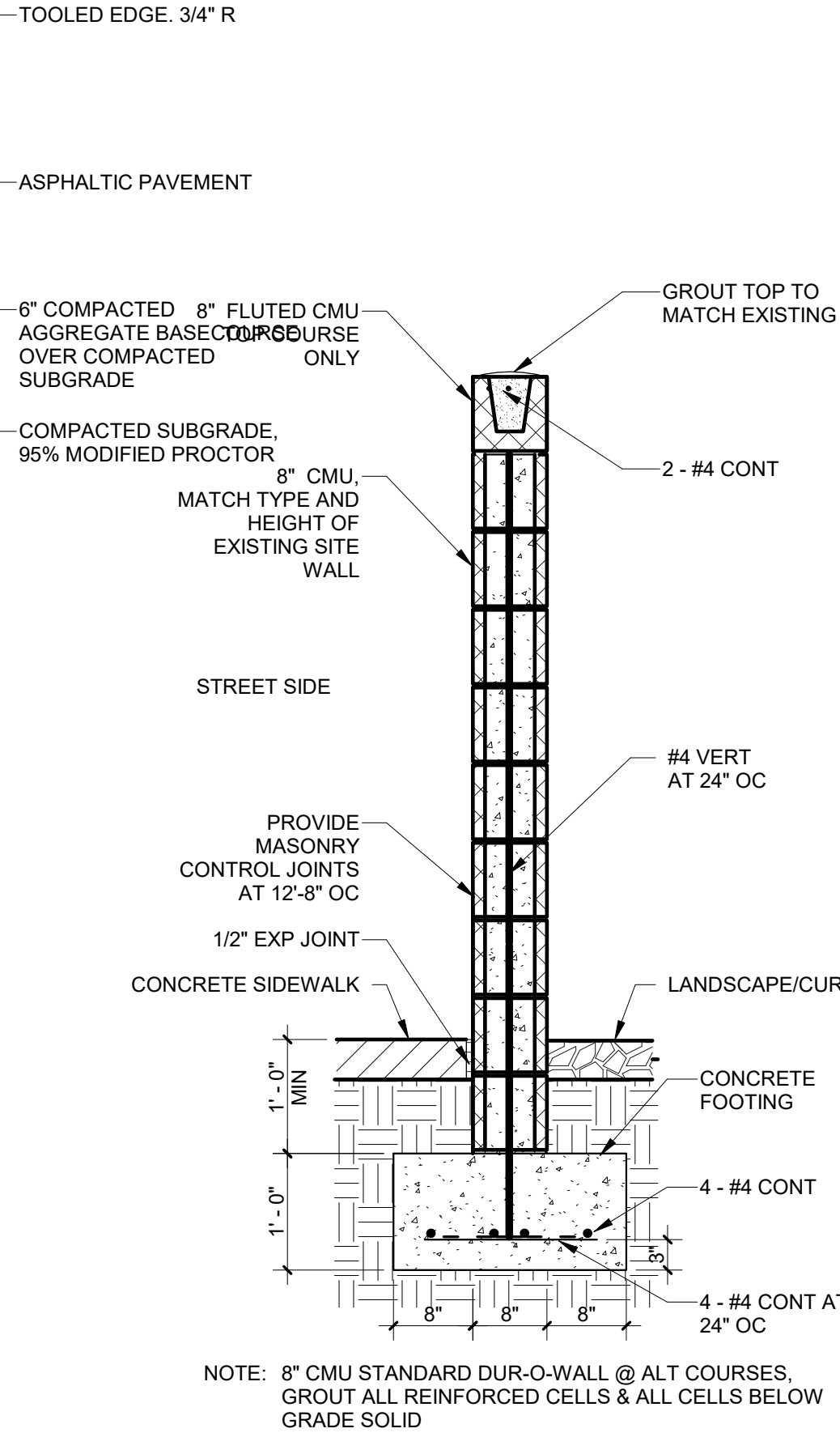
C1 HANDICAP RAMP, TYPICAL
Scale: 1/4" = 1'-0"



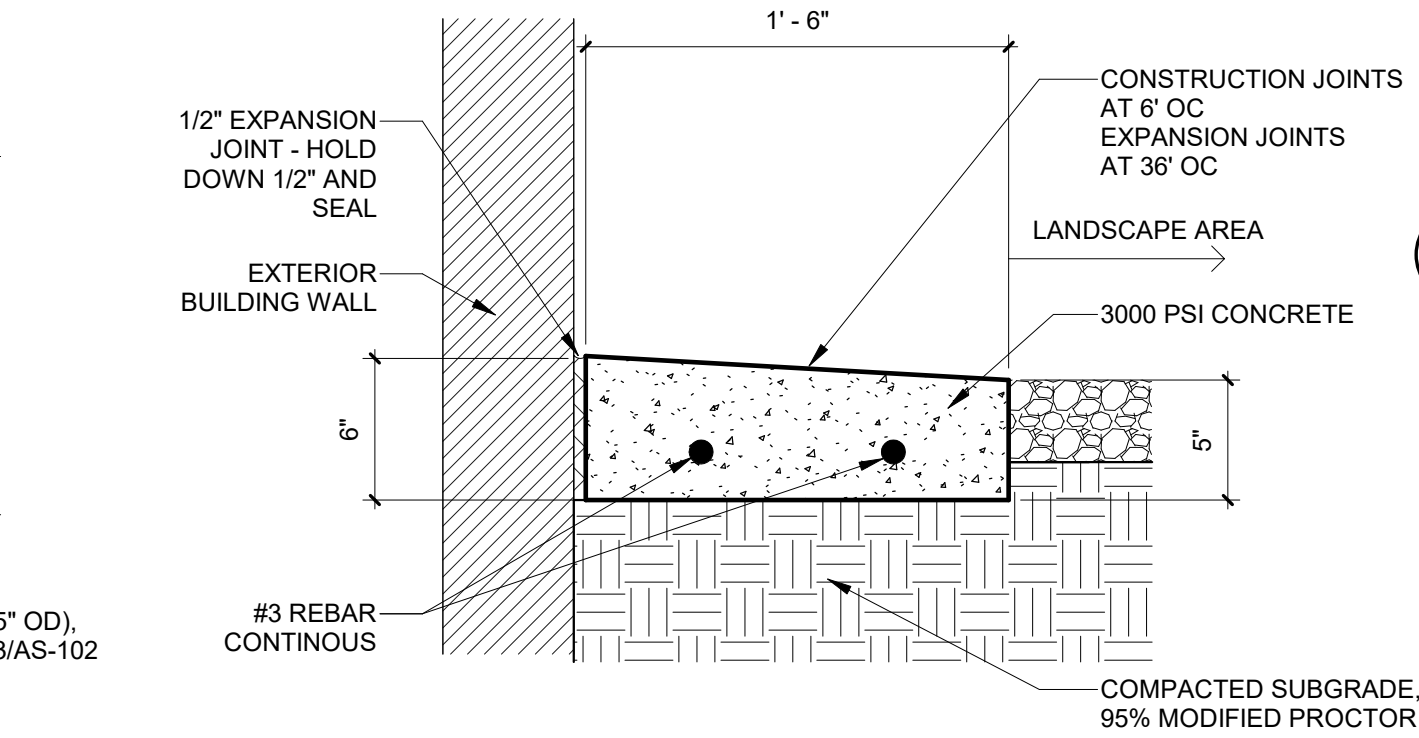
B1 BIKE RACK ELEVATIONS
Scale: 3/4" = 1'-0"



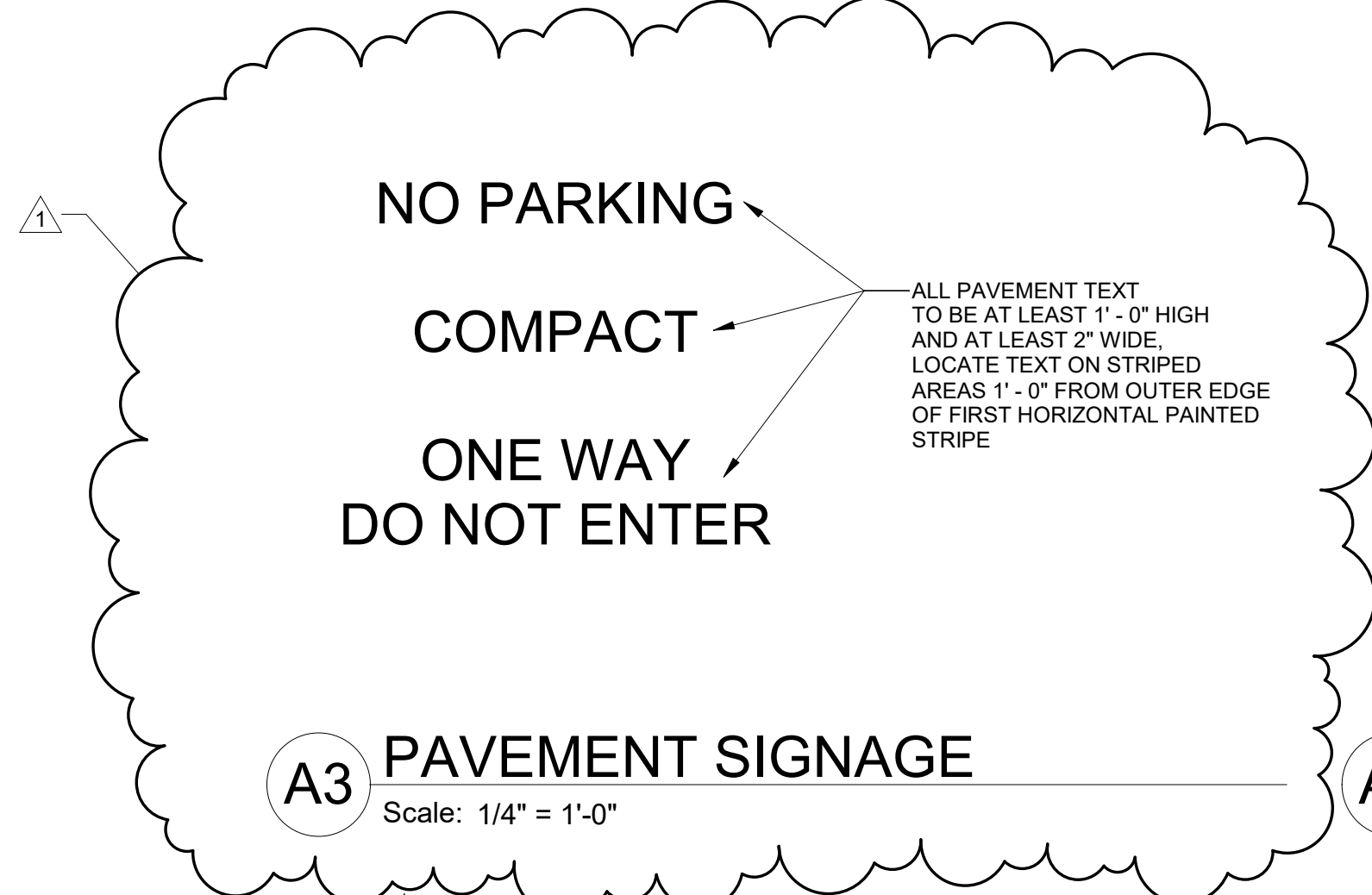
A1 NEW CURB CUT
Scale: 1/8" = 1'-0"



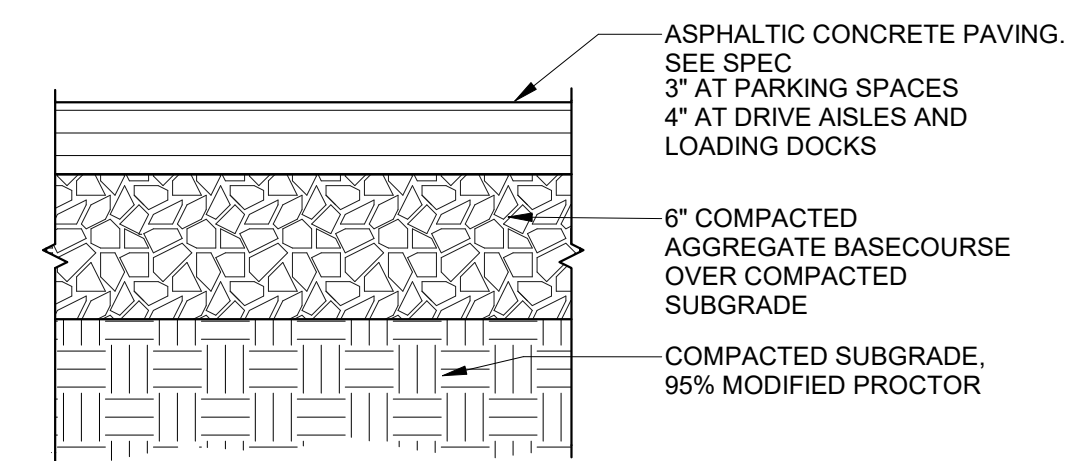
C3 SITE WALL, TYPICAL
Scale: 3/4" = 1'-0"



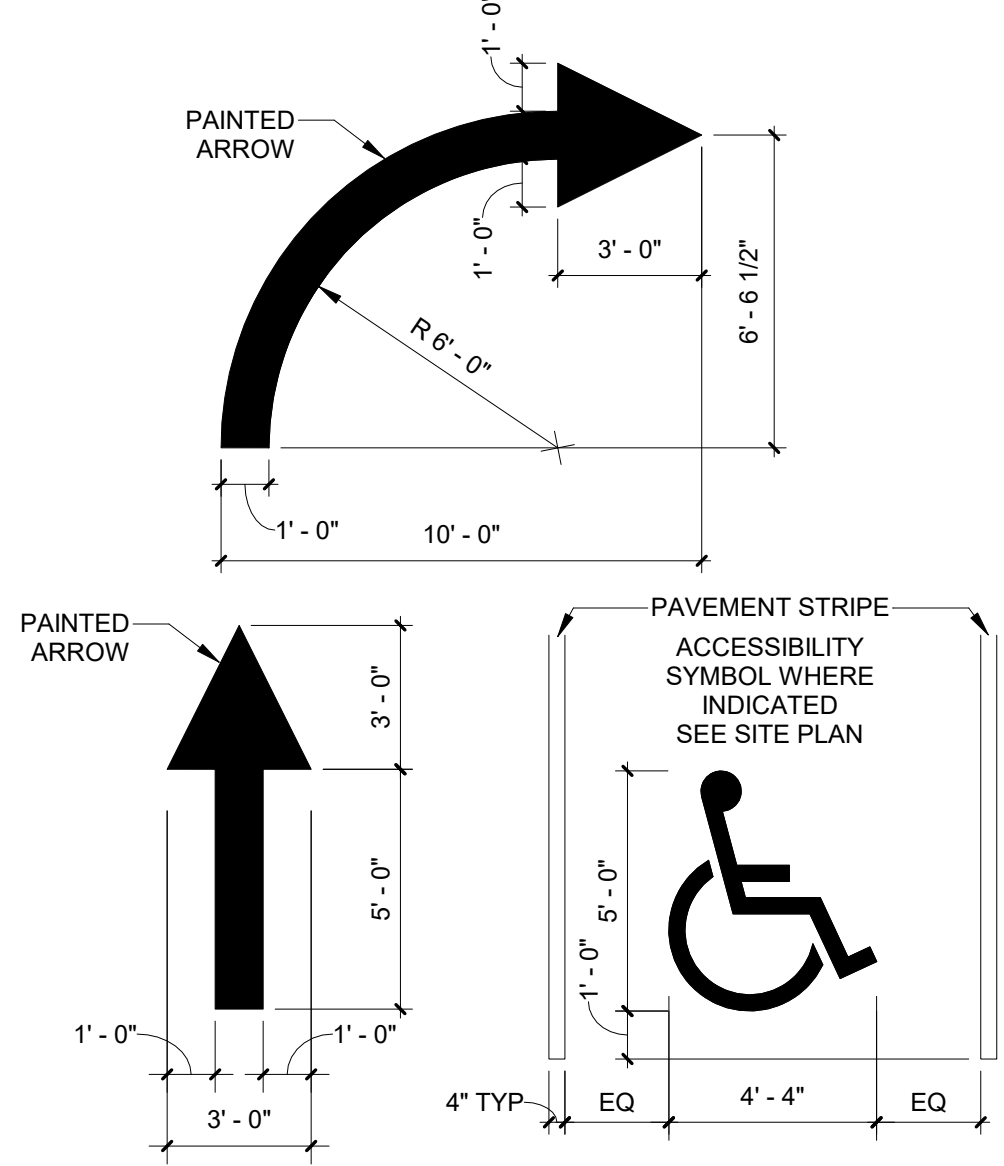
B3 CONCRETE MOW STRIP, TYP
Scale: 1 1/2" = 1'-0"



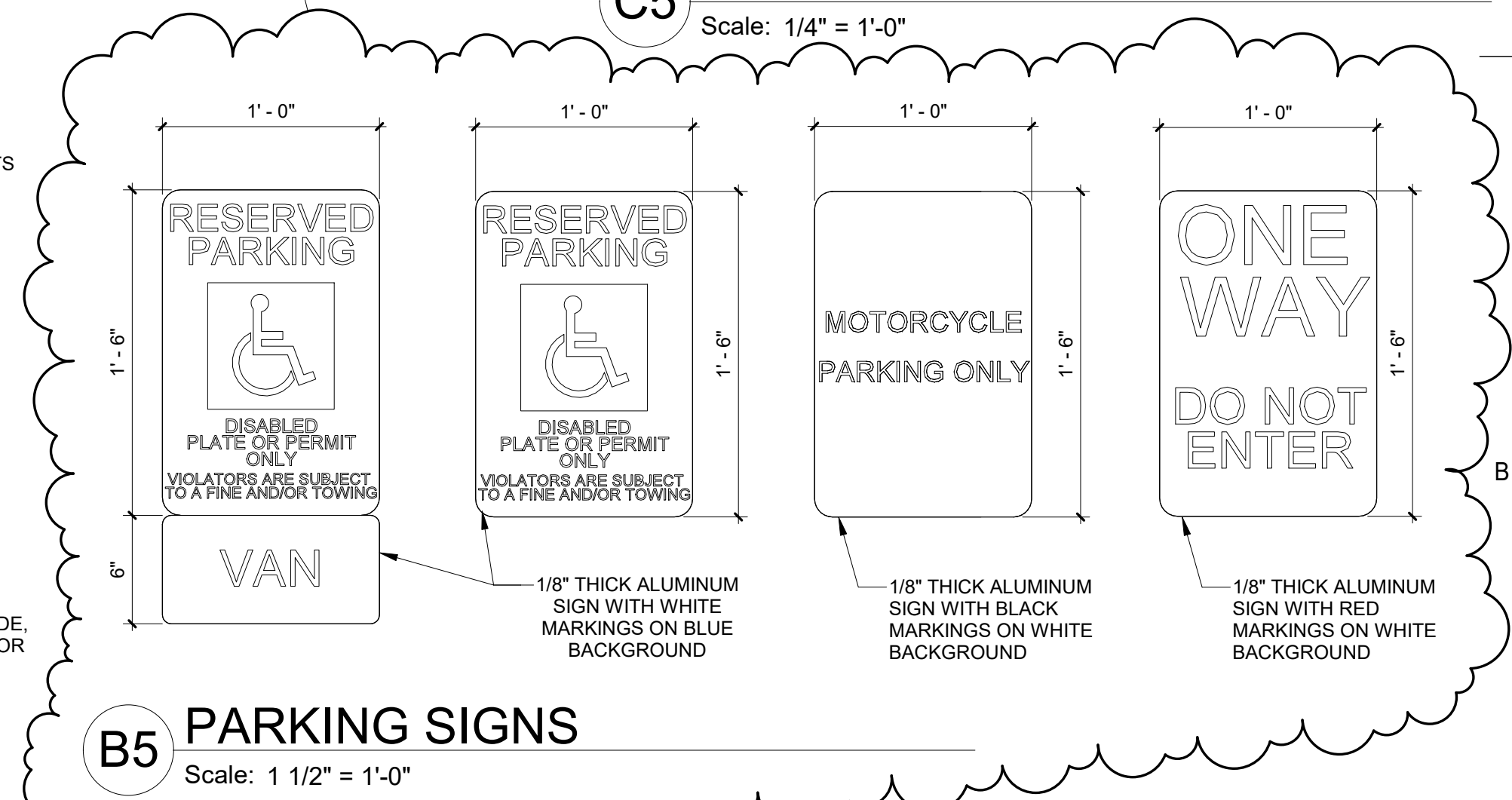
A3 PAVEMENT SIGNAGE
Scale: 1/4" = 1'-0"



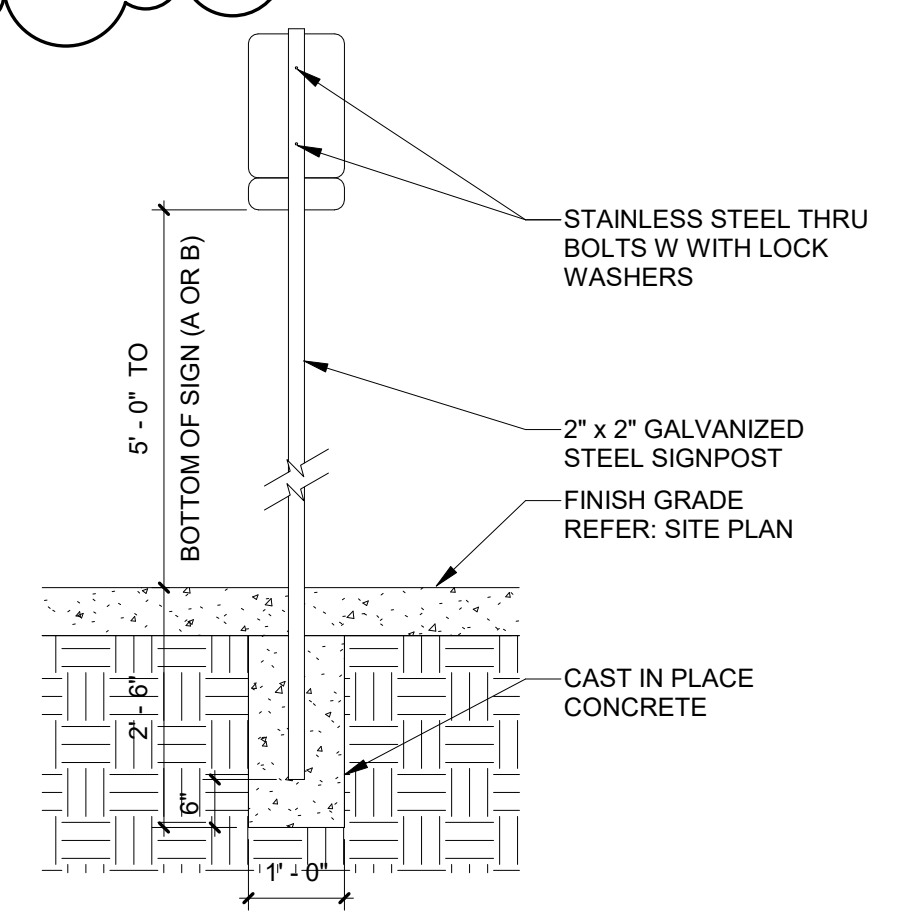
D5 ASPHALT PAVING, TYPICAL
Scale: 1 1/2" = 1'-0"



C5 PAVEMENT MARKINGS
Scale: 1/4" = 1'-0"



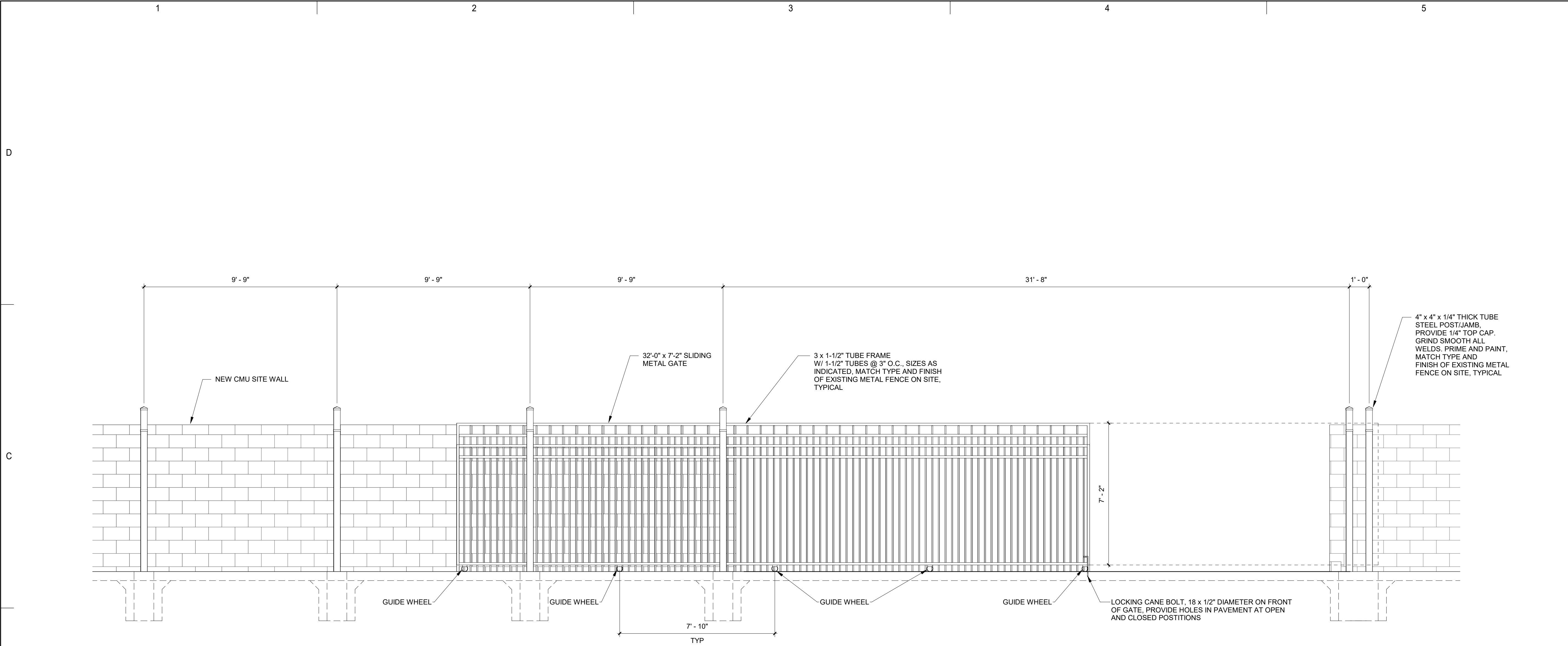
B5 PARKING SIGNS
Scale: 1 1/2" = 1'-0"



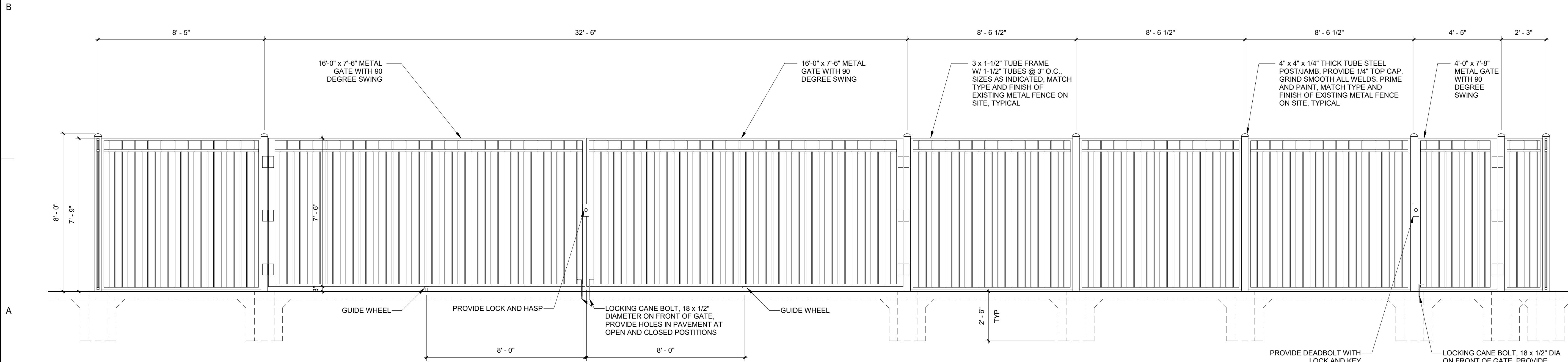
A5 PARKING SIGNAGE FOR A OR B
Scale: 1/2" = 1'-0"

1	08/10/17	TCL COMMENTS
Mark	Date	Description
Date		AUGUST, 2017
Project Number		15918
Drawn By		MOL
Checked By		KD
Copyright ©		VAN H. GILBERT ARCHITECT PC

C:\Users\mllee\Documents\REVIT LOCAL\15918 Garcia Honda Gen III\15918 - GARCIA HONDA GEN III_NEW SERVICE BUILDING_2018_A_mlee.rvt 8/9/2017 12:52:34 PM



B1 NEW SLIDING METAL GATE
Scale: 3/8" = 1'-0"



A1 NEW METAL FENCE WITH GATE
Scale: 3/8" = 1'-0"



2428 Baylor Dr SE Albuquerque, NM 87106
Tel 505-247-9955 Fax 505-247-1826
E-mail info@vhgarchitect.com
Web Site www.vhgarchitect.com

CONSULTANTS

PROJECT

GARCIA AUTOMOTIVE GROUP

GARCIA HONDA -
GENERATION 3 - NEW
SERVICE BUILDING

ADDRESS

8301 LOMAS BLVD,
ALBUQUERQUE, NM
87110

PERMIT SET

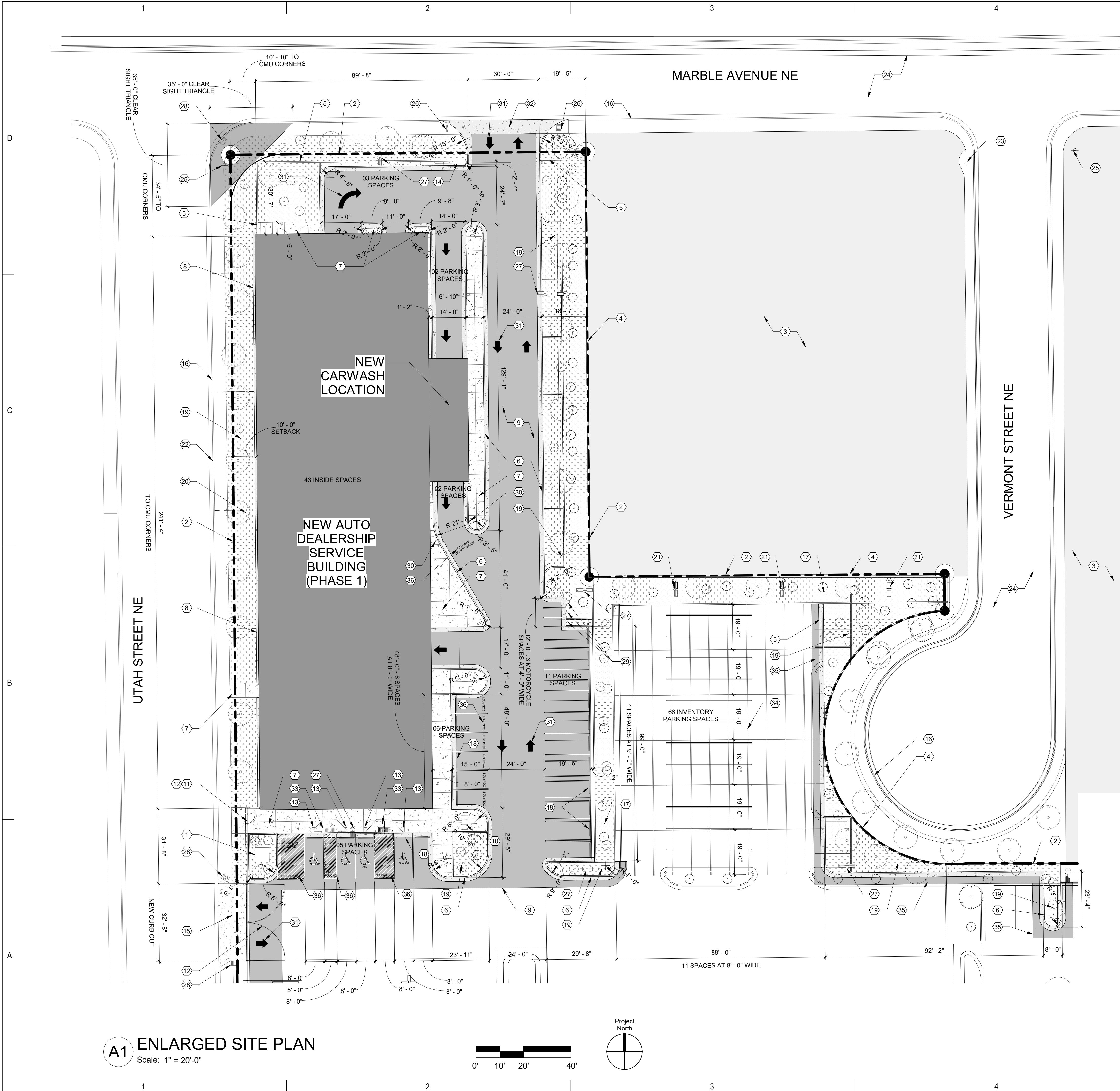


Mark	Date	Description
	AUGUST, 2017	
Project Number	15918	
Drawn By	MOL	
Checked By	KD	
Copyright ©	VAN H. GILBERT ARCHITECT PC	

SHEET TITLE

FENCE AND GATE
ELEVATIONS

C:\Users\mllee\Documents\REVIT LOCAL\15918 Garcia Honda Gen III\15918 - GARCIA HONDA GEN III_NEW SERVICE BUILDING_2018_A_mlee.rvt 8/9/2017 12:52:41 PM



A1 ENLARGED SITE PLAN
Scale: 1" = 20'-0"

SHEET KEYNOTES - TCL-104

- 1 NEW TRANSFORMER
- 2 PROPERTY LINE
- 3 EXISTING RESIDENTIAL PROPERTIES
- 4 EXISTING SITE WALL, TYPICAL
- 5 NEW SITE WALL, MATCH EXISTING IN SIZE, TYPE, AND FINISH, REFER: C3/TCL-102
- 6 NEW 6" CONCRETE CURB AND GUTTER, TYPICAL; REFER: D1/TCL-102
- 7 NEW 6" THICK CONCRETE SIDEWALK
- 8 NEW CONCRETE MOW STRIP, REFER: B3/TCL-102
- 9 NEW ASPHALT PAVING, TYPICAL; REFER: D5/TCL-102
- 10 NEW BICYCLE RACK, REFER: B1/TCL-102
- 11 NEW METAL FENCE, REFER: A1/TCL-103
- 12 NEW METAL GATE, REFER: A1/TCL-103
- 13 NEW ADA PARKING SIGNAGE, REFER: A5 AND B5/TCL-102
- 14 NEW SLIDING METAL GATE, REFER: B1/TCL-103
- 15 NEW CURB CUT TO EXTENT AND DIMENSIONS SHOWN
- 16 EXISTING SIDEWALK, CURB, AND GUTTER, TYPICAL
- 17 EXISTING LANDSCAPE
- 18 WHEEL STOP, TYPICAL
- 19 NEW LANDSCAPING, TYPICAL
- 20 10' - 0" SETBACK
- 21 EXISTING LIGHT POLE
- 22 EXISTING CURB CUTS TO BE DEMOLISHED, FILL IN WITH NEW GUTTER, CURB, AND SIDEWALK TO MATCH EXISTING
- 23 EXISTING FIRE HYDRANT
- 24 EXISTING PUBLIC STREET
- 25 EXISTING STOP SIGN / STREET SIGN
- 26 NEW 24" x 48" TRUNCATED DOME MAT AND ADA RAMP
- 27 NEW LIGHT POLE
- 28 NEW 24" x 48" TRUNCATED DOME MAT, REPLACE RAMP TO CABO STANDARDS IF EXISTING RAMP IS NOT UP TO CODE
- 29 NEW MOTORCYCLE SIGNAGE, REFER: A5 AND B5/TCL-102
- 30 NEW ONE WAY SIGNAGE, REFER: A5 AND B5/TCL-102
- 31 NEW PAVEMENT MARKINGS, TYPICAL; REFER: C5/TCL-102
- 32 NEW CURB CUT ENTRANCE, REFER: A1/TCL-102
- 33 HC RAMP, REFER: C1/TCL-102
- 34 NEW PARKING SPACE STRIPING, TYPICAL
- 35 ASPHALT PATCH AND REPAIR AT CURB/GUTTER, TYPICAL
- 36 PAVEMENT SIGNAGE, TYPICAL; REFER: A3/TCL-102

Van H. Gilbert Architect PC
ARCHITECTURE • INTERIORS • PLANNING



2428 Baylor Dr SE Albuquerque, NM 87106
Tel 505-247-9955 Fax 505-247-1826
E-mail info@vhgarchitect.com
Web Site www.vhgarchitect.com

CONSULTANTS

PROJECT

GARCIA AUTOMOTIVE GROUP

**GARCIA HONDA -
GENERATION 3 - NEW
SERVICE BUILDING**

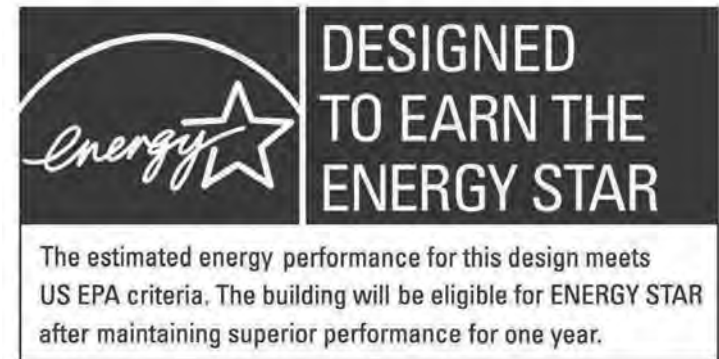
ADDRESS

**8301 LOMAS BLVD,
ALBUQUERQUE, NM
87110**

PERMIT SET



08/10/17



1	08/10/17	TCL COMMENTS
Mark	Date	Description
Date		AUGUST, 2017
Project Number		15918
Drawn By		MOL
Checked By		KD, MB
Copyright ©		VAN H. GILBERT ARCHITECT PC

SHEET TITLE
ENLARGED PLAN

TCL-104

SHEET 4 OF 4