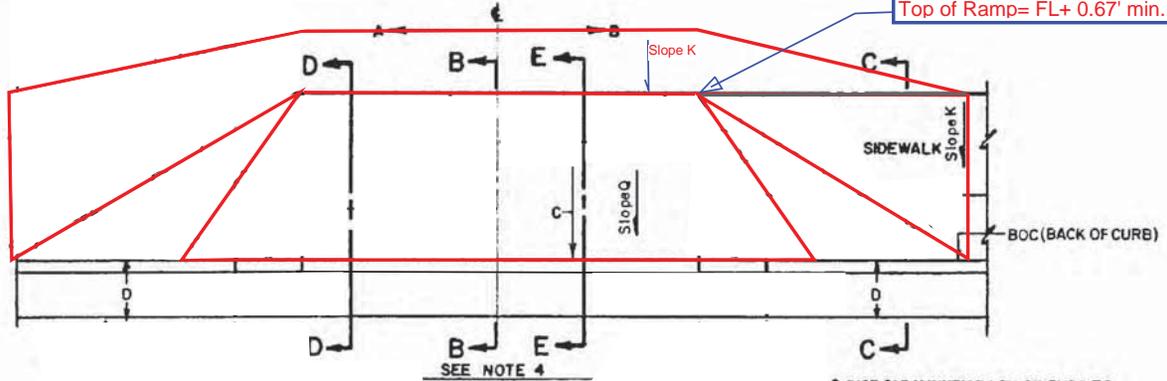


CHURCH's #695
- 5407 Central NW -

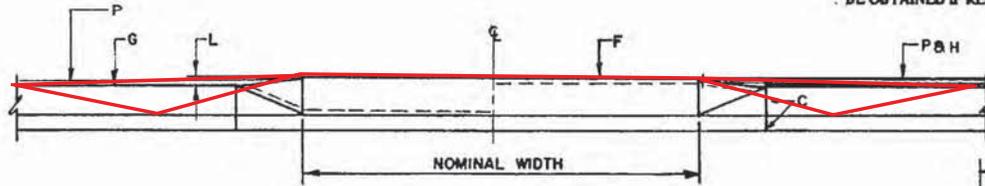
- Possible alternative driveway entrance, with flared ramp layout, and sidewalk pushed back into an easement on the property.

- You may be able to pull it all out into the R/W if R/W depth is adequate.

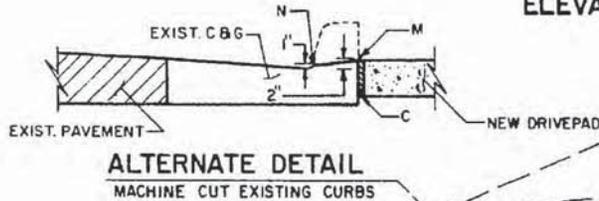


PLAN

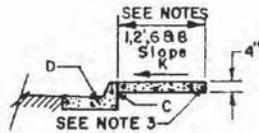
* (USE ONLY WHEN BACK OF CURB TO PROPERTY LINE IS 9' OR MORE. DRIVEPADS WITH LESS THAN 9'-0" IN DEPTH REQUIRE A SPECIAL DESIGN TO BE SUBMITTED TO AND APPROVED BY THE CITY ENGINEER. ADDITIONAL RIGHT-OF-WAY OR EASEMENTS SHALL BE OBTAINED IF REQUIRED.)



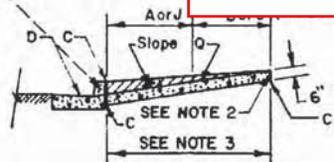
ELEVATION



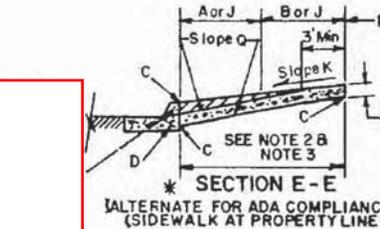
ALTERNATE DETAIL
MACHINE CUT EXISTING CURBS



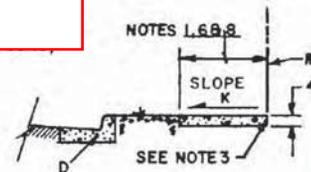
SECTION A-A



SECTION B-B
(ADA COMPLIANCE NOT REQUIRED OR POSSIBLE WO/ADDITIONAL ROW ACQUIS)



SECTION E-E
(ALTERNATE FOR ADA COMPLIANCE) (SIDEWALK AT PROPERTY LINE)



SECTION C-C

GENERAL NOTES

1. DEVIATIONS FROM THESE STANDARDS SHALL BE SUBMITTED TO THE CITY ENGINEER AND/OR TRAFFIC ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
2. REQUEST FOR SIDEWALK VARIANCES SHALL BE SUBMITTED TO THE DEVELOPMENT REVIEW BOARD.
3. USE 1/2" EXP. JT. WHERE SIDEWALK OR DRIVEPAD ABUTS BLDGS., FENCES, WALLS OR OTHER IMMOVABLE OBJECTS.
4. ALL DRIVEPADS SHALL BE A MIN. THICKNESS OF 6" AND SHALL BE CONSTRUCTED FROM BACK OF CURB TO P/L.
5. DRIVEPADS WIDER THAN 18' (NOMINAL) SHALL HAVE A 1/2" EXP. JT AT MIDPOINT. DRIVEPADS WIDER THAN 36' SHALL HAVE 2 OR MORE 1/2" EXP. JTS. EQUALLY SPACED, MAX., SPACING IS 18' APART.
6. SIDEWALK AT THE BACK OF CURB SHALL BE USED ONLY WHEN VARIANCE IS APPROVED.
7. FOR SIDEWALK WIDTH, SEE CHAPTER 23 THE DEVELOPMENT PROCESS MANUAL.
8. SUBGRADE UNDER SIDEWALK & DRIVEPAD SHALL BE COMPACTED AS PER SECTION 301.
9. ADA - AMERICANS WITH DISABILITIES ACT.

CONSTRUCTION NOTES

- A. CURB TYPE SIDEWALK.
- B. OFFSET SIDEWALK.
- C. 1/2' EXPANSION JOINT ADJACENT TO FIELD CONDITIONS ON REPLACEMENT WORK.
- D. CURB AND GUTTER.
- E. SLOPE TO BE ADJUSTED TO PROVIDE A UNIFORM TRANSITION BETWEEN SIDEWALK AND DRIVEPAD. (NOT TO EXCEED 1(VERTICAL) TO 10(HORIZONTAL)).
- F. TOP OF DRIVEPAD.
- G. TOP OF CURB.
- H. PROPERTY LINE.
- J. VARIABLE WIDTH.
- K. SLOPE 1(VERTICAL) TO 50(HORIZONTAL).
- L. THE FINISH GRADE ELEVATION DIFFERENCE BETWEEN TOP OF DRIVEPAD AT PROPERTY LINE AND TOP OF CURB AS DETERMINED BY A SLOPE OF 1(VERTICAL) TO 50(HORIZONTAL) FROM TOP OF CURB TO TOP OF DRIVEPAD, AND ANY DEVIATION FROM THIS SLOPE MUST BE APPROVED BY THE CITY ENGINEER.
- M. SAW CUT EXISTING CONCRETE FROM BACKSIDE OF CURB WITH SLOPE TOWARD FLOWLINE.
- N. EXPOSED CUT EDGES SHALL BE GROUND SMOOTH/ ROUNDED TO REMOVE SHARP EDGE.
- P. OUTSIDE EDGE OF SIDEWALK.
- Q. SLOPE REQUIRED TO MEET GIVEN OR SET BOUNDARY ELEVATIONS (PROPERTY LINE OR BOC, ETC.).

CITY OF ALBUQUERQUE

PAVING
DRIVEPADS

DWG. 2425

AUG. 1986

REVISIONS
1/91
11/14/91
3/30/94