

CITY OF ALBUQUERQUE

Planning Department
Brennon Williams, Director



Mayor Timothy M. Keller

June 1, 2022

Shannon VanDusen, RA
Scout Architecture+ Design
42154 Avenida La Resolana NE
Albuquerque, NM 87xxx

Re: Caterpillar Clubhouse
803 2nd St. SW
Traffic Circulation Layout
Architect Stamp 04-11-22 (K14-D227)

Dear Ms. VanDusen,

The TCL submittal received 05-09-2022 is approved for Building Permit. ***This is approved on the condition that the shared parking agreement approved by Code Enforcement is signed and recorded.*** A copy of the stamped and signed plan will be needed for each of the building permit plans. Please keep the original to be used for certification of the site for final C.O. for Transportation.

PO Box 1293

Albuquerque

NM 87103

www.cabq.gov

When the site construction is completed and an inspection for Certificate of Occupancy (C.O.) is requested, use the original City stamped approved TCL for certification. Redline any minor changes and adjustments that were made in the field. A NM registered architect or engineer must stamp, sign, and date the certification TCL along with indicating that the development was built in "substantial compliance" with the TCL. Submit this certification, the TCL, and a completed Drainage and Transportation Information Sheet to front counter personnel for log in and evaluation by Transportation.

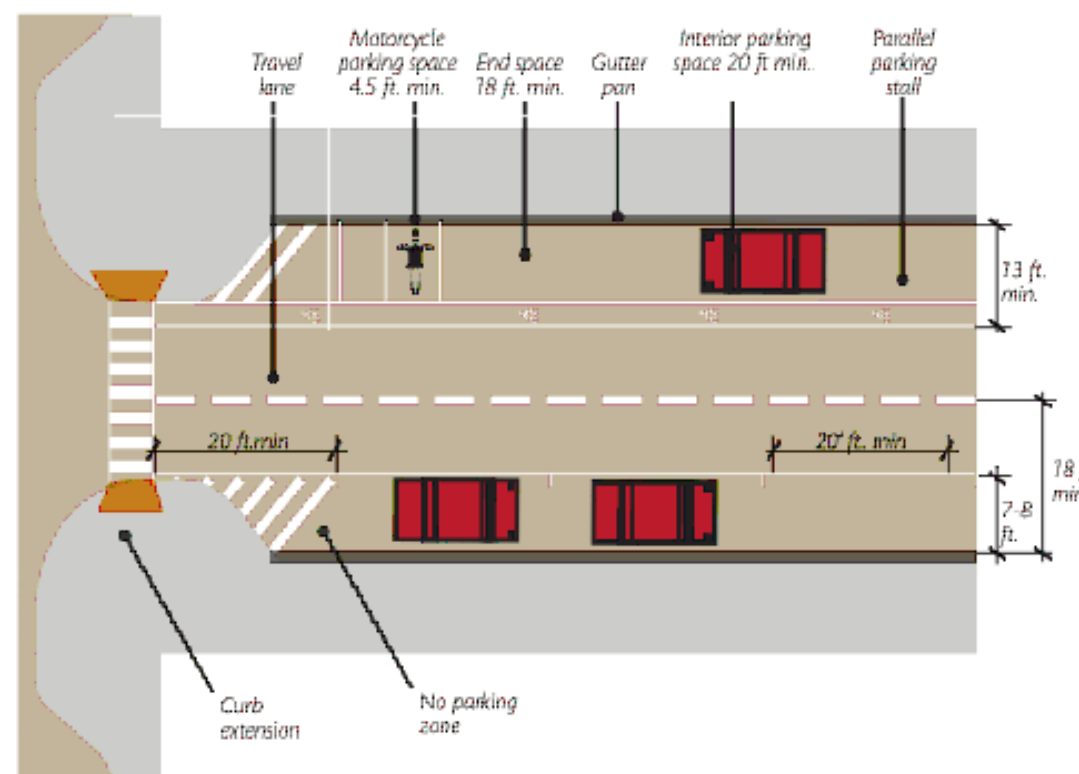
Once verification of certification is completed and approved, notification will be made to Building Safety to issue Final C.O. To confirm that a final C.O. has been issued, call Building Safety at 924-3690.

Sincerely,

Jeanne Wolfenbarger, P.E.
Traffic Engineer, Planning Dept.
Development Review Services

\xxx via: email
C: CO Clerk, File

FIGURE 7.4.88 Parallel Parking Dimensions



7-4(H)(2)(ii) Parallel Parking Design Guidance

- The required width of on-street parallel parking stalls is 7 feet to 8.5 feet, with wider stalls recommended on commercial streets with higher levels of parking turnover and on streets with speeds greater than 25 MPH. (See [TABLE 7.4.61](#))

Speed Limit	Stall Width
25 MPH	7 ft.
30-35 MPH	8 ft.

- The gutter pan may be used as part of the stall width.
- Where parallel on-street parking is adjacent to a bicycle lane (and there is insufficient space for reverse angle parking), the minimum combined width for the bicycle lane and the parallel parking stall is 13 feet (with a recommended 7 foot wide parallel parking stall and a 6 foot wide bicycle lane).
- The combined width of a parallel on-street parking stall and the adjacent travel lane should be a minimum of 18 feet.
- A 1.5 foot shy zone space or offset shall be provided between the curb edge and any vertical elements in the landscape/buffer zone.
- Individual stalls may be marked to increase the parking capacity. The minimum stall length is 20 feet for interior spaces and 18 feet for end spaces. (See [FIGURE 7.4.88](#))
- Per [MUTCD](#), there shall be a 20-foot long space between the crosswalk or pedestrian crossing and the nearest on-street parking space. A curb extension may be used within that 20 foot area.

TABULATED PARKING COUNTS

PROPOSED DAY CARE IS 3,594 SQUARE FEET.

PER TABLE 5-5-1 (IDO M9INIMUM PARKING REQUIREMENTS)
DAY CARE IN MAIN STREET REQUIRES 1 SPACE PER 600 S.F. GFA.
SITE IS WITHIN THE 4TH STREET MAIN STREET OVERLAY.

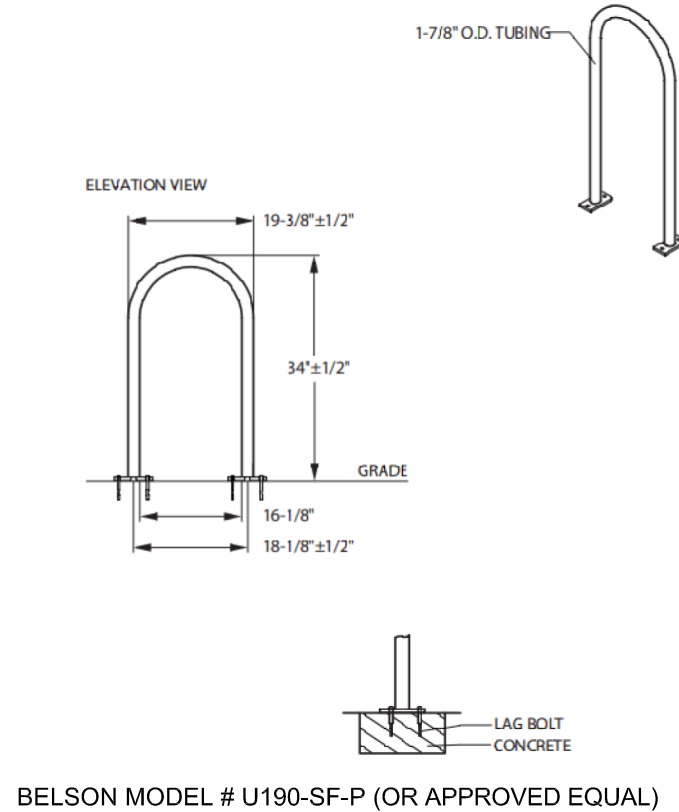
THIS REQUIRES 6 PARKING SPACES (INCLUDING 1 ADA).
PROVIDED PARKING IS 5 ON ON-STREET PARKING SPACES ADJACENT TO THE PROJECT SITE AND 1 UNDER A SHARED PARKING AGREEMENT.

1 MOTORCYCLE SPACE IS REQUIRED PER TABLE 5-5-4.
1 SPACE PROVIDED.

BICYCLE PARKING PER IDO TABLE 5-5-5
MINIMUM REQUIRED 3 SPACES
PROVIDED 2 RACKS *4 BICYCLE SPACES

GENERAL NOTES:

ALL BROKEN OR CRACKED SIDEWALKS OR CURB AND GUTTER ALONG THE PROJECT PARCEL FRONTAGE, TO BE REMOVED AND REPLACED PER CITY SIDEWALK DETAIL 2430 OF STANDARD CURB AND GUTTER PER DETAIL 2415A.

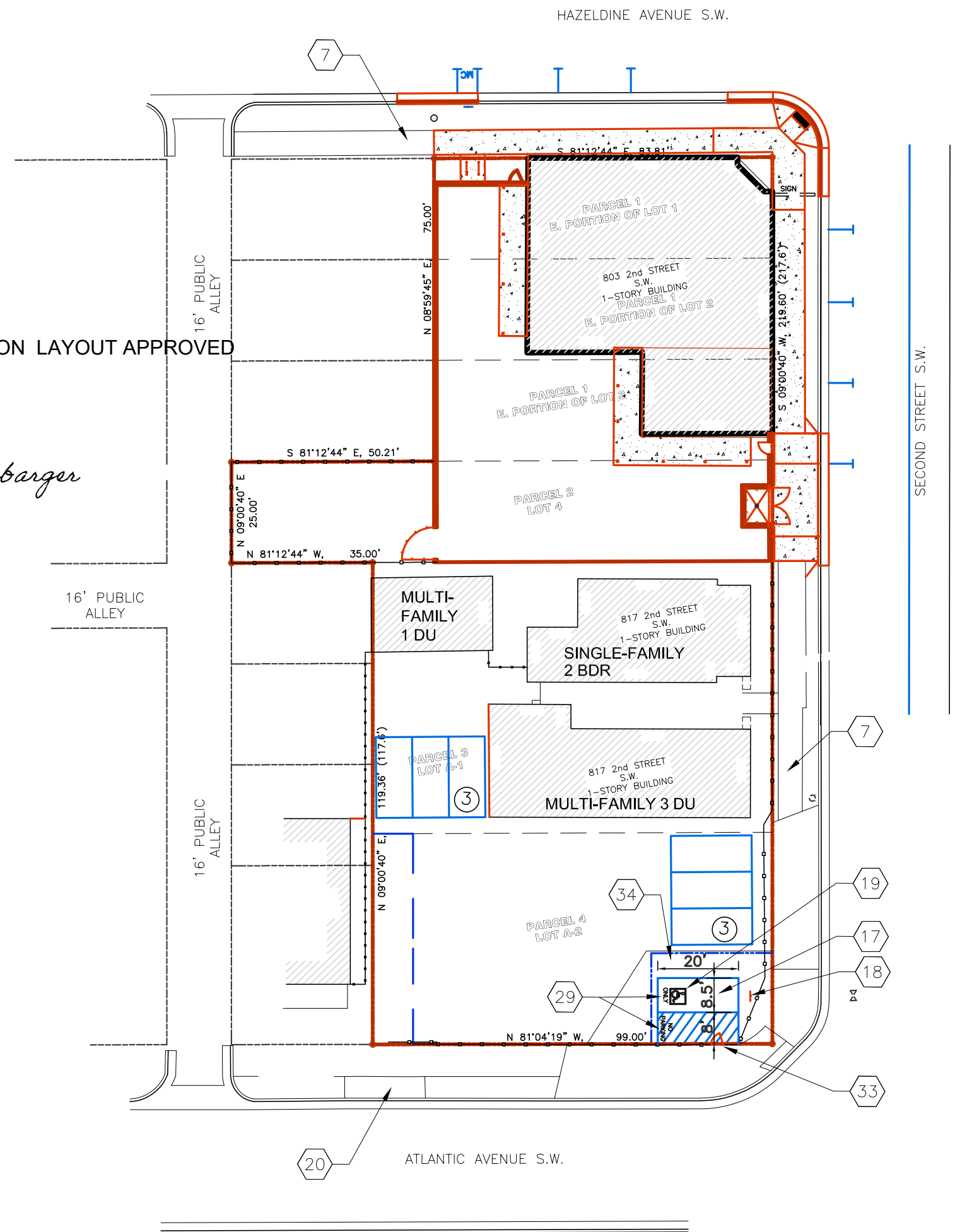


BELSON MODEL # U190-SF-P (OR APPROVED EQUAL)

E5 BICYCLE RACK

NOT TO SCALE

TRAFFIC CIRCULATION LAYOUT APPROVED
06/10/2022

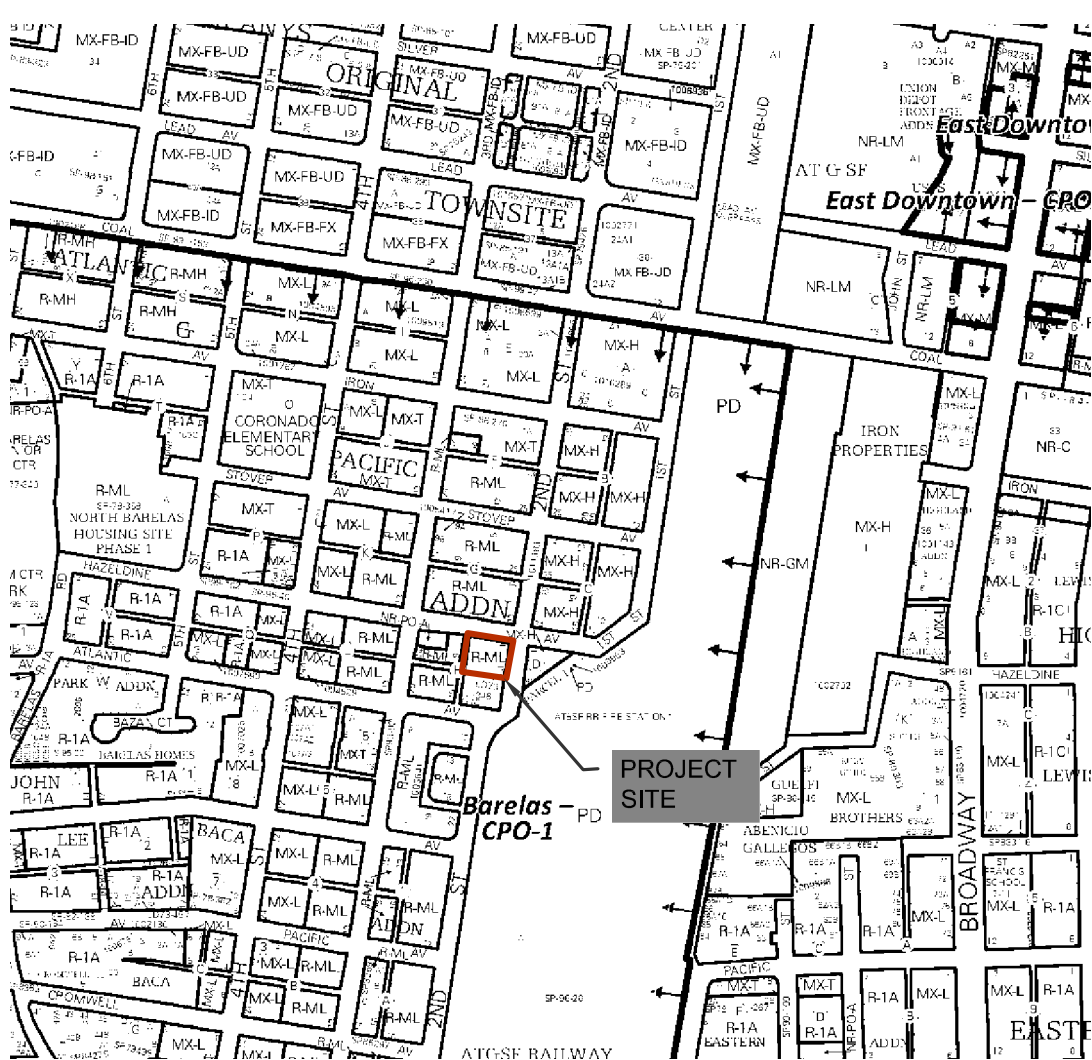
Jeanne Wolfenbarger


A4 OVERALL SITE LAYOUT

SCALE: 1" = 30'-0"

TABULATED PARKING COUNTS

LOT 3 AND 4 PARKING REQUIREMENTS.
PER TABLE 5-5-1 (IDO M9INIMUM PARKING REQUIREMENTS)
MULTI-FAMILY RESIDENTIAL USES (1/DU FOR UC/MS/PT)
SITE IS WITHIN THE 4TH STREET MAIN STREET OVERLAY.
SINGLE FAMILY 1SPACE FOR UP TO 2 BDR
TOTAL OF 6 SPACES REQUIRED (6 AVAILABLE PLUS ADA)

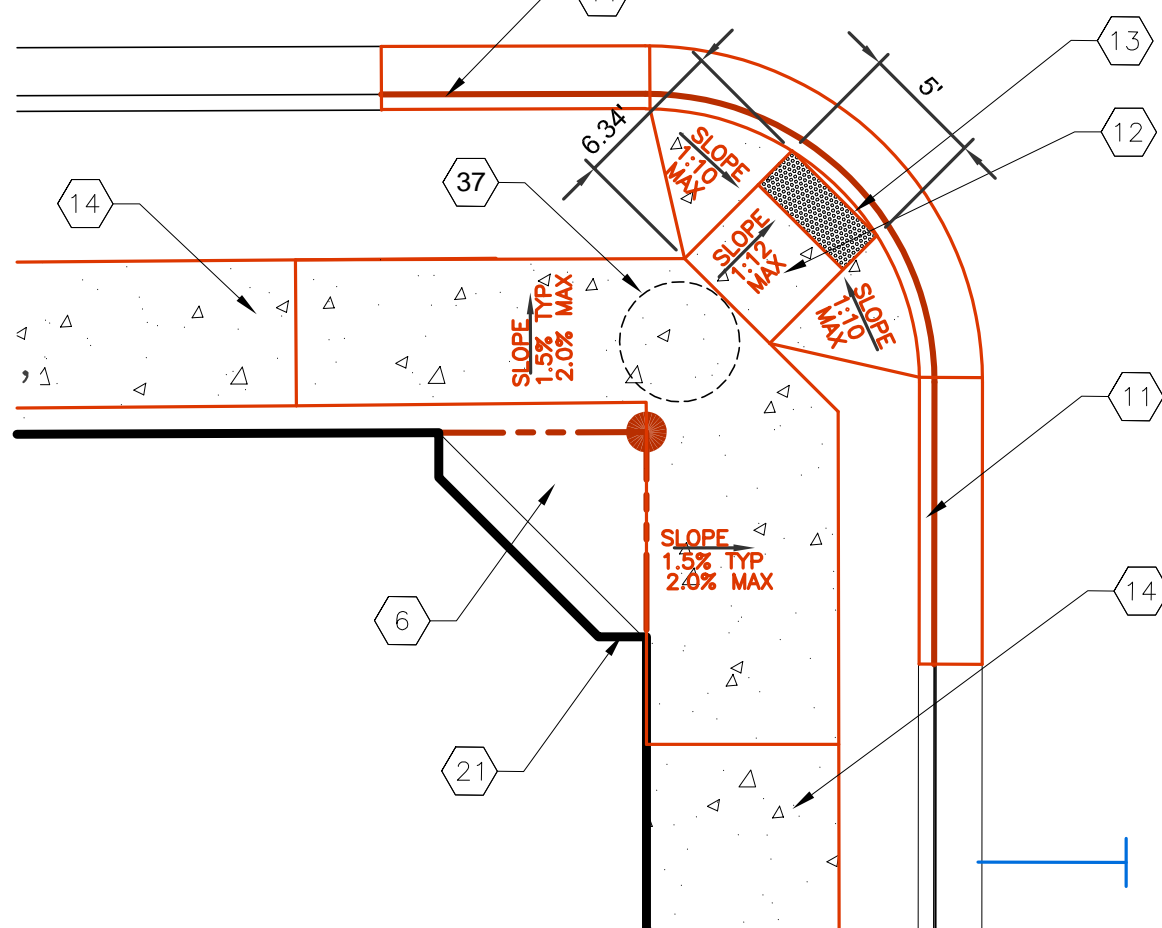


D3 ZONE ATLAS PAGE

NOT TO SCALE

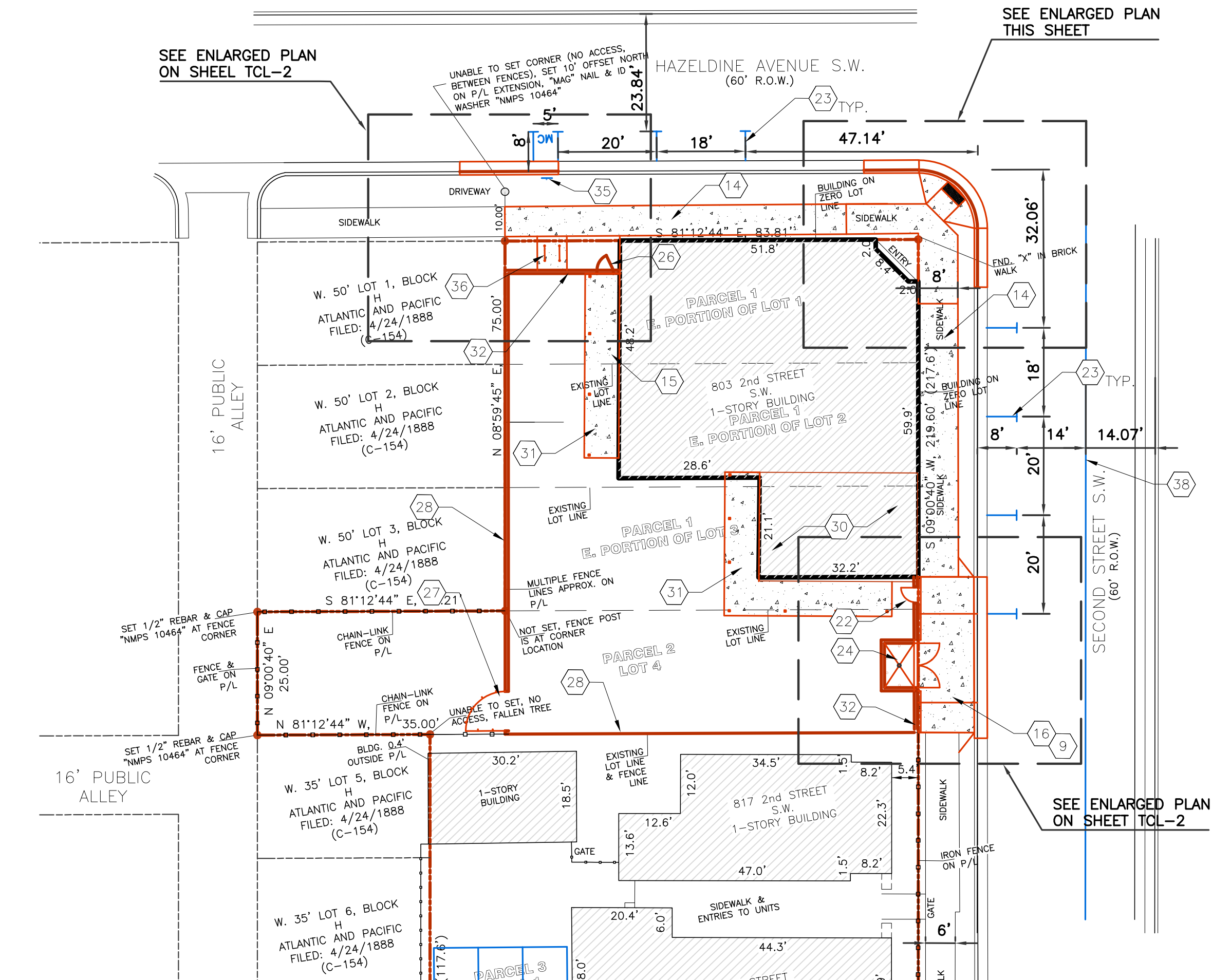
KEYED NOTES

- REMOVE AND DISPOSE OF CURB AND GUTTER.
- REMOVE AND DISPOSE OF CONCRETE CURB RAMP.
- REMOVE AND DISPOSE OF CONCRETE SIDEWALK OR DRIVEWAY.
- REMOVE AND DISPOSE OF PORTION OF CONCRETE DRIVEPAD.
- REMOVE AND SALVAGE FENCE FABRIC. REMOVE AND DISPOSE OF FENCE POSTS AND FOUNDATIONS.
- EXISTING BRICK PAVERS AT ENTRANCE TO REMAIN.
- EXISTING CONCRETE SIDEWALK TO REMAIN.
- EXISTING BUILDING TO BE REMODELED. REFER TO ARCHITECTURAL PLANS.
- CONCRETE DRIVEPAD PER COA STD DWGS 2425A AND 2425B SHAPED LIKE A PARALLEL CURB ACCESS RAMP PER 2443.
- CONCRETE HEADER CURB, 6" TALL, PER COA STD DWG 2415A.
- STANDARD CURB AND GUTTER PER COA STD DWG 2415A.
- CONCRETE CURB ACCESS RAMP PER COA STD DWGS 2440 AND 2446, SIDE WINGS TO BE 1:10 MAX SLOPE.
- DETECTABLE WARNING SURFACE PER COA STD DWGS 2440 AND 2441.
- CONCRETE SIDEWALK PER COA STD DWG 2430.
- 4" THICK CONCRETE PAVEMENT WITH 4X4XW1.4XW1.4 WELDED WIRE FABRIC. MEDIUM BROOM FINISH. CONTROL JOINTS AT 6' MAX SPACING.
- DRIVEPAD TO BE USED REFUSE COLLECTING ROLLING PATH. CONSTRUCT PER COA STD DWG 2443 WITH THICKENED PAVEMENT PER 2524A, WITH HEADER CURB ALONG PROPERTY LINE EXCEPT ALONG REFUSE ENCLOSURE.
- ADA RESERVED PARKING SPACE
- ADA VAN ACCESSIBLE RESERVED SIGN MOUNTED PER DETAIL D3/TCL-2.
- RESERVED PARKING SYMBOL PER DETAIL E1/TCL-2.
- EXISTING CONCRETE DRIVEWAY TO REMAIN.
- FIRE DEPARTMENT KNOX BOX.
- 6' TALL x 3' WIDE R PANEL PERSONNEL GATE. COLOR TO BE SELECTED BY ARCHITECT.
- 4" WIDE WHITE PARKING STRIPE SHOWN FOR PARKING TABULATION ONLY, NO STRIPING REQUIRED IN PUBLIC ROW.
- SWANSON 9'X6" REFUSE ENCLOSURE PAD PER DETAIL C2/TCL-2.
- REMOVE AND DISPOSE TO EXISTING ASPHALT DRIVEWAY.
- REMOVE AND DISPOSE OF EXISTING CHAIN LINK FENCE AND GATE.
- 3' WIDE 6' TALL R PANEL PERSONNEL SWING GATE.
- 6' TALL CONCRETE MASONRY UNIT SCREEN WALL.
- 12" TALL PAINTED LETTERS.
- PORTION OF BUILDING TO BE REMOVED AND REPLACED DUE TO STRUCTURAL CONCERNS.
- CONCRETE PATIO WITH 8'X8" TURN DOWN EDGE. SIDEWALK SIMILAR TO COA STD DWG 2430.
- 3' TALL CONCRETE MASONRY UNIT SCREEN WALL WITH R PANEL EXTENSION UP TO 6' TALL.
- NEW 3' WIDE (CLEAR OPENING) WROUGHT IRON GATE INSTALLING INTO EXISTING FENCING. MATCH EXISTING WROUGHT IRON HEIGHT, PICKET SIZE AND SPACING
- EXISTING CONCRETE PAD WILL BE USED FOR NEW VAN ACCESSIBLE ADA STALL, INCLUDING PARKING BLOCK, RESERVED PARKING SYMBOL, AND SIGNAGE. PROVIDE 8' WIDE UNLOADING ZONE ON SOUTH SIDE. PROVIDE STRIPING TO INDICATED NO PARKING ON UNLOADING SIDE OF ADA STALL.
- MOTORCYCLE PARKING SPACE 5'X8' WITH WHITE 12" TALL "MC" LETTER ON THE PAVEMENT AND "MOTOR-CYCLE PARKING ONLY" SIGN PER DETAILS A2/TCL-2 AND B2/TCL-2.
- TWO BICYCLE RACKS ON 4" THICK CONCRETE PAD PER DETAIL E5/TCL-1.
- 5' TURNING POINT FOR ADA PATH. TURNING SPACE SHALL HAVE A MAX 2% SLOPE IN ALL DIRECTIONS. ALL LOCATED WITHIN EXISTING ROW.
- EXISTING CENTERLINE STRIPING



A3 ENLARGED RAMP DETAIL

SCALE: 1" = 8'-0"



A1 TRAFFIC CIRCULATION LAYOUT

SCALE: 1" = 20'-0"

C1 SITE DEMOLITION PLAN

SCALE: 1" = 20'-0"