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Ernest Armijo, P.E.
Principal Engineer, Planning Department

Re: Bob's Burger
305 Eubank Blvd. NE
Traffic Circulation Layout
Architect's Stamp 05-17-2022 (D20-D049)

Dear Mr. Armijo,

This letter is intended to address the comments submitted to my office on May 23rd, 2022 by Nilo Salgado. Each comment has been addressed independently in order. Adjustments have been made as defined below if necessary. Nilo was consulted on 07.18.2022. His responses are provided in RED. Ernest was consulted on site 08.19.2022. His responses are also provided in RED.

1. Listed are the number of parking spaces (ADA stalls ?) required by the IDO as well as the proposed number of parking spaces including bicycle and motorcycle parking. Coordinate/Discuss with Zoning.

RESPONSE: The parking calculations are shown on the submitted TCL under "GENERAL INFORMATION". Per table 5-5-1 MINIMUM OFF-STREET PARKING REQUIREMENTS, a Restaurant is required to provide 8 spaces per 1000sq. ft. GFA. As demonstrated under "GENERAL INFORMATION", the parking calculations are defined as follows:

Restaurant GFA = 2,269
 $2,269/1000\text{sf} = 2.269$
 $2.269 \times 8 = 18 \text{ spaces}$

Provided: 19 spaces

Per table 5-5-4 MINIMUM MOTORCYCLE PARKING REQUIREMENTS the required Motorcycle Parking is defined as follows:

1-25 Required Off-street Vehicle Parking Spaces requires 1 motorcycle space.

Provided: 2 spaces

Per table 5-5-5 MINIMUM BICYCLE PARKING REQUIREMENTS, non-residential uses not listed in the table require 3 spaces or 10% of required off-street parking spaces, whichever is greater.

3 bicycle spaces required

Provided: 3 spaces

Per table 1106.1 ACCESSIBLE PARKING SPACES of the New Mexico Commercial Building Code, the required accessible spaces are defined as follows

1-25 Required Off-street Vehicle Parking Spaces required 1 Accessible Parking Space and shall be Van Accessible.



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Provided: 2 spaces van accessible

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2. **Proposed driveway entrance off Eubank Blvd: Please call out COA standard drawing 2426 (this will include 6 ft valley gutter and unidirectional ramps) for details.**

RESPONSE: Keyed note 25 has been changed to the following: “NEW RADIUS DRIVE ENTRY PER COA STANDARD DETAIL 2426.”

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

3. **Keyed Note 20:**
- **Why are you proposing this at norther end of Alley way?**

RESPONSE: This is an error on the keyed note. The keyed note has been changed to #21.

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- **Provide Alley way width.**

RESPONSE: The Alley way width is defined by the label “20’ Alley” already located on the drawing.

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4. **Keyed Note 24, 27, 29, 30, 35, 36, 39, & 40: Details will need to be provided and/or referenced COA std dwg on some.**

RESPONSE:

Keyed note 24 has been changed to read “DRIVEPAD WITH SIDEWALK AT BACK OF CURB PER COA STANDARD DETAIL 2425A AND 2425B.”

Keyed note 27 has been changed to read “NEW CURB TYPE SIDEWALK PER COA DETAIL 2430.”

Keyed note 29 has been changed to read “NEW STANDARD CURB AND GUTTER TO MATCH EXISTING PER COA DETAIL 2415A”

Keyed note 30 is a header curb on private property. Detail has been provided in attached AS-501 SITE DETAILS sheet. D3/AS-501.



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Keyed note 35 is for pole mounted Motorcycle Signage. Detail has been provided in attached AS-501 SITE DETAILS sheet. C5/AS-501.

Keyed note 36 is for pole mounted HC Signage. Detail has been provided in attached AS-501 SITE DETAILS sheet. C4/AS-501

Keyed note 39 is for HC ACCESS STRIPING. Detail has been provided in attached AS-501 SITE DETAILS sheet. A4/AS-501.

Keyed note 40 is striping for areas where there is "NO PARKING". Detail has been provided in attached AS-501 SITE DETAILS sheet. B3/AS-501.

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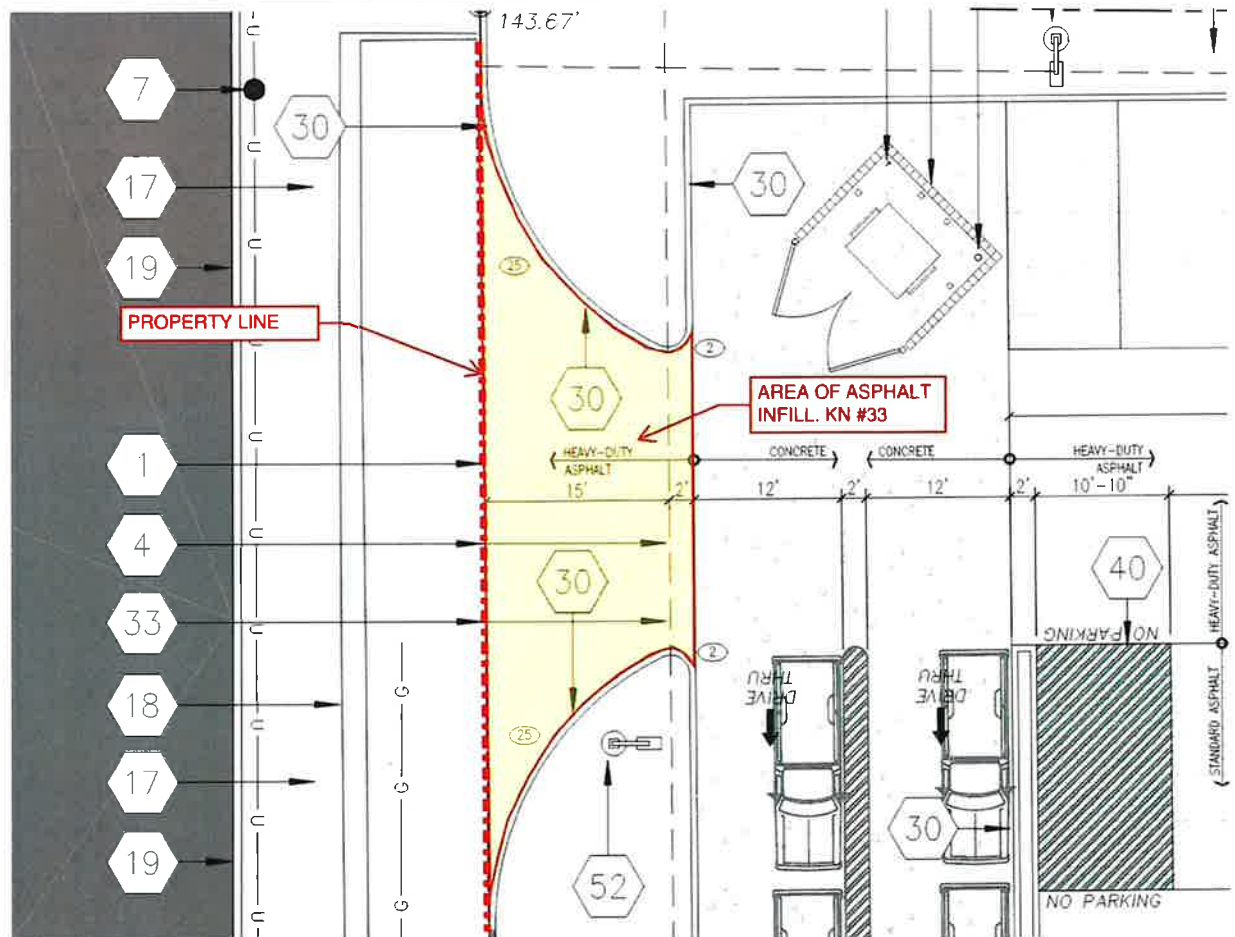
5. **Keyed Note 33: This will function appropriately on private property but not on Alley Way. Provide note to ensure this concern unless it is on Alley (Then a Work Order will be required)**

RESPONSE: As you can see keyed note 33 identifies an area on private property that will need to be filled in between the property line and the concrete drive pad of the drive-thru lane/dumpster enclosure. This area is currently filled with parking lot paving for the existing restaurant that will be demolished for new construction and left as blank dirt. We intend to fill in as part of the proposed drive aisle on the north end. See below.



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6. The minimum parking stall dimensions are (Some are not shown):

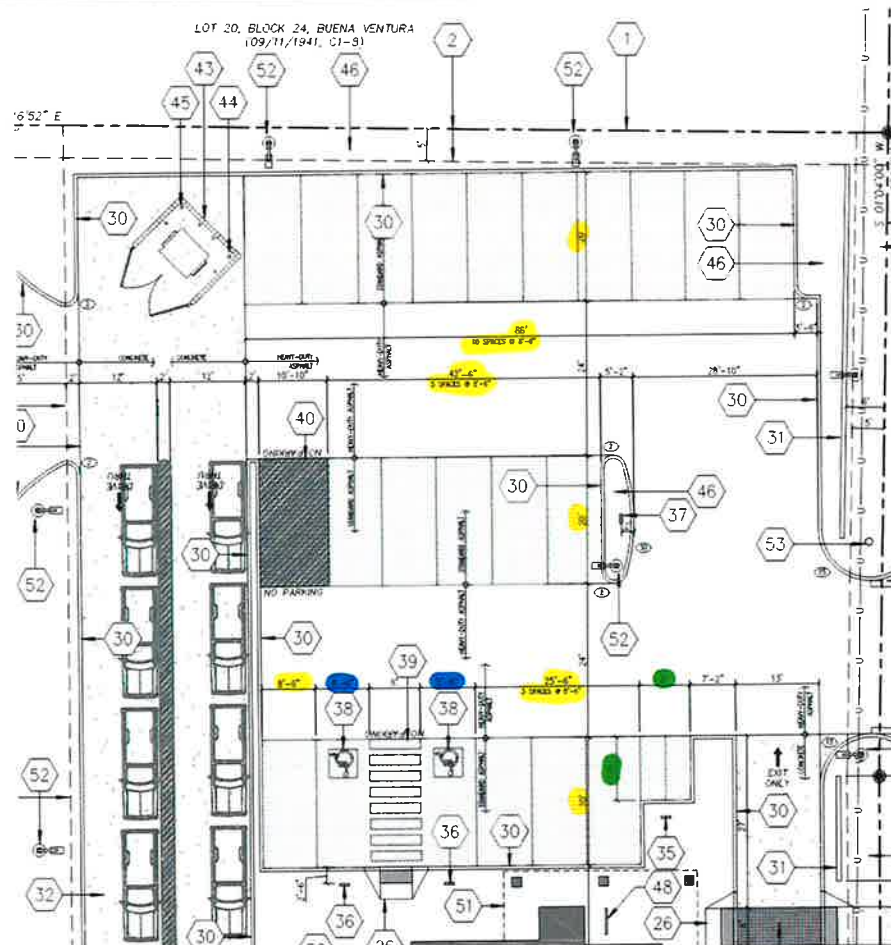
Type of parking	Min. Width	Min. Length	Min. Overhang
Standard	8.5'	18'	2'
Compact	7.5'	15'	1.5'
Motorcycle	4'	8'	N/A
ADA	8.5'	18'	2'

RESPONSE: All parking dimensions were identified. Standard spaces are designed better than code at 20' x 8.5'. We do not have any COMPACT spaces. Motorcycle spaces have been designed better than code at 10' x 4' each. ADA spaces have been designed better than code at 20' x 8.5'. I have highlighted all pertinent dimensions in the diagram below for your review:



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Yellow = standard spaces
Blue = HC Accessible spaces
Green = Motorcycle spaces

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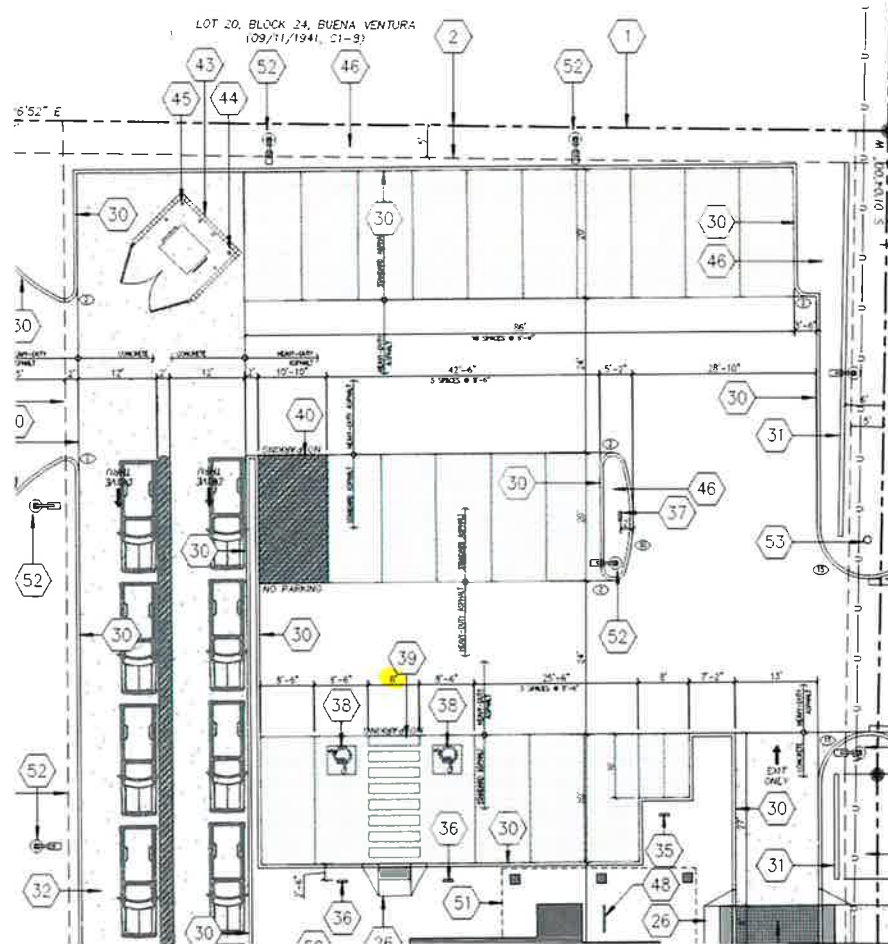
7. The ADA accessible spaces must include an access aisle. Van accessible aisles should be 8ft wide; all others should be 5ft wide.

RESPONSE: The van accessible aisle is provided at 8' wide. See attachment:



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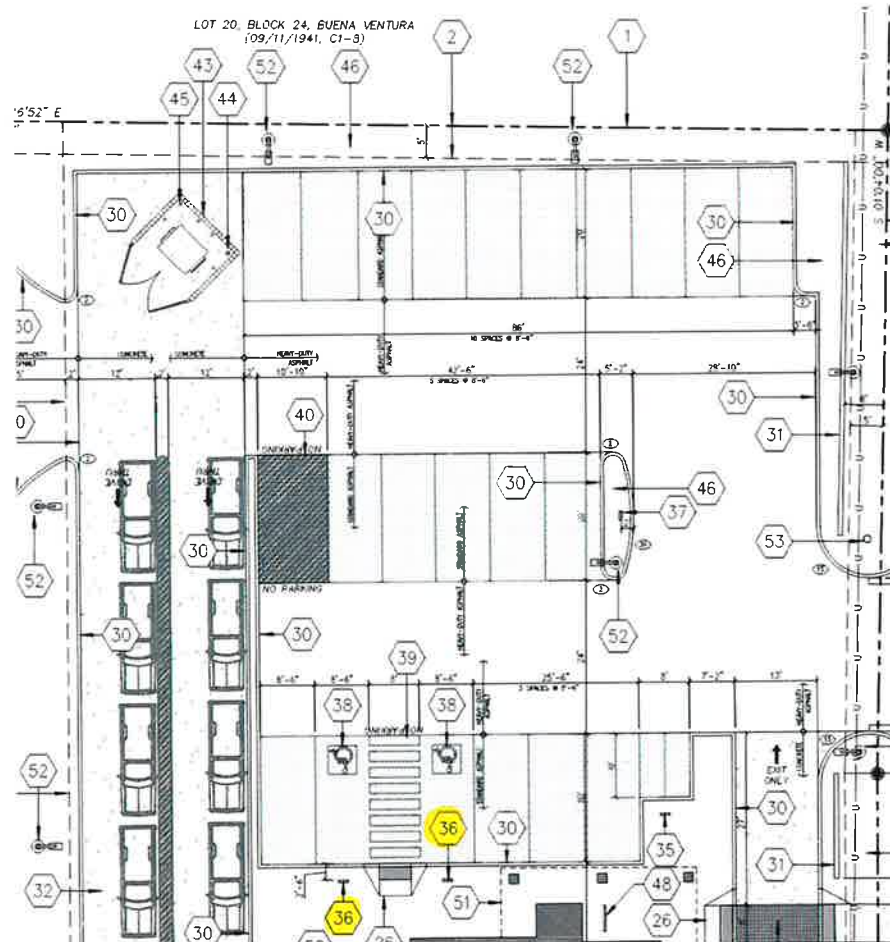
8. The ADA accessible parking sign must have the required language per 66-7-352.4C NMSA 1978 “Violators Are Subject to a Fine and/or Towing.” Please call out detail and location of signs.

RESPONSE: Keyed Note #36 has been modified to read “NEW POLE MOUNTED ACCESSIBLE PARKING SIGNAGE. SHALL INCORPORATE LANGUAGE “VIOLATORS ARE SUBJECT TO A FINE AND/OR TOWING” PER 66-7-352.4C NMSA 1978. SEE DETAIL ON SHEET AS-501.” Keyed note #36 can be found as highlighted below. C4/AS-501.



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9. The ADA access aisles shall have the words “NO PARKING” in capital letters, each of which shall be at least one foot high and at least two inches wide, placed at the rear of the parking space so as to be close to where an adjacent vehicle’s rear tire would be placed. (66-1-4.1.B NMSA 1978)

RESPONSE: Keyed Note #39 has been modified to read “NEW HANDICAP ACCESS AISLE STRIPING. SEE DETAIL A4/AS-501.

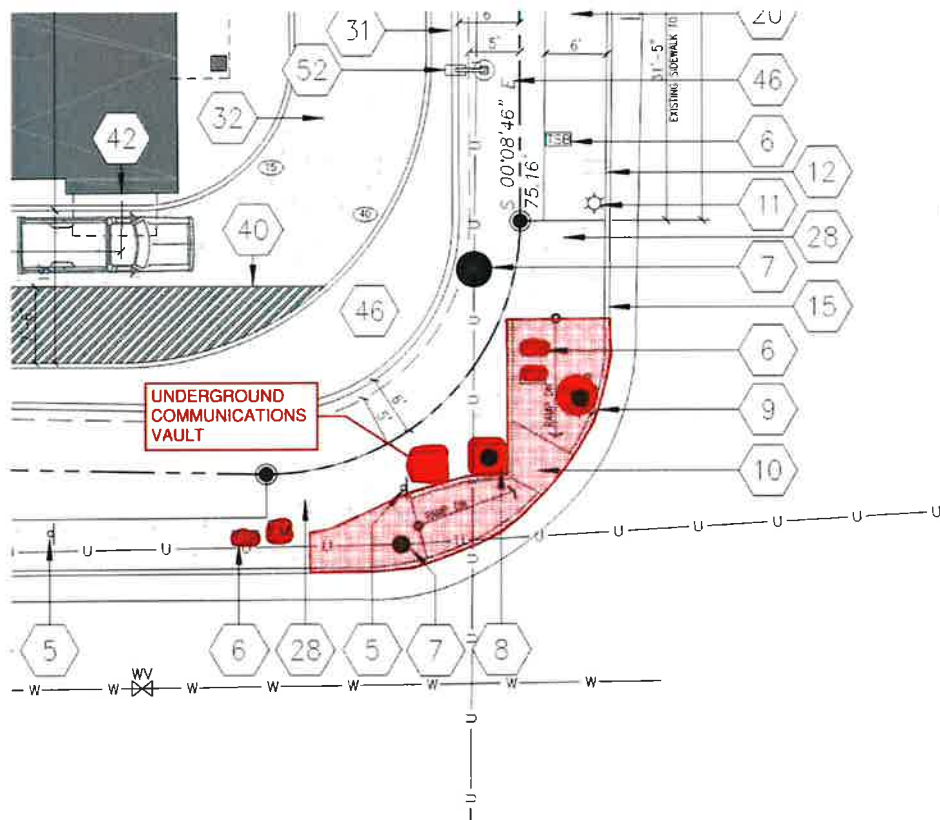
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10. ADA curb ramps:

- Eubank/Chico: Corner ramp must be current/updated design and have truncated domes installed for public ramps.
- Must be updated to current standards. And have truncated domes installed for public ramps and details must be provided for each type of proposed ramp.
- Keyed Note 14 & 28: You will need a full detail of ramp to include dimensions and concrete design and slopes.

RESPONSE: The red hatched area is the existing concrete and accessible HC ramp. We understand the existing conditions are expected to be upgraded in these circumstances per the IDO. However, in this particular case, upgrading the HC accessible ramp to the COA Standard detail 2441 or even COA Standard Detail 2442 cannot be applied in this case without substantial cost to the Owner.



As you can see we have numerous City of Albuquerque infrastructure in place. Keyed Note #6 shows the existing locations of Traffic Signal Boxes. Keyed Note #7 shows the location of an existing

PNM Power Pole. Keyed Note #9 shows the location of an existing Fire Hydrant. Keyed Note #8 shows the location of the existing Traffic Signal Mast Arm for Eubank and Chico. Not identified by Keyed Note but shown on the above picture is the location of an existing underground communications box holding existing fiber lines.



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We have consulted the contractor and have been given the following price tags for relocating all the above existing structures into a location that would allow us to upgrade the HC Accessible Ramp to either a COA 2441 or a COA 2442:

- Cost for relocation of above ground Traffic Signal Boxes: UNKNOWN
- Cost for relocation of existing PNM Power Pole: \$50-\$75,000.00
10-12 Months Work Order
- Cost for relocation of existing COA Traffic Mast Arm: \$120-\$175,000.00
- Cost for relocation of existing Fire Hydrant: \$50,000.00
- Cost for relocation of existing Underground Comms: \$150-\$750,000.00
(depending on how many lines of fiber are involved)

The City expects the Owner of the updated development to absorb these costs. As you can see, in this case, upgrading a \$2,500 HC Accessible ramp could cost the owner anywhere between \$370,000.00 to around a million dollars to relocate all the City infrastructure that is currently impeding the construction of a new ramp.

As explained in the original TCL Executive Summary that I submitted with the initial plan, this expectation would put an unnecessary hardship on the Owner and ultimately kill the project. We propose to leave the HC Accessible Ramp in place as it currently exists. Modification would be extraordinarily cost prohibitive to the Owner potentially doubling the cost of the entire new restaurant development. The proposed design calls for infill of concrete flatwork behind the existing infrastructure to provide ample walking area around the existing obstacles.

See Photo below of existing conditions:



07.18.2022 – SPOKE WITH NILO. HE DEFERRED TO ERNEST ARMIJO.



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08.19.2022 – MET WITH ERNEST AT THE SITE. HE CONFIRMED AND ACCEPTED OUR RESPONSE. EXISTING CONDITIONS WILL REMAIN.

11. Chico drivepad:

- Provide details for all proposed infrastructure on plans.
- A special detail will be needed for drivepad transitioning from pad to curb returns (from a gutter pan to a 6' valley gutter).
- Locate proposed sidewalk segment to property line and not at back of curb as shown.
- You will need adjacent property owner permission to perform work on his frontage. Provide letter of permission.
- Call out COA std dwg 2430 for Keyed Note 27.

RESPONSE: As was mentioned in the original TCL Executive Summary, Jeanne Wolfenbarger and I met via ZOOM call on 3/29 to discuss this project and the proposed design prior to formal submittal of TCL. All requested modifications and changes that were discussed during that meeting were incorporated as agreed upon. That ZOOM call was recorded and available upon request. The issues raised under Question 11 were discussed specifically during that meeting. These specific Questions were summarized in the TCL Executive Summary issued on 5/17/2022. I will clarify here again.

Mrs. Wolfenbarger requested a radiused entry to the Alley. The current condition more accurately reflects COA Detail 2428. At Mrs. Wolfenbarger request, we discussed changing the alley entry to COA Detail 2430 as mentioned above. Clear sight triangles were discussed in both directions from the alley (looking East and looking West). See Photograph below of existing conditions:





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Due to the existing conditions, it is clear that the West side of the Alley cannot be modified as requested. If this was to be done, the existing PNM power pole and the existing in-ground communications vault would need to be relocated to accommodate a radiused entry. Also the private wall would need to be truncated and rebuilt to accommodate a clear site triangle.

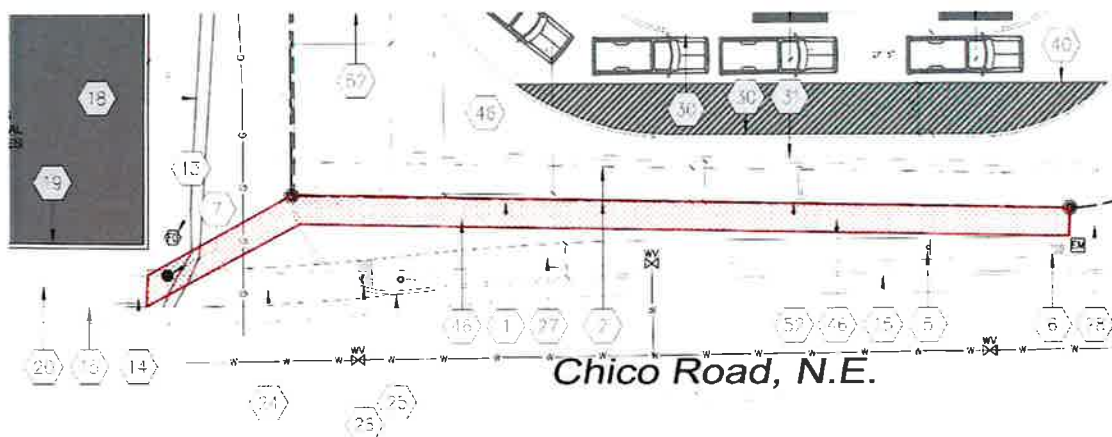
Relocation of the PNM power pole as discussed above under Question #10, would cost the Owner an estimated \$50-\$75,000.00 with an estimated 10-12 Months of time to complete the City Work Order. Relocation of the Communications Vault could cost anywhere between \$150-\$750,000.00 depending on how many lines of fiber are involved and what direction they are installed. We have been told that communication lines cannot be cut and adjusted. In fact they need to be re-run entirely between all the boxes feeding this location in every direction. Lines cannot be spliced and must remain whole from one box to the next.

Finally, in an effort to achieve a CLEAR SIGHT TRIANGLE, obtaining permission from the Property Land Owner to the West allowing us to lop of the corner of his yard wall, ultimately sacrificing land, will be extremely unlikely.

For these reasons, we propose to leave the West side of the Alley as it currently exists without modification. Updating will place an unnecessary hardship on the Owner and ultimately kill the project. Modification would be extraordinarily cost prohibitive.

As illustrated in the photo above and the submitted TCL, we propose to apply a new radiused entry on the East side of the Alley and the CLEAR SIGHT TRIANGLE is achievable.

The Owner has committed to reconstruct the sidewalk along Chico to achieve 5' of surface width. The existing sidewalk is currently 4' wide. We understand per the IDO the sidewalk is preferred to be located at the property line. However, due to the conflicts we have discussed above with the inability to update the West entry of the Alley, moving the new sidewalk away from the curb and up to the property line to create a landscape buffer along the street will ultimately create an awkward transition at the alley as the two sidewalks will no longer align. This transition will create a dangerous situation for pedestrians as they navigate from one sidewalk to the other having to enter traffic within the alley. See illustration below.



Due to these unique circumstances, we propose to rebuild the sidewalk in the existing location at the back of the curb. This transition at the alley creates synergy and safety as pedestrians cross the alley. The new radiused entry and HC Accessible ramp shall be constructed to COA Standard Detail 2426 and transition to the existing condition similar to COA Standard Detail 2430 as shown in the submitted TCL.



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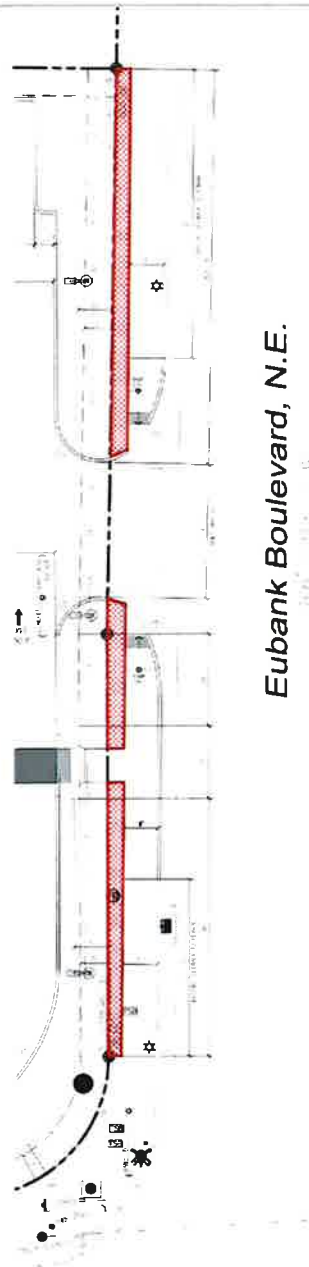
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The sidewalk along Eubank is already constructed at the proper width of 6'. We are only required to fill in where the old curb cut used to be. When we discussed this issue with Jeanne on 3/29, it was agreed that asking the owner to reconstruct the entire length of sidewalk along Eubank for the sole purpose of moving it over approximately 18" to the property line was unnecessary. The residual landscape area created would ultimately be insufficient to carry plant life. The area shown in red would be the distance the sidewalk would move toward the West to abut the property line. We feel the expense would be great for the Owner to incur for very little gain. Updating will place an unnecessary hardship on the Owner.



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07.18.2022 – SPOKE WITH NILO. HE DEFERRED TO ERNEST ARMIJO REGARDING THE ISSUE OF ALLEY DRIVE PAD DESIGN. HE AGREED THE PROPOSED LOCATION OF SIDEWALKS ARE ACCEPTABLE.

08.19.2022 – MET WITH ERNEST AT THE SITE. HE CONFIRMED AND ACCEPTED OUR RESPONSE. EXISTING CONDITIONS WILL REMAIN ON THE WEST SIDE OF THE DRIVE PAD ENTRY AND WE WILL UPDATE THE EAST SIDE AS PROPOSED. DETAIL HAS BEEN ADDED TO SHEET AS-502. SIDEWALKS WILL REMAIN AS PROPOSED WITHIN THIS TCL SUBMITTAL.



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PROPERTY OWNER TO THE WEST HAS BEEN NOTIFIED AND HER APPROVAL LETTER IS ATTACHED WITH SIGNATURE.

12. Motorcycle parking spaces: Spaces shall be designated by its own conspicuously posted upright sign, either free-standing or wall mounted per the zoning code. Label pavement MC (provide pavement markings-text details).

RESPONSE: Keyed Note #35 reads "NEW POLE MOUNTED MOTORCYCLE PARKING SIGNAGE. C5/AS-501.

Added Keyed Note #54 the reads, "NEW MOTORCYCLE PAVEMENT MARKING. REFER TO AS-501 FOR DETAIL." A2/AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

13. All bicycle racks shall be designed according to the following guidelines:

- The rack shall be a minimum 30 inches tall and 18" wide.
- The bicycle frame shall be supported horizontally at two or more places. Comb/toaster racks are not allowed.
- The rack shall be designed to support the bicycle in an upright position. See the IDO for additional information.
- The rack allows varying bicycle frame sizes and styles to be attached.
- The user is not required to lift the bicycle onto the bicycle rack.
- Each bicycle parking space is accessible without moving another bicycle.

RESPONSE: Modified Keyed Noted #48 shall read, "NEW BICYCLE RACK. REFER TO DETAIL ON SHEET AS-501." B1/AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

14. Bicycle racks shall be sturdy and anchored to a concrete pad.

RESPONSE: Modified Keyed Noted #48 shall read, "NEW BICYCLE RACK. REFER TO DETAIL ON SHEET AS-501." B1/AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

15. A 1-foot clear zone around the bicycle parking stall shall be provided.

RESPONSE: Modified Keyed Noted #48 shall read, "NEW BICYCLE RACK. REFER TO DETAIL ON SHEET AS-501." B1/AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.



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16. Bicycle parking spaces shall be at least 6 feet long and 2 feet wide.

RESPONSE: Modified Keyed Note #48 shall read, "NEW BICYCLE RACK. REFER TO DETAIL ON SHEET AS-501." B1/AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

17. Minimum radii's will need to be 15'R. Adjust site plan to reflect this concern.

RESPONSE: All turning radii are 15' minimum.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

18. Per the DPM, a 6'ft. wide ADA accessible pedestrian pathway is required from the public sidewalk to the building entrances. Please clearly show this pathway and provide details.

RESPONSE: This criteria is met. See exhibit below.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

19. Per the DPM, a 6'ft. wide ADA accessible pedestrian pathway is required from the ADA parking stall access aisles to the building entrances. Please clearly show this pathway and provide details.

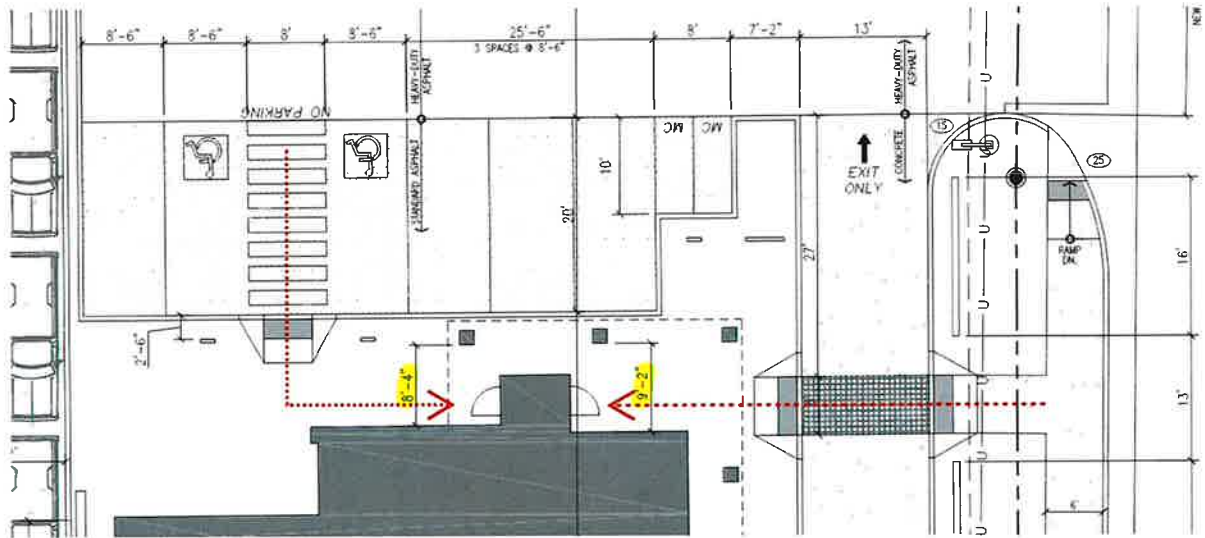
RESPONSE: This criteria is met. See exhibit below. See keyed note #34.

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20. All sidewalks along public streets should be placed at the property line.

RESPONSE: Please see comments for Question #11 above. This was discussed with Mrs. Wolfenbarger during ZOOM call on 3/29/22.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

08.19.2022 – CONFIRMED WITH ERNEST PROPOSED IS SUFFICIENT.

21. Curbing should be installed to delineate landscape, parking, and pedestrian ways and identify points of access. Please call out detail and location of barrier curb.

RESPONSE: This criteria is met. See keyed note #30 located throughout the plan inside the property line. Detail can be found on AS-501. D3/AS-501.

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- 22. Parking areas shall have barriers to prevent vehicles from extending over public sidewalk, public right-of-way, or abutting lots.**

RESPONSE: This comment does not apply. We do not have parking areas that extend over public sidewalks, public rights-of-way, or abutting lots.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

- 23. Provide a copy of refuse approval. The Refuse Vehicle maneuvering shall be contained on-site and shall not back into the public right of way.**

RESPONSE: I will engage Sanitation for approval after we reach a general consensus with the Traffic Control Layout. I don't want to get an approval from that department if TCL will required large scale layout changes.

08.23.2022 – APPROVED BY HERMAN GALLEGOS. APPROVAL ATTACHED.

- 24. Fire Department: An approval Fire 1 plan needs to be provided.**

RESPONSE: I will engage the Fire Department for Fire—1 approval after we reach a general consensus with the Traffic Control Layout. I don't want to get an approval from that department if TCL will required large scale layout changes.

08.29.2022 – APPROVED. APPROVAL ATTACHED.

- 25. What type of pavement surface is being proposed. A hard driving surface will be required as part of the proposed project for parking area and drive aisles. So, Please provide each type proposed.**

RESPONSE: Per the TCL submittal, we propose (2) different types of asphalt paving for the parking areas (Medium-Duty asphalt & Light-duty asphalt). Please see excerpt from the soils report:



Pavement Section Thicknesses

The following table provides options for AC and PCC Sections:

Asphaltic Concrete Design		
Layer	Thickness (inches)	
	Light Duty ¹	Med. Duty ¹
AC ^{2, 3}	2.5	3
Aggregate Base ²	6	6

1. See **Project Description** for more specifics regarding Light Duty and Medium Duty traffic.
2. All materials should meet the current City of Albuquerque (COA) or New Mexico Department of Transportation (NMDOT) Standard Specifications for Highway and Bridge Construction.
 - Asphaltic Surface – COA or NMDOT Type SP-III or IV: Section 423
 - Aggregate Base – COA or NMDOT Type I, Class I: Section 303
3. A minimum 1.5-inch surface course should be used on AC pavements.

Per the TCL submittal, we propose Heavy Duty concrete for the Dumpster Pad and the Drive-thru aisle. Please see excerpt from the soils report:

Portland Cement Concrete Design			
Layer	Thickness (inches)		
	Light Duty ¹	Med. Duty ¹	Dumpster Pad ³
PCC ²	5	5.5	6

1. See **Project Description** for more specifics regarding traffic classifications.
2. All materials should meet the current COA or NMDOT Standard Specifications for Highway and Bridge Construction.
 - Concrete Pavement - NMDOT Portland Cement Concrete Type C: Section 509
3. In areas of anticipated heavy traffic, fire trucks, delivery trucks, or concentrated loads (e.g. dumpster pads), and areas with repeated turning or maneuvering of heavy vehicles.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

26. All one-way drives shall have “One Way” and “Do Not Enter” signage and pavement markings. Please show detail and location of posted signs and striping (includes pavement markings and proposed texts).

RESPONSE: Markings are shown on TCL Plan. See Details on sheet AS-501. C2&C3 AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.



27. Drive Thru Lane:

- **Provide a queuing analysis to ensure storage lane will not stack into public streets (IF APPLICABLE).**

RESPONSE: During the 3/29 ZOOM call we discussed this issue. Code required 12 cars for stacking. This project accommodates 16 cars. Mrs. Wolfenbarger asked me to propose the question of a queuing analysis to Matt Grush while inquiring about TIS. Matt Grush responded with a signed TIS Determination on 4/27. TIS is not required. No requirement for queuing analysis recommended.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

- **The minimum drive through lane width is 12 feet with a 25 foot minimum radius (inside edge) for all turns. (a 15 foot radius can be used with an increase in lane width to 14 feet). Please dimension all lane widths and radii.**

RESPONSE: As the split order lane reconvenes south of the order boards, the drive thru lane opens up to 26' across. Based on this open dimension, the turning radius of 15' (inside corner) is acceptable as dimensioned on the TCL plan. After the order window, the drive-thru lane narrows to 15' across as dimensioned on the TCL. This allows a 15' radius (inside corner) as the drive-thru lane turns back North.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

28. All proposed pavement markings, striping, arrows will need details shown on Site Plan.

RESPONSE: See sheet AS-501 for details. See Details on sheet AS-501. C2&C3 AS-501.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

29. Please provide a site distance exhibit.

RESPONSE: As discussed above Question #11, the site triangle for the West side of the alley entrance is not obtainable with existing conditions. The East side of the alley entry is provided. The following criteria are required to determine the sight triangle:

Eubank Blvd: 50' ROW DIVIDED
40 MPH
Per COA Standards DI = 460'

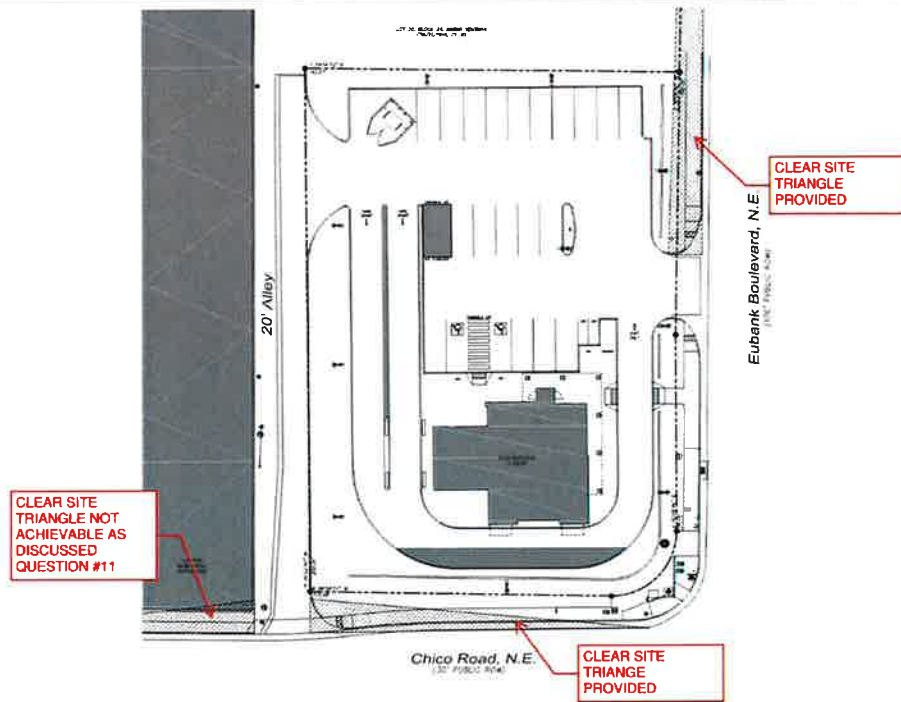
Chico Road: 30' R.O.W. UNDIVIDED
25 MPH
Per COA standards Dr = 100'
Per COA standards DI = 130'properties

SEE EXHIBIT BELOW:



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30. Show the clear sight triangle and add the following note to the plan: “Landscaping and signage will not interfere with clear sight requirements. Therefore, signs, walls, trees, and shrubbery between 3 and 8 feet tall (as measured from the gutter pan) will not be acceptable in the clear sight triangle.

RESPONSE: Preferred language has been added to the TCL. See General Note “N”.

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31. Please specify the City Standard Drawing Number when applicable.

RESPONSE: Sited as noted throughout this response.

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32. Shared Site access: driveways that straddle property lines or are entirely on one property but are to be used by another property, shall have an access easement. Please include a copy of your shared access agreement with the adjacent property owner.



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RESPONSE: This comment does not apply to this project. No cross-property agreements are presented or exist in this proposal.

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33. Add a note stating, "All broken or cracked sidewalk must be replaced with sidewalk and curb & gutter." A build note must be provided referring to the appropriate City Standard drawing (2430 & 2415A).

RESPONSE: See General Note "O" added to the TCL Plan.

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34. Please provide a letter of response for all comments given.

RESPONSE: Done.

35. Traffic Studies (IF APPLICABLE): See the Traffic Impact Study (TIS) thresholds. In general, a minimum combination of 100 vehicles entering and exiting in the peak hour warrants a Traffic Impact Study. Visit with Traffic Engineer for determination, and fill out a TIS Form that states whether one is warranted. In some cases, a trip generation may be requested for determination (Contact Matt Grush: mgrush@cabq.gov).

RESPONSE: Per the instruction of Mrs. Wolfenbarger via ZOOM call on 3/29, we issued a Traffic Scoping Form to Matt Grush. He responded with signature dated 4/27. TIS is not required. A copy of the approved and signed Scoping Form was attached to the Executive Summary that was turned in with this submittal on 5/17.

07.18.2022 – SPOKE WITH NILO AND RECEIVED CONFIRMATION AND ACCEPTANCE OF RESPONSE.

Sincerely,

Marc Gonzales, AIA / NCARB
Buffalo Design Architects

Cc: earmijo@cabq.gov
sbiazar@cabq.gov
fishman@consensusplanning.com

Attachments: Original Executive Summary w/signed Traffic Scoping Form dated 05.17.22
Updated TCL Plan incorporating comments above.



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AS-501 SITE DETAILS
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Email from Ernest dated 08.30.2022
Neighbor Approval Letter
Refuse Approval (See Updated TCL)
Fire 1 Approval