

FED EX FREIGHT DISTRIBUTION CENTER

REVISED TRAFFIC IMPACT ANALYSIS

JANUARY 2, 2018

Prepared For:
Kiernan West, LLC
P.O. Box 979
Telluride, CO 81435

Prepared By:

Bohannon  **Huston**

Engineering

Spatial Data

Advanced Technologies



**FED EX FREIGHT DISTRIBUTION CENTER
ALBUQUERQUE, NEW MEXICO
TRAFFIC IMPACT ANALYSIS**

JANUARY 2, 2018

REVISED SUBMITTAL

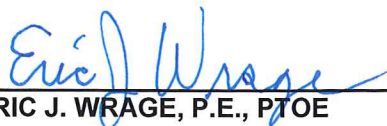
Prepared by:

**BOHANNAN HUSTON, INC.
7500 JEFFERSON ST NE
COURTYARD TWO
ALBUQUERQUE, NM 87109**

Prepared for:

**KIERNAN WEST, LLC
P.O. BOX 979
TELLURIDE, CO 81435**

PREPARED BY:


ERIC J. WRAGE, P.E., PTOE


JANUARY 2, 2018

**FED EX FREIGHT DISTRIBUTION CENTER
ALBUQUERQUE, NEW MEXICO
TRAFFIC IMPACT ANALYSIS**

TABLE OF CONTENTS

I.	INTRODUCTION AND SUMMARY	1
A.	Study Purpose.....	1
B.	Executive Summary	1
1.	Site Location and Study Area.....	1
2.	Principal Findings	2
3.	Recommendations	2
II.	PROPOSED DEVELOPMENT	6
A.	Land Use and Intensity.....	6
B.	Development Phasing and Timing.....	6
III.	STUDY AREA CONDITIONS	7
A.	Study Area	7
B.	Site Accessibility.....	7
C.	Data Sources	7
IV.	ANALYSIS OF EXISTING CONDITIONS.....	8
A.	Background.....	8
1.	Adjacent Roadways	8
2.	Adjacent Land	8
B.	Existing Traffic Conditions	8
C.	Existing Levels of Service.....	9
1.	Vehicular Analysis.....	9
2.	Bicycle Analysis	13
3.	Pedestrian Analysis.....	15
4.	Traffic Signal Warrant Analysis	16
D.	Safety Analysis.....	16
V.	PROJECTED TRAFFIC.....	18
A.	Site Traffic Forecasting.....	18
1.	Trip Generation	18
2.	Trip Distribution and Assignment.....	19
3.	2020 No Build Traffic Projections	19

VI. TRAFFIC AND IMPROVEMENT ANALYSIS	22
A. Level of Service Analysis.....	22
1. 2020 No Build Intersection Capacity Analysis	22
2. Peak Hour Traffic Signal Warrant Analysis.....	22
3. 2020 Build Traffic Volumes	26
4. Peak Hour Traffic Signal Warrant Analysis.....	28
5. Fair Share Analysis	30
VII. CONCLUSIONS AND RECOMMENDATIONS.....	33
A. Conclusions.....	33
B. Recommendations	33

FIGURES

Figure 1 – Vicinity Map.....	4
Figure 2 – Site Plan.....	5
Figure 3 – 2017 AM Peak Hour Traffic Volumes and LOS.....	12
Figure 4 – 2017 PM Peak Hour Traffic Volumes and LOS.....	13
Figure 5 – Crash History	17
Figure 6 – Trip Assignment Volumes – AM	20
Figure 7 – Trip Assignment Volumes – PM	21
Figure 8 – 2020 No Build AM Peak Hour Traffic Volumes and LOS	25
Figure 9 – 2020 No Build PM Peak Hour Traffic Volumes and LOS	26
Figure 10 – 2020 AM Build Peak Hour Traffic Volumes and LOS.....	29
Figure 11 – 2020 PM Build Peak Hour Traffic Volumes and LOS.....	30

TABLES

Table 1 – Bicycle Counts.....	9
Table 2 – LOS Definitions	9
Table 3 – 2017 Existing Unsignalized Intersection Results.....	10
Table 4 – Bicycle LOS Results	14
Table 5 – Pedestrian LOS Results	15
Table 6 – Trip Generation	18
Table 7 – 2020 No Build Unsignalized Intersection Results.....	23

Table 8 – 2020 Build Unsignalized Intersection Results	27
Table 9 – Fair Share Contribution Analysis – Atrisco Vista and I-40 North Ramp	31
Table 10 – Fair Share Contribution Analysis – Atrisco Vista and I-40 South Ramp	32

APPENDICES

Appendix A Future Road Network	
Appendix B Bernalillo County Traffic Study Scoping Report	
Appendix C Existing Traffic Counts	
Appendix D 2017 Existing Intersection Capacity Analysis	
Appendix E Traffic Signal Warrant Analysis	
Appendix F 2010 – 2015 Crash Summary Data	
Appendix G Trip Generation and Traffic Growth	
Appendix H 2020 No Build Intersection Capacity Analysis	
Appendix I 2020 Build Intersection Capacity Analysis	

I. INTRODUCTION AND SUMMARY

Fed Ex Freight is proposing to develop a 90,000 square-foot package distribution facility on approximately 50-acres on Western Albuquerque Land Holdings land, south of Cordero Mesa. The project is in the jurisdiction of Bernalillo County, however the County requested the City of Albuquerque (COA) and New Mexico Department of Transportation (NMDOT) be consulted in the development of the traffic study.

A. STUDY PURPOSE

The purpose of the traffic study is to determine the impacts of the proposed improvements on the surrounding roadway network, and to recommend any mitigation measures that may be necessary to support the new development.

B. EXECUTIVE SUMMARY

1. SITE LOCATION AND STUDY AREA

The site is located west of Atrisco Vista Boulevard and north of the north I-40 Frontage Road, in Bernalillo County, New Mexico. A vicinity map is shown in Figure 1, and the current site plan is shown in Figure 2. The site will consist of a 90,000 square-foot package distribution facility. The facility will operate 24 hours a day, seven days a week, and will employ 300 people, of which 200 are new jobs.

The study area consists of the following intersections:

- Shamrock Way and Tempur Pedic Parkway (existing full access two-way stop control unsignalized intersection)
- Atrisco Vista and Tempur Pedic Parkway (existing full access two-way stop control unsignalized T-intersection)
- Atrisco Vista and North I-40 Frontage Road (existing full access two-way stop control unsignalized intersection)
- Atrisco Vista and I-40 Westbound Ramp (existing full access two-way stop control unsignalized intersection)
- Atrisco Vista and I-40 Eastbound Ramp (existing full access two-way stop control unsignalized intersection)
- Atrisco Vista and Central Avenue (existing full access all-way stop control unsignalized intersection)

The intersection evaluations include analysis for the AM and PM peak hours for the following traffic conditions:

- Existing traffic (2017)
- 2020 Completion Year without proposed development (2020 No Build)
- 2020 Completion Year with buildout of the site (2020 Build)

2. PRINCIPAL FINDINGS

The traffic analysis shows that under existing 2017, 2020 No Build, and 2020 Build, all intersections will operate at an overall acceptable level of service (LOS), although the westbound left/through at the intersection of Atrisco Vista and the I-40 North Ramp (westbound) will operate at LOS F in the Existing 2017, 2020 No Build and 2020 Build scenarios. The eastbound left/through at the I-40 South Ramp (eastbound) will also operate at LOS E in the PM peak hour in the 2020 Build scenario.

A traffic signal is not warranted in the 2017 Existing, 2020 No Build scenarios at any of the intersections, although the Atrisco Vista and I-40 North (westbound) ramp intersection peak hour volume warranted will be satisfied in 2022 without development of the site.

With the addition of the build traffic, the intersection of Atrisco Vista and I-40 North Ramp (westbound) satisfies the peak hour volume and peak hour delay warrant. The proposed development generates 4.3% of the traffic at the Atrisco Vista and I-40 North Ramp (westbound) intersection during the peak hours.

The peak hour volume and delay warrant is not satisfied at the I-40 South (eastbound) ramp in any of the peak hours. The development contributes 2.4% of the volume to this intersection.

A northbound left turn lane is required under COA DPM and NMDOT SAMM criteria in Existing 2017, No Build 2020 and Build 2020 conditions.

3. RECOMMENDATIONS

- A northbound left turn lane should be constructed at the intersection of Atrisco Vista and Tempur Pedic Parkway. It is recommended this turn lane conform to NMDOT requirements due to the high number of trucks anticipated to use the turn lane. The expected queue is one vehicle. The NMDOT SAMM requires a 325-foot left turn lane, with a 125-foot taper, for a turn lane on a 40-MPH roadway. An additional 75 feet should be added to the left turn lane length to account for the queued truck.

- It is not recommended that a traffic signal be constructed by the developer at the intersection of Atrisco Vista and the I-40 North Ramp (westbound), as this development contributes just 4.3% of the traffic at this intersection during the peak hour. In addition, this development will be constructing the northbound left turn lane at Tempur Pedic Parkway, even though the left turn lane is required under existing traffic volumes.
- The extension of Shamrock Way across the AMAFCA channel to the site entrance will need to be constructed to County standards and will need an appropriately sized cul-de-sac available for any general public traffic that needs to turnaround.
- All designs shall satisfy Bernalillo County Public Works, NMDOT, COA, and Manual on Uniform Traffic Control Devices (MUTCD) requirements.



Client Logo



0 470 940
Feet
1 in = 941 ft

FedEx
West Albuquerque

Figure 1
Vicinity Map



II. PROPOSED DEVELOPMENT

A. LAND USE AND INTENSITY

The proposed development will be located on a lot totaling approximately 50 acres. The building is anticipated to be 90,000 square feet. See the site plan in Figure 2.

The site is located near Cordero Mesa, and adjacent to the Westland Master Plan area. A conceptual future roadway network for the Westland Master Plan area and adjacent lands is included in Appendix A.

The facility will operate 24 hours a day, seven days a week. Trips from the site will travel both to the interstate and to the metro area, however they only drive to destinations where they can go out and back in the same day.

The Bernalillo County Traffic Study Scoping Report prepared for this project is included in Appendix B.

The Tempur Pedic manufacturing facility, the Shamrock Foods distribution center, and several RV sales and service, and semi-tractor truck repair facilities are in the vicinity.

B. DEVELOPMENT PHASING AND TIMING

The project is anticipated to be constructed in one phase, and is expected to be complete by 2020. The year 2020 was used as the build year in the analysis.

III. STUDY AREA CONDITIONS

A. STUDY AREA

The study area consists of the intersection along Atrisco Vista from Tempur Pedic Parkway to Central Avenue. The intersection of Shamrock Way and Tempur Pedic Parkway is also included. See Figure 2 for visualization of the proposed driveway on Shamrock Way on the site plan.

B. SITE ACCESSIBILITY

Shamrock Way will be extended south across the AMAFCA channel to provide access to the site. The site will not directly access the north I-40 Frontage Road.

Access to the site will be via the driveway on Shamrock Way.

Currently ABQRide does not have any routes near the site.

There is an approximately 4-foot wide, non-buffered designated bicycle lane traveling in both directions on Atrisco Vista. This bicycle lane converts to an approximately 5-6-foot shoulder south of the north I-40 Frontage Road, through the I-40/West Central Interchange to Central Avenue.

There is an approximately 6-foot wide sidewalk on both sides of Tempur Pedic Parkway between Atrisco Vista and Shamrock Way. An approximately 6-foot wide sidewalk is located on the east side of Shamrock Way from Tempur Pedic Parkway to the terminus of the paved section of Shamrock Way to the south.

Sidewalks are present on both sides of Atrisco Vista from Central Avenue, north to the north I-40 Frontage Road.

C. DATA SOURCES

The data used in this report consist of the traffic counts described below, aerial photography and mapping from Google Earth®, and information provided by Kiernan West, LLC.

IV. ANALYSIS OF EXISTING CONDITIONS

A. BACKGROUND

1. ADJACENT ROADWAYS

Atrisco Vista is classified as a Limited Access Principal Arterial by the MRCOG. The posted speed limit is 40 miles per hour (MPH). Atrisco Vista has one travel lane in each direction, with an approximately 4-foot wide, non-buffered designated bicycle lane traveling in both directions on Atrisco Vista. The online MRCOG Average Weekday Traffic Map indicates in 2015, Atrisco Vista carried an average weekday traffic volume of approximately 3,815 vehicles per day (vpd) north of the north I-40 Frontage Road.

Tempur Pedic Parkway is not classified by MRCOG, nor is there any daily volume reported on their online traffic flow map. The posted speed limit is 25 MPH. Tempur Pedic parkway has two travel lanes in each direction.

Shamrock Way currently is a local roadway serving the Shamrock Foods distribution center. The road will be extended across the AMAFCA channel to provide access to the site.

2. ADJACENT LAND

The area near the site, Cordero Mesa, is identified as a future employment center in the Mid-Region Council of Governments (MRCOG) Futures 2020 Metropolitan Transportation Plan Preferred Scenario.

Adjacent to the North I-40 Frontage Road are numerous businesses.

The lands to the east of Cordero Mesa are in the Westland Master Plan, but are currently undeveloped. The lands to the west are currently not under any Master Plan, and is primarily used for ranching at this time.

B. EXISTING TRAFFIC CONDITIONS

Traffic counts for the intersection analyzed in the study area were collected Tuesday, October 3, 2017, while school was in session. Figure 3 is a summary of the existing peak hour traffic volumes, turning movements, and intersection level of service. Existing traffic counts are included in Appendix C. The traffic counts included counts for heavy vehicles, pedestrians, and bicyclists. No pedestrians were observed during the traffic count period. The following table lists the bicyclists that were counted at the intersection during the nine-hour peak hour counts (three hours in AM, three hours near noon, and three hours in PM). No bicyclists were observed on Tempur Pedic Parkway, west of Atrisco Vista.

Table 1 – Bicycle Counts	
Atrisco Vista & Central Avenue	5
Atrisco Vista & I-40 Eastbound Ramp	15
Atrisco Vista & I-40 Westbound Ramp	12
Atrisco Vista & North I-40 Frontage Road	11
Atrisco Vista & Tempur Pedic Parkway	2
Tempur Pedic Parkway and Shamrock Way	0

C. EXISTING LEVELS OF SERVICE

1. VEHICULAR ANALYSIS

The *Highway Capacity Manual Sixth Edition* (HCM) defines Level of Service (LOS) for un-signalized intersections is as follows:

Table 2 – LOS Definitions			
Level of Service	Definition	Signalized (sec/veh)	Unsignalized (sec/veh)
A	Most vehicles do not stop.	<10	<10
B	Some vehicles stop.	>10 and <20	>10 and <15
C	Significant numbers of vehicles stop.	>20 and <35	>15 and <25
D	Many vehicles stop.	>35 and <55	>25 and <35
E	Limit of acceptable delay.	>55 and <80	>35 and <50
F	Unacceptable delay.	>80	>50

Bernalillo County, the City of Albuquerque and the NMDOT, have established LOS D as the generally acceptable level of service in urban areas and when intersections operate below this level, improvements are considered, where feasible.

Existing intersection traffic volumes were analyzed using the VISTRO software package, which uses the intersection methodology from the Sixth Edition of the HCM. Individual intersection output for the existing conditions analysis is included in Appendix D.

The results are summarized in Table 3 and shown graphically in Figure 3.

The analysis indicates all the intersections in the study area operate at acceptable levels of service, although at the Atrisco Vista and North I-40 Ramp (westbound ramp), the westbound left/through movement operates at LOS F in the PM peak hour. It will be shown this intersection does not satisfy the Peak Hour Volume or Delay traffic signal warrant.

Typically, when a minor street left turn operates at poor level of service, a two-stage gap analysis is performed. This analysis was not performed at this location due to the large number of heavy trucks making this movement, as these vehicles cannot perform a two-stage gap maneuver, due to vehicle length. Passenger cars which can make the two-stage gap maneuver will likely operate with acceptable delay.

Table 3 – 2017 Existing Unsignalized Intersection Results								
Intersection/Movement	2017 AM Peak				2017 PM Peak			
	Delay	v/c	Queue* (ft)	LOS	Delay	v/c	Queue* (ft)	LOS
Tempur Pedic & Shamrock Way								
NB Approach	8.94	0.02	25	A	8.48	0.02	25	A
EB Left	7.27	0.00	25	A	7.27	0.00	0	A
WB Left	8.04	0.01	25	A	7.66	0.02	25	A
SB Approach	8.77	0.00	25	A	10.37	0.22	25	A
Atrisco Vista & Tempur Pedic								
EB Left	11.34	0.00	25	B	11.61	0.04	25	B
EB Right	10.61	0.02	25	B	9.51	0.15	25	A
NB Approach	2.18	0.04	25	A	7.55	0.03	25	A
Atrisco Vista & North Frontage								
EB Approach	9.30	0.05	25	A	11.0	0.18	25	B
WB Approach	12.03	0.00	0	B	13.27	0.00	0	B
NB Left	3.23	0.08	25	A	8.09	0.08	25	A
SB Left	0.00	0.00	0	A	7.68	0.00	0	A
Atrisco Vista & North I-40 Ramp								
WB Left/Through	21.09	0.43	75	C	53.39	0.77	150	F
WB Right	9.28	0.12	25	A	9.4	0.10	25	A
NB Left	8.11	0.07	25	A	8.69	0.14	25	A
Atrisco Vista & South I-40 Ramp								
EB Left/Through	19.27	0.03	25	C	31.11	0.26	25	D
EB Right	9.88	0.07	25	A	10.61	0.17	25	B
SB Left	8.85	0.06	25	A	9.01	0.15	25	A
Atrisco Vista & Central								
NB Approach	10.04	0.20	25	B	10.66	0.15	25	B
SB Approach	10.43	0.24	25	C	16.02	0.60	100	C
EB Approach	10.38	0.14	25	B	10.79	0.14	25	B
WB Approach	10.50	0.31	50	B	13.02	0.47	75	B
* - HCM 95 th percentile queue rounded to next 25-foot increment								

The intersection of Atrisco Vista and Tempur Pedic Parkway currently does not have a southbound right turn lane or a northbound left turn lane. Using COA Development Process Manual (DPM) criteria, a southbound right turn lane is not required with existing traffic volumes. A southbound right turn lane is also not required using criteria for deceleration lanes on two-lane urban highways in the NMDOT State Access Management Manual (SAMM).

A northbound left turn lane is required at Atrisco Vista and Tempur Pedi Parkway under existing traffic volumes per the COA DPM and the NMDOT SAMM.



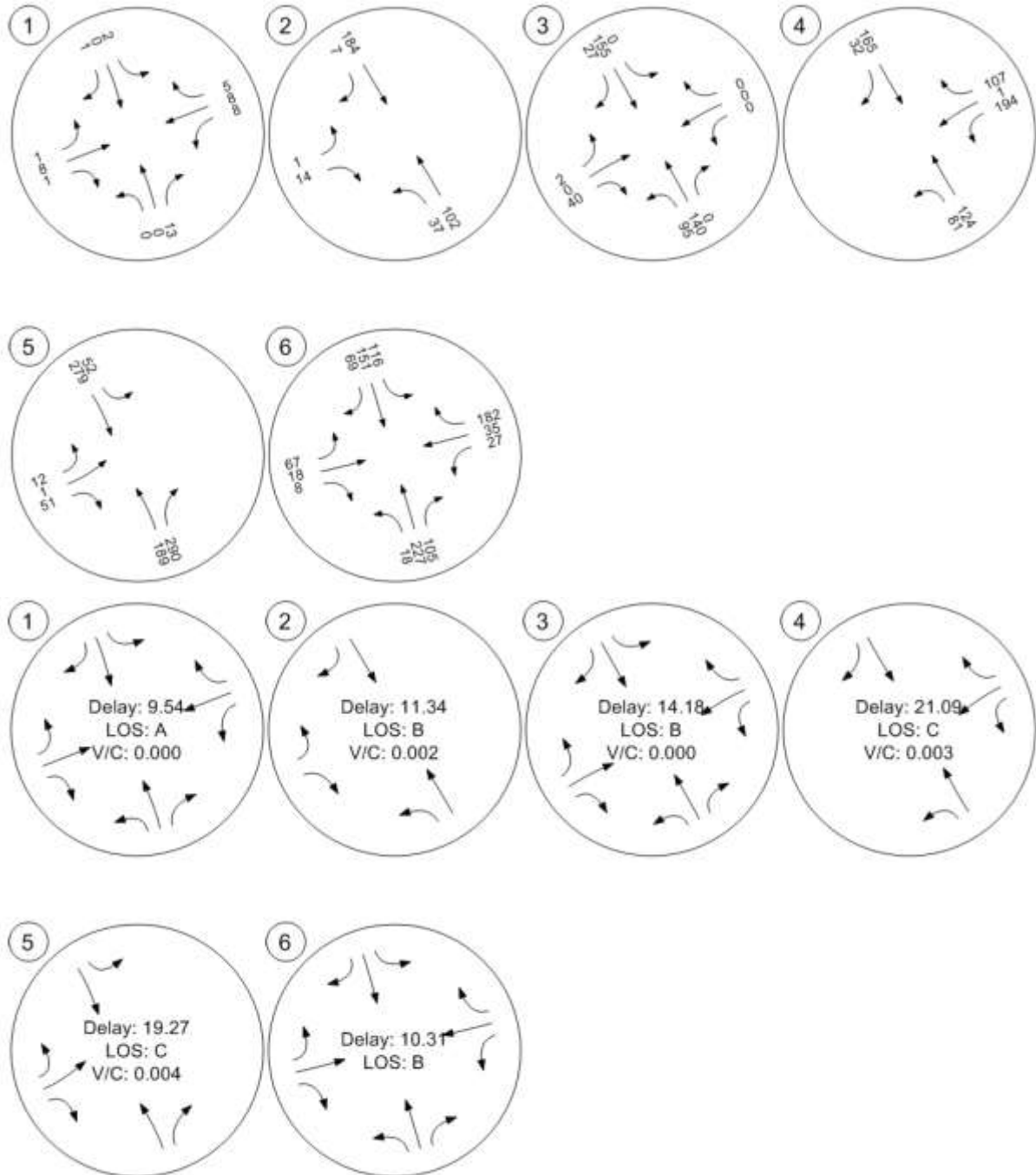


Figure 3 – 2017 AM Peak Hour Traffic Volumes and LOS

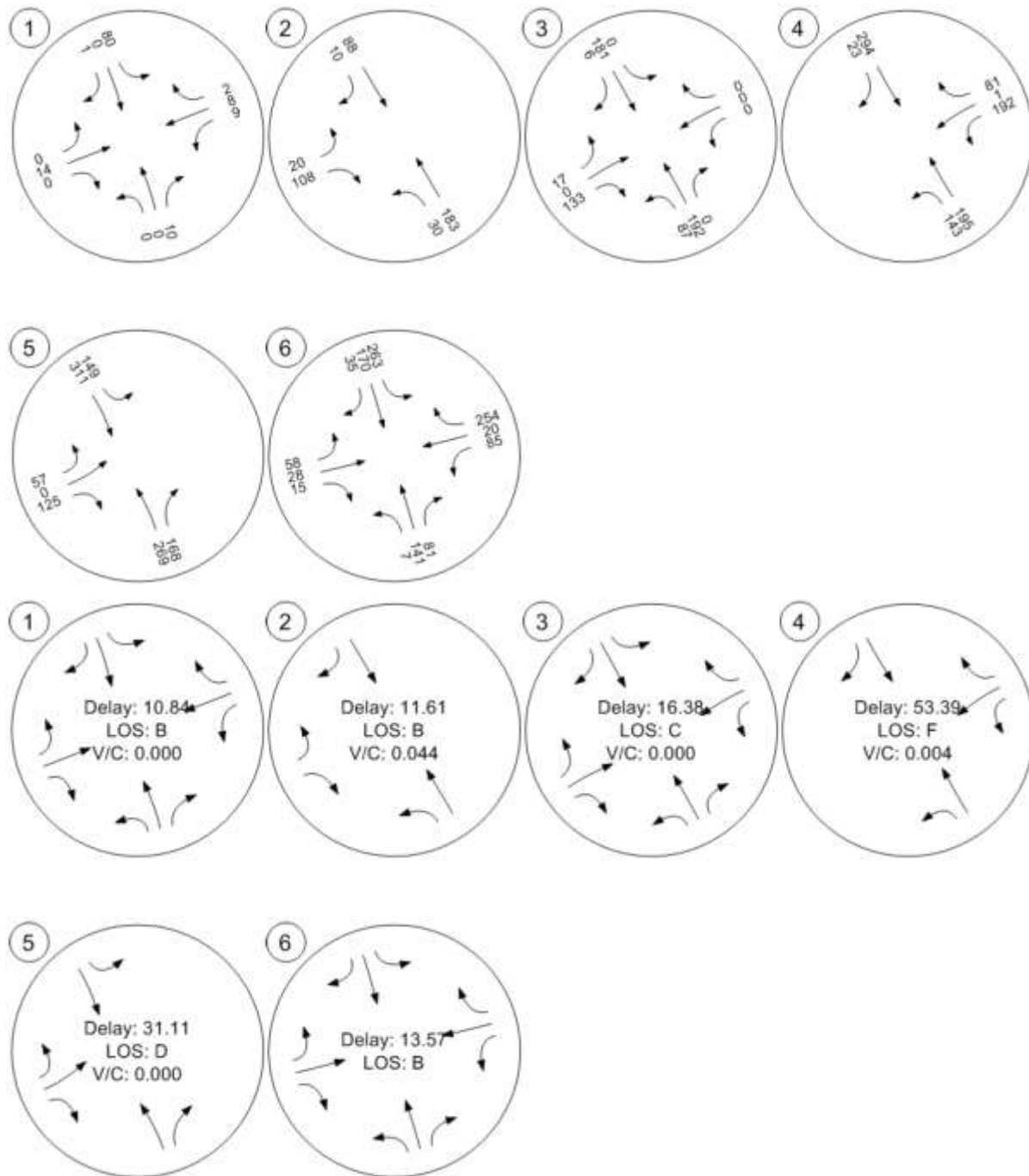


Figure 4 – 2017 PM Peak Hour Traffic Volumes and LOS

2. BICYCLE ANALYSIS

The bicycle LOS analysis evaluates the presence and quality of bicycle infrastructure as it contributes to the comfort and safety of the bicycle user. The analysis utilizes formulas

contained in the National Cooperative Highway Research Program's "Multimodal Level of Service Analysis for Urban Streets."

Criteria used in the analysis include:

1. Number of vehicle travel lanes
2. Median type
3. Average daily traffic
4. Speed limit
5. Percentage of heavy vehicles
6. Width of the outside vehicle lane
7. Width of the bicycle lane buffer
8. Width of the bicycle lane
9. Width of on-street parking
10. Pavement condition
11. Percentage of on-street parking that is occupied

The results of the bicycle LOS analysis are displayed in the Table 4 below. Each roadway section evaluated operates at an acceptable bicycle LOS under existing conditions.

Table 4 – Bicycle LOS Results		
Criteria	Atrisco Vista Blvd	Tempur Pedic Pkwy
Number of Lanes	2	4
Median Type	Undivided	Divided
Average Daily Traffic	3,813	400
Speed Limit	40 mph	25 mph
Percentage Heavy Vehicles	8.8	23.9
Width of Outside Lane	11 ft	11
Width of Buffer	NA	NA
Width of Bicycle Lane	4 ft	NA
Width of On-Street Parking	NA	NA
Pavement Condition	4	4
OSPA	0	0
Level of Service Score	4.42	0.26
Level of Service	D	A

3. PEDESTRIAN ANALYSIS

The pedestrian LOS analysis evaluates the presence and quality of pedestrian infrastructure as it contributes to the comfort and safety of the pedestrian. The pedestrian LOS analysis also utilizes formulas contained in the National Cooperative Highway Research Program's "Multimodal Level of Service Analysis for Urban Streets." The pedestrian LOS analysis evaluates similar criteria to the bicycle LOS analysis, in addition to the following:

1. Signals per mile
2. Sidewalk width
3. Sidewalk buffer width
4. Tree spacing

The percentage of heavy vehicles and pavement condition were not evaluated in the pedestrian LOS analysis.

The results of the pedestrian LOS analysis are displayed in Table 5 below. Each roadway section evaluated operates at an acceptable pedestrian LOS under existing conditions.

Table 5 – Pedestrian LOS Results		
Criteria	Atrisco Vista Blvd	Tempur Pedic Pkwy
Number of Lanes	2	4
Signals per Mile	4	4
Median Type	Undivided	Divided
Average Daily Traffic	3,813	400
Speed Limit	40 mph	25 mph
Outside Lane Width	11 ft	11 ft
Bicycle Lane Buffer Width	NA	NA
Bicycle Lane Width	4	NA
On-Street Parking Width	NA	NA
OSPA	0	0
Sidewalk Width	0	6
Sidewalk Buffer Width	0	2
Tree Spacing	NA	NA
Level of Service Score	3.82	1.55
Level of Service	D	B

4. TRAFFIC SIGNAL WARRANT ANALYSIS

A complete traffic signal warrant analysis was performed for the existing traffic counts. This warrant analysis found a traffic signal is not warranted with existing traffic volumes at any of the intersections studied. The above traffic operations analysis did find the Atrisco Vista and I-40 North Ramp (westbound) does have a movement with high delay and LOS F under existing traffic levels, though neither the volume or delay satisfied Peak Hour Volume or Delay warrants.

The traffic signal warrant analyses for each intersection is included in Appendix E.

D. SAFETY ANALYSIS

A graphic showing the locations of crashes in the vicinity from 2010-2015 is shown in Figure 5. A total of 82 crashes occurred in the area in the six (6) years of data. The majority of the crashes were at the ramp intersections with I-40 (35 crashes), followed by Central Avenue with 32, and the North Frontage Road with 14 crashes. Most of the crashes were due to driver behavior (inattention, disregard traffic control, following too close, failure to yield, etc.). One (1) fatality was reported, and was an alcohol involved crash.

A listing of the crash types and other pertinent data from the crash report is included in Appendix F.



V. PROJECTED TRAFFIC

A. SITE TRAFFIC FORECASTING

1. TRIP GENERATION

Generated trips are broken down into three types; 1) primary, 2) pass-by trips, and 3) diverted link. The Trip Generation report defines these trips as follows:

- **Primary Trips** - These trips are made for the specific purpose of visiting the generator. The stop at that generator is the primary reason for the trip. For example, a home to shopping to home combination of trips is a primary trip set.
- **Pass-by Trips** - These trips are made as intermediate stops on the way from an origin to a primary trip generation. Pass-by trips are attracted from the traffic passing the site on an adjacent street that contains direct access to the generator site. These trips do not require a diversion from another roadway. For example, stopping at the store on the way home from work is an example of a pass-by trip. No pass-by trips were assigned to this development.
- **Diverted Linked Trips** - These trips are attracted from the traffic volume on the roadway within the vicinity of the generator, but which require a diversion from that roadway to another roadway to gain access to the site. The roadways could include streets or freeways adjacent to the generator, but without access to the generator. For this study, the diverted link trips have been included in with the primary trips.

All trips to the site were considered primary trips.

Fed Ex Freight provided an estimate of trips to the site by hour of the day, based on previous projects. This estimate included truck trips, truck driver employee trips, and employee trips for non-truck drivers. This trip generation estimate provided by Fed Ex is included in Appendix G.

Table 6 – Trip Generation						
Land Use	Size	Daily	AM Enter	AM Exit	PM Enter	PM Exit
Distribution Center	90,000 sf	602	42	9	19	24

2. TRIP DISTRIBUTION AND ASSIGNMENT

Due to the regional nature of proposed development, the vast majority of the trips are expected to travel on Atrisco Vista to I-40, though some traffic will proceed north for deliveries to Rio Rancho and the west side of Albuquerque, as well as employee trips. The trip distribution percentages were generally considered as 20% from the north and south, and 50% from the east and 10% from the west. The assigned traffic volumes for the development is shown in Figure 6 and Figure 7.

3. 2020 NO BUILD TRAFFIC PROJECTIONS

The traffic along Atrisco Vista has seen modest growth over the past five years. The data indicate a growth rate of 2.5% a year is appropriate. Spreadsheets showing the development of the growth rates is included in Appendix G. Figure 8 and Figure 9 shows the 2020 No Build traffic volumes and level of service.

The No Build analysis assumed that the proposed project is not completed.

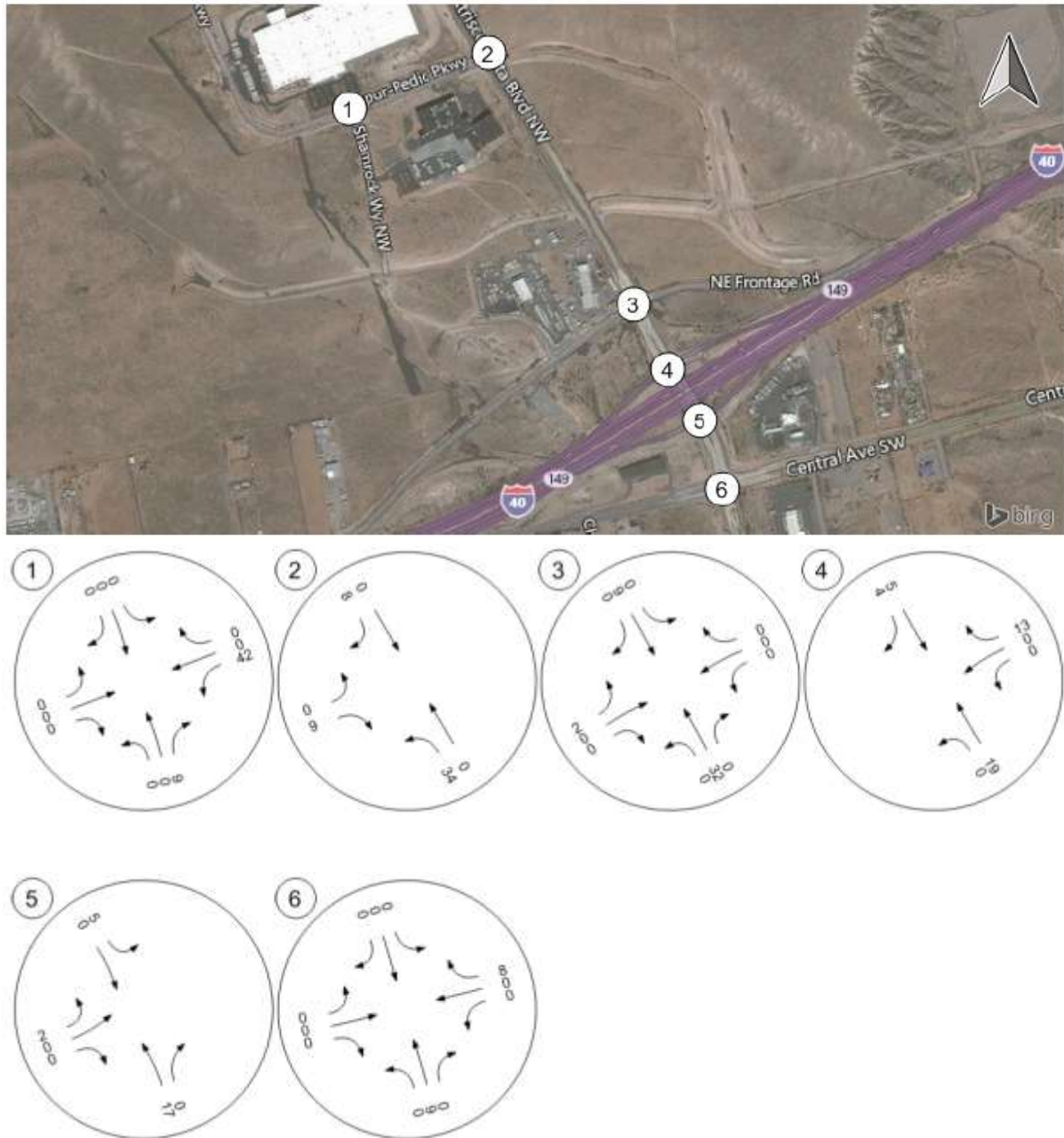


Figure 6 – Trip Assignment Volumes – AM

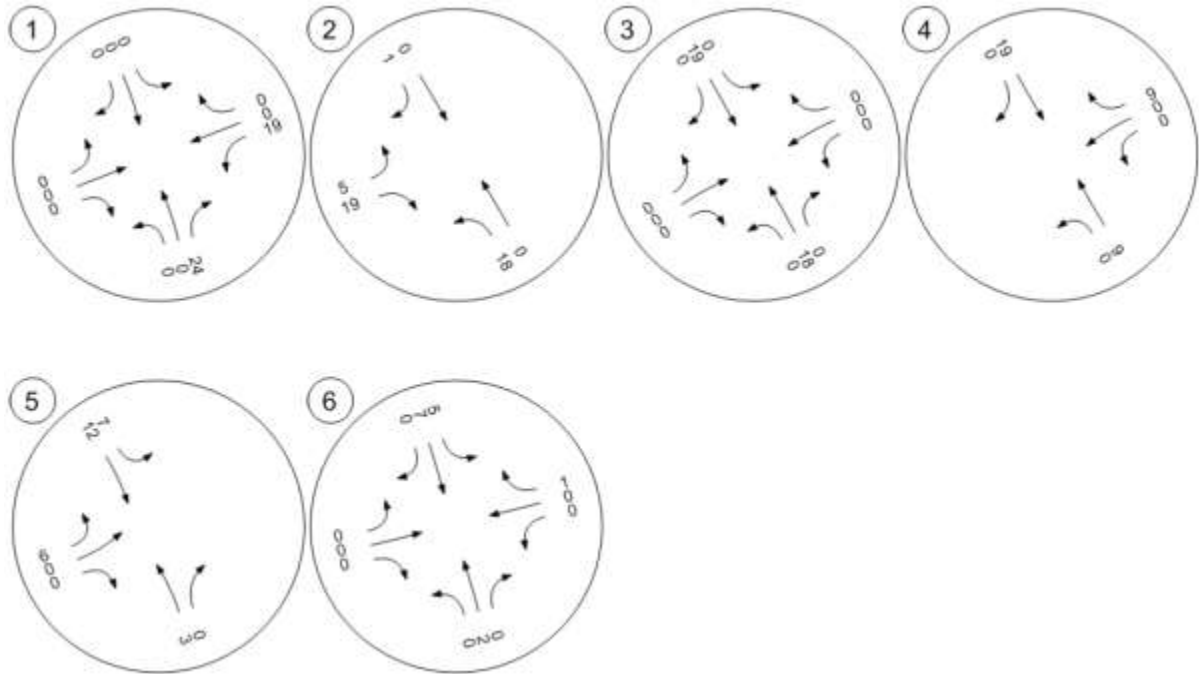


Figure 7 – Trip Assignment Volumes – PM

VI. TRAFFIC AND IMPROVEMENT ANALYSIS

The following section will discuss the results of the future year traffic analysis.

A. LEVEL OF SERVICE ANALYSIS

1. 2020 NO BUILD INTERSECTION CAPACITY ANALYSIS

For the 2020 No Build scenario, the intersections were again analyzed using VISTRO. Table 7 show the 2020 No Build results. The results are shown graphically in Figure 8 and Figure 9. The VISTRO output is included in Appendix H.

As with the existing 2017 conditions, all intersections operate at an acceptable level of service in the 2020 No Build scenario, although the I-40 North Ramp (westbound) still has the westbound left/through movement operating at LOS F.

The intersection of Atrisco Vista and Tempur Pedic Parkway currently does not have a southbound right turn lane or a northbound left turn lane. Using COA Development Process Manual (DPM) criteria, a southbound right turn lane is not required with 2020 No Build scenario traffic volumes. A southbound right turn lane is also not required using criteria for deceleration lanes on two-lane urban highways in the NMDOT State Access Management Manual (SAMM).

A northbound left turn lane at Atrisco Vista and Tempur Pedic Parkway is required under 2020 No Build traffic volumes per the COA DPM and the NMDOT SAMM.

2. PEAK HOUR TRAFFIC SIGNAL WARRANT ANALYSIS

The Peak Hour Volume and Delay warrant was evaluated for the intersection of Atrisco Vista and the I-40 North Ramp (westbound) for the 2020 No Build peak hour. Neither the peak hour volume or peak hour delay satisfied the signal warrant criteria, though they are both very close to warranting a traffic signal. The peak hour delay criteria is 5 hours, and under 2020 No Build traffic volumes, the PM peak hour delay is 4.70 hours. With a 5% increase in traffic, or about two years at present growth rates, a traffic signal will be warranted in the No Build scenario. A spreadsheet showing the 2020 No Build peak hour volume and delay warrant evaluation is included in Appendix E.

Table 7 – 2020 No Build Unsignalized Intersection Results								
Intersection/Movement	2020 AM Peak				2020 PM Peak			
	Delay	v/c	Queue* (ft)	LOS	Delay	v/c	Queue* (ft)	LOS
Tempur Pedic & Shamrock Way								
NB Approach	8.93	0.08	25	A	8.50	0.03	25	A
EB Left	7.27	0.02	25	A	7.27	0.00	0	A
WB Left	8.05	0.05	25	A	7.67	0.02	25	A
SB Approach	8.80	0.02	25	A	10.61	0.24	25	B
Atrisco Vista & Tempur Pedic								
EB Left	11.61	0.00	25	B	11.91	0.05	25	B
EB Right	10.69	0.03	25	B	9.62	0.16	25	A
NB Approach	2.20	0.04	25	A	1.06	0.03	25	A
Atrisco Vista & North Frontage								
EB Approach	9.38	0.05	25	A	11.30	0.19	25	B
WB Approach	12.42	0.00	0	B	13.84	0.00	0	B
NB Left	8.05	0.09	25	A	8.16	0.09	25	A
SB Left	7.56	0.00	0	A	7.72	0.00	0	A
Atrisco Vista & North I-40 Ramp								
WB Left/Through	23.56	0.49	75	C	80.26	0.91	200	F
WB Right	9.35	0.12	25	A	9.49	0.11	25	A
NB Left	8.17	0.07	25	A	8.84	0.15	25	A
Atrisco Vista & South I-40 Ramp								
EB Left/Through	20.71	0.04	25	C	36.0	0.31	50	D
EB Right	10.0	0.07	25	A	10.82	0.19	25	B
SB Left	9.0	0.06	25	A	9.19	0.17	25	A
Atrisco Vista & Central								
NB Approach	10.29	0.22	25	B	10.99	0.17	25	B
SB Approach	10.69	0.26	50	B	17.59	0.65	125	C
EB Approach	10.45	0.15	25	B	11.0	0.15	25	B
WB Approach	10.77	0.33	50	B	13.83	0.51	75	B
* - HCM 95 th percentile queue rounded to next 25-foot increment								



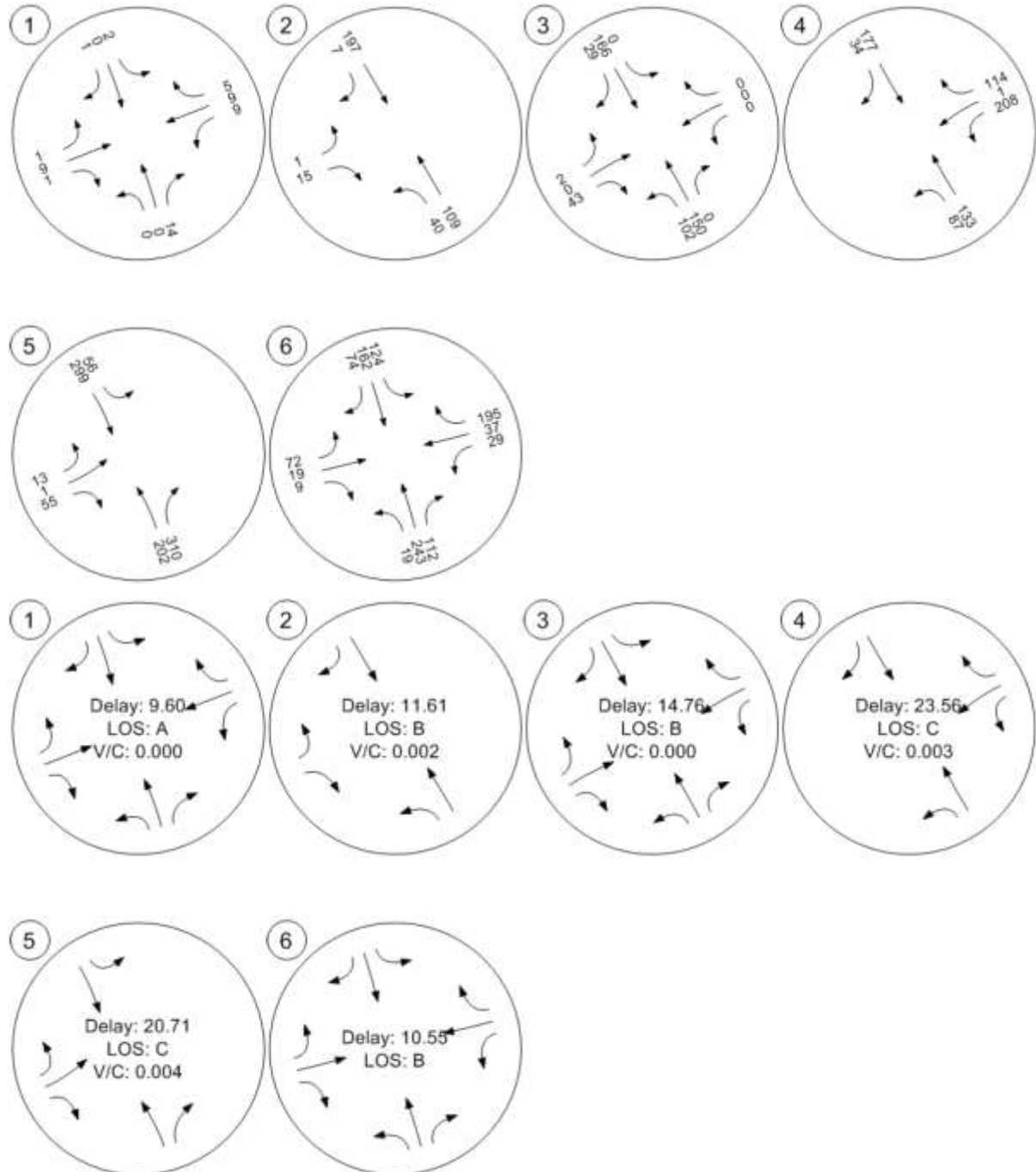


Figure 8 – 2020 No Build AM Peak Hour Traffic Volumes and LOS

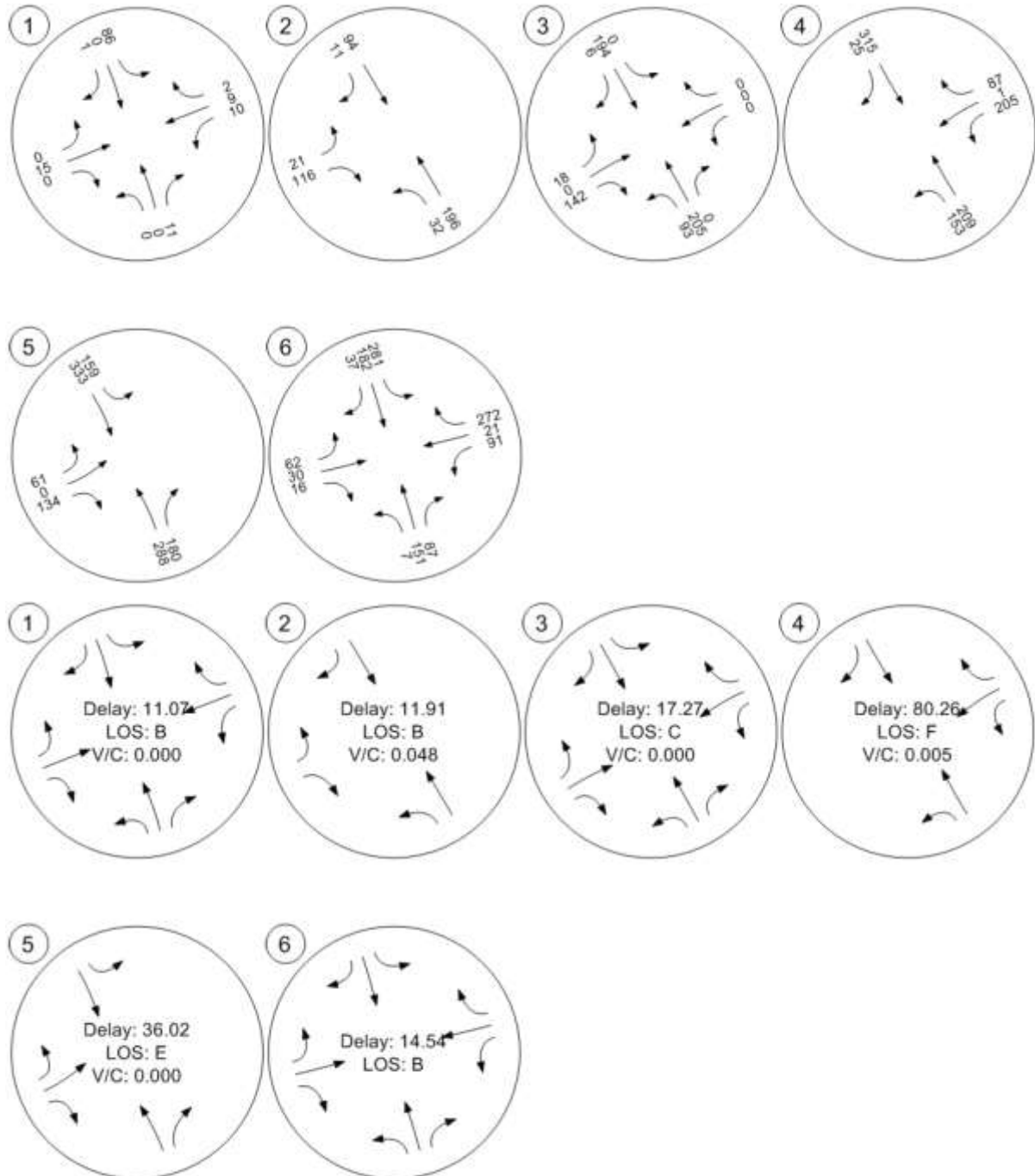


Figure 9 – 2020 No Build PM Peak Hour Traffic Volumes and LOS

3. 2020 BUILD TRAFFIC VOLUMES

The trips generated by the site (Table 6) were assigned to the intersections as shown in Figure 6 and Figure 7. These trips were added to the 2020 No Build traffic projections in

Figure 8 and Figure 9. The 2020 Build volumes and LOS are shown in Figure 10 and Figure 11. The individual intersection output is included in Appendix I.

As in the 2017 Existing and 2020 No Build scenarios, all intersections and movements operate at acceptable levels of service, except the westbound left/through movement at the I-40 North Ramp (westbound) continues to operate at LOS F, and the eastbound left/through movement at the I-40 South Ramp (eastbound) now operates at LOS E.

Table 8 – 2020 Build Unsignalized Intersection Results								
Intersection/Movement	2020 AM Peak				2020 PM Peak			
	Delay	v/c	Queue* (ft)	LOS	Delay	v/c	Queue* (ft)	LOS
Tempur Pedic & Shamrock Way								
NB Approach	9.03	0.04	25	A	8.71	0.08	25	A
EB Left	7.27	0.00	25	A	7.27	0.00	0	A
WB Left	8.25	0.07	25	A	7.76	0.05	25	A
SB Approach	9.66	0.00	25	A	12.72	0.31	50	B
Atrisco Vista & Tempur Pedic								
EB Left	12.67	0.00	25	B	12.64	0.07	25	B
EB Right	10.83	0.04	25	A	9.77	0.18	25	A
NB Approach	3.36	0.07	25	A	1.55	0.04	25	A
Atrisco Vista & North Frontage								
EB Approach	9.65	0.05	25	A	11.52	0.19	50	B
WB Approach	12.87	0.00	0	B	14.33	0.00	0	B
NB Left	8.08	0.09	25	A	8.23	0.09	25	A
SB Left	7.64	0.00	0	A	7.77	0.00	0	A
Atrisco Vista & North I-40 Ramp								
WB Left/Through	24.95	0.51	75	C	90.11	0.94	225	F
WB Right	9.49	0.14	25	A	9.59	0.12	25	A
NB Left	8.19	0.07	25	A	8.92	0.16	25	A
Atrisco Vista & South I-40 Ramp								
EB Left/Through	21.51	0.04	25	C	39.66	0.36	50	E
EB Right	10.0	0.07	25	A	10.89	0.19	25	B
SB Left	9.09	0.07	25	A	9.25	0.17	25	A
Atrisco Vista & Central								
NB Approach	10.40	0.23	25	B	11.04	0.17	25	B
SB Approach	10.75	0.26	50	B	17.93	0.66	125	C
EB Approach	10.47	0.15	25	B	11.03	0.15	25	B
WB Approach	10.93	0.34	50	B	13.92	0.52	75	B
* - HCM 95 th percentile queue rounded to next 25-foot increment								

The intersection of Atrisco Vista and Tempur Pedic Parkway currently does not have a southbound right turn lane or a northbound left turn lane. Using COA Development Process Manual (DPM) criteria, a southbound right turn lane is not required with 2020 Build scenario

traffic volumes. A southbound right turn lane is also not required using criteria for deceleration lanes on two-lane urban highways in the NMDOT State Access Management Manual (SAMM).

A northbound left turn lane is required under 2020 Build traffic volumes per the COA DPM and the NMDOT SAMM.

4. PEAK HOUR TRAFFIC SIGNAL WARRANT ANALYSIS

The Peak Hour Volume and Delay warrant was evaluated for the intersection of Atrisco Vista and the I-40 North Ramp (westbound) for the 2020 Build peak hour. Both the peak hour volume and peak hour delay satisfied the signal warrant criteria with the addition of the build traffic. The peak hour delay criteria is 5 hours, and with the proposed development, the PM peak hour delay is 5.28 hours. The spreadsheet showing the peak hour warrant analysis is included in Appendix E.

A Peak Hour Volume and Delay warrant was evaluated for the intersection of Atrisco Vista and the I-40 South Ramp (eastbound) for the 2020 Build peak hour. A traffic signal is not warranted at this time, though the PM peak hour volume is getting close. The PM peak hour delay for the minor street approach is 1.01 hours.



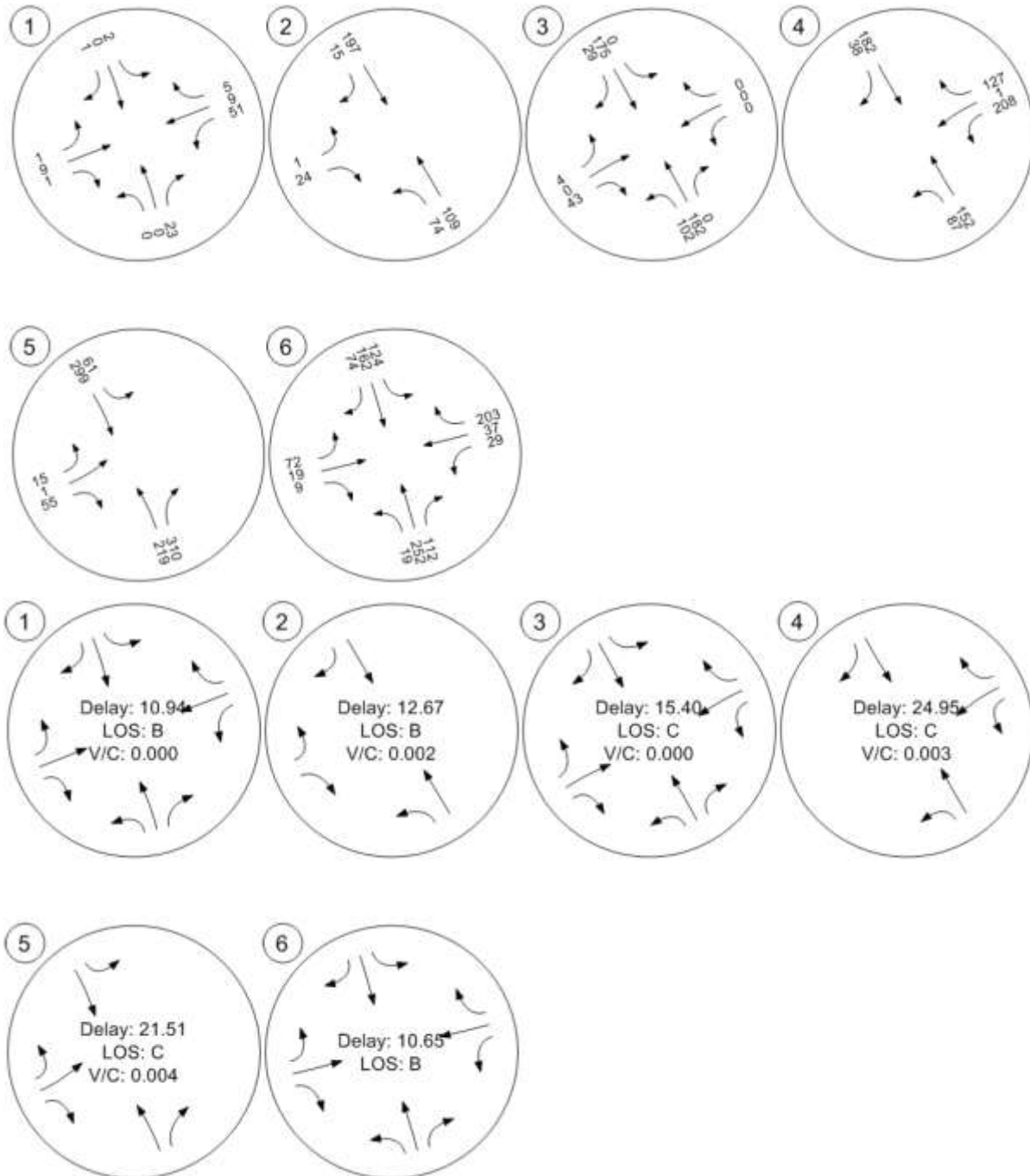


Figure 10 – 2020 AM Build Peak Hour Traffic Volumes and LOS

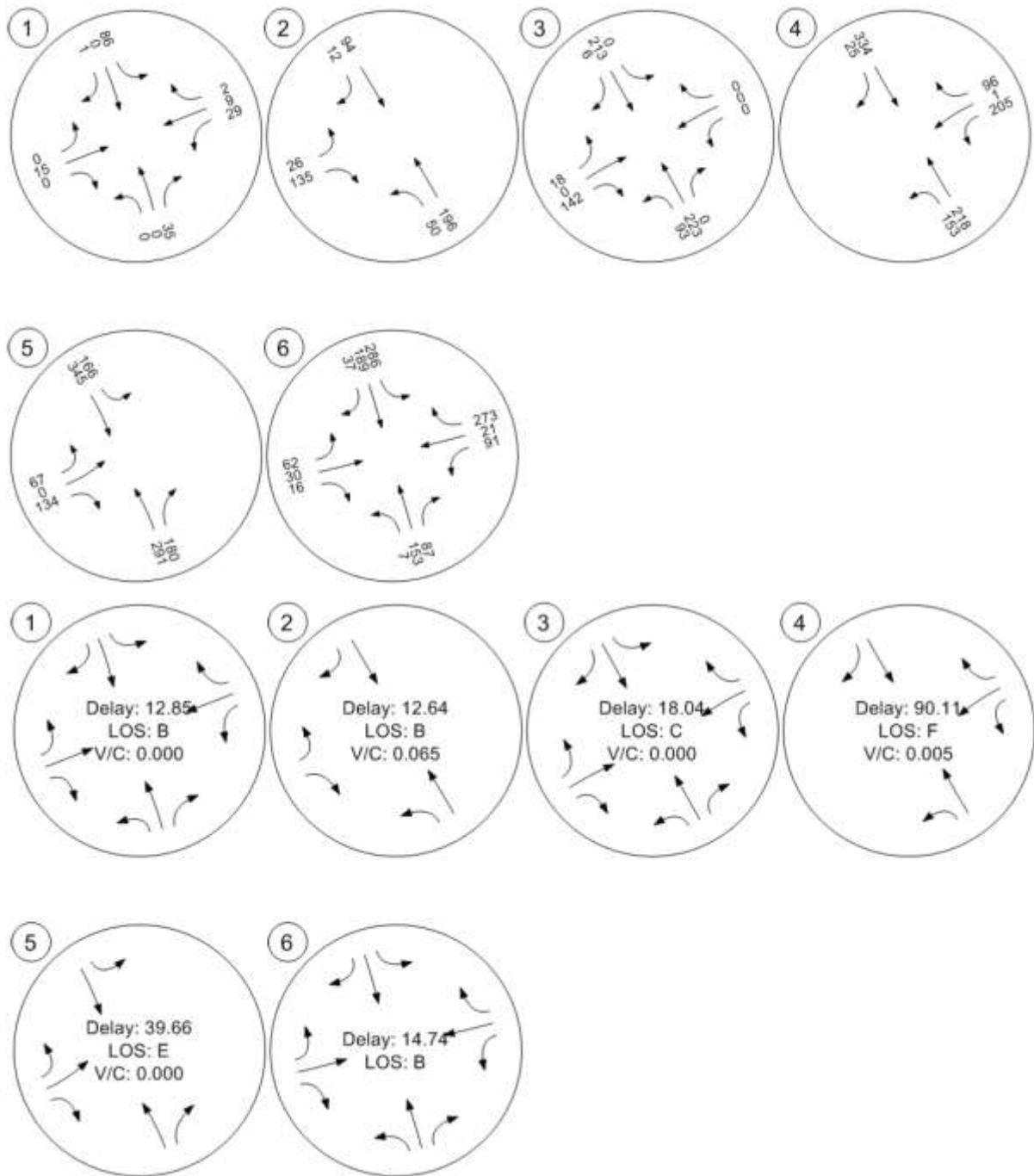


Figure 11 – 2020 PM Build Peak Hour Traffic Volumes and LOS

5. FAIR SHARE ANALYSIS

A fair share analysis was performed to determine the proportion of site traffic traveling through the Atrisco Vista and I-40 North (westbound) ramp and I-40 South (eastbound)

ramp intersections. A table showing the 2020 No Build volumes and the 2020 Build volumes is below. The table shows the increase in trips at the intersection due to the proposed development.

Table 9 shows the proposed development contributes 4.3% of the traffic to the Atrisco Vista and I-40 North (westbound) ramp intersection in the 2020 Build scenario. Also of note, the proposed development does not contribute any traffic to the westbound left/through movement, the movement with the high delay and poor LOS.

Table 9 – Fair Share Contribution Analysis – Atrisco Vista and I-40 North Ramp							
	WB			SB		NB	
AM Peak Hour	Left	Thru	Right	Right	Thru	Left	Thru
2020 No Build	208	1	114	34	177	87	133
2020 Build	208	1	127	38	182	87	152
Fed Ex Contribution - Intersection				41			
Fed Ex Contribution - Intersection %				5.2%			
	WB			SB		NB	
PM Peak Hour	Left	Thru	Right	Right	Thru	Left	Thru
2020 No Build	205	1	87	25	315	153	209
2020 Build	205	1	96	25	334	153	218
Fed Ex Contribution - Intersection				37			
Fed Ex Contribution - Intersection %				3.6%			
Average Peak Hour by Intersection %				4.3%			

Table 10 shows the proposed development contributes 2.4% of the traffic to the Atrisco Vista and I-40 South (eastbound) ramp intersection in the 2020 Build scenario.

Table 10 – Fair Share Contribution Analysis – Atrisco Vista and I-40 South Ramp							
	EB			SB		NB	
AM Peak Hour	Left	Thru	Right	Left	Thru	Right	Thru
2020 No Build	13	1	55	56	299	310	202
2020 Build	15	1	55	61	299	310	219
Fed Ex Contribution - Intersection				24			
Fed Ex Contribution - Intersection %				2.5%			
	EB			SB		NB	
PM Peak Hour	Left	Thru	Right	Right	Thru	Left	Thru
2020 No Build	61	0	134	159	333	180	288
2020 Build	67	0	134	166	345	180	291
Fed Ex Contribution - Intersection				28			
Fed Ex Contribution - Intersection %				2.4%			
Average Peak Hour by Intersection %				2.4%			

VII. CONCLUSIONS AND RECOMMENDATIONS

A. CONCLUSIONS

The traffic analysis shows that under existing 2017, 2020 No Build, and 2020 Build, all intersections will operate at an overall acceptable level of service (LOS), although the westbound left/through at the intersection of Atrisco Vista and the I-40 North Ramp (westbound) will operate at LOS F in the Existing 2017, 2020 No Build and 2020 Build scenarios. The eastbound left/through at the I-40 South Ramp (eastbound) will also operate at LOS E in the PM peak hour in the 2020 Build scenario.

A traffic signal is not warranted in the 2017 Existing, 2020 No Build scenarios at any of the intersections, although the Atrisco Vista and I-40 North (westbound) ramp intersection peak hour volume warranted would be satisfied in 2022 without development of the site.

With the addition of the build traffic, the intersection of Atrisco Vista and I-40 North Ramp (westbound) satisfies the peak hour volume and peak hour delay warrant. The proposed development generates 4.3% of the traffic at the Atrisco Vista and I-40 North Ramp (westbound) intersection during the peak hours.

The peak hour volume and delay warrant is not satisfied at the I-40 South (eastbound) ramp in any of the peak hours. The development contributes 2.4% of the volume to this intersection.

A northbound left turn lane is required under COA DPM and NMDOT SAMM criteria in Existing 2017, No Build 2020 and Build 2020 conditions.

B. RECOMMENDATIONS

- A northbound left turn lane should be constructed at the intersection of Atrisco Vista and Tempur Pedic Parkway. It is recommended this turn lane conform to NMDOT requirements due to the high number of trucks anticipated to use the turn lane. The expected queue is one vehicle. The NMDOT SAMM requires a 325-foot left turn lane, with a 125-foot taper, for a turn lane on a 40-MPH roadway. An additional 75 feet should be added to the left turn lane length to account for the queued truck.
- It is not recommended that a traffic signal be constructed by the developer at the intersection of Atrisco Vista and the I-40 North Ramp (westbound), as this development contributes just 4.3% of the traffic at this intersection during the peak hour. In addition, this development will be constructing the northbound left turn lane at Tempur Pedic Parkway, even though the left turn lane is required under existing traffic volumes.

- The extension of Shamrock Way across the AMAFCA channel to the site entrance will need to be constructed to County standards and will need an appropriately sized cul-de-sac available for any general public traffic that needs to turnaround.
- All designs shall satisfy Bernalillo County Public Works, NMDOT, COA, and Manual on Uniform Traffic Control Devices (MUTCD) requirements.

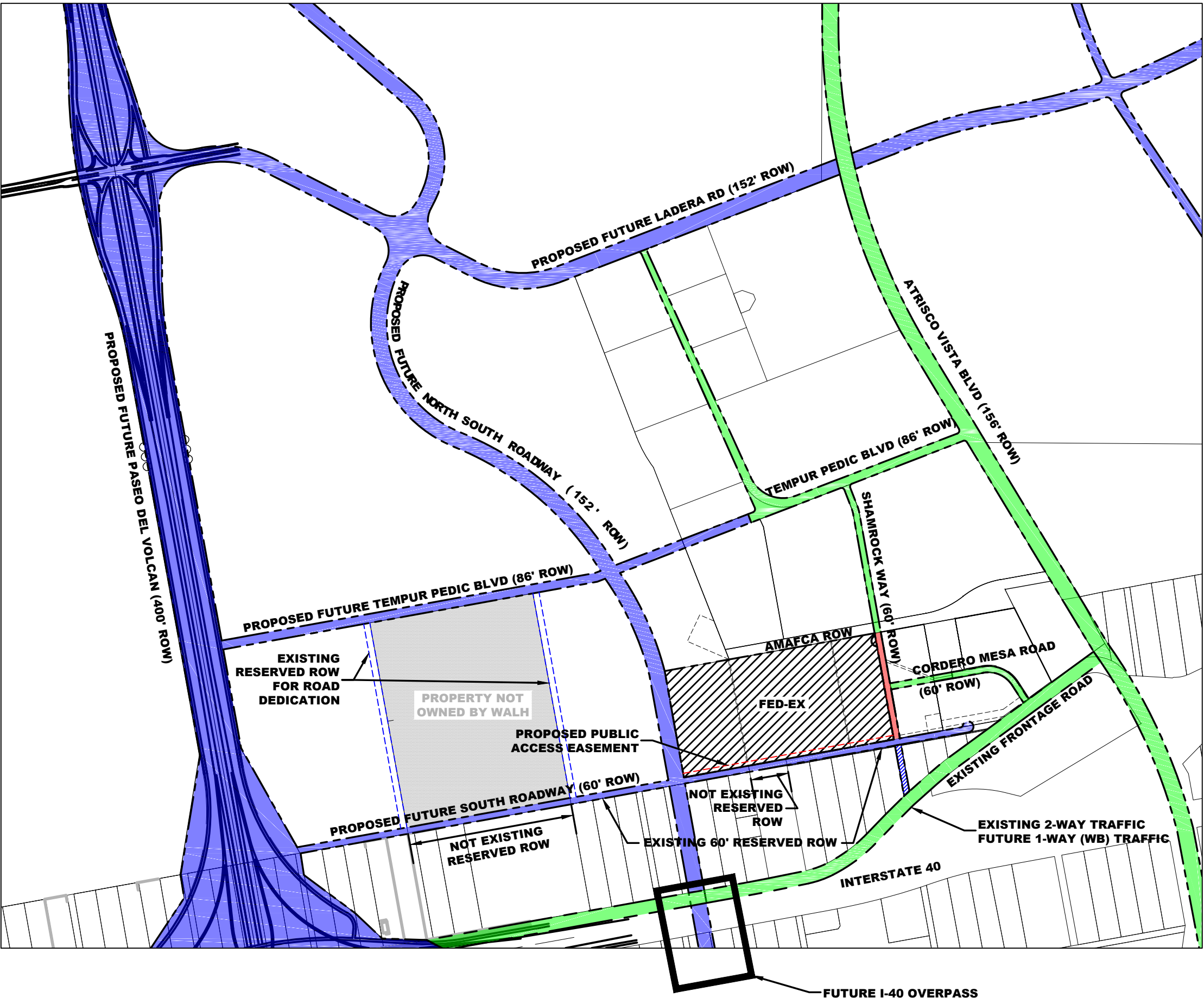
Appendix A

Future Road Network

EXHIBIT 'B'

EXISTING AND FUTURE
RIGHT-OF-WAY

November 3, 2017



LEGEND



RIGHT OF WAY TO BE GRANTED WITH THIS PLAT



EXISTING RIGHT OF WAY / EASEMENTS



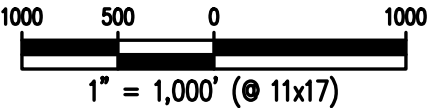
PROPOSED FUTURE RIGHT OF WAY



POSSIBLE FUTURE FRONTAGE ROAD CONNECTION (TO BE BASED ON DEVELOPMENT PLANS ASSOCIATED TRAFFIC IMPACT STUDIES AND NMDOT APPROVALS)

NOTES:

1. PLAN IS ILLUSTRATIVE ONLY AND SUBJECT TO CHANGE.
2. INTERSECTIONS MAY NEED MORE ROW THAN IMPLIED (TO BE BASED ON FUTURE TRAFFIC IMPACT STUDY).
3. PROPOSED FUTURE NETWORK AS SHOWN IS PRELIMINARY AND SCHEMATIC AND SUBJECT TO REVISION BASED ON FUTURE TRAFFIC IMPACT STUDY AND MASTERPLANNING.
4. FUTURE ROADWAYS ARE LOCATED ON MULTIPLE LAND OWNERSHIPS.



Appendix B
Bernalillo County Traffic Study
Scoping Report

Traffic Scoping Report Form

Name: _____ Date: May 25, 2017
PWSR: _____
Phone: _____ Permit/Case No.: _____

Site Information

Site Address: _____ Build Out/Implementation Year: 2018

Land Use: Package Distribution Facility

New ☒ Change of Use () Unchanged () Same use, increased/decreased intensity ()

Business Hours/Days: 24 hours a day/7 days a week

Number of Employees/Daily Visitors/Customers/Attendees: approximately 133 employees

Bldg. Size (sq. ft.): 67,500 initial, 90,000 full expansion

Expected Traffic Volumes and Vehicle Mix (cars, trucks, buses):

Approximately 602 vehicles a day - 336 delivery vehicles and 266 passenger (employee vehicles) - estimated 51 total trips in AM peak hour and 43 in PM peak hour

Roadway Information

Driveway(s) Located On: Extension of Shamrock Road south of Tempur Pedic Parkway (cul-de-sac)

Adjacent Roadway(s) Posted Speed(s):	Street Name: <u>Shamrock Road</u>	Speed: <u>25 MPH</u>
	Street Name: <u>Tempur Pedic Parkway</u>	Speed: <u>35 MPH (estimated)</u>

Jurisdiction of roadway(s) serving the site (NMDOT requires driveway permit):
Tempur Pedic Parkway and Atrisco Vista

Site Plan Checklist

- ▶ Site plan drawn to scale, building size in sq. ft. (show new, existing, remodel), 11" x 17"
- ▶ Access -- location and width of driveways
- ▶ Parking layout, location of bike racks, internal traffic circulation, delivery points
- ▶ Sidewalks (generally required on all collectors and arterials)
- ▶ Bike Lanes (check for designated bike routes, long range bikeway system)
- ▶ Location of multi-use trails, if applicable
- ▶ Nearby transit stops
- ▶ Adjacent roadway(s) configuration (number of lanes, lane widths, turn bays,
- ▶ Site access improvements (speed change lanes, ADA, line of sight,
- ▶ Access point(s) sight distance evaluation
- ▶ Distance from access point(s) to nearest adjacent driveways/intersections

Appendix C

Existing Traffic Counts

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH11

File Name : Atrisco Vista & Central
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

	Central Ave Eastbound					Central Ave Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
06:45	12	2	0	0	14	5	20	41	0	66	10	53	21	0	84	39	35	23	0	97	261
Total	12	2	0	0	14	5	20	41	0	66	10	53	21	0	84	39	35	23	0	97	261
07:00	22	5	6	0	33	10	1	43	0	54	2	60	23	0	85	26	46	10	0	82	254
07:15	20	8	2	0	30	5	5	46	0	56	2	66	31	0	99	23	43	19	0	85	270
07:30	13	3	0	0	16	7	9	52	0	68	4	48	30	0	82	28	27	17	0	72	238
07:45	11	4	1	0	16	9	8	40	0	57	5	38	22	0	65	38	29	19	0	86	224
Total	66	20	9	0	95	31	23	181	0	235	13	212	106	0	331	115	145	65	0	325	986
08:00	3	2	1	0	6	16	1	41	1	59	3	34	22	0	59	41	38	25	0	104	228
08:15	7	8	5	0	20	10	5	36	0	51	1	48	33	0	82	48	40	14	0	102	255
08:30	19	6	1	0	26	10	6	55	0	71	2	50	21	0	73	32	23	14	0	69	239
08:45	24	6	1	0	31	10	6	35	0	51	4	28	12	0	44	39	13	15	0	67	193
Total	53	22	8	0	83	46	18	167	1	232	10	160	88	0	258	160	114	68	0	342	915
09:00	13	4	2	0	19	11	3	36	0	50	4	16	9	0	29	40	14	15	0	69	167
09:15	10	6	0	0	16	4	5	49	0	58	1	22	6	0	29	35	10	24	0	69	172
09:30	11	6	2	0	19	14	7	41	0	62	3	20	18	0	41	41	10	20	0	71	193
*** BREAK ***																					
Total	34	16	4	0	54	29	15	126	0	170	8	58	33	0	99	116	34	59	0	209	532
*** BREAK ***																					
11:00	27	10	4	0	41	13	5	48	0	66	1	13	9	0	23	42	12	22	0	76	206
11:15	17	3	2	0	22	10	10	38	0	58	2	19	9	0	30	44	11	16	0	71	181
11:30	22	8	2	0	32	13	4	56	0	73	3	17	10	0	30	49	20	14	0	83	218
11:45	16	6	2	0	24	7	8	47	1	63	2	12	10	0	24	52	9	15	0	76	187
Total	82	27	10	0	119	43	27	189	1	260	8	61	38	0	107	187	52	67	0	306	792
12:00	16	4	3	0	23	13	5	56	0	74	0	13	17	0	30	48	12	15	0	75	202
12:15	22	7	3	0	32	11	8	39	0	58	0	13	20	0	33	44	14	18	0	76	199
12:30	25	9	2	0	36	8	3	56	0	67	0	26	18	0	44	33	18	15	0	66	213
12:45	13	9	3	0	25	17	9	50	0	76	0	8	16	0	24	58	16	22	0	96	221
Total	76	29	11	0	116	49	25	201	0	275	0	60	71	0	131	183	60	70	0	313	835
13:00	19	9	0	0	28	11	10	47	0	68	0	21	11	0	32	45	26	10	0	81	209
13:15	18	7	0	0	25	17	14	61	0	92	4	12	15	0	31	44	14	29	0	87	235
13:30	12	5	2	0	19	14	11	55	0	80	4	14	17	0	35	40	23	23	0	86	220
13:45	22	5	1	0	28	11	3	48	0	62	2	16	9	0	27	33	11	18	0	62	179
Total	71	26	3	0	100	53	38	211	0	302	10	63	52	0	125	162	74	80	0	316	843

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH11

File Name : Atrisco Vista & Central
Site Code :
Start Date : 10/3/2017
Page No : 2

Groups Printed- Car - Truck

Start Time	Central Ave Eastbound					Central Ave Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	41	16	11	0	68	17	6	47	1	71	3	27	14	0	44	36	30	9	0	75	258
15:15	41	14	9	0	64	19	3	49	0	71	3	34	12	0	49	48	34	16	0	98	282
15:30	17	14	5	0	36	16	2	56	0	74	0	26	11	0	37	49	38	12	0	99	246
15:45	20	10	1	0	31	17	6	67	0	90	0	24	19	0	43	42	53	15	0	110	274
Total	119	54	26	0	199	69	17	219	1	306	6	111	56	0	173	175	155	52	0	382	1060
16:00	18	6	2	0	26	29	9	41	0	79	2	35	27	0	64	47	55	14	0	116	285
16:15	21	6	2	0	29	35	5	37	0	77	2	33	14	0	49	39	46	9	0	94	249
16:30	19	7	1	0	27	19	5	61	0	85	1	33	21	1	56	72	43	12	0	127	295
16:45	14	8	4	0	26	21	4	64	0	89	3	31	14	0	48	58	26	4	0	88	251
Total	72	27	9	0	108	104	23	203	0	330	8	132	76	1	217	216	170	39	0	425	1080
17:00	14	6	6	0	26	20	6	57	0	83	1	43	24	0	68	55	49	9	0	113	290
17:15	11	7	4	0	22	25	5	72	0	102	2	34	22	0	58	78	52	10	0	140	322
17:30	14	6	0	0	20	21	6	51	0	78	6	23	23	0	52	52	45	10	0	107	257
17:45	13	8	1	0	22	34	3	50	0	87	4	30	10	1	45	48	36	3	0	87	241
Total	52	27	11	0	90	100	20	230	0	350	13	130	79	1	223	233	182	32	0	447	1110
Grand Total	637	250	91	0	978	529	226	1768	3	2526	86	1040	620	2	1748	1586	1021	555	0	3162	8414
Apprch %	65.1	25.6	9.3	0		20.9	8.9	70	0.1		4.9	59.5	35.5	0.1		50.2	32.3	17.6	0		
Total %	7.6	3	1.1	0	11.6	6.3	2.7	21	0	30	1	12.4	7.4	0	20.8	18.8	12.1	6.6	0	37.6	
Car	423	203	65	0	691	504	181	1264	3	1952	63	985	596	2	1646	1105	960	330	0	2395	6684
% Car	66.4	81.2	71.4	0	70.7	95.3	80.1	71.5	100	77.3	73.3	94.7	96.1	100	94.2	69.7	94	59.5	0	75.7	79.4
Truck	214	47	26	0	287	25	45	504	0	574	23	55	24	0	102	481	61	225	0	767	1730
% Truck	33.6	18.8	28.6	0	29.3	4.7	19.9	28.5	0	22.7	26.7	5.3	3.9	0	5.8	30.3	6	40.5	0	24.3	20.6

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH11

File Name : Atrisco Vista & Central
Site Code :
Start Date : 10/3/2017
Page No : 3

	Central Ave Eastbound				Central Ave Westbound				Atrisco Vista Blvd Northbound				Atrisco Vista Blvd Southbound				
Start Time	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 06:45 to 09:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 06:45																	
06:45	12	2	0	14	5	20	41	66	10	53	21	84	39	35	23	97	261
07:00	22	5	6	33	10	1	43	54	2	60	23	85	26	46	10	82	254
07:15	20	8	2	30	5	5	46	56	2	66	31	99	23	43	19	85	270
07:30	13	3	0	16	7	9	52	68	4	48	30	82	28	27	17	72	238
Total Volume	67	18	8	93	27	35	182	244	18	227	105	350	116	151	69	336	1023
% App. Total	72	19.4	8.6		11.1	14.3	74.6		5.1	64.9	30		34.5	44.9	20.5		
PHF	.761	.563	.333	.705	.675	.438	.875	.897	.450	.860	.847	.884	.744	.821	.750	.866	.947
Car	46	16	8	70	25	35	132	192	15	223	103	341	87	147	60	294	897
% Car	68.7	88.9	100	75.3	92.6	100	72.5	78.7	83.3	98.2	98.1	97.4	75.0	97.4	87.0	87.5	87.7
Truck	21	2	0	23	2	0	50	52	3	4	2	9	29	4	9	42	126
% Truck	31.3	11.1	0	24.7	7.4	0	27.5	21.3	16.7	1.8	1.9	2.6	25.0	2.6	13.0	12.5	12.3
Peak Hour Analysis From 10:00 to 13:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 12:45																	
12:45	13	9	3	25	17	9	50	76	0	8	16	24	58	16	22	96	221
13:00	19	9	0	28	11	10	47	68	0	21	11	32	45	26	10	81	209
13:15	18	7	0	25	17	14	61	92	4	12	15	31	44	14	29	87	235
13:30	12	5	2	19	14	11	55	80	4	14	17	35	40	23	23	86	220
Total Volume	62	30	5	97	59	44	213	316	8	55	59	122	187	79	84	350	885
% App. Total	63.9	30.9	5.2		18.7	13.9	67.4		6.6	45.1	48.4		53.4	22.6	24		
PHF	.816	.833	.417	.866	.868	.786	.873	.859	.500	.655	.868	.871	.806	.760	.724	.911	.941
Car	37	23	4	64	59	33	145	237	5	51	57	113	121	71	34	226	640
% Car	59.7	76.7	80.0	66.0	100	75.0	68.1	75.0	62.5	92.7	96.6	92.6	64.7	89.9	40.5	64.6	72.3
Truck	25	7	1	33	0	11	68	79	3	4	2	9	66	8	50	124	245
% Truck	40.3	23.3	20.0	34.0	0	25.0	31.9	25.0	37.5	7.3	3.4	7.4	35.3	10.1	59.5	35.4	27.7
Peak Hour Analysis From 14:00 to 17:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 16:30																	
16:30	19	7	1	27	19	5	61	85	1	33	21	55	72	43	12	127	294
16:45	14	8	4	26	21	4	64	89	3	31	14	48	58	26	4	88	251
17:00	14	6	6	26	20	6	57	83	1	43	24	68	55	49	9	113	290
17:15	11	7	4	22	25	5	72	102	2	34	22	58	78	52	10	140	322
Total Volume	58	28	15	101	85	20	254	359	7	141	81	229	263	170	35	468	1157
% App. Total	57.4	27.7	14.9		23.7	5.6	70.8		3.1	61.6	35.4		56.2	36.3	7.5		
PHF	.763	.875	.625	.935	.850	.833	.882	.880	.583	.820	.844	.842	.843	.817	.729	.836	.898
Car	52	24	9	85	80	17	203	300	6	137	79	222	204	164	30	398	1005
% Car	89.7	85.7	60.0	84.2	94.1	85.0	79.9	83.6	85.7	97.2	97.5	96.9	77.6	96.5	85.7	85.0	86.9
Truck	6	4	6	16	5	3	51	59	1	4	2	7	59	6	5	70	152
% Truck	10.3	14.3	40.0	15.8	5.9	15.0	20.1	16.4	14.3	2.8	2.5	3.1	22.4	3.5	14.3	15.0	13.1

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH12

File Name : Atrisco Vista & I-40 South Ramps
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

Start Time	I-40 EB On Ramp Eastbound					I-40 EB Off Ramp Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
06:45	0	1	18	0	19	0	0	0	0	0	0	51	62	0	113	6	81	0	0	87	219
Total	0	1	18	0	19	0	0	0	0	0	0	51	62	0	113	6	81	0	0	87	219
07:00	5	0	10	0	15	0	0	0	1	1	0	34	90	0	124	20	71	0	0	91	231
07:15	0	0	11	0	11	0	0	0	0	0	0	50	80	0	130	12	70	0	0	82	223
07:30	7	0	12	0	19	0	0	0	1	1	0	54	58	0	112	14	57	0	0	71	203
07:45	4	0	18	1	23	0	0	0	0	0	0	45	42	0	87	14	69	0	0	83	193
Total	16	0	51	1	68	0	0	0	2	2	0	183	270	0	453	60	267	0	0	327	850
08:00	7	0	15	0	22	0	0	0	1	1	0	39	41	0	80	12	87	0	0	99	202
08:15	5	1	16	0	22	0	0	0	0	0	0	39	47	0	86	17	84	0	0	101	209
08:30	3	0	16	0	19	0	0	0	0	0	0	54	70	0	124	19	50	0	0	69	212
08:45	3	0	19	0	22	0	0	0	0	0	0	37	49	0	86	22	47	0	0	69	177
Total	18	1	66	0	85	0	0	0	1	1	0	169	207	0	376	70	268	0	0	338	800
09:00	6	0	12	0	18	0	0	0	1	1	0	29	35	0	64	17	57	0	0	74	157
09:15	1	0	18	0	19	0	0	0	0	0	0	48	32	0	80	21	50	0	0	71	170
09:30	3	2	16	0	21	0	0	0	0	0	0	41	30	0	71	21	51	0	0	72	164
*** BREAK ***																					
Total	10	2	46	0	58	0	0	0	1	1	0	118	97	0	215	59	158	0	0	217	491
*** BREAK ***																					
11:00	13	0	16	1	30	0	0	0	0	0	0	41	47	0	88	22	59	0	0	81	199
11:15	2	2	21	1	26	0	0	0	0	0	0	37	38	0	75	14	46	0	0	60	161
11:30	4	0	21	0	25	0	0	0	0	0	0	57	37	0	94	25	58	0	0	83	202
11:45	9	1	12	0	22	0	0	0	1	1	0	44	35	0	79	33	59	0	0	92	194
Total	28	3	70	2	103	0	0	0	1	1	0	179	157	0	336	94	222	0	0	316	756
12:00	4	0	19	0	23	0	0	0	0	0	0	39	39	0	78	38	58	0	0	96	197
12:15	4	1	20	0	25	0	0	0	0	0	0	37	34	0	71	20	50	0	0	70	166
12:30	2	1	21	0	24	0	0	0	0	0	0	53	50	0	103	20	47	0	0	67	194
12:45	5	1	25	0	31	0	0	0	0	0	0	37	32	0	69	31	70	0	0	101	201
Total	15	3	85	0	103	0	0	0	0	0	0	166	155	0	321	109	225	0	0	334	758
13:00	9	0	18	0	27	0	0	0	0	0	0	46	36	0	82	23	61	0	0	84	193
13:15	6	0	27	0	33	0	0	0	1	1	0	45	43	0	88	30	57	0	0	87	209
13:30	2	0	20	0	22	0	0	0	1	1	0	40	39	0	79	23	64	0	0	87	189
13:45	5	1	14	0	20	0	0	0	0	0	0	47	38	0	85	24	45	0	0	69	174
Total	22	1	79	0	102	0	0	0	2	2	0	178	156	0	334	100	227	0	0	327	765

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH12

File Name : Atrisco Vista & I-40 South Ramps
Site Code :
Start Date : 10/3/2017
Page No : 2

Groups Printed- Car - Truck

Start Time	I-40 EB On Ramp Eastbound					I-40 EB Off Ramp Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	13	0	19	1	33	0	0	0	1	1	0	53	58	0	111	25	49	0	0	74	219
15:15	10	2	24	1	37	0	0	0	0	0	0	55	58	0	113	38	73	0	0	111	261
15:30	8	1	25	0	34	0	0	0	0	0	0	41	61	0	102	28	72	0	0	100	236
15:45	9	1	18	0	28	0	0	0	0	0	0	57	47	0	104	29	94	0	0	123	255
Total	40	4	86	2	132	0	0	0	1	1	0	206	224	0	430	120	288	0	0	408	971
16:00	8	1	28	0	37	0	0	0	0	0	0	45	53	0	98	30	77	0	0	107	242
16:15	13	0	31	0	44	0	0	0	0	0	0	41	47	0	88	20	63	0	0	83	215
16:30	14	0	31	0	45	0	0	0	0	0	0	52	52	0	104	49	92	0	0	141	290
16:45	14	0	22	0	36	0	0	0	0	0	0	63	49	0	112	35	63	0	0	98	246
Total	49	1	112	0	162	0	0	0	0	0	0	201	201	0	402	134	295	0	0	429	993
17:00	22	0	28	0	50	0	0	0	0	0	0	77	36	0	113	47	86	0	0	133	296
17:15	7	0	44	0	51	0	0	0	1	1	0	77	31	0	108	18	90	0	0	108	268
17:30	8	0	26	0	34	0	0	0	0	0	0	47	40	0	87	23	75	0	0	98	219
17:45	7	1	28	0	36	0	0	0	1	1	0	61	33	0	94	16	60	0	0	76	207
Total	44	1	126	0	171	0	0	0	2	2	0	262	140	0	402	104	311	0	0	415	990
Grand Total	242	17	739	5	1003	0	0	0	10	10	0	1713	1669	0	3382	856	2342	0	0	3198	7593
Apprch %	24.1	1.7	73.7	0.5		0	0	0	100		0	50.7	49.3	0		26.8	73.2	0	0		
Total %	3.2	0.2	9.7	0.1	13.2	0	0	0	0.1	0.1	0	22.6	22	0	44.5	11.3	30.8	0	0	42.1	
Car	212	6	482	5	705	0	0	0	10	10	0	1413	1224	0	2637	737	1840	0	0	2577	5929
% Car	87.6	35.3	65.2	100	70.3	0	0	0	100	100	0	82.5	73.3	0	78	86.1	78.6	0	0	80.6	78.1
Truck	30	11	257	0	298	0	0	0	0	0	0	300	445	0	745	119	502	0	0	621	1664
% Truck	12.4	64.7	34.8	0	29.7	0	0	0	0	0	0	17.5	26.7	0	22	13.9	21.4	0	0	19.4	21.9

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH12

File Name : Atrisco Vista & I-40 South Ramps
Site Code :
Start Date : 10/3/2017
Page No : 3

	I-40 EB On Ramp Eastbound				I-40 EB Off Ramp Westbound				Atrisco Vista Blvd Northbound				Atrisco Vista Blvd Southbound				
Start Time	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 06:45 to 09:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 06:45																	
06:45	0	1	18	19	0	0	0	0	0	51	62	113	6	81	0	87	219
07:00	5	0	10	15	0	0	0	0	0	34	90	124	20	71	0	91	230
07:15	0	0	11	11	0	0	0	0	0	50	80	130	12	70	0	82	223
07:30	7	0	12	19	0	0	0	0	0	54	58	112	14	57	0	71	202
Total Volume	12	1	51	64	0	0	0	0	0	189	290	479	52	279	0	331	874
% App. Total	18.8	1.6	79.7		0	0	0		0	39.5	60.5		15.7	84.3	0		
PHF	.429	.250	.708	.842	.000	.000	.000	.000	.000	.875	.806	.921	.650	.861	.000	.909	.950
Car	10	0	36	46	0	0	0	0	0	154	251	405	46	252	0	298	749
% Car	83.3	0	70.6	71.9	0	0	0	0	0	81.5	86.6	84.6	88.5	90.3	0	90.0	85.7
Truck	2	1	15	18	0	0	0	0	0	35	39	74	6	27	0	33	125
% Truck	16.7	100	29.4	28.1	0	0	0	0	0	18.5	13.4	15.4	11.5	9.7	0	10.0	14.3

Peak Hour Analysis From 10:00 to 13:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 12:30

12:30	2	1	21	24	0	0	0	0	0	53	50	103	20	47	0	67	194
12:45	5	1	25	31	0	0	0	0	0	37	32	69	31	70	0	101	201
13:00	9	0	18	27	0	0	0	0	0	46	36	82	23	61	0	84	193
13:15	6	0	27	33	0	0	0	0	0	45	43	88	30	57	0	87	208
Total Volume	22	2	91	115	0	0	0	0	0	181	161	342	104	235	0	339	796
% App. Total	19.1	1.7	79.1		0	0	0		0	52.9	47.1		30.7	69.3	0		
PHF	.611	.500	.843	.871	.000	.000	.000	.000	.000	.854	.805	.830	.839	.839	.000	.839	.957
Car	19	1	59	79	0	0	0	0	0	141	98	239	87	155	0	242	560
% Car	86.4	50.0	64.8	68.7	0	0	0	0	0	77.9	60.9	69.9	83.7	66.0	0	71.4	70.4
Truck	3	1	32	36	0	0	0	0	0	40	63	103	17	80	0	97	236
% Truck	13.6	50.0	35.2	31.3	0	0	0	0	0	22.1	39.1	30.1	16.3	34.0	0	28.6	29.6

Peak Hour Analysis From 14:00 to 17:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 16:30

16:30	14	0	31	45	0	0	0	0	0	52	52	104	49	92	0	141	290
16:45	14	0	22	36	0	0	0	0	0	63	49	112	35	63	0	98	246
17:00	22	0	28	50	0	0	0	0	0	77	36	113	47	86	0	133	296
17:15	7	0	44	51	0	0	0	0	0	77	31	108	18	90	0	108	267
Total Volume	57	0	125	182	0	0	0	0	0	269	168	437	149	331	0	480	1099
% App. Total	31.3	0	68.7		0	0	0		0	61.6	38.4		31	69	0		
PHF	.648	.000	.710	.892	.000	.000	.000	.000	.000	.873	.808	.967	.760	.899	.000	.851	.928
Car	54	0	93	147	0	0	0	0	0	246	131	377	141	295	0	436	960
% Car	94.7	0	74.4	80.8	0	0	0	0	0	91.4	78.0	86.3	94.6	89.1	0	90.8	87.4
Truck	3	0	32	35	0	0	0	0	0	23	37	60	8	36	0	44	139
% Truck	5.3	0	25.6	19.2	0	0	0	0	0	8.6	22.0	13.7	5.4	10.9	0	9.2	12.6

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH15

File Name : Atrisco Vista & I-40 North Ramps
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

Start Time	I-40 WB On Ramp Eastbound					I-40 WB Off Ramp Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
06:45	0	0	0	1	1	59	1	19	0	79	23	28	0	0	51	0	26	13	0	39	170
Total	0	0	0	1	1	59	1	19	0	79	23	28	0	0	51	0	26	13	0	39	170
07:00	0	0	0	0	0	54	0	12	0	66	21	19	0	1	41	0	40	15	0	55	162
07:15	0	0	0	0	0	47	0	20	0	67	33	22	0	0	55	0	38	18	0	56	178
07:30	0	0	0	0	0	38	0	22	1	61	17	43	0	0	60	0	31	12	0	43	164
07:45	0	0	0	1	1	45	0	30	0	75	18	33	0	0	51	0	42	9	0	51	178
Total	0	0	0	1	1	184	0	84	1	269	89	117	0	1	207	0	151	54	0	205	682
08:00	0	0	0	0	0	52	0	26	1	79	16	29	0	0	45	0	45	8	0	53	177
08:15	0	0	0	0	0	57	1	24	0	82	18	27	0	0	45	0	46	9	0	55	182
08:30	0	0	0	0	0	40	0	27	0	67	29	35	0	0	64	0	32	6	0	38	169
08:45	0	0	0	0	0	37	0	15	0	52	22	20	0	0	42	0	33	6	0	39	133
Total	0	0	0	0	0	186	1	92	1	280	85	111	0	0	196	0	156	29	0	185	661
09:00	0	0	0	1	1	36	1	14	0	51	16	20	0	0	36	0	39	7	0	46	134
09:15	0	0	0	0	0	36	0	21	0	57	30	21	0	0	51	0	32	5	0	37	145
09:30	0	0	0	0	0	39	0	21	0	60	24	24	0	0	48	0	34	8	0	42	150
*** BREAK ***																					
Total	0	0	0	1	1	111	1	56	0	168	70	65	0	0	135	0	105	20	0	125	429
*** BREAK ***																					
11:00	0	0	0	0	0	53	0	18	0	71	29	29	0	0	58	0	30	5	0	35	164
11:15	0	0	0	0	0	36	0	25	0	61	22	19	0	0	41	0	23	2	0	25	127
11:30	0	0	0	0	0	35	2	21	0	58	34	28	0	0	62	0	53	7	0	60	180
11:45	0	0	0	0	0	38	0	27	0	65	25	28	0	0	53	0	59	7	0	66	184
Total	0	0	0	0	0	162	2	91	0	255	110	104	0	0	214	0	165	21	0	186	655
12:00	0	0	0	0	0	31	1	23	0	55	25	23	0	0	48	0	62	3	1	66	169
12:15	0	0	0	0	0	42	0	29	0	71	25	20	0	0	45	0	36	5	0	41	157
12:30	0	0	0	0	0	32	0	22	0	54	27	30	0	0	57	0	36	7	0	43	154
12:45	0	0	0	0	0	54	1	34	0	89	22	21	0	0	43	0	47	3	0	50	182
Total	0	0	0	0	0	159	2	108	0	269	99	94	0	0	193	0	181	18	1	200	662
13:00	0	0	0	0	0	51	0	37	0	88	31	25	0	0	56	0	36	7	0	43	187
13:15	0	0	0	0	0	44	0	27	0	71	29	27	0	0	56	0	44	3	0	47	174
13:30	0	0	0	0	0	46	0	17	0	63	28	16	0	0	44	0	40	3	0	43	150
13:45	0	0	0	0	0	37	2	15	0	54	31	20	0	0	51	0	37	4	0	41	146
Total	0	0	0	0	0	178	2	96	0	276	119	88	0	0	207	0	157	17	0	174	657

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH15

File Name : Atrisco Vista & I-40 North Ramps
Site Code :
Start Date : 10/3/2017
Page No : 2

Groups Printed- Car - Truck

Start Time	I-40 WB On Ramp Eastbound					I-40 WB Off Ramp Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	0	0	0	1	1	33	0	24	1	58	25	45	0	0	70	0	42	6	0	48	177
15:15	0	0	0	0	0	58	1	23	0	82	25	42	0	0	67	0	52	0	0	52	201
15:30	0	0	0	0	0	46	0	16	0	62	27	30	0	0	57	0	55	9	0	64	183
15:45	0	0	0	0	0	67	0	30	0	97	38	32	0	0	70	0	58	2	0	60	227
Total	0	0	0	1	1	204	1	93	1	299	115	149	0	0	264	0	207	17	0	224	788
16:00	0	0	0	0	0	63	1	18	0	82	27	28	0	0	55	0	45	8	0	53	190
16:15	0	0	0	0	0	44	2	15	0	61	23	32	0	0	55	0	39	6	0	45	161
16:30	0	0	0	0	0	46	1	20	0	67	34	34	0	0	68	0	98	8	0	106	241
16:45	0	0	0	0	0	39	0	30	0	69	32	47	0	0	79	0	61	6	0	67	215
Total	0	0	0	0	0	192	4	83	0	279	116	141	0	0	257	0	243	28	0	271	807
17:00	0	0	0	0	0	48	0	19	0	67	39	63	0	0	102	0	86	3	0	89	258
17:15	0	0	0	0	0	59	0	12	0	71	38	51	0	0	89	0	49	6	0	55	215
17:30	0	0	0	0	0	43	1	15	1	60	31	26	0	0	57	0	57	3	0	60	177
17:45	0	0	0	1	1	47	0	16	0	63	33	38	0	0	71	0	32	9	1	42	177
Total	0	0	0	1	1	197	1	62	1	261	141	178	0	0	319	0	224	21	1	246	827
Grand Total	0	0	0	5	5	1632	15	784	4	2435	967	1075	0	1	2043	0	1615	238	2	1855	6338
Apprch %	0	0	0	100		67	0.6	32.2	0.2		47.3	52.6	0	0		0	87.1	12.8	0.1		
Total %	0	0	0	0.1	0.1	25.7	0.2	12.4	0.1	38.4	15.3	17	0	0	32.2	0	25.5	3.8	0	29.3	
Car	0	0	0	5	5	1205	9	671	4	1889	741	961	0	1	1703	0	1420	217	2	1639	5236
% Car	0	0	0	100	100	73.8	60	85.6	100	77.6	76.6	89.4	0	100	83.4	0	87.9	91.2	100	88.4	82.6
Truck	0	0	0	0	0	427	6	113	0	546	226	114	0	0	340	0	195	21	0	216	1102
% Truck	0	0	0	0	0	26.2	40	14.4	0	22.4	23.4	10.6	0	0	16.6	0	12.1	8.8	0	11.6	17.4

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH15

File Name : Atrisco Vista & I-40 North Ramps

Site Code :

Start Date : 10/3/2017

Page No : 3

	I-40 WB On Ramp Eastbound				I-40 WB Off Ramp Westbound				Atrisco Vista Blvd Northbound				Atrisco Vista Blvd Southbound				
Start Time	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 06:45 to 09:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:45																	
07:45	0	0	0	0	45	0	30	75	18	33	0	51	0	42	9	51	177
08:00	0	0	0	0	52	0	26	78	16	29	0	45	0	45	8	53	176
08:15	0	0	0	0	57	1	24	82	18	27	0	45	0	46	9	55	182
08:30	0	0	0	0	40	0	27	67	29	35	0	64	0	32	6	38	169
Total Volume	0	0	0	0	194	1	107	302	81	124	0	205	0	165	32	197	704
% App. Total	0	0	0		64.2	0.3	35.4		39.5	60.5	0		0	83.8	16.2		
PHF	.000	.000	.000	.000	.851	.250	.892	.921	.698	.886	.000	.801	.000	.897	.889	.895	.967
Car	0	0	0	0	137	0	94	231	65	112	0	177	0	144	32	176	584
% Car	0	0	0	0	70.6	0	87.9	76.5	80.2	90.3	0	86.3	0	87.3	100	89.3	83.0
Truck	0	0	0	0	57	1	13	71	16	12	0	28	0	21	0	21	120
% Truck	0	0	0	0	29.4	100	12.1	23.5	19.8	9.7	0	13.7	0	12.7	0	10.7	17.0

Peak Hour Analysis From 10:00 to 13:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 12:30

12:30	0	0	0	0	32	0	22	54	27	30	0	57	0	36	7	43	154
12:45	0	0	0	0	54	1	34	89	22	21	0	43	0	47	3	50	182
13:00	0	0	0	0	51	0	37	88	31	25	0	56	0	36	7	43	187
13:15	0	0	0	0	44	0	27	71	29	27	0	56	0	44	3	47	174
Total Volume	0	0	0	0	181	1	120	302	109	103	0	212	0	163	20	183	697
% App. Total	0	0	0		59.9	0.3	39.7		51.4	48.6	0		0	89.1	10.9		
PHF	.000	.000	.000	.000	.838	.250	.811	.848	.879	.858	.000	.930	.000	.867	.714	.915	.932
Car	0	0	0	0	103	1	92	196	79	90	0	169	0	142	18	160	525
% Car	0	0	0	0	56.9	100	76.7	64.9	72.5	87.4	0	79.7	0	87.1	90.0	87.4	75.3
Truck	0	0	0	0	78	0	28	106	30	13	0	43	0	21	2	23	172
% Truck	0	0	0	0	43.1	0	23.3	35.1	27.5	12.6	0	20.3	0	12.9	10.0	12.6	24.7

Peak Hour Analysis From 14:00 to 17:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 16:30

16:30	0	0	0	0	46	1	20	67	34	34	0	68	0	98	8	106	241
16:45	0	0	0	0	39	0	30	69	32	47	0	79	0	61	6	67	215
17:00	0	0	0	0	48	0	19	67	39	63	0	102	0	86	3	89	258
17:15	0	0	0	0	59	0	12	71	38	51	0	89	0	49	6	55	215
Total Volume	0	0	0	0	192	1	81	274	143	195	0	338	0	294	23	317	929
% App. Total	0	0	0		70.1	0.4	29.6		42.3	57.7	0		0	92.7	7.3		
PHF	.000	.000	.000	.000	.814	.250	.675	.965	.917	.774	.000	.828	.000	.750	.719	.748	.900
Car	0	0	0	0	171	1	76	248	127	187	0	314	0	275	21	296	858
% Car	0	0	0	0	89.1	100	93.8	90.5	88.8	95.9	0	92.9	0	93.5	91.3	93.4	92.4
Truck	0	0	0	0	21	0	5	26	16	8	0	24	0	19	2	21	71
% Truck	0	0	0	0	10.9	0	6.2	9.5	11.2	4.1	0	7.1	0	6.5	8.7	6.6	7.6

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH10

File Name : Atrisco Vista & Frontage Rd
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

Start Time	Frontage Rd Eastbound					Frontage Rd Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
06:45	0	0	5	0	5	0	0	0	0	0	22	25	0	0	47	0	27	7	0	34	86
Total	0	0	5	0	5	0	0	0	0	0	22	25	0	0	47	0	27	7	0	34	86
07:00	0	0	2	0	2	0	0	0	0	0	14	17	0	0	31	0	42	10	0	52	85
07:15	1	0	6	0	7	0	0	0	0	0	19	24	0	0	43	0	41	9	0	50	100
07:30	0	0	6	1	7	0	0	0	0	0	19	45	0	0	64	0	36	7	0	43	114
07:45	0	0	6	1	7	0	0	0	0	0	31	34	0	0	65	0	46	11	1	58	130
Total	1	0	20	2	23	0	0	0	0	0	83	120	0	0	203	0	165	37	1	203	429
08:00	1	0	9	0	10	0	0	0	1	1	23	30	0	0	53	0	38	8	0	46	110
08:15	1	0	19	0	20	0	0	0	0	0	22	31	0	0	53	0	35	1	0	36	109
08:30	1	0	16	0	17	0	0	0	0	0	24	36	0	0	60	0	23	3	0	26	103
08:45	0	0	21	0	21	0	0	0	0	0	16	22	0	0	38	0	18	3	0	21	80
Total	3	0	65	0	68	0	0	0	1	1	85	119	0	0	204	0	114	15	0	129	402
09:00	1	0	27	0	28	0	0	0	0	0	17	16	0	0	33	0	19	0	0	19	80
09:15	1	0	26	0	27	0	0	0	0	0	26	17	0	0	43	0	10	3	0	13	83
09:30	2	0	13	0	15	0	0	0	0	0	25	20	0	0	45	0	29	4	0	33	93
*** BREAK ***																					
Total	4	0	66	0	70	0	0	0	0	0	68	53	0	0	121	0	58	7	0	65	256
*** BREAK ***																					
11:00	1	0	22	0	23	0	0	0	0	0	17	27	0	0	44	0	13	6	0	19	86
11:15	0	0	17	0	17	0	0	0	0	0	27	18	0	0	45	0	9	3	0	12	74
11:30	0	0	31	0	31	0	0	0	0	0	21	28	0	0	49	0	26	5	0	31	111
11:45	5	0	32	0	37	0	0	0	0	0	26	29	0	0	55	0	34	3	0	37	129
Total	6	0	102	0	108	0	0	0	0	0	91	102	0	0	193	0	82	17	0	99	400
12:00	2	0	42	1	45	0	0	0	0	0	21	25	0	0	46	0	26	3	0	29	120
12:15	3	0	22	0	25	0	0	0	0	0	28	18	0	0	46	0	18	1	0	19	90
12:30	1	0	25	0	26	0	0	0	0	0	29	24	1	0	54	0	17	5	0	22	102
12:45	2	1	33	0	36	1	1	0	0	2	32	25	1	0	58	0	16	4	0	20	116
Total	8	1	122	1	132	1	1	0	0	2	110	92	2	0	204	0	77	13	0	90	428
13:00	3	1	23	0	27	0	0	0	0	0	35	26	0	0	61	0	20	2	0	22	110
13:15	3	0	26	0	29	1	1	0	0	2	32	20	0	0	52	0	21	2	0	23	106
13:30	4	0	24	0	28	0	0	0	0	0	21	12	0	0	33	0	20	5	0	25	86
13:45	4	0	25	0	29	0	0	0	0	0	16	19	0	0	35	0	16	4	0	20	84
Total	14	1	98	0	113	1	1	0	0	2	104	77	0	0	181	0	77	13	0	90	386

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH10

File Name : Atrisco Vista & Frontage Rd

Site Code :

Start Date : 10/3/2017

Page No : 2

Groups Printed- Car - Truck

Start Time	Frontage Rd Eastbound					Frontage Rd Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	4	0	25	0	29	0	0	0	1	1	29	39	0	0	68	0	23	0	1	24	122
15:15	2	0	38	1	41	0	0	0	0	0	31	34	0	0	65	0	15	3	0	18	124
15:30	3	0	22	0	25	0	0	0	1	1	17	27	0	0	44	0	43	3	0	46	116
15:45	7	0	33	0	40	0	0	0	0	0	32	31	0	0	63	0	29	3	0	32	135
Total	16	0	118	1	135	0	0	0	2	2	109	131	0	0	240	0	110	9	1	120	497
16:00	5	0	27	0	32	0	0	0	0	0	18	31	0	0	49	0	24	3	0	27	108
16:15	4	0	18	0	22	0	0	1	0	1	15	28	1	0	44	0	29	4	0	33	100
16:30	4	0	28	0	32	0	0	0	0	0	24	33	0	0	57	0	81	3	0	84	173
16:45	2	0	33	0	35	0	0	0	0	0	24	49	0	0	73	0	29	0	0	29	137
Total	15	0	106	0	121	0	0	1	0	1	81	141	1	0	223	0	163	10	0	173	518
17:00	8	0	43	0	51	0	0	0	0	0	20	65	0	0	85	0	43	1	0	44	180
17:15	3	0	29	0	32	0	0	0	0	0	19	45	0	0	64	0	28	2	0	30	126
17:30	7	0	29	1	37	0	0	0	0	0	14	27	0	1	42	0	33	3	0	36	115
17:45	5	0	17	0	22	0	0	0	0	0	22	32	0	0	54	0	23	0	0	23	99
Total	23	0	118	1	142	0	0	0	0	0	75	169	0	1	245	0	127	6	0	133	520
Grand Total	90	2	820	5	917	2	2	1	3	8	828	1029	3	1	1861	0	1000	134	2	1136	3922
Apprch %	9.8	0.2	89.4	0.5		25	25	12.5	37.5		44.5	55.3	0.2	0.1		0	88	11.8	0.2		
Total %	2.3	0.1	20.9	0.1	23.4	0.1	0.1	0	0.1	0.2	21.1	26.2	0.1	0	47.5	0	25.5	3.4	0.1	29	
Car	82	1	691	5	779	1	2	1	3	7	696	933	3	1	1633	0	904	122	2	1028	3447
% Car	91.1	50	84.3	100	85	50	100	100	100	87.5	84.1	90.7	100	100	87.7	0	90.4	91	100	90.5	87.9
Truck	8	1	129	0	138	1	0	0	0	1	132	96	0	0	228	0	96	12	0	108	475
% Truck	8.9	50	15.7	0	15	50	0	0	0	12.5	15.9	9.3	0	0	12.3	0	9.6	9	0	9.5	12.1

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH10

File Name : Atrisco Vista & Frontage Rd
Site Code :
Start Date : 10/3/2017
Page No : 3

	Frontage Rd Eastbound				Frontage Rd Westbound				Atrisco Vista Blvd Northbound				Atrisco Vista Blvd Southbound				
Start Time	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 06:45 to 09:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30																	
07:30	0	0	6	6	0	0	0	0	19	45	0	64	0	36	7	43	113
07:45	0	0	6	6	0	0	0	0	31	34	0	65	0	46	11	57	128
08:00	1	0	9	10	0	0	0	0	23	30	0	53	0	38	8	46	109
08:15	1	0	19	20	0	0	0	0	22	31	0	53	0	35	1	36	109
Total Volume	2	0	40	42	0	0	0	0	95	140	0	235	0	155	27	182	459
% App. Total	4.8	0	95.2		0	0	0		40.4	59.6	0		0	85.2	14.8		
PHF	.500	.000	.526	.525	.000	.000	.000	.000	.766	.778	.000	.904	.000	.842	.614	.798	.896
Car	2	0	38	40	0	0	0	0	85	129	0	214	0	140	26	166	420
% Car	100	0	95.0	95.2	0	0	0	0	89.5	92.1	0	91.1	0	90.3	96.3	91.2	91.5
Truck	0	0	2	2	0	0	0	0	10	11	0	21	0	15	1	16	39
% Truck	0	0	5.0	4.8	0	0	0	0	10.5	7.9	0	8.9	0	9.7	3.7	8.8	8.5

Peak Hour Analysis From 10:00 to 13:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 11:30

11:30	0	0	31	31	0	0	0	0	21	28	0	49	0	26	5	31	111
11:45	5	0	32	37	0	0	0	0	26	29	0	55	0	34	3	37	129
12:00	2	0	42	44	0	0	0	0	21	25	0	46	0	26	3	29	119
12:15	3	0	22	25	0	0	0	0	28	18	0	46	0	18	1	19	90
Total Volume	10	0	127	137	0	0	0	0	96	100	0	196	0	104	12	116	449
% App. Total	7.3	0	92.7		0	0	0		49	51	0		0	89.7	10.3		
PHF	.500	.000	.756	.778	.000	.000	.000	.000	.857	.862	.000	.891	.000	.765	.600	.784	.870
Car	9	0	103	112	0	0	0	0	72	77	0	149	0	91	11	102	363
% Car	90.0	0	81.1	81.8	0	0	0	0	75.0	77.0	0	76.0	0	87.5	91.7	87.9	80.8
Truck	1	0	24	25	0	0	0	0	24	23	0	47	0	13	1	14	86
% Truck	10.0	0	18.9	18.2	0	0	0	0	25.0	23.0	0	24.0	0	12.5	8.3	12.1	19.2

Peak Hour Analysis From 14:00 to 17:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 16:30

16:30	4	0	28	32	0	0	0	0	24	33	0	57	0	81	3	84	173
16:45	2	0	33	35	0	0	0	0	24	49	0	73	0	29	0	29	137
17:00	8	0	43	51	0	0	0	0	20	65	0	85	0	43	1	44	180
17:15	3	0	29	32	0	0	0	0	19	45	0	64	0	28	2	30	126
Total Volume	17	0	133	150	0	0	0	0	87	192	0	279	0	181	6	187	616
% App. Total	11.3	0	88.7		0	0	0		31.2	68.8	0		0	96.8	3.2		
PHF	.531	.000	.773	.735	.000	.000	.000	.000	.906	.738	.000	.821	.000	.559	.500	.557	.856
Car	16	0	114	130	0	0	0	0	75	189	0	264	0	177	6	183	577
% Car	94.1	0	85.7	86.7	0	0	0	0	86.2	98.4	0	94.6	0	97.8	100	97.9	93.7
Truck	1	0	19	20	0	0	0	0	12	3	0	15	0	4	0	4	39
% Truck	5.9	0	14.3	13.3	0	0	0	0	13.8	1.6	0	5.4	0	2.2	0	2.1	6.3

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH18

File Name : Atrisco Vista & Tempur Pedic Pkwy
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

Start Time	Tempur Pedic Pkwy Eastbound					Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
06:45	0	0	1	0	1	0	0	0	0	0	12	15	0	0	27	0	33	0	0	33	61
Total	0	0	1	0	1	0	0	0	0	0	12	15	0	0	27	0	33	0	0	33	61
07:00	0	0	4	0	4	0	0	0	0	0	5	15	0	0	20	0	48	0	0	48	72
07:15	0	0	3	0	3	0	0	0	0	0	6	17	0	0	23	0	47	2	0	49	75
07:30	0	0	4	0	4	0	0	0	0	0	9	39	0	0	48	0	41	0	0	41	93
07:45	1	0	4	0	5	0	0	0	0	0	11	26	0	0	37	0	54	3	0	57	99
Total	1	0	15	0	16	0	0	0	0	0	31	97	0	0	128	0	190	5	0	195	339
08:00	0	0	3	0	3	0	0	0	0	0	11	20	0	0	31	0	42	2	0	44	78
08:15	0	0	1	0	1	0	0	0	0	0	6	25	0	0	31	0	36	2	0	38	70
08:30	0	0	6	0	6	0	0	0	0	0	11	26	0	0	37	0	17	3	0	20	63
08:45	0	0	7	0	7	0	0	0	0	0	7	15	0	0	22	0	16	3	0	19	48
Total	0	0	17	0	17	0	0	0	0	0	35	86	0	0	121	0	111	10	0	121	259
09:00	2	0	10	0	12	0	0	0	0	0	2	12	0	0	14	0	9	0	0	9	35
09:15	2	0	3	0	5	0	0	0	0	0	6	12	0	0	18	0	11	1	0	12	35
09:30	0	0	12	0	12	0	0	0	0	0	10	11	0	0	21	0	17	3	0	20	53
*** BREAK ***																					
Total	4	0	25	0	29	0	0	0	0	0	18	35	0	0	53	0	37	4	0	41	123
*** BREAK ***																					
11:00	1	0	6	0	7	0	0	0	0	0	7	18	0	0	25	0	12	1	0	13	45
11:15	0	0	8	0	8	0	0	0	0	0	3	18	0	0	21	0	7	0	0	7	36
11:30	3	0	7	0	10	0	0	0	0	0	3	21	0	0	24	0	19	3	0	22	56
11:45	0	0	13	0	13	0	0	0	0	0	10	23	0	0	33	0	24	0	0	24	70
Total	4	0	34	0	38	0	0	0	0	0	23	80	0	0	103	0	62	4	0	66	207
12:00	5	0	13	0	18	0	0	0	0	0	11	19	0	0	30	0	15	1	0	16	64
12:15	1	0	5	0	6	0	0	0	0	0	10	10	0	0	20	0	13	2	0	15	41
12:30	2	0	5	0	7	0	0	0	0	0	11	11	0	0	22	0	16	1	0	17	46
12:45	1	0	8	0	9	0	0	0	0	0	12	15	0	0	27	0	10	0	0	10	46
Total	9	0	31	0	40	0	0	0	0	0	44	55	0	0	99	0	54	4	0	58	197
13:00	0	0	9	0	9	0	0	0	0	0	9	20	0	0	29	0	13	1	0	14	52
13:15	2	0	12	0	14	0	0	0	0	0	8	14	0	0	22	0	11	0	0	11	47
13:30	1	0	8	0	9	0	0	0	0	0	4	11	0	0	15	0	16	0	0	16	40
13:45	2	0	6	0	8	0	0	0	0	0	8	17	0	0	25	0	15	1	0	16	49
Total	5	0	35	0	40	0	0	0	0	0	29	62	0	0	91	0	55	2	0	57	188

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH18

File Name : Atrisco Vista & Tempur Pedic Pkwy
Site Code :
Start Date : 10/3/2017
Page No : 2

Groups Printed- Car - Truck

Start Time	Tempur Pedic Pkwy Eastbound					Westbound					Atrisco Vista Blvd Northbound					Atrisco Vista Blvd Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	2	0	5	0	7	0	0	0	0	0	2	38	0	0	40	0	19	1	0	20	67
15:15	3	0	8	2	13	0	0	0	0	0	2	35	0	0	37	0	10	0	0	10	60
15:30	2	0	13	0	15	0	0	0	0	0	5	27	0	0	32	0	33	0	0	33	80
15:45	3	0	9	0	12	0	0	0	0	0	6	30	0	0	36	0	20	2	0	22	70
Total	10	0	35	2	47	0	0	0	0	0	15	130	0	0	145	0	82	3	0	85	277
16:00	3	0	5	0	8	0	0	0	0	0	5	32	0	0	37	0	23	0	0	23	68
16:15	1	0	10	0	11	0	0	0	0	0	6	26	0	0	32	0	23	1	0	24	67
16:30	10	0	67	0	77	0	0	0	0	0	4	35	0	0	39	0	22	0	0	22	138
16:45	3	0	16	0	19	0	0	0	0	0	10	41	0	0	51	0	12	5	0	17	87
Total	17	0	98	0	115	0	0	0	0	0	25	134	0	0	159	0	80	6	0	86	360
17:00	6	0	18	0	24	0	0	0	0	0	11	59	0	0	70	0	31	3	0	34	128
17:15	1	0	7	0	8	0	0	0	0	0	5	48	0	0	53	0	23	2	0	25	86
17:30	1	0	4	0	5	0	0	0	0	0	0	31	0	0	31	0	31	1	0	32	68
17:45	0	0	7	0	7	0	0	0	0	0	6	31	0	0	37	0	16	0	0	16	60
Total	8	0	36	0	44	0	0	0	0	0	22	169	0	0	191	0	101	6	0	107	342
Grand Total	58	0	327	2	387	0	0	0	0	0	254	863	0	0	1117	0	805	44	0	849	2353
Apprch %	15	0	84.5	0.5		0	0	0	0		22.7	77.3	0	0		0	94.8	5.2	0		
Total %	2.5	0	13.9	0.1	16.4	0	0	0	0	0	10.8	36.7	0	0	47.5	0	34.2	1.9	0	36.1	
Car	56	0	274	2	332	0	0	0	0	0	184	818	0	0	1002	0	748	40	0	788	2122
% Car	96.6	0	83.8	100	85.8	0	0	0	0	0	72.4	94.8	0	0	89.7	0	92.9	90.9	0	92.8	90.2
Truck	2	0	53	0	55	0	0	0	0	0	70	45	0	0	115	0	57	4	0	61	231
% Truck	3.4	0	16.2	0	14.2	0	0	0	0	0	27.6	5.2	0	0	10.3	0	7.1	9.1	0	7.2	9.8

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH18

File Name : Atrisco Vista & Tempur Pedic Pkwy
Site Code :
Start Date : 10/3/2017
Page No : 3

	Tempur Pedic Pkwy Eastbound				Westbound				Atrisco Vista Blvd Northbound				Atrisco Vista Blvd Southbound				
Start Time	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 06:45 to 09:45 - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15																	
07:15	0	0	3	3	0	0	0	0	6	17	0	23	0	47	2	49	75
07:30	0	0	4	4	0	0	0	0	9	39	0	48	0	41	0	41	93
07:45	1	0	4	5	0	0	0	0	11	26	0	37	0	54	3	57	99
08:00	0	0	3	3	0	0	0	0	11	20	0	31	0	42	2	44	78
Total Volume	1	0	14	15	0	0	0	0	37	102	0	139	0	184	7	191	345
% App. Total	6.7	0	93.3		0	0	0		26.6	73.4	0		0	96.3	3.7		
PHF	.250	.000	.875	.750	.000	.000	.000	.000	.841	.654	.000	.724	.000	.852	.583	.838	.871
Car	1	0	3	4	0	0	0	0	26	98	0	124	0	179	7	186	314
% Car	100	0	21.4	26.7	0	0	0	0	70.3	96.1	0	89.2	0	97.3	100	97.4	91.0
Truck	0	0	11	11	0	0	0	0	11	4	0	15	0	5	0	5	31
% Truck	0	0	78.6	73.3	0	0	0	0	29.7	3.9	0	10.8	0	2.7	0	2.6	9.0

Peak Hour Analysis From 10:00 to 13:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 11:30

11:30	3	0	7	10	0	0	0	0	3	21	0	24	0	19	3	22	56
11:45	0	0	13	13	0	0	0	0	10	23	0	33	0	24	0	24	70
12:00	5	0	13	18	0	0	0	0	11	19	0	30	0	15	1	16	64
12:15	1	0	5	6	0	0	0	0	10	10	0	20	0	13	2	15	41
Total Volume	9	0	38	47	0	0	0	0	34	73	0	107	0	71	6	77	231
% App. Total	19.1	0	80.9		0	0	0		31.8	68.2	0		0	92.2	7.8		
PHF	.450	.000	.731	.653	.000	.000	.000	.000	.773	.793	.000	.811	.000	.740	.500	.802	.825
Car	9	0	35	44	0	0	0	0	22	62	0	84	0	64	4	68	196
% Car	100	0	92.1	93.6	0	0	0	0	64.7	84.9	0	78.5	0	90.1	66.7	88.3	84.8
Truck	0	0	3	3	0	0	0	0	12	11	0	23	0	7	2	9	35
% Truck	0	0	7.9	6.4	0	0	0	0	35.3	15.1	0	21.5	0	9.9	33.3	11.7	15.2

Peak Hour Analysis From 14:00 to 17:45 - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 16:30

16:30	10	0	67	77	0	0	0	0	4	35	0	39	0	22	0	22	138
16:45	3	0	16	19	0	0	0	0	10	41	0	51	0	12	5	17	87
17:00	6	0	18	24	0	0	0	0	11	59	0	70	0	31	3	34	128
17:15	1	0	7	8	0	0	0	0	5	48	0	53	0	23	2	25	86
Total Volume	20	0	108	128	0	0	0	0	30	183	0	213	0	88	10	98	439
% App. Total	15.6	0	84.4		0	0	0		14.1	85.9	0		0	89.8	10.2		
PHF	.500	.000	.403	.416	.000	.000	.000	.000	.682	.775	.000	.761	.000	.710	.500	.721	.795
Car	20	0	108	128	0	0	0	0	29	181	0	210	0	83	9	92	430
% Car	100	0	100	100	0	0	0	0	96.7	98.9	0	98.6	0	94.3	90.0	93.9	97.9
Truck	0	0	0	0	0	0	0	0	1	2	0	3	0	5	1	6	9
% Truck	0	0	0	0	0	0	0	0	3.3	1.1	0	1.4	0	5.7	10.0	6.1	2.1

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH3

File Name : Tempur Pedic Pkwy & Shamrock Way
Site Code :
Start Date : 10/3/2017
Page No : 1

Groups Printed- Car - Truck

	Tempur Pedit Pkwy Eastbound					Tempur Pedit Pkwy Westbound					Shamrock Way Northbound					Tempur Pedit Entrance Southbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
06:45	0	0	0	0	0	3	3	3	0	9	0	0	1	0	1	0	0	0	0	0	10
Total	0	0	0	0	0	3	3	3	0	9	0	0	1	0	1	0	0	0	0	0	10
07:00	0	4	0	0	4	0	2	2	0	4	0	0	2	0	2	0	0	0	0	0	10
07:15	0	0	0	0	0	1	1	2	0	4	0	0	2	0	2	0	0	0	0	0	6
07:30	0	2	0	0	2	4	0	1	0	5	0	0	2	0	2	0	0	0	0	0	9
07:45	0	0	0	0	0	3	1	0	0	4	0	0	2	0	2	1	0	0	0	1	7
Total	0	6	0	0	6	8	4	5	0	17	0	0	8	0	8	1	0	0	0	1	32
08:00	0	1	0	0	1	2	2	2	0	6	0	0	2	0	2	0	0	0	0	0	9
08:15	0	1	0	0	1	1	2	0	0	3	0	0	1	0	1	0	0	0	0	0	5
08:30	0	1	0	0	1	5	3	1	0	9	0	0	4	0	4	0	0	0	0	0	14
08:45	0	1	0	0	1	2	1	0	0	3	0	0	1	0	1	1	0	0	0	1	6
Total	0	4	0	0	4	10	8	3	0	21	0	0	8	0	8	1	0	0	0	1	34
09:00	0	3	0	0	3	1	0	0	0	1	0	0	7	0	7	0	0	0	0	0	11
09:15	0	1	1	0	2	1	3	2	0	6	0	0	2	0	2	0	0	0	0	0	10
09:30	1	3	0	0	4	4	4	3	0	11	0	0	3	0	3	1	0	1	0	2	20
*** BREAK ***																					
Total	1	7	1	0	9	6	7	5	0	18	0	0	12	0	12	1	0	1	0	2	41
*** BREAK ***																					
11:00	0	1	0	0	1	3	2	0	0	5	0	0	3	0	3	0	0	0	0	0	9
11:15	0	1	0	0	1	1	1	0	0	2	0	0	2	0	2	1	0	0	0	1	6
11:30	0	1	0	0	1	2	1	0	0	3	0	0	1	0	1	0	0	0	0	0	5
11:45	0	0	0	0	0	4	2	2	0	8	0	0	1	0	1	3	0	0	0	3	12
Total	0	3	0	0	3	10	6	2	0	18	0	0	7	0	7	4	0	0	0	4	32
12:00	0	6	0	0	6	1	5	4	0	10	0	0	4	0	4	2	0	0	0	2	22
12:15	0	1	0	0	1	7	3	1	0	11	1	0	1	0	2	1	0	0	0	1	15
12:30	0	3	0	0	3	6	2	1	0	9	0	0	3	0	3	0	0	0	0	0	15
12:45	0	3	0	0	3	5	5	1	0	11	0	0	5	0	5	1	0	0	0	1	20
Total	0	13	0	0	13	19	15	7	0	41	1	0	13	0	14	4	0	0	0	4	72
13:00	0	1	0	0	1	1	1	2	0	4	0	0	3	0	3	1	0	0	0	1	9
13:15	0	6	0	0	6	3	2	1	0	6	0	0	6	0	6	1	0	0	0	1	19
13:30	0	3	0	0	3	0	0	1	0	1	0	0	3	0	3	1	0	0	0	1	8
13:45	0	0	0	0	0	3	0	1	0	4	0	0	5	0	5	0	0	0	0	0	9
Total	0	10	0	0	10	7	3	5	0	15	0	0	17	0	17	3	0	0	0	3	45

*** BREAK ***

Mike Henderson Consulting, LLC

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

Collected by: MH3

File Name : Tempur Pedic Pkwy & Shamrock Way
Site Code :
Start Date : 10/3/2017
Page No : 2

Groups Printed- Car - Truck

Start Time	Tempur Pedic Pkwy Eastbound					Tempur Pedic Pkwy Westbound					Shamrock Way Northbound					Tempur Pedic Entrance Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
15:00	0	2	0	0	2	2	1	0	0	3	0	0	2	0	2	1	0	0	0	1	8
15:15	0	1	0	0	1	1	0	0	0	1	0	0	1	0	1	2	0	0	0	2	5
15:30	0	3	0	0	3	1	1	0	0	2	0	0	3	0	3	1	0	0	0	1	9
15:45	0	0	1	0	1	3	0	1	0	4	0	0	5	0	5	0	0	0	0	0	10
Total	0	6	1	0	7	7	2	1	0	10	0	0	11	0	11	4	0	0	0	4	32
16:00	0	0	0	0	0	3	0	0	0	3	0	0	0	0	0	2	0	0	0	2	5
16:15	0	3	0	0	3	2	4	1	0	7	0	0	3	0	3	3	0	0	0	3	16
16:30	0	7	0	0	7	2	1	0	0	3	0	0	1	0	1	64	0	0	0	64	75
16:45	0	3	0	0	3	0	3	1	0	4	0	0	1	0	1	11	0	1	0	12	20
Total	0	13	0	0	13	7	8	2	0	17	0	0	5	0	5	80	0	1	0	81	116
17:00	0	1	0	0	1	5	0	0	0	5	0	0	5	0	5	2	0	0	0	2	13
17:15	0	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	0	4
17:30	0	1	0	0	1	1	0	0	0	1	0	0	0	0	0	1	0	0	0	1	3
17:45	0	2	0	0	2	1	3	0	0	4	0	0	2	0	2	0	0	0	0	0	8
Total	0	4	0	0	4	7	3	0	0	10	0	0	11	0	11	3	0	0	0	3	28
Grand Total	1	66	2	0	69	84	59	33	0	176	1	0	93	0	94	101	0	2	0	103	442
Apprch %	1.4	95.7	2.9	0		47.7	33.5	18.8	0		1.1	0	98.9	0		98.1	0	1.9	0		
Total %	0.2	14.9	0.5	0	15.6	19	13.3	7.5	0	39.8	0.2	0	21	0	21.3	22.9	0	0.5	0	23.3	
Car	1	48	1	0	50	26	45	33	0	104	0	0	58	0	58	101	0	2	0	103	315
% Car	100	72.7	50	0	72.5	31	76.3	100	0	59.1	0	0	62.4	0	61.7	100	0	100	0	100	71.3
Truck	0	18	1	0	19	58	14	0	0	72	1	0	35	0	36	0	0	0	0	0	127
% Truck	0	27.3	50	0	27.5	69	23.7	0	0	40.9	100	0	37.6	0	38.3	0	0	0	0	0	28.7

5301 Camino Sandia NE
Albuquerque, NM 87111
(505) 275-5706

File Name : Tempur Pedic Pkwy & Shamrock Way
Site Code :
Start Date : 10/3/2017
Page No : 3

12:00	0	6	0	6	1	5	4	10	0	0	4	4	2	0	0	2	22
12:15	0	1	0	1	7	3	1	11	1	0	1	2	1	0	0	1	15
12:30	0	3	0	3	6	2	1	9	0	0	3	3	0	0	0	0	15
12:45	0	3	0	3	5	5	1	11	0	0	5	5	1	0	0	1	20
Total Volume	0	13	0	13	19	15	7	41	1	0	13	14	4	0	0	4	72
% App. Total	0	100	0		46.3	36.6	17.1		7.1	0	92.9		100	0	0		
PHF	.000	.542	.000	.542	.679	.750	.438	.932	.250	.000	.650	.700	.500	.000	.000	.500	.818
Car	0	11	0	11	3	11	7	21	0	0	9	9	4	0	0	4	45
% Car	0	84.6	0	84.6	15.8	73.3	100	51.2	0	0	69.2	64.3	100	0	0	100	62.5
Truck	0	2	0	2	16	4	0	20	1	0	4	5	0	0	0	0	27
% Truck	0	15.4	0	15.4	84.2	26.7	0	48.8	100	0	30.8	35.7	0	0	0	0	37.5

[illegible]

Appendix D
2017 Existing Intersection Capacity Analysis

Fed Ex Freight TIA - West Albuquerque

Vistro File: P:\...\FedEx.vistro
Report File: C:\...\2017 AM.pdf

Scenario 1 2017 AM
12/7/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	NB Thru	0.000	9.5	A
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.002	11.3	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	14.2	B
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.003	21.1	C
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.004	19.3	C
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.306	10.3	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & Tempur Peditc Parkway

Control Type:	Two-way stop	Delay (sec / veh):	9.5
Analysis Method:	HCM 6th Edition	Level Of Service:	A
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	13	1	8	1	8	8	5	2	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	50.00	2.00	25.00	2.00	75.00	12.50	2.00	2.00	2.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	13	1	8	1	8	8	5	2	0	1
Peak Hour Factor	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	6	0	3	0	3	3	2	1	0	0
Total Analysis Volume [veh/h]	0	0	22	2	14	2	14	14	9	3	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.02	0.00	0.00	0.00	0.01	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	8.98	9.54	8.94	7.27	0.00	0.00	8.04	0.00	0.00	9.01	9.45	8.40
Movement LOS	A	A	A	A	A	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.07	0.07	0.07	0.02	0.01	0.00	0.05	0.02	0.00	0.02	0.02	0.02
95th-Percentile Queue Length [ft]	1.81	1.81	1.81	0.43	0.21	0.00	1.18	0.59	0.00	0.39	0.39	0.39
d_A, Approach Delay [s/veh]	8.94			0.81			3.04			8.77		
Approach LOS	A			A			A			A		
d_I, Intersection Delay [s/veh]	4.48											
Intersection LOS	A											

Intersection Level Of Service Report

Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	11.3
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.002

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	1	14	37	102	184	7
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	80.00	30.00	4.00	3.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	1	14	37	102	184	7
Peak Hour Factor	0.8700	0.8700	0.8700	0.8700	0.8700	0.8700
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	4	11	29	53	2
Total Analysis Volume [veh/h]	1	16	43	117	211	8
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.02	0.04	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	11.34	10.56	8.11	0.00	0.00	0.00
Movement LOS	B	B	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.01	0.07	0.46	0.46	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.13	1.85	11.49	11.49	0.00	0.00
d_A, Approach Delay [s/veh]	10.61		2.18		0.00	
Approach LOS	B		A		A	
d_I, Intersection Delay [s/veh]	1.34					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	14.2
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	2	0	40	0	0	0	95	140	0	0	155	27
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	5.00	2.00	2.00	2.00	10.00	8.00	2.00	2.00	10.00	4.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	2	0	40	0	0	0	95	140	0	0	155	27
Peak Hour Factor	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	1	0	11	0	0	0	27	39	0	0	43	8
Total Analysis Volume [veh/h]	2	0	45	0	0	0	106	156	0	0	173	30
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.05	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	13.39	14.18	9.12	13.25	14.13	8.72	7.99	0.00	0.00	7.53	0.00	0.00
Movement LOS	B	B	A	B	B	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.17	0.17	0.17	0.00	0.00	0.00	0.26	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	4.21	4.21	4.21	0.00	0.00	0.00	6.59	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	9.30			12.03			3.23			0.00		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	2.51											
Intersection LOS	B											

Intersection Level Of Service Report
Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	21.1
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.003

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	194	1	107	81	124	0	0	165	32
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	30.00	50.00	13.00	20.00	10.00	2.00	2.00	13.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	194	1	107	81	124	0	0	165	32
Peak Hour Factor	1.0000	1.0000	1.0000	0.9670	0.9670	0.9670	0.9670	0.9670	1.0000	1.0000	0.9670	0.9670
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	50	0	28	21	32	0	0	43	8
Total Analysis Volume [veh/h]	0	0	0	201	1	111	84	128	0	0	171	33
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.43	0.00	0.12	0.07	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	18.70	21.09	9.28	8.11	0.00	0.00	0.00	0.00	0.00
Movement LOS				C	C	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	2.18	2.18	0.39	0.22	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	54.62	54.62	9.86	5.43	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			15.37			3.21			0.00		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	7.53											
Intersection LOS	C											

Intersection Level Of Service Report
Intersection 5: Atrisco Vista & I-40 Eastbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	19.3
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.004

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	189	290	12	1	51	0	0	0	52	279	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	19.00	14.00	17.00	2.00	30.00	2.00	2.00	2.00	12.00	10.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	189	290	12	1	51	0	0	0	52	279	0
Peak Hour Factor	1.0000	0.9500	0.9500	0.9500	0.9500	0.9500	1.0000	1.0000	1.0000	0.9500	0.9500	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	50	76	3	0	13	0	0	0	14	73	0
Total Analysis Volume [veh/h]	0	199	305	13	1	54	0	0	0	55	294	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.03	0.00	0.07	0.00	0.00	0.00	0.06	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	14.33	19.27	9.88	0.00	0.00	0.00	8.85	0.00	0.00
Movement LOS		A	A	B	C	A				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	0.11	0.11	0.22	0.00	0.00	0.00	0.18	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	2.82	2.82	5.48	0.00	0.00	0.00	4.41	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			10.87			0.00			1.39		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	1.33											
Intersection LOS	C											

Intersection Level Of Service Report Intersection 6: Atrisco Vista & Central

Control Type: All-way stop
Analysis Method: HCM 6th Edition
Analysis Period: 15 minutes

Delay (sec / veh): 10.3
Level Of Service: B
Volume to Capacity (v/c): 0.306

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	18	227	105	116	151	69	67	18	8	27	35	182
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	17.00	2.00	2.00	25.00	3.00	13.00	32.00	12.00	2.00	8.00	2.00	28.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	18	227	105	116	151	69	67	18	8	27	35	182
Peak Hour Factor	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	60	28	31	40	18	18	5	2	7	9	48
Total Analysis Volume [veh/h]	19	240	111	122	159	73	71	19	8	29	37	192
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	524	586	586	659	511	581	581	634	509	571	640	552	603	603	627
Degree of Utilization, x	0.04	0.20	0.20	0.17	0.24	0.14	0.14	0.12	0.14	0.03	0.01	0.05	0.03	0.03	0.31

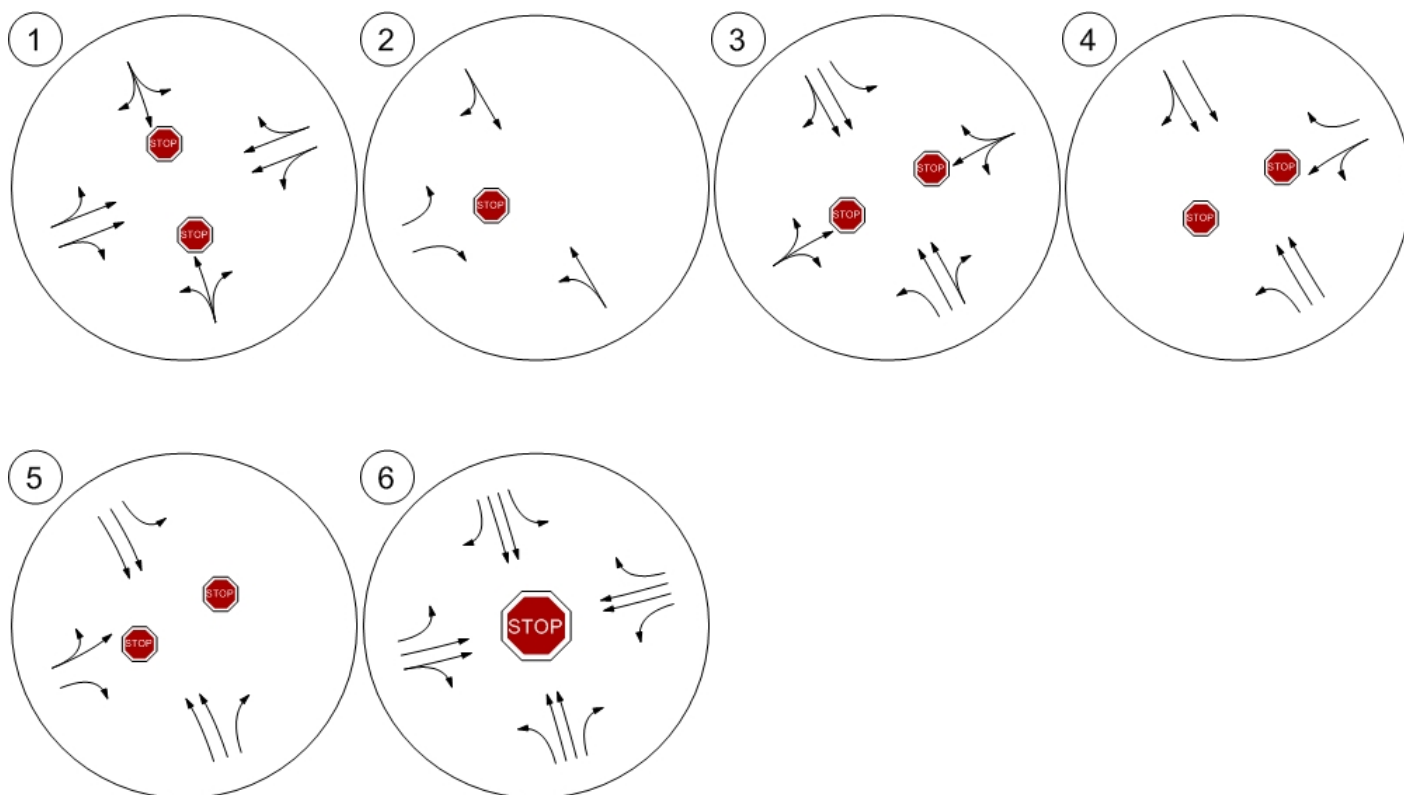
Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.11	0.76	0.76	0.60	0.92	0.47	0.47	0.39	0.48	0.10	0.04	0.17	0.09	0.09	1.29
95th-Percentile Queue Length [ft]	2.82	19.06	19.06	15.06	23.09	11.80	11.80	9.71	12.04	2.58	0.95	4.15	2.37	2.37	32.33
Approach Delay [s/veh]	10.04				10.43				10.38			10.50			
Approach LOS	B				B				B			B			
Intersection Delay [s/veh]	10.31														
Intersection LOS	B														

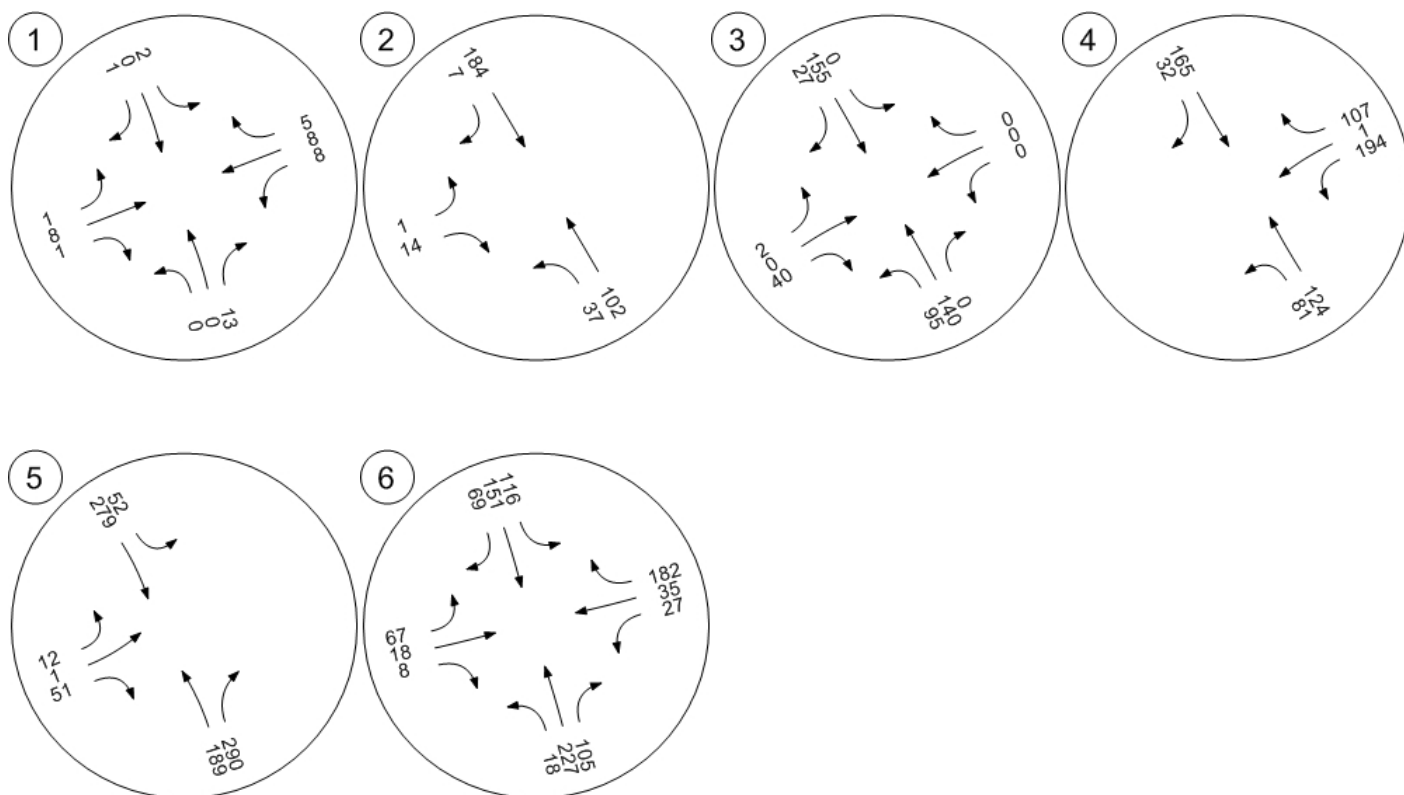
Study Intersections



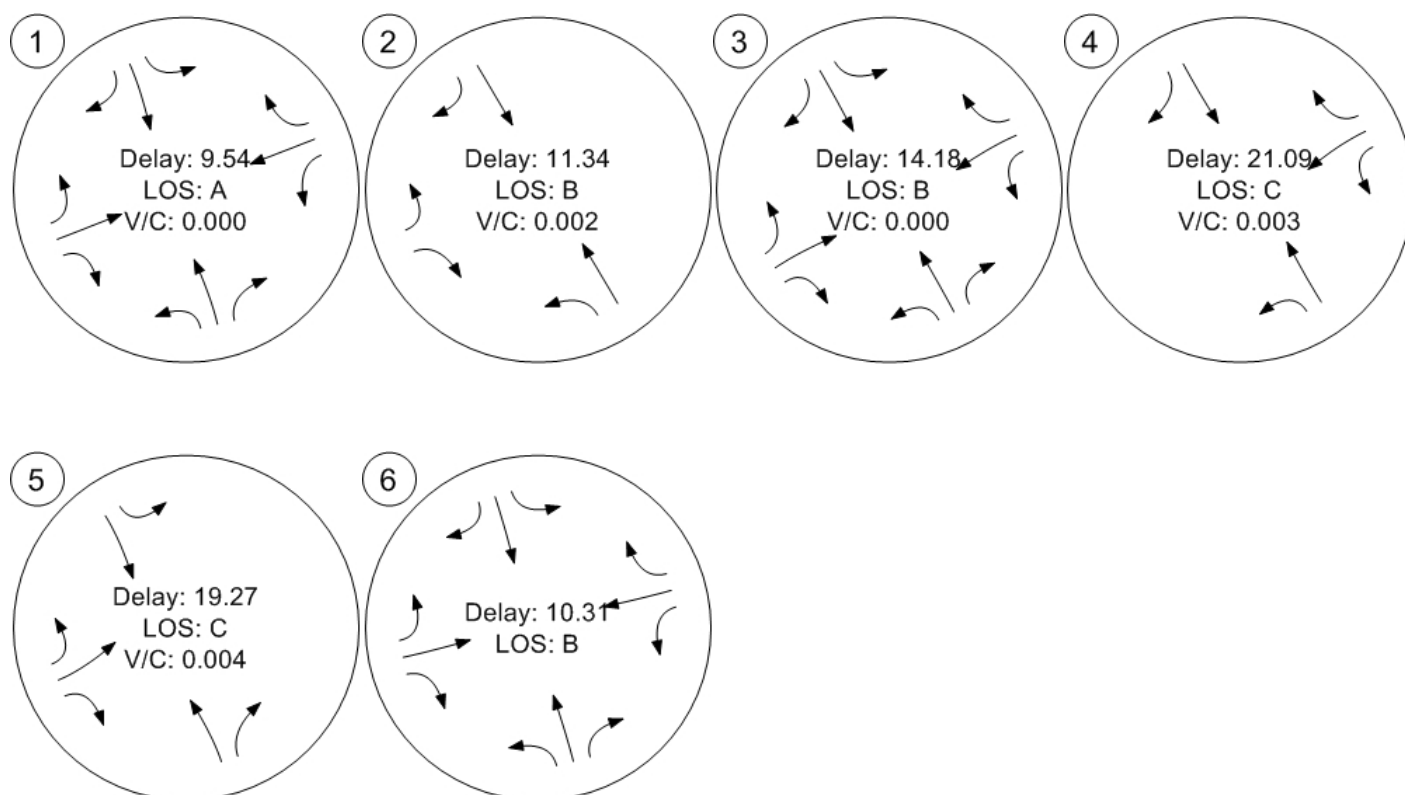
Lane Configuration and Traffic Control



Traffic Volume - Base Volume



Traffic Conditions



Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro
Report File: C:\...\2017 PM.pdf

Scenario 2 2017 PM
12/7/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	SEB Thru	0.000	10.8	B
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.044	11.6	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	16.4	C
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.004	53.4	F
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.000	31.1	D
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.599	13.6	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & Tempur Peditc Parkway

Control Type:	Two-way stop	Delay (sec / veh):	10.8
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	10	0	14	0	9	8	2	80	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	33.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	10	0	14	0	9	8	2	80	0	1
Peak Hour Factor	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	6	0	8	0	5	5	1	48	0	1
Total Analysis Volume [veh/h]	0	0	24	0	34	0	22	19	5	194	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.02	0.00	0.00	0.00	0.02	0.00	0.00	0.22	0.00	0.00
d_M, Delay for Movement [s/veh]	9.19	9.73	8.48	7.27	0.00	0.00	7.66	0.00	0.00	10.38	10.84	9.59
Movement LOS	A	A	A	A	A	A	A	A	A	B	B	A
95th-Percentile Queue Length [veh]	0.07	0.07	0.07	0.00	0.00	0.00	0.05	0.03	0.00	0.87	0.87	0.87
95th-Percentile Queue Length [ft]	1.74	1.74	1.74	0.00	0.00	0.00	1.27	0.64	0.00	21.71	21.71	21.71
d_A, Approach Delay [s/veh]	8.48			0.00			3.66			10.37		
Approach LOS	A			A			A			B		
d_I, Intersection Delay [s/veh]	8.01											
Intersection LOS	B											

Intersection Level Of Service Report

Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	11.6
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.044

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	20	108	30	183	88	10
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	4.00	2.00	6.00	10.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	20	108	30	183	88	10
Peak Hour Factor	0.7950	0.7950	0.7950	0.7950	0.7950	0.7950
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	6	34	9	58	28	3
Total Analysis Volume [veh/h]	25	136	38	230	111	13
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.04	0.15	0.03	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	11.61	9.51	7.55	0.00	0.00	0.00
Movement LOS	B	A	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.14	0.51	0.68	0.68	0.00	0.00
95th-Percentile Queue Length [ft/ln]	3.44	12.71	16.92	16.92	0.00	0.00
d_A, Approach Delay [s/veh]	9.83		1.07		0.00	
Approach LOS	A		A		A	
d_I, Intersection Delay [s/veh]	3.38					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	16.4
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	17	0	133	0	0	0	87	192	0	0	181	6
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	6.00	2.00	15.00	2.00	2.00	2.00	14.00	2.00	2.00	2.00	3.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	17	0	133	0	0	0	87	192	0	0	181	6
Peak Hour Factor	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	0	39	0	0	0	25	56	0	0	53	2
Total Analysis Volume [veh/h]	20	0	155	0	0	0	102	224	0	0	211	7
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.05	0.00	0.18	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	15.42	16.38	10.42	15.84	15.07	8.91	8.09	0.00	0.00	7.68	0.00	0.00
Movement LOS	C	C	B	C	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.87	0.87	0.87	0.00	0.00	0.00	0.26	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	21.63	21.63	21.63	0.00	0.00	0.00	6.56	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	11.00			13.27			2.53			0.00		
Approach LOS	B			B			A			A		
d_I, Intersection Delay [s/veh]	3.82											
Intersection LOS	C											

Intersection Level Of Service Report

Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	53.4
Analysis Method:	HCM 6th Edition	Level Of Service:	F
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.004

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	192	1	81	143	195	0	0	294	23
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	11.00	2.00	7.00	12.00	5.00	2.00	2.00	7.00	9.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	192	1	81	143	195	0	0	294	23
Peak Hour Factor	1.0000	1.0000	1.0000	0.9000	0.9000	0.9000	0.9000	0.9000	1.0000	1.0000	0.9000	0.9000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	53	0	23	40	54	0	0	82	6
Total Analysis Volume [veh/h]	0	0	0	213	1	90	159	217	0	0	327	26
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.77	0.00	0.10	0.14	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	51.51	53.39	9.40	8.69	0.00	0.00	0.00	0.00	0.00
Movement LOS				F	F	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	5.86	5.86	0.33	0.49	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	146.60	146.60	8.22	12.19	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			39.05			3.68			0.00		
Approach LOS	A			E			A			A		
d_I, Intersection Delay [s/veh]	12.83											
Intersection LOS	F											

Intersection Level Of Service Report

Intersection 5: Atrisco Vista & I-40 Eastbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	31.1
Analysis Method:	HCM 6th Edition	Level Of Service:	D
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	269	168	57	0	125	0	0	0	149	311	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	9.00	22.00	6.00	2.00	26.00	2.00	2.00	2.00	6.00	11.00	2.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	269	168	57	0	125	0	0	0	149	311	0
Peak Hour Factor	1.0000	0.9280	0.9280	0.9280	0.9280	0.9280	1.0000	1.0000	1.0000	0.9280	0.9280	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	72	45	15	0	34	0	0	0	40	84	0
Total Analysis Volume [veh/h]	0	290	181	61	0	135	0	0	0	161	335	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.26	0.00	0.17	0.00	0.00	0.00	0.15	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	25.38	31.11	10.61	0.00	0.00	0.00	9.01	0.00	0.00
Movement LOS		A	A	D	D	B				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	1.00	1.00	0.63	0.00	0.00	0.00	0.54	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	24.88	24.88	15.65	0.00	0.00	0.00	13.38	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			15.20			0.00			2.92		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	3.81											
Intersection LOS	D											

**Intersection Level Of Service Report
Intersection 6: Atrisco Vista & Central**

Control Type: All-way stop
 Analysis Method: HCM 6th Edition
 Analysis Period: 15 minutes

Delay (sec / veh): 13.6
 Level Of Service: B
 Volume to Capacity (v/c): 0.599

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	7	141	81	263	170	35	58	28	15	85	20	254
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	15.00	3.00	3.00	23.00	4.00	15.00	11.00	15.00	40.00	6.00	15.00	21.00
Growth Rate	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	7	141	81	263	170	35	58	28	15	85	20	254
Peak Hour Factor	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	2	39	23	73	47	10	16	8	4	24	6	71
Total Analysis Volume [veh/h]	8	157	90	293	189	39	65	31	17	95	22	283
Pedestrian Volume [ped/h]	0			0			0			0		

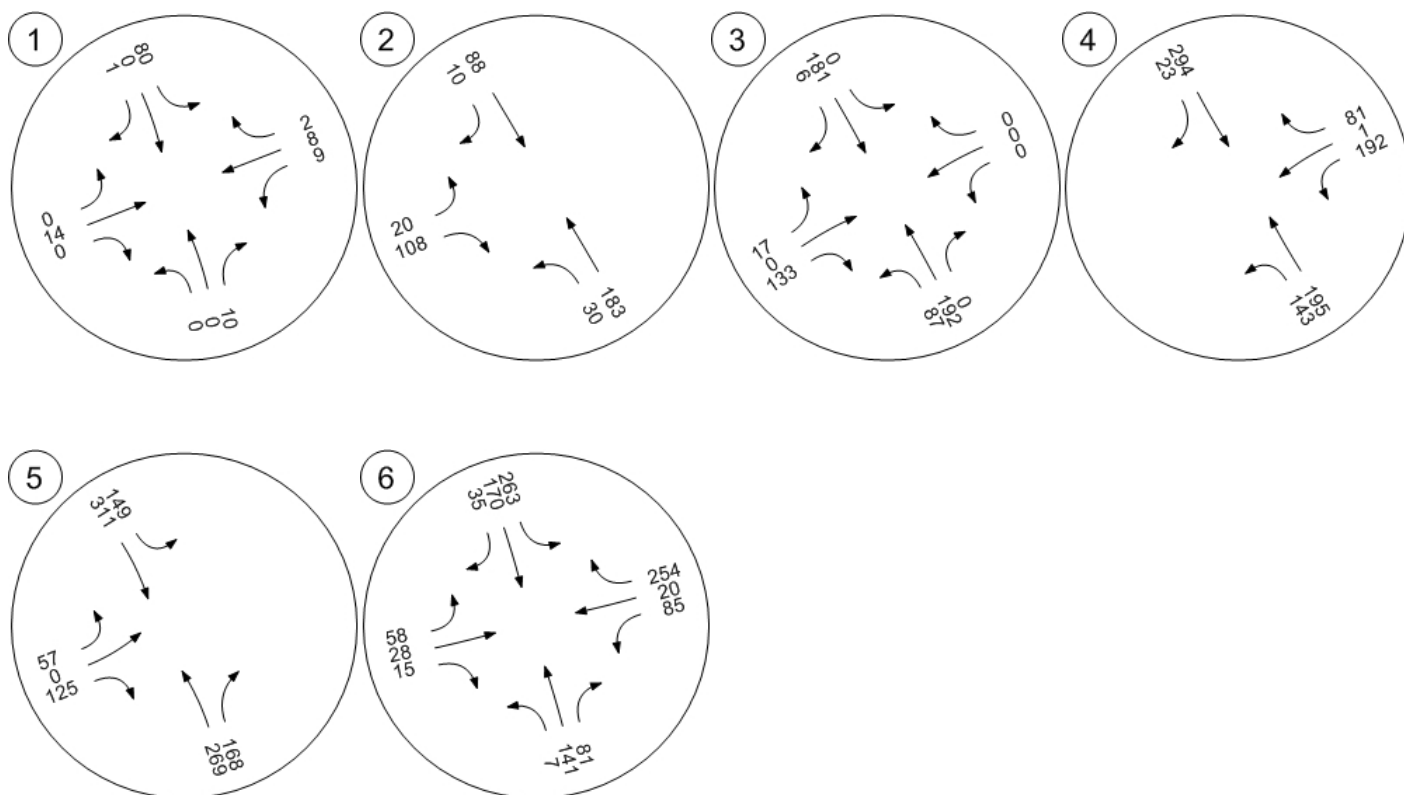
Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	470	516	516	571	490	550	550	595	480	506	546	525	550	550	599
Degree of Utilization, x	0.02	0.15	0.15	0.16	0.60	0.17	0.17	0.07	0.14	0.06	0.03	0.18	0.02	0.02	0.47

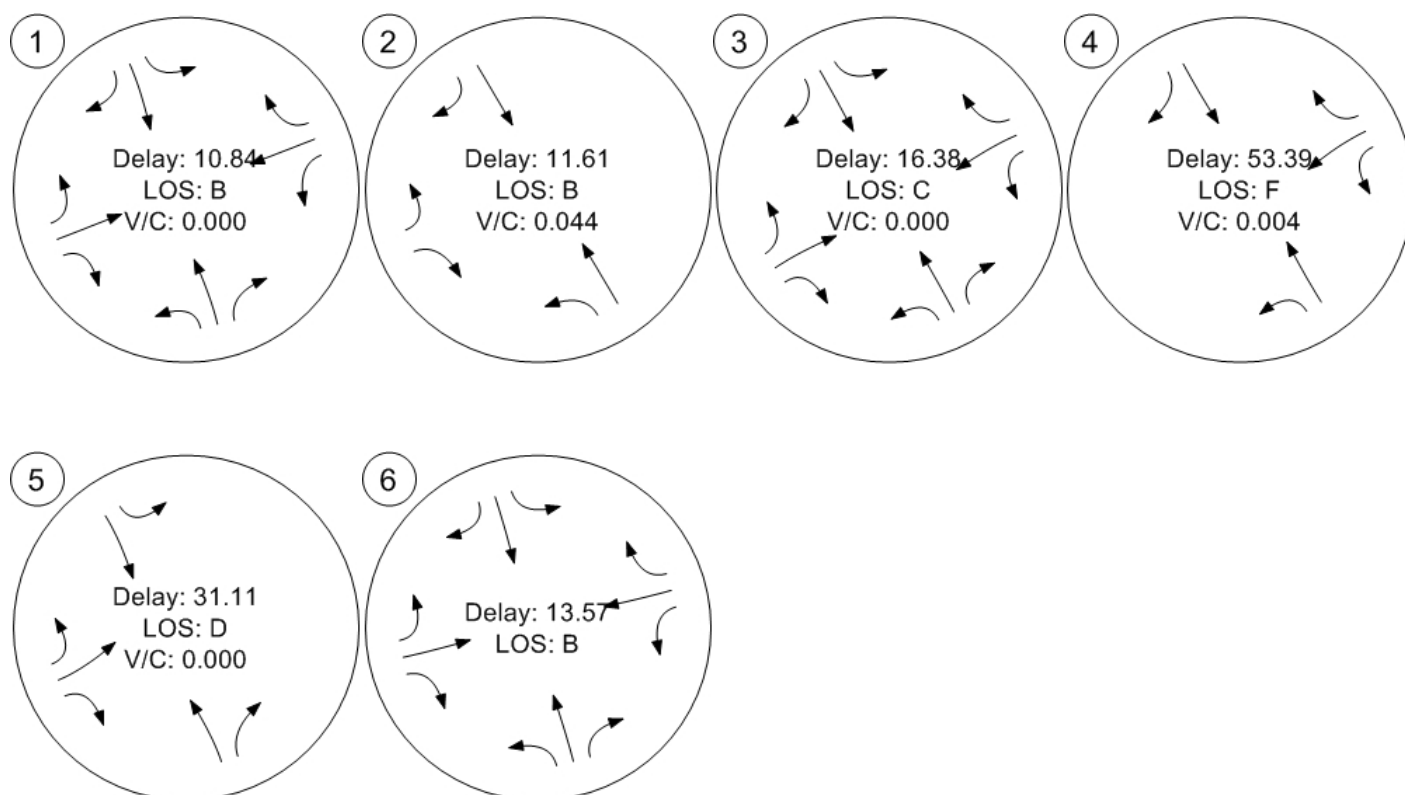
Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.05	0.53	0.53	0.56	3.86	0.62	0.62	0.21	0.47	0.19	0.10	0.66	0.06	0.06	2.52
95th-Percentile Queue Length [ft]	1.30	13.34	13.34	13.91	96.62	15.41	15.41	5.24	11.65	4.87	2.41	16.38	1.53	1.53	63.01
Approach Delay [s/veh]	10.66				16.02				10.79				13.02		
Approach LOS	B				C				B				B		
Intersection Delay [s/veh]	13.57														
Intersection LOS	B														

Traffic Volume - Base Volume



Traffic Conditions



Appendix E
Traffic Signal Warrant Analysis

TEAPAC[Ver 9.01.01] - MUTCD Warrant Analysis

Conditions Used for Warrant Analysis 2009 MUTCD

Intersection # 1

Major Street Direction	NorthSouth
Number of Lanes in North-South direction	2
Number of Lanes in East-West direction	1
Approach speed on major street is greater than 40 mph	No
Isolated community has population less than 10,000	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to improve conditions	Yes
Number of accidents correctable by a signal	2
Peak hour stop sign delay for worst minor approach (veh-hours)	2
Number of accidents correctable by a multi-way stop	0
Peak hour average delay for all minor approaches (sec/veh)	14

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis - 8-Hour Minimum Vehicular Volume

Start Time	1615	1515	645	745	1245	1115	1715	845	Req.
Minor Volume	334	314	244	237	316	267	267	221	150
Major Volume	642	616	686	640	472	419	488	419	600
Warrant Met?	Yes	Yes	Yes	Yes	No	No	No	No	8

Number of 1-hour periods meeting the warrant	4
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1A IS NOT MET <<

Warrant 1B Analysis - 8-Hour Interruption of Continuous Traffic

Start Time	1630	1530	1245	1115	645	745	845	1730	Req.
Minor Volume	359	320	316	267	244	237	221	165	75
Major Volume	697	612	472	419	686	640	419	290	900
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1B IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis (80%) - 8-Hour Minimum Vehicular Volume

Start Time	1700	1600	1500	730	630	1245	1115	830	Req.
Minor Volume	350	330	305	234	176	316	267	230	120
Major Volume	669	641	555	652	532	472	419	449	480
Warrant Met?	Yes	Yes	Yes	Yes	Yes	No	No	No	8
Number of 1-hour periods meeting the warrant									5

Warrant 1B Analysis (80%) - 8-Hour Interruption of Continuous Traf

Start Time	1630	1530	1245	1115	645	745	845	1730	Req.
Minor Volume	359	320	316	267	244	237	221	165	60
Major Volume	697	612	472	419	686	640	419	290	720
Warrant Met?	No	No	No	No	No	No	No	No	8
Number of 1-hour periods meeting the warrant									0

Warrant 1C Analysis - 8-Hour Combination of Warrants

80% of Warrants 1A and 1B are met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce delays	Yes

>> WARRANT 1C IS NOT MET <<

Warrant 2 Analysis - 4-Hour Vehicular Volume

Start Time	1615	1515	1245	1115	1715	645	745	845	Req.
Minor Volume	334	314	316	267	267	244	237	221	—
Minor Reqrmt	271	283	354	381	346	251	272	381	<--
Warrant Met?	Yes	Yes	No	No	No	No	No	No	4
Number of 1-hour periods meeting the warrant									2
Signal will not seriously disrupt progressive traffic flow									Yes

>> WARRANT 2 IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 3A Analysis - Peak Hour Delay

Start Time	1700	1600	1500	1230	1130	645	745	845	Req.
Minor Volume	350	330	305	303	267	244	237	221	100
Total Volume	1109	1079	1059	878	805	1023	945	725	800
Warrant Met?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	1

Number of 1-hour periods meeting the warrant	7
Signal will not seriously disrupt progressive traffic flow	Yes
Delay for worst minor approach (must be at least 4 veh-hours)	2

>> WARRANT 3A IS NOT MET <<

Warrant 3B Analysis - Peak Hour Volume

Start Time	1630	1530	1245	1115	645	745	845	1730	Req.
Minor Volume	359	320	316	267	244	237	221	165	—
Minor Reqrmt	417	459	529	556	422	445	556	620	<--
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 3B IS NOT MET <<

Warrant 7 Analysis - Crash Experience

80% of Warrant 1A or 1B is met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce accidents	Yes
Number of correctable accidents (must be 5 or more per year)	2

>> WARRANT 7 IS NOT MET <<

Summary of MUTCD Traffic Signal Warrant Analysis

Warrant 1A 8-Hour Minimum Vehicular Volume	NOT MET
Warrant 1B 8-Hour Interruption of Continuous Traffic	NOT MET
Warrant 1C 8-Hour Combination of Warrants	NOT MET
Warrant 2 4-Hour Vehicular Volume	NOT MET
Warrant 3A Peak Hour Delay	NOT MET
Warrant 3B Peak Hour Volume	NOT MET
Warrant 7 Crash Experience	NOT MET

>> Traffic Signal Warrant is NOT MET <<

TEAPAC[Ver 9.01.01] - MUTCD Warrant Analysis

Conditions Used for Warrant Analysis 2009 MUTCD

Intersection # 1

Major Street Direction	NorthSouth
Number of Lanes in North-South direction	2
Number of Lanes in East-West direction	1
Approach speed on major street is greater than 40 mph	No
Isolated community has population less than 10,000	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to improve conditions	Yes
Number of accidents correctable by a signal	1
Peak hour stop sign delay for worst minor approach (veh-hours)	1
Number of accidents correctable by a multi-way stop	1
Peak hour average delay for all minor approaches (sec/veh)	15

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis - 8-Hour Minimum Vehicular Volume

Start Time	1645	1545	1230	1445	1100	730	845	1200	Req.
Minor Volume	171	154	115	102	101	85	80	48	150
Major Volume	857	848	681	611	652	719	587	315	600
Warrant Met?	Yes	Yes	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	2
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1A IS NOT MET <<

Warrant 1B Analysis - 8-Hour Interruption of Continuous Traffic

Start Time	1630	1530	1230	1100	730	845	1730	1430	Req.
Minor Volume	182	143	115	101	85	80	70	68	75
Major Volume	917	805	681	652	719	587	355	409	900
Warrant Met?	Yes	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	1
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1B IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis (80%) - 8-Hour Minimum Vehicular Volume

Start Time	1700	1600	1500	1230	1100	730	845	1200	Req.
Minor Volume	171	162	130	115	101	85	80	48	120
Major Volume	817	831	838	681	652	719	587	315	480
Warrant Met?	Yes	Yes	Yes	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 3

Warrant 1B Analysis (80%) - 8-Hour Interruption of Continuous Traf

Start Time	1700	1600	1500	745	645	1230	1100	845	Req.
Minor Volume	171	162	130	85	64	115	101	80	60
Major Volume	817	831	838	729	810	681	652	587	720
Warrant Met?	Yes	Yes	Yes	Yes	Yes	No	No	No	8

Number of 1-hour periods meeting the warrant 5

Warrant 1C Analysis - 8-Hour Combination of Warrants

80% of Warrants 1A and 1B are met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce delays	Yes

>> WARRANT 1C IS NOT MET <<

Warrant 2 Analysis - 4-Hour Vehicular Volume

Start Time	1630	1530	1230	1100	730	845	1730	1430	Req.
Minor Volume	182	143	115	101	85	80	70	68	—
Minor Reqrmt	166	203	254	267	237	297	413	386	<--
Warrant Met?	Yes	No	No	No	No	No	No	No	4

Number of 1-hour periods meeting the warrant 1

Signal will not seriously disrupt progressive traffic flow Yes

>> WARRANT 2 IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 3A Analysis - Peak Hour Delay

Start Time	1645	1545	1200	1300	1445	1100	730	845	Req.
Minor Volume	171	154	103	102	102	101	85	80	100
Total Volume	1028	1002	758	763	713	753	804	667	650
Warrant Met?	Yes	Yes	Yes	Yes	Yes	Yes	No	No	1

Number of 1-hour periods meeting the warrant	6
Signal will not seriously disrupt progressive traffic flow	Yes
Delay for worst minor approach (must be at least 4 veh-hours)	1

>> WARRANT 3A IS NOT MET <<

Warrant 3B Analysis - Peak Hour Volume

Start Time	1630	1530	1230	1100	730	845	1730	1430	Req.
Minor Volume	182	143	115	101	85	80	70	68	—
Minor Reqrmt	318	368	425	439	406	472	588	561	<--
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 3B IS NOT MET <<

Warrant 7 Analysis - Crash Experience

80% of Warrant 1A or 1B is met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce accidents	Yes
Number of correctable accidents (must be 5 or more per year)	1

>> WARRANT 7 IS NOT MET <<

Summary of MUTCD Traffic Signal Warrant Analysis

Warrant 1A 8-Hour Minimum Vehicular Volume	NOT MET
Warrant 1B 8-Hour Interruption of Continuous Traffic	NOT MET
Warrant 1C 8-Hour Combination of Warrants	NOT MET
Warrant 2 4-Hour Vehicular Volume	NOT MET
Warrant 3A Peak Hour Delay	NOT MET
Warrant 3B Peak Hour Volume	NOT MET
Warrant 7 Crash Experience	NOT MET

>> Traffic Signal Warrant is NOT MET <<

TEAPAC[Ver 9.01.01] - MUTCD Warrant Analysis

Conditions Used for Warrant Analysis 2009 MUTCD

Intersection # 1

Major Street Direction	NorthSouth
Number of Lanes in North-South direction	2
Number of Lanes in East-West direction	1
Approach speed on major street is greater than 40 mph	No
Isolated community has population less than 10,000	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to improve conditions	Yes
Number of accidents correctable by a signal	2
Peak hour stop sign delay for worst minor approach (veh-hours)	3
Number of accidents correctable by a multi-way stop	0
Peak hour average delay for all minor approaches (sec/veh)	0

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis - 8-Hour Minimum Vehicular Volume

Start Time	1615	1515	1245	745	645	1100	845	1715	Req.
Minor Volume	264	323	311	302	272	255	220	193	150
Major Volume	611	478	382	402	400	400	341	373	600
Warrant Met?	Yes	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	1
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1A IS NOT MET <<

Warrant 1B Analysis - 8-Hour Interruption of Continuous Traffic

Start Time	1515	1245	745	1630	645	1100	845	1200	Req.
Minor Volume	323	311	302	274	272	255	220	180	75
Major Volume	478	382	402	655	400	400	341	299	900
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1B IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis (80%) - 8-Hour Minimum Vehicular Volume

Start Time	1500	1600	1700	1245	745	645	1100	845	Req.
Minor Volume	298	279	260	311	302	272	255	220	120
Major Volume	488	528	564	382	402	400	400	341	480
Warrant Met?	Yes	Yes	Yes	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 3

Warrant 1B Analysis (80%) - 8-Hour Interruption of Continuous Traf

Start Time	1515	1245	745	1630	645	1100	845	1200	Req.
Minor Volume	323	311	302	274	272	255	220	180	60
Major Volume	478	382	402	655	400	400	341	299	720
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 0

Warrant 1C Analysis - 8-Hour Combination of Warrants

80% of Warrants 1A and 1B are met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce delays	Yes

>> WARRANT 1C IS NOT MET <<

Warrant 2 Analysis - 4-Hour Vehicular Volume

Start Time	1630	1515	1245	745	645	1100	845	1200	Req.
Minor Volume	274	323	311	302	272	255	220	180	—
Minor Reqrmt	265	351	399	389	390	390	420	441	<--
Warrant Met?	Yes	No	No	No	No	No	No	No	4

Number of 1-hour periods meeting the warrant 1

Signal will not seriously disrupt progressive traffic flow Yes

>> WARRANT 2 IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 3A Analysis - Peak Hour Delay

Start Time	745	1500	1600	1300	645	1200	1700	1100	Req.
Minor Volume	302	298	279	276	272	269	260	255	100
Total Volume	704	786	807	657	672	661	824	655	650
Warrant Met?	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	1

Number of 1-hour periods meeting the warrant	8
Signal will not seriously disrupt progressive traffic flow	Yes
Delay for worst minor approach (must be at least 4 veh-hours)	3

>> WARRANT 3A IS NOT MET <<

Warrant 3B Analysis - Peak Hour Volume

Start Time	1515	1245	745	1630	645	1100	845	1200	Req.
Minor Volume	323	311	302	274	272	255	220	180	—
Minor Reqrmt	526	574	564	438	565	565	595	616	<--
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 3B IS NOT MET <<

Warrant 7 Analysis - Crash Experience

80% of Warrant 1A or 1B is met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce accidents	Yes
Number of correctable accidents (must be 5 or more per year)	2

>> WARRANT 7 IS NOT MET <<

Summary of MUTCD Traffic Signal Warrant Analysis

Warrant 1A 8-Hour Minimum Vehicular Volume	NOT MET
Warrant 1B 8-Hour Interruption of Continuous Traffic	NOT MET
Warrant 1C 8-Hour Combination of Warrants	NOT MET
Warrant 2 4-Hour Vehicular Volume	NOT MET
Warrant 3A Peak Hour Delay	NOT MET
Warrant 3B Peak Hour Volume	NOT MET
Warrant 7 Crash Experience	NOT MET

>> Traffic Signal Warrant is NOT MET <<

TEAPAC[Ver 9.01.01] - MUTCD Warrant Analysis

Conditions Used for Warrant Analysis 2009 MUTCD

Intersection # 1

Major Street Direction	NorthSouth
Number of Lanes in North-South direction	2
Number of Lanes in East-West direction	1
Approach speed on major street is greater than 40 mph	No
Isolated community has population less than 10,000	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to improve conditions	Yes
Number of accidents correctable by a signal	1
Peak hour stop sign delay for worst minor approach (veh-hours)	1
Number of accidents correctable by a multi-way stop	0
Peak hour average delay for all minor approaches (sec/veh)	0

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis - 8-Hour Minimum Vehicular Volume

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	150
Major Volume	402	312	344	294	253	218	417	120	600
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1A IS NOT MET <<

Warrant 1B Analysis - 8-Hour Interruption of Continuous Traffic

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	75
Major Volume	402	312	344	294	253	218	417	120	900
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1B IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis (80%) - 8-Hour Minimum Vehicular Volume

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	120
Major Volume	402	312	344	294	253	218	417	120	480
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 0

Warrant 1B Analysis (80%) - 8-Hour Interruption of Continuous Traf

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	60
Major Volume	402	312	344	294	253	218	417	120	720
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 0

Warrant 1C Analysis - 8-Hour Combination of Warrants

80% of Warrants 1A and 1B are met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce delays	Yes

>> WARRANT 1C IS NOT MET <<

Warrant 2 Analysis - 4-Hour Vehicular Volume

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	—
Minor Reqrmt	389	434	418	443	464	481	382	530	<--
Warrant Met?	No	No	No	No	No	No	No	No	4

Number of 1-hour periods meeting the warrant 0

Signal will not seriously disrupt progressive traffic flow Yes

>> WARRANT 2 IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 3A Analysis - Peak Hour Delay

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	100
Total Volume	556	449	481	413	346	272	459	160	650
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes
Delay for worst minor approach (must be at least 4 veh-hours)	1

>> WARRANT 3A IS NOT MET <<

Warrant 3B Analysis - Peak Hour Volume

Start Time	1645	1130	1515	1245	830	1615	730	1030	Req.
Minor Volume	154	137	137	119	93	54	42	40	—
Minor Reqrmt	564	609	593	618	639	656	557	705	<--
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 3B IS NOT MET <<

Warrant 7 Analysis - Crash Experience

80% of Warrant 1A or 1B is met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce accidents	Yes
Number of correctable accidents (must be 5 or more per year)	1

>> WARRANT 7 IS NOT MET <<

Summary of MUTCD Traffic Signal Warrant Analysis

Warrant 1A 8-Hour Minimum Vehicular Volume	NOT MET
Warrant 1B 8-Hour Interruption of Continuous Traffic	NOT MET
Warrant 1C 8-Hour Combination of Warrants	NOT MET
Warrant 2 4-Hour Vehicular Volume	NOT MET
Warrant 3A Peak Hour Delay	NOT MET
Warrant 3B Peak Hour Volume	NOT MET
Warrant 7 Crash Experience	NOT MET

>> Traffic Signal Warrant is NOT MET <<

TEAPAC[Ver 9.01.01] - MUTCD Warrant Analysis

Conditions Used for Warrant Analysis

2009 MUTCD

Intersection # 1

Major Street Direction	NorthSouth
Number of Lanes in North-South direction	2
Number of Lanes in East-West direction	1
Approach speed on major street is greater than 40 mph	No
Isolated community has population less than 10,000	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to improve conditions	Yes
Number of accidents correctable by a signal	1
Peak hour stop sign delay for worst minor approach (veh-hours)	1
Number of accidents correctable by a multi-way stop	1
Peak hour average delay for all minor approaches (sec/veh)	1

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis - 8-Hour Minimum Vehicular Volume

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	150
Major Volume	289	177	230	144	135	194	323	74	600
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1A IS NOT MET <<

Warrant 1B Analysis - 8-Hour Interruption of Continuous Traffic

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	75
Major Volume	289	177	230	144	135	194	323	74	900
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 1B IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 1A Analysis (80%) - 8-Hour Minimum Vehicular Volume

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	120
Major Volume	289	177	230	144	135	194	323	74	480
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 0

Warrant 1B Analysis (80%) - 8-Hour Interruption of Continuous Traf

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	60
Major Volume	289	177	230	144	135	194	323	74	720
Warrant Met?	No	No	No	No	No	No	No	No	8

Number of 1-hour periods meeting the warrant 0

Warrant 1C Analysis - 8-Hour Combination of Warrants

80% of Warrants 1A and 1B are met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce delays	Yes

>> WARRANT 1C IS NOT MET <<

Warrant 2 Analysis - 4-Hour Vehicular Volume

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	—
Minor Reqrmt	446	502	475	518	523	493	429	553	<--
Warrant Met?	No	No	No	No	No	No	No	No	4

Number of 1-hour periods meeting the warrant 0

Signal will not seriously disrupt progressive traffic flow Yes

>> WARRANT 2 IS NOT MET <<

TEAPAC[Ver 9.01.01] - Warrant Analysis for Traffic Signal

Warrant 3A Analysis - Peak Hour Delay

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	100
Total Volume	420	226	276	185	171	214	339	87	650
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes
Delay for worst minor approach (must be at least 4 veh-hours)	1

>> WARRANT 3A IS NOT MET <<

Warrant 3B Analysis - Peak Hour Volume

Start Time	1615	1115	1515	1245	845	1715	700	1215	Req.
Minor Volume	131	49	46	41	36	20	16	13	—
Minor Reqrmt	621	677	650	693	698	668	604	728	<--
Warrant Met?	No	No	No	No	No	No	No	No	1

Number of 1-hour periods meeting the warrant	0
Signal will not seriously disrupt progressive traffic flow	Yes

>> WARRANT 3B IS NOT MET <<

Warrant 7 Analysis - Crash Experience

80% of Warrant 1A or 1B is met	No
Signal will not seriously disrupt progressive traffic flow	Yes
Trials of other remedies have failed to reduce accidents	Yes
Number of correctable accidents (must be 5 or more per year)	1

>> WARRANT 7 IS NOT MET <<

Summary of MUTCD Traffic Signal Warrant Analysis

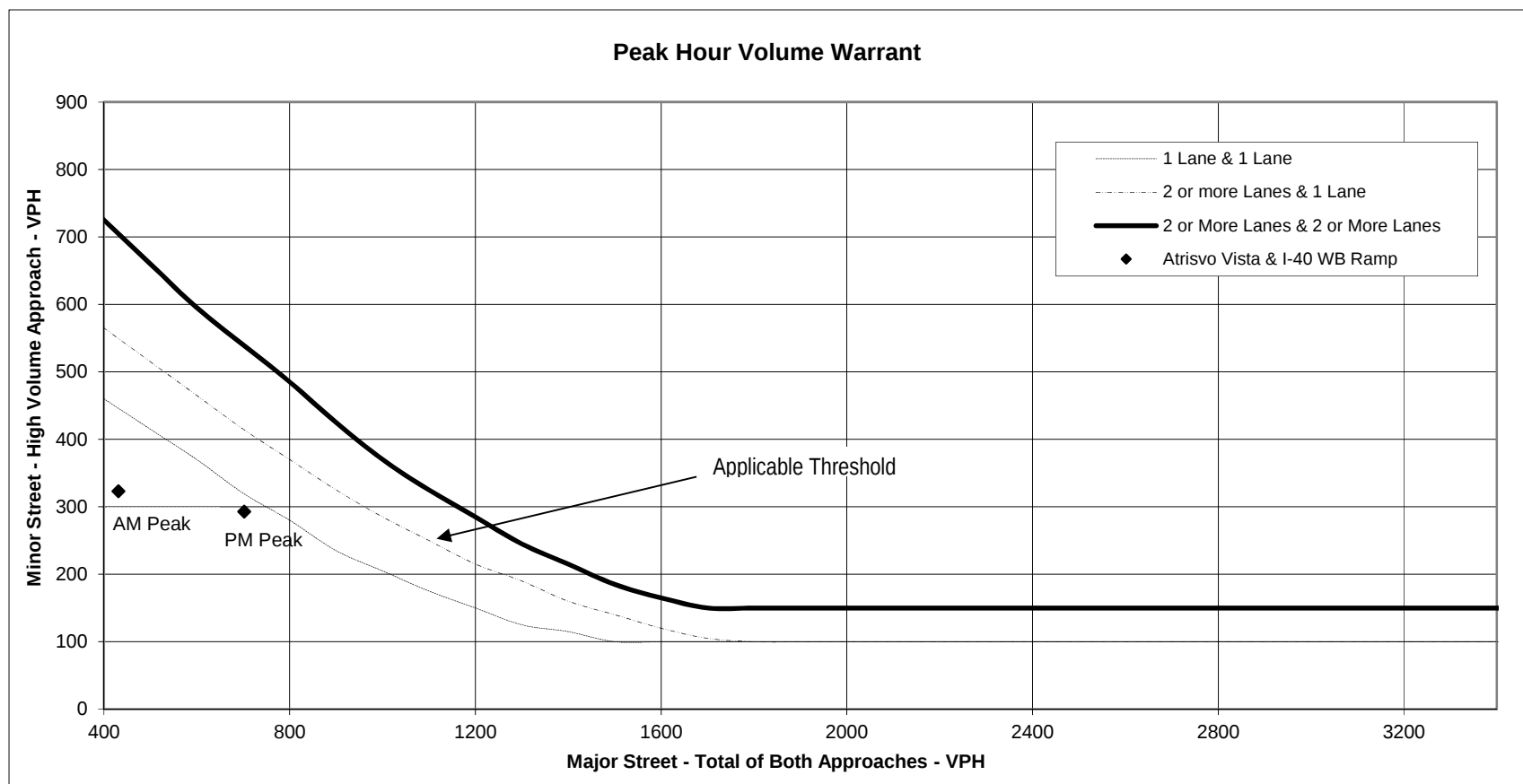
Warrant 1A 8-Hour Minimum Vehicular Volume	NOT MET
Warrant 1B 8-Hour Interruption of Continuous Traffic	NOT MET
Warrant 1C 8-Hour Combination of Warrants	NOT MET
Warrant 2 4-Hour Vehicular Volume	NOT MET
Warrant 3A Peak Hour Delay	NOT MET
Warrant 3B Peak Hour Volume	NOT MET
Warrant 7 Crash Experience	NOT MET

>> Traffic Signal Warrant is NOT MET <<

PEAK HOUR VOLUME SIGNAL WARRANT ANALYSIS

Scenario: 2020 No Build **Peak Hour Delay** **1.51** Hours in AM Criteria - 5 Hours
 Intersection: Atrisco Vista & I-40 WB Ramp **4.70** Hours in PM
 Type: 2 or more Lane/1 Lane
 Major Street (Orientation): Atrisco Vista (N/S)
 Minor Street (Orientation): I-40 WB Ramp (E/W)

	Minor Street Approach Volume			Major Street Approach Volume			Satisfies
Time	WB	EB	High Vol Approach	NB	SB	NB + SB	Warrant 11?
AM Peak	323		323	220	211	431	NO
PM Peak	293		293	362	340	702	NO

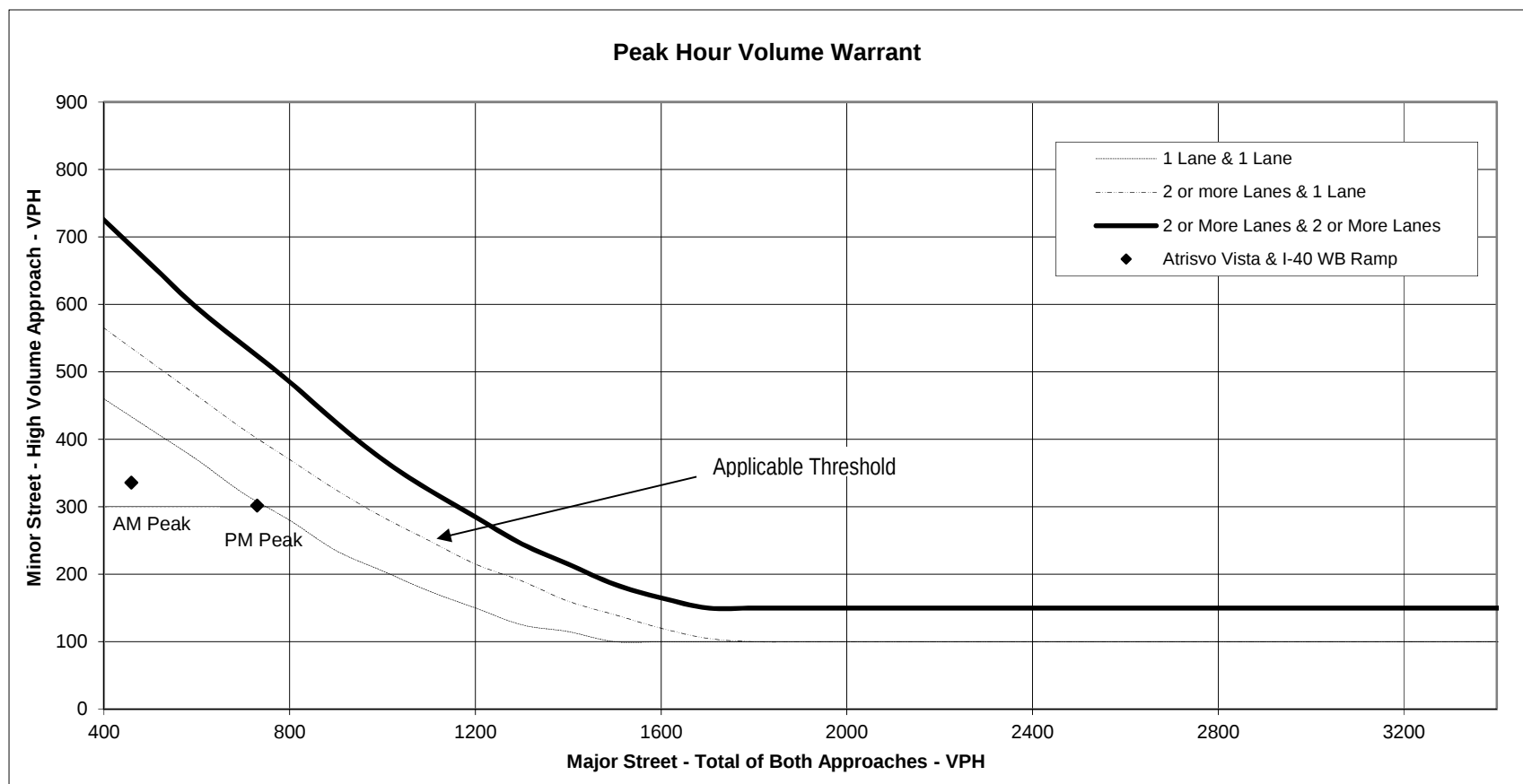


Note: 150 VPH applies as the lower threshold for minor street approach with 2 or more lanes & 100 VPH as the threshold for a minor street approach with one lane

PEAK HOUR VOLUME SIGNAL WARRANT ANALYSIS

Scenario: 2020 Build **Peak Hour Delay 1.62** Hours in AM Criteria - 5 Hours
 Intersection: Atrisco Vista & I-40 WB Ramp **5.28** Hours in PM
 Type: 2 or more Lane/1 Lane
 Major Street (Orientation): Atrisco Vista (N/S)
 Minor Street (Orientation): I-40 WB Ramp (E/W)

	Minor Street Approach Volume			Major Street Approach Volume			Satisfies
Time	WB	EB	High Vol Approach	NB	SB	NB + SB	Warrant 11?
AM Peak	336		336	239	220	459	NO
PM Peak	302		302	371	359	730	NO

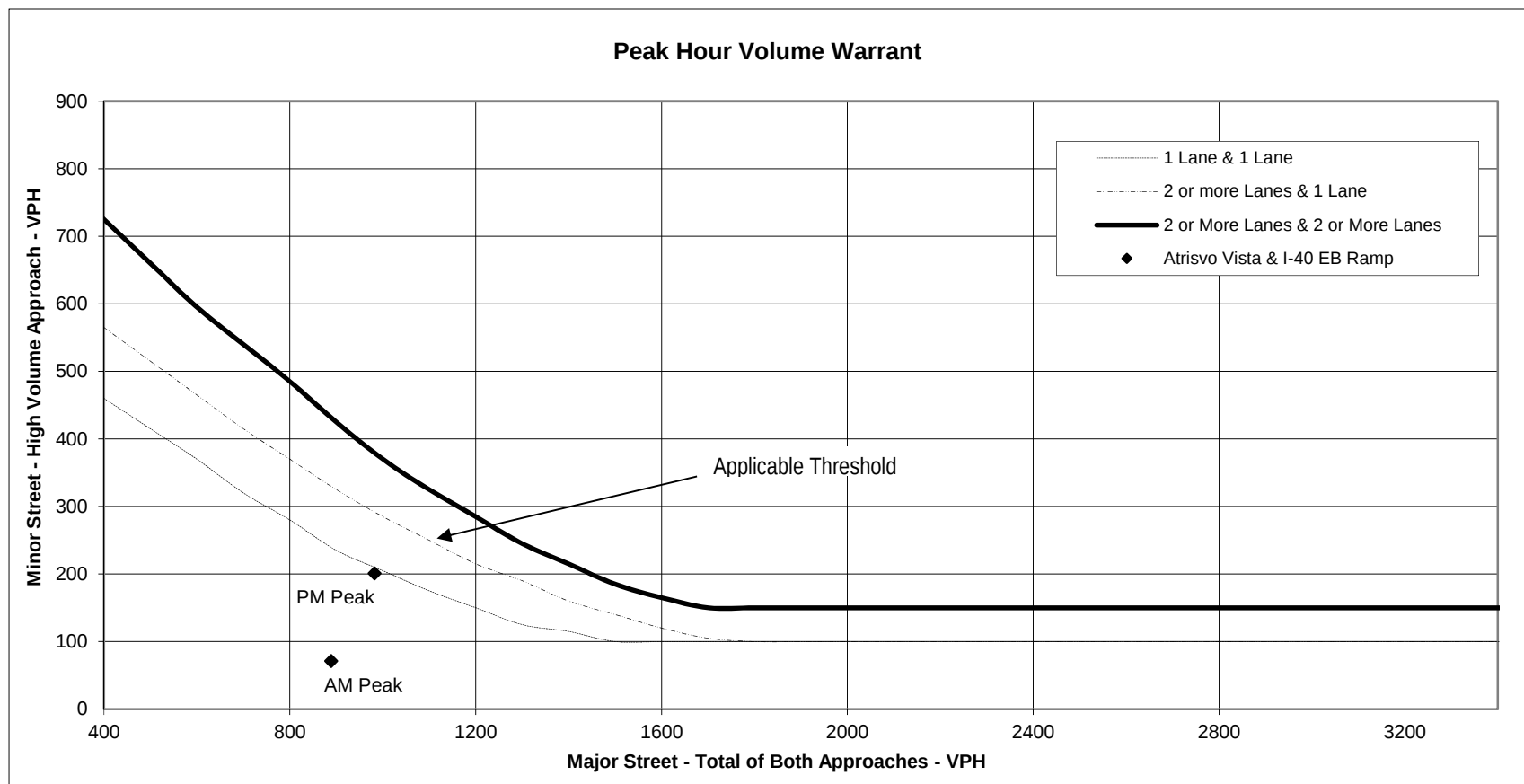


Note: 150 VPH applies as the lower threshold for minor street approach with 2 or more lanes & 100 VPH as the threshold for a minor street approach with one lane

PEAK HOUR VOLUME SIGNAL WARRANT ANALYSIS

Scenario: 2020 Build **Peak Hour Delay** **0.22** Hours in AM Criteria - 5 Hours
 Intersection: Atrisco Vista & I-40 EB Ramp **1.01** Hours in PM
 Type: 2 or more Lane/1 Lane
 Major Street (Orientation): Atrisco Vista (N/S)
 Minor Street (Orientation): I-40 WB Ramp (E/W)

	Minor Street Approach Volume			Major Street Approach Volume			Satisfies
Time	WB	EB	High Vol Approach	NB	SB	NB + SB	Warrant 11?
AM Peak		71	71	529	360	889	NO
PM Peak		201	201	471	511	982	NO



Note: 150 VPH applies as the lower threshold for minor street approach with 2 or more lanes & 100 VPH as the threshold for a minor street approach with one lane

Appendix F
2010 – 2015 Crash Summary Data

DATE	ASTREET	BSTREET	VNUM	TOTAL	KILLED	CLASSA	CLASSB	CLASSC	UNHURT	SEVERITY	CLASS	TOPCFACC	ANALYSIS
6/20/2014		ARTISCO VISTA BLVD	0	2	0	0	1	0	1	Injury Crash	Pedestrian	None	Pedestrian Collision - Vehicle Going Straight
9/12/2010	14405 NORTH FRONTAGE RD	R-140/CENTRAL AVE W-ECO	1	1	0	0	0	0	1	Property Damage Only Crash	Other Non-Coll.	Driver Inattention	Other Non-Collision
11/9/2011	2 SHAMROCK WAY NW		2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Improper Turn	Backing Fr Driveway
	40 AND ATRISCO VISTA		0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failed to Yield Right of Way	Other Vehicle - From Same Direction/Sideswipe Collision
9/22/2011	4TH ST NW	R-140/CENTRAL AVE W-ECO	1	2	0	0	0	0	2	Property Damage Only Crash	Fixed Object	Driver Inattention	Sign Traffic
12/15/2011	4TH ST NW	R-140/CENTRAL AVE W-ECO	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Follow Too Close	Sd-Rear End
	ARTISCO VISTA BLVD	I-40 OFF RAMP	0	2	0	0	0	0	2	Property Damage Only Crash	Fixed Object	Disregarded Traffic Signal	Fixed Object - Median Raised Or Curb
3/10/2014	ATRISCO VIS	I 40	0	2	0	0	0	1	0	Injury Crash	Overturn/Rollover	Driver Inattention	Overturn/Rollover - Right Side of Road
12/6/2014	ATRISCO VIS NW	I 40	0	2	0	0	0	0	1	Injury Crash	Fixed Object	Failed to Yield Right of Way	Fixed Object - Man-made Items (Phone Boxes, Picnic Tables, etc.)
	ATRISCO VISTA	CENTRAL AVE SW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Other Vehicle - All Others/Entering At Angle
	ATRISCO VISTA BLVD	CENTRAL AVE	0	3	0	0	0	0	1	Injury Crash	Other Vehicle	Following Too Closely	Other Vehicle - Both Going Straight/Entering At Angle
	ATRISCO VISTA BLVD	CENTRAL AVE	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Other Vehicle - From Same Direction/All Others
	ATRISCO VISTA BLVD	CENTRAL AVE	0	1	0	0	0	0	1	Property Damage Only Crash	Other Vehicle	Missing Data	Other Vehicle - All Others/Entering At Angle
8/10/2011	ATRISCO VISTA BLVD	CENTRAL AVE E	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Angle-Straight
5/29/2014	ATRISCO VISTA BLVD	CENTRAL AVE W	0	1	0	0	0	0	1	Property Damage Only Crash	Fixed Object	Missing Data	Fixed Object - Median Raised Or Curb
	ATRISCO VISTA BLVD	I 40 WEST - BD FW	0	3	0	0	0	1	0	Injury Crash	Other Vehicle	Disregarded Traffic Signal	Other Vehicle - One Left Turn/Entering At Angle
10/10/2011	ATRISCO VISTA BLVD	I 40 WESTBOUND OFF RAMP	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	Angle-1 Left
	ATRISCO VISTA BLVD	I-40 OFF RAMP	0	3	0	0	0	0	3	Property Damage Only Crash	Fixed Object	Failed to Yield Right of Way	Fixed Object - Sign or Sign Post (Traffic)
10/2/2013	ATRISCO VISTA BLVD	INTERSTATE 40	0	6	1	0	0	0	4	Fatal Crash	Other Vehicle	Alcohol/Drug Involved	Intersection - From Opposite Direction/Both Going Straight
12/14/2012	ATRISCO VISTA BLVD NW	CENTRAL AVE W	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	
4/7/2012	ATRISCO VISTA BLVD NW	CENTRAL AVE W	0	3	0	0	0	0	3	Property Damage Only Crash	Other Vehicle	Failure To Yield	Angle-1 Left
12/21/2012	ATRISCO VISTA BLVD NW	I 40 RAMP NW	0	5	0	0	0	0	5	Property Damage Only Crash	Other Vehicle	Follow Too Close	
10/11/2012	ATRISCO VISTA BLVD NW	I 40 RAMP NW	0	3	0	0	0	1	0	Injury Crash	Other Vehicle	Driver Inattention	Angle-1 Left
2/11/2012	ATRISCO VISTA BLVD NW	I 40 RAMP NW	0	4	0	0	0	0	4	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Angle-Straight
5/6/2012	ATRISCO VISTA BLVD NW	I 40 RAMP NW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	Od-Sideswipe
4/8/2012	ATRISCO VISTA BLVD NW	I40 EASTBOUND NW	0	1	0	0	0	0	1	Property Damage Only Crash	Other Non-Coll.	Excessive Speed	Submersion-River
10/13/2012	ATRISCO VISTA BLVD NW	I40 WESTBOUND NW	0	5	0	0	0	0	5	Property Damage Only Crash	Other Vehicle	Alcohol/Drug Involved	Sd-Both Straight
8/6/2013	ATRISCO VISTA BLVD NW	INTERSTATE 40 ONRAMP NW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	Non-Intersection - From Opposite Direction/Sideswipe Collision
7/18/2012	ATRISCO VISTA BLVD SW	CENTRAL AVE W	0	1	0	0	0	0	1	Property Damage Only Crash	Fixed Object	Driver Inattention	Barricade
7/9/2011	ATRISCO VISTA RD SW	I 40	2	7	0	0	0	0	7	Property Damage Only Crash	Other Vehicle	Driver Inattention	U-Turn
10/30/2011	ATRISCO VISTA RD. SW	CENTRAL AVE W	2	3	0	0	0	0	1	Injury Crash	Other Vehicle	Red Light Running	Sd-One Stopped
9/11/2013	ATRKRN VISTA ROAD	SOUTH FRONTAGE ROAD	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	No Indication	Non-Intersection - From Same Direction/Rear End Collision
3/21/2014	CENTRAL	ATRISCO VIS	0	2	0	0	0	0	1	Injury Crash	Other Vehicle	Passed Stop Sign	Intersection - From Same Direction/All Others
	CENTRAL	ATRISCO VISTA	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Missing Data	Other Vehicle - One Right Turn/Entering At Angle
	CENTRAL	ATRISCO VISTA SW	0	2	0	0	0	1	0	Injury Crash	Other Vehicle	Failed to Yield Right of Way	Other Vehicle - From Opposite Direction/One Left Turn
10/8/2013	CENTRAL AVE	ATRISCA VISTA BLVD NW	0	1	0	0	0	0	1	Property Damage Only Crash	Fixed Object	Avoid Pedestrian, Etc.	Fixed Object - Light Standard (Light Pole)
7/20/2013	CENTRAL AVE	ATRISCO VISTA BLVDSW	0	1	0	0	0	0	1	Property Damage Only Crash	Overturn/Rollover	Alcohol/Drug Involved	Overturn/Rollover - Left Side of Road
3/14/2013	CENTRAL AVE	N / A	0	1	0	0	0	0	1	Property Damage Only Crash	Other (Object)	Other - No Driver Error	Other Object - Object Dropped From Vehicle - Tire
2/16/2014	CENTRAL AVE E	ATRISCO VISTA BLVD	0	3	0	0	0	1	0	Injury Crash	Overturn/Rollover	Alcohol/Drug Involved	Overturn/Rollover - Right Side of Road
10/4/2013	CENTRAL AVE SW	ATRISCO VISTA BLVD SW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Non-Intersection - From Opposite Direction/Sideswipe Collision
	CENTRAL AVE SW	ATRISCO VISTA BLVD SW	0	3	0	0	0	0	3	Property Damage Only Crash	Other Vehicle	Speed Too Fast for Conditions	Other Vehicle - From Same Direction/Both Going Straight
7/18/2011	CENTRAL AVE W	ATRISCO VISTA BLVD	1	1	0	0	0	1	0	Injury Crash	Overturn	Excessive Speed	Overturn On Rd
9/18/2010	CENTRAL AVE. SW.	PASEO DEL VOLCAN	2	4	0	0	0	0	4	Property Damage Only Crash	Other Vehicle	Other-No Error	Sd-Rear End
	CENTRAL AVENUE NW	ATRISCO VISTA BOULEVARD NW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Alcohol/Drug Involved	Other Vehicle - All Others/Entering At Angle
	CENTRAL BLVD	ATRICSO VISTA BLVD	0	3	0	0	0	0	1	Injury Crash	Other Vehicle	Passed Stop Sign	Other Vehicle - All Others/Entering At Angle
	FRONTAGE RD	ATRISCO VISTA	0	2	0	0	0	0	2	Property Damage Only Crash	Left Blank	Missing Data	Invalid Code
	I - 40 NORTH FRONTAGE	ATRISCO VISTA BLVD NW	0	4	0	0	0	0	4	Property Damage Only Crash	Other Vehicle	Speed Too Fast for Conditions	Other Vehicle - From Opposite Direction/One Vehicle Spun On Roadway Before Being Hit
10/14/2013	I 40 98TH ST NW	CENTRAL	0	2	0	0	0	0	2	Property Damage Only Crash	Parked Vehicle	No Indication	Parked Vehicle - Unknown/Not Stated
11/7/2012	I 40 FRONTAGE	PASEO DEL VOLCAN	0	3	0	0	0	0	3	Property Damage Only Crash	Other Vehicle	No Indication	
2/19/2011	I 40 FRONTAGE RD	PASEO DEL VOLCAN	1	2	0	0	0	0	2	0 Injury Crash	Fixed Object	Alcohol/Drug Involved	Fixed Object-Ns
7/25/2010	I 40 N FRONTAGE WB NW	PASEO DEL VOLCAN	2	3	0	0	0	0	2	1 Injury Crash	Other Vehicle	Driver Inattention	Sd-Sideswipe
4/30/2012	I 40 RAMP NW	ATRISCO VISTA BLVD NW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Driver Inattention	Sd-Rear End
8/20/2010	I-40 NORTH FRONTAGE WB NW	PASEO DEL VULCAN NW	2	3	0	0	0	0	3	Property Damage Only Crash	Other Vehicle	Imp. Lane Change	Sd-Sideswipe
1/4/2012	I-40 WEST-BD FW	ATRISCO VISTA	0	2	0	0	0	0	2	Property Damage Only Crash	Other Object	Driver Inattention	Dropped Load - Furn
9/28/2010	I-40 WEST-BD FW NE	PASEO DEL VULCAN NW	2	3	0	0	0	0	3	Property Damage Only Crash	Other Vehicle	Poor Driving	Sd-Sideswipe
11/24/2010	I-40 WEST-BD FW NE	PASEO DEL VULCAN NW	2	3	0	0	0	0	2	1 Injury Crash	Fixed Object	Avoid Vehicle	Barricade
12/5/2011	I-40 WEST-BD FW NW	ATRISCO DR NW	1	1	0	0	0	0	1	Property Damage Only Crash	Overturn	Too Fast For Conditions	Overturn-Left
	INTERSTATE 40 W/B OFF RAMP MP	ATRISCO VISTA SW	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Driver Inattention	Other Vehicle - From Same Direction/Rear End Collision
7/13/2011	N. FRONTAGE RD	ATRISCO VISTA	2	4	0	0	0	0	4	Property Damage Only Crash	Other Vehicle	Failure To Yield	Angle-1 Left
7/27/2011	N. FRONTAGE ROAD NW	ATRISCO VISTA RD NW	2	5	0	0	0	0	1	4 Injury Crash	Other Vehicle	Driver Inattention	Sd-One Stopped
6/21/2010	NM 333 (OLD US 66)	R-140/CENTRAL AVE E-BW	2	4	0	0	0	1	2	1 Injury Crash	Other Vehicle	Failure To Yield	Angle-1 Left
10/26/2010	PASEO DEL VOLACAN RD	I 40	2	3	0	2	1	0	0	0 Injury Crash	Other Vehicle	Left Of Center	Od-Sideswipe
1/6/2011	PASEO DEL VOLCAN	CENTRAL AVE E	3	3	0	0	0	0	3	Property Damage Only Crash	Parked Vehicle	Driver Inattention	Proper Park
2/16/2012	PASEO DEL VOLCAN	CENTRAL AVE W	0	3	0	0	0	0	3	Property Damage Only Crash	Other Non-Coll.	Road Defect	Other Non-Collision
4/6/2012	PASEO DEL VOLCAN	I 40 W FRONTAGE	0	7	0	0	0	0	7	Property Damage Only Crash	Other Vehicle	Failure To Yield	Leave Driveway
3/25/2012	PASEO DEL VOLCAN	I-40 WEST-BD FW	0	1	0	0	0	0	1	Property Damage Only Crash	Fixed Object	Alcohol/Drug Involved	Traffic Signal
6/14/2010	PASEO DEL VULCAN NW	I-40 N.W. FRONTAGE ROAD	1	1	0	0	0	0	1	Property Damage Only Crash	Fixed Object	Driver Inattention	Median Or Curb
10/4/2011	R-140/CENTRAL AVE W-ECO	4TH ST NW	2	2	0	0	0	0	1	1 Injury Crash	Fixed Object	Excessive Speed	Divider Wall
7/3/2011	R-140/CENTRAL AVE W-ECO	7401 PASEO DEL VOLCAN NW	1	1	0	0	0	0	1	0 Injury Crash	Overturn	Driver Inattention	Overturn-Right
12/9/2011	R-140/CENTRAL AVE W-ECO	ATRISCO VISTA	1	2	0	0	0	2	0	0 Injury Crash	Overturn	Alcohol/Drug Involved	Overturn-Right
9/4/2010	R-140/CENTRAL AVE W-ECO	CENTRAL AVE E	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	None	Sd-Sideswipe
3/9/2011	R-140/CENTRAL AVE W-ECO	CENTRAL AVE E	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Passed Stop Sign	Od-1 Left Turn
9/4/2010	R-140/CENTRAL AVE W-ECO	CENTRAL AVE W	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	None	Sd-Both Straight
9/16/2010	R-140/CENTRAL AVE W-ECO	CENTRAL AVE W	1	1	0	0	0	0	1	Property Damage Only Crash	Overturn	Defect Tires	Overturn-Left
10/14/2010	R-140/CENTRAL AVE W-ECO	CENTRAL AVE W	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Improper Turn	Sd-One Left Turn
1/25/2010	R-140/CENTRAL AVE W-ECO	CENTRAL AVE W	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Mech. Defect	Sd-Rear End
9/28/2010	R-140/CENTRAL AVE W-ECO	DENNIS CHAVEZ SW	1	1	0	0	0	1	0	0 Injury Crash	Fixed Object	Alcohol/Drug Involved	Fence
7/7/2010	R-140/CENTRAL AVE W-ECO	I 40	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Defect Brakes	Sd-One Stopped
8/11/2010	R-140/CENTRAL AVE W-ECO	I 40	2	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	Angle-Straight
3/10/2011	R-140/CENTRAL AVE W-ECO	I 40	2	2	0	0	0	0	2	Property Damage Only Crash	Parked Vehicle	Failure To Yield	Proper Park
6/21/2010	R-140/CENTRAL AVE W-WB	PASEO DEL VULCAN NW	2	6	0	0	0	0	6	Property Damage Only Crash	Other Vehicle	Driver Inattention	Sd-Both Straight
8/7/2013	VISTAATRISCO	140 FRONTAGE	0	2	0	0	0	0	2	Property Damage Only Crash	Other Vehicle	Failure To Yield	Intersection - One Right Turn/Entering At Angle

Appendix G

Trip Generation and Traffic Growth

FedEx Freight

Estimated

Equipment Arrival and Departure Times

	City Departures	City Arrivals	Road Departures	Road Arrivals	Total
0001 - 0100	0	1	12	39	52
0100 - 0200	0	0	16	19	35
0200 - 0300	0	0	12	6	18
0300 - 0400	0	0	10	0	10
0400 - 0500	0	0	8	0	8
0500 - 0600	2	0	11	0	13
0600 - 0700	2	0	8	2	12
0700 - 0800	0	2	9	0	11
0800 - 0900	8	1	0	0	9
0900 - 1000	50	2	0	0	52
1000 - 1100	1	0	0	0	1
1100 - 1200	2	1	1	0	4
1200 - 1300	0	3	1	0	4
1300 - 1400	0	11	0	0	11
1400 - 1500	0	6	0	0	6
1500 - 1600	0	6	0	1	7
1600 - 1700	0	11	0	1	12
1700 - 1800	0	7	0	4	11
1800 - 1900	0	10	0	5	15
1900 - 2000	0	5	3	4	12
2000 - 2100	0	0	0	0	0
2100 - 2200	0	0	2	0	2
2200 - 2300	2	0	0	7	9
2300 - 2400	0	1	8	13	22
	67	67	101	101	336

Estimated

Employee arrival and depart times

	Arrive	Leave	Total	Total	Arrivals	Departures
0001 - 0100	0	0	0	52	40	12
0100 - 0200	2	0	2	37	21	16
0200 - 0300	0	0	0	18	6	12
0300 - 0400	0	2	2	12	0	12
0400 - 0500	1	0	1	9	1	8
0500 - 0600	9	2	11	24	9	15
0600 - 0700	22	0	22	34	24	10
0700 - 0800	40	0	40	51	42	9
0800 - 0900	15	0	15	24	16	8
0900 - 1000	10	0	10	62	12	50
1000 - 1100	0	2	2	3	0	3
1100 - 1200	0	0	0	4	1	3
1200 - 1300	1	0	1	5	4	1
1300 - 1400	11	14	25	36	22	14
1400 - 1500	5	26	31	37	11	26
1500 - 1600	6	23	29	36	13	23
1600 - 1700	7	24	31	43	19	24
1700 - 1800	0	10	10	21	11	10
1800 - 1900	0	0	0	15	15	0
1900 - 2000	2	0	2	14	11	3
2000 - 2100	0	10	10	10	0	10
2100 - 2200	2	10	12	14	2	12
2200 - 2300	0	5	5	14	7	7
2300 - 2400	0	5	5	27	14	13
	133	133	266	602	301	301

Fed Ex Freight Growth Rate Determination

AWDT on Atrisco Vista
(North of I-40)

Year	AWDT
2011	6,000
2012	5,940
2013	6,503
2014	6,529
2015	6,633

$$\text{Linear Growth Rate} = \{[(6,633 - 6,000)/4]/6,633\} \times 100 = 2.39\%$$

Regression Output	
R Square	0.82
Standard Error	1.61E+02
Observations	5
Intercept	-367,091
Std Err of Intercept	1.E+05
Coefficient	186
Std Err of Coefficient	51

Projected AWDT

2011	5,950
2012	6,136
2013	6,321
2014	6,507
2015	6,692
2016	6,878
2017	7,063
2018	7,249
2019	7,434
2020	7,620

Regression Equation

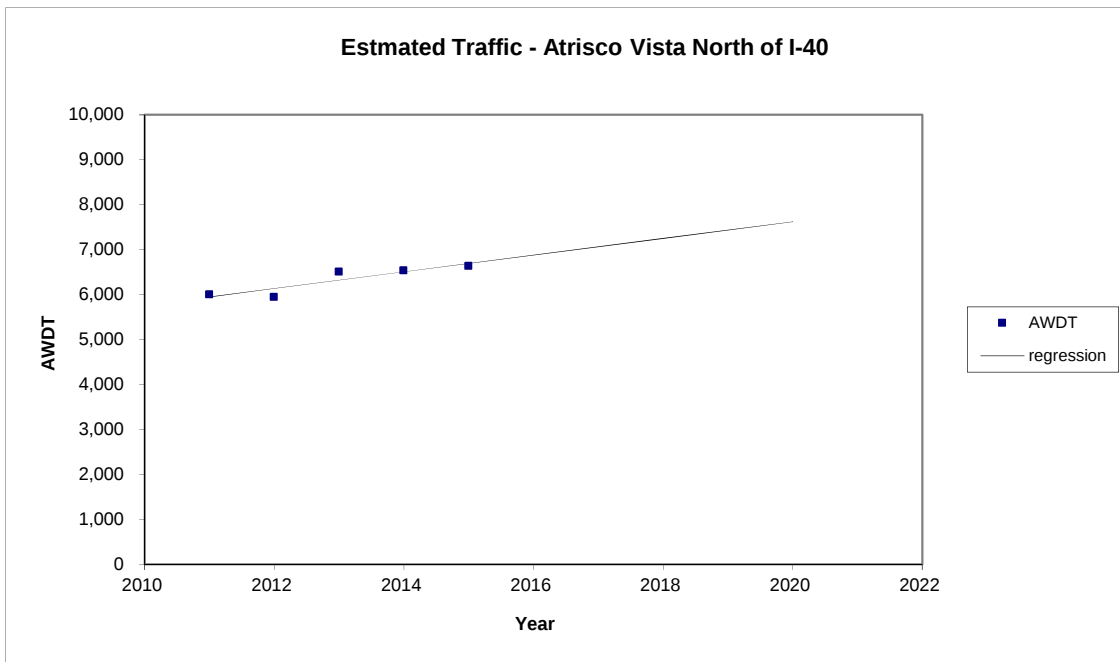
$$\text{AWDT} = 186 \times \text{Year} - 367,091$$

Coefficient Growth Rate 2.80%

Estimated Annual Growth Rate

$$[(7,620 - 6,633)/6,633] \times 100\% = 14.87\%$$

$$14.87\%/5 = 2.97\%$$



Fed Ex Freight Growth Rate Determination

AWDT on Atrisco Vista
(North of Frontage Road)

Year	AWDT
2011	2,961
2012	3,024
2013	3,000
2014	3,012
2015	3,813

$$\text{Linear Growth Rate} = \{[(3,813 - 2,961)/4]/3,813\} \times 100 = 5.59\%$$

Regression Output	
R Square	0.538
Standard Error	2.86E+02
Observations	5
Intercept	-337,438
Std Err of Intercept	1.82E+05
Coefficient	169
Std Err of Coefficient	90

Projected AWDT

2011	2,824
2012	2,993
2013	3,162
2014	3,331
2015	3,500
2016	3,670
2017	3,839
2018	4,008
2019	4,177
2020	4,346

Regression Equation

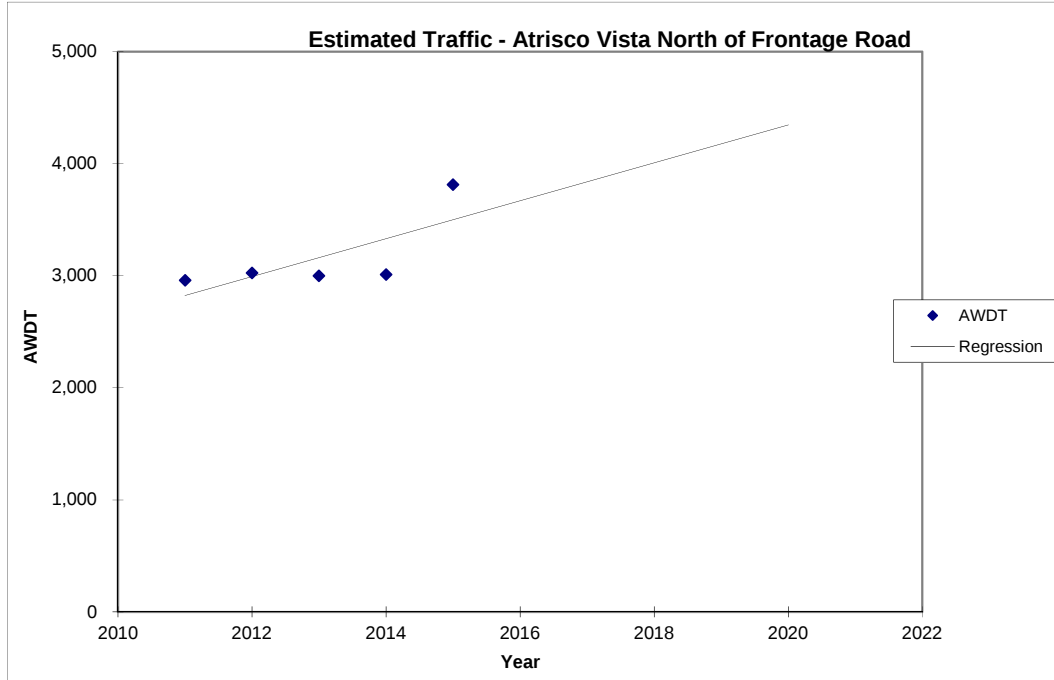
$$\text{AWDT} = 169 \times \text{Year} - 337,438$$

Coefficient Growth Rate 4.44%

Estimated Annual Growth Rate

$$((4,346 - 3,813)/3,813) \times 100\% = 13.99\%$$

$$13.99\%/5 = 2.80\%$$



Fed Ex Freight Growth Rate Determination

AWDT on Atrisco Vista
(North of TempurPedic Parkway)

Year	AWDT
2011	2,341
2012	2,071
2013	2,054
2014	2,062
2015	2,095

$$\text{Linear Growth Rate} = \{[(2,095-2,341)/4]/2,095\} \times 100 =$$

Regression Output	
R Square	0.42
Standard Error	1.07E+02
Observations	5
Intercept	102,976
Std Err of Intercept	68,146
Coefficient	-50
Std Err of Coefficient	3.39E+01

Projected AWDT

Year	Projected AWDT
2011	2,225
2012	2,175
2013	2,125
2014	2,075
2015	2,024
2016	1,974
2017	1,924
2018	1,874
2019	1,824
2020	1,774

Regression Equation

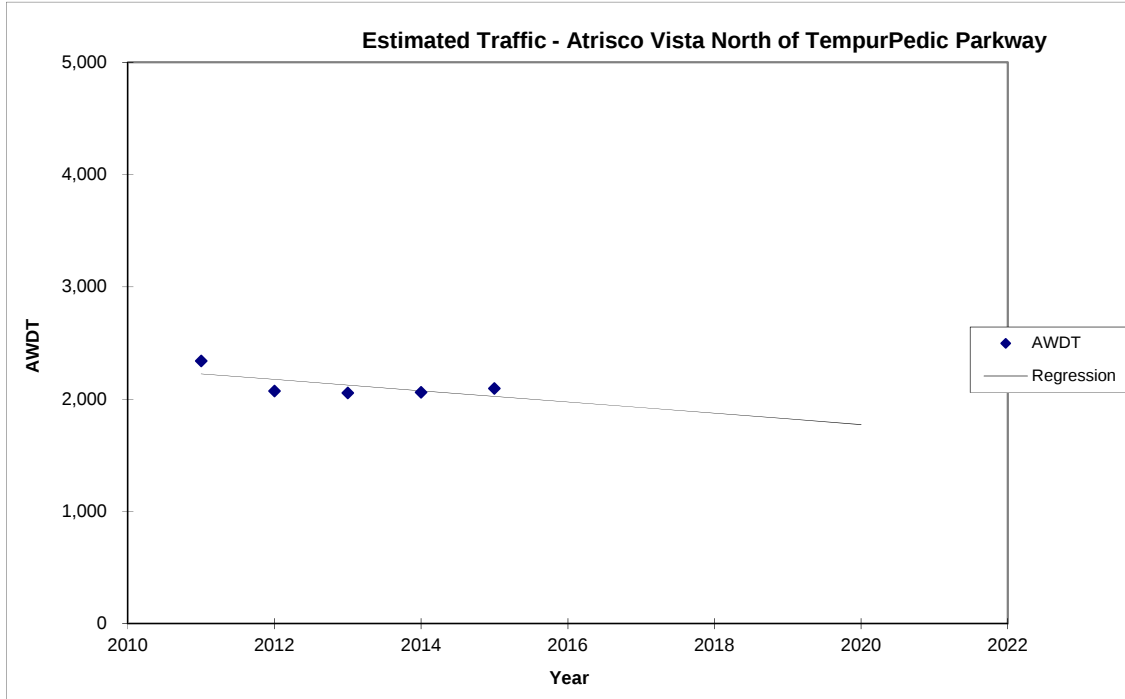
$$\text{AWDT} = -50 \times \text{Year} + 102,976$$

Coefficient Growth Rate -2.39%

Estimated Annual Growth Rate

$$[(1,774 - 2,095)/2,095] \times 100\% = -15.33\%$$

$$-15.33\%/5 = -3.07\%$$



Fed Ex Freight Growth Rate Determination

AWDT ALL	
Year	AWDT
2011	11,302
2012	11,035
2013	11,557
2014	11,603
2015	12,541

Linear Growth Rate = $\{(12,541-11,302)/4\}/12,541 \times 100 = 2.47\%$

Regression Output	
R Square	0.72
Standard Error	3.50E+02
Observations	5
Intercept	-601,552
Std Err of Intercept	222,728
Coefficient	305
Std Err of Coefficient	1.11E+02

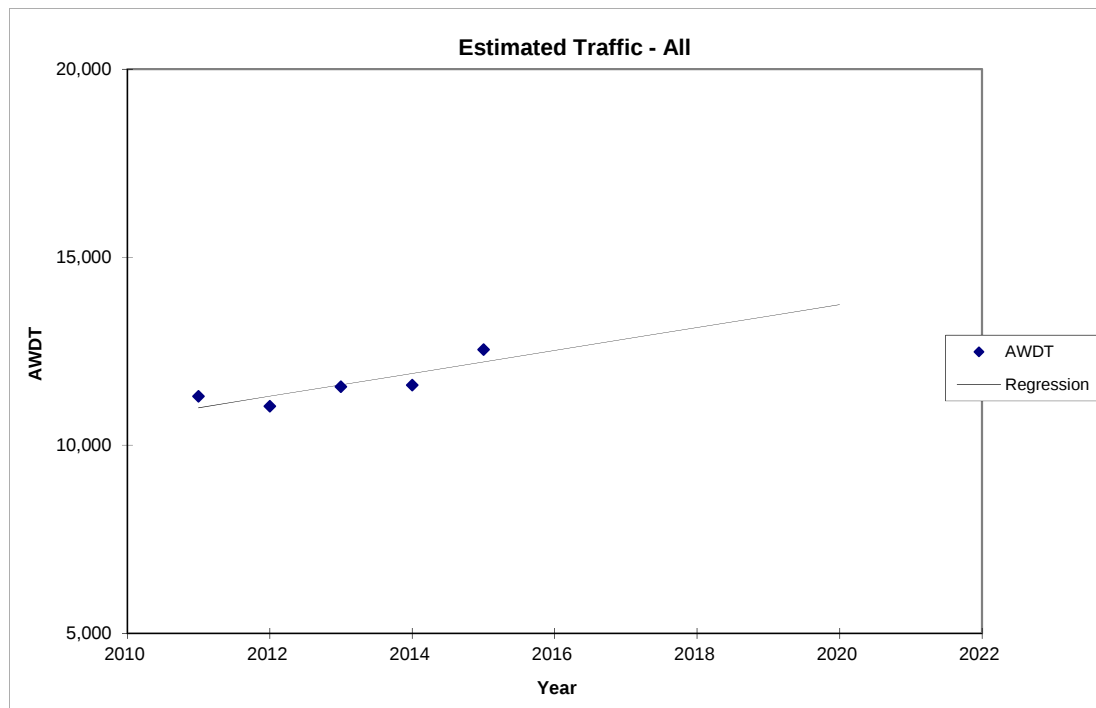
Projected AWDT

2011	10,998
2012	11,303
2013	11,608
2014	11,912
2015	12,217
2016	12,521
2017	12,826
2018	13,131
2019	13,435
2020	13,740

Regression Equation
AWDT = 305x Year - 601,552

USE
Coefficient Growth Rate 2.43%
ROUND TO 2.5%

Estimated Annual Growth Rate
 $\{(13,740-12,541)/12,541\} \times 100\% = 9.56\%$
 $9.56\%/5 = 1.91\%$



Appendix H
2020 No Build Intersection Capacity Analysis

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 3 2020 No Build AM

Report File: C:\...\2020 No Build AM.pdf

12/18/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	NB Thru	0.000	9.6	A
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.002	11.6	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	14.8	B
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.003	23.6	C
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.004	20.7	C
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.330	10.6	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & Tempur Pedit Parkway

Control Type:	Two-way stop	Delay (sec / veh):	9.6
Analysis Method:	HCM 6th Edition	Level Of Service:	A
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPedit Pkwy			TempurPedit Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPedit Pkwy			TempurPedit Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	13	1	8	1	8	8	5	2	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	47.00	2.00	25.00	2.00	75.00	12.50	2.00	2.00	2.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	14	1	9	1	9	9	5	2	0	1
Peak Hour Factor	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	6	0	4	0	4	4	2	1	0	0
Total Analysis Volume [veh/h]	0	0	24	2	16	2	16	16	9	3	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.03	0.00	0.00	0.00	0.01	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	9.03	9.60	8.93	7.27	0.00	0.00	8.05	0.00	0.00	9.07	9.50	8.40
Movement LOS	A	A	A	A	A	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.08	0.08	0.08	0.02	0.01	0.00	0.05	0.03	0.00	0.02	0.02	0.02
95th-Percentile Queue Length [ft]	1.96	1.96	1.96	0.48	0.24	0.00	1.31	0.65	0.00	0.40	0.40	0.40
d_A, Approach Delay [s/veh]	8.93			0.73			3.14			8.80		
Approach LOS	A			A			A			A		
d_I, Intersection Delay [s/veh]	4.46											
Intersection LOS	A											

Intersection Level Of Service Report
Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	11.6
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.002

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	1	14	37	102	184	7
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	80.00	30.00	4.00	3.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	1	15	40	109	197	7
Peak Hour Factor	0.8700	0.8700	0.8700	0.8700	0.8700	0.8700
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	4	11	31	57	2
Total Analysis Volume [veh/h]	1	17	46	125	226	8
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.03	0.04	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	11.61	10.69	8.16	0.00	0.00	0.00
Movement LOS	B	B	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.01	0.08	0.50	0.50	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.14	2.01	12.60	12.60	0.00	0.00
d_A, Approach Delay [s/veh]	10.74		2.20		0.00	
Approach LOS	B		A		A	
d_I, Intersection Delay [s/veh]	1.34					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	14.8
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	2	0	40	0	0	0	95	140	0	0	155	27
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	5.00	2.00	2.00	2.00	10.00	8.00	2.00	2.00	10.00	4.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	2	0	43	0	0	0	102	150	0	0	166	29
Peak Hour Factor	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	1	0	12	0	0	0	28	42	0	0	46	8
Total Analysis Volume [veh/h]	2	0	48	0	0	0	114	167	0	0	185	32
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.05	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	13.94	14.76	9.19	13.80	14.71	8.75	8.05	0.00	0.00	7.56	0.00	0.00
Movement LOS	B	B	A	B	B	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.18	0.18	0.18	0.00	0.00	0.00	0.29	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	4.55	4.55	4.55	0.00	0.00	0.00	7.23	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	9.38			12.42			3.27			0.00		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	2.53											
Intersection LOS	B											

Intersection Level Of Service Report

Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	23.6
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.003

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	194	1	107	81	124	0	0	165	32
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	30.00	50.00	13.00	20.00	10.00	2.00	2.00	13.00	2.00
Growth Rate	1.00	1.00	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	208	1	114	87	133	0	0	177	34
Peak Hour Factor	1.0000	1.0000	1.0000	0.9670	0.9670	0.9670	0.9670	0.9670	1.0000	1.0000	0.9670	0.9670
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	54	0	29	22	34	0	0	46	9
Total Analysis Volume [veh/h]	0	0	0	215	1	118	90	138	0	0	183	35
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.49	0.00	0.12	0.07	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	20.96	23.56	9.35	8.17	0.00	0.00	0.00	0.00	0.00
Movement LOS				C	C	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	2.67	2.67	0.43	0.24	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	66.66	66.66	10.65	5.93	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			16.87			3.22			0.00		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	8.17											
Intersection LOS	C											

Intersection Level Of Service Report**Intersection 5: Atrisco Vista & I-40 Eastbound Ramp**

Control Type:	Two-way stop	Delay (sec / veh):	20.7
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.004

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	189	290	12	1	51	0	0	0	52	279	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	19.00	14.00	17.00	2.00	30.00	2.00	2.00	2.00	12.00	10.00	2.00
Growth Rate	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.00	1.07	1.07	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	202	310	13	1	55	0	0	0	56	299	0
Peak Hour Factor	1.0000	0.9500	0.9500	0.9500	0.9500	0.9500	1.0000	1.0000	1.0000	0.9500	0.9500	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	53	82	3	0	14	0	0	0	15	79	0
Total Analysis Volume [veh/h]	0	213	326	14	1	58	0	0	0	59	315	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.04	0.00	0.07	0.00	0.00	0.00	0.06	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	15.02	20.71	10.00	0.00	0.00	0.00	9.00	0.00	0.00
Movement LOS		A	A	C	C	A				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	0.13	0.13	0.24	0.00	0.00	0.00	0.20	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	3.24	3.24	6.02	0.00	0.00	0.00	4.91	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			11.11			0.00			1.42		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	1.36											
Intersection LOS	C											

**Intersection Level Of Service Report
Intersection 6: Atrisco Vista & Central**

Control Type: All-way stop
 Analysis Method: HCM 6th Edition
 Analysis Period: 15 minutes

Delay (sec / veh): 10.6
 Level Of Service: B
 Volume to Capacity (v/c): 0.330

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	18	227	105	116	151	69	67	18	8	27	35	182
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	17.00	2.00	2.00	25.00	3.00	13.00	32.00	12.00	2.00	8.00	2.00	28.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	19	243	112	124	162	74	72	19	9	29	37	195
Peak Hour Factor	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	64	30	33	43	20	19	5	2	8	10	51
Total Analysis Volume [veh/h]	20	257	118	131	171	78	76	20	10	31	39	206
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	518	579	579	648	505	573	573	623	509	569	637	550	600	600	624
Degree of Utilization, x	0.04	0.22	0.22	0.18	0.26	0.15	0.15	0.13	0.15	0.04	0.02	0.06	0.03	0.03	0.33

Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.12	0.84	0.84	0.66	1.03	0.52	0.52	0.43	0.52	0.11	0.05	0.18	0.10	0.10	1.44
95th-Percentile Queue Length [ft]	3.01	21.11	21.11	16.52	25.72	13.05	13.05	10.65	13.06	2.73	1.20	4.47	2.52	2.52	36.01
Approach Delay [s/veh]	10.29				10.69				10.45				10.77		
Approach LOS	B				B				B				B		
Intersection Delay [s/veh]	10.55														
Intersection LOS	B														

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 3 2020 No Build AM

Report File: C:\...\2020 No Build AM.pdf

12/18/2017

Turning Movement Volume: Summary

ID	Intersection Name	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	0	0	14	1	9	1	9	9	5	2	0	1	51

ID	Intersection Name	Eastbound		Northwestbound		Southeastbound		Total Volume
		Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	1	15	40	109	197	7	369

ID	Intersection Name	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	2	0	43	0	0	0	102	150	0	0	166	29	492

ID	Intersection Name	Southwestbound			Northwestbound		Southeastbound		Total Volume
		Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	208	1	114	87	133	177	34	754

ID	Intersection Name	Northbound		Eastbound			Southeastbound		Total Volume
		Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	202	310	13	1	55	56	299	936

ID	Intersection Name	Northbound			Southbound			Eastbound			Westbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	19	243	112	124	162	74	72	19	9	29	37	195	1095

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 3 2020 No Build AM

Report File: C:\...\2020 No Build AM.pdf

12/18/2017

Turning Movement Volume: Detail

ID	Intersection Name	Volume Type	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	Final Base	0	0	13	1	8	1	8	8	5	2	0	1	47
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	0	0	14	1	9	1	9	9	5	2	0	1	51

ID	Intersection Name	Volume Type	Eastbound		Northwestbound		Southeastbound		Total Volume
			Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	Final Base	1	14	37	102	184	7	345
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Future Total	1	15	40	109	197	7	369

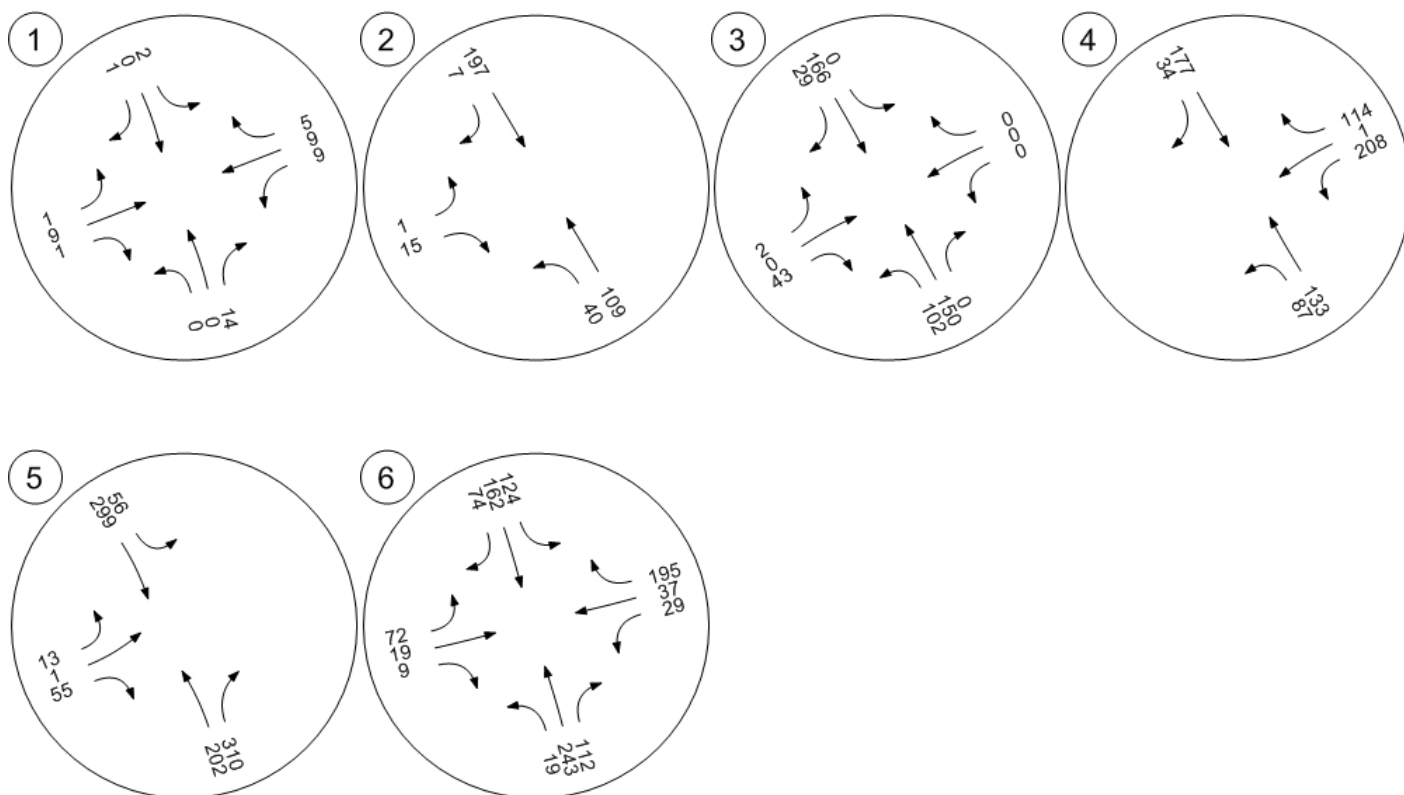
ID	Intersection Name	Volume Type	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	Final Base	2	0	40	0	0	0	95	140	0	0	155	27	459
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	2	0	43	0	0	0	102	150	0	0	166	29	492

ID	Intersection Name	Volume Type	Southwestbound			Northwestbound		Southeastbound		Total Volume
			Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	Final Base	194	1	107	81	124	165	32	704
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0
		Future Total	208	1	114	87	133	177	34	754

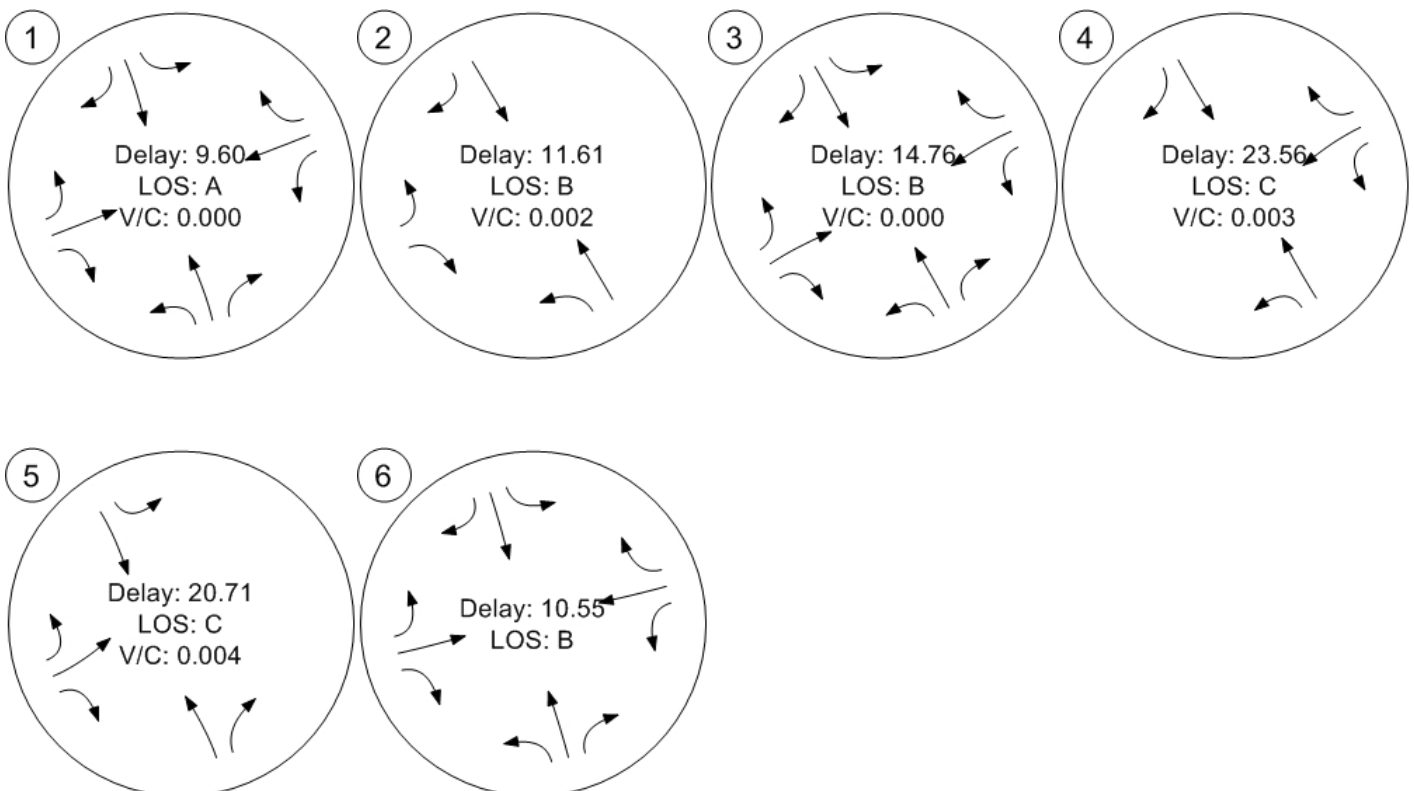
ID	Intersection Name	Volume Type	Northbound		Eastbound			Southeastbound		Total Volume
			Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	Final Base	189	290	12	1	51	52	279	874
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0
		Future Total	202	310	13	1	55	56	299	936

ID	Intersection Name	Volume Type	Northbound			Southbound			Eastbound			Westbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	Final Base	18	227	105	116	151	69	67	18	8	27	35	182	1023
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	19	243	112	124	162	74	72	19	9	29	37	195	1095

Traffic Volume - Future Background Volume



Traffic Conditions



Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 4 2020 No Build PM

Report File: C:\...\2020 No Build PM.pdf

12/18/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	SEB Thru	0.000	11.1	B
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.048	11.9	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	17.3	C
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.005	80.3	F
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.000	36.0	E
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.650	14.5	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & Tempur Pedit Parkway

Control Type:	Two-way stop	Delay (sec / veh):	11.1
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPedit Pkwy			TempurPedit Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPedit Pkwy			TempurPedit Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	10	0	14	0	9	8	2	80	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	33.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	11	0	15	0	10	9	2	86	0	1
Peak Hour Factor	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	7	0	9	0	6	5	1	52	0	1
Total Analysis Volume [veh/h]	0	0	27	0	36	0	24	22	5	208	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.03	0.00	0.00	0.00	0.02	0.00	0.00	0.24	0.00	0.00
d_M, Delay for Movement [s/veh]	9.25	9.80	8.50	7.27	0.00	0.00	7.67	0.00	0.00	10.61	11.07	9.76
Movement LOS	A	A	A	A	A	A	A	A	A	B	B	A
95th-Percentile Queue Length [veh]	0.08	0.08	0.08	0.00	0.00	0.00	0.06	0.03	0.00	0.97	0.97	0.97
95th-Percentile Queue Length [ft]	1.97	1.97	1.97	0.00	0.00	0.00	1.42	0.71	0.00	24.26	24.26	24.26
d_A, Approach Delay [s/veh]	8.50			0.00			3.61			10.61		
Approach LOS	A			A			A			B		
d_I, Intersection Delay [s/veh]	8.15											
Intersection LOS	B											

Intersection Level Of Service Report

Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	11.9
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.048

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	20	108	30	183	88	10
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	4.00	2.00	6.00	10.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	21	116	32	196	94	11
Peak Hour Factor	0.7950	0.7950	0.7950	0.7950	0.7950	0.7950
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	7	36	10	62	30	3
Total Analysis Volume [veh/h]	26	146	40	247	118	14
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.05	0.16	0.03	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	11.91	9.62	7.57	0.00	0.00	0.00
Movement LOS	B	A	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.15	0.56	0.74	0.74	0.00	0.00
95th-Percentile Queue Length [ft/ln]	3.73	13.97	18.56	18.56	0.00	0.00
d_A, Approach Delay [s/veh]	9.96		1.06		0.00	
Approach LOS	A		A		A	
d_I, Intersection Delay [s/veh]	3.41					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	17.3
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	17	0	133	0	0	0	87	192	0	0	181	6
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	6.00	2.00	15.00	2.00	2.00	2.00	14.00	2.00	2.00	2.00	3.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	18	0	142	0	0	0	93	205	0	0	194	6
Peak Hour Factor	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	0	41	0	0	0	27	60	0	0	57	2
Total Analysis Volume [veh/h]	21	0	166	0	0	0	109	239	0	0	227	7
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.06	0.00	0.19	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	16.26	17.27	10.67	16.79	15.77	8.96	8.16	0.00	0.00	7.72	0.00	0.00
Movement LOS	C	C	B	C	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.97	0.97	0.97	0.00	0.00	0.00	0.29	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	24.25	24.25	24.25	0.00	0.00	0.00	7.17	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	11.30			13.84			2.56			0.00		
Approach LOS	B			B			A			A		
d_I, Intersection Delay [s/veh]	3.91											
Intersection LOS	C											

Intersection Level Of Service Report

Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	80.3
Analysis Method:	HCM 6th Edition	Level Of Service:	F
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.005

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	192	1	81	143	195	0	0	294	23
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	11.00	2.00	7.00	12.00	5.00	2.00	2.00	7.00	9.00
Growth Rate	1.00	1.00	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	205	1	87	153	209	0	0	315	25
Peak Hour Factor	1.0000	1.0000	1.0000	0.9000	0.9000	0.9000	0.9000	0.9000	1.0000	1.0000	0.9000	0.9000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	57	0	24	43	58	0	0	88	7
Total Analysis Volume [veh/h]	0	0	0	228	1	97	170	232	0	0	350	28
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.91	0.00	0.11	0.15	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	78.12	80.26	9.49	8.84	0.00	0.00	0.00	0.00	0.00
Movement LOS				F	F	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	7.95	7.95	0.36	0.54	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	198.83	198.83	9.04	13.52	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			57.70			3.74			0.00		
Approach LOS	A			F			A			A		
d_I, Intersection Delay [s/veh]	18.37											
Intersection LOS	F											

Intersection Level Of Service Report**Intersection 5: Atrisco Vista & I-40 Eastbound Ramp**

Control Type:	Two-way stop	Delay (sec / veh):	36.0
Analysis Method:	HCM 6th Edition	Level Of Service:	E
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	269	168	57	0	125	0	0	0	149	311	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	9.00	22.00	6.00	2.00	26.00	2.00	2.00	2.00	6.00	11.00	2.00
Growth Rate	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.00	1.07	1.07	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	288	180	61	0	134	0	0	0	159	333	0
Peak Hour Factor	1.0000	0.9280	0.9280	0.9280	0.9280	0.9280	1.0000	1.0000	1.0000	0.9280	0.9280	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	78	48	16	0	36	0	0	0	43	90	0
Total Analysis Volume [veh/h]	0	310	194	66	0	144	0	0	0	171	359	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.31	0.00	0.19	0.00	0.00	0.00	0.17	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	29.22	36.02	10.82	0.00	0.00	0.00	9.19	0.00	0.00
Movement LOS		A	A	D	E	B				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	1.26	1.26	0.69	0.00	0.00	0.00	0.59	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	31.40	31.40	17.31	0.00	0.00	0.00	14.86	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			16.60			0.00			2.97		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	4.07											
Intersection LOS	E											

**Intersection Level Of Service Report
Intersection 6: Atrisco Vista & Central**

Control Type: All-way stop
 Analysis Method: HCM 6th Edition
 Analysis Period: 15 minutes

Delay (sec / veh): 14.5
 Level Of Service: B
 Volume to Capacity (v/c): 0.650

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	7	141	81	263	170	35	58	28	15	85	20	254
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	15.00	3.00	3.00	23.00	4.00	15.00	11.00	15.00	40.00	6.00	15.00	21.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	7	151	87	281	182	37	62	30	16	91	21	272
Peak Hour Factor	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	2	42	24	78	51	10	17	8	4	25	6	76
Total Analysis Volume [veh/h]	8	168	97	313	203	41	69	33	18	101	23	303
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	460	504	504	557	482	540	540	583	472	499	535	519	544	544	592
Degree of Utilization, x	0.02	0.17	0.17	0.17	0.65	0.19	0.19	0.07	0.15	0.07	0.03	0.19	0.02	0.02	0.51

Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.05	0.59	0.59	0.63	4.58	0.69	0.69	0.23	0.51	0.21	0.10	0.71	0.06	0.06	2.92
95th-Percentile Queue Length [ft]	1.33	14.83	14.83	15.66	114.4	17.17	17.17	5.65	12.71	5.30	2.61	17.87	1.62	1.62	72.88
Approach Delay [s/veh]	10.99				17.59				11.00			13.83			
Approach LOS	B				C				B			B			
Intersection Delay [s/veh]	14.54														
Intersection LOS	B														

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 4 2020 No Build PM

Report File: C:\...\2020 No Build PM.pdf

12/18/2017

Turning Movement Volume: Summary

ID	Intersection Name	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	0	0	11	0	15	0	10	9	2	86	0	1	134

ID	Intersection Name	Eastbound		Northwestbound		Southeastbound		Total Volume
		Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	21	116	32	196	94	11	470

ID	Intersection Name	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	18	0	142	0	0	0	93	205	0	0	194	6	658

ID	Intersection Name	Southwestbound			Northwestbound		Southeastbound		Total Volume
		Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	205	1	87	153	209	315	25	995

ID	Intersection Name	Northbound		Eastbound			Southeastbound		Total Volume
		Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	288	180	61	0	134	159	333	1155

ID	Intersection Name	Northbound			Southbound			Eastbound			Westbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	7	151	87	281	182	37	62	30	16	91	21	272	1237

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 4 2020 No Build PM

Report File: C:\...\2020 No Build PM.pdf

12/18/2017

Turning Movement Volume: Detail

ID	Intersection Name	Volume Type	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	Final Base	0	0	10	0	14	0	9	8	2	80	0	1	124
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	0	0	11	0	15	0	10	9	2	86	0	1	134

ID	Intersection Name	Volume Type	Eastbound		Northwestbound		Southeastbound		Total Volume
			Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	Final Base	20	108	30	183	88	10	439
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Future Total	21	116	32	196	94	11	470

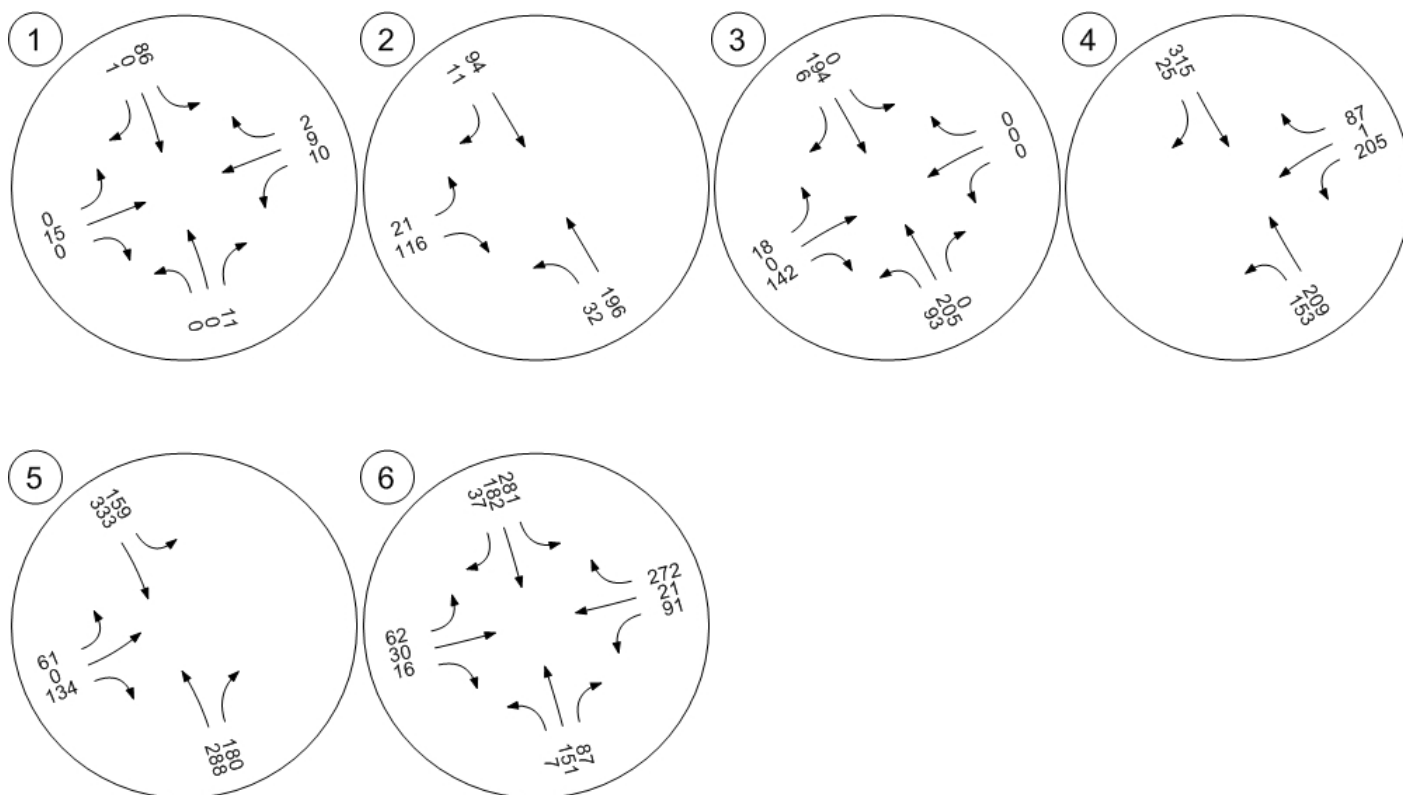
ID	Intersection Name	Volume Type	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	Final Base	17	0	133	0	0	0	87	192	0	0	181	6	616
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	18	0	142	0	0	0	93	205	0	0	194	6	658

ID	Intersection Name	Volume Type	Southwestbound			Northwestbound		Southeastbound		Total Volume
			Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	Final Base	192	1	81	143	195	294	23	929
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0
		Future Total	205	1	87	153	209	315	25	995

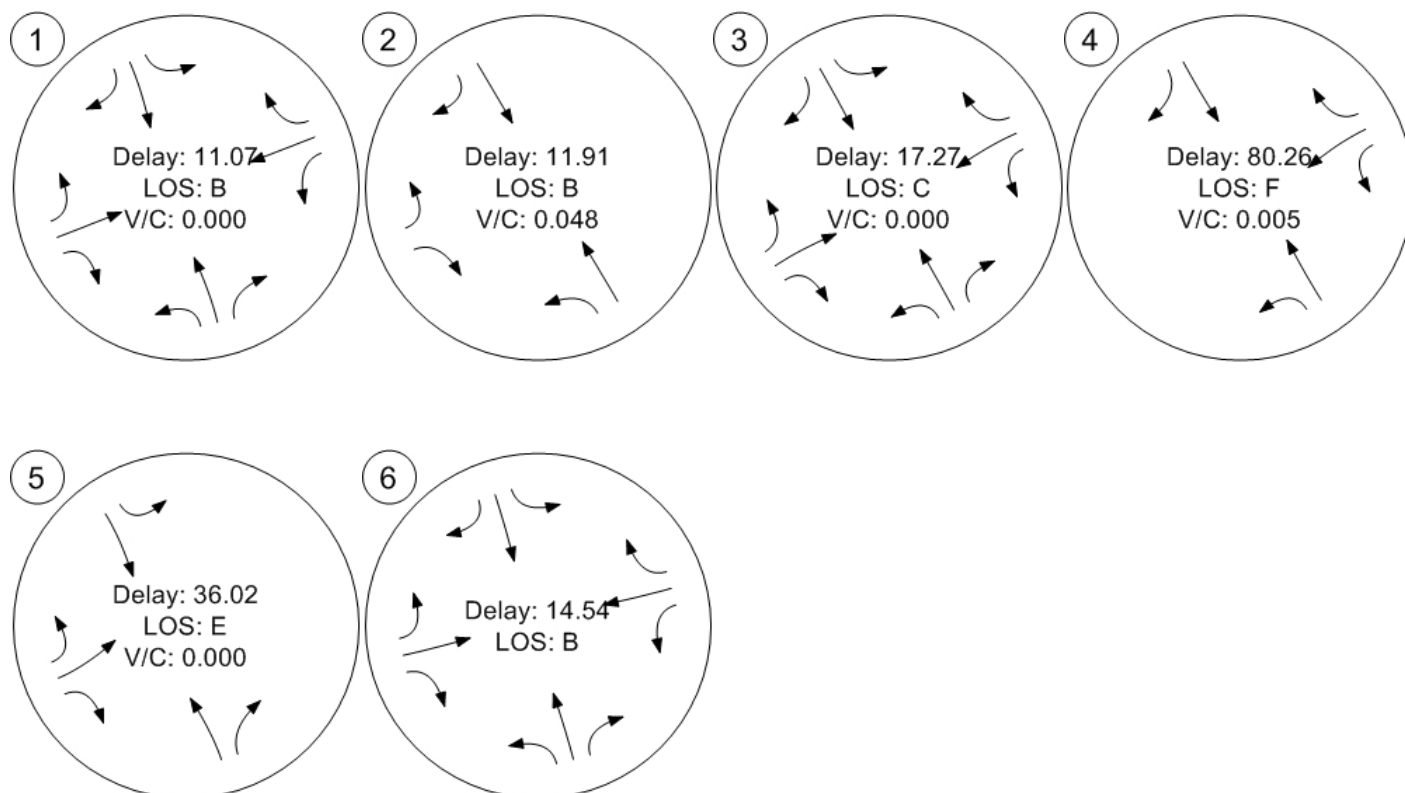
ID	Intersection Name	Volume Type	Northbound		Eastbound			Southeastbound		Total Volume
			Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	Final Base	269	168	57	0	125	149	311	1079
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0
		Future Total	288	180	61	0	134	159	333	1155

ID	Intersection Name	Volume Type	Northbound			Southbound			Eastbound			Westbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	Final Base	7	141	81	263	170	35	58	28	15	85	20	254	1157
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	7	151	87	281	182	37	62	30	16	91	21	272	1237

Traffic Volume - Future Background Volume



Traffic Conditions



Appendix I
2020 Build Intersection Capacity Analysis

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 5 2020 Build AM

Report File: C:\...\2020 Build AM.pdf

12/18/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	NB Thru	0.000	10.9	B
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.002	12.7	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	15.4	C
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.003	24.9	C
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.004	21.5	C
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.344	10.6	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & Tempur Peditc Parkway

Control Type:	Two-way stop	Delay (sec / veh):	10.9
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPeditc Pkwy			TempurPeditc Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	13	1	8	1	8	8	5	2	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	50.00	2.00	25.00	2.00	75.00	12.50	2.00	2.00	2.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	9	0	0	0	42	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	23	1	9	1	51	9	5	2	0	1
Peak Hour Factor	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800	0.5800
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	10	0	4	0	22	4	2	1	0	0
Total Analysis Volume [veh/h]	0	0	40	2	16	2	88	16	9	3	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.04	0.00	0.00	0.00	0.07	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	10.38	10.94	9.03	7.27	0.00	0.00	8.25	0.00	0.00	10.49	10.77	8.41
Movement LOS	B	B	A	A	A	A	A	A	A	B	B	A
95th-Percentile Queue Length [veh]	0.13	0.13	0.13	0.02	0.01	0.00	0.24	0.12	0.00	0.02	0.02	0.02
95th-Percentile Queue Length [ft]	3.35	3.35	3.35	0.48	0.24	0.00	5.95	2.97	0.00	0.49	0.49	0.49
d_A, Approach Delay [s/veh]	9.03			0.73			6.43			9.66		
Approach LOS	A			A			A			A		
d_I, Intersection Delay [s/veh]	6.46											
Intersection LOS	B											

Intersection Level Of Service Report

Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	12.7
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.002

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	1	14	37	102	184	7
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	80.00	30.00	4.00	3.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	9	34	0	0	8
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	1	24	74	109	197	15
Peak Hour Factor	0.8700	0.8700	0.8700	0.8700	0.8700	0.8700
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	7	21	31	57	4
Total Analysis Volume [veh/h]	1	28	85	125	226	17
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.04	0.07	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	12.67	10.83	8.30	0.00	0.00	0.00
Movement LOS	B	B	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.01	0.14	0.65	0.65	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.16	3.40	16.22	16.22	0.00	0.00
d_A, Approach Delay [s/veh]	10.90		3.36		0.00	
Approach LOS	B		A		A	
d_I, Intersection Delay [s/veh]	2.12					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	15.4
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	2	0	40	0	0	0	95	140	0	0	155	27
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	5.00	2.00	2.00	2.00	10.00	8.00	2.00	2.00	10.00	4.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	2	0	0	0	0	0	0	32	0	0	9	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	4	0	43	0	0	0	102	182	0	0	175	29
Peak Hour Factor	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960	0.8960
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	1	0	12	0	0	0	28	51	0	0	49	8
Total Analysis Volume [veh/h]	4	0	48	0	0	0	114	203	0	0	195	32
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.01	0.00	0.05	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	14.39	15.40	9.26	14.42	15.33	8.85	8.08	0.00	0.00	7.64	0.00	0.00
Movement LOS	B	C	A	B	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.20	0.20	0.20	0.00	0.00	0.00	0.29	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	5.03	5.03	5.03	0.00	0.00	0.00	7.30	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	9.65			12.87			2.91			0.00		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	2.39											
Intersection LOS	C											

Intersection Level Of Service Report

Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	24.9
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.003

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	194	1	107	81	124	0	0	165	32
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	30.00	50.00	13.00	20.00	10.00	2.00	2.00	13.00	2.00
Growth Rate	1.00	1.00	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	13	0	19	0	0	5	4
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	208	1	127	87	152	0	0	182	38
Peak Hour Factor	1.0000	1.0000	1.0000	0.9670	0.9670	0.9670	0.9670	0.9670	1.0000	1.0000	0.9670	0.9670
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	54	0	33	22	39	0	0	47	10
Total Analysis Volume [veh/h]	0	0	0	215	1	131	90	157	0	0	188	39
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.51	0.00	0.14	0.07	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	22.19	24.95	9.49	8.19	0.00	0.00	0.00	0.00	0.00
Movement LOS				C	C	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	2.84	2.84	0.49	0.24	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	71.07	71.07	12.21	5.98	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			17.40			2.99			0.00		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	8.25											
Intersection LOS	C											

Intersection Level Of Service Report
Intersection 5: Atrisco Vista & I-40 Eastbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	21.5
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.004

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	189	290	12	1	51	0	0	0	52	279	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	19.00	14.00	17.00	2.00	30.00	2.00	2.00	2.00	12.00	10.00	2.00
Growth Rate	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.00	1.07	1.07	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	17	0	2	0	0	0	0	0	5	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	219	310	15	1	55	0	0	0	61	299	0
Peak Hour Factor	1.0000	0.9500	0.9500	0.9500	0.9500	0.9500	1.0000	1.0000	1.0000	0.9500	0.9500	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	58	82	4	0	14	0	0	0	16	79	0
Total Analysis Volume [veh/h]	0	231	326	16	1	58	0	0	0	64	315	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.04	0.00	0.07	0.00	0.00	0.00	0.07	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	15.49	21.51	10.00	0.00	0.00	0.00	9.09	0.00	0.00
Movement LOS		A	A	C	C	A				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	0.15	0.15	0.24	0.00	0.00	0.00	0.22	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	3.83	3.83	6.02	0.00	0.00	0.00	5.45	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			11.32			0.00			1.54		
Approach LOS	A			B			A			A		
d_I, Intersection Delay [s/veh]	1.42											
Intersection LOS	C											

**Intersection Level Of Service Report
Intersection 6: Atrisco Vista & Central**

Control Type: All-way stop
 Analysis Method: HCM 6th Edition
 Analysis Period: 15 minutes

Delay (sec / veh): 10.6
 Level Of Service: B
 Volume to Capacity (v/c): 0.344

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	18	227	105	116	151	69	67	18	8	27	35	182
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	17.00	2.00	2.00	25.00	3.00	13.00	32.00	12.00	2.00	8.00	2.00	28.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	9	0	0	0	0	0	0	0	0	0	8
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	19	252	112	124	162	74	72	19	9	29	37	203
Peak Hour Factor	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470	0.9470
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	67	30	33	43	20	19	5	2	8	10	54
Total Analysis Volume [veh/h]	20	266	118	131	171	78	76	20	10	31	39	214
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	516	576	576	645	502	569	569	619	507	567	634	550	599	599	623
Degree of Utilization, x	0.04	0.23	0.23	0.18	0.26	0.15	0.15	0.13	0.15	0.04	0.02	0.06	0.03	0.03	0.34

Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.12	0.89	0.89	0.66	1.04	0.53	0.53	0.43	0.52	0.11	0.05	0.18	0.10	0.10	1.53
95th-Percentile Queue Length [ft]	3.02	22.17	22.17	16.61	25.90	13.14	13.14	10.73	13.10	2.74	1.20	4.47	2.52	2.52	38.15
Approach Delay [s/veh]	10.40				10.75				10.47			10.93			
Approach LOS	B				B				B			B			
Intersection Delay [s/veh]	10.65														
Intersection LOS	B														

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 5 2020 Build AM

Report File: C:\...\2020 Build AM.pdf

12/18/2017

Turning Movement Volume: Summary

ID	Intersection Name	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	0	0	23	1	9	1	51	9	5	2	0	1	102

ID	Intersection Name	Eastbound		Northwestbound		Southeastbound		Total Volume
		Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	1	24	74	109	197	15	420

ID	Intersection Name	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	4	0	43	0	0	0	102	182	0	0	175	29	535

ID	Intersection Name	Southwestbound			Northwestbound		Southeastbound		Total Volume
		Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	208	1	127	87	152	182	38	795

ID	Intersection Name	Northbound		Eastbound			Southeastbound		Total Volume
		Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	219	310	15	1	55	61	299	960

ID	Intersection Name	Northbound			Southbound			Eastbound			Westbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	19	252	112	124	162	74	72	19	9	29	37	203	1112

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 5 2020 Build AM

Report File: C:\...\2020 Build AM.pdf

12/18/2017

Turning Movement Volume: Detail

ID	Intersection Name	Volume Type	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	Final Base	0	0	13	1	8	1	8	8	5	2	0	1	47
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	9	0	0	0	42	0	0	0	0	0	51
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	0	0	23	1	9	1	51	9	5	2	0	1	102

ID	Intersection Name	Volume Type	Eastbound		Northwestbound		Southeastbound		Total Volume
			Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	Final Base	1	14	37	102	184	7	345
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0
		Net New Trips	0	9	34	0	0	8	51
		Other	0	0	0	0	0	0	0
		Future Total	1	24	74	109	197	15	420

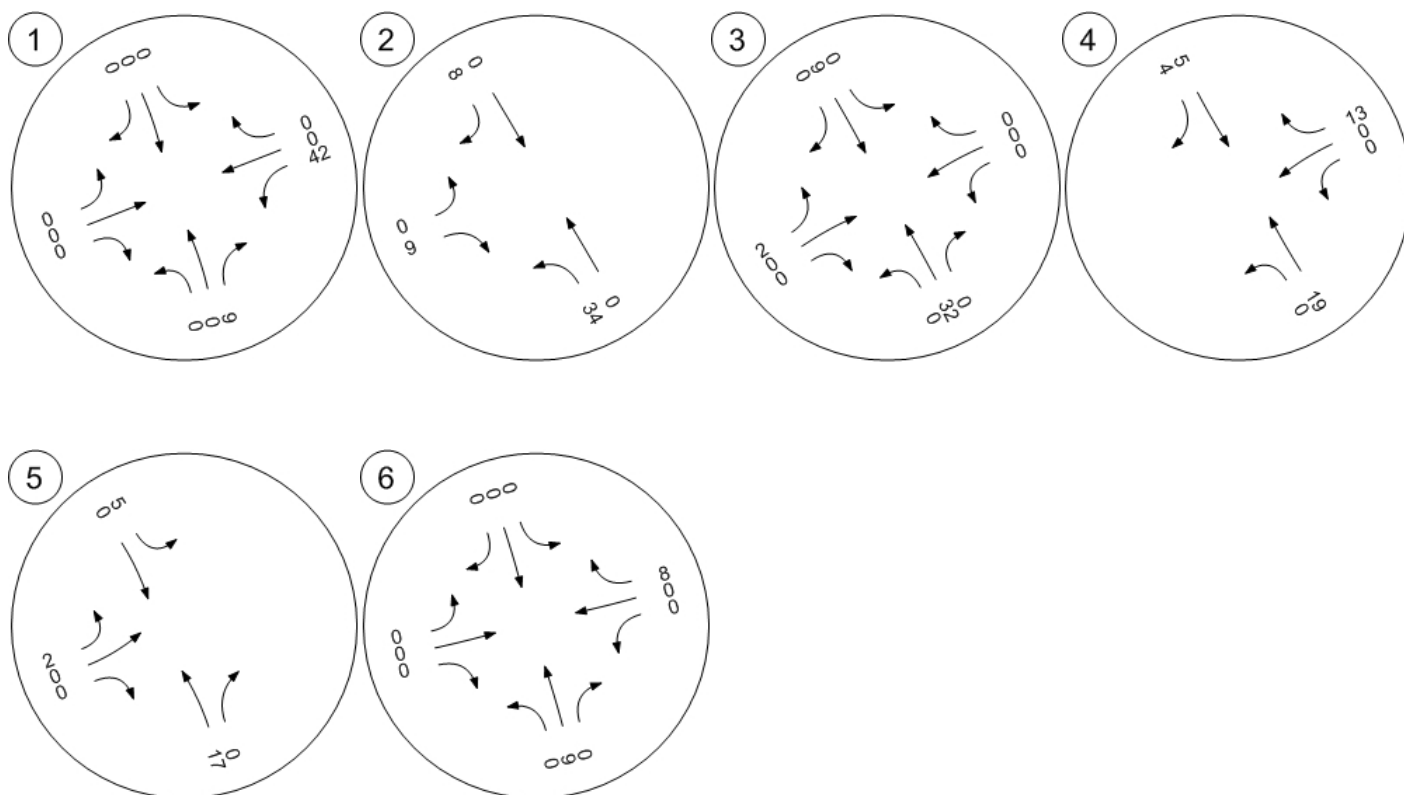
ID	Intersection Name	Volume Type	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	Final Base	2	0	40	0	0	0	95	140	0	0	155	27	459
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	2	0	0	0	0	0	0	32	0	0	9	0	43
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	4	0	43	0	0	0	102	182	0	0	175	29	535

ID	Intersection Name	Volume Type	Southwestbound			Northwestbound		Southeastbound		Total Volume
			Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	Final Base	194	1	107	81	124	165	32	704
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	13	0	19	5	4	41
		Other	0	0	0	0	0	0	0	0
		Future Total	208	1	127	87	152	182	38	795

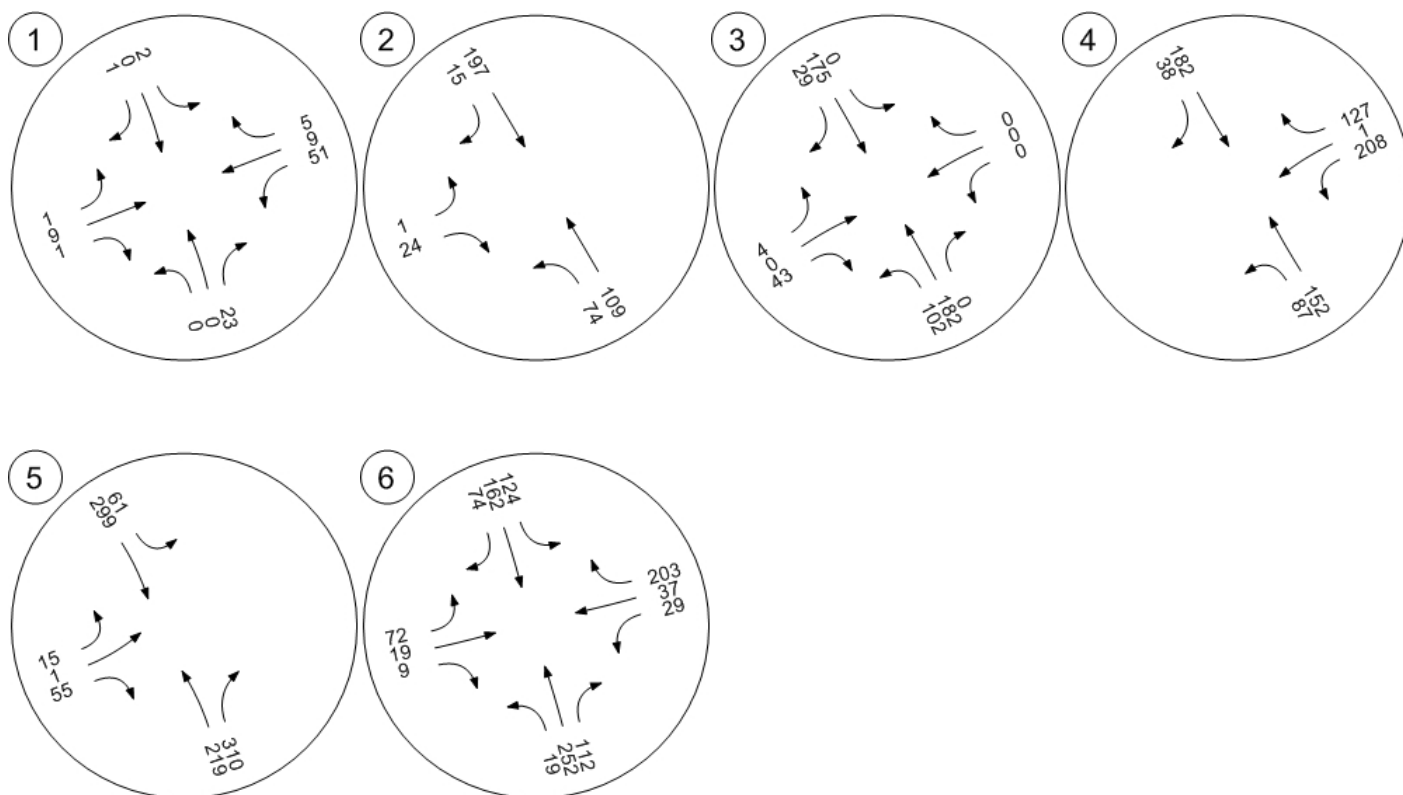
ID	Intersection Name	Volume Type	Northbound		Eastbound			Southeastbound		Total Volume
			Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	Final Base	189	290	12	1	51	52	279	874
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	17	0	2	0	0	5	0	24
		Other	0	0	0	0	0	0	0	0
		Future Total	219	310	15	1	55	61	299	960

ID	Intersection Name	Volume Type	Northbound			Southbound			Eastbound			Westbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	Final Base	18	227	105	116	151	69	67	18	8	27	35	182	1023
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	9	0	0	0	0	0	0	0	0	0	8	17
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	19	252	112	124	162	74	72	19	9	29	37	203	1112

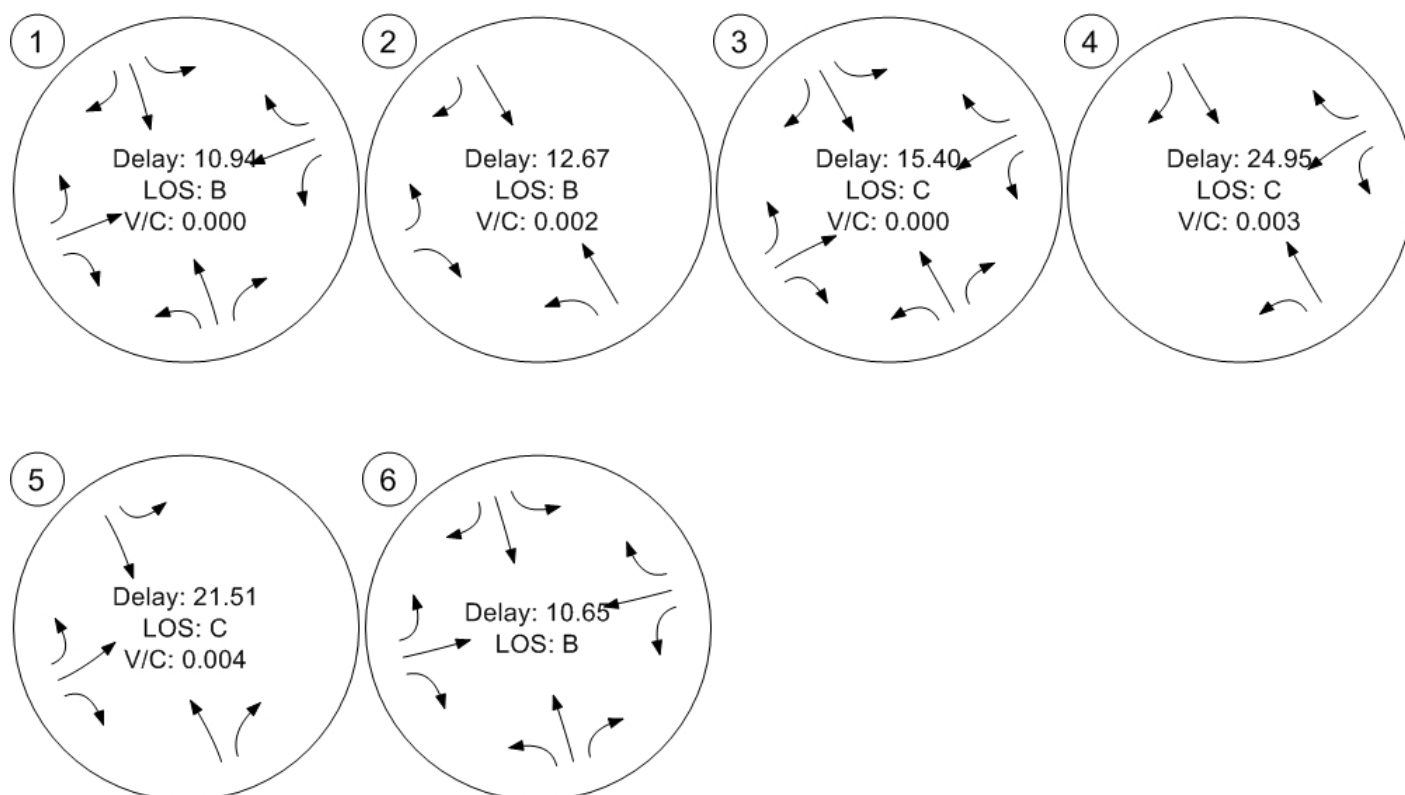
Traffic Volume - Net New Site Trips



Traffic Volume - Future Total Volume



Traffic Conditions



Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 6 2020 Build PM

Report File: C:\...\2020 Build PM.pdf

12/18/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
1	Shamrock Way & Tempur Pedic Parkway	Two-way stop	HCM 6th Edition	SEB Thru	0.000	12.9	B
2	Atrisco Vista & TempurPedic Parkway	Two-way stop	HCM 6th Edition	EB Left	0.065	12.6	B
3	Atrisco Vista & North Frontage/Bluewater	Two-way stop	HCM 6th Edition	NEB Thru	0.000	18.0	C
4	Atrisco Vista & I-40 Westbound Ramp	Two-way stop	HCM 6th Edition	SWB Thru	0.005	90.1	F
5	Atrisco Vista & I-40 Eastbound Ramp	Two-way stop	HCM 6th Edition	EB Thru	0.000	39.7	E
6	Atrisco Vista & Central	All-way stop	HCM 6th Edition	SB Left	0.661	14.7	B

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.

Intersection Level Of Service Report

Intersection 1: Shamrock Way & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	12.9
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Sharmrock Way			TempurPedic Pkwy			TempurPedic Pkwy			Tempur Entrance		
Approach	Northbound			Eastbound			Westbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Sharmrock Way			TempurPedic Pkwy			TempurPedic Pkwy			Tempur Entrance		
Base Volume Input [veh/h]	0	0	10	0	14	0	9	8	2	80	0	1
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	33.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	24	0	0	0	19	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	35	0	15	0	29	9	2	86	0	1
Peak Hour Factor	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130	0.4130
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	21	0	9	0	18	5	1	52	0	1
Total Analysis Volume [veh/h]	0	0	85	0	36	0	70	22	5	208	0	2
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Free	Free	Stop
Flared Lane	No			No
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No			No
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.08	0.00	0.00	0.00	0.05	0.00	0.00	0.31	0.00	0.00
d_M, Delay for Movement [s/veh]	10.26	10.78	8.71	7.27	0.00	0.00	7.76	0.00	0.00	12.73	12.85	10.77
Movement LOS	B	B	A	A	A	A	A	A	A	B	B	B
95th-Percentile Queue Length [veh]	0.26	0.26	0.26	0.00	0.00	0.00	0.16	0.08	0.00	1.32	1.32	1.32
95th-Percentile Queue Length [ft]	6.55	6.55	6.55	0.00	0.00	0.00	4.02	2.01	0.00	33.09	33.09	33.09
d_A, Approach Delay [s/veh]	8.71			0.00			5.60			12.72		
Approach LOS	A			A			A			B		
d_I, Intersection Delay [s/veh]	9.24											
Intersection LOS	B											

Intersection Level Of Service Report

Intersection 2: Atrisco Vista & TempurPedic Parkway

Control Type:	Two-way stop	Delay (sec / veh):	12.6
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.065

Intersection Setup

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Approach	Eastbound		Northwestbound		Southeastbound	
Lane Configuration						
Turning Movement	Left	Right	Left	Thru	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00		45.00		45.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	TempurPedic Pkwy		Atrisco Vista		Atrisco Vista	
Base Volume Input [veh/h]	20	108	30	183	88	10
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	4.00	2.00	6.00	10.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	5	19	18	0	0	1
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	26	135	50	196	94	12
Peak Hour Factor	0.7950	0.7950	0.7950	0.7950	0.7950	0.7950
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	8	42	16	62	30	4
Total Analysis Volume [veh/h]	33	170	63	247	118	15
Pedestrian Volume [ped/h]	0		0		0	

Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.07	0.18	0.04	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	12.64	9.77	7.62	0.00	0.00	0.00
Movement LOS	B	A	A	A	A	A
95th-Percentile Queue Length [veh/ln]	0.21	0.67	0.82	0.82	0.00	0.00
95th-Percentile Queue Length [ft/ln]	5.23	16.77	20.47	20.47	0.00	0.00
d_A, Approach Delay [s/veh]	10.23		1.55		0.00	
Approach LOS	B		A		A	
d_I, Intersection Delay [s/veh]	3.96					
Intersection LOS	B					

Intersection Level Of Service Report**Intersection 3: Atrisco Vista & North Frontage/Bluewater**

Control Type:	Two-way stop	Delay (sec / veh):	18.0
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	45.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	North Frontage			Bluewater			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	17	0	133	0	0	0	87	192	0	0	181	6
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	6.00	2.00	15.00	2.00	2.00	2.00	14.00	2.00	2.00	2.00	3.00	2.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0	0	18	0	0	19	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	18	0	142	0	0	0	93	223	0	0	213	6
Peak Hour Factor	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560	0.8560
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	5	0	41	0	0	0	27	65	0	0	62	2
Total Analysis Volume [veh/h]	21	0	166	0	0	0	109	261	0	0	249	7
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.06	0.00	0.19	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	16.93	18.04	10.84	17.52	16.44	9.02	8.23	0.00	0.00	7.77	0.00	0.00
Movement LOS	C	C	B	C	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	1.00	1.00	1.00	0.00	0.00	0.00	0.29	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	25.08	25.08	25.08	0.00	0.00	0.00	7.32	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	11.52			14.33			2.42			0.00		
Approach LOS	B			B			A			A		
d_I, Intersection Delay [s/veh]	3.75											
Intersection LOS	C											

Intersection Level Of Service Report

Intersection 4: Atrisco Vista & I-40 Westbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	90.1
Analysis Method:	HCM 6th Edition	Level Of Service:	F
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.005

Intersection Setup

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	1	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	30.00			30.00			45.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			No			No		

Volumes

Name	On-Ramp			Off-Ramp			Atrisco Vista			Atrisco Vista		
Base Volume Input [veh/h]	0	0	0	192	1	81	143	195	0	0	294	23
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	11.00	2.00	7.00	12.00	5.00	2.00	2.00	7.00	9.00
Growth Rate	1.00	1.00	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	9	0	9	0	0	19	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	0	0	205	1	96	153	218	0	0	334	25
Peak Hour Factor	1.0000	1.0000	1.0000	0.9000	0.9000	0.9000	0.9000	0.9000	1.0000	1.0000	0.9000	0.9000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	0	0	57	0	27	43	61	0	0	93	7
Total Analysis Volume [veh/h]	0	0	0	228	1	107	170	242	0	0	371	28
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.94	0.00	0.12	0.16	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	87.75	90.11	9.59	8.92	0.00	0.00	0.00	0.00	0.00
Movement LOS				F	F	A	A	A			A	A
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	8.46	8.46	0.41	0.55	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	211.57	211.57	10.18	13.83	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			62.86			3.68			0.00		
Approach LOS	A			F			A			A		
d_I, Intersection Delay [s/veh]	19.74											
Intersection LOS	F											

Intersection Level Of Service Report
Intersection 5: Atrisco Vista & I-40 Eastbound Ramp

Control Type:	Two-way stop	Delay (sec / veh):	39.7
Analysis Method:	HCM 6th Edition	Level Of Service:	E
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Approach	Northbound			Eastbound			Southwestbound			Southeastbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	225.00	100.00	100.00
Speed [mph]	45.00			40.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			Yes			Yes			No		

Volumes

Name	Atrisco Vista			Off-Ramp			On-Ramp			Atrisco Vista		
Base Volume Input [veh/h]	0	269	168	57	0	125	0	0	0	149	311	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	9.00	22.00	6.00	2.00	26.00	2.00	2.00	2.00	6.00	11.00	2.00
Growth Rate	1.00	1.07	1.07	1.07	1.07	1.07	1.00	1.00	1.00	1.07	1.07	1.00
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	3	0	6	0	0	0	0	0	7	12	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	291	180	67	0	134	0	0	0	166	345	0
Peak Hour Factor	1.0000	0.9280	0.9280	0.9280	0.9280	0.9280	1.0000	1.0000	1.0000	0.9280	0.9280	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	78	48	18	0	36	0	0	0	45	93	0
Total Analysis Volume [veh/h]	0	314	194	72	0	144	0	0	0	179	372	0
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings

Priority Scheme	Free	Stop	Stop	Free
Flared Lane				
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance		No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.36	0.00	0.19	0.00	0.00	0.00	0.17	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	0.00	0.00	32.55	39.66	10.89	0.00	0.00	0.00	9.25	0.00	0.00
Movement LOS		A	A	D	E	B				A	A	
95th-Percentile Queue Length [veh]	0.00	0.00	0.00	1.53	1.53	0.70	0.00	0.00	0.00	0.63	0.00	0.00
95th-Percentile Queue Length [ft]	0.00	0.00	0.00	38.19	38.19	17.53	0.00	0.00	0.00	15.76	0.00	0.00
d_A, Approach Delay [s/veh]	0.00			18.11			0.00			3.01		
Approach LOS	A			C			A			A		
d_I, Intersection Delay [s/veh]	4.37											
Intersection LOS	E											

Intersection Level Of Service Report Intersection 6: Atrisco Vista & Central

Control Type: All-way stop
Analysis Method: HCM 6th Edition
Analysis Period: 15 minutes

Delay (sec / veh): 14.7
Level Of Service: B
Volume to Capacity (v/c): 0.661

Intersection Setup

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration												
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	1	0	1	1	0	1	1	0	0	1	0	1
Pocket Length [ft]	200.00	100.00	200.00	200.00	100.00	250.00	200.00	100.00	100.00	200.00	100.00	200.00
Speed [mph]	30.00			30.00			30.00			30.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	Yes			Yes			Yes			Yes		

Volumes

Name	Atrisco Vista			Atrisco Vista			Central			Central		
Base Volume Input [veh/h]	7	141	81	263	170	35	58	28	15	85	20	254
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	15.00	3.00	3.00	23.00	4.00	15.00	11.00	15.00	40.00	6.00	15.00	21.00
Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	2	0	5	7	0	0	0	0	0	0	1
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	7	153	87	286	189	37	62	30	16	91	21	273
Peak Hour Factor	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980	0.8980
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	2	43	24	80	53	10	17	8	4	25	6	76
Total Analysis Volume [veh/h]	8	170	97	318	210	41	69	33	18	101	23	304
Pedestrian Volume [ped/h]	0			0			0			0		

Intersection Settings**Lanes**

Capacity per Entry Lane [veh/h]	459	503	503	554	481	538	538	582	470	497	534	518	543	543	590
Degree of Utilization, x	0.02	0.17	0.17	0.17	0.66	0.19	0.19	0.07	0.15	0.07	0.03	0.19	0.02	0.02	0.52

Movement, Approach, & Intersection Results

95th-Percentile Queue Length [veh]	0.05	0.60	0.60	0.63	4.75	0.72	0.72	0.23	0.51	0.21	0.10	0.72	0.06	0.06	2.95
95th-Percentile Queue Length [ft]	1.33	15.10	15.10	15.73	118.7	17.93	17.93	5.67	12.75	5.32	2.61	17.92	1.62	1.62	73.72
Approach Delay [s/veh]	11.04				17.93				11.03			13.92			
Approach LOS	B				C				B			B			
Intersection Delay [s/veh]	14.74														
Intersection LOS	B														

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 6 2020 Build PM

Report File: C:\...\2020 Build PM.pdf

12/18/2017

Turning Movement Volume: Summary

ID	Intersection Name	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	0	0	35	0	15	0	29	9	2	86	0	1	177

ID	Intersection Name	Eastbound		Northwestbound		Southeastbound		Total Volume
		Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	26	135	50	196	94	12	513

ID	Intersection Name	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	18	0	142	0	0	0	93	223	0	0	213	6	695

ID	Intersection Name	Southwestbound			Northwestbound		Southeastbound		Total Volume
		Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	205	1	96	153	218	334	25	1032

ID	Intersection Name	Northbound		Eastbound			Southeastbound		Total Volume
		Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	291	180	67	0	134	166	345	1183

ID	Intersection Name	Northbound			Southbound			Eastbound			Westbound			Total Volume
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	7	153	87	286	189	37	62	30	16	91	21	273	1252

Fed Ex Freight West Albuquerque TIA

Vistro File: P:\...\FedEx.vistro

Scenario 6 2020 Build PM

Report File: C:\...\2020 Build PM.pdf

12/18/2017

Turning Movement Volume: Detail

ID	Intersection Name	Volume Type	Northbound			Eastbound			Westbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
1	Shamrock Way & Tempur Pedic Parkway	Final Base	0	0	10	0	14	0	9	8	2	80	0	1	124
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	24	0	0	0	19	0	0	0	0	0	43
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	0	0	35	0	15	0	29	9	2	86	0	1	177

ID	Intersection Name	Volume Type	Eastbound		Northwestbound		Southeastbound		Total Volume
			Left	Right	Left	Thru	Thru	Right	
2	Atrisco Vista & TempurPedic Parkway	Final Base	20	108	30	183	88	10	439
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0
		Net New Trips	5	19	18	0	0	1	43
		Other	0	0	0	0	0	0	0
		Future Total	26	135	50	196	94	12	513

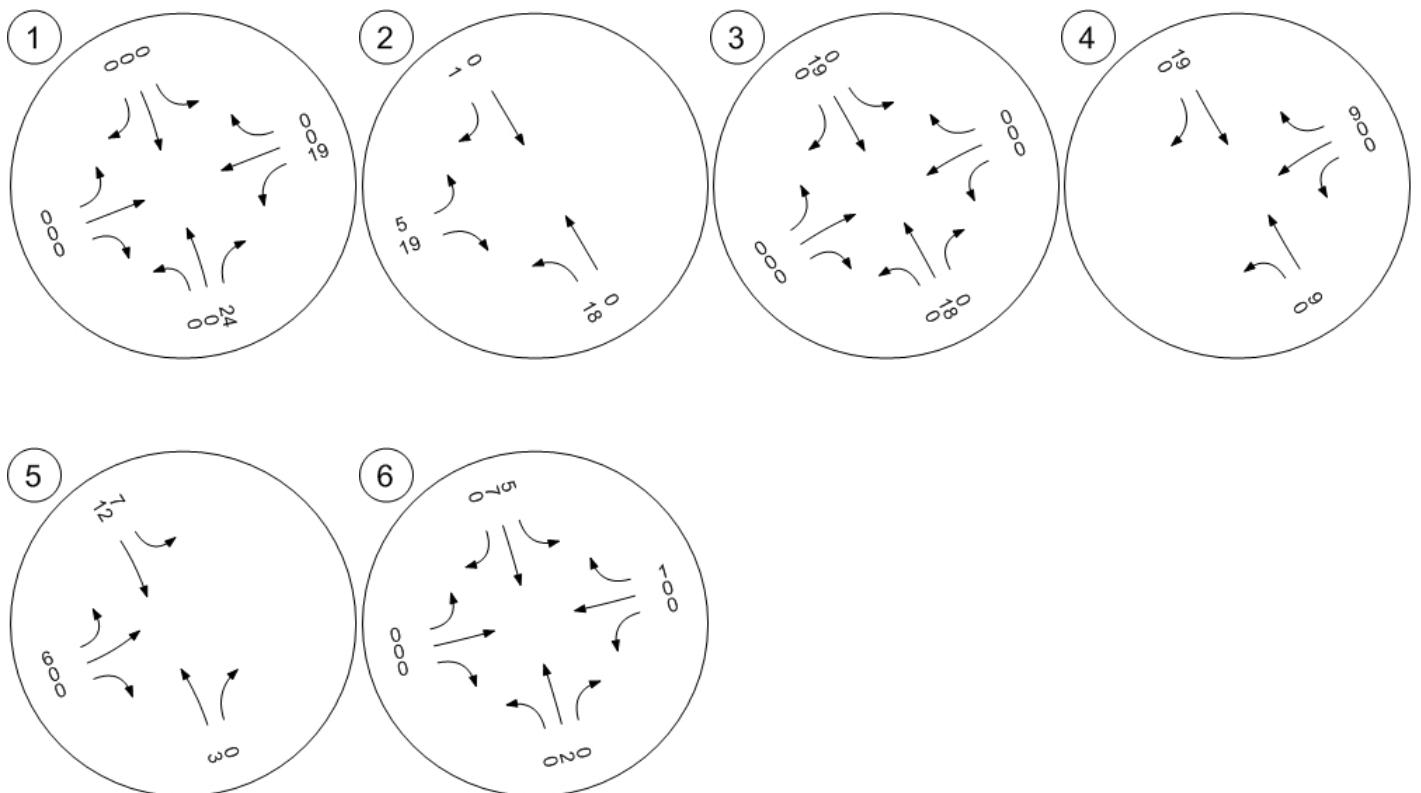
ID	Intersection Name	Volume Type	Northeastbound			Southwestbound			Northwestbound			Southeastbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
3	Atrisco Vista & North Frontage/Bluewater	Final Base	17	0	133	0	0	0	87	192	0	0	181	6	616
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	0	0	0	0	0	0	18	0	0	19	0	37
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	18	0	142	0	0	0	93	223	0	0	213	6	695

ID	Intersection Name	Volume Type	Southwestbound			Northwestbound		Southeastbound		Total Volume
			Left	Thru	Right	Left	Thru	Thru	Right	
4	Atrisco Vista & I-40 Westbound Ramp	Final Base	192	1	81	143	195	294	23	929
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	0	0	9	0	9	19	0	37
		Other	0	0	0	0	0	0	0	0
		Future Total	205	1	96	153	218	334	25	1032

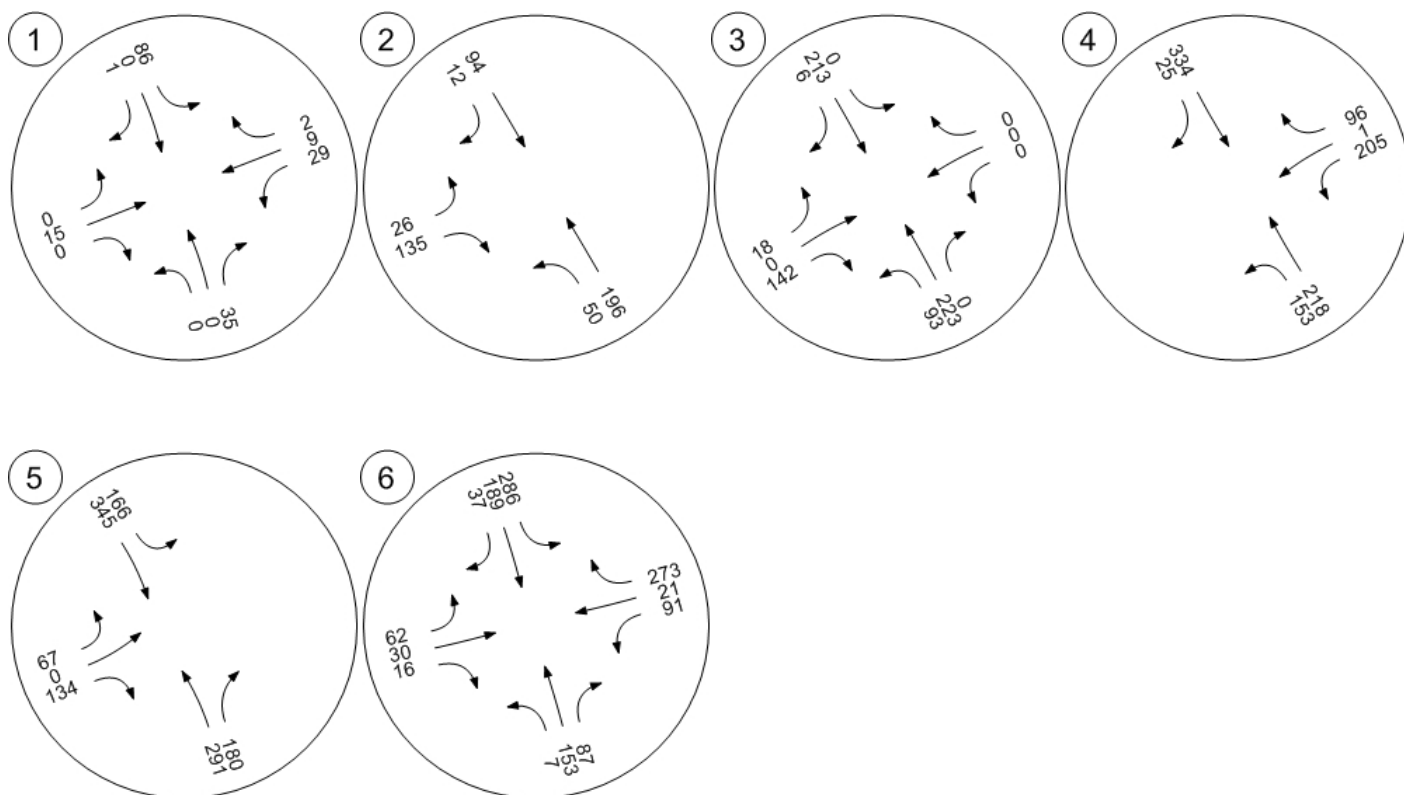
ID	Intersection Name	Volume Type	Northbound		Eastbound			Southeastbound		Total Volume
			Thru	Right	Left	Thru	Right	Left	Thru	
5	Atrisco Vista & I-40 Eastbound Ramp	Final Base	269	168	57	0	125	149	311	1079
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0
		Net New Trips	3	0	6	0	0	7	12	28
		Other	0	0	0	0	0	0	0	0
		Future Total	291	180	67	0	134	166	345	1183

ID	Intersection Name	Volume Type	Northbound			Southbound			Eastbound			Westbound			Total Volume
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6	Atrisco Vista & Central	Final Base	7	141	81	263	170	35	58	28	15	85	20	254	1157
		Growth Rate	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	1.07	-
		In Process	0	0	0	0	0	0	0	0	0	0	0	0	0
		Net New Trips	0	2	0	5	7	0	0	0	0	0	0	1	15
		Other	0	0	0	0	0	0	0	0	0	0	0	0	0
		Future Total	7	153	87	286	189	37	62	30	16	91	21	273	1252

Traffic Volume - Net New Site Trips



Traffic Volume - Future Total Volume



Traffic Conditions

