

ZONE ATLAS MAP L-21 1921 Central Ave NE (NM corner with Elizabeth

OT 10A BLOCK 3 LONGFIELD ADDITION ECTION 28 TOWNSHIP 10 N., RANGE 4 EAST 1PM BERNALILLO COUNTY, NEW MEXICO

LOT AREA: 30,374 s.f. BUILDING AREA: 9,992 s.f. gross (NO ADDITION; INCLUDES THICKENED WALLS) OCCUPANCY: B (former Retail Store - no occupant now SHELL ONLY) CONSTRUCTION TYPE: V-B

PARKING: Parking area is to be repaved - and rearranged to avoid new facade improvements and to provide for ADA space and related ADA passenger loading area. Previously existing was I I spaces (no ADA). 9 spaces are provided. ADA accessible space delineated at entry

EXECUTIVE SUMMARY: SHELL PERMIT

THERE ARE NO KNOWN OR APPLICABLE EPC DECISIONS, VARIANCES.

MASTERPLANS OR SIMILAR OVERARCHING GUIDELINES. MX-H ZONE. THE WORK OF THIS SHELL PROJECT IS PRIMARILY ENHANCEMENT OF THE EXTERIOR AT THE FRONT, SIDES AND REAR. THERE IS NO USE STIPULATED, AND NO OCCUPANTS ANTICIPATED EXCEPT FOR WORKERS. LATER PERMITS ARE EXPECTED WHEN TENANTS ARE ENGAGED AND THOSE PERMITS WILL INCLUDE STIPULATED OCCUPANTS, AND WITH THESE A STIPULATED REQUIRED PARKING COUNT AND LAYOUT.

EXISTING SHELL IS METAL BUILDING WITH STEEL PRIMARY AND SECONDARY STRUCTURE AND ORIGINALLY HAD METAL ROOF PANELS AND METAL PANEL SIDING. STUCCO WAS LATER ADDED AND WILL BE

REFINISHED WITH THE SHELL PERMIT

ORIGINALLY A 50' x 100' BUILDING. THE STRUCTURE HAD 2 MAJOR ADDITIONS ENLARGING SQUARE FOOTAGE.

THE ROOF AT SOME POINT HAD A SECOND METAL ROOF OVERLAID. AT DOWNSLOPES OVERLAYING ORIGINAL ROOF

NEW WORK THIS PERMIT:

- a.) NEW EXTERIOR WALLS ALL SIDES:
- INSTALL NEW CONTINUOUS INSULATION WITH ELASTOMERIC FINISH SYSTEM
- INSTALL TWO STOREFRONT ENTRY SYSTEMS AT SOUTH AND REMOVE PREVIOUS STOREFRONT ENTRY AT SOUTH. * note: AT WEST WALL, EXISTING WALL IS NOT BEING RECONFIGURED
- EIFS MEMBRANE IS TO BE ADDED AT EXTERIOR, AND
- INSULATION OVER EXISTING AT INTERIOR

(REAR)

- b.) ADD H.C. RAMP AND STAIRS AND PLATFORM AT NORTH (REAR) c.) ADD 2 OVERHEAD DOORS AT SOUTH AND TWO AT NORTH
- a.) INSTALL NEW DECORATIVE STEEL TRELLIS FEATURES IN FRONT OF SOUTH (ENTRY) FACADE.
- e.) INSTALL NEW DECORATIVE ENTRY STRUCTURE
- f.) EXISTING ROOF HAD 17 SKYLIGHTS SURFACE MOUNTED TRANSLUCENT PANELS. SOME WERE LEAKING. THESE ARE TO BE REMOVED AND OPENINGS COVERED OVER WITH METAL PANEL TO MATCH EXISTING ROOF
- a.) NEW HVAC ROOFTOP PACKAGE UNITS TO BE INSTALLED ON ROOF
- h.) REMOVAL OF EXISTING TOILET ROOMS AND SANITARY SEWER AND DOMESTIC WATER LINE FEED AND INSTAL-LATION OF NEW SEWER AND WATER AND NEW MINIMAL TOILET ROOMS (EXPANSION OF THESE AT A LATER DATE WILL CONFORM WITH TENANT OCCUPANCY REQUIREMENTS)

KEYED NOTES

- NEW DECORATIVE TRELLIS (2 THUS)
- EXISTING WATER METER TO REMAIN; NEW 1-1/4" WATER SUPPLY IN TRENCH
- NEW SEWER STUB TO BE DEVELOPED THIS AREA; CONNECT AT STREET AND CARRY IN FROM EXTERIOR WALL AND RISE AND CAP
- 4. FUTURE ELECTRICAL PANELS THIS AREA (PRIMARY BREAKER OUTSIDE, HOUSE PANELS INSIDE, SEE ELECTRICAL)
- NEW ENTRY CANOPY BASE (CONCRETE); 4 THUS
- NEW LOADING PLATFORM, RAMP, AND EXTERIOR STAIRS TO GRADE
- THIS AREA UNDIFFERENTIATED TRAFFIC AND PARKING NOT DELINEATED
- ALL BROKEN OR CRACKED SIDEWALK MUST BE REPLACED WITH SIDEWALK AND CURB AND GUTTER PER COA STANDARD DRAWING 2430
- 9. 4" THICK CONCRETE SIDEWALK; I 2" TURNDOWN AT PLANTERS. 6' WIDE SIDEWALK CONTINUES TO MEET CITY SIDEWALK AT ELIZABETH STREET.
- 10. PLANTER: BRING SOIL TO WITHIN 1/2" OF SIDEWALK
- 11. CONCRETE PARKING BUMPER EACH SPACE
- 12. EXISTING CURB CUT AND DRIVE TO REMAIN. MODIFY SIDEWALK RAMP IF SLOPES ARE NOT CORRECT AND ADD TACTILE TRUNCATED DOME WARNINGS PER COA STANDARD DRAWING 2425B (DRIVE AT BACK OF CURB).
- EXISTING CÙRB CÙT WIDENED. EXTEND CONCRETE SIDEWALK TO NORTH TO PROPERTY LINE AND ENSURE CROSS SLOPE IS NOT GREATER THAN 2%, 1.5% PREFERRED. ADD 12" CONCRETE STRIP NORTH OF PROPERTY LINE SEPARATED WITH FIBROUS CONTROL JOINT FOR POSSIBLE FUTURE GATE TRACK. INSTALLATION TO COMPLY WITH COA DRIVEPAD DWG 2425B EXAMPLE WITH SIDEWALK AT PROPERTY
- EXISTING IS SET OVER PROPERTY LINE. REMOVE SIGN AND POSTS
- EXISTING LARGE SIGN TO REMAIN
- 15. TRANSFORMER ON PAD: SEE ELECTRICAL WITH 6 BOLLARDS PER NOTE 21
- 16. VENT THROUGH ROOF: SEE PLUMBING
- 17. PROVIDE SIGN ON POST AT HEAD OF HC SPACE (VAN ACCESSIBLE) SEE DETAIL
- 18. EXISTING ASPHALT REPLACED; REVISE NEW AT REVISION TO CENTRAL AVE DRIVE PAD ENTRY SO THAT ASPHA; T MEETS NEW (LOWER) BACK OF SIDEWALK ELEVATION AND GENTLY RAMPS UP TO MEET EXISTING ASHPALT AT MAIN E-W DRIVE AISLE
- 19. THIS DRIVEWAY CUT DOES NOT COMPLY WITH CURRENT STANDARDS AND WILL BE TAKEN OUT AND REPLACED WITH STANDARD SIDEWALK, CURB & GUTTER per COA STANDARD DRAWING 2430

LIKELY MORE!

1.5% CROSS SLOPE PREF.

2% MAXIMUM-

EXISTING GUTTER-

EXISTING DRIVEPAD

& MARNING MARKS

MAY REMAIN

CENTRAL AVE.—

PROPERTY LINE

CROWN NEW ASPHALT TO

MEET EXISTING SMOOTHLY

-SAMCUT TRANSITION

-EXISTING ASPHALT

TO REMAIN

1" = 10' - 0"

1/2" ISOLATION STRIP

FLAT CONCRETE FOR

CITY SIDEWALK EXTENDED

-SAMCUT TRANSITION

- REMOVE AND REPLACE

WITH CONCRETE AND

EXISTING ASPHALT -

PARTIAL SITE PLAN AT

ENLARGED DETAIL

CENTRAL AVE DRIVEPAD

ASPHALT - BRING FLUSH

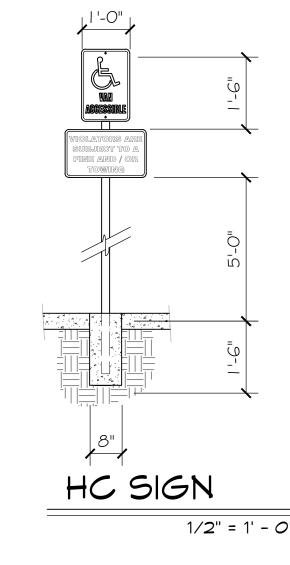
FUTURE GATE

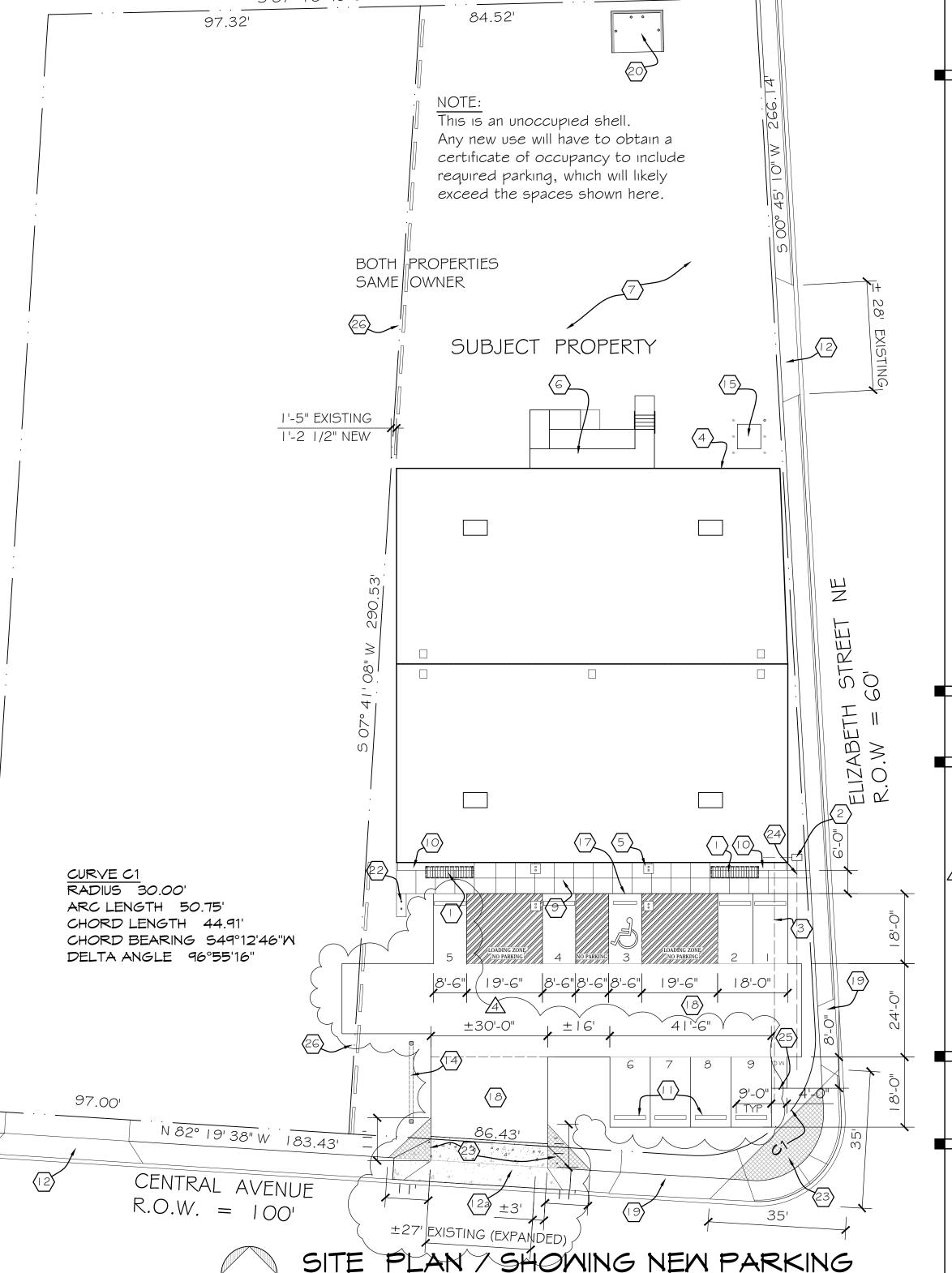
KEYED NOTES: (CONTINUED)

- DUMPSTER IN ENCLOSURE WITH GATES
- 4" Ø CONCRETE FILLED STEEL PIPE BOLLARD TO 42" ABOVE GRADE, 24" BURIAL PAINTED TRAFFIC YELLOW
- 22. PROVIDE 2-BIKE MINIMUM CAPACITY BIKE RACK: "BELSON INVERTED U" OR EQUAL, 36" HIGH x 22" WIDE, SET IN 12" DEEP CONCRETE PAD BOTH ENDS ON SIDEWALK 6' LONG x 30" WIDE
- 23. CLEAR SIGHT TRIANGLE: LANDSCAPING AND SIGNAGE WILL NOT INTERFERE WITH CLEAR SIGHT REQUIREMENTS. THEREFORE, SIGNS, WALLS, TREES AND SHRUBBERY BETWEEN 3 AND 8 FEET TALL (AS MEASURED FROM THE GUTTER PAN) WILL NOT BE ACCEPTABLE IN THE CLEAR SIGHT TRIANGLE.
- 24. EXTEND CONC. ENTRY WALK TO EXISTING SIDEWALK (ADA ACCESS): 1:20 MAX SLOPE; THE SIDEWALK ON PROPERTY IS SLOPED LESS THAN 1:20 \$ 1/96 CROSS SLOPE TO FRONT DOORS
- 25. INSTALL MOTORCYCLE PARKING SIGN ON POST AT HEAD OF SPACE SET IN CONC.
- POST SIMILAR TO HC SIGN THIS SHEET INSTALL RAILROAD TIES (OR FENCE) AT OPEN AREAS ALONG PROPERTY LINE TO
- PREVENT CROSS-ACCESS TO PROPERTY TO WEST.
- BURY TWO I" CONDUIT SLEEVES 18" BELOW ASPHALT WITH SWEEPS RISING UP AND CAPPED (FOR POSSIBLE FUTURE ELECTRIC GATE)

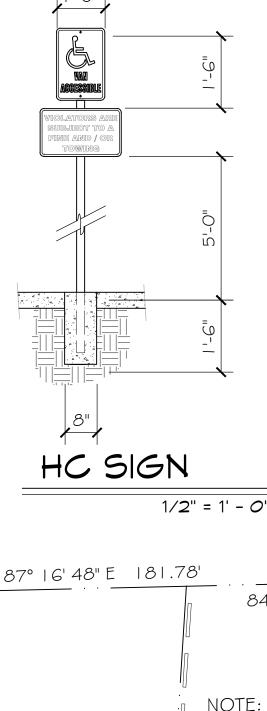
PARTIAL SITE PLAN AT FRONT SHOWING

PREVIOUS / EXISTING PARKING





1" = 20' - 0"



1/2" = 1' - 0" 5 87° 16′ 48″ E 181.78′

RAME NO STATE OF THE PARTY OF T **PROJECT** 2119

DATE REV Jan 6, 2023 Mar 9, 2023 Nov 6, 2023 ▲ Jan 26, 2024

TCL