CITY OF ALBUQUERQUE



Planning Department Transportation Development Services

December 17, 2015

Troy Kelts Merrick and Company 5970 Greenwood Plaza Blvd. Greenwood Village, CO 80015

Re:

Chick-fil-A

210 Eubank Blvd., SE

Request for Certificate of Occupancy-Transportation Development

Administrative Amendment Stamp dated 11-6-15 (L21-D045B)

Certification dated 12-9-15

Dear Mr. Kelts,

PO Box 1293

Based upon the information provided in your submittal received 12-10-15, Transportation Development has no objection to the issuance of a <u>Permanent Certificate of Occupancy</u>. This letter serves as a "green tag" from Transportation Development for a <u>Permanent Certificate of Occupancy</u> to be issued by the Building and Safety Division.

Albuquerque

If you have any questions, please contact Gary Sandoval at (505) 924-3675 or me at (505) 924-3991.

New Mexico 87103

Sincerely,

www.cabq.gov

Racquel M. Michel, P.E.

Traffic Engineer, Planning Dept. Development Review Services

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via: email

Č:

CO Clerk, File



City of Albuquerque

Planning Department

Development & Building Services Division

DRAINAGE AND TRANSPORTATION INFORMATION SHEET

(REV 02/2013)

(REV 02/2013)	. 70.
Building Permit #:_	
PC-40027	Work Order#:
	Contact: Troy Kelts
	- " ' 1-1-0
-/52-4451	E-mail: troy.kelts@merrick.com
	Contact: Don Ikler
49-2732	
	E-mail: don.ikeler@chick-fil-a.com
	Contact: Jose Carlos de la Vega
92780	
	E-mail: Carlos@CRHO.com
	Contact: Robert Fierro
, 87120	
	E-mail: robertfierro.fc@gmail.com
	Contact: Heather Hager
as, Texas 75254	
-375-7341	E-mail: hhager@panterraconstruction
CHECK TYPE OF APPROV	AL/ACCEPTANCE SOUGHT:
SIA/FINANCIAL GUARAN	ITEE RELEASE
PRELIMINARY PLAT APP	ROVAL
S. DEV. PLAN FOR SUB'D	APPROVAL
S. DEV. FOR BLDG. PERM	MAPPROVALE TV E
SECTOR PLAN APPROVA	4
FINAL PLAT APPROVAL	IN DEC 1 0 2015 IIII
X CERTIFICATE OF OCCUP.	AND (PERM)
CERTIFICATE OF OCCUP	ANCY (TCL TEMP)
	
GRADING CERTIFICATIO	ON OTHER (SPECIFY)
Yes No C	Copy Provided
By: Troy D. Kelts	
	2PC-40027 2pod Village, CO 80015 2752-4451 49-2732 292780 283, Texas 75254 2975-7341 CHECK TYPE OF APPROVAL APPROVA

Requests for approvals of Site Development Plans and/or Subdivision Plats shall be accompanied by a drainage submittal. The particular nature, location, and scope to the proposed development defines the degree of drainage detail. One or more of the following levels of submittal may be required based on the followin

- Conceptual Grading and Drainage Plan: Required for approval of Site Development Plans greater than five (5) acres and Sector Plans
 Drainage Plans: Required for building permits, grading permits, paving permits and site plans less than five (5) acres
- Drainage Report: Required for subdivision containing more than ten (10) lots or constituting five (5) acres or more
- Erosion and Sediment Control Plan: Required for any new development and redevelopment site with 1-acre or more of land disturbing area, including project less than 1-acre than are part of a larger common plan of development



Merrick & Company 5970 Greenwood Plaza Blvd. Greenwood Village, CO 80111 Tel: 303-751-0741

Fax: 303-751-2581 www.merrick.com

TRAFFIC CERTIFICATION

CHICK-FIL-A - 210 EUBANK BOULEVARD SE

1/11
1, TROY Kets, NMPE 2210Z, OF THE FIRM MERRICK & Company
HEREBY CERTIFY THAT THIS PROJECT IS IN SUBSTANTIAL COMPLIANCE WITH AND IN ACCORDANCE WITH
THE DESIGN INTENT OF THE APPROVED PLAN DATED $3/9/15$. THE RECORD INFORMATION
EDITED ONTO THE ORIGINAL DESIGN DOCUMENT HAS BEEN OBTAINED BY TROY KE 175 OF THE
FIRM METTICE & COMMENT. I FURTHER CERTIFY THAT I HAVE PERSONALLY VISITED THE PROJECT SITE ON
12/9/15 AND HAVE DETERMINED BY VISUAL INSPECTION THAT THE SURVEY DATA PROVIDED IS
REPRESENTATIVE OF ACTUAL SITE CONDITIONS AND IS TRUE AND CORRECT TO THE BEST OF MY
KNOWLEDGE AND BELIEF. THIS CERTIFICATION IS SUBMITTED IN SUPPORT OF A REQUEST FOR
CELTIFICATE OF OCCUPANCY

THE RECORD INFORMATION PRESENTED HEREON IS NOT NECESSARILY COMPLETE AND INTENDED ONLY TO VERIFY SUBSTANTIAL COMPLIANCE OF THE TRAFFIC ASPECTS OF THIS PROJECT. THOSE RELYING ON THE RECORD DOCUMENT ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY OTHER PURPOSE.

Signature of Engineer of Architect

Date

ENGINEER'S OR ARCHITECT'S STAMP



GENERAL NOTES

PERMANENT ACCESS DRIVE EASEMENT. THE "DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS REGARDING OUTPARCEL" (RECORDED AS DOCUMENT NO. 2012138716 ON THE 31ST DAY OF DECEMBER 2012) STATES WITHIN SECTION 32 THAT THE OWNER OF TRACT B-I-A-I "HEREBY GRANTS TO THE OUTPARCEL OWNER (TRACT B-1-A-2), AS GRANTEE, FOR THE BENEFIT OF THE OUTPARCEL OWNER AND ITS RESPECTIVE PERMITEES APPURTENANT TO THE OUTPARCEL, A PERPETUAL NON-EXCLUSIVE EASEMENT FOR INGRESS AND EGRESS BY VEHICULAR AND PEDESTRIAN TRAFFIC UPON, OVER AND ACROSS THAT PORTION OF THE COMMON AREA LOCATED ON THE HOME DEPOT PARCEL (TRACT B-1-A-1) SHOWN ON EXHIBIT "B" AS THE PERMANENT ACCESS

RECIPROCAL STORM DRAINAGE EASEMENT. THE "DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS REGARDING OUTPARCEL" (RECORDED AS DOCUMENT NO. 2012138716 ON THE 31ST DAY OF DECEMBER, 2012) STATES WITHIN SECTION 3.5 THAT THE OWNER OF TRACT B-1-A-1 AND THE OWNER OF TRACT B-1-A-2 RECIPROCALLY GRANT TO ONE ANOTHER "A NON-EXCLUSIVE EASEMENT OVER AND UNDER ITS PARCEL FOR SURFACE WATER DRAINAGE OVER AND THROUGH THE DRAINAGE PATTERNS AND STORM WATER DRAINAGE SYSTEMS THAT ARE ESTABLISH FROM TIME TO TIME AMONG THE PARCELS.

GRANT OF UTILITY EASEMENTS. THE "DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS REGARDING OUTPARCEL "(RECORDED AS DOCUMENT NO. 2012138716 ON THE 31ST DAY OF DECEMBER, 2012) STATES WITHIN SECTION 3.1(a) THAT THE OWNER OF TRACT B-1-A-1 AND OWNER OF TRACT B-1-A-2 RECIPROCALLY GRANT TO ONE ANOTHER "A NON-EXCLUSIVE AND PERPETUAL EASEMENT UNDER, THROUGH AND ACROSS THE COMMON AREA OF THE HOME DEPOT PARCEL (TRACT B-1-A-1) OR THE OUTPARCEL (TRACT B-1-A-2), AS APPLICABLE (EXCLUSIVE OF ANY PORTION WITHIN A BUILDING AREA), FOR THE INSTALLATION, OPERATION, MAINTENANCE, REPAIR AND REPLACEMENT OF UTILITY LINES."

. THE "DRIVEWAY EASEMENT AGREEMENT" DATED JUNE 6, 2006 RECORDED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLO COUNTY, NEW MEXICO AS DOCUMENT NO 2000055083 (BOOK A-6, PAGE-4862) PROVIDES VEHICULAR AND PEDESTRIAN CROSS ACCESS EASEMENT BETWEEN LOT B-IA-I AND LOT B-I-A-2 AND THE "COSTCO PARCEL" LEGALLY DESCRIBED AS "TRACT A AND TRACT C-I OF PLAT OF MANZANO

5. ON ADJOINING TRACT A-1 OF THE COSTCO DEVELOPMENT PLAT THERE IS AN EXISTING 70'X185' PRIVATE ACCESS EASEMENT FOR THE BENEFIT AND USE OF TRACT B-1, VIDEO ADDITION, ALBUQUERQUE, NEW MEXICO AS THE SAME IS SHOWN AND DESIGNATED ON THE PLAT THERE OF, RECORDED IN THE OFFICE OF THE COUNTY CLERK OF BERNALILLOW COUNTY, NEW MEXICO ON AUGUST 9, 1994 IN YOLUME 94C, FOLIO 263 FILED: FEBRUARY 20, 2001 (2001C-56).

6. AS SHOWN ON THE PLAT FILED IN PLAT BOOK 94C, PAGE 263 THERE IS A RECIPROCAL ACCESS EASEMENT BY THE OWNER OF TRACT BI AND THE OWNER OF TRACT D: EACH GRANTED TO THE OTHER RECIPROCAL EASEMENT FOR CROSS ACCESS AND VEHICULAR PARKING ON AND OVER TRACT BI FOR THE BENEFIT OF TRACTS D AND ON AND OVER TRACT D FOR THE BENEFIT OF TRACT BI. THE OWNER OF TRACT BI AND THE OWNER OF TRACT D GRANTED TO THE OWNER OF TRACT C VEHICULAR ACCESS ACROSS TRACTS BI AND D RESPECTIVELY FOR PURPOSES OF INGRESS AND EGRESS.

DESIGN GROUND-MOUNTED EQUIPMENT SCREENING TO ALLOW FOR ACCESS TO UTILITY FACILITIES. ALL SCREENING AND VEGETATION SURROUNDING GROUND-MOUNTED TRANSFORMERS AND UTILITY PADS ARE TO ALLOW 10 FEET OF CLEARANCE IN FRONT OF THE EQUIPMENT DOOR AND 5-6 FEET OF CLEARANCE ON THE REMAINING THREE SIDES FOR SAFE OPERATION, MAINTENANCE AND REPAIR PURPOSES. REFER TO THE PMN ELECTRIC SERVICE GUIDE AT WWW.PNM.COM FOR SPECIFICATIONS.

8. LOT NUMBERS TBD UPON RECORDATION OF PENDING SUBDIVISION PLAT. 9. THE DEVELOPER SHALL BE RESPONSIBLE FOR PERMANENT IMPROVEMENTS TO THE TRANSPORTATION FACILITIES ADJACENT TO THE PROPOSED SITE DEVELOPMENT PLAN AS MAY BE REQUIRED BY THE DEVELOPMENT REVIEW BOARD (DRB).

10. SITE PLAN SHALL COMPLY AND BE IN ACCORDANCE WITH ALL APPLICABLE CITY OF ALBUQUERQUE REQUIREMENTS, INCLUDING THE DEVELOPMENT PROCESS MANUAL AND CURRENT ADA CRITERIA. II. APPLICANT SHALL BE RESPONSIBLE TO ABIDE BY ANY CONDITIONS OR TERMS OF UTILITY EASEMENTS. AN

EXISTING OVERHEAD ELECTRIC DISTRIBUTION IS LOCATED ALONG THE EAST SIDE OF EUBANK BOULEYARD

LEGEND

PROPERTY LINE BUILDING LINE NEW CURBS 4 4 4 4 4 NEW SIDEWALK EXISTING CURBS AND SIDEWALKS COLORED AND TEXTURED CONCRETE AT PEDESTRIAN PARKING LIGHT FIXTURE ______ EASEMENT

Deviations Requested- Staff Summary Tables

- Table 1: Commercial Building & Lot Standards -

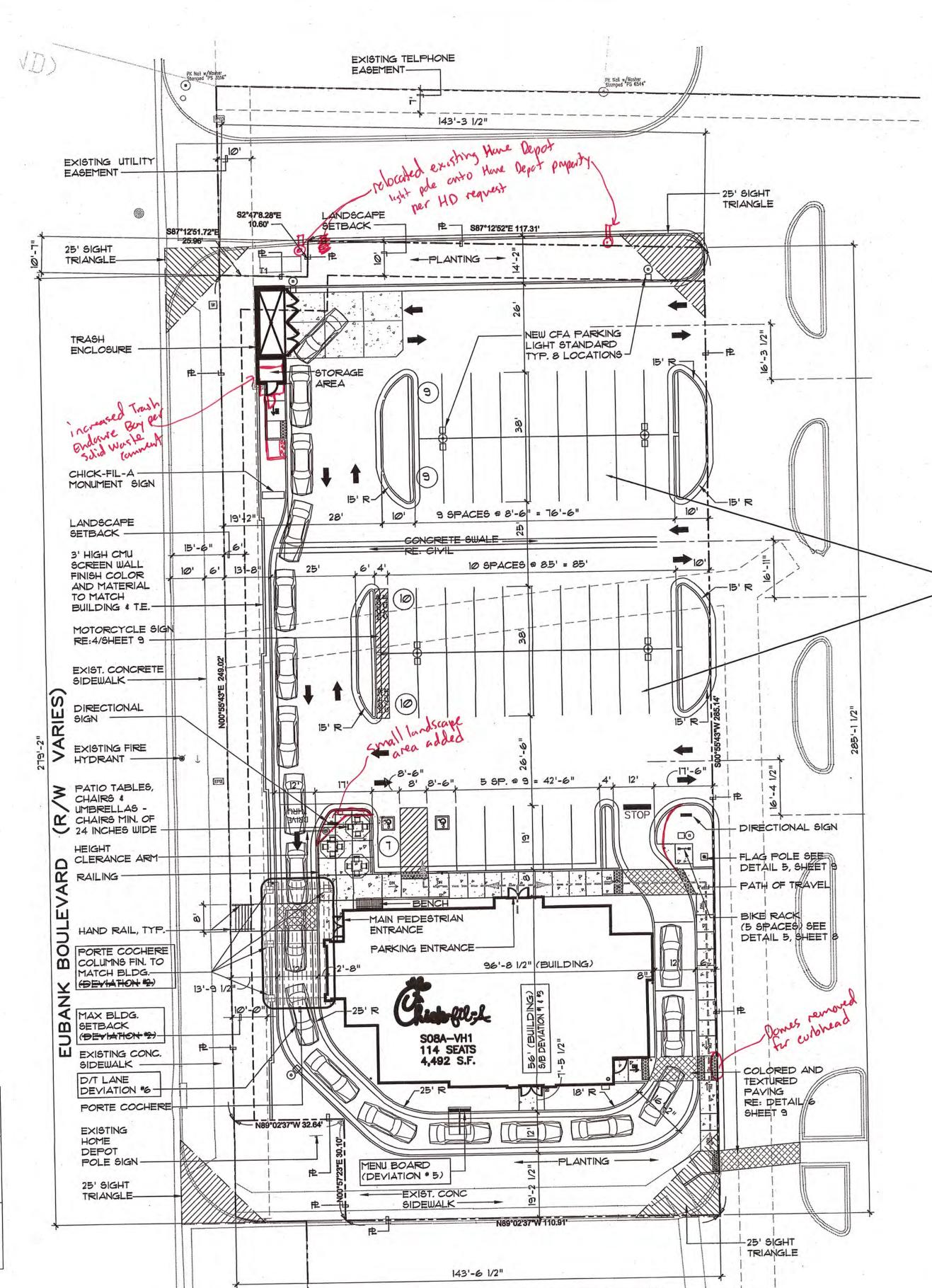
		Requirement			Difference between	Deviation &
#	Page	Name	Amount	Proposed	Required & Proposed	Classification
1	5-36	Building Front Façade Lot Width Coverage	50% minimum	142.5 feet is 50%. 56 feet is proposed and is 20%	Lot is 285 feet wide.142.5-56=86.5 feet difference, or 30%	30%- Major
2	5-36	Front Yard Setback	10 feet maximum	24.75 feet from building	14.75 feet, 147.5% of the standard.	50% deviation is 15 feet. The EPC cannot grant deviations >50%*

- Table 2: General Design Regulations -

	T.	Requirement			Difference between	Deviation
#	Page	Name	Amount	Proposed	Required & Proposed	Classification
3	5-46	Building Façade at Least 50% of Street Frontage	50% minimum	142.5 feet is 50%. 56 feet is proposed and is 20%	Lot is 285 feet wide.142.5-56=86.5 feet difference, or 30%	30%- Major
4	5-48	Maximum Parking Allowed	ZC min plus 10% (33 spaces)	45 spaces.	12 extra spaces, a third of the standard	36%- Major
5	5-55	Drive-up windows and Ordering Panels at Rear of Building		1 window at rear, 1 window at side, ordering panel at side	The side window and ordering panel should be at the rear of the building	non-dimensional standard requiring review
6	5-56	No portion of queuing lane within 40 feet of street-facing façades		Queuing lane is within 25 feet of street facing facade	Relocate queuing lane to another location (many available)	non-dimensional standard requiring review

Revised application is for 5 deviations instead of 6 (#2 is addressed). Three deviations (#s 1, 3 & 4) are still major in scope.

Two deviations are still to non-dimensional standards.



PRELIMINARY SITE PLAN

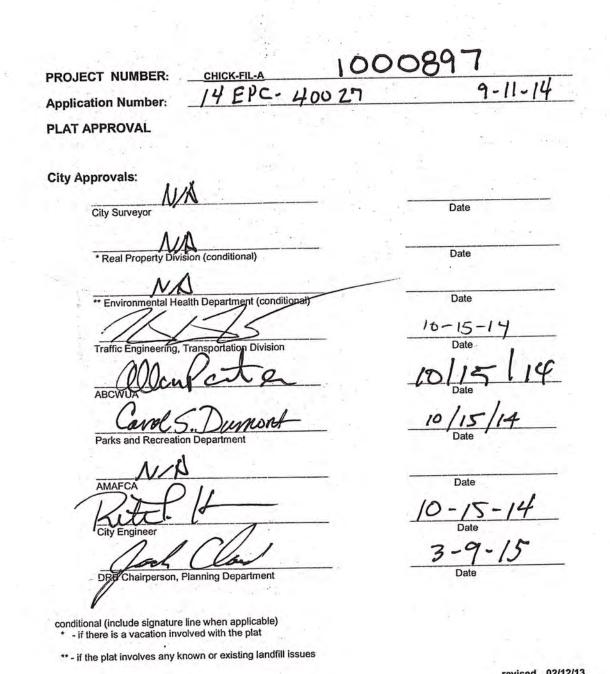
1'= 20'-0"

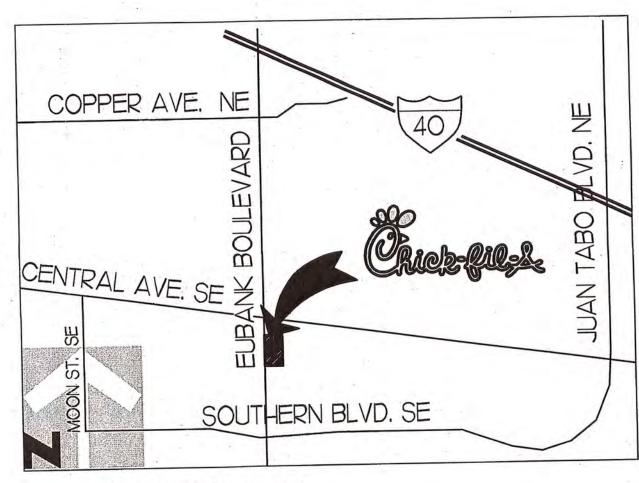
RECORD DOCUMENT 12/9/15

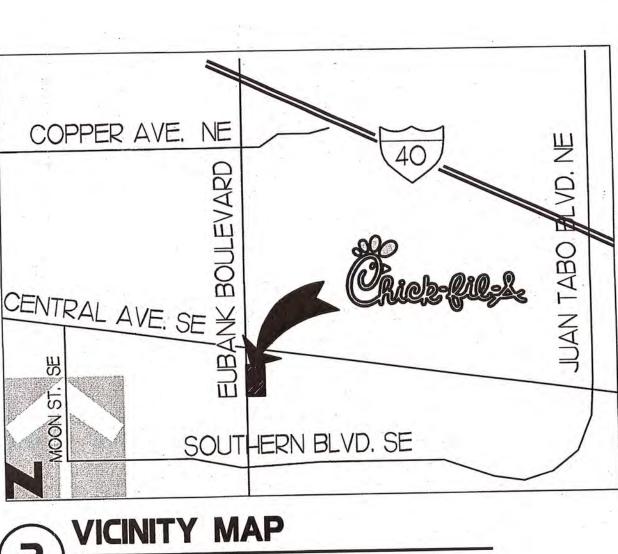
AA approved on 11/6/15

Chick-fil-A SITE SPECIFIC PARKING CALCULATION PATIO SEATING REGULAR STALL SIZE 9'X19' & 8.5x19 COMPACT STALL SIZE 8'X15' 4'X8' MIN. MOTORCYCLE PARKING DRIVE AISLE 1/4 SEATS PARKING FORMULA 119/4 = 30 STALLREQUIRED PARKING REQUIRED 48 SPACES PARKING ALLOWED BY EGSDP 33 SPACES PLUS (DEVIATION #4) 50% (MAXIMUM) TOTAL PARKING PROVIDED 45 (DEVIATION #4) MOTORCYCLE PARKING HANDICAP PARKING BICYCLE SPACES REQUIRED. 1 PER 20 PARKING SPACES 45/20 = 2.25TOTAL BICYCLE PARKING PROVIDED

PARKING (DEVIATION #4)







5200 Buffington Rd Atlanta Georgia, 30349-2998 Revisions: Mark Date 08/27/14

Mark Date E 09/16/14 Mark Date Mark Date By 10/24/14 EF Mark Date Mark Date 11/21/14 Mark Date 12/22/14 Mark Date Mark Date Mark Date 02/09/15 Mark Date 02/17/15

> $C \cdot R \cdot H \cdot O$ Architecture Interior Planning 195 South "C" Street 200 Tustin, California 92780 714 832-1834 FAX 832-1910

CHICK-FIL-A #03235 RESTAURANT CENTRAL & EUBANK

210 Eubank Blvd SE ALBUQUERQUE, NM 87123 SHEET TITLE
SITE PLAN

VERSION: HV1 ISSUE DATE: 07-2014

: 13-232 : 03235 . 3-26-14 Checked By: RH







