

# CITY OF ALBUQUERQUE



May 19, 2015

Jeff Wooten, P.E.  
Wooten Engineering  
3708 Saint Andrews SE  
Rio Rancho, New Mexico 87124

**RE: Gibson/University Cross Access Road  
Gibson/University  
Grading and Drainage Plan  
Engineers Stamp Date 5/14/15 (M15D012D)**

Dear Mr. Wooten,

Based upon the information provided in your submittal received 5/15/15, this plan is acceptable for Grading Permit and Paving Permit.

Please attach a copy of this approved plan to the construction sets in the permitting process prior to sign-off by Hydrology.

When completed please provide a copy of the as-build for our records.

PO Box 1293

If you have any questions, please contact me at 924-3695 or Rudy Rael at 924-3977.

Albuquerque

Sincerely,

New Mexico 87103

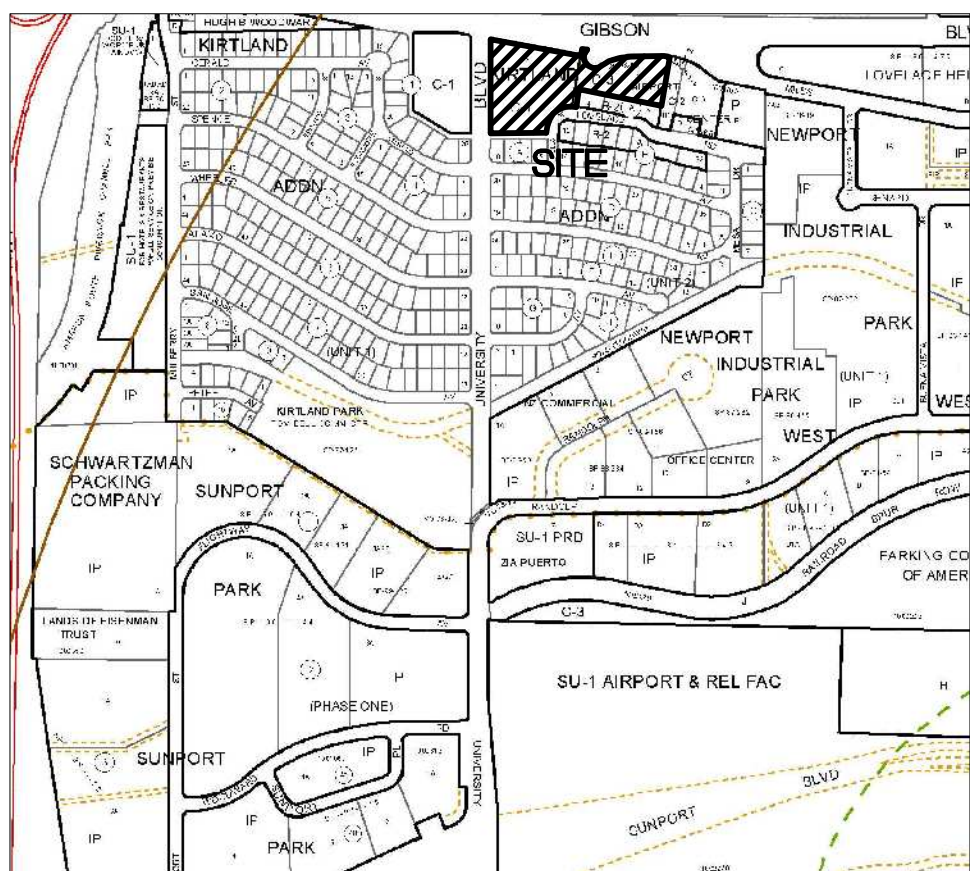
Rita Harmon, P.E.  
Senior Engineer, Hydrology  
Planning Department

[www.cabq.gov](http://www.cabq.gov)

RR/RH  
C: File

CONSTRUCTION PLANS  
FOR  
SEC GIBSON AT UNIVERSITY  
CROSS ACCESS DRIVEWAY  
ALBUQUERQUE, NM

MARCH 2015

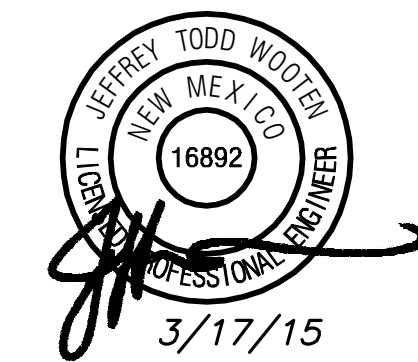


**VICINITY MAP - Zone Map M-15-Z**  
Legal Description: Lots 9-A-1 & 9-B-1, Kirtland  
Addition Block A; Tract A, Airport Center

SHEET INDEX

- 0. COVER SHEET
- 1. OVERALL SITE PLAN
- 2. OVERALL DRAINAGE MANAGEMENT PLAN
- 3. DEMOLITION PLAN
- 4. SITE PLAN
- 5. GRADING PLAN
- 6. UTILITY PLAN
- 7. DETAIL SHEET

**Wooten Engineering**  
4700 Lincoln NE, Suite 111  
Albuquerque, NM 87109  
Ph: 505.980.3560  
TX Firm No. F-16140

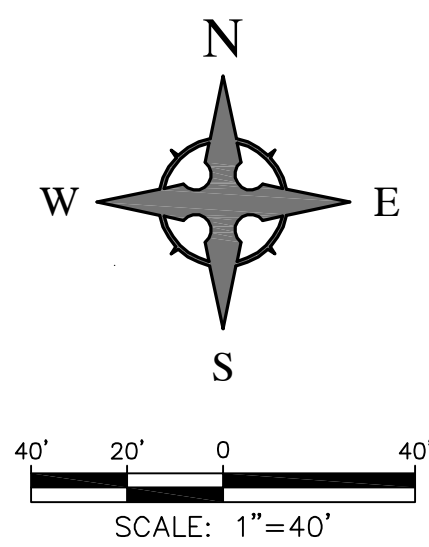


**SEC Gibson at University**  
**Cross Access Driveway**  
Albuquerque, New Mexico

Project Number 2014055  
Drawn By JW  
Checked JW  
Issue Date March 17, 2015  
Cover Sheet

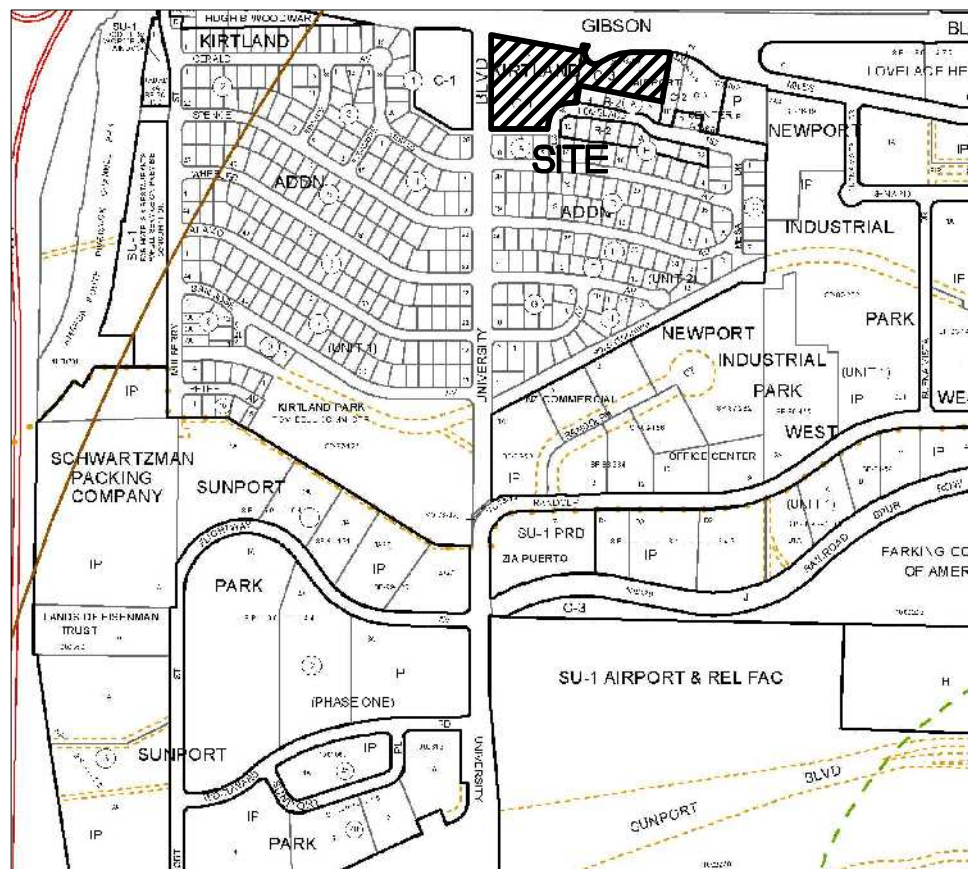
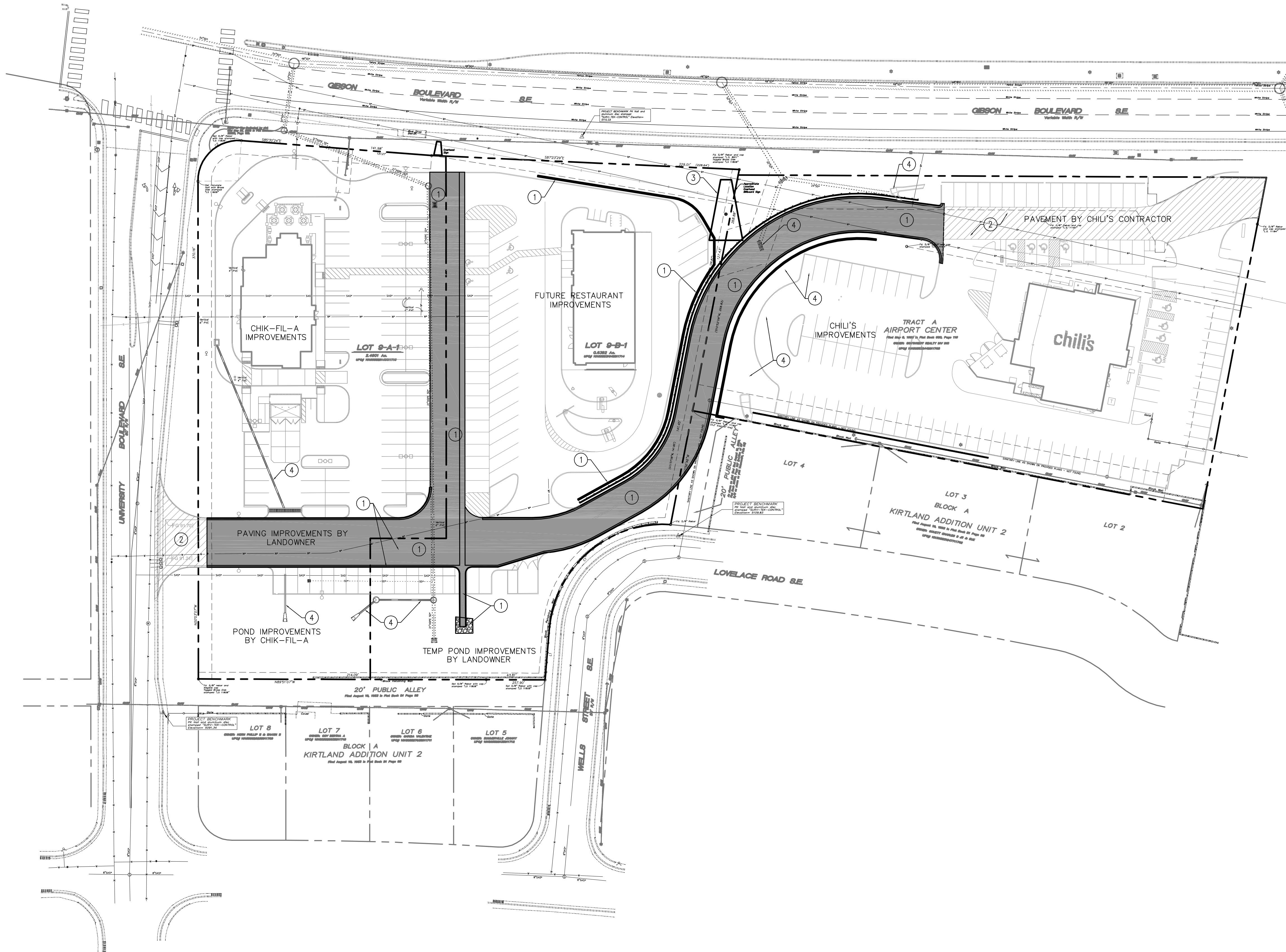
DEVELOPER  
**ZIA RISING CONSTRUCTION MANAGEMENT**  
4101 INDIAN SCHOOL ROAD NE, SUITE 400  
ALBUQUERQUE, NM 87110  
PH. (505) 262-2323  
CONTACT: JOSH SKARSGARD





CAUTION - NOTICE TO CONTRACTOR

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VICINITY MAP - Zone Map M-15-Z

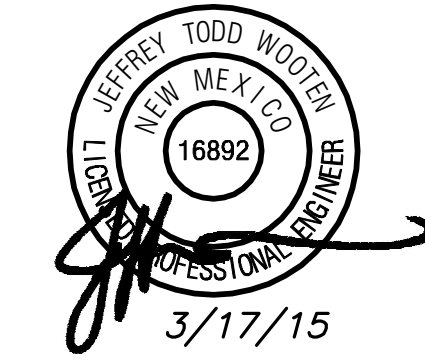
Legal Description: Lots 9-A-1 & 9-B-1, Kirtland Addition Block A; Tract A, Airport Center

KEYED NOTES

1. REF. SHEET C4, SITE PLAN, FOR IMPROVEMENTS BY LANDOWNER.
2. PAVEMENT EXTENSION BY OTHERS.
3. EXISTING BILLBOARD TO REMAIN.
4. PROPOSED DRAINAGE IMPROVEMENTS BY OTHERS.

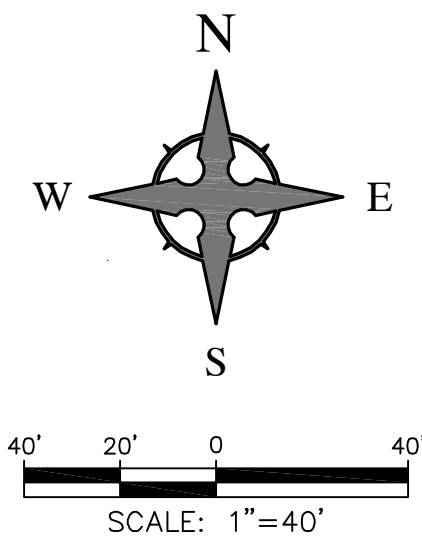
SEC Gibson at University  
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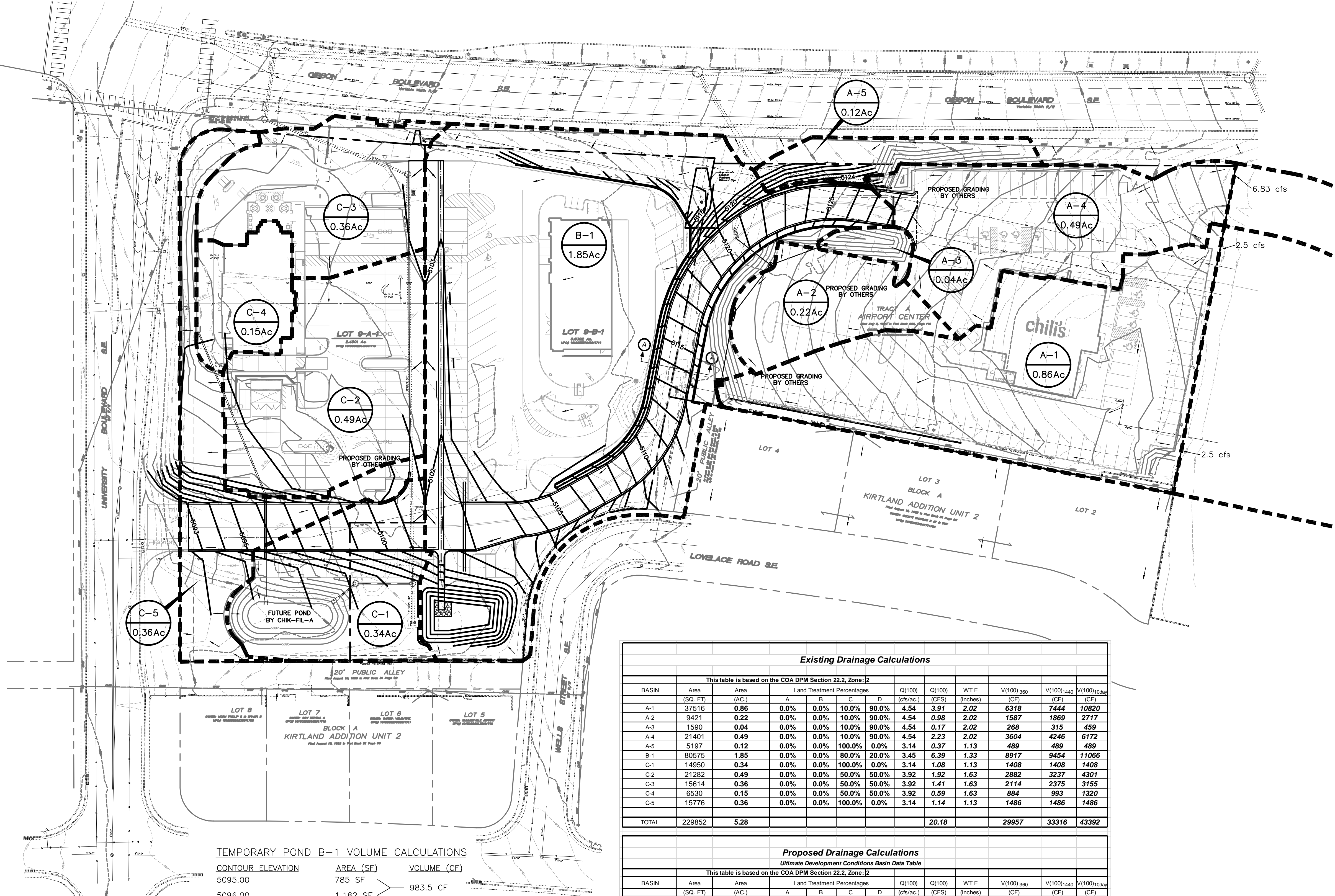
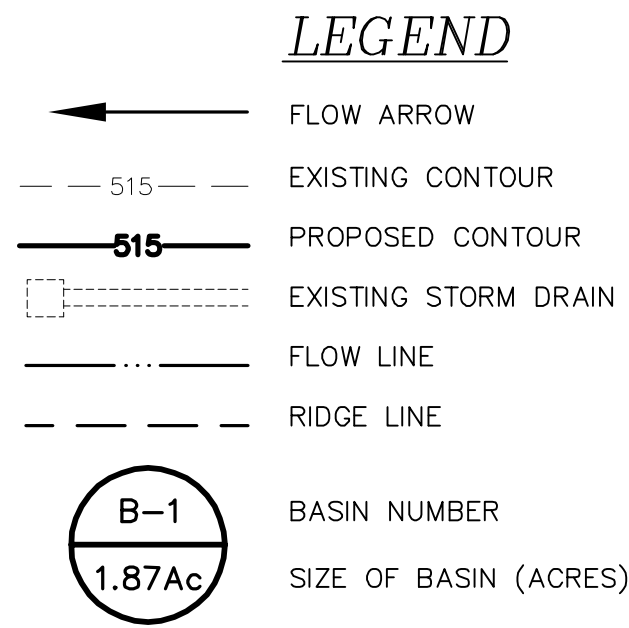
Project Number 2014055  
Drawn By JW  
Checked JW  
Issue Date March 17, 2015  
Overall Site Plan





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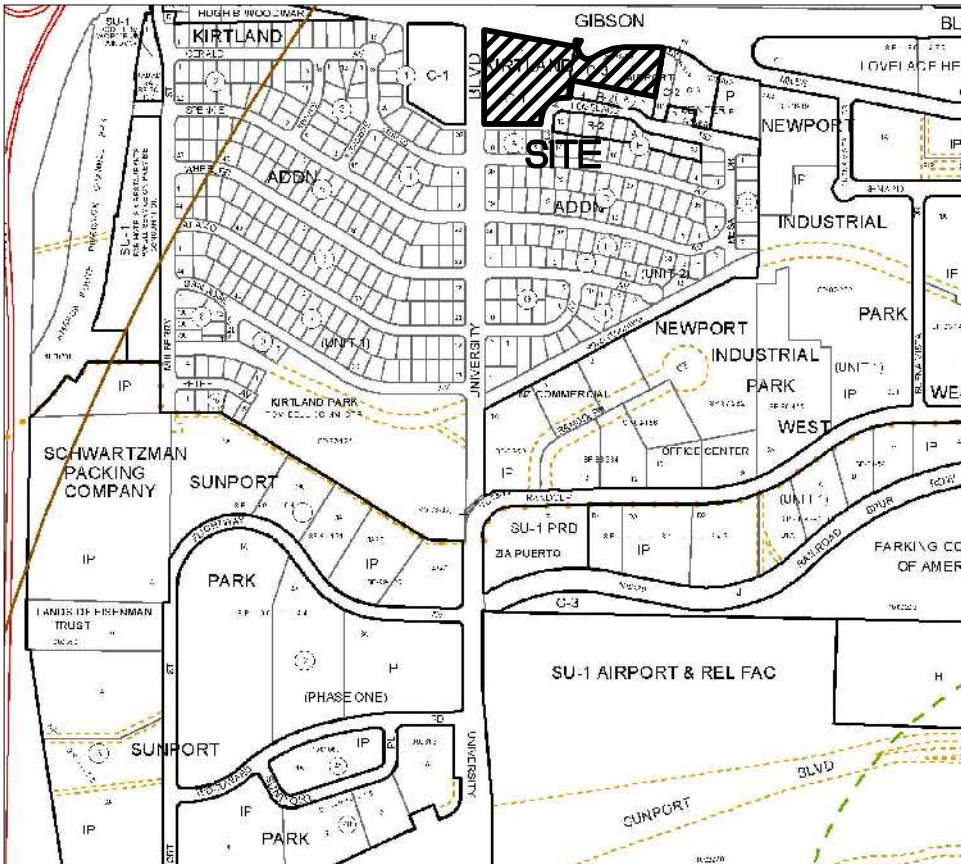
TEMPORARY POND B-1 VOLUME CALCULATIONS

CONTOUR ELEVATION	AREA (SF)	VOLUME (CF)
5095.00	785 SF	983.5 CF
5096.00	1,182 SF	1,414 CF
5097.00	1,646 SF	1,907.5 CF
5098.00	2,169 SF	1,155 CF
5098.50	2,451 SF	
TOTAL		5,460 CF

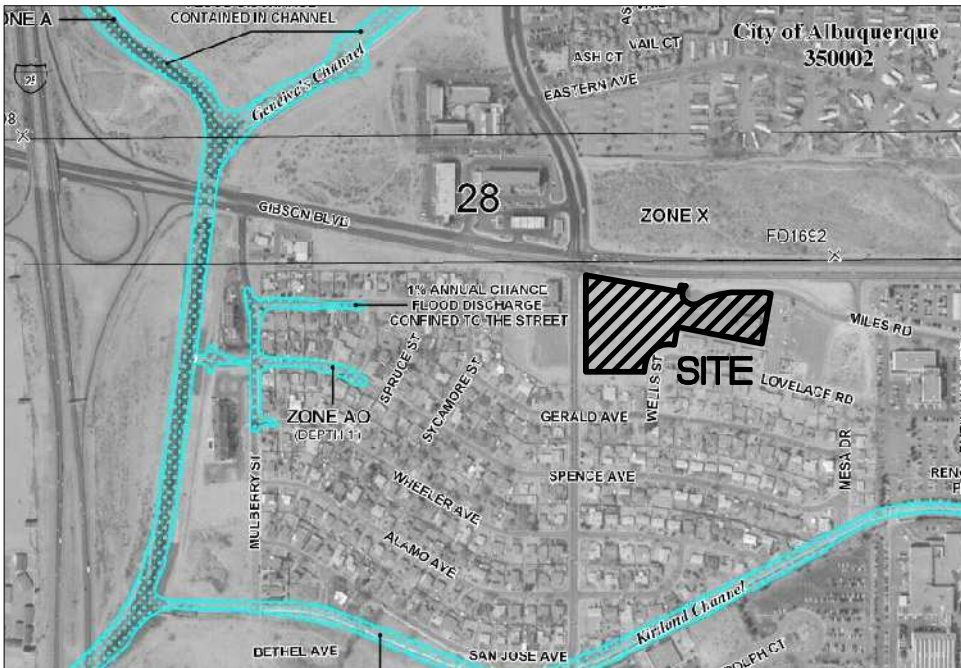
NOTE: THE ABOVE POND VOLUMES SHALL BE VERIFIED AS PART OF THE DRAINAGE CERTIFICATION AND WILL BE REQUIRED PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY.

Existing Drainage Calculations												
This table is based on the COA DPM Section 22.2, Zone: 2												
BASIN	Area (SQ. FT)	Area (AC.)	Land Treatment Percentages				Q(100) (cfs/ac)	Q(100) (CFS)	WT E (inches)	V(100) <sub>360</sub> (CF)	V(100) <sub>1440</sub> (CF)	V(100) <sub>today</sub> (CF)
A-1	37516	0.86	0.0%	0.0%	10.0%	90.0%	4.54	3.91	2.02	6318	7444	10820
A-2	9421	0.22	0.0%	0.0%	10.0%	90.0%	4.54	0.98	2.02	1587	1869	2717
A-3	1590	0.04	0.0%	0.0%	10.0%	90.0%	4.54	0.17	2.02	268	315	459
A-4	21401	0.49	0.0%	0.0%	10.0%	90.0%	4.54	2.23	2.02	3604	4246	6172
A-5	5197	0.12	0.0%	0.0%	100.0%	0.0%	3.14	0.37	1.13	489	489	489
B-1	80575	1.85	0.0%	0.0%	80.0%	20.0%	3.45	6.39	1.33	8917	9454	11068
C-1	14950	0.34	0.0%	0.0%	100.0%	0.0%	3.14	1.08	1.13	1408	1408	1408
C-2	21282	0.49	0.0%	0.0%	50.0%	50.0%	3.92	1.92	1.63	2882	3237	4301
C-3	15614	0.36	0.0%	0.0%	50.0%	50.0%	3.92	1.41	1.63	2114	2375	3155
C-4	6530	0.15	0.0%	0.0%	50.0%	50.0%	3.92	0.59	1.63	884	993	1320
C-5	15776	0.36	0.0%	0.0%	100.0%	0.0%	3.14	1.14	1.13	1486	1486	1486
TOTAL	229852	5.28						20.18		29957	33316	43392

			Proposed Drainage Calculations											
			Ultimate Development Conditions Basin Data Table											
This table is based on the COA DPM Section 22.2, Zone: 2														
BASIN	Area (SQ. FT)	Area (AC.)	Land Treatment Percentages				Q(100) (cfs/ac.)	Q(100) (CFS)	WT E (inches)	V(100) 360 (CF)	V(100) 1440 (CF)	V(100) 10day (CF)		
A-1	37516	0.86	0.0%	0.0%	10.0%	90.0%	4.54	3.91	2.02	6318	7444	10820		
A-2	9421	0.22	0.0%	0.0%	0.0%	100.0%	4.70	1.02	2.12	1664	1978	2921		
A-3	1590	0.04	0.0%	0.0%	100.0%	0.0%	3.14	0.11	1.13	150	150	150		
A-4	21401	0.49	0.0%	0.0%	10.0%	90.0%	4.54	2.23	2.02	3604	4246	6172		
A-5	5197	0.12	0.0%	0.0%	100.0%	0.0%	3.14	0.37	1.13	489	489	489		
B-1	80575	1.85	0.0%	0.0%	80.0%	20.0%	3.45	6.39	1.33	8917	9454	11066		
C-1	14950	0.34	0.0%	0.0%	70.0%	30.0%	3.61	1.24	1.43	1778	1927	2376		
C-2	21282	0.49	0.0%	0.0%	10.0%	90.0%	4.54	2.22	2.02	3584	4223	6138		
C-3	15614	0.36	0.0%	0.0%	60.0%	40.0%	3.76	1.35	1.53	1986	2194	2818		
C-4	6530	0.15	0.0%	0.0%	15.0%	85.0%	4.47	0.67	1.97	1073	1258	1813		
C-5	15776	0.36	0.0%	0.0%	75.0%	25.0%	3.53	1.28	1.38	1811	1942	2337		
TOTAL	229852	5.28						20.79		31374	35306	47100		



VICINITY MAP - Zone Map M-15-Z



FIRM MAP 35001C0342G

Per FIRM Map 35001C0342G, dated September 26, 2008, the site is not located in the Floodplain and determined to be outside the 0.2% chance Annual Floodplain.

DRAINAGE MANAGEMENT PLAN

**INTRODUCTION**  
The purpose of this submittal is to provide an overall drainage management plan for development located at the SEC of University Blvd and Gibson Blvd Rd in Albuquerque, NM. The site contains approximately 4.99 acres. The proposed development currently consists of a Chik-Fil-A restaurant at the hard corner of University and Gibson. The middle tract is to remain vacant at this time with plans for a new McDonald's restaurant or similar facility. The easternmost tract will be a Chili's Restaurant. The Chik-Fil-A DMP has already been submitted for review by Merrick & Company. The Chili's DMP has been submitted by Tierra West LLC. We have coordinated these plan with both the Chik-Fil-A and Chili's. These plans have been prepared for the purpose of constructing a new access drive connecting all three developments as well as the required retaining walls.

**EXISTING HYDROLOGIC CONDITIONS**  
The site located at the corner of University/Gibson currently consists of three buildings that were built in approximately 2008. When the economy took a downturn, the project was abandoned and not completed. This site currently drains to several existing inlets that were built on site and the remainder areas drain to University Blvd to the west.

The easternmost site where the Chili's is to be located consists of of an unoccupied car rental facility. This facility drained to the end of the Miles Road Cul-de-Sac, where the flows entered a storm drain system that connects into the existing mains located in Gibson.

Per the calculations table this sheet, the total existing flow discharging from the site is 20.18 cfs during the 100-Yr event.

**PROPOSED HYDROLOGIC CONDITIONS**  
As noted in the introduction, plans for both the Chik-Fil-A Chili's sites have been submitted by Merrick & Company and Tierra West, respectively. For more detailed calculations and the Drainage Management Plans, please refer to those studies.

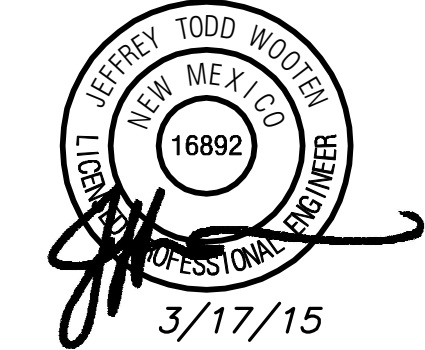
The remainder area not accounted for in either of the above studies consists of Basin B-1. We have designed Basin B-1 in a temporary condition and will be sufficient until such time that the proposed McDonald's restaurant or other facility develops on this tract. An updated study will be required by the engineer of that tract and must be submitted/approved prior to building permit approval. According to the Basin Calculations table this sheet, there is approximately 6.39 cfs in the 100-Yr Storm that will drain to the Temporary Pond B-1. This pond will need to be redesigned as a detention/retention pond at such time that the property is developed. The pond will capture approximately 5,460 cubic feet, which is just under half of the 10-Day volume. The intent of this pond is to capture the first flush volume (see below) and to provide excess capacity for water quality purposes. The overflow from the pond will discharge into an existing Type D Inlet that can adequately pass the 6.39 cfs draining into it.

**FIRST FLUSH CALCULATIONS**  
The first flush impervious area from Basin B-1 is 17,500 SF \* 0.34" / 12 = 495.83 cubic feet. All of Basins B-1 is being routed through Temporary Pond B-1, which has a capacity of 5,460 cubic feet.

First Flush Calculations for the Chik-Fil-A and Chili's sites can be found on the plans prepared by Merrick & Company and Tierra West.

**CONCLUSION**  
This drainage management plan provides for grading and drainage elements which are capable of safely passing the 100-year storm and contains the First Flush from Basin B-1 and meet city requirements. The proposed interim improvements for the site, including the new cross access drive across and proposed retaining walls should not have any negative impacts to facilities downstream. With this submittal, we are requesting grading, paving, and retaining wall permit approvals.

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4700 Lincoln NE, Suite 111  
Albuquerque, NM 87109  
Ph: 505.980.3560  
TX Firm No. F-16140

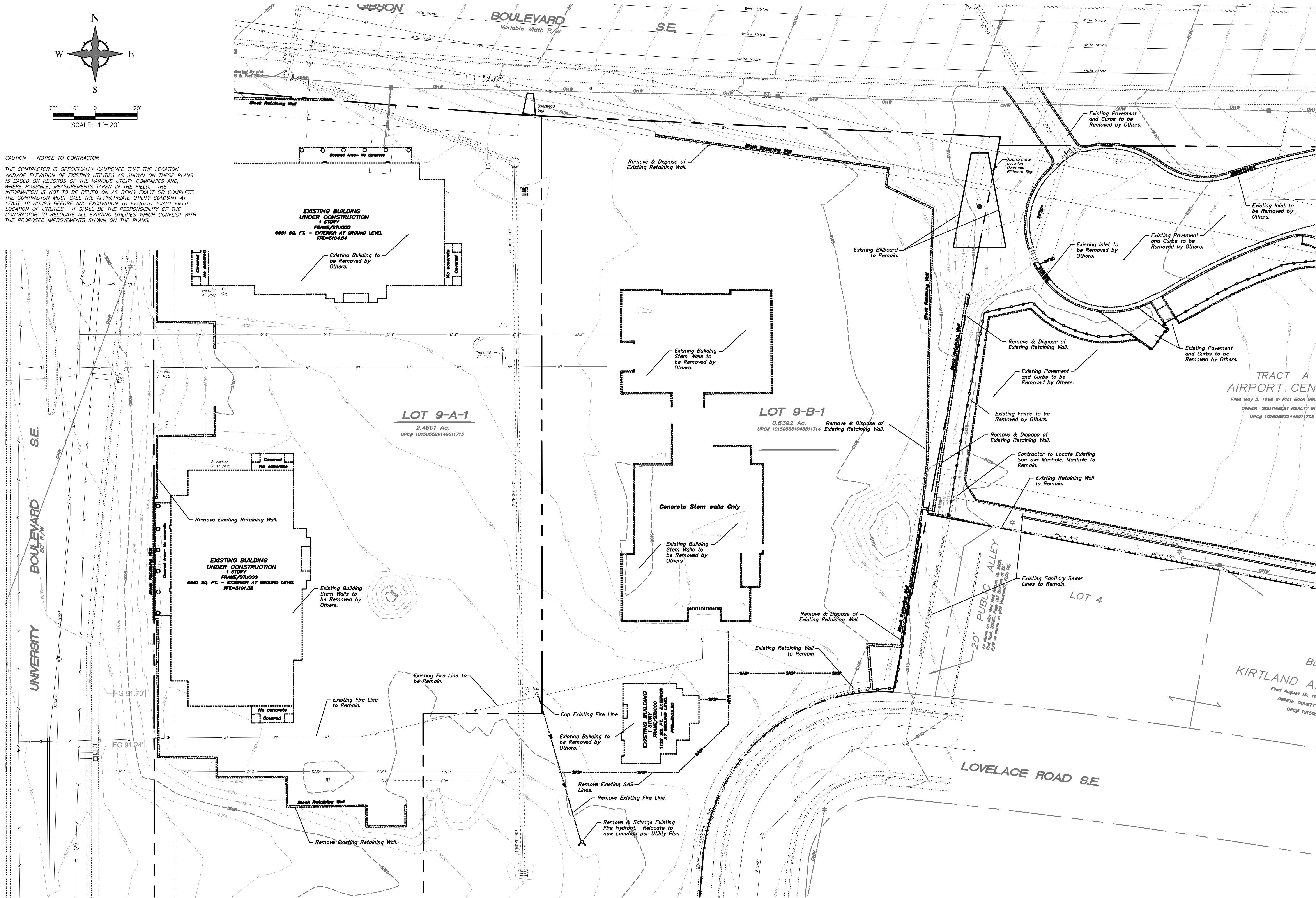


**SEC Gibson at University**  
Cross Access Driveway  
Albuquerque, New Mexico

Project Number 2014055  
Drawn By JW  
Checked JW  
Issue Date March 17, 2015

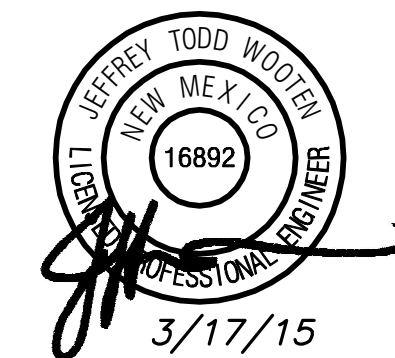
**Overall Drainage Management Plan**





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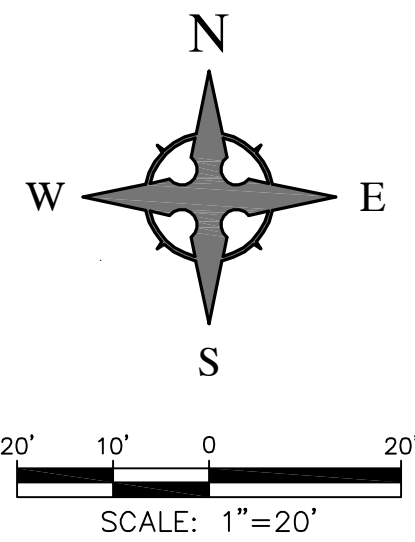


SEC Gibson at University  
Cross Access Driveway  
Albuquerque, New Mexico

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## Demolition Plan



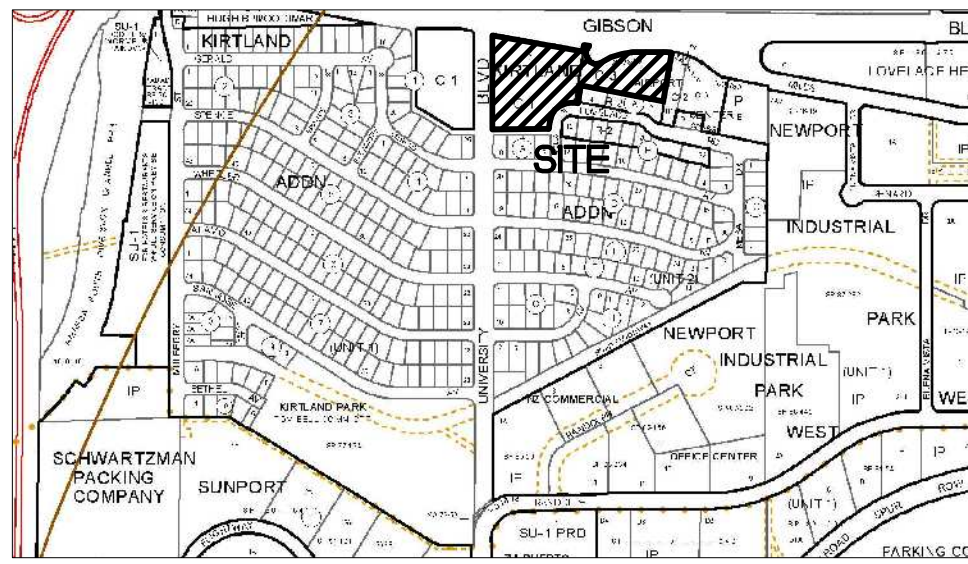


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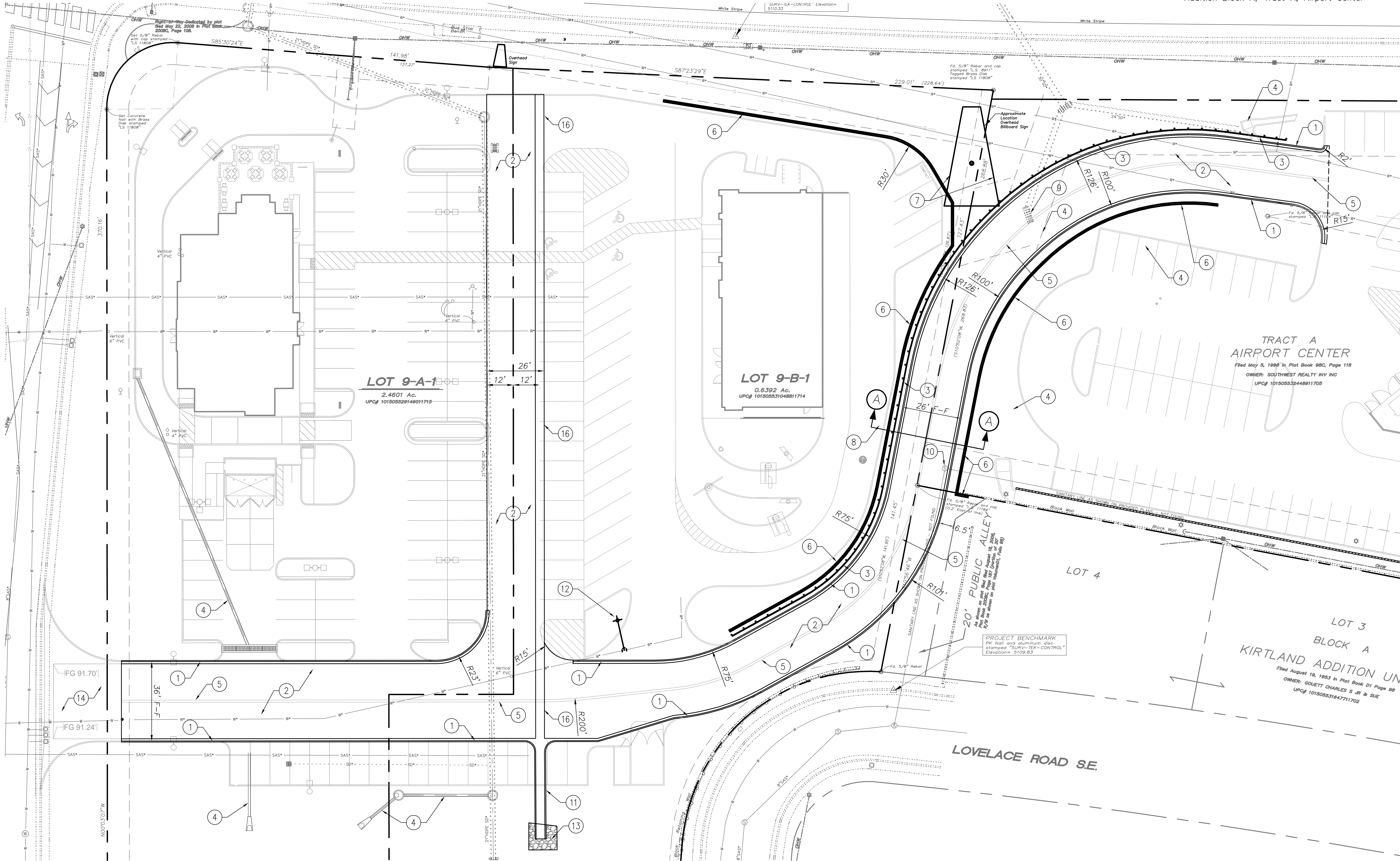
KEYED NOTES

1. INSTALL MEDIAN CURB/GUTTER (6" HIGH) PER COA STD DWG 2415B.
2. ASPHALT PAVEMENT. SEE PAVING DETAILS ON DETAIL SHEET.
3. INSTALL GUIDE RAIL PER DETAIL SHEET.
4. PROPOSED STORM DRAIN BY OTHERS.
5. INSTALL TWO COATS OF DOUBLE 4" WIDE PAINT STRIPES (YELLOW).
6. RETAINING WALL WITH RAILING PER DETAIL SHEET. STEP RAILING AS NEEDED. REF. RETAINING WALL PLANS FOR WALL DETAILS.
7. EXISTING BILLBOARD TO REMAIN.
8. REF DETAIL SHEET FOR SECTION A-A.
9. EXISTING INLET TO BE CONVERTED TO JUNCTION BOX WITH MANHOLE COVER BY OTHERS. CONTRACTOR TO ADJUST RIM TO FINAL PAVEMENT GRADE AND SLOPE WITH PAVEMENT AS NEEDED.
10. CONTRACTOR TO LOCATE EXISTING MANHOLE PRIOR TO CONSTRUCTION AND ADJUST RIM TO FINAL PAVEMENT GRADE AND SLOPE WITH PAVEMENT AS NEEDED.
11. INSTALL 4' WIDE CONCRETE RUNDOWN PER DETAIL ON DETAIL SHEET.
12. NEW FIRE LINE AND HYDRANT. REF UTILITY PLAN.
13. INSTALL 3 SY 3"-6" ROCK RIP-RAP OVER LANDSCAPE FABRIC.
14. DRIVEWAY TO BE PERMITTED BY CHIK-FIL-A. TO BE CONSTRUCTED BY LANDLORD.
15. PROPOSED STORM DRAIN BY LANDLORD. REF UTILITY PLAN.
16. INSTALL 4' WIDE VALLEY GUTTER PER COA STD DWG 2421, SEC A-A.



VICINITY MAP - Zone Map M-15-Z

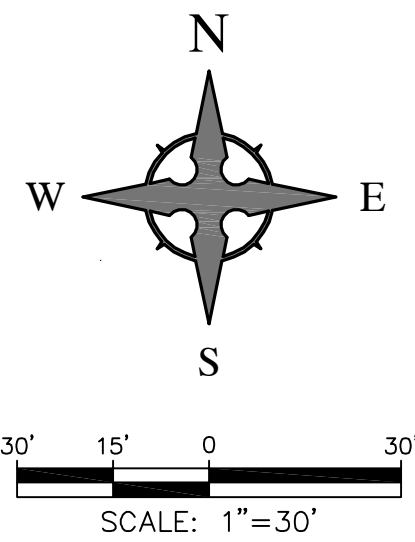
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SEC Gibson at University  
Cross Access Driveway  
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Traffic Circulation  
Layout



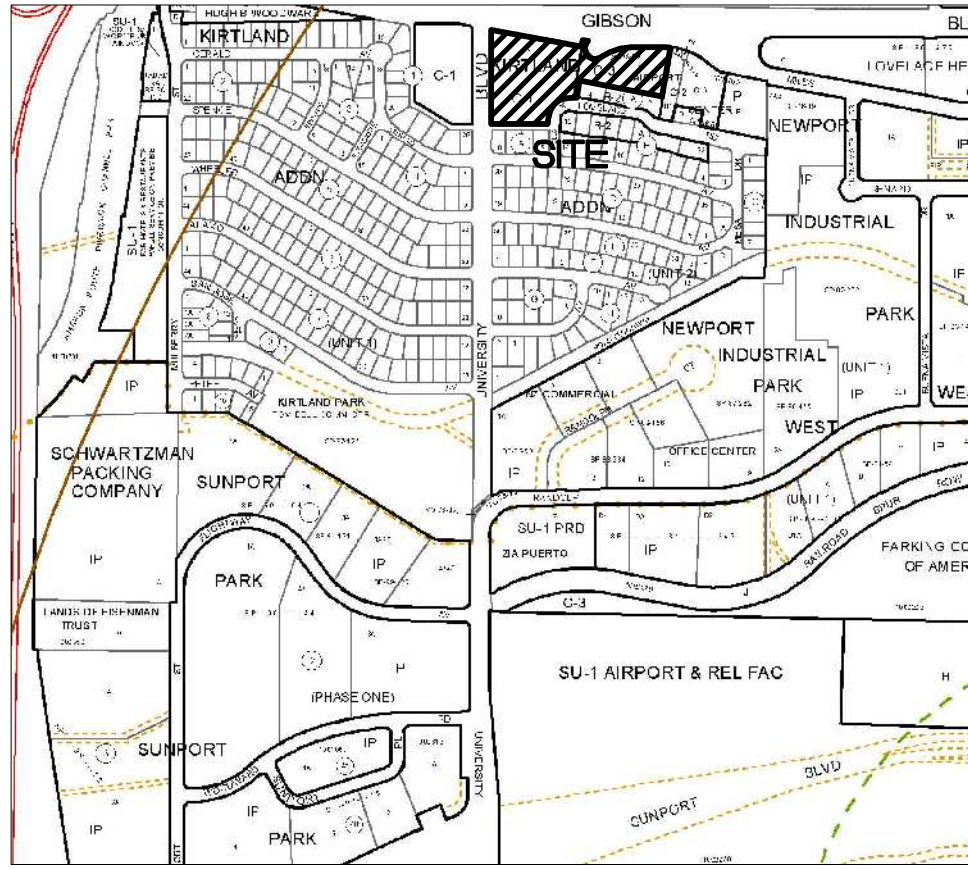


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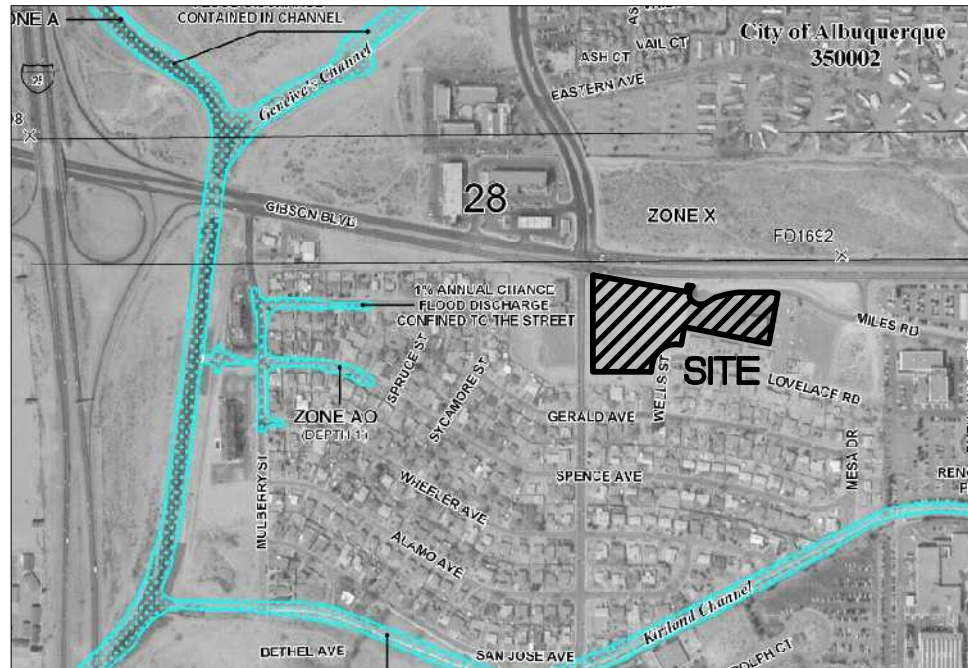
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LEGEND

- ← FLOW ARROW
- 27.8 PROPOSED TOP OF GRADE/PVMT ELEVATIONS
- FL27.8 PROPOSED FLOW LINE/GUTTER ELEVATIONS
- TC27.8 PROPOSED TOP OF CURB ELEVATIONS
- FGH83.40 FINISHED GRADE AT TOP OF WALL
- FGL83.40 FINISHED GRADE AT BOTTOM OF WALL
- 515 EXISTING CONTOUR
- 615 PROPOSED CONTOUR
- EXISTING STORM DRAIN



VICINITY MAP - Zone Map M-15-Z



FIRM MAP 35001C0342G

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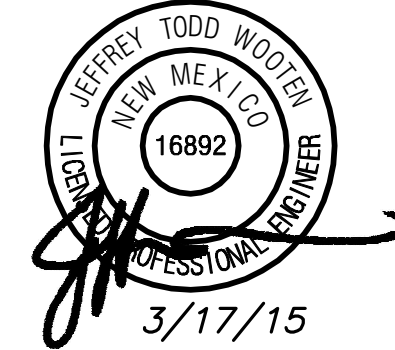
NOTICE TO CONTRACTORS

1. AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN CITY RIGHT-OF-WAY.
2. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF ALBUQUERQUE INTERIM STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 UPDATE NO. 8.
3. TWO WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NM ONE CALL FOR LOCATION OF EXISTING UTILITIES. (NM ONE CALL = '811')
4. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL CONSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
5. BACKFILL COMPACTION SHALL BE ACCORDING TO TRAFFIC/STREET USE.
6. MAINTENANCE OF THESE FACILITIES SHALL BE THE RESPONSIBILITY OF THE OWNER OF THE PROPERTY SERVED.
7. WORK ON ARTERIAL STREETS SHALL BE PERFORMED ON A 24-HOUR BASIS.

GRADING NOTES

1. EXCEPT AS PROVIDED HEREIN, GRADING SHALL BE PERFORMED AT THE ELEVATIONS AND IN ACCORDANCE WITH THE DETAILS SHOWN ON THIS PLAN.
2. THE COST FOR REQUIRED CONSTRUCTION DUST AND EROSION CONTROL MEASURES SHALL BE INCIDENTAL TO THE PROJECT COST.
3. ALL WORK RELATIVE TO FOUNDATION CONSTRUCTION, SITE PREPARATION, AND PAVEMENT INSTALLATION, AS SHOWN ON THIS PLAN, SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "GEOTECHNICAL INVESTIGATION," AS PROVIDED BY THE ARCHITECT OR OWNER. ALL OTHER WORK SHALL, UNLESS OTHERWISE STATED OR PROVIDED FOR HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE PROJECT, (FIRST PRIORITY) SPECIFICATIONS, AND/OR THE CITY OF ALBUQUERQUE (COA) STANDARD SPECIFICATIONS FOR PUBLIC WORKS (SECOND PRIORITY).
4. EARTH SLOPES SHALL NOT EXCEED 3 HORIZONTAL TO 1 VERTICAL UNLESS SHOWN OTHERWISE.
5. IT IS THE INTENT OF THESE PLANS THAT THIS CONTRACTOR SHALL NOT PERFORM ANY WORK OUTSIDE OF THE PROPERTY BOUNDARIES EXCEPT AS REQUIRED BY THIS PLAN.
6. THE CONTRACTOR IS TO ENSURE THAT NO SOIL ERODES FROM THE SITE ONTO ADJACENT PROPERTY OR PUBLIC RIGHT-OF-WAY. THIS SHOULD BE ACHIEVED BY CONSTRUCTING TEMPORARY BERMS OR SILT FENCE AT THE PROPERTY LINES AND WETTING THE SOIL TO PROTECT IT FROM WIND EROSION.
7. A DISPOSAL SITE FOR ANY & ALL EXCESS EXCAVATION MATERIAL, AND UNSUITABLE MATERIAL AND/OR A BORROW SITE CONTAINING ACCEPTABLE FILL MATERIAL SHALL BE OBTAINED BY THE CONTRACTOR IN COMPLIANCE WITH APPLICABLE ENVIRONMENTAL REGULATIONS AND APPROVED BY THE OBSERVER. ALL COSTS INCURRED IN OBTAINING A DISPOSAL OR BORROW SITE AND HAUL TO OR FROM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE.
8. PAVING AND ROADWAY GRADES SHALL BE +/- 0.05' FROM PLAN ELEVATIONS. PAD ELEVATION SHALL BE +/- 0.05' FROM BUILDING PLAN ELEVATION.
9. ALL PROPOSED CONTOURS AND SPOT ELEVATIONS REFLECT TOP OF PAVEMENT ELEVATIONS IN THE PARKING AREA AND MUST BE ADJUSTED FOR PAVEMENT, MEDIANS, AND ISLANDS.
10. VERIFY ALL ELEVATIONS SHOWN ON PLAN FROM BASIS OF ELEVATION CONTROL STATION (IF APPLICABLE) PRIOR TO BEGINNING CONSTRUCTION.
11. THE CONTRACTOR SHALL PROVIDE THE SWPPP DOCUMENT (IF NECESSARY) AND SHALL ABIDE BY ALL LOCAL, STATE, AND FEDERAL LAWS, RULES AND REGULATIONS WHICH APPLY TO THE CONSTRUCTION OF THESE IMPROVEMENTS, INCLUDING EPA REQUIREMENTS WITH RESPECT TO STORM WATER DISCHARGE.

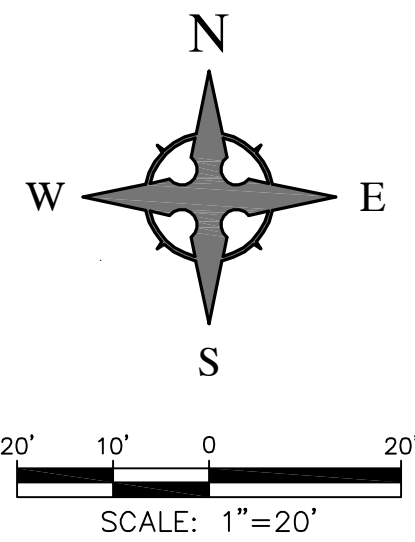
Wooten Engineering  
4700 Lincoln NE, Suite 111  
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TX Firm No. F-16140



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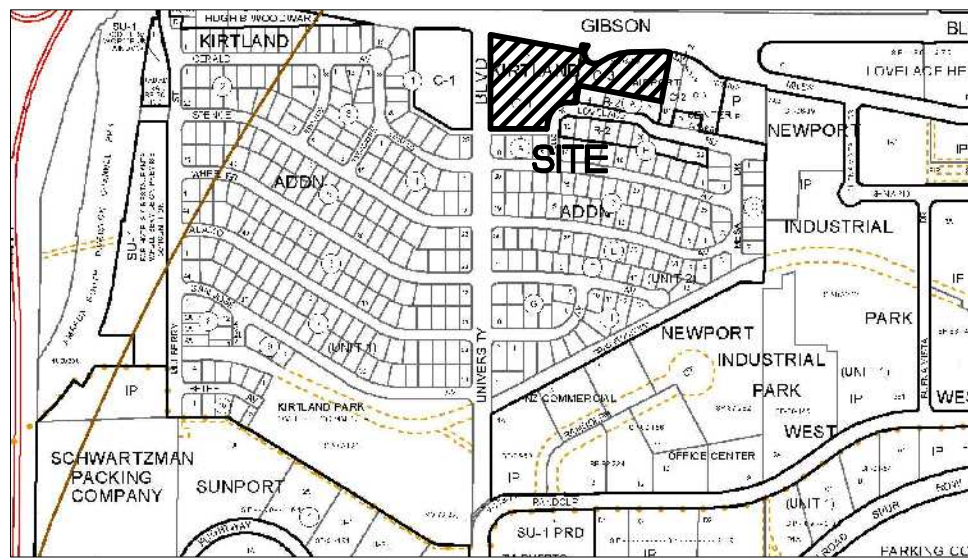
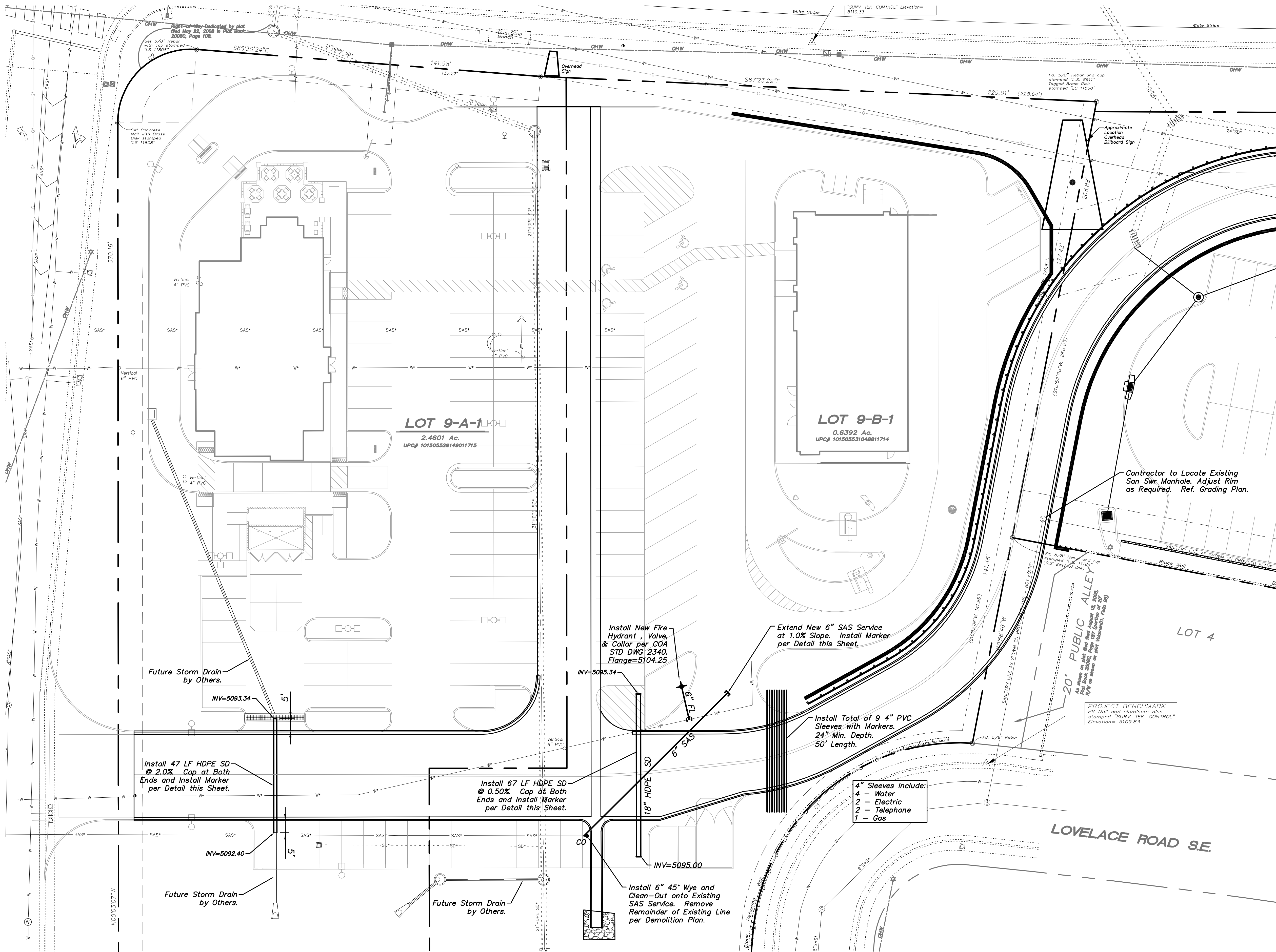
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Checked JW  
Issue Date March 17, 2015  
Grading Plan





CAUTION - NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

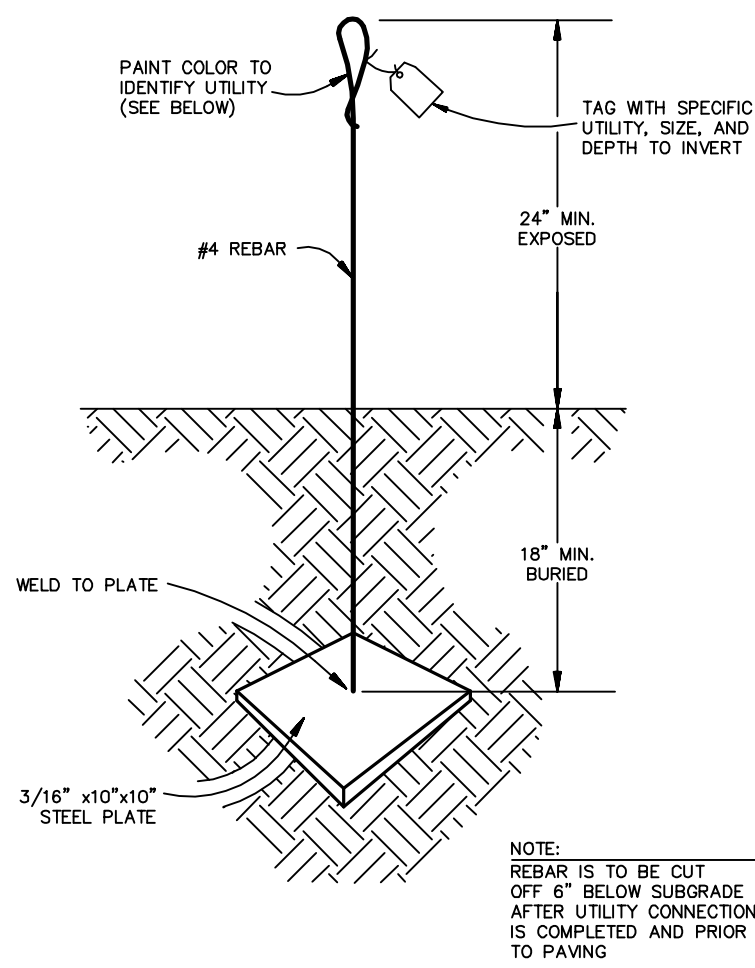


VICINITY MAP - Zone Map M-15-Z

Legal Description: Lots 9-A-1 & 9-B-1, Kirtland Addition Block A; Tract A, Airport Center

GENERAL NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COMPLETE INSTALLATION OF ALL WORK RELATED TO MECHANICAL UTILITIES AS SHOWN ON THIS PLAN INCLUDING: TRENCHING, BACKFILL, SUPPORTS, CLEANOUT PADS, SERVICE STOPS AND BOXES, SERVICE LINES, TESTING, CLEANING, AND STERILIZING. ANY WORK NOT ACCEPTED BY THE ARCHITECT OR ENGINEER DUE TO IMPROPER WORKMANSHIP OR LACK OF PROPER COORDINATION SHALL BE REMOVED AND CORRECTLY INSTALLED AT THE CONTRACTOR'S EXPENSE, AS DIRECTED.
2. MINIMUM DEPTHS OF COVER SHALL BE: 36" FOR WATERLINES AND 48" FOR SEWER, EXCEPT AT BUILDING CONNECTION.
3. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED UNDER CONTRACT SHALL, EXCEPT AS OTHERWISE STATED OR PROVIDED OF HEREON, BE CONSTRUCTED IN ACCORDANCE WITH THE UNIFORM PLUMBING CODE, LATEST EDITION.
4. UTILITY LINES SHALL BE INSTALLED PRIOR TO PAVEMENT, CURB AND GUTTER, AND/OR SIDEWALK, AS APPLICABLE.
5. ROUGH GRADING OF SITE (+0.5') SHALL BE COMPLETED PRIOR TO INSTALLATION OF UTILITY LINES.
6. ALL VALVES SHALL BE ANCHORED PER CITY OF ALBUQUERQUE STANDARDS.
7. FIRE LINES (IF REQUIRED) SHALL USE PIPE MATERIALS UNDERWRITERS LABORATORIES LISTED AND APPROVED FOR FIRE SERVICE.
8. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WATER METER, FIRE LINE, AND SEWER HOOKUP FEES FOR INSTALLATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR UTILITY EXPANSION CHARGES, PRORATA AND OTHER SPECIAL ASSESSMENTS.
9. CONTRACTOR SHALL VERIFY INVERTS AND LOCATIONS OF EXISTING WATER/SAS LINES PRIOR TO BEGINNING WORK. ALL CONFLICTS SHALL BE BROUGHT TO ATTENTION OF THE ENGINEER AND RESOLVED PRIOR TO BEGINNING WORK.



USE AT CAPPED END OF ALL UTILITY SERVICE CONNECTIONS	
UTILITY	COLOR
SEWER	GREEN
WATER	BLUE
STORM DRAIN	WHITE
ELECTRICAL	RED
GAS	YELLOW
TELCO/CATV	ORANGE
FUTURE SITE LIGHTING	PINK
FIBER OPTIC (CAS STATION)	PURPLE

UTILITY MARKER DETAIL

N.T.S.

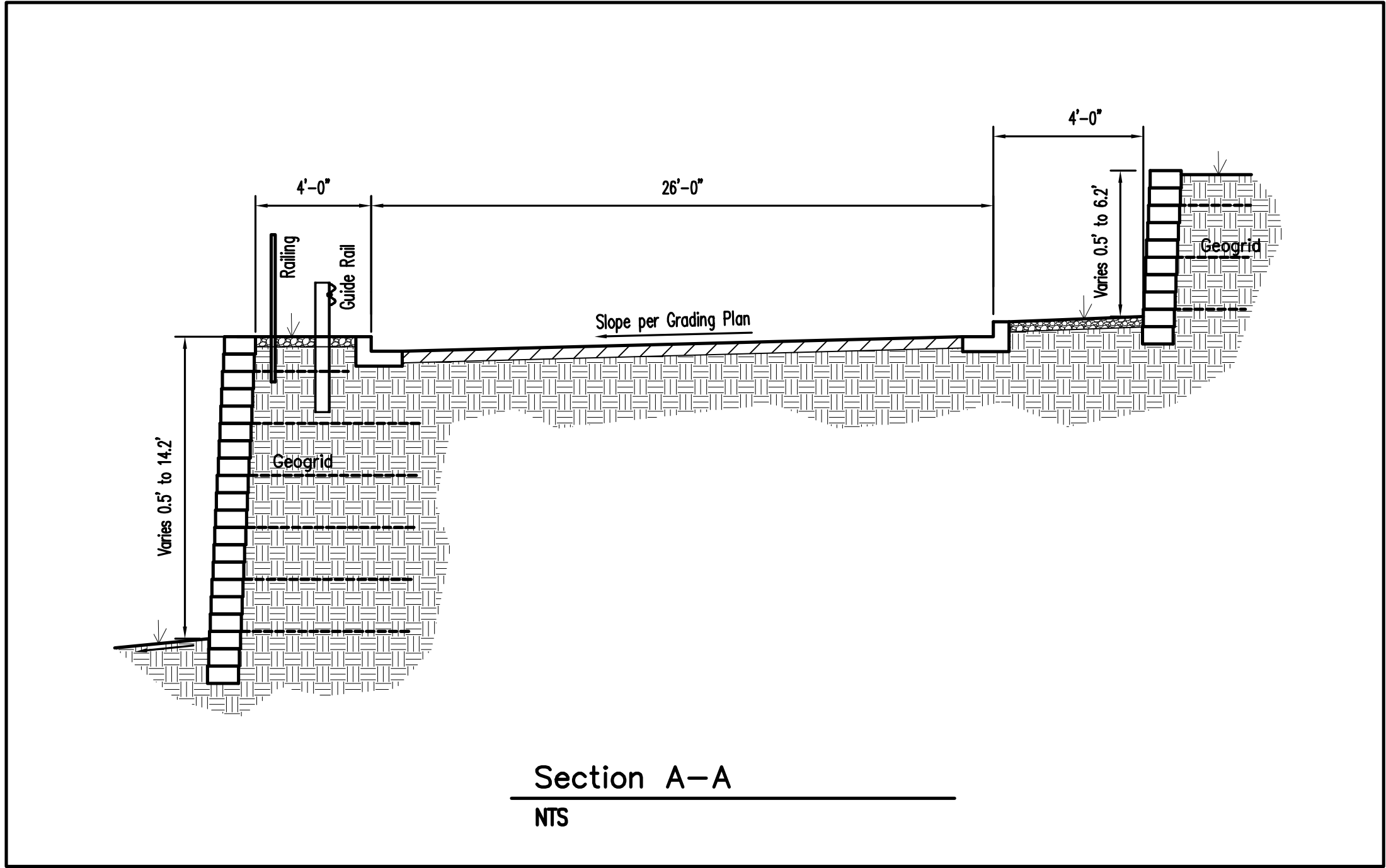
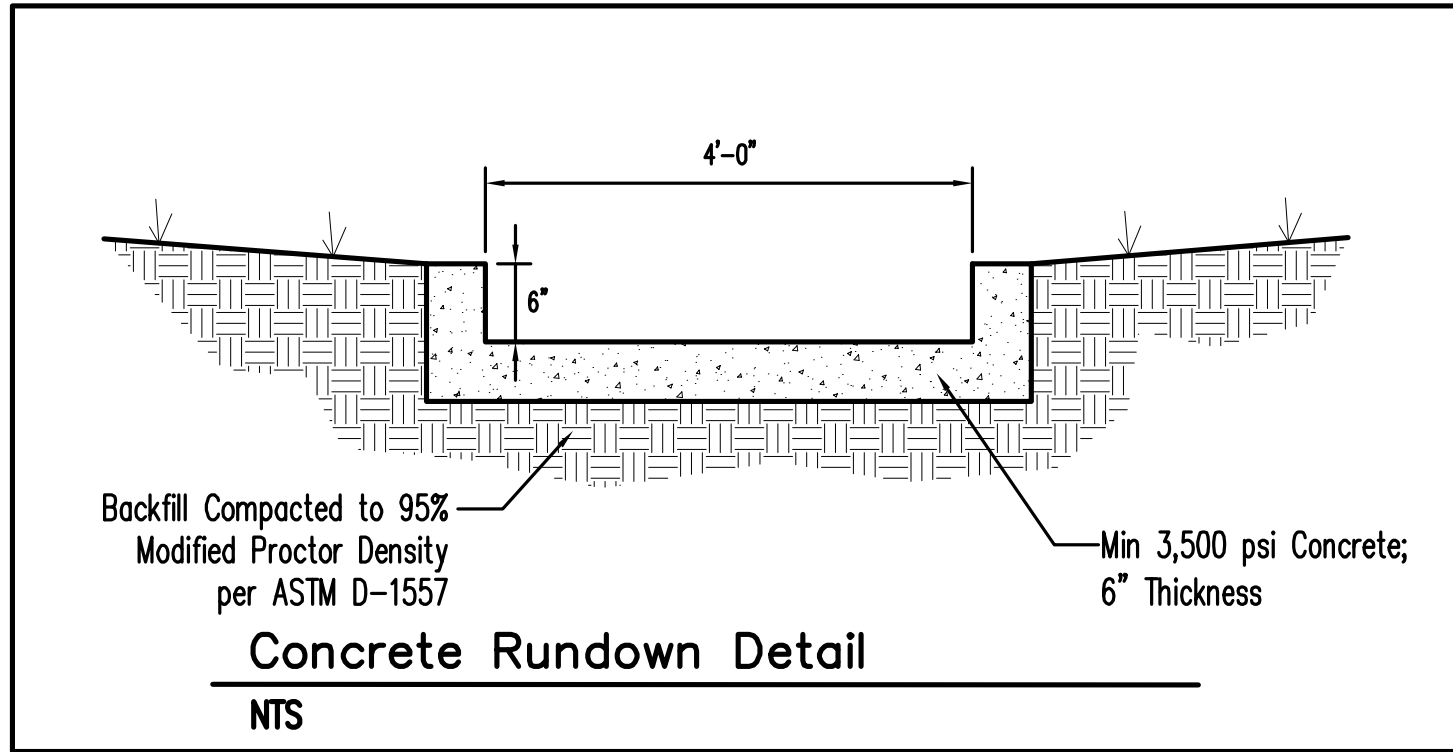
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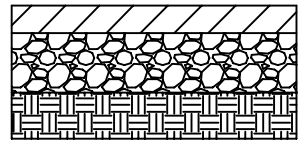


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#### DRIVEWAYS AND TRUCK LOADING PAVEMENT

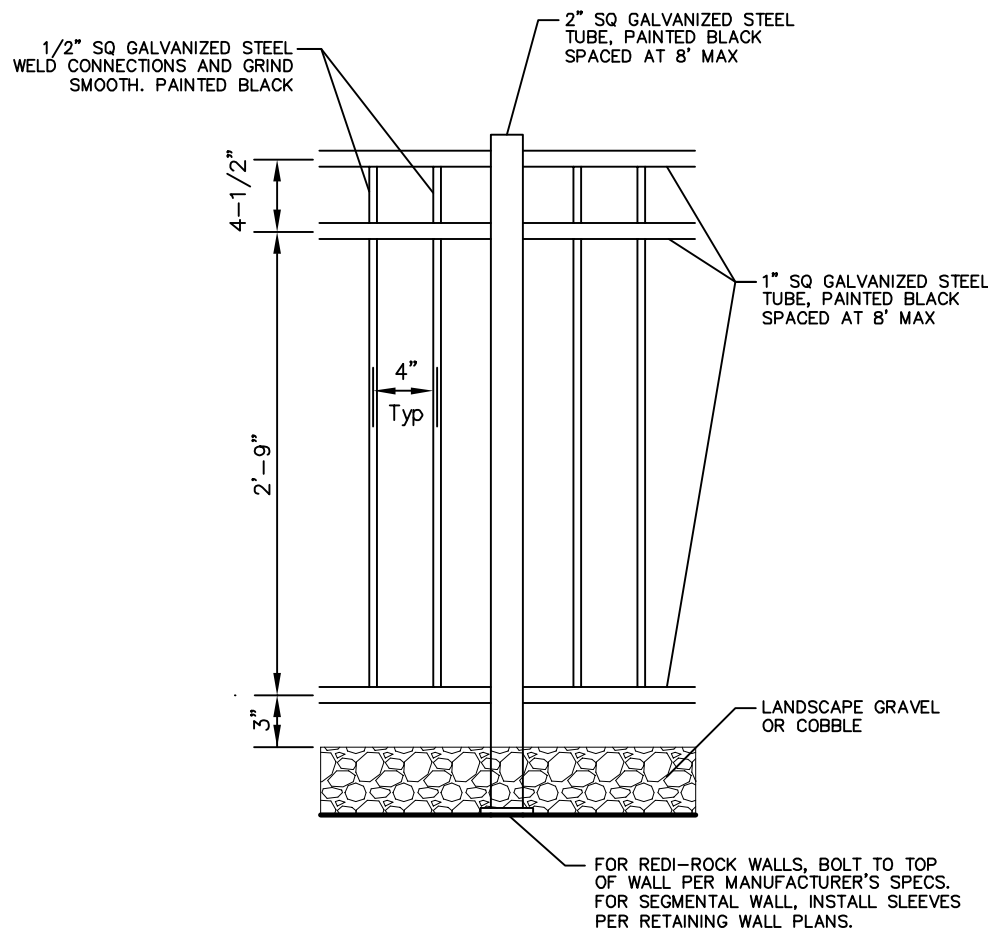


4" ASPHALTIC CONCRETE (2 LIFTS)  
6" AGGREGATE BASE COURSE (2 LIFTS)  
COMPACTED SUBGRADE (12" DEEP)

NOTE: REFERENCE GEOTECHNICAL REPORT FOR ACTUAL RECOMMENDATIONS, CONCRETE STRENGTH, REINFORCEMENT, SUBGRADE TREATMENT, LIME TREATMENT, AND JOINT SPACING.

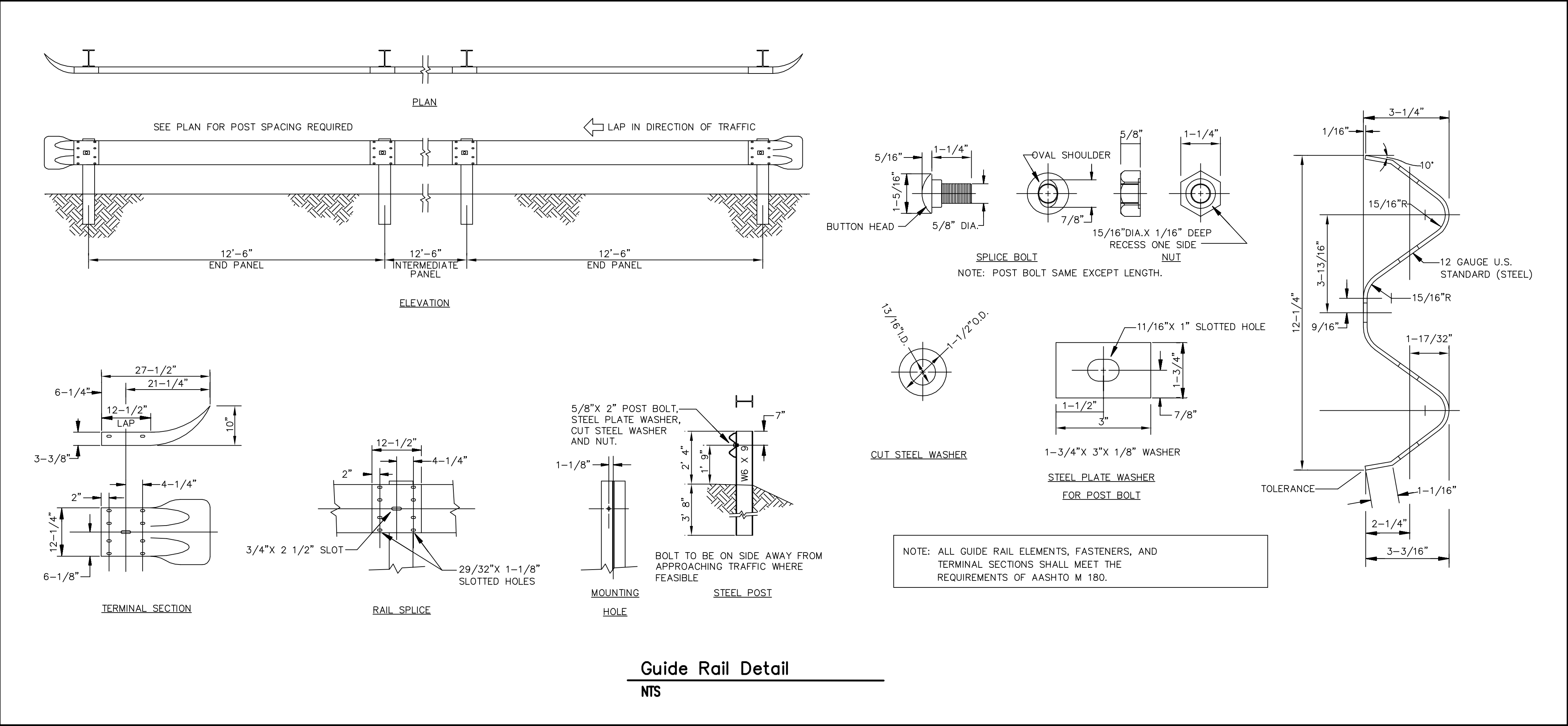
#### Pavement Sections

NTS



#### Railing Detail

NTS

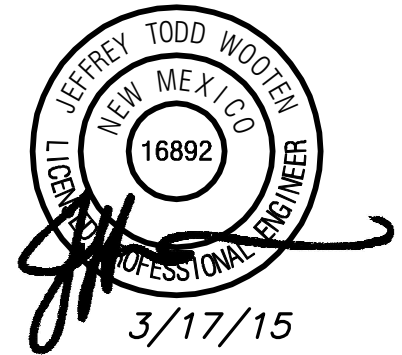


#### Guide Rail Detail

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Scale: 1" = 20'