

County of Bernalillo
State of New Mexico
Technical Services Department
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7/26/2021

Jonathon Kruse, PE, PTOE
Lee Engineering, LLC
8220 San Pedro Dr. NM, Suite 150
Albuquerque, NM 87113

RE: FINAL Aspire Subdivision Traffic Impact Study (PWTR2020-0008)

Dear Mr. Kruse,

BCPW Transportation Planning has reviewed the FINAL Traffic Impact Study (TIS) for Aspire Subdivision dated 6/21/2021. Exhibit "A" shows the vicinity map and site plan for the development. Bernalillo County's review of the subject TIA is limited to impacts at Dennis Chavez Blvd and Condershire Dr.

The summary of off-site recommendations for Dennis Chavez Blvd and Condershire Dr. (Exhibit "B" page iii) provides the information needed to determine development's percent share for Bernalillo County's upcoming turn lane project on Dennis Chavez Blvd/NM 500 involving Grace Vigil Rd. and Condershire Dr. The 6.6% share is derived by combining the total site AM and PM trips divided by the total AM and PM peak hour vehicles for 2027.

The subject TIS is approved base on the following condition:

The developer shall coordinate with Bernalillo County Public Works to enter into a financial contribution agreement for Bernalillo County's Turn Lane Project located along Dennis Chavez Blvd/NM 500 involving Grace Vigil Rd. and Condershire Dr. A copy of this agreement is provided in Exhibit "C".

Thank you,

Julie Luna
Bernalillo County Transportation Planner

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Attachments:

Exhibit "A" – Site Plan – Preliminary Site Layout Plan

Exhibit "B" – TIS Summary of Recommendations

Exhibit "C" – Agreement for Financial Contribution

EXHIBIT A



Figure 1: Vicinity Map

EXHIBIT A

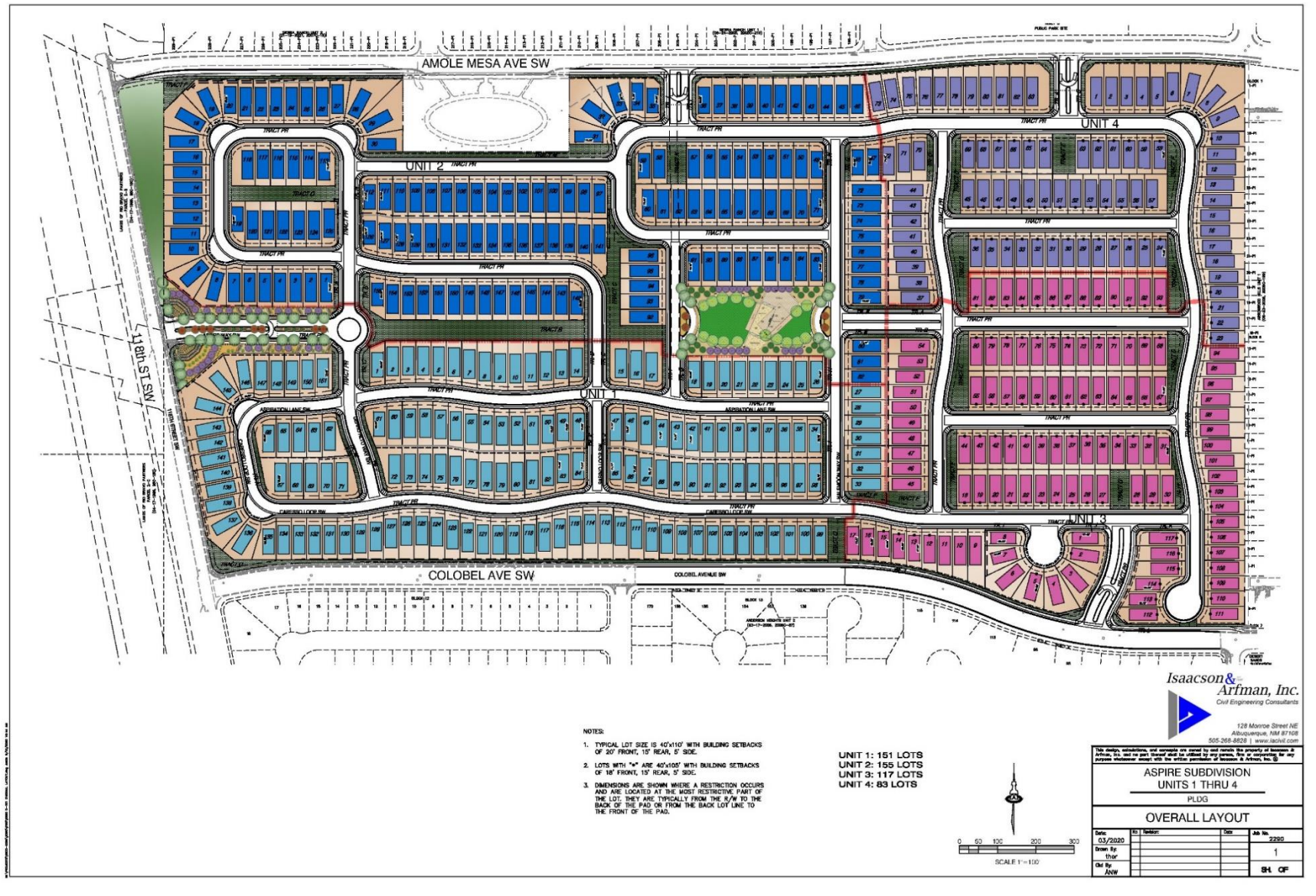


Figure 2: Site Plan

A volume adjustment factor was calculated and applied to study intersections where traffic data was collected during the Covid-19 pandemic (see traffic counts section for details). Traffic data for Dennis Chavez & 118th and Dennis Chavez & 98th was taken from the Ceja Vista Traffic Study. Furthermore, while the Ceja Vista study was completed in 2018, count data was taken from the Atrisco Heritage Academy High School Traffic Study, which collected data in 2017. Therefore, traffic data for Dennis Chavez & 118th and Dennis Chavez & 98th was forecasted from the 2017 counts using MRCOG travel demand growth rates.

SUMMARY OF RECOMMENDATIONS

As shown in the capacity analysis, a general corridor-wide capacity issue is observed to exist on Dennis Chavez Blvd. This contributes to poor levels of service on both Dennis Chavez Blvd and side streets, restricting possible near-term improvements as any additional auxiliary lanes feeding Dennis Chavez Blvd would not have receiving lanes departing the intersections. Currently, Dennis Chavez Blvd is shown in the MRCOG 2040 plan to be widened with an additional eastbound and westbound travel lane; however, funding has not yet been programmed in the current STIP. Widening of Dennis Chavez would be anticipated to include additional eastbound and westbound travel lane(s) and thereby have significant impacts at each traffic signal and intersection. Additional lanes would mitigate poor levels of service and allow for auxiliary lanes to be constructed at intersections. It is therefore recommended that the NMDOT & Bernalillo County consider developing a future project to widen Dennis Chavez Blvd. It should be noted that these overcapacity conditions, specifically due to lack of through capacity on Dennis Chavez Blvd, carry through all phased build-out analyses and thus, the proposed Aspire Development is not solely responsible for those associated movements and intersections operating at an unacceptable LOS and/or over capacity. As a widening project on Rio Bravo has not been developed or funded, capacity analysis did not consider additional lanes on Rio Bravo or at the Dennis Chavez Blvd & Coors Blvd intersection in intersection geometries. The following table and paragraph below details capacity mitigations and recommendations for each intersection.

DENNIS CHAVEZ BLVD & 118TH ST

It is recommended that the traffic signal be periodically re-time and adjusted as developments in the surrounding area are constructed. It is also noted that the development does not contribute traffic to the northbound left and right movements. Additional through lanes and right turn lanes are not recommended at this intersection as receiving lanes is not currently present departing the intersection. Additionally, it is understood that Bernalillo County is in the process of designing minor signal improvements to add flashing yellow arrow left turns at the intersection. However, the details of this project are not currently finalized. Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 7% (170 trips generated / 2608 total peak hour vehicles) during the AM peak and 16% (226 trips generated / 1413 total peak hour vehicles) during the PM peak.

DENNIS CHAVEZ BLVD & 98TH ST

It is understood that a construction project to add additional lanes at 98th & Dennis Chavez Blvd is currently underway as part of the Ceja Vista development. Current construction efforts are widening the intersection to accommodate additional lane geometry, including a southbound left-turn auxiliary lane, eastbound and westbound through lanes, and northbound lanes. It is understood that while the project is constructing an additional southbound left turn lane, the additional lanes will not have receiving lanes on Dennis Chavez Blvd outside of the intersection and, therefore, will not be activated until Dennis Chavez is widened. Auxiliary lanes are being constructed therefore satisfy the above recommendation. Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 6% (172 trips

generated / 2728 total peak hour vehicles) during the AM peak and 10% (231 trips generated / 2416 total peak hour vehicles) during the PM peak.

DENNIS CHAVEZ BLVD & UNSER BLVD

It is recommended that an additional southbound left turn auxiliary lane be constructed at the intersection. Currently, space exists between the southbound right turn lane and the southbound left-turn lane that could be used as an additional left-turn lane. To accommodate the additional southbound left turn lane, it is recommended that the westbound approach be re-stripped moving back existing stop bar and adding additional pavement to receiving eastbound legs will allow for both left south bound left turns to make dual movement. Furthermore, extending eastbound left storage bay to 400' by restriping lanes will provide more capacity. Concept drawing with roadway re-configuration is shown below in Figure 15. It is noted that the development does not contribute traffic to the southbound left turn movement. Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 5% (172 trips generated / 3616 total peak hour vehicles) during the AM peak and 6% (231 trips generated / 4034 total peak hour vehicles) during the PM peak.

DENNIS CHAVEZ & CONDESHIRE BLVD

No recommended improvements as deficiencies exist under 2020 conditions, and the development is not anticipated to contribute traffic to the failing side-street movements. The addition of sidewalks and bike facilities should be considered to meet current street element dimensions set forth by DPM. Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 6% (147 trips generated / 2445 total peak hour vehicles) during the AM peak and 7% (196 trips generated / 2714 total peak hour vehicles) during the PM peak.

DENNIS CHAVEZ & COORS BLVD

The following recommendations are made:

- For the eastbound left, it is recommended that the signal be re-timed with the completion of other improvements at this intersection. It is noted that recommendations below for other movements will allow additional green time to be distributed around the signal.
- For the eastbound through/right turn lane, it is recommended that a dedicated right turn auxiliary lane be constructed and restriping and removing chevron median markings to add additional eastbound through lane. Additionally, for the newly-created right turn lane it is recommended that the sweeping portion of the turn be modified to remove the curvature as much as possible. The development's traffic volume contribution to this movement, based on the fully constructed development, is calculated to be approximately 4.82% of the movement's total combined peak hour traffic volume (53 total peak trips / 1,100 total peak hour vehicles).
- For the westbound left turn, it is recommended that additional capacity be added by restriping existing pavement, currently configured as a chevron striped median between the through and left-turn lane, into an additional left-turn lane. It is also recommended that signal control for this movement be changed from protected-permitted to protected only because of sight distance restrictions.
- For the westbound through, it is recommended that the signal be re-timed with the completion of other improvements. It is noted that recommendations below for other movements will allow additional green time to be distributed around the signal.
- For the northbound left turn, it is recommended that the storage capacity be extended to approximately 400' by reconfiguring median south of the intersection to a "back-to-back" curb configuration. Possibility exists to add an additional turn lane and construct a merge point west of

EXHIBIT B

the intersection; however, this could cause additional safety issues and traffic slow-downs due to vehicles merging on a high-speed roadway. Therefore, dual left-turn lanes for the north to west movement are not recommended until Dennis Chavez has been widened to accommodate dual movements.

- For the northbound through, it is recommended that the signal be re-timed with the completion of other improvements. It is noted that recommendations below for other movements will allow additional green time to be distributed around the signal.
- For the southbound left, it is recommended that the signal be re-timed with the completion of other improvements. It is noted that the southbound left-turn current utilizes dual-auxiliary lanes, and recommendations to add additional capacity for other movements would free additional green time at the traffic signal that could be added to the southbound left-turn movement.

Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 4% (147 trips generated / 4167 total peak hour vehicles) during the AM peak and 4% (196 trips generated / 4916 total peak hour vehicles) during the PM peak.

98TH ST & AMOLE MESA AVE

It is recommended that a traffic signal warrant analysis be performed for the intersection once traffic volumes return to non-COVID conditions. As previously stated, a traffic signal could be warranted in the future as traffic volumes grow. If future operation of intersection becomes unacceptable but does not warrant a traffic signal, then a two-lane roundabout should be considered. Construction of multi-lane roundabout could pose challenges geometrically. Furthermore, cost-to-benefit of installing a roundabout should be examined. See the signal warrant section for more details. Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 9% (105 trips generated / 1183 total peak hour vehicles) during the AM peak and 11% (141 trips generated / 1325 total peak hour vehicles) during the PM peak.

98TH ST & COLOBEL AVE

Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 8% (90 trips generated / 1082 total peak hour vehicles) during the AM peak and 10% (121 trips generated / 1215 total peak hour vehicles) during the PM peak. No recommended improvements.

AMOLE MESA AVE & MESSINA DR

Under 2027 full-build conditions the developments share of contribution to traffic volumes at this intersection is projected to be 33% (131 trips generated / 395 total peak hour vehicles) during the AM peak and 35% (175 trips generated / 506 total peak hour vehicles) during the PM peak. No recommended improvements.

**AGREEMENT FOR FINANCIAL CONTRIBUTION
BETWEEN BERNALILLO COUNTY, NEW MEXICO and
- PRICE LAND DEVELOPMENT GROUP -**

No. PWTR 2021-0008

THIS AGREEMENT is made this _____ day of July, 2021, between the County of Bernalillo, New Mexico (hereinafter referred to as "County") and, **Price Land Development Group, a New Mexico [ENTITY TYPE]**, its heirs, executors, administrators, successors, transferees, and assigns jointly and severally (hereinafter referred to as "Developer") and shall be in full force and effect through 31st day of December, 2023 except as otherwise provided below under TERMINATION.

WHEREAS, the Developer wishes to develop certain lands within the County of Bernalillo, State of New Mexico known as **Aspire Subdivision** (described on Exhibit A as the "Proposed Development", and hereinafter referred to as the "Development"); and

WHEREAS, the Developer has submitted and the County has reviewed a Traffic Impact Assessment for the Development identified as **PWTR2020-0008**; and

WHEREAS, the County has identified the need for constructed improvements located **along NM 500 involving Grace Vigil Rd. and Condershire Rd.** as shown conceptually in **Exhibit B** (the "Turn Lane Improvements"); and

WHEREAS, the County is willing to construct the Turn Lane Improvements, but requires the Developer to mitigate offsite traffic impacts of said Development as a condition of approval of the Traffic Impact Assessment, and the City of Albuquerque Integrated Development Ordinance allows the consideration of review agencies (such as the County) affected by the Development; and

WHEREAS, the Developer is willing to provide a "fair share" monetary contribution towards the Turn Lane Improvements; and

WHEREAS, the Traffic Impact Assessment determines that the Development will contribute 6.6% of the traffic that creates the need for the Turn Lane Improvements; and

WHEREAS, the cost of the Turn Lane Improvements is not yet known; and

WHEREAS, the County requires the execution of an Agreement by the Developer for a financial contribution to the Turn Lane Improvements prior to proceeding with the evaluation, design or construction of the Turn Lane Improvements;

NOW, THEREFORE, in consideration of the above, the County and Developer hereby agree as follows:

1. The County shall, on or before the **31st day of December, 2023**, provide to the Developer an engineer's cost estimate for the Turn Lane Improvements.
2. The Developer shall, upon receipt of the engineer's cost estimate, make a financial contribution

to the County in the amount of 6.6% of the cost of the Turn Lane Improvements as provided in the engineer's cost estimate. If the engineer's cost estimate is over one million dollars, the financial contribution of the Developer shall be \$66,000, plus 3.3% of the amount by which the engineer's cost estimate exceeds one million dollars.

3. The County shall issue a receipt of payment for the financial contribution.

4. This Agreement shall not be assigned except with the written consent of the parties hereto. If so assigned, this Agreement, shall extend to and be binding upon the heirs, executors, administrators, successors, and assigns of the parties hereto.

5. In the event of the sale, conveyance, or assignment of the Development or any portion thereof, the County will not release the Developer from its obligations under this Agreement and will continue to hold the Developer responsible for the payment of the Developer's contribution hereunder until a successor in interest to the Developer has entered into replacement for this Agreement with the County in form and substance acceptable to the County, obligating such successor to the commitments of the Developer hereunder. At such time as replacement agreement has been executed the County will release the Developer from its obligations hereunder.

6. TERMINATION. This Agreement shall terminate if: (a) prior to the County entering into an agreement or otherwise committing or expending funds for the design or construction of the Turn Lane Improvements, the Developer advises the County in writing that the Development is not moving forward, or (b) the County elects in its discretion not to pursue the construction of the Turn Lane Improvements.

7. If any part of this Agreement is held to be invalid or unenforceable, such as holding shall not affect the validity or enforceability of any other part of this Agreement as long as the remainder of the Agreement is reasonably capable of completion.

8. NOTICES. Notice required to be given under this Agreement shall be given to the parties as shown below the signatures of each party below.

9. This Agreement shall not be changed or amended unless by a written instrument executed by all parties hereto.

EXHIBIT C

IN WITNESS WHEREOF, the parties hereto have executed this Agreement as of the date first above written.

COUNTY COMMISSION: **BERNALILLO COUNTY PUBLIC WORKS DIVISION**
2400 Broadway SE
Albuquerque, NM 87102

Bernalillo County Public Works Division

Date

DEVELOPER: PRICE LAND DEVELOPMENT GROUP
[Address]

Turn Lane Project Conceptual Drawing (Referred to as Exhibit B in Financial Contribution Agreement)