

GENERAL NOTES:

- 1.) NO PERIMETER BOUNDARY CORNERS HAVE BEEN FIELD ESTABLISHED PER THIS SURVEY OF THE SUBJECT PROPERTY.
- 2.) NO SEARCH HAS BEEN MADE FOR BASEMENTS OF RECORD OTHER THAN SHOWN HEREON

LEGEND:

TOP OF CURB ELEVATION = $TC = 07.53$
 CURB FLOWLINE ELEVATION = $FL = 07.52$
 EXISTING SPOT ELEVATION = $SE = 07.41$
 EXISTING CONTOUR ELEVATION = $CE = 08.0$
 PROPOSED SPOT ELEVATION = $PS = 07.73$
 PROPOSED CONTOUR ELEVATION = $PC = 08.0$
 PROPOSED OR EXISTING CONCRETE SURFACE = S
 EXISTING FENCE LINE = F

- A → EXISTING LANDSCAPED AREA
- B → PROVIDE 6" WIDE CONCRETE RESINER WALL WITH TOP OF WALL ELEVATION @ 07.52

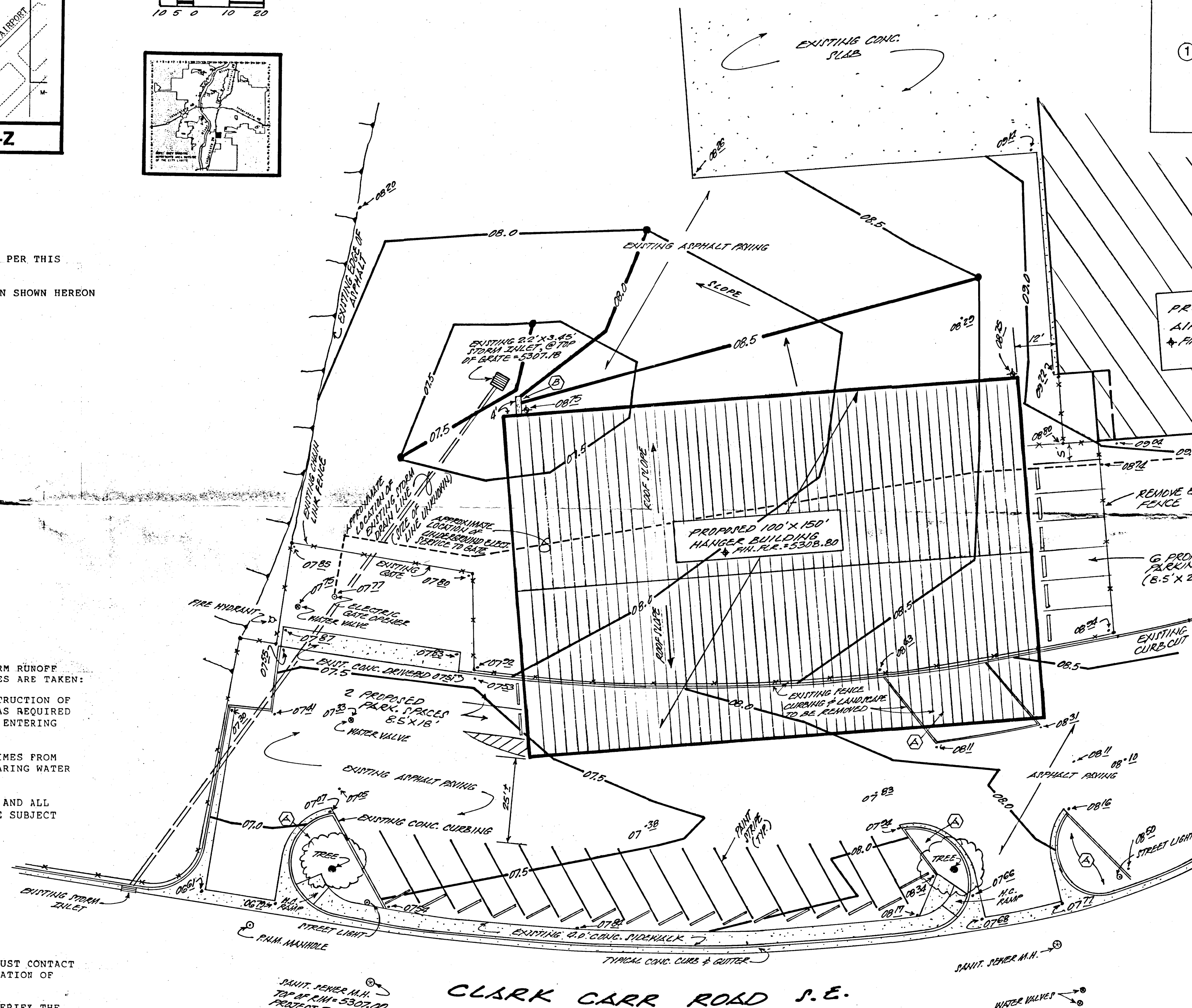
EROSION CONTROL MEASURES:

THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR MANAGEMENT OF STORM RUNOFF DURING CONSTRUCTION; HE SHALL INSURE THAT THE FOLLOWING MEASURES ARE TAKEN:

- 1.) ADJACENT PROPERTY SHALL BE PROTECTED AT ALL TIMES BY CONSTRUCTION OF BERMS, DIKES, SWALES, PONDS, AND OTHER TEMPORARY GRADING AS REQUIRED TO PREVENT STORM RUNOFF FROM LEAVING THE SUBJECT SITE AND ENTERING ADJACENT PROPERTIES.
- 2.) ADJACENT PUBLIC RIGHT-OF-WAYS SHALL BE PROTECTED AT ALL TIMES FROM STORM WATER RUNOFF FROM THE SUBJECT SITE. NO SEDIMENT BEARING WATER SHALL BE PERMITTED TO ENTER PUBLIC STREET RIGHT-OF-WAYS.
- 3.) THE CONTRACTOR SHALL IMMEDIATELY AND THOROUGHLY REMOVE ANY AND ALL SEDIMENT FROM PUBLIC STREETS THAT HAS BEEN ERODED FROM THE SUBJECT SITE AND DEPOSITED THEREON.

CONSTRUCTION NOTES:

- 1.) TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT LINE LOCATING SERVICE AT 260-1990 FOR THE ACTUAL FIELD LOCATION OF THE EXISTING SURFACE OR SUB-SURFACE UTILITIES.
- 2.) PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATION(S) OF ALL POTENTIAL OBSTRUCTIONS; SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM OF DELAY.
- 3.) ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
- 4.) ALL CONSTRUCTION WITHIN PUBLIC STREET RIGHT-OF-WAY(S) SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE CITY OF ALBUQUERQUE/BERNALILLO COUNTY STANDARDS AND PROCEDURES.

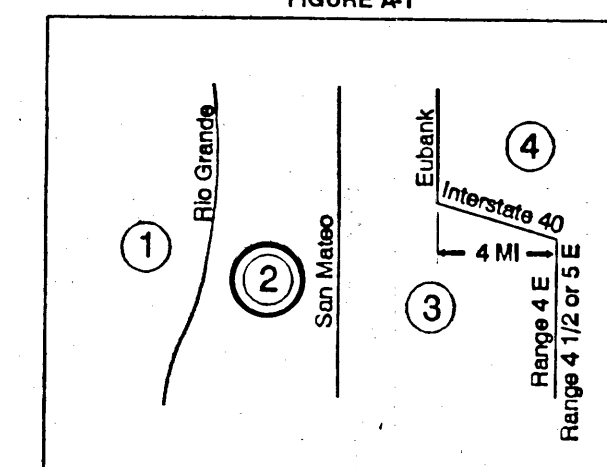


A.1 PRECIPITATION ZONES

Bernalillo County's four precipitation zones are indicated in TABLE A-1 and on FIGURE A-1.

TABLE A-1. PRECIPITATION ZONES	
Zone	Location
1	West of the Rio Grande
2	Between the Rio Grande and San Mateo
3	Between San Mateo and Eubank, North of Interstate 40; and between San Mateo and the East boundary of Range 4 East, South of Interstate 40
4	East of Eubank, North of Interstate 40; and East of the East boundary of Range 4 East, South of Interstate 40

FIGURE A-1



Where a watershed extends across a zone boundary, use the zone which contains the largest portion of the watershed.

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TABLE A-4. LAND TREATMENTS	
Treatment	Land Condition
A	Soil uncompacted by human activity with 0 to 10 percent slopes. Native grasses, weeds and shrubs in typical densities with minimal disturbance to grading, groundcover and infiltration capacity. Croplands. Unlined arroyos.
B	Irrigated lawns, parks and golf courses with 0 to 10 percent slopes. Native grasses, weeds and shrubs, and soil uncompacted by human activity with slopes greater than 10 percent and less than 20 percent.
C	Soil compacted by human activity. Minimal vegetation. Unpaved parking, roads, trails. Most vacant lots. Gravel or rock on plastic (desert landscaping). Irrigated lawns and parks with slopes greater than 10 percent. Native grasses, weeds and shrubs, and soil uncompacted by human activity with slopes at 20 percent or greater. Native grass, weed and shrub areas with clay or clay loam soils and other soils of very low permeability as classified by SCS Hydrologic Soil Group D.
D	Impervious areas, pavement and roofs.

Most watersheds contain a mix of land treatments. To determine proportional treatments, measure respective subareas. In lieu of specific measurement for treatment D, the areal percentages in TABLE A-5 may be employed.

TABLE A-5. PEAK DISCHARGE (cfs/acre)				
Zone	Treatment			100-YR [2-YR, 10-YR]
	A	B	C	D
1	1.29 [0.00, 0.24]	2.03 [0.03, 0.78]	2.87 [0.47, 1.49]	4.37 [1.69, 2.89]
2	1.56 [0.00, 0.38]	2.28 [0.00, 0.95]	3.14 [0.00, 1.71]	4.70 [1.85, 3.14]
3	1.87 [0.00, 0.58]	2.80 [0.21, 1.19]	3.45 [0.78, 2.00]	5.02 [2.04, 3.39]
4	2.20 [0.05, 0.87]	2.92 [0.38, 1.45]	3.73 [1.00, 2.26]	5.25 [2.17, 3.57]

TABLE A-10. PEAK INTENSITY (MMH at $t_c = 0.2$ hour)		
Zone	Intensity	100-YR [2-YR, 10-YR]
1	4.70 [1.84, 3.14]	
2	5.05 [2.04, 3.41]	
3	5.38 [2.21, 3.65]	
4	5.81 [2.34, 3.83]	

LEGAL DESCRIPTION:

COMPRISING A WESTERLY PORTION OF THE ALBUQUERQUE INTERNATIONAL AIRPORT, (WEST OF AND ADJACENT TO THE PRESBYTERIAN AIR AMBULANCE FACILITIES BUILDING), IN THE CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO.

BENCH MARK REFERENCE:

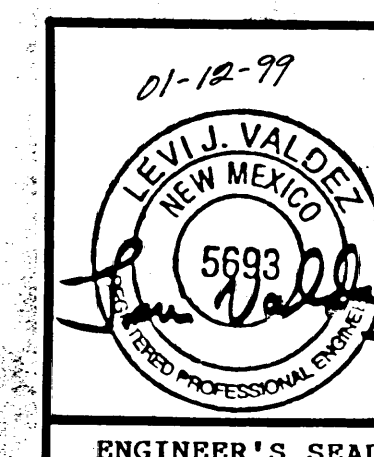
AS STATION "1-N16 RESET", IN SECTION 10, TOWNSHIP 9 NORTH, RANGE 3 EAST, N.M.P.M., M.S.L.D. (NAD 1927) ELEVATION = 5308.73; PROJECT T.B.M. AS SHOWN ON THE PLAN HEREON.

DRAINAGE COMMENTS:

AS SHOWN ON THE VICINITY MAP HEREON, THE SUBJECT SITE IS LOCATED AT THE WESTERLY SIDE OF THE ALBUQUERQUE INTERNATIONAL AIRPORT, ON THE NORTH SIDE OF CLARK CARR ROAD N.E. AND EAST OF UNIVERSITY BLVD. N.E., IN THE CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO.

THE SUBJECT PROJECT CONSISTS OF CONSTRUCTING A PROPOSED METAL BUILDING FOR AN AIRPLANE HANGER FACILITY; SAID BUILDING WILL BE CONSTRUCTED OVER AN AREA THAT IS PRESENTLY AN ASPHALT PAVED SURFACE THAT IS USED FOR PARKING OF SMALL PRIVATE AIRPLANES.

BEING THAT THE PROPOSED BUILDING WILL BE CONSTRUCTED OVER AN EXISTING IMPERVIOUS AREA THERE WILL BE NO INCREASE OF STORM FLOWS GENERATED BY SAID NEW IMPROVEMENTS; FLOWS WILL CONTINUE TO BE DISCHARGED AS EXISTING AND SHOWN ON THE PLAN HEREON.



A SITE PLAN & DRAINAGE PLAN
FOR
A PROPOSED HANGER BUILDING
CUTTER FLYING SERVICE, INC.
ALBUQUERQUE, NEW MEXICO
JANUARY, 1999

RECEIVED
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HYDROLOGY SECTION